



**Riverside, 4 Canning Road, Lowestoft, Suffolk,
NR33 0EQ**

Cabinet

Members:

Councillor Steve Gallant (Leader)
Councillor Craig Rivett (Deputy Leader and Economic Development)
Councillor Norman Brooks (Transport)
Councillor Stephen Burroughes (Customer Experience, ICT and Commercial Partnerships)
Councillor Maurice Cook (Resources)
Councillor Richard Kerry (Housing)
Councillor James Mallinder (The Environment)
Councillor David Ritchie (Planning & Coastal Management)
Councillor Mary Rudd (Community Health)
Councillor Letitia Smith (Communities, Leisure and Tourism)

Members are invited to a **Meeting of the Cabinet** to be held in the Conference Room, Riverside, Lowestoft, on **Tuesday, 4 October 2022 at 6.30pm**

This meeting will be broadcast to the public via the East Suffolk YouTube Channel at <https://youtu.be/OdgJugssTM>

An Agenda is set out below.

Part One – Open to the Public

Pages

1 Apologies for Absence

To receive apologies for absence, if any.

2 Declarations of Interest

Members and Officers are invited to make any declarations of interests, and the nature of that interest, that they may have in relation to items on the Agenda and are also reminded to make any declarations at any stage during the Meeting if it becomes apparent that this may be required when a particular item or issue is considered.

3 Announcements

To receive any announcements.

4 Minutes

To confirm as a correct record the Minutes of the Meeting held on 6 September 2022

1 - 14

KEY DECISIONS

5 East Suffolk Cycling and Walking Strategy ES/1291

Report of the Cabinet Member with responsibility for Planning and Coastal Management

15 - 1603

6 Budget Monitoring Report Quarter 2 2022/23 ES/1292

Report of the Cabinet Member with responsibility for Resources

**1604 -
1627**

7 Exempt/Confidential Items

It is recommended that under Section 100A(4) of the Local Government Act 1972 (as amended) the public be excluded from the meeting for the following items of business on the grounds that they involve the likely disclosure of exempt information as defined in Paragraph 3 of Part 1 of Schedule 12A of the Act.

Part Two – Exempt/Confidential

Pages

8 Exempt Minutes

- Information relating to the financial or business affairs of any particular person (including the authority holding that information).
- Information relating to any consultations or negotiations, or contemplated consultations or negotiations, in connection with any labour relations matter arising between the authority or a Minister of the Crown and employees of, or office holders under, the authority.

KEY DECISIONS

9 Debt Write Off

- Information relating to the financial or business affairs of any particular person (including the authority holding that information).

10 Halesworth Road, Ilketshall Proposed Housing Development

- Information relating to the financial or business affairs of any particular person (including the authority holding that information).

11 Programme of Fire Safety Works for Social Housing: Procurement of Works from Specialist Contractors

- Information relating to the financial or business affairs of any particular person (including the authority holding that information).

Close



Stephen Baker, Chief Executive

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The national Charter and Charter Plus Awards for Elected Member Development
 East Suffolk Council is committed to achieving excellence in elected member development
www.local.gov.uk/Community-Leadership

Unconfirmed



Minutes of a Meeting of the **Cabinet** held in the Deben Conference Room, East Suffolk House, on **Tuesday, 6 September 2022 at 6:30 pm**

Members of the Cabinet present:

Councillor Norman Brooks, Councillor Stephen Burroughes, Councillor Maurice Cook, Councillor Steve Gallant, Councillor Richard Kerry, Councillor James Mallinder, Councillor David Ritchie, Councillor Mary Rudd, Councillor Letitia Smith

Other Members present:

Councillor Edward Back, Councillor David Beavan, Councillor Judy Cloke, Councillor Tony Cooper, Councillor Mike Deacon, Councillor Louise Gooch, Councillor Ed Thompson, Councillor Steve Wiles

Officers present:

Chris Bing (Head of Legal and Democratic Services), Kerry Blair (Head of Operations), Sharon Bleese (Coastal Manager - South), Neil Cockshaw (Programmes and Partnerships Manager), Karen Cook (Democratic Services Manager), Andy Jarvis (Strategic Director), Bridget Law (Programme Manager), Ryan Lee-Hagger (Senior Procurement Officer), Sue Meeken (Political Group Support Officer (Labour)), Brian Mew (Chief Finance Officer & Section 151 Officer), Amy Moye (Contract and Supply Specialist), Agnes Ogundiran (Conservative Political Group Support Officer), Nicola Parrish (Infrastructure Delivery Manager), Tom Potter (Communications and Marketing Officer), Lorraine Rogers (Deputy Chief Finance Officer), Tim Snook (Leisure Development Partnership Manager), Alli Stone (Democratic Services Officer), Ryan Taylor (Development Officer), Marie Webster-Fitch (Economic Development Manager), Ben Woolnough (Planning Manager (Development Management))

1 Apologies for Absence

Apologies for absence were received from Councillor Rivett and Councillor Jepson.

2 Declarations of Interest

There were no declarations of interest.

3 Minutes

It was by consensus

RESOLVED

That the Minutes of the Meeting held on 5 July 2022 be agreed as a correct record and signed by the Chairman.

4 Announcements

The Leader acknowledged that Her Majesty Queen Elizabeth II had appointed a new Prime Minister earlier that day and that over the coming days both a new Cabinet and plans to address the issues faced by the nations would be announced by the Government. The Leader wished the new Prime Minister and her Cabinet well.

The Cabinet Member with responsibility for Community Health reminded everyone to get their winter flu jab, citing the importance of doing so as COVID-19 had not gone away.

5 Formation of Project Board – Resilient Coasts Project

Cabinet received report **ES/1255** by the Cabinet Member with responsibility for Planning and Coastal Management, the purpose of which was to recommend the formation of a Resilient Coasts Project Board and governance structure, led by East Suffolk Council (ESC) as Lead Authority.

The report, which was introduced by the Cabinet Member with responsibility for Transport, set out that the Resilient Coasts Project was funded by the Department for the Environment, Food and Rural Affairs (Defra) and the Environment Agency (EA) through an innovation programme and was one of 25 projects nationally that had received funding, and was one of only five projects on the coastline.

Cabinet was reminded of the dynamic coastline in the district and the ongoing issues with flooding and erosion, exacerbated by rising sea levels and climate change. The project would be run jointly with Great Yarmouth Borough Council (GYBC), with £9.1m of funding provided for six years. The project's aims were stated as to provide the tools and mechanisms to support communities at risk of flooding and coastal erosion and would create a resilience master plan.

The Cabinet Member with responsibility for Planning and Coastal Management noted that a draft business case had been approved by Cabinet at its meeting in June 2022 and this business case had since been provisionally approved by the EA. The project would be delivered by Coastal Partnership East (CPE) and would concentrate on four pilot locations - Southwold, Hemsby, Great Yarmouth and Thorpeness, along with twin location as Pakefield, Shotley Gate and Corton with Gunton.

Cabinet was advised that the Cabinet Member with responsibility for Planning and Coastal Management would chair the project board, which would be made up of members, officers and other partners. It was confirmed that the project board would make recommendations and not hold decision-making responsibilities for ESC spending.

The Cabinet Member with responsibility for Planning and Coastal Management highlighted recruitment that had recently taken place at CPE, funded by the project, which would both support the project and increase the partnership's capacity.

Cabinet gave its full support for the recommendations. The Cabinet Member with responsibility for Housing praised the report and queried the inclusion of Shotley Gate in the project; the Coastal Manager (South) explained that the project provided an opportunity to share learning with twin areas and highlighted that Shotley Gate suffered from issues in common with some of the areas included in the project. The Cabinet Member with responsibility for the Environment welcomed the project as it would enable the Council to support communities to adapt to issues in a changed climate.

Councillor Deacon expressed support for the recommendation and sought further information on the composition of the project board; the Coastal Manager noted that the full governance structure was set out in the report and the first board meeting would inform the Key Stakeholder Group and Strategic Steering Group.

The Coastal Manager confirmed that Ward Members from the selected areas would form part of the board and the fine detail of this would be agreed outside of this meeting. It was anticipated that parish councils would engage with the project through the Key Stakeholder Group; the Coastal Manager said the project was designed to be co-creative and collaborative and that parish councils would be critical in the work of the project.

Councillor Beavan was in favour of the project and was of the view that relevant parties in Southwold would be involved; he looked forward to joining the board as Ward Member for Southwold.

The Leader considered that the project was a prime example of the benefits of the CPE and the experience its officers brought to the Council.

On the proposition of Councillor Ritchie, seconded by Councillor Mallinder, it was by a unanimous vote

RESOLVED

1. That the formation of a Resilient Coasts Project Board to support governance to deliver the £9.1m programme of innovative coastal adaptation along the East Suffolk and Great Yarmouth coastal frontages be supported.
2. That the appointment of the Cabinet Member with responsibility for Planning and Coastal Management as Chair of any approved project board be supported.
3. That the proposed governance structure for the Resilient Coasts Project be approved.

Cabinet received report **ES/1256** by the Cabinet Member with responsibility for Planning and Coastal Management, which set out the statutory Infrastructure Funding Statement for 2021/22, the District CIL Allocations for 2022/23 and a revised CIL Spending Strategy.

The Assistant Cabinet Member for Planning and Coastal Management was invited to introduce the report; Councillor Cooper noted that the report detailed information on CIL projects implemented and the progress of others that had received funding. Cabinet was advised that the Major Sites and Infrastructure Team had created an interactive map showing locations of CIL projects and the status of their delivery.

Councillor Cooper explained that the CIL Spending Working Group recommended the proposed CIL Spending Strategy changes to Cabinet and that these changes would align with best practice guidance.

Cabinet gave its full support to the proposals, noting the positive projects achieved through CIL and that local projects could be delivered by communities with Neighbourhood Plans, who would receive 25% of CIL contributions. The Cabinet Member with responsibility for Housing noted that where significant development took place the public did not always understand that bodies such as the NHS needed to apply for CIL funding to expand their infrastructure and suggested that this information be presented in a simple format.

The Assistant Cabinet Member for Planning and Coastal Management, along with the Infrastructure Delivery Manager, outlined a recent case in Leiston where CIL funding could not be granted to expand a GP surgery as the building was owned by the Practice; the Infrastructure Delivery Manager explained that unless a GP surgery enters into a funding agreement with the NHS, CIL funding cannot be allocated and surgeries were often reluctant to do so.

The Leader considered that the benefits of CIL were starting to be seen across the district and agreed that a simple explanation of how CIL worked should be promoted through the Council's social media channels. The Assistant Cabinet Member for Planning and Coastal Management stated that some promotional work was already planned on what had been achieved through CIL and said this could form part of that campaign.

On the proposition of Councillor Ritchie, seconded by Councillor Kerry, it was by a unanimous vote

RESOLVED

1. That the Infrastructure Funding Statement 2021-22 at Appendix A of report ES/1256 be approved for publication by 31 December 2022, subject to further minor financial, typographical, and presentational amendments confirmed through the Assistant Cabinet Member for Planning & Coastal Management and the Cabinet Member with responsibility for Resources.

2. That the recommendations for allocating Community Infrastructure Levy funding towards the infrastructure projects as outlined in Appendix B of report ES/1256 be approved.
3. That the extension of the spend period of the Jetty Lane Project, Woodbridge to June 2025, subject to revised planning application being submitted by 1st April 2023 as outlined in Appendix B of report ES/1256, be approved.
4. That the recommended changes to the CIL Spending Strategy outlined in Appendix C of report ES/1256 be approved.

7 Procurement Strategy 2022-2025

Cabinet received report **ES/1257** by the Deputy Leader and Cabinet Member with responsibility for Economic Development, which presented a new Procurement Strategy for Cabinet's approval along with an implementation plan.

The report was introduced by the Assistant Cabinet Member for Economic Development on behalf of the Deputy Leader and Cabinet Member with responsibility for Economic Development, who had given his apologies for the meeting. Councillor Wiles explained that the adoption of the new Strategy would support the delivery of the Council's Strategic Plan through efficient procurement and management of expenditure, and would improve the management of contracts and supplier relationships.

Cabinet was advised that the new Procurement Strategy responded to the 11 recommendations made by the independent procurement and contract review undertaken and also responded to recommendations made by the Procurement Task and Finish Group. The Assistant Cabinet Member for Economic Development considered that the Strategy would modernise operations through fundamental changes and reappraisal, which was captured in the detailed implementation plan, and would be in step with rapid market changes in the changing economic landscape.

Cabinet gave its full support to the new Procurement Strategy. The Leader noted the significant preparatory work that had gone into developing it and that officers had worked hard to develop something that would deliver against the Council's Strategic Plan.

The Cabinet Member with responsibility for the Environment added that a new Strategy would contribute to the Council meeting its environmental vision and allow it to influence how its partners worked; the Cabinet Member with responsibility for Housing reflected on his own experiences of procurement and concurred that having an effective Strategy in place would allow the ESC to procure goods and services with the best green credentials.

Councillor Deacon supported the new Strategy and thanked the officers involved along with the Procurement Task and Finish Group. Councillor Deacon was pleased to see that the Procurement Strategy included a statement noting its importance to everyone involved in procurement activity and promoted sustainable procurement. The Assistant Cabinet Member for Economic Development highlighted that the

development of the Strategy had included a desire to be understandable to everyone involved in procurement activity.

On the proposition of Councillor Gallant, seconded by Councillor Brooks, it was by a unanimous vote

RESOLVED

1. That the East Suffolk Procurement Strategy 2022 be approved.
2. That the East Suffolk Procurement Strategy Implementation Plan be approved.
3. That responsibility be delegated to the Head of Operations to implement the new plan.

8 First Light Festival - National Portfolio Organisation application and match funding

Cabinet received report **ES/1258** by the Deputy Leader and Cabinet Member with responsibility for Economic Development and the Cabinet Member with responsibility for Communities, Leisure and Tourism, which sought approval to provide First Light Festival Community Interest Company (CIC) with £500k of match funding over three years to support its National Portfolio Organisation (NPO) application to the Arts Council.

The report was introduced by the Cabinet Member with responsibility for Communities Leisure and Tourism, who highlighted the positive relationship between the CIC and ESC, considering that the CIC had demonstrated it could deliver an excellent Cultural Strategy for Lowestoft.

The Cabinet Member with responsibility for Communities, Leisure and Tourism said that it was important that the CIC sought alternative funding and was not wholly reliant on the Council and that the proposed match funding would support its NPO application to the Arts Council. Cabinet was informed that there was no guarantee that this application would be successful but should it be so, the match funding would support the CIC to expand its work in East Suffolk.

It was confirmed that funding would be provided on a sliding scale over three years, ie £180k for 2023/24, £170k for 2024/25 and £150k in 2025/26 as the CIC generates additional income through both the festival and as operators of East Point Pavilion.

Cabinet fully supported the proposals set out in the report.

Councillor Deacon referred to the forthcoming full evaluation of the 2022 First Light Festival mentioned in the report and asked if this information would be circulated to all Members. Both the Leader and the Head of Operations confirmed that this information would be circulated to all Members as soon as it was available.

On the proposition of Councillor Smith, seconded by Councillor Rudd, it was by a unanimous vote

RESOLVED

1. That, in support of the Strategic Plan and the Council's emerging cultural strategy, £500k over the 3 year period, 2023/24 to 2025/26 be approved to support the continuation of First Light Festival CIC's regionally impactful cultural development programme, festival weekend and application for National Portfolio Organisation status. Council funding would be provided on a sliding scale over the three years i.e. £180k for 2023/24, £170k for 2024/25 and £150k in 2025/26 as the CIC generates additional income through both the festival and as operators of East Point Pavilion.

2. That the use of the New Homes Bonus Reserve to fund the £500k growth to the revenue budget over the three year period 2023/24 – 2025/26 be approved.

9 East Suffolk Visitor Economy Strategy

Cabinet received report **ES/1259** by the Cabinet Member with responsibility for Communities, Leisure and Tourism, which set out the Council's new Visitor Economy Strategy for approval.

The report, introduced by the Cabinet Member with responsibility for Communities, Leisure and Tourism, outlined the value of the East Suffolk visitor economy; Councillor Smith highlighted that in 2019 its value had been just under £700m and supported approximately 11,000 jobs, accounting for 15% of employment in East Suffolk. It was estimated that there had been an overall spend of £525m in East Suffolk in 2019.

The Cabinet Member with responsibility for Communities, Leisure and Tourism said that the Council's contribution and involvement with the East Suffolk visitor economy came at a crucial time, after Brexit and COVID-19, and the new Strategy looked to to help achieve sustainability, net zero and use the digital economy to promote East Suffolk.

Cabinet was informed of the comprehensive method used to draft the Visitor Economy Strategy. The Cabinet Member with responsibility for Communities, Leisure and Tourism outlined the structure of the East Suffolk Visitor Economy Strategy Group which would be established to oversee and guide the delivery of the new Strategy, which would include the Suffolk Coast DMO and work with groups such as Visit East of England and Visit Britain to promote East Suffolk as a tourist destination.

Cabinet fully supported the new East Suffolk Visitor Economy Strategy, commenting on the quality of the document. The Cabinet Member with responsibility for the Environment considered anything that would promote the depth of biodiversity in the district to be positive. The Cabinet Member with responsibility for Housing cited the increase in tourism in Felixstowe in 2022 and considered the new Strategy would expand on this and bring more visitors to East Suffolk.

Councillor Deacon praised the report and the new Strategy and queried if there would be a return of Tourist Information Centres across the district. The Leader, supported by the Economic Development Manager, outlined that whilst a return to traditional Tourist Information Centres was unlikely a similar service would be provided through digital technology and this would be supported by the free Wi-Fi being rolled out to

market towns in East Suffolk; it was noted that there was a network of visitor information points across the district, housed in visitor and community facilities, which directed people to online resources including the The Suffolk Coast DMO and Suffolk Onboard.

The Assistant Cabinet Member for Economic Development noted that footfall in Felixstowe over the summer period had exceeded what had been expected and considered the future of tourism in East Suffolk to be bright.

The Assistant Cabinet Member for Resources noted that a photograph of the Moot Hall in Aldeburgh within the new Strategy document was labelled incorrectly; the Economic Development Manager said she would raise this with the design team and ensure it was corrected.

Councillor Beavan commented that tourism was prevalent in his ward and that this industry should be supported; he highlighted that in Southwold several restaurants did not operate in the evening as they could not recruit kitchen staff, as workers could not afford to live in the area and the bus service was inadequate in the evening. Councillor Beavan considered more should be done to resolve these issues.

On the proposition of Councillor Smith, seconded by Councillor Brooks, it was by a unanimous vote

RESOLVED

That the East Suffolk Visitor Economy Strategy be approved.

10 Southwold Harbour– Draft Budget Monitoring Report Quarter 4 2021/22

Cabinet received report **ES/1268** by the Cabinet Member with responsibility for Resources, which provided the Southwold Harbour Draft Budget Monitoring Report for Quarter 4 of the 2021/22 financial year.

The report, which had been considered by the Southwold Harbour Management Committee at its meeting of 14 July 2022 and recommended for Cabinet's consideration, set out that both the Harbour and the Caravan and Camping Site had performed well in 2021/22; the Cabinet Member with responsibility for Resources reported that direct income at the Harbour had been £26,000, slightly above budget and that direct income at the Caravan and Camping Site had been £26,000, slightly under budget.

It was noted that both sites had been impacted by COVID-19 restrictions in the early part of 2021/22 but had recovered in the latter half of the year. The Cabinet Member with responsibility for Resources advised that £112,000 in compensation for loss of income at the Caravan and Camping Site had been received from the government and this was included in the figures.

Direct expenditure at both sites was reported as being under budget, with some areas requiring a review in the new year to align with actual activities and inflationary pressures.

The Cabinet Member with responsibility for Resources considered that the year-end position had been favourable; the overall net outturn position for the Harbour was a surplus of £77k and for the Caravan and Camping Site was £144k.

Cabinet noted the Draft Budget Monitoring Report.

On the proposition of Councillor Cook, seconded by Councillor Gallant, it was by a unanimous vote

RESOLVED

That the Budget Monitoring Report Quarter 4 2021/22 as reviewed by the Southwold Harbour Management Committee be noted.

11 Environment Task Group - Quarterly Update

Cabinet received report **ES/1269** by the Cabinet Member with responsibility for the Environment, which provided a quarterly update on the work of the Environment Task Group.

The report, introduced by the Cabinet Member with responsibility for the Environment, set out the establishment of the Environment Task Group in response to the Council's declaration of a climate emergency in 2019 and the tangible differences it had made to the way the Council worked in relation to achieving its environmental ambitions.

Cabinet was apprised of recent highlights, including focus on the climate action plan, the reduction of the Council's carbon footprint by converting vehicles to hydrotreated vegetable oil (HVO) fuel, biodiversity grants, the development of the Sustainable Construction Supplementary Planning Document (SPD), and the removal of the Woodbridge Air Quality Management status.

The Cabinet Member with responsibility for the Environment said the Council's commitment to the environment was stronger than ever before and that Cabinet and Full Council were working together to make the right decisions and take action to make a difference.

Cabinet commended the work of the Environment Task Group. The Leader asked the Group to look at further ways to engage the public in playing its part to contribute to the environment, suggesting looking at external as well as internal resources. The Leader considered the appointment of Paul Mackie as the Council's Lead Officer for Environment and Climate Change to be key in the Council delivering against its environmental agenda. The Cabinet Member with responsibility for Communities, Leisure and Tourism noted that the Council would review how its leisure facilities could contribute to the environmental agenda and was of the view that it was facing these challenges head on.

Councillor Gooch commended the Cabinet Member with responsibility for the Environment for his chairing of the Environment Task Group, describing the positive cross-party work and support from officers. Councillor Gooch raised water quality

issues and hoped that the Environment Task Group would revisit this topic as it was important to hold authorities to account on water standards.

The Assistant Cabinet Member for Economic Development highlighted that the Government had announced £59bn of funding to address outages from sewage; he hoped that this provision would be spread evenly across the country and tackle the most important issues first.

On the proposition of Councillor Mallinder, seconded by Councillor Burroughes, it was by a unanimous vote

RESOLVED

1. That the report from the Environment Task Group be noted.
2. That it be confirmed that the Group is to continue to deliver the task it was set to investigate ways to cut East Suffolk Council's carbon and other harmful emissions.

12 Shareholder Agreement and Governance Arrangements for the Council's Group Companies

Cabinet received report **ES/1267** by the Leader of the Council, which sought the approval to enter into a shareholder agreement with the Council's companies, create a Shareholder Reference Group (SRG) as a committee of Cabinet to undertake the Council's functions as shareholder/owner and to appoint Cabinet Members to the SRG.

The report, introduced by the Leader, set out that East Suffolk Services Limited (ESSL) had been incorporated to deliver Council services currently undertaken by East Suffolk Norse from July 2023. Councillor Gallant noted that ESS had been incorporated within the group structure East Suffolk Holdings Limited (ESHL), which contained three dormant companies - East Suffolk Property Investments Limited, East Suffolk Property Developments Limited, and East Suffolk Construction Services Limited.

The Leader explained that to provide the necessary control and governance for the Council's group companies it was proposed that a Shareholder Agreement be entered into and the SRG be established, which would be a committee of the Cabinet and have some delegated decision-making responsibilities for the group companies. Cabinet was advised that some matters would be reserved to Cabinet and Full Council, along with some rights that would be reserved to the companies themselves.

It was also proposed that the Leader of the Council and the Cabinet Members with responsibility for Economic Development, Resources, the Environment, and Customer Experience, ICT and Commercial Partnerships be appointed to the SRG each for a term of four years.

The Leader advised that he would be tabling a sixth recommendation, not in the published report, to delegate authority to the Head of Legal and Democratic Services, in consultation himself, to draft Terms of Reference for the SRG for incorporation into East Suffolk Council's Constitution.

There being no questions or comments, it was on the proposition of Councillor Gallant, seconded by Councillor Cook, and by a unanimous vote

RESOLVED

1. That the entering into of the Shareholder Agreement and any associated “deeds of adherence” which commit group companies to the obligations set out in the shareholder agreement be approved.
2. That the establishment of a committee of Cabinet, to be known as the Shareholder Reference Group, be approved to undertake the functions of the Council as shareholder/ultimate owner of its group of companies and exercise the Council’s rights under the Articles of each company and under the Shareholder Agreement, with the exception of any rights which the Cabinet or Leader reserves to itself/himself from time to time or which can only be exercised by Full Council.
3. That the approval of the Consolidated Business Case, which is to be produced by the Council’s Holding Company on an annual basis, be reserved to the Cabinet (this will accordingly be a matter which is not within the delegated authority of the SRG).
4. That the appointment of the Leader of the Council along with the Cabinet Members with responsibility for Economic Development, Resources, The Environment and Customer Experience, ICT and Commercial Partnerships to act as members of the Shareholder Reference Group be approved, each for a term of 4 years (provided that where an individual Member ceases to hold the relevant role during the allotted term, their replacement in that role or such role as the Leader may identify as its replacement shall also take over the role of Member of the Shareholder Reference Group for the remainder of the term or such other term as the Leader may specify).
5. That on the appointment of the Members whose roles are identified above, the Shareholder Reference Group be hereby established on 6 September 2022 and the terms of office for the Members referred to in the second resolution above shall also commence on the same date.
6. That the Head of Legal and Democratic Services, in consultation with the Leader of the Council, be given the delegated authority to draft Terms of Reference for the Shareholder Reference Group (SRG) for incorporating into East Suffolk Council’s Constitution, including a quorum of three for meetings of the SRG which must include the Leader of the Council (or the Deputy Leader of the Council in the absence of the Leader), the appointment of a Shareholder Representative from the members of the SRG and the inclusion of such other provisions as will enable the SRG to provide strategic oversight of the Council’s companies and support the development of those companies.

13 Capital Programme Review 2022/23 to 2025/26

Cabinet received report **ES/1263** by the Cabinet Member with responsibility for Resources, which provided an update of the Council's Capital Review Programme agreed by Full Council in January 2022 as part of the budget-setting process.

The report, introduced by the Cabinet Member with responsibility for Resources, accurately reflected the updates made to the Council's Capital Programme, including new schemes agreed and phased projects from the 2021/22 financial year.

The Cabinet Member with responsibility for Resources advised that the carry forwards from 2021/22 and budget increases totalled £5.68m. New projects totalled £3.2m and Councillor Cook summarised the breakdown as detailed in the report. Cabinet was advised that the re-phasing of the 2022/23 budget to later years totalled £12.14m.

Cabinet was informed that although subject to many of the same pressures as the General Fund Capital Programme, the Housing Revenue Account (HRA) Capital Programme consisted of three main block allocations (repair, project development and the new build programme) and there was no requirement for approval of additional funding at that point, with revision unlikely to occur until the budget-setting process began later in the year.

The Cabinet Member with responsibility for Resources considered it was positive that there was no requirement to increase capital finance requirements, which meant no additional borrowing would be required from 2022/23 to 2025/26.

Councillor Beavan highlighted the project at Southwold Enterprise Hub and asked how the provision of cover required by the CIPFA code would be required. The Chief Finance Officer and Strategic Director advised that the detail of this was contained within the report on the project approved by Cabinet earlier in the year and that provision would be achieved both through the purchase of the equivalent investment share in the Hub's freehold and rental income, as set out in the Heads of Terms agreed with Southwold Town Council.

On the proposition of Councillor Cook, seconded by Councillor Ritchie, it was by a unanimous vote

RESOLVED

That the revised General Fund Capital Programme for 2022/23 to 2025/26 including revisions as shown in Appendix B to report ES/1263 be recommended for approval by Full Council.

14 Exempt/Confidential Items

The Leader reported that, in exceptional circumstances, the Council may, by law, exclude members of the public from all, or part of, an executive decision-making meeting. The Council should, the Leader added, unless there were urgent circumstances, give notice of its intention to do so via the Forward Plan, which he reported was updated and published on its website 28 clear days prior to the meeting.

There were various reasons that the Council, on occasions, had to do this and examples were because a report contained information relating to an individual, information relating to the financial or business affairs of a particular person, or information relating to any consultations or negotiations.

Tonight, the Leader advised, Cabinet had three substantive exempt matters to consider and they were as outlined on the published agenda.

Firstly, agenda item 16 related to the North Felixstowe Garden Neighbourhood. The purpose of this report was to set out the current status of the development and seek approval and funding for ESC to progress to the next stage of work. The purpose of the recommendations within the report were to assist the Council to help drive forward the best overall solution for the land allocations within the North Felixstowe Garden Neighbourhood and to ensure the new leisure centre was built in the optimum location, the best housing, education, leisure and employment/commercial mix was found across the whole development, and to secure release of existing leisure centre site assets in Felixstowe.

Secondly, the Leader reported, agenda item 17 related to a Local Authority Trading Company Business Case. The purpose of this report was to seek approval of the Business Case that had been prepared for East Suffolk Holdings Limited, enabling it to commence its operational activities. The Business Case set out the Strategic, Legal, Commercial, Financial and Management/Operational case for ESHL.

The Leader concluded that agenda item 18 related to the purchase of New Section 106 Units in Wrentham. The recommendations within the report sought approval for expenditure for the purchase. The report provided a clear rationale for the purchase of the properties to add to ESC's Housing Revenue Account. This would assist ESC in its ambition to increase overall numbers of new build homes available to East Suffolk residents through multiple tenures. The development of this scheme would deliver 15 quality new homes and would contribute to the delivery of a number of the Council's key priorities, as set out in our Strategic Plan and associated Housing and Housing Development strategies.

On the proposition of Councillor Gallant, seconded by Councillor Mallinder, it was by a unanimous vote

RESOLVED

That under Section 100A(4) of the Local Government Act 1972 (as amended) the public be excluded from the meeting for the following items of business on the grounds that they involve the likely disclosure of exempt information as defined in Paragraphs 3 and 4 of Part 1 of Schedule 12A of the Act.

15 Exempt Minutes

- Information relating to the financial or business affairs of any particular person (including the authority holding that information).

16 North Felixstowe Garden Neighbourhood

- Information relating to the financial or business affairs of any particular person (including the authority holding that information).

17 LATCo Holding Company Business Case

- Information relating to the financial or business affairs of any particular person (including the authority holding that information).
- Information relating to any consultations or negotiations, or contemplated consultations or negotiations, in connection with any labour relations matter arising between the authority or a Minister of the Crown and employees of, or office holders under, the authority.

18 Purchase of New s106 Units at Chapel Road, Wrentham

- Information relating to the financial or business affairs of any particular person (including the authority holding that information).

The meeting concluded at 8.31 pm.

.....
Chairman



CABINET
Tuesday, 04 October 2022

Subject	East Suffolk Cycling and Walking Strategy
Report of	Councillor David Ritchie Cabinet Member with responsibility for Planning and Coastal Management
Supporting Officer	Anthony Taylor Senior Planner (Policy and Delivery) Anthony.taylor@eastsoffolk.gov.uk 07825 693964

Is the report Open or Exempt?	OPEN
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Category of Exempt Information and reason why it is NOT in the public interest to disclose the exempt information.	Not applicable.
Wards Affected:	All Wards

Purpose and high-level overview

Purpose of Report:

The purpose of this report is to recommend the adoption of the East Suffolk Cycling and Walking Strategy (Strategy). The purpose of the Strategy is to create safe, coherent, direct, comfortable, and attractive cycling, walking and wheeling environments that lead to improvements in health and wellbeing, facilitate greater social interaction and play, encourage more environmentally sustainable lifestyles, reduce road congestion, and support economic growth. In order to create these environments, the Strategy identifies cycling and walking infrastructure recommendations across East Suffolk including in the Broads Authority Area. These recommendations are supported by an Infrastructure Delivery Framework that aligns with the Infrastructure Delivery Frameworks appended to the Suffolk Coastal Local Plan and the Waveney Local Plan. The pdf version of the Strategy is appended at Appendices A, B and C of this report, and the interactive version of the Strategy can be viewed here:

<https://storymaps.arcgis.com/collections/4bd40e1d6e6c4637a7fceb840827c843?item=1>.

Options:

Adopt the Strategy. This will mean the Council has an up-to-date Strategy covering the entire East Suffolk area, which will serve as the Local Cycling and Walking Infrastructure Plan for East Suffolk.

An alternative option would be to not adopt the Strategy and continue with the adopted Waveney Cycle Strategy (2016) and without a cycling and walking strategy for the former Suffolk Coastal area. However, this would be a missed opportunity to provide up to date cycling and walking infrastructure recommendations for East Suffolk and bring the entire East Suffolk area up to the benchmark of having a plan in place for cycling and walking infrastructure across the district.

Recommendation/s:

1. That the East Suffolk Cycling and Walking Strategy, attached as Appendix A to the report, be adopted.
2. That the Head of Planning and Coastal Management, in consultation with the Cabinet Member with responsibility for Planning and Coastal Management, be authorised to make any presentational or typographical amendments to the East Suffolk Cycling and Walking Strategy prior to it being published.
3. That the East Suffolk Cycling and Walking Strategy be presented to Full Council for its information and to ensure Full Council is informed of the Strategy's importance to the district.

Corporate Impact Assessment

Governance:

No impacts.

ESC policies and strategies that directly apply to the proposal:

The Strategy supports the implementation of site allocations and policies addressing sustainable transport within the Suffolk Coastal Local Plan (2020) and Waveney Local Plan (2019).

Environmental:

The Strategy makes cycling and walking infrastructure recommendations across East Suffolk, in support of development sites allocated in the Suffolk Coastal Local Plan and the Waveney Local Plan, as well as in support of existing communities. The introduction of cycling and walking infrastructure where it currently does not exist could initially harm the environment through the replacement of for example agricultural land with cycling and walking tracks. However, if the introduction or improvement of cycling and walking infrastructure generates modal shift (the conversion of trips taken by one mode of transport to another, e.g. cycling to local shops rather than driving) then the environmental impact of the cycling and walking infrastructure could be positive. If the Strategy was to be adopted, the detailed design solutions for each recommendation would be considered after adoption and through further community engagement. The exact material treatment and lighting design of each recommendation will have an impact on the environment, but these detailed matters are not set out within the Strategy's recommendations and the impact is therefore not precisely known at this time. A Strategic Environmental Assessment Screening Opinion (Appendix E) was undertaken and concluded that a full Strategic Environmental Assessment would not be necessary. A Habitats Regulations Assessment Screening Statement (Appendix F) was also undertaken and concluded that the Strategy will not lead to likely significant effects on protected Habitat sites. If through the implementation of any individual recommendation it is considered that project level Habitats Regulations Assessments or Environmental Impact Assessment are required, then these will be carried out as necessary. Overall, the environmental impact of the Strategy is considered to be positive in facilitating more sustainable lifestyles that do not revolve around the private car.

Equalities and Diversity:

An Equality Impact Assessment Screening Opinion was produced in October 2021 to accompany consultation on the Draft Strategy, and updated after the consultation in September 2022 (Appendix G). The Strategy has also been subject to separate Equality Impact Analysis as part of the production of this report (ref: EQIA445334398) in August 2022. Both assessments concluded no differential negative impacts on those with protected characteristics.

Financial:

The production and adoption of the Strategy is covered by the existing budget of the Planning Policy and Delivery Team. The Strategy, in identifying cycling and walking infrastructure recommendations across East Suffolk, is supported by an Infrastructure Delivery Framework (IDF) (Appendix C), the purpose of which is to outline the delivery and funding methods that could be most appropriate for specific recommendations. Funding sources identified in the IDF consist of developer contributions in the form of CIL

and section 106 (which the Council collects from development sites) and funding pots available to be bid into at the national and county level.

In the long term (i.e. beyond the current medium term financial strategy), future revenue budgets for cleansing, grounds maintenance rolling maintenance of our infrastructure will be required.

Human Resources:

No Impacts.

ICT:

No Impacts.

Legal:

The Strategy, if adopted, will serve as the Local Cycling and Walking Infrastructure Plan (LCWIP) for East Suffolk. There is no legislation governing LCWIPs. The Government's first 'Cycling and Walking Investment Strategy' (2017), second 'Cycling and Walking Investment Strategy' (2022) and 'Local Cycling and Walking Infrastructure Plan Technical Guidance for Local Authorities' (2017) provide guidance to help support local authorities in preparing LCWIPs. This Government guidance has been considered alongside a collaborative working relationship with Suffolk County Council throughout the preparation of the Strategy.

The Strategy has been subject to a Strategic Environmental Assessment Screening Opinion in accordance with the Environmental Assessment of Plans and Programmes Regulations 2004 (Appendix E). It has also been subject to a Habitats Regulations Assessment Screening Statement, in accordance with the Conservation of Habitats and Species Regulations 2017 (as amended) (Appendix F). An Equality Impact Assessment Screening opinion was produced to meet the requirements of the Equality Act 2010 (Appendix G).

Risk:

There remains a small risk that national priorities shift, thereby reducing funding pots available to deliver the Strategy. This risk is minimal given the overwhelming interest in preserving our environments.

External Consultees:

The Strategy has been subject to two rounds of consultation, which returned a significant number of consultation responses, and which have been a valuable source of information. During the formal consultation officers met with various Community Partnerships to discuss the content of the Strategy and receive feedback. Engagement with neighbouring Local Authorities has also been undertaken. The list of respondents, summaries of their comments and how these have been responded to can be found in the Consultation Statement which is appended to this report (Appendix D). An officer steering group comprising East Suffolk Council and Suffolk County Council officers was established to oversee and help guide the preparation of the Strategy.

Strategic Plan Priorities

Select the priorities of the Strategic Plan which are supported by this proposal: (Select only one primary and as many secondary as appropriate)		Primary priority	Secondary priorities
T01	Growing our Economy		
P01	Build the right environment for East Suffolk	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P02	Attract and stimulate inward investment	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P03	Maximise and grow the unique selling points of East Suffolk	<input type="checkbox"/>	<input type="checkbox"/>
P04	Business partnerships	<input type="checkbox"/>	<input type="checkbox"/>
P05	Support and deliver infrastructure	<input type="checkbox"/>	<input checked="" type="checkbox"/>
T02	Enabling our Communities		
P06	Community Partnerships	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P07	Taking positive action on what matters most	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P08	Maximising health, well-being and safety in our District	<input checked="" type="checkbox"/>	<input type="checkbox"/>
P09	Community Pride	<input type="checkbox"/>	<input type="checkbox"/>
T03	Maintaining Financial Sustainability		
P10	Organisational design and streamlining services	<input type="checkbox"/>	<input type="checkbox"/>
P11	Making best use of and investing in our assets	<input type="checkbox"/>	<input type="checkbox"/>
P12	Being commercially astute	<input type="checkbox"/>	<input type="checkbox"/>
P13	Optimising our financial investments and grant opportunities	<input type="checkbox"/>	<input type="checkbox"/>
P14	Review service delivery with partners	<input type="checkbox"/>	<input type="checkbox"/>
T04	Delivering Digital Transformation		
P15	Digital by default	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P16	Lean and efficient streamlined services	<input type="checkbox"/>	<input type="checkbox"/>
P17	Effective use of data	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P18	Skills and training	<input type="checkbox"/>	<input type="checkbox"/>
P19	District-wide digital infrastructure	<input type="checkbox"/>	<input type="checkbox"/>
T05	Caring for our Environment		
P20	Lead by example	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P21	Minimise waste, reuse materials, increase recycling	<input type="checkbox"/>	<input type="checkbox"/>
P22	Renewable energy	<input type="checkbox"/>	<input type="checkbox"/>
P23	Protection, education and influence	<input type="checkbox"/>	<input checked="" type="checkbox"/>
XXX	Governance		
XXX	How ESC governs itself as an authority	<input type="checkbox"/>	<input type="checkbox"/>
How does this proposal support the priorities selected?			
<p>The Strategy supports the delivery of Strategic Plan priority P08 by identifying cycling and walking infrastructure recommendations that can encourage safe cycling and walking and thereby support improvements in mental and physical health and wellbeing, and reduce social isolation and loneliness. The Strategy also supports the delivery of a number of secondary Strategic Plan priorities.</p> <p>The Strategy supports the implementation of Local Plan policies for the delivery of development sites and regeneration opportunities through placed based initiatives enabling plan led growth (P01), and enhances opportunities to secure funding for cycling</p>			

and walking infrastructure projects in East Suffolk (P02). The identification of cycling and walking infrastructure recommendations across the district that are supported by an Infrastructure Delivery Framework can inform CIL spend on infrastructure and help to facilitate healthy and sustainable economic growth (P05).

Every Community Partnership has identified priorities that can be supported by the implementation of the Strategy’s recommendations. Such Community Partnership priorities include but are not limited to delivering sustainable and active transport opportunities, enabling physically active and healthy lives, and reducing social isolation and loneliness (P06). Data and community intelligence have been integral to the formulation of the Strategy’s place-based recommendations across rural and urban areas of East Suffolk. The Strategy will also function as an evidence base for community projects, such as neighbourhood plans (P07).

A digital first approach has been taken throughout the preparation of the Strategy in order to present the Strategy in the most effective way possible, including through the use of interactive map based engagement during both the initial and formal consultations. Hard copies of the Strategy documentation have been made available, and thus the digital approach has not undermined or excluded those that prefer to or can only engage with the Strategy via traditional means (P15). The Strategy is supported by a wealth of data, including but not limited to consultation responses, Quiet Lanes, the Public Rights of Way network, existing and planned development, Government supported Propensity to Cycle Tool (web-based tool to estimate cycling potential on any given street), and Strava Metro data (trip data collected from Strava Metro app users) (P17).

Included within the Strategy are recommendations that relate to Local Plan site allocations, including those that are or contain Council assets. These recommendations support the delivery of cycling and walking infrastructure, which would help the Council reach its carbon neutral targets (P20). Cycling and walking infrastructure as identified in the Strategy’s place-based recommendations can, through the conversion of trips to more sustainable transport modes, help create cleaner, quieter, and healthier environments across East Suffolk. The Strategy will also function as an evidence base to support the creation of local policies, for example in the form of local plan and neighbourhood plan policies, that reflect the Council’s environmental concerns and targets. (P23).

Background and Justification for Recommendation

1 Background facts	
1.1	The Council has two adopted Local Plans: the East Suffolk Council – Waveney Local Plan (March 2019) and the East Suffolk Council – Suffolk Coastal Local Plan (September 2020). These Local Plans both contain policies relating to sustainable transport, namely policy SCLP7.1 (Sustainable Transport) of the Suffolk Coastal Local Plan and policy WLP8.21 (Sustainable Transport) of the Waveney Local Plan, as well as a number of site allocation policies for development across East Suffolk.
1.2	The Council has an adopted Waveney Cycle Strategy (July 2016), which makes recommendations for improvements to cycling infrastructure across the former Waveney area. There is currently no adopted cycling and/or walking strategy for the former Suffolk Coastal area.

1.3	<p>Since the adoption of the Waveney Cycle Strategy in July 2016 the Government has published a number of cycling and walking policy and guidance documents, including but not limited to the following:</p> <ul style="list-style-type: none"> - The first Local Cycling and Walking Investment Strategy (April 2017) - Local Cycling and Walking Infrastructure Plans Technical Guidance for Local Authorities (April 2017) - Gear Change: A Bold Vision for Cycling and Walking (July 2020) - Local Transport Note 1/20: Cycle Infrastructure Design (July 2020) - The second Local Cycling and Walking Investment Strategy (July 2022) <p>Taken together these Government documents set new targets and standards for cycling and walking infrastructure, which have been considered through the preparation of the Strategy. The Strategy therefore provides up to date cycling and walking infrastructure recommendations based on Government targets and standards.</p>
1.4	<p>The Strategy outlines recommendations across both rural and urban parts of East Suffolk, in the form of:</p> <ul style="list-style-type: none"> - Key Corridors – Key routes between, and through, settlements where there are significant opportunities for modal shift from vehicular travel to cycling and walking by improving cycling and walking infrastructure. - Local Plan Site Allocation Recommendations – Recommendations that seek to guide the design and delivery of Local Plan site allocations, covering both on and off-site opportunities. - Leisure Route Recommendations – A number of rural routes have been identified, connecting some of our more rural settlements. The primary function of these routes is to facilitate leisure opportunities but could just as easily function for other purposes. - Community Recommendations – Recommendations submitted to the Council as part of the initial consultation. These have been assessed against the methodology set out under the ‘Community Recommendations’ tab of the digital version of the Strategy and chapter 4 of the pdf version of the Strategy. These have also been useful in identifying opportunities for the above three recommendation types. <p>The Strategy is also supported by an Infrastructure Delivery Framework (IDF), the purpose of which is to outline the delivery and funding methods that could be most appropriate for specific recommendations. The IDF has been prepared to be consistent with the two Infrastructure Delivery Frameworks appended to the Suffolk Coastal Local Plan and Waveney Local Plan.</p>
1.5	<p>The Strategy does not create new policies but rather seeks to provide infrastructure recommendations that will help with the implementation of relevant policies in the Suffolk Coastal and Waveney Local Plans.</p>
1.6	<p>The preparation of the Strategy has been undertaken with the support and oversight of an officer steering group, which included officers from both East Suffolk Council and Suffolk County Council. On occasion, officers from other local authorities attended steering group meetings. For example, officers from the Broads Authority attended steering group meetings where recommendations within the parts of East Suffolk that fall within and are closely related to the Broads were discussed. The preparation of the Strategy has been overseen by the Local Plan Working Group.</p>
1.7	<p>The Strategy was subject to two rounds of consultation during its preparation, the details of which are contained in the Consultation Statement (Appendix D). The</p>

	<p>first was an initial consultation to inform the scope and content of the Strategy. The initial consultation was carried out between 19 October 2020 and 7 December 2020, to which 797 responses were submitted. The responses received to the initial consultation were used to inform the content of the draft Strategy, and are included in the Strategy as ‘community recommendations’. The formal consultation on the draft Strategy was carried out between 1 November 2021 and 10 January 2022, to which 1,193 responses were received. Both consultations were advertised on the Council’s website and social media channels. All those on the planning policy mailing list which includes town and parish councils, individuals and organisations, including those who were previously contacted or responded to the initial consultation were engaged. Both initial and formal consultations were primarily map based and utilised digital mapping technology that provided a user friendly engagement experience. The consultation responses, and how they were addressed in drafting the Strategy, are contained in the Consultation Statement (Appendix D).</p>
1.8	<p>Under the Environmental Assessment of Plans and Programmes Regulations 2004 screening was carried out on the draft Strategy to determine whether a full Strategic Environmental Assessment would be required. The screening concluded that this was not required. The final Strategic Environmental Assessment Screening Opinion is appended to this report (Appendix E).</p>
1.9	<p>Habitats Regulations Assessment screening was also undertaken which concluded that implementation of the Strategy would not lead to likely significant effects on protected Habitat sites and that it is therefore not considered necessary to undertake an Appropriate Assessment. The final Habitats Regulations Assessment Screening Statement is appended to this report (Appendix F).</p>
1.10	<p>An Equality Impact Assessment Screening Opinion was produced in October 2021 to accompany consultation on the draft Strategy, and updated after the consultation in September 2022 (Appendix G). The Strategy has also been subject to separate Equality Impact Analysis as part of the production of this report (ref: EQIA445334398) in August 2022. Both assessments concluded no differential negative impacts on those with protected characteristics.</p>

2 Current position

2.1	<p>Adopted planning policy on sustainable transport is set out in the Council’s two Local Plans. The two adopted local plans are the East Suffolk Council – Suffolk Coastal Local Plan (September 2020) and the East Suffolk Council – Waveney Local Plan (March 2019).</p> <p>Guidance is also currently provided in the adopted Waveney Cycle Strategy (July 2016), which covers the former Waveney area. However, the Waveney Cycle Strategy is over six years old and over this period the Government’s cycling and walking targets and standards have been updated. The Waveney Cycle Strategy also refers to planning policies that are no longer in place as it was adopted before the Waveney Local Plan (March 2019). There is no current cycling and walking guidance for the former Suffolk Coastal area.</p>
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3 How to address current situation

3.1	The Strategy contains cycling and walking infrastructure recommendations across East Suffolk, which will support existing communities and new communities that come together from the implementation of planning policies contained in the adopted Suffolk Coastal and Waveney Local Plans. The Strategy will also function as an evidence base in the review of our two Local Plans as well as neighbourhood plans prepared by community groups. The Strategy is supported by an Infrastructure Delivery Framework (IDF), the purpose of which is to outline the delivery and funding methods that could be most appropriate for specific recommendations. The Strategy will also enhance opportunities to secure funding for cycling and walking infrastructure projects in East Suffolk.
3.2	On adoption the Strategy will be a material consideration in the determination of planning applications, replace the Waveney Cycle Strategy, and serve as the Local Cycling and Walking Infrastructure Plan for East Suffolk.

4 Reason/s for recommendation

4.1	Adoption of the Strategy will provide up to date cycling and walking infrastructure recommendations across the entire East Suffolk area, serving as the Local Cycling and Walking Infrastructure Plan for East Suffolk, and will assist with the implementation of the Council's Local Plan policies which relate to sustainable transport and site allocations.
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Appendices

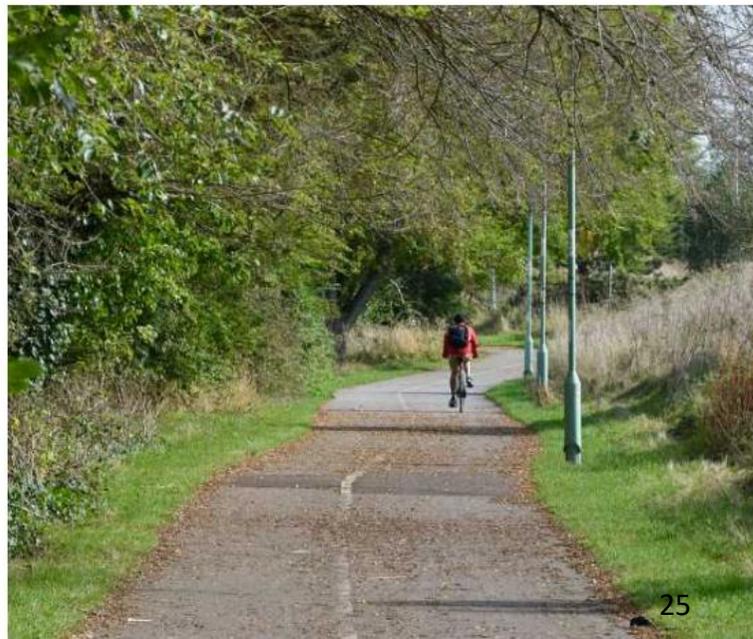
Appendices:	
Appendix A	East Suffolk Cycling and Walking Strategy
Appendix B	East Suffolk Cycling and Walking Strategy Appendix 1: Community Recommendations
Appendix C	East Suffolk Cycling and Walking Strategy Appendix 2: Infrastructure Delivery Framework
Appendix D	Consultation Statement (October 2022)
Appendix E	Strategic Environmental Assessment Screening Opinion (October 2021, updated September 2022) (produced to accompany consultation on Draft SPD, and updated September 2022)
Appendix F	Habitats Regulations Assessment Screening Statement (October 2021, updated September 2022) (produced to accompany consultation on Draft SPD, and updated September 2022)
Appendix G	Equality Impact Assessment Screening Opinion (October 2021, updated September 2022) (produced to accompany consultation on Draft SPD, and updated in September 2022)

Background reference papers:

Date	Type	Available From
March 2019	East Suffolk Council- Waveney Local Plan	https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/local-plans/
September 2020	East Suffolk Council- Suffolk Coastal Local Plan	https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/local-plans/

CYCLING AND WALKING STRATEGY

OCTOBER 2022



How to use this document

The best way to view the Strategy is to use the interactive version on the Council's website (<https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/supplementary-planning-documents/>). The best way to navigate through the document is by using the interactive contents. Clicking on a specific chapter in the contents, will automatically take you to that part of the document. To move to another part of the document, click the 'home' symbol in the top right corner of the page, which will take you back to the contents. There is also a guide on the right-hand side of each page, which will show you what chapter of the document you are currently in, and where that sits within the whole of the document. Throughout the document there are hyperlinks which you can click to access further information.

Contents

1	Introduction	1
	Purpose of the Strategy	1
	Preparation of the Strategy	1
2	Implementation and Monitoring of the Strategy	4
3	Policy Context	7
4	Recommendations.....	14
	Key Corridors	14
	Ipswich to Melton Key Corridor.....	17
	Ipswich to Felixstowe Key Corridor.....	35
	Martlesham to Felixstowe Key Corridor	44
	Felixstowe Internal Routes Key Corridor	47
	Lowestoft Internal Key Corridor	93
	Lowestoft to Hopton Key Corridor.....	120
	Lowestoft to Kessingland Key Corridor.....	124
	Lowestoft to Bungay Key Corridor.....	128
	Leisure Routes.....	152
	Site Allocation Recommendations.....	174
	Community Recommendations	210
	Glossary.....	213

1 Introduction



1 Introduction

Purpose of the Strategy

- 1.1 The purpose of the East Suffolk Cycling and Walking Strategy (the Strategy) is to create safe, coherent, direct, comfortable, and attractive cycling, walking and wheeling environments that lead to improvements in health and wellbeing, facilitate greater social interaction and play, encourage more environmentally sustainable lifestyles, reduce road congestion, and support economic growth. In order to create these environments, the Strategy identifies cycling and walking infrastructure recommendations across the East Suffolk district (including the Broads Authority Area), focussing on the identification of new and improved infrastructure rather than the maintenance of existing infrastructure. It also provides context and information to support detailed infrastructure proposals and inform plan and decision making to support cycling, walking, and equestrian use.
- 1.2 This Strategy replaces the Waveney Cycle Strategy (2016)¹ and serves as the Local Cycling and Walking Infrastructure Plan (LCWIP) for East Suffolk Council.

¹ <https://www.eastsuffolk.gov.uk/assets/Planning/Waveney-Local-Plan/Waveney-Cycle-Strategy.pdf>

Preparation of the Strategy

- 1.3 In preparing the Strategy, the following key stages have been undertaken:
 - **Review of the Waveney Cycle Strategy (2016), national guidance and best practice** - Prior to starting work on the Strategy, a review of the measures within the existing Waveney Cycle Strategy was undertaken. This was done in parallel with a review of existing and emerging guidance and best practice. The outcomes of this process highlighted the need for the new Strategy to focus on site specific opportunities, make the best use of digital presentation tools, and to avoid repeating general design principles that are already covered in other guidance.
 - **Initial map-based consultation (19 October 2020 to 7 December 2020)** - Consultees were invited to identify existing cycling and walking issues across East Suffolk and, where possible, suggest solutions to them. Respondents were encouraged to plot their response on an online map. 797 comments were submitted, and these can be viewed on the initial consultation map².
 - **Assessment of comments** - Comments submitted as part of the initial consultation have been assessed against the methodology set out in the 'Community Recommendations' section of the Strategy and included within the Strategy as Community

² <https://eastsuffolk.maps.arcgis.com/apps/webappviewer/index.html?id=810e5f8977e144509f13120a00a341d6>

Recommendations. The Community Recommendations can be found in Appendix 1.

- **Identification of Key Corridors and Site Allocation Recommendations** - Officers identified Key Corridors and Local Plan Site Allocation Recommendations using initial consultation comments, Strava Metro³ data (made freely available), Propensity to Cycle Tool (PCT)⁴, and Public Rights of Way (PROW) Definitive Maps⁵.
- **Steering group** - In recognition of the fact that East Suffolk Council (ESC) is the Local Planning Authority and Suffolk County Council (SCC) is the Local Highways Authority, a steering group of ESC and SCC officers was established to inform and guide the development of the Strategy. In addition, the Broads Authority (BA) were part of the steering group where appropriate in recognition of the fact that the BA executive area covers part of East Suffolk.
- **Consultation on Draft Strategy (01 November 2021 to 10 January 2022)** – The Draft Strategy was published for comments to which over 1000 comments were received, and these can be viewed in the Consultation Statement.
- **Finalising the Strategy** – The comments submitted on the draft strategy were considered and assessed with the Local Planning Authority’s responses available to view within the Consultation

³<https://metro.strava.com>

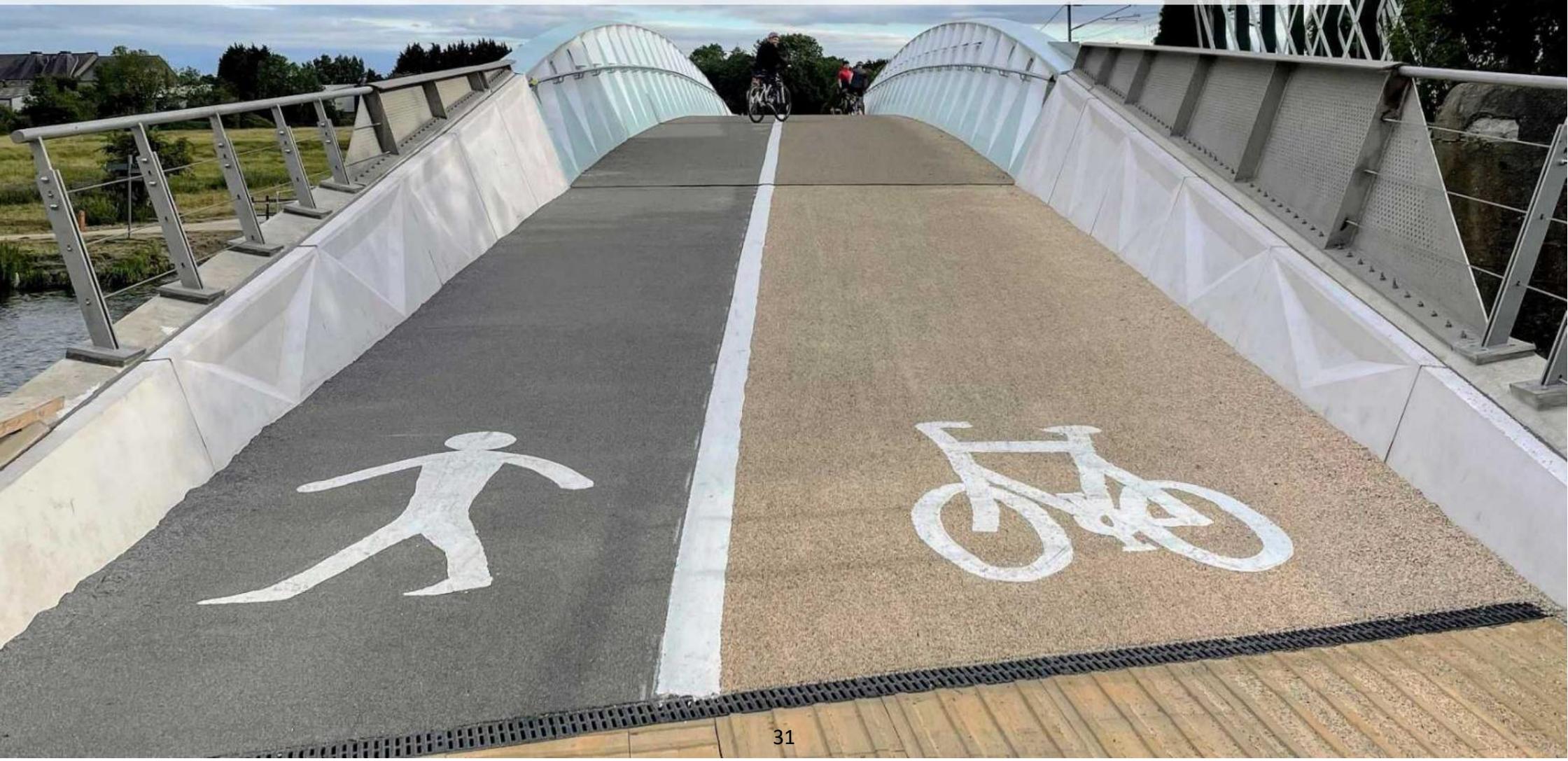
⁴ <https://www.pct.bike/m/?r=suffolk>

Statement. Changes to the Strategy were made in response to the comments received and officer assessments informed by further site visits.

- **Adoption** - The Strategy was adopted by the Council on
- **Post Adoption** - The implementation of the recommendations will be subject to ongoing monitoring work, as detailed in the ‘Implementation and Monitoring of the Strategy’ section below.

⁵<https://www.suffolk.gov.uk/roads-and-transport/public-rights-of-way-in-suffolk/view-definitive-maps-of-public-rights-of-way/>

2 Implementation and Monitoring of the Strategy



2 Implementation and Monitoring of the Strategy

- 2.1 The implementation and monitoring of the recommendations within this Strategy is key to ensuring that communities have access to high quality cycling and walking infrastructure. Through the review of the Waveney Cycle Strategy (2016) it was recognised that, while many cycling and walking infrastructure improvements were identified, relatively few had been implemented. The measures in this Strategy have therefore been through a thorough assessment process.
- 2.2 The infrastructure recommendations identified within this Strategy should not be read as prescriptive proposals, or as the only way in which the infrastructure improvements can be delivered. The Strategy should also not be seen as an exhaustive list of all of the cycling and walking infrastructure potential in the district.
- 2.3 The implementation of any recommendations in this Strategy are likely to need to be informed by further evidence gathering and detailed discussions with SCC as the Local Highways Authority as part of a long-term strategy for the district. Where appropriate the Broads Authority will also be involved in the detailed discussions. Further assessments of individual recommendations may also be required as part of the implementation, including (where necessary) Habitats Regulation Assessment.

⁶ <https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/supplementary-planning-documents/>

- 2.4 In implementing the Strategy, it is important to draw upon various funding and delivery opportunities, including but not limited to:
- Delivery through planning permissions on Local Plan site allocations, Neighbourhood Plan site allocations and windfall development.
 - Delivery linked to permitted Nationally Significant Infrastructure Projects (NSIPs).
 - Suffolk County Council funding opportunities including the ongoing Active Travel Fund.
 - District and Neighbourhood Community Infrastructure Levy (CIL).
 - Future Central Government funding initiatives.
 - Other delivery options at the national and local level.
- 2.5 To provide clarity as to the anticipated delivery opportunities and financial contributions for each of the Strategy's recommendations an Infrastructure Delivery Framework (IDF) (Appendix 2)⁶ has been prepared and is based on infrastructure costs agreed with SCC. To ensure alignment across infrastructure planning the IDF has been designed to be consistent with the East Suffolk Infrastructure Funding Statement (IFS), information about which can be found on the Council's website.⁷ Ongoing monitoring may update the adopted IDF.
- 2.6 Whilst the primary purpose of the Strategy is to identify cycling and walking infrastructure recommendations, it also provides a useful function as an evidence base. For example, the Strategy could be used by a Neighbourhood Planning group to identify cycling and walking

⁷ <https://www.eastsuffolk.gov.uk/planning/developer-contributions/infrastructure-funding-statement/>

infrastructure improvements within a Neighbourhood Plan, thereby providing greater weight to such improvements in planning terms. It could also be used to inform a Development Brief for a Local Plan site allocation.

- 2.7 Monitoring will be important in understanding how effective the Strategy is in achieving its aims. Monitoring will (as appropriate) form part of the Council's Authority Monitoring Report (AMR)⁸, which is published annually.
- 2.8 To ensure efficient use of resources and a responsive approach to implementation, the Strategy's recommendations will be subject to prioritisation assessments on an on-going basis. This prioritisation work will be in addition to any prioritisation set out within the Strategy such as the 'very high' to 'medium' prioritisation of the key corridors and the Multi Criteria Assessment Framework (MCAF) for the community recommendations. There is therefore scope for the priority of recommendations set out in the Strategy to differ to the priority identified in this ongoing prioritisation work.
- 2.9 This work will be undertaken using the Prioritisation Methodology agreed with SCC and based on the methodology detailed in the Department for Transport's Local Cycling and Walking Infrastructure Plans Technical Guidance⁹. The Prioritisation Methodology will be regularly updated and made available to view on the Council's website¹⁰.

⁸ <https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/open-data/>

⁹ https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/908535/cycling-walking-infrastructure-technical-guidance-document.pdf

¹⁰ TBC

- 2.10 As part of this work, it will be important to consider the extent to which recommendations would contribute towards modal shift¹¹ mitigation in the Ipswich Strategic Planning Area (see paragraph 3.22).

¹¹ *Modal shift is the conversion of transport trips from one form of transport to another. In the context of the Strategy modal shift conveys the conversion of transport trips from one form of transport to cycling and walking.*

3 Policy Context



3 Policy Context

3.1 There is an extensive policy context in relation to cycling and walking at the national, county, local and neighbourhood level. With the aim of adding value to this, the Strategy’s focus is on site specific cycling and walking infrastructure opportunities, rather than repeating general guidance held in other documents. The Strategy should therefore be read in conjunction with the following key documents:

National

3.2 **National Planning Policy Framework (NPPF)**, Department for Levelling Up, Housing and Communities (DLUHC) (July 2021)¹² - The NPPF sets out the Government’s planning policies for England, including policies that support the delivery of cycling and walking infrastructure.

3.3 **Cycling and Walking Investment Strategies 1¹³ and 2¹⁴**, Department for Transport (DfT), (April 2017 & July 2022) - These Strategies outline the Government’s ambition for cycling and walking, which is to make cycling, walking and wheeling the natural choices for shorter journeys, or as part of a longer journey.

¹²https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/1005759/NPPF_July_2021.pdf

¹³ <https://www.gov.uk/government/publications/cycling-and-walking-investment-strategy>

¹⁴ [https://www.gov.uk/government/publications/the-second-cycling-and-walking-investment-strategy#:~:text=The%20second%20cycling%20and%20walking%20investment%20strategy%20\(%20CWIS2%20\)%20outlines%20the,a%20longer%20journey%20by%202040.](https://www.gov.uk/government/publications/the-second-cycling-and-walking-investment-strategy#:~:text=The%20second%20cycling%20and%20walking%20investment%20strategy%20(%20CWIS2%20)%20outlines%20the,a%20longer%20journey%20by%202040.)

3.4 **Local Cycling and Walking Infrastructure Plans Technical Guidance for Local Authorities**, DfT (April 2017)¹⁵ - This guidance sets out a recommended approach to planning networks of walking and cycling routes that connect people with places they need to get to, whether for work, education, shopping or other reasons.

3.5 **Gear Change: A bold vision for Cycling and Walking**, DfT (July 2020)¹⁶ - The Government’s vision for achieving a ‘step change in cycling and walking’. The document has four main aims as its vision for England: healthier, happier and greener communities; safer streets; convenient and accessible travel, and cycling and walking being put at the heart of transport decision making. The achievement of these aims is further explored in the document under the following four themes:

- better streets for cycling and people
- cycling and walking at the heart of decision-making
- empowering and encouraging local authorities
- enabling people to cycle and protecting them when they do

3.6 **Local Transport Note 1/20: Cycle Infrastructure Design (LTN 1/20)**, DfT (July 2020)¹⁷ - This guidance document (LTN 1/20) supports the delivery of high-

¹⁵

https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/908535/cycling-walking-infrastructure-technical-guidance-document.pdf

¹⁶https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/904146/gear-change-a-bold-vision-for-cycling-and-walking.pdf

¹⁷https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/951074/cycle-infrastructure-design-ltn-1-20.pdf

quality cycle infrastructure and includes examples of current best practice standards and relevant legal requirements. It has been of particular importance in identifying the recommendations within the Strategy.

- 3.7 **The Highway Code**, DfT (March 2022)¹⁸ - The aim of the Highway Code is to promote safety on the road for all users, whilst also supporting a healthy, sustainable and efficient transport system.
- 3.8 **Inclusive Mobility**, DfT (December 2021)¹⁹ - This document is a guide to best practice on access to pedestrian and transport infrastructure. Creating and maintaining an accessible public realm is crucial for ensuring that disabled people are not excluded from playing a full role in society.
- 3.9 **Future of Transport: Rural Strategy Call for Evidence Summary of Responses**, DfT (September 2021)²⁰ - The document summarises the responses to the DfT consultation on understanding future rural transport issues, needs and opportunities. Cycling and walking is referenced as an important sustainable transport solution for rural areas, especially given the potential of electric micromobility options such as e-bikes and e-scooters. The need for cycling and walking infrastructure is also referenced as important in facilitating greater rural cycling and walking.
- 3.10 **Future of Mobility: Urban Strategy**, DfT (March 2019)²¹ - The Strategy sets out the approach Government will take to seize the opportunities from

¹⁸ <https://www.gov.uk/guidance/the-highway-code>

¹⁹ https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/1044542/inclusive-mobility-a-guide-to-best-practice-on-access-to-pedestrian-and-transport-infrastructure.pdf

²⁰ https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/1017933/future-of-transport-rural-strategy-call-for-evidence-summary-of-responses.pdf

current and future changes in urban transport. The approach includes the principle that active travel will remain the best option for short urban journeys.

- 3.11 **Manual for Streets 1**, DfT (March 2007) & **Manual for Streets 2**, Chartered Institution of Highways and Transportation (CIHT) (September 2010)²² - These two complementary guidance documents seek to provide guidance for the design of residential streets for England and Wales, with the aim to create sustainable and inclusive public spaces. A third version is currently being prepared.
- 3.12 **National Design Guide**, DLUHC (October 2019)²³ - The Guide illustrates how well-designed places can be achieved in practice. One of the identified 10 characteristics for achieving well-designed places is ‘movement’. Movement refers to the ease with which people can move around and navigate places and is therefore of particular importance to this Strategy.
- 3.13 **National Model Design Code**, DLUHC (July 2021)²⁴ - The Code provides detailed guidance on the production of design codes, guides and policies to promote good design. As with the National Design Guide, the Code recognises the importance of cycling and walking in relation to well-designed street networks, active travel solutions and public transport.

²¹

https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/846593/future-of-mobility-strategy.pdf

²² <https://www.ciht.org.uk/knowledge-resource-centre/resources/revising-manual-for-streets/>

²³ <https://www.gov.uk/government/publications/national-design-guide>

²⁴ <https://www.gov.uk/government/publications/national-model-design-code>

County

- 3.14 **Suffolk Local Transport Plan (Part 1 and Part 2)**, SCC (2011)²⁵ - The plan sets out SCC's long-term transport strategy for Suffolk and explains how SCC will implement it. Part 1 is a 20-year strategy that highlights SCC's long-term ambitions for the transport network. Part 2 is a four-year implementation plan indicating how SCC are proposing to address the issues identified within the longer-term transport strategy.
- 3.15 **Suffolk Design Streets Guide**, SCC (2022)²⁶ - This guidance sets out the ways in which designers should identify movement frameworks and detailed street designs based on the appropriate user hierarchy, and will assist with the creation of new places in delivering sustainable transport compatible layouts which support and incentivise walking and cycling over less sustainable transport modes.
- 3.16 **Suffolk Local Cycling and Walking Infrastructure Plan (LCWIP)**, SCC (2021)²⁷ - The document sets out a series of measures to achieve a transformational change in the levels of cycling and walking across Suffolk.

²⁵ <https://www.suffolk.gov.uk/roads-and-transport/transport-planning/transport-planning-strategy-and-plans/>

²⁶ <https://www.suffolk.gov.uk/assets/planning-waste-and-environment/planning-and-development-advice/Suffolk-Design-Streets-Guide-R-compressed.pdf>

²⁷ <https://www.suffolk.gov.uk/assets/coronavirus/Advice-on-travel/Local-Cycling-and-Walking-Infrastructure-Plan-for-Suffolk.pdf>

- 3.17 **Suffolk Guidance for Parking**, SCC (2019)²⁸ - This guidance sets out the vehicle and cycle parking standards for Suffolk as well as design considerations for different parking arrangements.
- 3.18 **Suffolk Green Access Strategy 2020-2030**, SCC (2020)²⁹ - The Green Access Strategy outlines SCC's future plans for public rights of way in Suffolk, including management of and improvements to the public rights of way network. In drafting the recommendations of the Strategy, ESC has paid particular attention to our public rights of way.
- 3.19 **Suffolk Transitional Joint Health and Wellbeing Strategy 2022-2023**, SCC (2021)³⁰ - The Strategy sets out a number of key strategic priorities that will lead to an improvement in health and wellbeing outcomes and a reduction in health inequalities across Suffolk, including through the facilitation of more active lifestyles.
- 3.20 **Transport East Active Travel Strategy**, Transport East (October 2021)³¹ - The Strategy seeks to identify solutions that would deliver a step change in cycling and walking across the region, which would facilitate sustainable growth while decarbonising transport and levelling up the region's rural and coastal areas.

²⁸ <https://www.suffolk.gov.uk/assets/planning-waste-and-environment/planning-and-development-advice/Suffolk-Guidance-for-Parking-2019-Adopted-by-SCC.pdf>

²⁹ <https://www.suffolk.gov.uk/roads-and-transport/public-rights-of-way-in-suffolk/green-access-strategy/>

³⁰ https://www.healthysuffolk.org.uk/uploads/Transitional_JHWS.pdf

³¹ <https://storymaps.arcgis.com/stories/2a9dc81f27494cde86f636abde155a49>

Local

- 3.21 **East Suffolk Council Strategic Plan 2020-2024**, ESC (2020)³² - The Strategic Plan sets out the five key themes (growing our economy, enabling our communities, remaining financially sustainable, delivering digital transformation, and caring for our environment) that will help the council achieve its aim of delivering the highest quality of life possible for everyone who lives in, works in, and visits East Suffolk. This Strategy seeks to deliver on this aim and build on all five Strategic Plan themes.
- 3.22 **East Suffolk Council Local Plans**, - Waveney Local Plan³³ adopted March 2019 and Suffolk Coastal Local Plan³⁴ adopted September 2020. Both Local Plans seek to encourage walking and cycling, and both include specific policies relating to sustainable transport.
- Suffolk Coastal Local Plan Policy SCLP7.1: Sustainable Transport
 - Waveney Local Plan Policy WLP8.21: Sustainable Transport
- 3.23 The Local Plans also allocate land for development, in the form of site allocation policies. Where specific cycling and walking measures were identified through the local plan process these are detailed as policy criteria in the specific site allocation policies. In addition to these policy requirements, the Strategy has, where appropriate, identified a number of additional opportunities for cycling and walking associated with these site

³² <https://www.paperturn-view.com/uk/east-suffolk/strategic-plan-2020-2024?pid=Nzg78875&v=1.1>

³³ <https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/local-plans/>

³⁴ <https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/local-plans/>

allocations, and these are detailed in the Key Corridor and Site Allocation Recommendation sections.

- 3.24 The Suffolk Coastal Local Plan, as part of the Ipswich Strategic Planning Area (ISPA)³⁵, sets out a further commitment to improvements to support sustainable transport to address the impacts of planned growth on the road network in and around Ipswich. An ISPA Transport Mitigation Strategy was prepared by SCC in 2019³⁶. The Council is working with other authorities in the ISPA to develop an approach for funding and delivery of key improvements. This work is anticipated to include a mechanism for identifying the extent to which cycling and walking projects would contribute to addressing highways capacity issues in and around Ipswich, which could be taken forward through the post adoption prioritisation work explained in paragraphs 2.08-2.10.
- 3.25 The East Suffolk Cycling and Walking Strategy will support the strategic priorities/objectives outlined in both Local Plans.

Suffolk Coastal Local Plan

- Strategic Priority - To support healthy, safe, cohesive and active communities through improving health, wellbeing and education opportunities for all;

³⁵ The ISPA Authorities are East Suffolk Council, Ipswich Borough Council, Babergh & Mid Suffolk District Councils, and Suffolk County Council. Further information on the ISPA can be found here: <https://www.ipswich.gov.uk/content/ipswich-strategic-planning-area>

³⁶ <https://www.suffolk.gov.uk/assets/Roads-and-transport/public-transport-and-transport-planning/ISPA-Transport-Mitigation-v13F.pdf>

- Strategic Priority - Mitigate human impact on the environment and reduce contributions to climate change by conserving natural resources.

Waveney Local Plan

- Objective/Priority 1 - To improve health, wellbeing and education opportunities for the population;
- Objective/Priority 4 - To reduce contributions to climate change and mitigate the effects and conserve natural resources.

3.26 **Local Plan for the Broads**, Broads Authority (May 2019)³⁷ - Part of East Suffolk falls within the Broads Authority area. The Broads Authority Local Plan contains policies to address sustainable transport and safeguard cycling and walking routes. The Broads Authority has recently embarked on a review of the Local Plan for the Broads; the relevant improvements identified in this Strategy may be used to inform that review.

3.27 **The Broads Plan**, Broads Authority (2017)³⁸ - This key management plan for the Broads executive area sets out a long-term vision to enhance the special qualities of the area, covering the period 2017-2022. The vision recognises the important role that cycling and walking play in facilitating the enjoyment of the special qualities of the Broads.

³⁷ https://www.broads-authority.gov.uk/__data/assets/pdf_file/0036/259596/Local-Plan-for-the-Broads.pdf

³⁸ https://www.broads-authority.gov.uk/_data/assets/pdf_file/0023/240665/Broads-Plan-2017.pdf

³⁹ https://www.broads-authority.gov.uk/_data/assets/pdf_file/0020/260822/Appendix-Broads-Integrated-Access-Strategy-and-action-plan.pdf

3.28 **Integrated Access Strategy for the Broads**, Broads Authority (2019)³⁹ - The Strategy aims to deliver the access and transport elements of the Broads Plan and in so doing seeks to encourage sustainable transport choices such as public walking and cycling.

3.29 **Development Briefs**, ESC (2020 onwards)⁴⁰ - ESC is in the process of preparing Development Briefs for some of our Local Plan site allocations, the of aim of which is to highlight key design considerations and design solutions for specific development sites. Cycling and walking is an important design consideration and the Strategy will therefore be a useful evidence base to support the preparation of the Development Briefs. The first Development Brief was adopted for Policy WLP2.14 (Land North of Union Lane, Oulton)⁴¹, and highlights the key cycling and walking infrastructure opportunities both on and off site.

3.30 **Suffolk Coast and Heaths AONB Management Plan**, The Suffolk Coast and Heaths AONB Partnership (2018)⁴² - The Management Plan outlines how relevant authorities will pay regard to the purposes of the AONB, provides a framework for the co-ordination of actions that might impact on the AONB, and identifies the need to monitor changes to the AONB.

3.31 **Suffolk Coast and Heaths AONB Walking and Cycling Guides**, Suffolk Coast and Heaths AONB Partnership⁴³ - The guides seek to provide opportunities

⁴⁰ <https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/supplementary-planning-documents/>

⁴¹ <https://www.eastsuffolk.gov.uk/assets/Planning/Planning-Policy-and-Local-Plans/Supplementary-documents/Residential-development-Union-Lane-Oulton/Residential-Development-Brief-for-WLP2.14-Land-North-of-Union-Lane-Oulton.pdf>

⁴² <https://www.suffolkcoastandheaths.org/managing/management-plan/>

⁴³ <https://www.suffolkcoastandheaths.org/exploring/visitor-guides/>

for cycling and walking exploration of the AONB in a manner sensitive to its special qualities.

Neighbourhood

3.32 In addition to the Local Plans there are a number of made and emerging Neighbourhood Plans⁴⁴ across East Suffolk, some of which include

sustainable transport policies and/or site allocations that will help provide or improve existing cycling and walking infrastructure and have influenced the recommendations within the Strategy. Neighbourhood Plan groups are encouraged to use this Strategy to inform their approach to cycling and walking.

⁴⁴ <https://www.eastsuffolk.gov.uk/planning/neighbourhood-planning/neighbourhood-plans-in-the-area/>

4 Recommendations



4 Recommendations

- 4.1 Analysing available evidence and community representations, and utilising officer knowledge at both ESC and SCC, has led the Strategy to the following four types of recommendations:
- 4.2 **Key Corridors** - Key routes between, and through, settlements where there are significant opportunities for modal shift to arise from improved cycling and walking infrastructure.
- 4.3 **Leisure routes** - A number of routes have been identified, connecting some more rural settlements. The primary function of these routes is to facilitate leisure opportunities.
- 4.4 **Local Plan Site Allocation Recommendations** - Recommendations for Local Plan site allocations, covering both on and off-site opportunities.
- 4.5 **Community Recommendations** - Recommendations submitted to the Council as part of the initial consultation. These have been assessed against the methodology set out in chapter 3. These have also been useful in identifying opportunities for the above three recommendation types.
- 4.6 The recommendations will be subject to detailed design work and further engagement before implementation.

Key Corridors

- 4.7 Key Corridors are routes between and through settlements, serving homes and destinations, where there are significant opportunities for modal shift, generated from improving cycling and walking infrastructure. They include:
 - Ipswich to Melton
 - Ipswich to Felixstowe
 - Martlesham to Felixstowe
 - Felixstowe Internal Routes
 - Lowestoft Internal Routes
 - Lowestoft to Hopton (to Great Yarmouth)
 - Lowestoft to Kessingland
 - Lowestoft to Bungay
- 4.8 These Key Corridors have been informed by analysis of the responses to the initial map-based and draft Strategy consultations, and the identification of patterns in those responses, between and through settlements.
- 4.9 After identifying the broad Key Corridor locations, officers considered the following before making infrastructure recommendations for each Key Corridor:
 - Local Plan and Neighbourhood Plan site allocations along and around the Key Corridor,

- comments made to the initial map-based consultation⁴⁵ (community recommendations),
- comments made to the draft Strategy consultation⁴⁶,
- the quality of existing cycling and walking infrastructure within each Key Corridor,
- trip data in the form of Strava Metro data⁴⁷,
- estimations of cycling potential in the form of Propensity to Cycle Tool⁴⁸,
- the Public Rights of Way network⁴⁹,
- key destinations,
- designated and proposed Quiet Lanes⁵⁰ (a project that seeks to designate suitable rural roads/lanes as Quiet Lanes and to make them safer),
- satellite imagery and ‘Streetview’ technology⁵¹,
- Sustrans National Cycle Network⁵²
- the England Coast Path, and
- ESC and SCC officer feedback.

⁴⁵<https://eastsuffolk.maps.arcgis.com/apps/webappviewer/index.html?id=810e5f8977e144509f13120a00a341d6>

⁴⁶<https://eastsuffolk.maps.arcgis.com/apps/webappviewer/index.html?id=81afd408d4524f8d93aea3ad85a74310>

⁴⁷ <https://metro.strava.com/>

⁴⁸ <https://www.pct.bike/>

4.10 Site visits have also been undertaken in order to clarify observations from satellite imagery, ‘Streetview’ technology, and officer knowledge of the local area.

4.11 In order to highlight their importance, each recommendation has been ranked as either medium, high or very high priority. In determining the priority of a particular recommendation, officers considered the importance of the recommendation within the context of the wider Key Corridor, whether the recommendation is an alternative to a more important recommendation, and the likely potential for delivery. This initial prioritisation was based on officer judgement to inform the finalisation of the Strategy. As part of the implementation of the recommendations a detailed Prioritisation Methodology has been developed by ESC and SCC, which is explained in more detail in the ‘Implementation and Monitoring of the Strategy’ section above.

4.12 Due to the terms of the Council’s licencing agreement with Strava Metro, we are unfortunately not able to display images of the Strava Metro data that were factored into the assessments within the Strategy. However, where Strava Metro data has aided the understanding of an area and the

⁴⁹ <https://www.suffolk.gov.uk/roads-and-transport/public-rights-of-way-in-suffolk/view-definitive-maps-of-public-rights-of-way/>

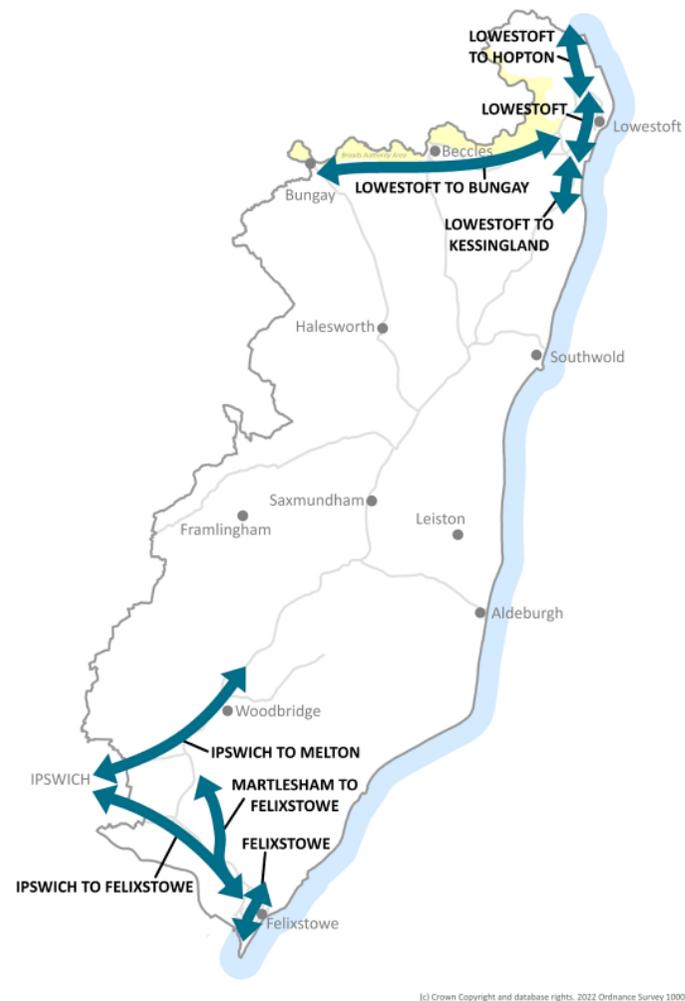
⁵⁰ <https://www.quietlanessuffolk.co.uk/>

⁵¹ <https://www.google.com/maps/place/Suffolk/@52.3022587,1.4882965,10.35z/data=!4m5!3m4!1s0x47d81562eecf1ae1:0xb8cf4391eed96afa!8m2!3d52.1872472!4d0.9707801?hl=en>

⁵²<https://www.sustrans.org.uk/national-cycle-network>

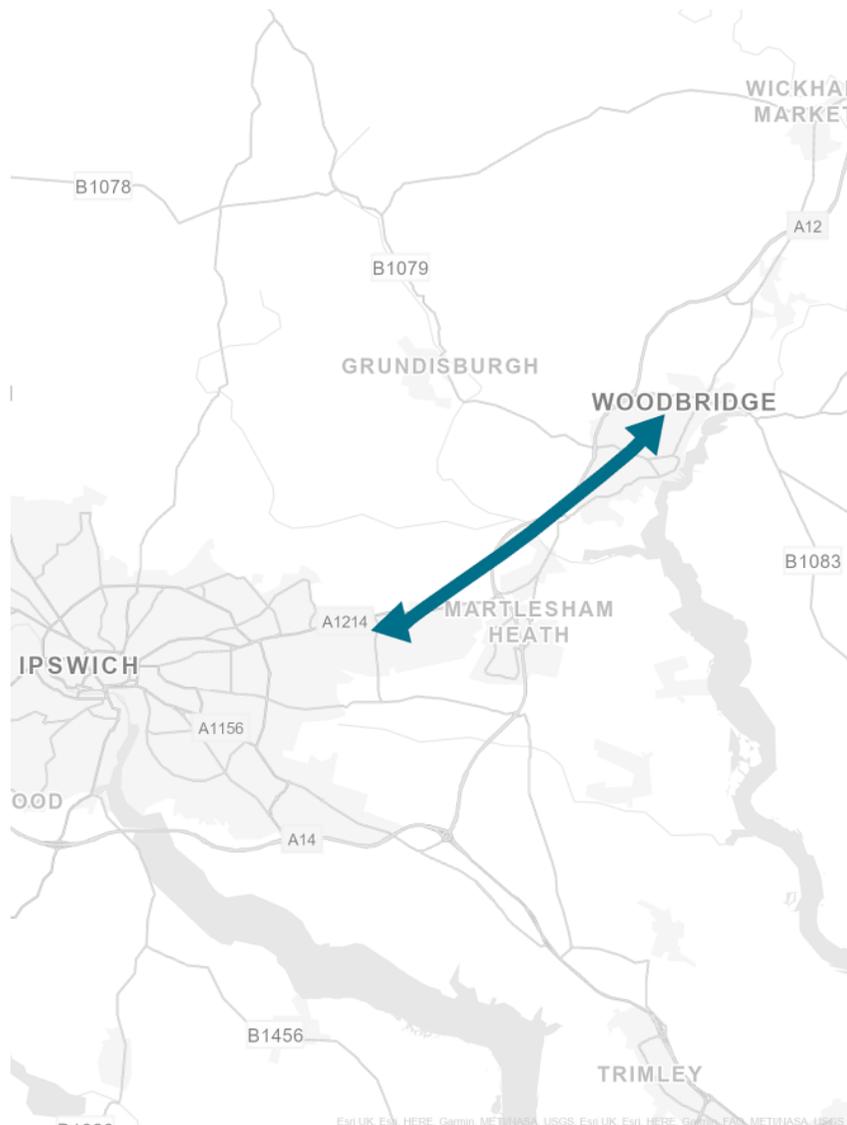
potential infrastructure opportunities, written summaries of the Strava Metro data are included. There is a publicly accessible version of Strava Metro⁵³, which may be of interest.

- 4.13 The Key Corridors typically connect the more urban areas of the district. Understandably these areas present greater opportunities to enable modal shift towards more sustainable transport modes and often represent a greater benefit to cost ratio when it comes to funding such infrastructure. This includes through developer contributions within those corridors. However, consideration has been given to cycling and walking opportunities within rural communities through the identified leisure routes.
- 4.14 Each Key Corridor and their associated recommendations are set out in the tables below. The following Key Corridor tables are presented to highlight the specific cycling and walking infrastructure recommendations for each section of a key corridor, alongside a description of the route, the identified priority for delivery, and a reference code.



⁵³ <https://www.strava.com/heatmap#11.85/1.28283/52.06159/hot/all>

Ipswich to Melton Key Corridor



4.15 The key opportunities that have been identified in support of this Key Corridor include: The Ipswich to Melton Key Corridor seeks to provide a cohesive cycling and walking network, which maximises off-road routes and delivers improvements of the highest quality. The route encompasses the parishes of Rushmere St Andrew, Kesgrave, Foxhall, Martlesham, Brightwell, Bucklesham, Woodbridge and Melton. Some of the recommendations that form part of the Key Corridor are as follows: Enhanced cycling and walking infrastructure along Main Road, Kesgrave, connecting into Kesgrave High School. Cycling and walking infrastructure through Rushmere Common providing a traffic free cycling and walking route between Ipswich and Kesgrave.

- Cycling and walking route along the south of Kesgrave, Long Strops Bridleway.
- Improvements to Felixstowe Road, Martlesham to enhance cycling and walking environment.
- Removal of through traffic from Sandy Lane.
- Cycling and walking infrastructure along Melton Road.
- Cycling and walking infrastructure along Wilford Bridge Road.

4.16 The key opportunities that have been identified in support of this Key Corridor include:

- The planned development in both the Suffolk Coastal Local Plan and Neighbourhood Plans,
- Primary and secondary schools,
- Ipswich Hospital,
- Martlesham Retail Park,
- Brightwell Lakes allocation,
- Adastral Park,
- Martlesham Park and Ride,

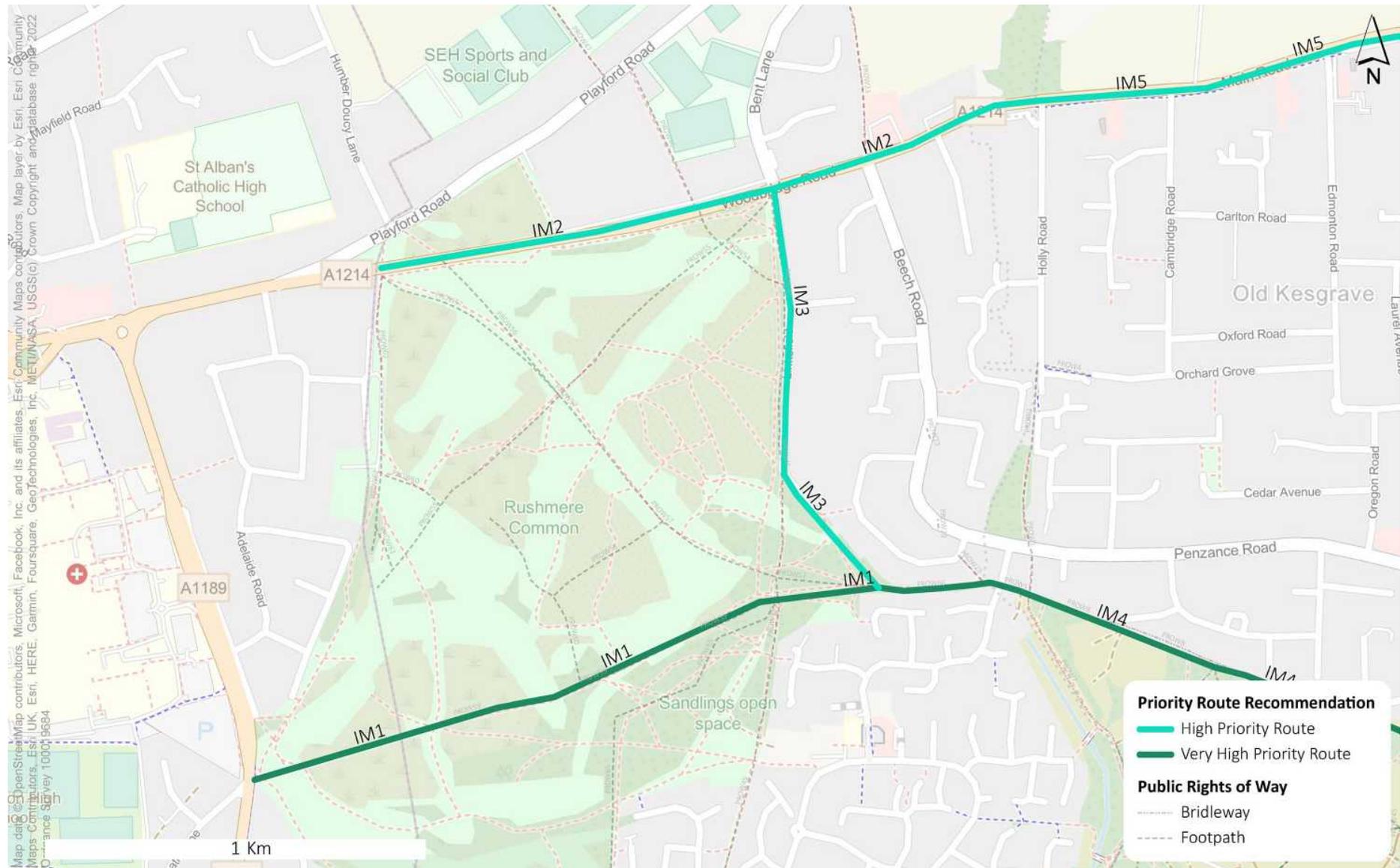
- Melton and Woodbridge Train Stations,
- Tourism destination such as Sutton Hoo,
- Woodbridge Mini-Holland feasibility study⁵⁴,
- SCC's A12 Major Road Network proposals⁵⁵, and
- Further services and facilities within the settlements throughout the Key Corridor.

4.17 Within this Key Corridor the Propensity to Cycle Tool (PCT) map shows the importance of Main Road, Kesgrave as the most popular route for cyclists and also capable of significant modal shift. It is worth noting however that PCT does not calculate modal shift potential on off-road routes such as Public Rights of Way (PROW), and therefore doesn't show the potential modal shift opportunities along routes such as the Long Strops bridleway. As Strava Metro compiles data for both on and off-road routes, it provides a clearer picture than PCT as to which routes are popular. From this data it is clear that Public Rights of Way provide a very important network of traffic free cycling and walking routes, albeit the quality of the PROW infrastructure could be improved in order to generate more significant modal shift. Making the most of and improving the quality of the off-road and traffic free PROW are important in increasing cycling and walking numbers to ensure users feel safe and comfortable.

⁵⁴ <https://www.suffolk.gov.uk/council-and-democracy/council-news/show/woodbridge-set-to-go-dutch-as-suffolk-gets-millions-from-government-to-boost-walking-and-cycling-2>

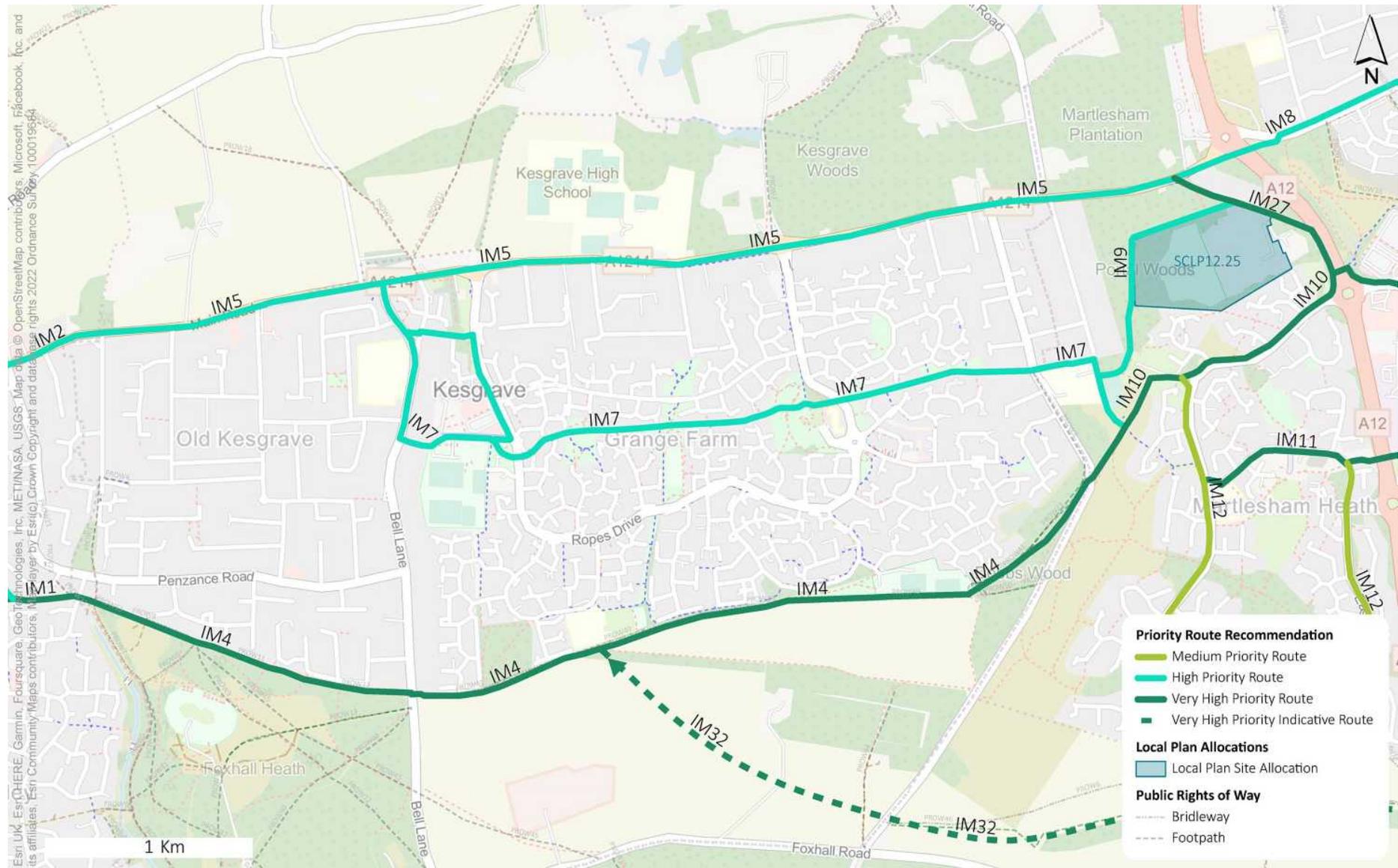
⁵⁵ <https://www.suffolk.gov.uk/council-and-democracy/consultations-petitions-and-elections/consultations/a12-improvements/#:~:text=The%20Scheme%20will%20enhance%20highway,cycling%20and%20public%20transport%20facilities>

Ipswich – Kesgrave



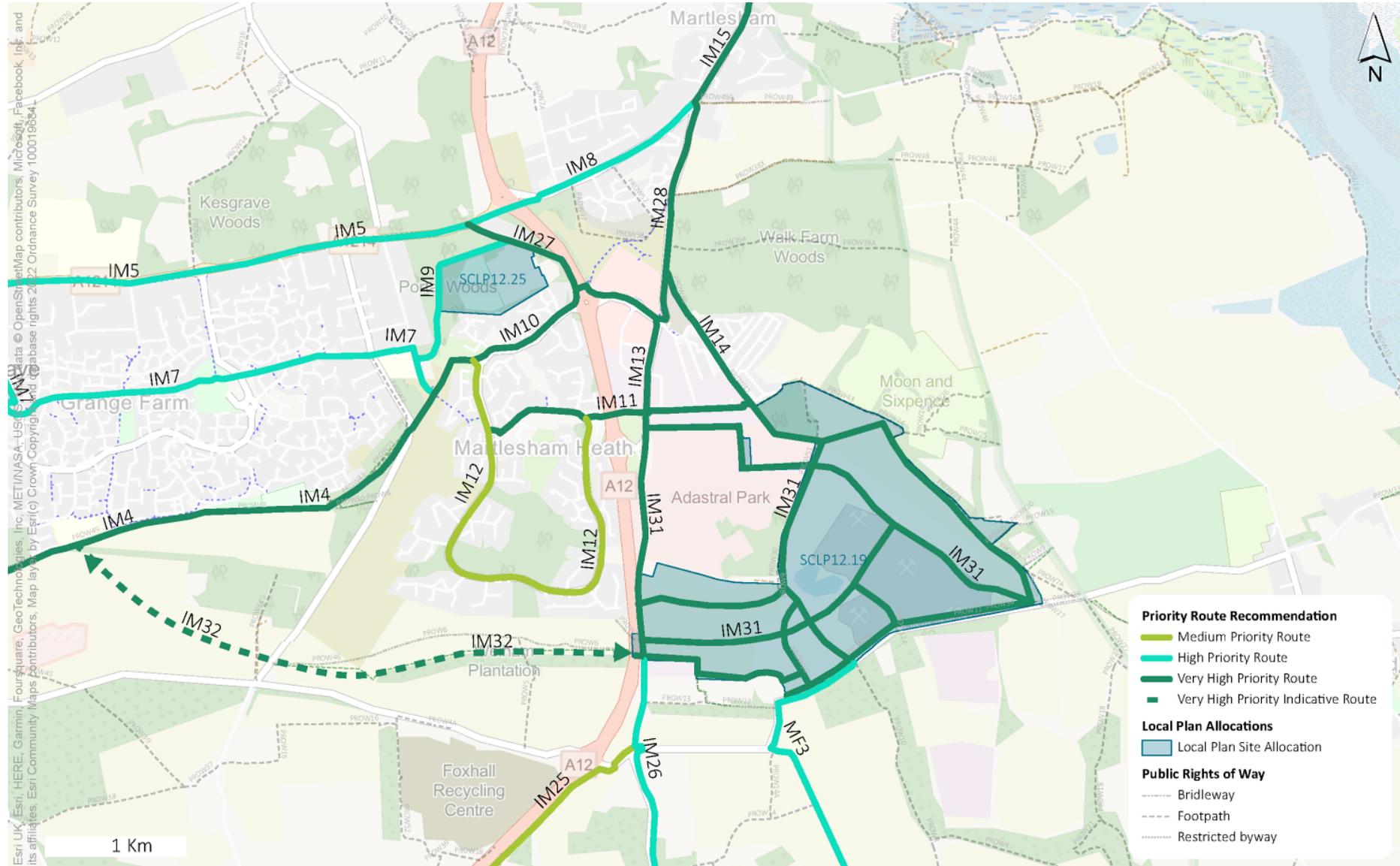
Map Reference	Location	Description	Recommendation
IM1	PROW59/66	Existing footpaths through Rushmere Heath provide a safe and desirable off-road walking route to Ipswich Hospital and Ipswich beyond, making it a very high priority route.	Upgrade Footpaths 59, a short section of 45 and 57, and 66 to bridleways, widen and resurface accordingly. ESC/Ipswich Borough Council (IBC) boundary crosses the Heath before reaching Heath Road. No PROW on IBC side of Rushmere Heath. Introduce Bridleway from Footpath 59 to Heath Road.
IM2	Woodbridge Road	Main east-west vehicle and cycling and walking route along Kesgrave, as evidenced by Strava Metro data, connecting to Kesgrave High School. For these reasons the route is a high priority route.	Introduce segregated cycling and walking infrastructure along Woodbridge Road. This may require road space and junction reconfiguration between Bent Lane and Holly Road. Introduce cycling and walking crossing points at Footpath 57 and at Beech Road junction.
IM3	PROW57	Footpath 45 and Linksfield provide a direct walking connection between Woodbridge Road and Long Strops Bridleway, and could provide a direct cycling route if upgraded. This recommendation is high priority , but would be a very high priority if IM1 can't be delivered.	Introduce a shared cycling and walking path. Surfacing and lighting should be appropriate to the natural setting whilst accommodating commuter cyclists.

Kesgrave – Martlesham Heath



Map Reference	Location	Description	Recommendation
IM4	Long Strops Bridleway	Long Strops Bridleway runs along the southern edge of Kesgrave, linking to Rushmere St Andrew in the west and Martlesham in the east. Long Strops is a dirt track and therefore not as desirable a route for cycling and wheelchair access as it could be, especially when wet. However, Strava Metro data shows the route to be well used. If the route were to be upgraded it could become a highly desirable off-road route all year round while retaining its status as a desirable recreational walking and cycling route, making it a very high priority route.	Widen and resurface Bridleways 8, 11, and 49 to accommodate bi-directional cycling and walking. Introduce cycling and walking crossing point on Bell Lane. Upgrade Footpaths 43, 23, and 3 to bridleways and widen and resurface accordingly. Introduce a signalised cycling and walking crossing point where Footpath 3 meets Dobbs Lane, and a village gateway on Dobbs Lane south of the proposed crossing point to slow northbound traffic.
IM5	Main Road	This stretch of road is the main vehicular route through Kesgrave and provides access to Kesgrave High School. Existing cycling and walking infrastructure provides some benefit for cyclists and walkers, however it could be improved. Although Main Road is particularly wide in places, with wide turning lanes and central cross hatching, the road narrows between Dr Watsons Lane and Ropes Drive, and introducing cycling and walking infrastructure along this stretch may be challenging. Due to the benefits associated with cycling and walking to school (e.g. Kesgrave High School) this route is high priority route.	Widen and resurface segregated cycling and walking track along Main Road. This may require road space and junction reconfiguration between Holly Road and Deben Avenue. Ensure cyclists and pedestrians have priority when crossing Main Road side streets. Ensure clear demarcation between cyclist and pedestrian space, provided appropriate infrastructure widths can be achieved.
IM7	Grange Farm cycleway	Through the middle of Grange Farm, Kesgrave runs a high quality segregated cycling and walking track, from Twelve Arce Approach in the west to Grange Lane in the east. However, it lacks a good connection to the cycling and walking infrastructure along Main Road which avoids the narrow section of Main Road between Dr Watsons Lane and Ropes Drive. Due to the high quality nature of the route and the need to connect it into the wider Key Corridor this route is high priority route.	Introduce cycling and walking track along Bell Lane, segregated from vehicles where space allows. Introduce cycling and walking crossing point on Bell Lane, aligned with Lankester Way desire line. Introduce cycling and walking track between Bell Lane crossing point and Lankester Way. Introduce crossing point on Bell Lane into Lankester Way. Remove bollards on Lankester Way and widen if possible. Along Ropes Drive, introduce cycle and walking priority across Ferguson Way, retail car park access, and Bus Lane. To enable greater mobility scooter access remove some of the bollards on Grange Lane, Dobbs Lane and Deben Avenue. Opportunities to move/remove the on street drop off area on Bell Lane outside Heath Primary School should be considered. This could facilitate improved cycling and walking infrastructure along Bell Lane.
IM27	Portal Avenue - Eagle Way	This route acts as a connection between Main Road and Eagle Way and the two A12 underpasses and for this reason is identified as very high priority route.	Introduce a cycling and walking track along Portal Avenue, avoiding tree removal where possible. Introduce sensitive lighting where necessary and resurface route between Portal Avenue and Eagle Way including a modal filter.

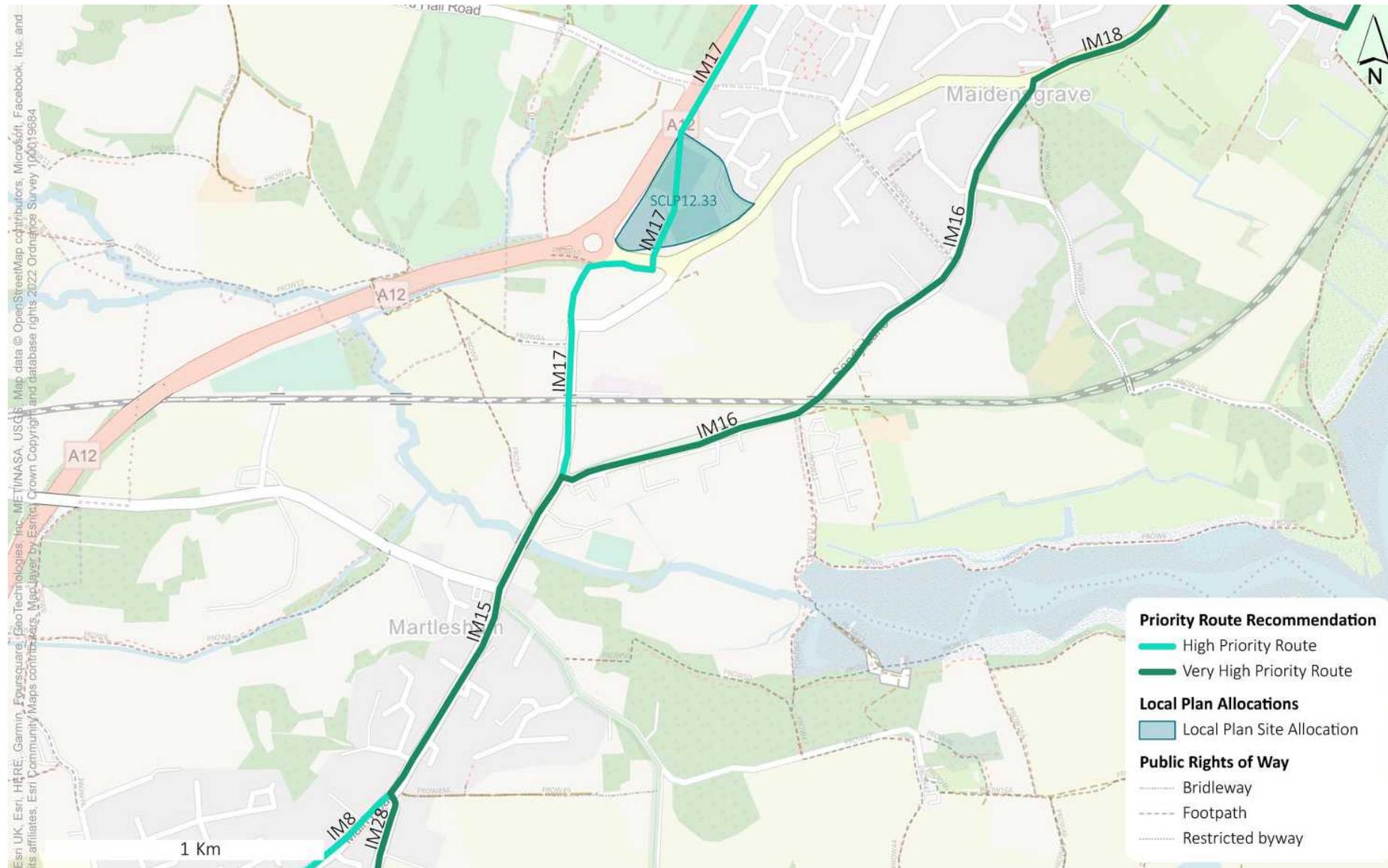
Martlesham Heath – Martlesham Village



Map Reference	Location	Description	Recommendation
IM8	Main Road	Main Road is wide in places which makes crossing, particularly for elderly people, more difficult. The continuous and direct nature of Main Road makes it a high priority route.	Widen existing cycling and walking infrastructure immediately east of the Portal Avenue junction. This may require road narrowing and/or signage removal. Ensure A12 underpass is suitable for cyclists and walkers. Introduce cycling and walking infrastructure between the A12 underpass and the existing cycling and walking infrastructure along the southern side of Main Road. Introduce crossing point where A12 underpass joins Main Road east of the roundabout. Introduce cycling and walking track along Main Road. Ensure cyclists and pedestrians have priority when crossing Main Road side streets. Consider narrowing Main Road, removing turning lanes, whilst ensuring safe cycling and walking crossing points.
IM9	Suffolk Police HQ	This route acts as a desirable off-road link between the cycling and walking network of Martlesham Heath/Kesgrave and Main Road, and is shown by Strava Metro to be well used, making it a high priority route.	Repair the existing cycling and walking track north and west of Suffolk Police HQ to accommodate cyclists, pedestrians and mobility scooters, whilst being sensitive to the natural and historic setting. Introduce sensitive lighting where necessary. Introduce cycling and walking connections between the route and the Suffolk Police HQ site allocation.
IM10	Dobbs Lane / Felixstowe Road	This route seeks to link Long Strops Bridleway to Felixstowe Road whilst avoiding the Martlesham Heath Site of Special Scientific Interest (SSSI), Martlesham Common County Wildlife Site (CWS), and the narrow route through Broomfield. Due to the direct link between Long Strops bridleway, Martlesham retail park and Felixstowe Road this route is very high priority route.	Introduce a cycling and walking track from the recommended crossing point at Dobbs Lane through to Felixstowe Road. This will require widening of existing infrastructure along Eagle Way, and giving priority to cyclists and pedestrians when crossing Eagle Way side streets and Anson Road roundabout arms. May also require moving the Eagle Way bus stop closer to the road, to provide space for track. Introduce Eagle Way crossing point into Manor Road. Ensure A12 underpass is suitable for cyclists and pedestrians.
IM11	Eagle Way / Betts Avenue	Using the existing cycling and walking infrastructure, this route connects Martlesham Heath, Martlesham Retail Park, and Brightwell Lakes. The planned Brightwell Lakes access point onto Gloster Road may provide an alternative east-west cycling and walking route to that of Betts Avenue. Due to the significant connections made by this route it is very high priority route.	Introduce segregated cycling and walking track along Eagle Way and Valiant Road over the existing A12 cycling and walking bridge, along Betts Avenue and into the planned Brightwell Lakes cycling and walking network. Where the cycling and walking track crosses vehicle access roads and side streets priority should be given to cyclists and pedestrians. The existing crossing point on Eagle Way (just north of Eagle Way/Valiant Road junction) may need to be moved further south to facilitate a cycling and walking track on the east of Eagle Way. Introduce crossing point on Valiant Road to link into existing cycling and walking infrastructure. Introducing the track on Eagle Way fronting the car park should avoid vegetation removal where possible. Introduce crossing point on Eagle Way, aligned with A12 cycling and walking bridge. Introduce crossing point on Gloster Road.
IM12	Martlesham Woods / Brightwell Lakes	This route seeks to provide cycling infrastructure along Eagle Way, linking into the existing A12 cycling and walking bridge and IM10. This is a medium priority route.	Introduce cycling infrastructure along Eagle Way, connecting into the A12 cycling and walking bridge, recommendation IM10, and recommendation IM11.

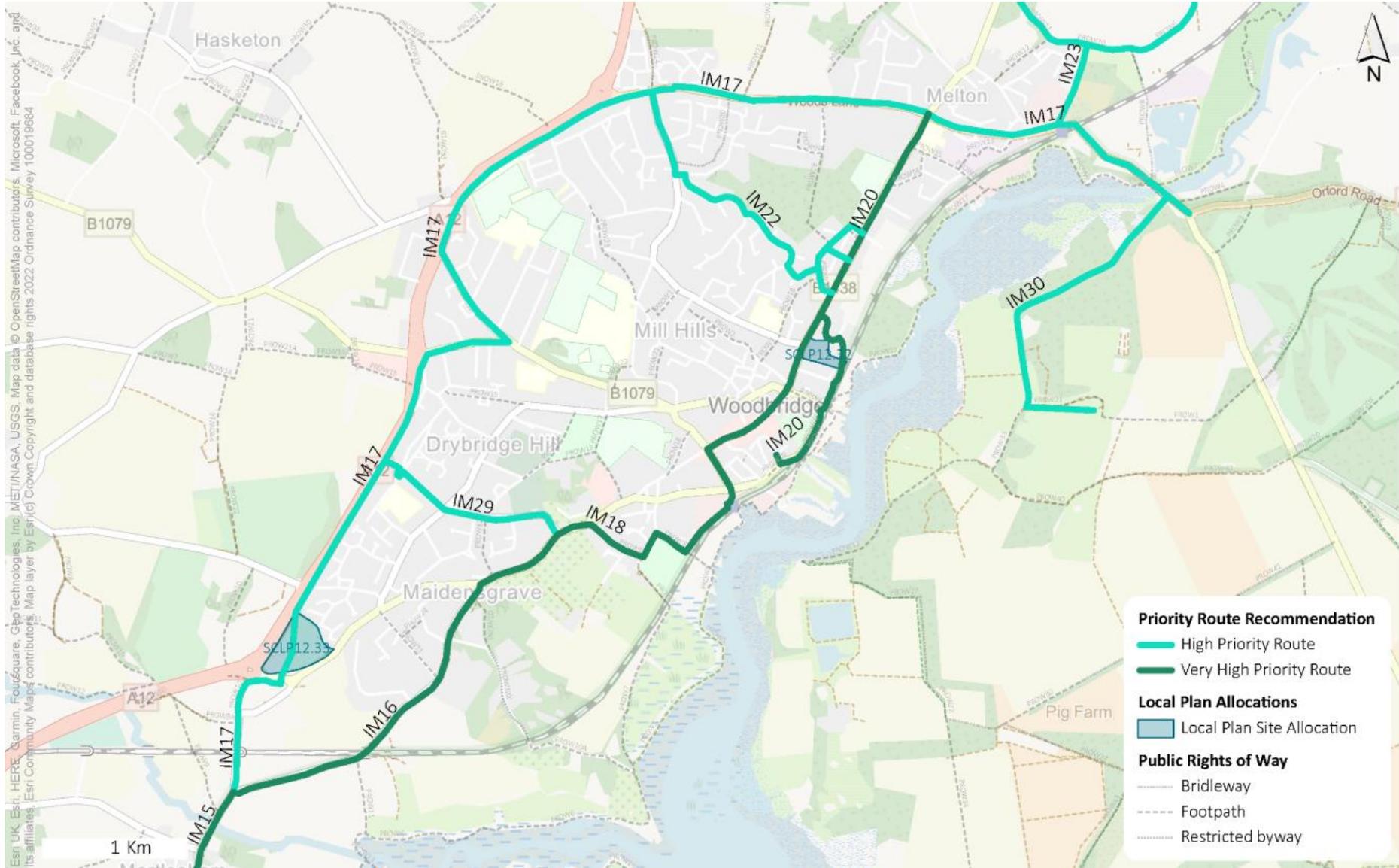
Map Reference	Location	Description	Recommendation
IM13	Barrack Square/Gloster Road	This route is designed to link the planned Brightwell Lakes cycling and walking infrastructure through Martlesham Retail Park to Felixstowe Road. Due to the connections to retail and employment as well as onward travel along Felixstowe Road this route is very high priority route.	Introduce segregated cycling and walking track along Barrack Square and Gloster Road. Where the cycling and walking track crosses vehicle access roads and side streets priority should be given to cyclists and pedestrians. Connect into the planned Brightwell Lakes cycling and walking infrastructure. Replace zebra crossing on Anson Road with a parallel crossing. Introduce parallel crossings along Gloster Road to reflect desire lines of pedestrians and cyclists throughout Martlesham retail park. Sufficient cycle parking should be introduced at all destinations within the retail park.
IM14	Footpaths/Felixstowe Road	This route seeks to provide an off-road route from the planned Brightwell Lakes cycling and walking infrastructure to Felixstowe Road, which provides a strong link to onward travel via Felixstowe Road, making it a very high priority route.	Introduce a cycling and walking track along Footpaths 42 and 40. Upgrade Footpaths 42 and 40 to bridleways and widen and resurface accordingly. Ensure cycling and walking track is appropriately lit, with natural surveillance where possible.
IM28	Felixstowe Road	Felixstowe Road is used heavily as a through route to access services and facilities at Martlesham Retail Park. However, the alternative vehicular route of Main Road and the lack of safe cycling and walking provision along Felixstowe Road has led this recommendation to be identified as a very high priority route.	Introduce modal filter on Felixstowe Road to prohibit vehicle through traffic.
IM31	Brightwell Lakes	These routes align with those planned as part of the consented Brightwell Lakes development and link into recommendations beyond the development. This route is very high priority route.	Cycling and walking infrastructure to be delivered as part of the consented Brightwell Lakes development.
IM32	Long Stroops - Brightwell Lakes	This route seeks to provide a high quality traffic free cycling and walking route between Long Stroops Bridleway and Brightwell Lakes, avoiding the Martlesham Heath SSSI, and for this reason is a very high priority route.	Introduce a segregated cycling and walking track between Long Stroops Bridleway and Brightwell Lakes, avoiding harm to the Martlesham Heath SSSI, and along a realigned Bridleway 6 as secured by condition 69 to the Brightwell Lakes outline permission. Introduce a cycling and walking crossing point across the A12. Ensure the route is well lit. Part of the route is secured through condition 69 to the Brightwell Lakes outline permission (DC/17/1435/OUT) which states: "Prior to first occupation of the residential development, a scheme shall be submitted to and agreed in writing by the Local Planning Authority in consultation with the County Council, for the diversion and/or improvement of the existing Bridleway BR6 running to the west of the A12 towards Dobbs Lane. Such scheme shall make provision for (1) mitigation of the impact of users on the Martlesham Heath SSSI and (2) implementation of the approved scheme. Reason: To facilitate improvements to the alignment of the existing bridleway whilst ensuring that the new alignment has no material impact on the Martlesham Heath SSSI.

Martlesham Village – Woodbridge



Map Reference	Location	Description	Recommendation
IM15	Main Road/Woodbridge Town Football Club site	This route is a continuation of the route along Main Road, however the cycling infrastructure in this stretch is poor or non-existent. There are also narrow stretches which may limit the ability to introduce high quality cycling and walking infrastructure. However, Main Road between Felixstowe Road and Sandy Lane is of very high priority due to its presence as part of the wider very high priority route along the key corridor.	Introduce crossing points along Main Road. Due to the narrowness of Main Road between Nunn Close and Bealings Road it is unclear what cycling and walking infrastructure could be introduced. Introduce segregated cycling and walking track at Bealings Road junction going north along The Street. This may require removal of central hatching turning lanes, and widening the River Fynn Bridge.
IM16	Sandy Lane	Sandy Lane provides access to a number of properties and businesses, however, is also used as a rat run and is not needed as a through route due to alternative vehicle route options along the B1438. With a relatively cost effective intervention this route could become a key cycling and walking connection between Martlesham and Woodbridge, making it a very high priority route.	Introduce a modal filter on Sandy Lane to restrict vehicle through traffic while allowing pedestrians, cyclists, emergency vehicles, and refuse vehicles.

Outer Woodbridge and Melton, Sandy Lane – Woodbridge Train Station and Woodbridge Train Station – Melton Crossroads

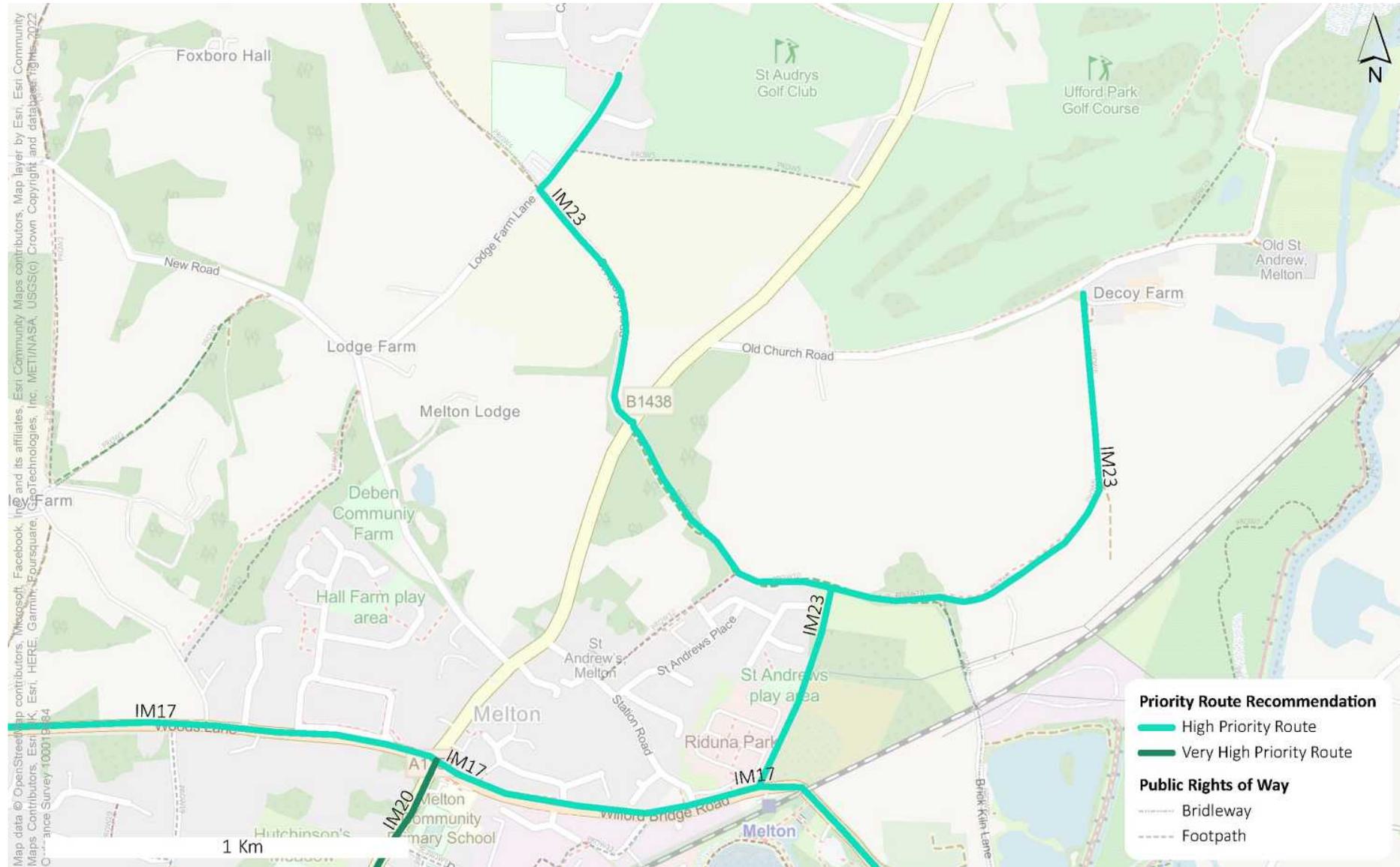


Map Reference	Location	Description	Recommendation
IM17	Outer Woodbridge route	This route seeks to provide a cohesive connection between Martlesham, Woodbridge, Melton Train Station and Common Lane, whilst making the most of existing cycling and walking infrastructure alongside the A12, making it a high priority route.	Introduce a segregated cycling and walking track from the Top Street/Sandy Lane junction, along Top Street, Footpath 10, through the Woodbridge Town Football Club site, along the eastern side of the A12 utilising the existing track to the rear of properties on Peterhouse Crescent where possible, along Woods Lane, Wilford Bridge Road, connecting into Melton Train Station, and along Orford Road to Common Lane which is a designated Quiet Lane. There is not sufficient space to introduce segregated cycling and walking infrastructure east of the A12 between the B1079 and Hasketon Road. A one way system should therefore be introduced along the B1079 and Hasketon Road, and the remaining roadspace reallocated to segregated cycling and walking infrastructure. Upgrade Footpath 10 to a bridleway and widen and resurface accordingly. Where a segregated cycling and walking track exists, widen and resurface to LTN 1/20 standards. Introduce crossing points at Haugh Lane, Bredfield Road, Station Road and on Wilford Bridge Road into Melton Train Station. Introduce cycling and walking priority at the Farlingaye High School A12 car park entrance and exit, Valley Farm Road, Nightingale Close, Hall Farm Road, and Church View Close. Replace fencing and curb at the western extent of Seckford Hall Road with bollards and connect to recommended segregated cycling and walking track along the A12. Whilst it is desirable that this route provides cycling and walking infrastructure along Woods Lane, it may not be possible due to insufficient space. IM22 seeks to provide an alternative route.
IM18	Ipswich Road - Woodbridge Train Station	This route provides a connection between Sandy Lane and Woodbridge Train Station and for this reason is identified as very high priority .	Where sufficient width, introduce segregated cycling and walking infrastructure along Ipswich Road. No need for cycling infrastructure along Cherry Tree Road, Kingston Farm Road, and Jetty Lane due to low traffic volume and speed. Introduce secure cycle storage at Woodridge Train Station.
IM20	Thoroughfare/Melton Road	This route follows the main vehicular route through Woodbridge from the north, and which, according to Strava Metro data, is also the main cycling and walking route into Woodbridge from the north. This is most likely due to the directness of the route and the unsuitability of other routes for cycling and walking. Due to the strong and direct route into Woodbridge Town Centre and Woodbridge Train Station the route is very high priority .	Introduce one way system or modal filter and widen footways on Quay Street to facilitate safe cycling and walking between the town centre and Woodbridge Train Station. Restrict vehicle access to the Thoroughfare to deliveries at specific times, emergency vehicles, and residents. Introduce more cycle storage at both ends of the Thoroughfare. Introduce advanced stop lines at Lime Kiln Quay Road/Thoroughfare junction, particularly for southbound cyclists. Melton Hill does not appear wide enough to introduce cycling and walking infrastructure. However, north of the Old Maltings Approach the road widens, at which point segregated cycling and walking infrastructure should be introduced along Melton Road to Melton Crossroads. On-street parking may require removal. Introduce advanced

Map Reference	Location	Description	Recommendation
			stop lines at the Melton Crossroads. An alternative less direct route should also be considered, from Old Maltings Approach through the former Council offices site, between the railway line and dwelling off Deben Road, along Deben Road and Quayside to the Hamblin Road car park. Introduce more cycle storage at Hamblin Road car park.
IM22	Bredfield Road/Melton Road	The route seeks to connect Woods Lane and Melton Road, and provide cycling and walking infrastructure to Woodbridge Primary School and Melton Playing Fields, whilst avoiding wooded areas. The route is identified as high priority .	Introduce segregated cycling and walking infrastructure, potentially stepped cycle tracks, from the existing infrastructure on the A12, as far along Bredfield Road as possible, Bury Hill, Saxon Way, and Melton Grange Road. Retain as much green verge as possible. Remove cycle barriers between Green Man Way and Turnpike Lane, and widen and resurface where possible. Upgrade Footpath 26 (Turnpike Lane) to a Bridleway, and resurface. Resurface the area of Hackney Road that remains poorly surfaced.
IM29	Peterhouse Crescent-Ipswich Road	This route provides a direct connection between the outer Woodbridge cycling and walking route and Ipswich Road, so for this reason is high priority .	Resurface the existing track between dwellings on Peterhouse Crescent with hard smooth surface and link to IM17. Where sufficient width, introduce segregated cycling and walking infrastructure along Warren Hill Road. No need for cycling infrastructure along the short section of Peterhouse Crescent and Clare Avenue due to low traffic volume and speed.
IM30	Sutton Hoo	This route seeks to provide a safe low traffic cycling and walking route between Wilford Bridge Road and Sutton Hoo. Due to the connection between Melton, Melton Train Station and a key trip attractor in Sutton Hoo this route is identified as high priority .	Natural England published plans ⁵⁶ in December 2020 to create a footpath along this route, which if approved by the Secretary of State for Environment, Food, and Rural Affairs will form part of the England Coast Path. The creation of this footpath will be highly beneficial in enabling safe walking between Melton and Sutton Hoo. However, opportunities should be taken to also allow cycling along this route through the creation of bridleways and the upgrading of Footpath 21 to a bridleway. Due to the highly sensitive natural and historic environment setting this route should not be lit and new surfacing should avoid an urbanising effect.

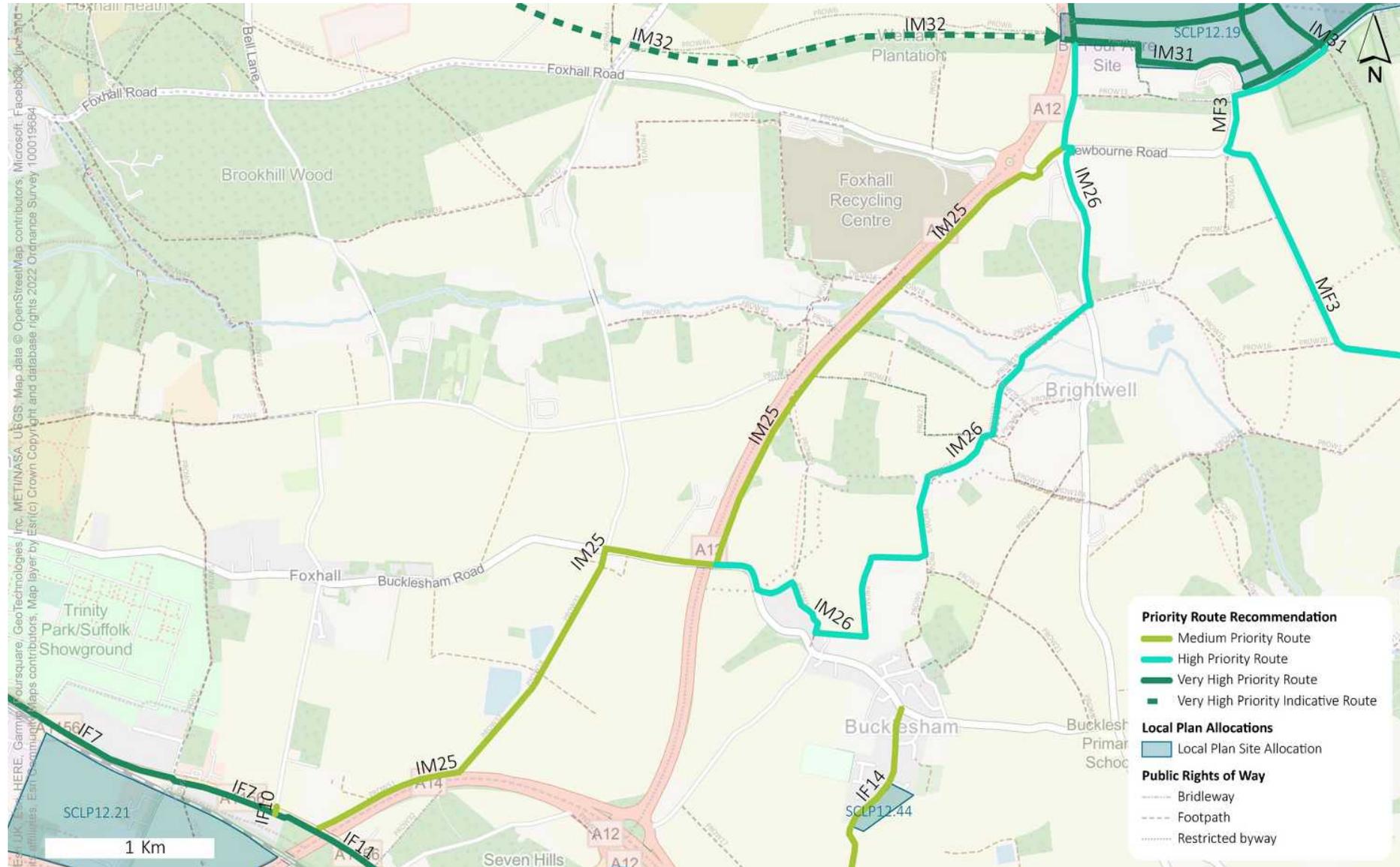
⁵⁶ <https://www.gov.uk/government/collections/england-coast-path-felixstowe-ferry-to-bawdsey>

Melton – Melton Park



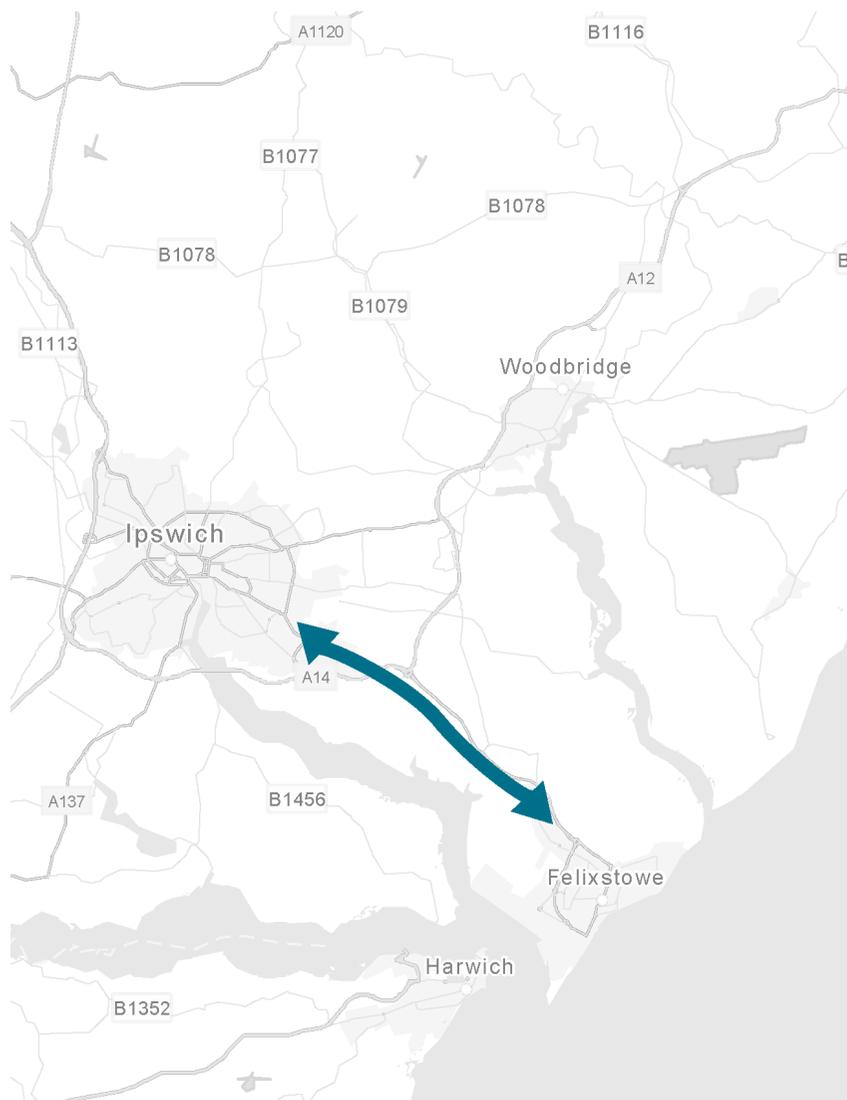
Map Reference	Location	Description	Recommendation
IM23	Melton Train Station/Melton Park	This route is intended to provide a safe and desirable cycling and walking route between Melton Park and Melton Train Station, as well as Ufford and Melton Train Station. Strava Metro shows Bridleway 10 and 11 to be well used, but this could be optimised with improvements to the infrastructure. Due to the planned connection to Melton Train Station that avoids Yarmouth Road the route is high priority .	Introduce a cycling and walking track through the Melton Neighbourhood Plan allocation (policy Mel20) connecting Melton Train Station directly to Bridleway 10. Widen and resurface Bridleways 10 and 11. Ensure the bridleways are appropriately lit. A modal filter exists on Lodge Farm Lane and therefore St Audrys Road and Lodge Farm Lane are likely subject to low traffic volumes and speeds. Upgrade Footpath 6 to a bridleway and widen and resurface accordingly. Consider opportunities to facilitate safe cycling and walking crossing of Yarmouth Road, between Bridleway 11 and St Audrys Road.

Brightwell Lakes – Ipswich



Map Reference	Location	Description	Recommendation
IM25	A12/Bridleway 31/18/32/51	This route seeks to provide a cycling and walking connection between Brightwell Lakes/Martlesham Heath and Felixstowe Road, thereby supporting onward travel to Ipswich and Felixstowe along the Ipswich – Felixstowe Key Corridor. This could be achieved whilst maximising the use of and enhancements to off-road routes. This route is a medium priority route due to the challenges associated with a new route and crossings over the Mill River.	Introduce a segregated cycling and walking track from Brightwell Lakes, along a short stretch of Newbourne Road, east of the A12, Bucklesham Road, Bridleways 31, 18, 32, and 51. Widen and resurface Bridleways 31, 18, 32, and 51. Introduce a cycling and walking crossing point on Newbourne Road, Bucklesham Road, and Felixstowe Road. Introduce a crossing point over Mill River and other watercourses. Avoid tree removal where possible. Ensure route is well lit, particularly in areas lacking natural surveillance. Widen the Bucklesham Road bridge over the A12 in order to introduce a segregated cycling and walking route.
IM26	Brightwell/Bucklesham	This route follows existing public rights of way and connect into and through Bucklesham, from Brightwell Lakes, and for this reason is identified as a high priority route .	Introduce a segregated cycling and walking track along Brightwell Road. Upgrade Footpaths 3, 19, 20, 21, 24, 3, 2, & 1 to bridleways, and widen and resurface accordingly. Introduce a segregated cycling and walking track between Bucklesham Road bridge and Footpath 1. Introduce a cycling and walking crossing point over Mill River and other watercourses.

Ipswich to Felixstowe Key Corridor



4.18 The Ipswich to Felixstowe Key Corridor’s route is made up of a combination of shared paths, modal filtered carriageway, and cycle tracks. It is intended to channel cyclists between Ipswich and Trimley St Martin, after which 'internal routes' will facilitate onward travel towards the centre of Felixstowe, the Port, the coastline, the North Felixstowe Garden Neighbourhood area, and Felixstowe Ferry. It is proposed to be provided through the following route:

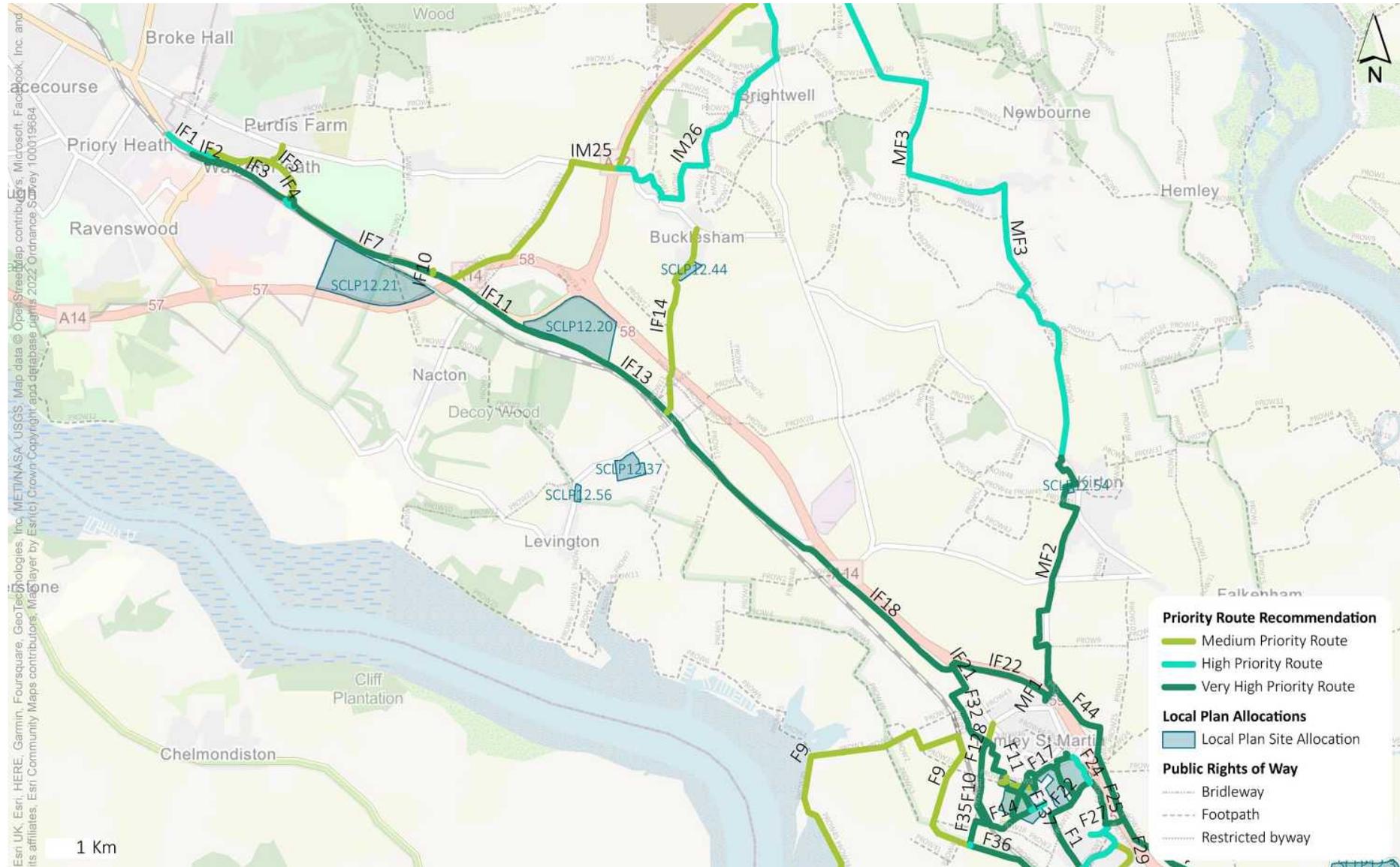
- New crossing needed over the southern arm of the Ransomes Way/Felixstowe Road roundabout (Warren Heath)
- Connect Felixstowe Road ('west') with Warren Heath via Murrills Road at Trinity Park roundabout
- Continuous infrastructure provided along Felixstowe Road 'west' between Warren Heath and Nacton, and then along Felixstowe Road 'east' to Morston Hall Road
- Add a new connection between Brightwell and Levington, via Felixstowe Road 'east', improving the crossing between Levington Lane and Felixstowe Road 'east'
- Introduce either a modal filter or a cycle track on Morston Hall Road
- Connect to High Road and A14 pedestrian/cycle bridge

4.19 Within this Key Corridor, Strava data shows moderate to high level of cycling activity along the main route between September 2020 and August 2021 and moderate amount of walking and/or running activity. The most popular cycling route correlates with the Key Corridor’s main route.

4.20 The key opportunities that have been identified in support of this Key Corridor include:

- Providing a commuter corridor between Ipswich, Nacton, Levington and Trimley St Martin for onward travel to Trimley St Mary and Felixstowe or up to the villages to the west of the river Deben or Brightwell Lakes.
 - The planned development in the Suffolk Coastal Local Plan,
 - Primary and secondary schools,
 - The Port of Felixstowe and associated employment opportunities,
- Opportunities associated with reallocating roadspace along Felixstowe Road,
 - Felixstowe and Trimley railway stations,
 - Felixstowe promenade and associated tourism opportunities, and
 - Further services and facilities within the settlements throughout the Key Corridor.

Ipswich – Felixstowe



Map Reference	Location	Description	Recommendation
IF1	Felixstowe Road 'west' (A1156) existing shared path, Warren Heath	There is an existing shared path that runs along the northern edge of Felixstowe Road 'west' via Warren Heath until it meets the Ransomes Way/Felixstowe Road 'west' roundabout extension (the pear-shaped extension) which serves to direct traffic into and out of the Sainsbury's/B&M site safely. This extension currently has platform crossings for pedestrians and cyclists. These platform crossings cross a wide stretch of carriageway and are not very deep for cyclists/pedestrians stood beside their bikes/pushchairs etc. This is considered a high priority route.	The existing shared path section that lies to the west of the Ransomes Way/Felixstowe Road roundabout, on the north side of the road, is to be widened all the way up to the northern arm of the roundabout. The crossing points over the roundabout's northern extension (for managing vehicular access into the Sainsbury's/B&M site car park) are recommended to be widened, deepened and for the carriageway to be narrowed by creating 'pinch point' crossings, so pedestrians and cyclists have less carriageway to cross. On the eastern side of the extension, there is an existing shared path that runs to an existing toucan crossing over Felixstowe Road 'west'; this section of shared path should also be widened and realigned slightly to make the transition safer and more convenient.
IF2	Ransomes Way (A1189)/Felixstowe Road 'west' (A1156) roundabout	The current refuge crossing point is located just as the lanes are widening out to join the roundabout. This has the added impact of reducing the person crossing the ability to assess when it is safe to cross compared to crossing several metres south on Ransomes Way. This is considered a very high priority route.	The existing platform crossing point over Ransomes Way is to be moved southwards to reduce the distance to be crossed by the pedestrian/cyclist. The crossing should also be upgraded to a toucan crossing. The existing shared path over the southern arm of the Ransomes Way/Felixstowe Road roundabout will need to be extended to connect to the new crossing's location. The shared path should also be widened and realigned to better connect into the new shared paths recommended for Felixstowe Road 'west' (IF3) and the existing toucan crossing over the eastern arm of the Ransomes Way/Felixstowe Road 'west' roundabout.
IF3	The section of Felixstowe Road (A1156) 'west' between Ransomes Way (A1189)/Felixstowe Road 'west' roundabout and the Trinity Park roundabout	This section of Felixstowe Road 'west' currently has an initial 40mph speed limit before increasing to national speed limit, yet has narrow mandatory (solid painted line) cycle lanes, which offer no segregation for cyclists from vehicular traffic, despite being the main vehicular route between two major settlements (Ipswich and Felixstowe). There are no pavements or formal footpaths along this stretch, only worn 'desire lines'. This is considered a very high priority route.	Introduce new shared paths on both sides of Felixstowe Road 'west' up to the Trinity Park roundabout, replacing the current mandatory cycle lanes; the advantage of shared paths over segregated cycle lanes here is that they would provide pedestrian access between these two roundabouts/the Trinity Park site (and onward towards Felixstowe if infrastructure is continued, as is intended) for the first time. If further assessment considers shared paths on both sides of Felixstowe Road 'west' to be undeliverable, then the southern side is to be prioritised for delivery if done in conjunction with delivery of IF4 (roundabout bypass) and at least the western arm crossing recommended under IF5 (for access to Murrills Road). . The

Map Reference	Location	Description	Recommendation
			south side is preferred as it best links into IF4, which allows cyclists and pedestrians to avoid crossing over the northern arms of the Trinity Park roundabout when travelling onward towards Felixstowe (via IF7). If only the north-side is assessed to be deliverable, this is an acceptable second fall-back option if delivered in conjunction with at least the crossing over the western arm through IF5 (for access to Murrills Road), and the improvements to the northern arms of the Trinity Park roundabout through IF36.
IF4	Trinity Park roundabout (southern side)	The Trinity Park roundabout currently has no pedestrian infrastructure for crossing it or moving between the access for Trinity Park and Felixstowe Road 'west' – likely because no pavements exist to the east or west for onward travel on foot. This is considered a high priority route.	Introduce a new fully-segregated pedestrian/cycle roundabout bypass that runs around the south side of the Trinity Park roundabout. The carriageway absorbed to create the roundabout bypass must still allow sufficient width for HGVs to safely move around the roundabout. As for other recommendations in this area, shared paths are preferred for both sides of Felixstowe Road 'west' along both of the sections that lead up to the roundabout, with a fall-back preference for delivery on the south side if shared paths on both sides cannot be delivered. However, if further assessment shows that only north-side shared paths can be delivered, then this bypass is not recommended, as the bypass would become too inconvenient to use - people would likely just cycle over the roundabout or exclusively walk around it via IF3.
IF5	Murrills Road	Murrills Road provides access to multiple residential streets, and is currently used moderately by cyclists. There is no cycling infrastructure along its length, and the pavement varies in quality. The pavement also stops entirely before the Trinity Park roundabout is reached, reflecting the current lack of pedestrian infrastructure on Felixstowe Road 'west', and the lack of infrastructure for pedestrian access to Trinity Park. Murrills Road is also a useful connection between Bucklesham Road and Warren Heath, providing access to the goods and services available in the industrial/retail areas of the Ransomes Industrial Estate and Futura Park. It currently has no cycling infrastructure, and the pavement surfacing could be improved. Murrills Park currently has cycle barriers at both the north and south access points. This is considered a medium priority route.	The provision of a continuous shared path on one side of Murrills Road is recommended, connecting to the large retail stores at the western end and the Trinity Park roundabout's western arm at the other end. The shared path should then connect directly to IF3 and over to IF4, via a new crossing. If a shared path can only be delivered on the north side of IF3 (second fallback option) then a crossing over the western arm of the roundabout would not be necessary; instead, improved crossing points over the two northern arms of the roundabout would be recommended, and a crossing over the eastern arm of the roundabout provided if shared paths are delivered on both sides, or just the south side, at IF7. The cycle barriers to Murrills Park are to be removed at both the north and south access points.

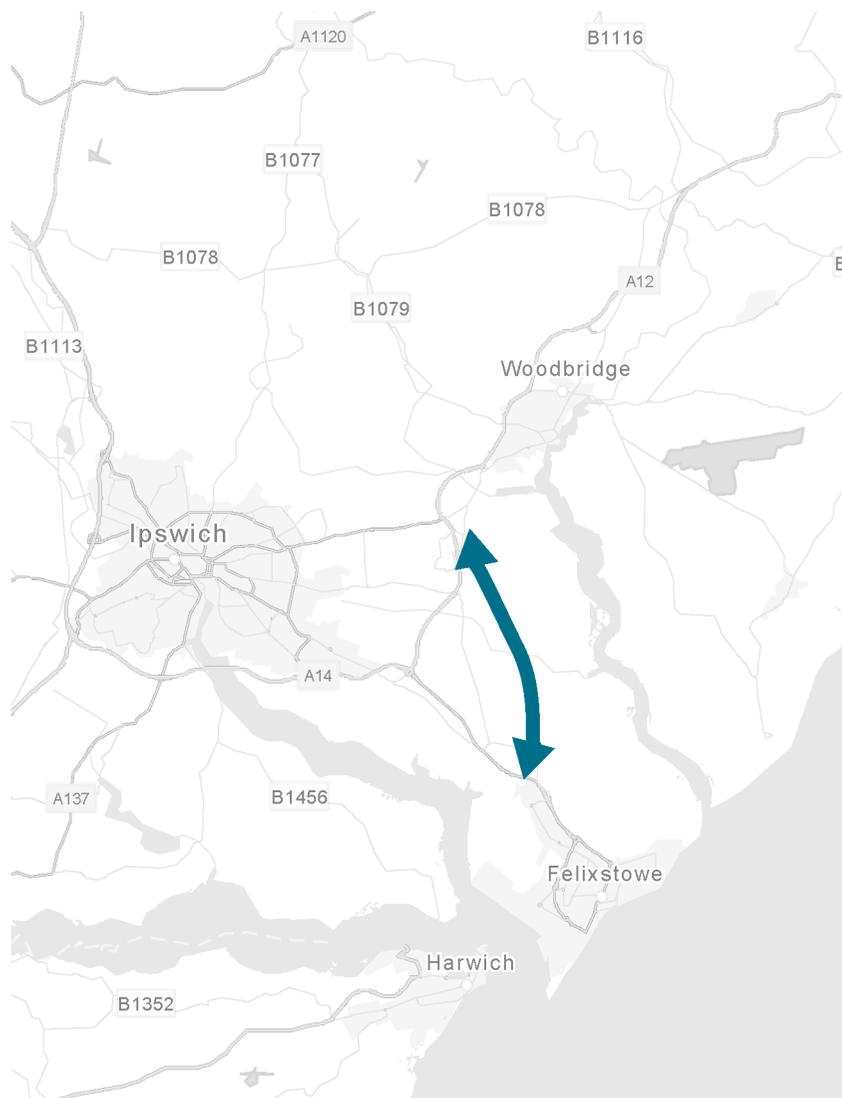
Map Reference	Location	Description	Recommendation
IF7	Felixstowe Road 'west' - Trinity Park to A14 bridge	No segregated cycle lanes, despite being an initially 40mph road, and then later national speed limit road, between two major settlements (Ipswich and Felixstowe). The A14 bridge represents a pinch point along this route, which limits the infrastructure options available to continuing the shared path only on the south side over the bridge. This is considered a very high priority route.	Continuation of shared paths along both sides of Felixstowe Road 'west' is recommended up until the junction with Straight Road is reached, in advance of the A14 bridge. At the point that Straight Road is reached, it is recommended that the north side shared path should terminate, and a (toucan) crossing be provided for transferring pedestrians and cyclists over to the south side, where a south side shared path is recommended to continue towards Felixstowe until the eastern side of the A14 bridge has been reached. This route should incorporate the sections of service road that occur in turn on both sides; the service roads are not recommended to be improved with further segregation from vehicles, but are recommended to at least be resurfaced, if not more holistically redesigned to be more attractive spaces. If following assessment a shared path is only considered deliverable on one side, then the south side is the preferred fallback option to ensure connectivity to IF4 and IF11. However, ideally a crossing point over to Straight Road should still be delivered, as this was identified through the consultation as being an important leisure cycling route. If shared paths are provided on both sides or just the northern side, and IF5 and IF3 are delivered, a crossing over the eastern arm of the roundabout should be delivered.
IF10	Straight Road	Straight Road provides a connection between Felixstowe Road 'west' and Bucklesham Road, which is an important connection for travel from villages to the west of the river Deben – most directly, Bucklesham. There is currently no crossing infrastructure for crossing Felixstowe Road 'west' to reach Straight Road. This is considered a medium priority route.	Add a platform crossing on Felixstowe Road 'west' for access to Straight Road.
IF11	Felixstowe Road 'west' A14 bridge to Felixstowe Road 'east'	There is currently inadequate existing cycling and walking infrastructure along this section despite large hatched areas of carriageway. This is considered a very high priority route.	A new bi-directional pedestrian/cycle track (i.e. fully separate from the carriageway using a buffer or substantial segregation means) is recommended to run along the southside of Felixstowe Road 'west' from the eastern end of the A14 bridge down to the pedestrian/cycle 'slipway' to Felixstowe Road 'east'. It is intended that this track will be created through the 'absorption' of the excess hatched reservation area on Felixstowe Road 'west' that occurs here. Reducing the extent of this hatched area may also have the effect of reducing average vehicle speeds on Felixstowe Road, which would be desirable

Map Reference	Location	Description	Recommendation
			given the crossing point recommended at Straight Road (IF10). A cycle/pedestrian priority crossing is recommended for safe transfer over 'The Street'.
IF13	Felixstowe Road 'east'	There is inadequate cycling and walking infrastructure along Felixstowe Road 'east' despite being the most direct and accessible route for active travel between Ipswich and Felixstowe (the route through Levington is considered to be 'hillier' and therefore more suitable for leisure/fitness cycling than commuting). This is considered a very high priority route.	<p>Two potential options are recommended for consideration at the design and implementation stage:</p> <p>Option A: A new bi-directional fully-segregated cycling and walking track is recommended to run parallel to the western border of Felixstowe Road 'east'; this track is expected to be separate from but parallel to the carriageway for as much of the length as possible, but becoming a segregated cycle lane (on-carriageway) where width is constrained towards the south. The track should include a cycle/pedestrian priority crossing over Bridge Road and Stratton Hall Drift. The track should avoid any incursion into the vehicle parking area to the south of Bridge Road, and the small parking area that is provided for the wooded area known as 'Potter's Hole', if possible.</p> <p>Option B: A: A new bi-directional fully-segregated cycling and walking track is recommended to run parallel to the western border of Felixstowe Road 'east'; this track is expected to be separate from but parallel to the carriageway for as much of the length as possible, but becoming a segregated cycle lane (on-carriageway) where width is constrained towards the south. The track should include a cycle/pedestrian priority crossing over Bridge Road and Stratton Hall Drift. The track should avoid the small parking area that is provided for the wooded area known as 'Potter's Hole', if possible. Where the section of dualling occurs, it is recommended that the west side is considered for reallocation and incorporation into the track, therefore diverting all vehicular movements to and from Felixstowe to the east side of the dualled section only. It is understood that the east side is used for Operation Stack, and that the parking area to the south is used for HGV drivers to rest, so the impacts of removing the west side of the dualling from the vehicular network will need to be fully assessed and this need for HGV parking space be met elsewhere. Similarly,</p>

Map Reference	Location	Description	Recommendation
			care will need to be taken in the redesign of the dualled section and any refuge/turning boxes etc to ensure pedestrian and cyclist safety, visibility and sightlines where vehicles must cross the track to access Bridge Road, and vice versa.
IF14	Levington Lane to Main Road, Bucklesham	The re-joining of Levington Lane using a new cyclist/pedestrian bridge over the A14 would provide a direct connection from Bucklesham (and other villages to the west of the river Deben who have had their access severed by the A14) to the Ipswich to Felixstowe Key Corridor route. Currently the only workable alternative to access south-east Ipswich/the Warren Heath employment/industrial/retail areas from these villages by bike is Bucklesham Road, as Foxhall Road is too unsuitable. Pedestrians have no access as neither of these existing alternatives to a new bridge/reconnection of Levington Lane have suitable footpaths throughout their length. This is considered a medium priority route.	It is recommended that the two halves of Levington Lane are reconnected through the installation of a new bridleway bridge over the A14. It is also recommended that a continuous pavement up to the centre of Bucklesham (at least) is provided. If a bridge is delivered, suitable crossing facilities will have to be provided over Felixstowe Road 'east' in order to access the recommended scheme on the western side (see IF13).
IF18	Morston Hall Road	The existing footpath in this location is of poor surfacing quality, is too narrow and is too exposed to the traffic, traffic noise and air pollution from the A14 to feel safe or enjoyable/healthy to cycle. This is considered a very high priority route.	<p>The existing foot/cycle way immediate to the A14 is recommended to be completely removed and replaced with infrastructure to improve the safety and comfort of cyclists and pedestrians using Morston Hall Road. Two options are recommended to be taken forward for further consideration:</p> <p>Option A: A new bi-directional cycle and pedestrian track is recommended to be delivered separate to but parallel with the western edge of Morston Hall Road, absorbing the immediate farmland verge. This option is understood to be the preferred option locally.</p> <p>Option B: An Automatic Number Plate Recognition camera modal filter system is recommended to filter the section of Morston Hall Road between the western junction with Felixstowe Road 'east' and the junction with the access route to Morston Hall and Morston Hall Cottages. This system would need to be programmed using a 'white list' system to recognise and give access to local buses, emergency vehicles and the vehicles of residents of the Morston Hall area. This option has the added benefit of eliminating rat running along Morston Hall Road, where Option A does not. However, this option does not eliminate the possibility of pedestrians and cyclists mixing with permitted traffic, though this would still be significantly</p>

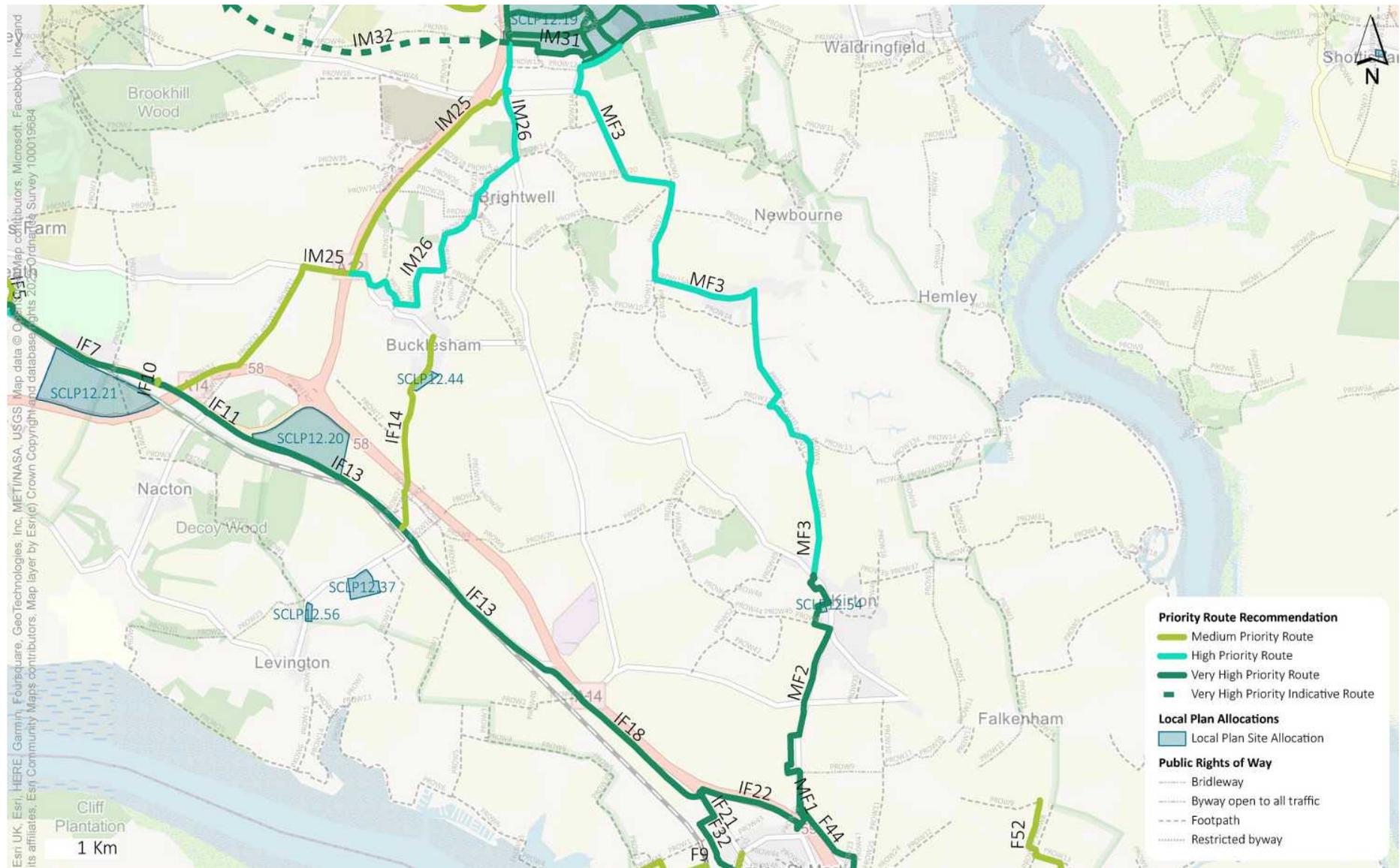
Map Reference	Location	Description	Recommendation
			reduced from current levels of traffic movements along Morston Hall Road.
IF20	High Road, Trimley St Martin	A crossing in this location is required to connect the Ipswich to Felixstowe Key Corridor to the Martlesham to Felixstowe Key Corridor. This is considered a very high priority route.	A formal crossing is recommended on High Road to connect IF18 and IF22.
IF21	High Road, Trimley St Martin	The existing shared path located here is currently in poor condition and needs widening and resurfacing. This is considered a very high priority route.	This section of existing shared path is retained and improved.
IF22	Field edge, north of Trimley St Martin	This recommendation is intended to provide a means for cyclists and pedestrians to bypass the parallel section of High Road/Mill Lane when travelling to/from the Land at Howlett Way allocated site (SCLP12.64) or the North Felixstowe Garden Neighbourhood if travelling to/from Ipswich. It also serves to directly connect the Ipswich to Felixstowe and Martlesham to Felixstowe Key Corridors. This is considered a very high priority route.	A cycle/pedestrian track is recommended for delivery around the inside edge of the northern boundary of this field.
IF36	Trinity Park roundabout (northern arms)	This recommendation covers the northern arms of the Trinity Park roundabout. There is currently no means for pedestrians to safely navigate around this roundabout, due to the lack of pedestrian or cycling infrastructure leading up to/from it. However, there will be a need for it if the surrounding recommendations in this area (IF4, IF5 and IF7) are delivered. This is considered a very high priority route.	Appropriate Crossing points over the northern arms of the Trinity Park roundabout are recommended.

Martlesham to Felixstowe Key Corridor



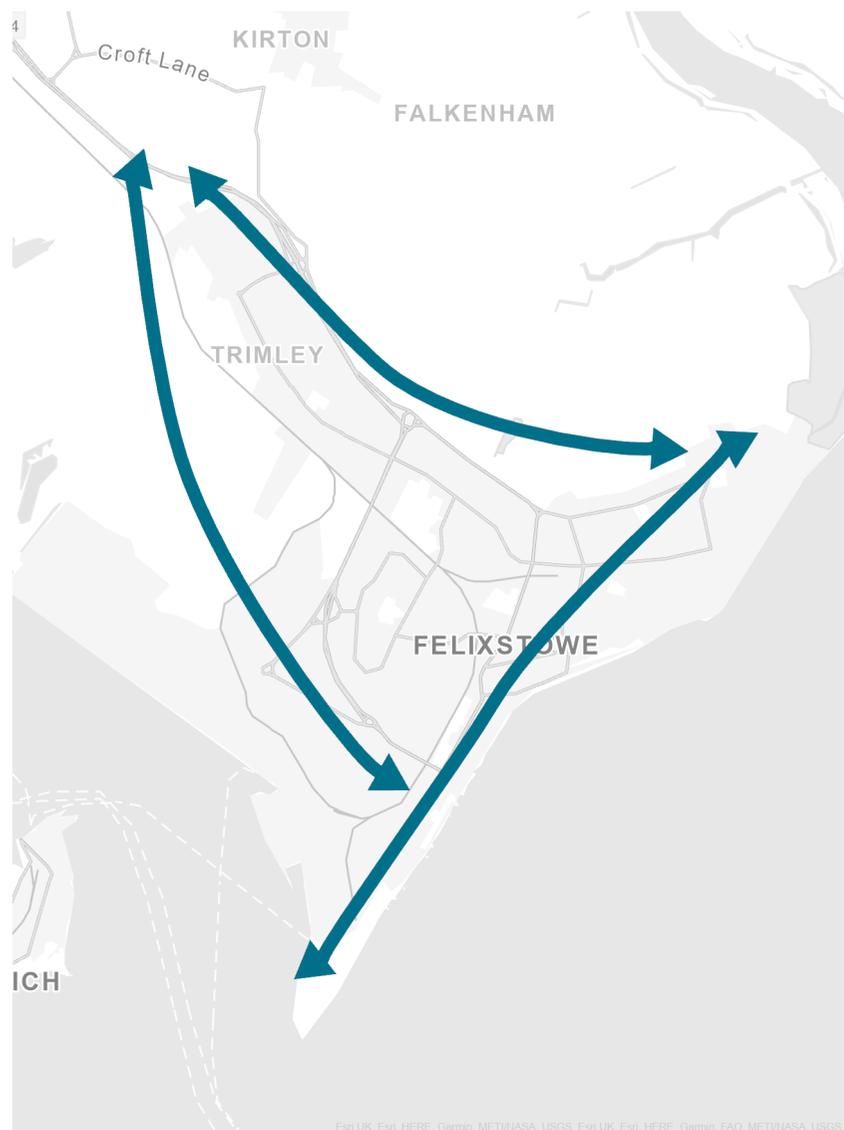
4.21 The Martlesham to Felixstowe route is made up of cycle tracks and improvements to the existing public right of way network as well as the bridge at Trimley St Martin. This route follows Kirton Road up through Kirton and into Newbourne via existing PROW routes, then terminates at Brightwell Lakes. It is made up of three recommendations, MF1, MF2 and MF3.

Martlesham – Felixstowe



Map Reference	Location	Description	Recommendation
MF1	A14 pedestrian bridge, Trimley St Martin	The existing A14 pedestrian bridge is not suitable for cycling over due to its cycle barriers, steep gradient and relatively low parapet. The bridge's gradient and barriers also make it difficult for dismounted cyclists and pedestrians with push chairs or using wheelchairs to use it. The restrictive width of the bridge makes it unsuitable for two-way travel, which is a problem as the bridge is critical to the operation of the Martlesham to Felixstowe Key Corridor, which would commence here, and would expect moderate cyclist and pedestrian flows. Aside from the leisure and commuting value of an improved (for bi-directional cycling and walking) bridge at this location, if improved the bridge is likely to be well used by families from Kirton with primary school age children following the relocation of the Trimley St Martin Primary School to the Land Adjacent to Reeve Lodge site (Policy SCLP12.65). This is considered a very high priority route.	The bridge is recommended to be fully replaced with a design that is: wide enough for cyclists and pedestrians to safely pass each other, allowing for two way flows; has a gentler gradient to make it more accessible and able to be cycled over, and; has appropriate parapet heights for mounted cyclist use. If there is no potential for the bridge to be replaced, at a minimum the approaches to both ends of the bridge should be widened and extended with the gradient reduced, and the existing cycling barriers should be removed.
MF2	Kirton Road/Trimley Road/Bucklesham Road	The popular Kirton Road/Trimley Road/Bucklesham Road route currently has no cycling infrastructure and is too narrow to accommodate segregated cycle lanes. The pavements that run along it are also too narrow to be safe and inclusive of different pedestrian mobility needs. An off-carriageway cycling/walking solution is therefore needed. It is therefore suggested that the route through Kirton it sent around the rear of the properties to the west of the route and emerges back onto Bucklesham Road for transfer over to Footpath 50 (which is recommended to be upgraded and improved). The recommended route also passes over Innocence Lane, which is a well-used local road. As this section of the Key Corridor is particularly likely to be used by school children en route to Trimley St Martin Primary School (in its current or eventual new location in Trimley St Martin) or Felixstowe (for Felixstowe Academy), a formal crossing point is appropriate. This is considered a very high priority route.	An all-user track (cyclists, pedestrians and horse riders) is recommended along the western side of the Kirton Road/Trimley Road/Bucklesham Road route, segregated from the road by existing vegetation. This track would likely pass around the rear boundaries of the properties located on this west side. The potential for co-delivery of additional plantings should be considered and consulted on where the track passes around the rear of these properties to both screen it and provide biodiversity net gain. Continue the route around the rear of the properties to the west of Bucklesham Road, emerging back onto Bucklesham Road opposite Footpath 50 (which is recommended to be upgraded to a bridleway and improved). The introduction of a parallel crossing point over Innocence Lane and over Bucklesham Road for access to Footpath 50 that is suitable for children on bikes to use is also recommended.
MF3	Bucklesham to Brightwell Lakes via various PROWs and Ipswich Road	This recommendation spans the main section of the Martlesham to Felixstowe Key Corridor, connecting Brightwell Lakes to Kirton via Ipswich/Newbourne Road and PROW routes through Newbourne. The majority of this route will allow for total segregation from vehicles, with the exception of Watermill Road, Jackson Road and the crossing points over Ipswich/Newbourne Road. This recommendation has been rerouted following the consultation to avoid the Foxburrow plantation, leaving these leisure routes as earth paths, and avoiding any encouragement for cyclists to attempt to cycle within the Newbourne Springs Nature Reserve (a SSSI). This is considered a high priority route.	It is recommended that a cycling and walking track be installed from Footpath 50 (from Bucklesham Road/MF2), Footpath 55, Bridleway 13 (Kirton), Footpath 11, Watermill Road, along Jackson Road (Footpath 15A), Lower House Lane (Footpath 17), along the eastern edge of Ipswich Road/Newbourne Road, and then into the Brightwell Lakes site.

Felixstowe Internal Routes Key Corridor



4.22 The Felixstowe Internal Routes Key Corridor is the sum of infrastructure recommendations for routes within the Felixstowe and Trimley villages area.

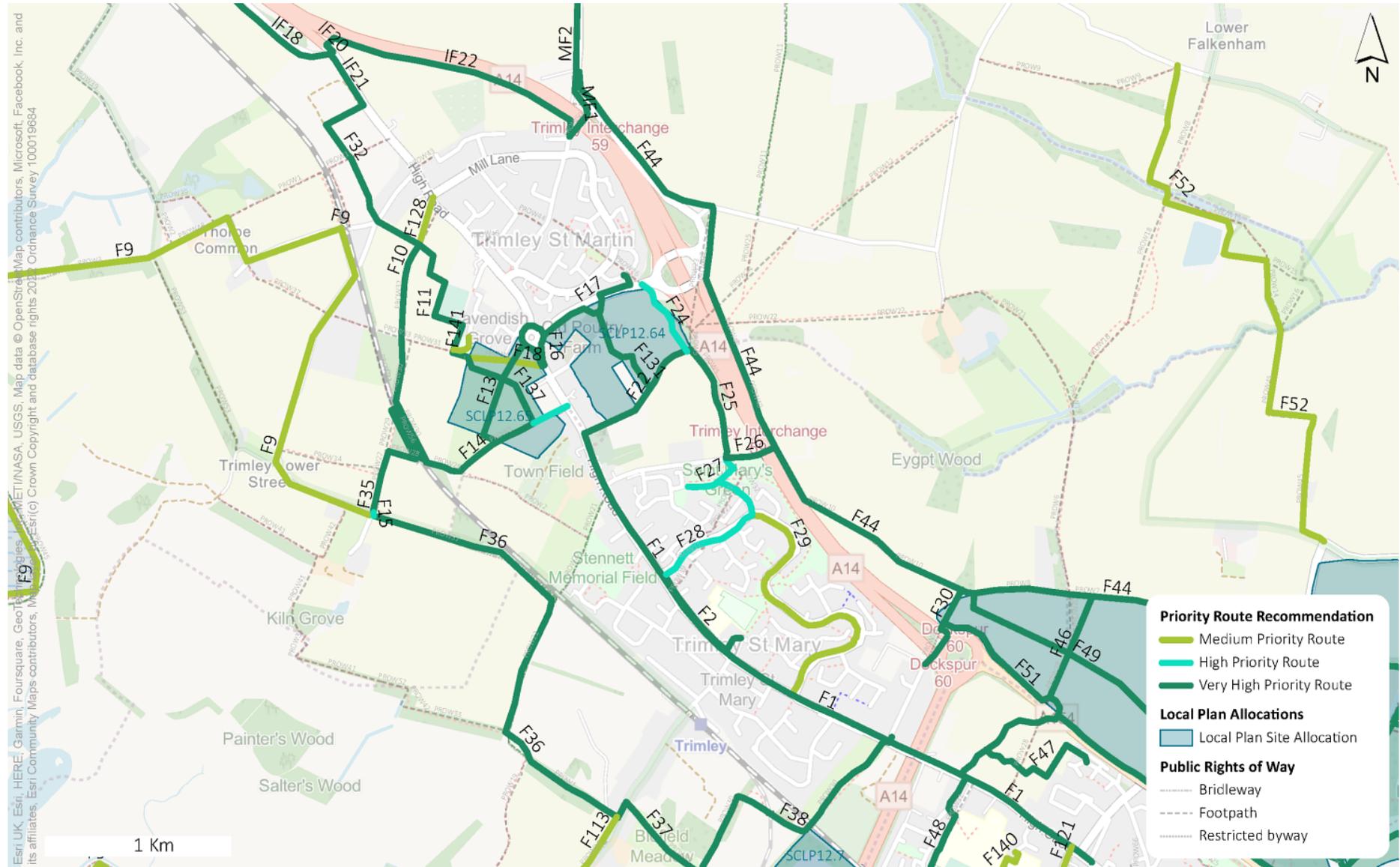
4.23 Within this Key Corridor Strava Metro data shows that the most highly used routes between September 2020 and August 2021 were (approximately ranked from highest to lowest activity):

- The whole of the High Road and Cliff Road (most activity)
- Howlett Way to Kirton Road
- Undercliff Road
- Garrison Lane/Langer Road/Walton Avenue
- View Point Road (for Landguard Nature Reserve and ferry service)
- Gulpher Road
- Golf Road
- Grange Road
- The Promenade
- Sea Road
- Ferry Road (for Felixstowe Ferry)
- Hamilton Road

4.24 The key opportunities that have been identified in support of this Key Corridor include:

- The planned development in the Suffolk Coastal Local Plan,
- Primary and secondary schools,
- The Port of Felixstowe and associated employment opportunities,
- Felixstowe and Trimley railway stations, and
- Felixstowe promenade and associated tourism opportunities.

Trimley St Martin and Trimley St Mary



Map Reference	Location	Description	Recommendation
F10	Grimston Lane to bridleway bridge via Footpath 32	This section of the Trimleys to the Port route runs along the Trimleys' western edge, via new or existing PROWs, utilising Footpaths 30, 32 and 33 to connect Grimston Lane with the bridleway bridge (over the railway line) that lies to the west of Gun Lane. This route is intended to become a commuter route and should therefore be delivered at a quality suited to use by cyclists on commuter bikes. Most of the Trimleys to Port route is intended to be made up of all-user tracks that through their design and use of appropriate surfacing meet the needs of commuter cyclists, pedestrians and horse riders. This is considered a very high priority route.	This section is to be designed as an all-user track (horse riders, cyclists and pedestrians) suitable for bi-directional commuter travel and that provides adequate space and surfacing for horses.
F11	Land between PROW 32 and the Land Adjacent to Reeve Lodge site's western boundary/Footpath 31	This route provides an alternative to the High Road via the western edge of the Trimleys. This is considered a very high priority route.	A new cycle/pedestrian track that follows the current field boundaries, navigates around the outside edges of the Trimley sports facilities, and connects directly into the existing Trimley St Mary play area and the Land Adjacent to Reeve Lodge site's north-western corner is recommended. This is via a small subsection of Footpath 31, which will need to be upgraded to a bridleway and adequately surfaced for the transition down to the pedestrian/cycle track which is intended to run between the existing play area and the site's spinal north-south route.
F13	Land adjacent to Reeve Lodge, High Road, Trimley St Martin - Primary route	This section provides a connection from Footpath 31 through the site allocation up to the roundabout at the northern end and down to Gun Lane at the southern end. This connection is intended to ensure the existing and new communities in this area are able to join the Trimleys to Port route along the western edge of the Trimleys via this site. This is considered a very high priority route.	Introduce cycle/pedestrian infrastructure through the SCLP12.65 site, to connect pedestrians and cyclists with Footpath 31 to the west, the High Road roundabout to the north and Gun Lane to the south. The connecting section of route between the north to south access route and Footpath 31 to the west is provided so as to allow Footpath 4 to remain a footpath for pedestrian use only.
F14	Gun Lane to bridleway bridge	Gun Lane provides an important cycling and walking connection between the High Road and the Trimley to Port route, which at this point is travelling north to south via the bridleway bridge. Gun Lane is particularly important for residents and children attending the soon-to-be-relocated Trimley St Mary Primary School to be delivered on the SCLP12.65 Land Adjacent to Reeve Lodge site allocation. Gun Lane is largely an unsurfaced agricultural track with a short surfaced section to the eastern end for the existing properties there; some vehicle access is permitted as it is a byway not a bridleway. Equestrian use is an important consideration along this route. This is considered a very high priority route.	Gun Lane is recommended to be designed as an all-user track (equestrian, cycling and walking) suitable for bi-directional commuter travel and appropriate space and surfacing for horses. Surfacing treatments that meet the needs of all bridleway user types, such as crushed materials rolled in a smooth finish at a width of three metres, is recommended. The infrastructure scheme must run to the footing of the bridleway bridge to ensure safe access.
F15	South-western most point of Grimston Lane	Signage needed at this corner to alert cyclists, pedestrians and equestrians that are continuing west towards the river wall of the	Due to concerns regarding the potential adverse wildlife impact of intensifying cycling and walking to the west of the Land Adjacent to

Map Reference	Location	Description	Recommendation
		need to use the bridleway adjacent, and not the river wall, to travel along (and to keep dogs on leads). Wayfinding signage at this point could also be improved as per the intention for this route to be used as a commuter route to the Port. This is considered a high priority route.	Reeve Lodge site, it is recommended that informative signage is installed which advises users of the risks posed by use of protected, sensitive areas and therefore the need for them to stick to the bridleway rather than walking/riding on the river wall. This signage should also provide route information on the travel time and distance between the Port and the Trimleys by bike and on foot.
F16	The Howlett Way/High Road/Land Adjacent to Reeve Lodge access road roundabout	This roundabout needs improvements to allow safe off-carriageway circulation of pedestrians and cyclists with shared paths. Use of this roundabout will be intensified following the build out of the Land at Howlett Way and Land at Reeve Lodge sites. This is considered a very high priority route.	<p>The Howlett Way/High Road/Land Adjacent to Reeve Lodge access road roundabout is recommended to be developed to a Dutch-style roundabout level of quality, which will provide for full circulation of pedestrians and cyclists around and across all five (post-development) arms of the roundabout. Pedestrians and cyclists would also be given priority to cross over each arm, communicated through design.</p> <p>There must be sufficient space for vehicles to safely clear the roundabout's circulation area before the crossings are reached, so that (most) vehicles can have completely cleared the roundabout when stopped to wait for a pedestrian or cyclist to cross over the arm they are exiting on to.</p>
F17	Howlett Way	Howlett Way is currently part of a key cycling route from the villages to the west of the river Deben, Martlesham, Woodbridge and an alternative route from Ipswich to the Trimleys and Felixstowe via the Bucklesham Road/Trimley Road/Kirton Road route in. However, Howlett Way has no existing cycling infrastructure along its length, and the existing footpath along the north edge is not suitable for use by people with limited mobility. The need for improvements to Howlett Way's infrastructure offer is further increased by the planned development on the peninsula, in particular of the Land at Howlett Way (SCLP12.64) and the Land Adjacent to Reeve Lodge sites (SCLP12.65). F17 is a key route within a larger network of recommendations in this area for: improving connectivity between the Trimley villages, Kirton, the Land at Howlett Way site and the Land Adjacent to Reeve Lodge (which the Trimley St Martin Primary School is being relocated to) site, and; facilitating onward travel towards Ipswich (via the Ipswich to Felixstowe Key Corridor), Martlesham (via the Martlesham to Felixstowe Key Corridor), the Port (via the Trimleys to the Port route along the western perimeter), and the North Felixstowe Garden Neighbourhood and central Felixstowe (via Thurmans Lane). This is considered a very high priority route.	Create a new segregated bi-directional cycle/pedestrian track to run along the southern side of Howlett Way (parallel to but separate from the carriageway, using an appropriate buffer strip and bollards). The track should run continuously from the High Road/Howlett Way roundabout up to the site's access roundabout, where it should then transfer over the carriageway to the northern side of Howlett Way (via a suitable crossing over the access roundabout's eastern arm), where it should then connect directly to Ash Ground Close.

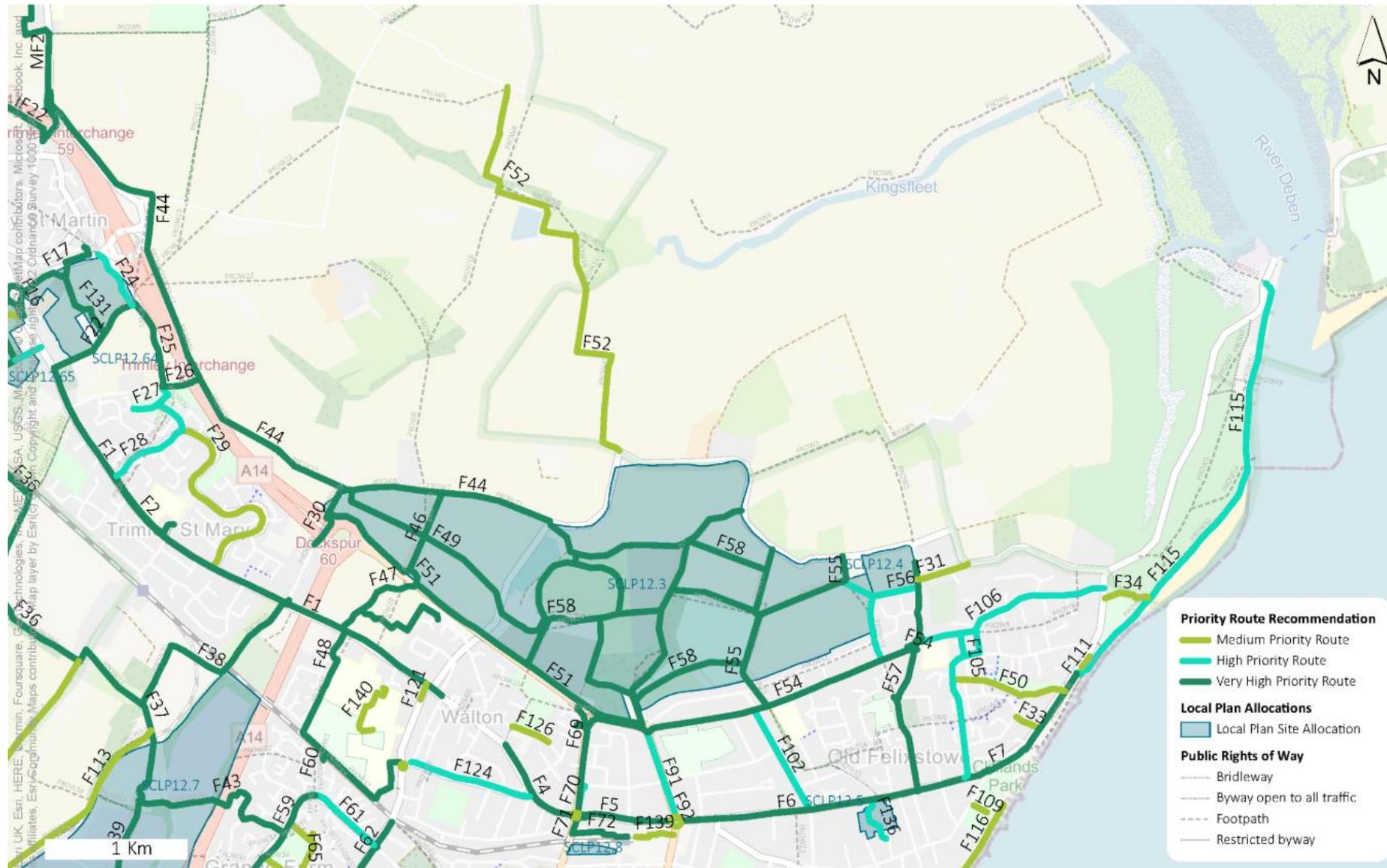
Map Reference	Location	Description	Recommendation
F18	Footpath 4	This recommendation covers the existing footpath (Footpath 4) which runs through the Land Adjacent to Reeve Lodge site allocation (SCLP12.65). This is considered a medium priority route.	Footpath 4 is expected to be retained and improved for pedestrian use with widening and appropriate surfacing to increase accessibility.
F22	Bridleway 5 (east to west section)	Bridleway 5 is important for facilitating movement along the southern edge of the Policy SCLP12.64 Land at Howlett Way site allocation, between Church Lane and for accessing Bridleway 10 (Thurmans Lane) for access to the North Felixstowe Garden Neighbourhood (SCLP12.3) to the south east. This is considered a very high priority route.	Bridleway 5 is recommended to be resurfaced with a firm, compacted, unsealed, slightly cambered, free draining aggregate surface treatment throughout its length at a width of 3m. It should be well connected to the SCLP12.65 Land at Howlett Way site allocation and Bridleway 10 (Thurmans Lane, for the North Felixstowe Garden Neighbourhood).
F24	PROW Footpath 26 (southern half)	This recommendation follows the southern half of Footpath 26, the northern half of which runs through Trimley St Martin, emerging onto and crossing Howlett Way. This route runs through the small woodland area that lies to the east of the Land at Howlett Way (SCLP12.64) site, which helps to act as a buffer between the site and the A14. This is considered a high priority route.	The southern half of Footpath 26 is to remain a footpath as there is significant value of retaining as much of the woodland and the attractive earth track surfacing along Footpath 26 as possible, and there are other options available for creating a north-south route through the site (see F131), connecting Howlett Way and Church Lane/Bridleway 5. However, this position is conditional on a segregated cycling and walking route being provided through the site. If this is not possible, then the southern part of Footpath 26 should be surfaced as a cycle/pedestrian track from the Howlett Way crossing all the way down to Bridleway 10.
F25	Bridleway 5 (north to south section)	This recommendation covers the north to south section of Bridleway 5, which is currently an earth track through agricultural fields to the south of site allocation SCLP12.64 Land off Howlett Way and down to Thurmans Lane Bridleway 10. This route provides onward access to Trimley St Mary via F27 (Footpath 11) or the SCLP12.3 North Felixstowe Garden Neighbourhood via F26 and F44 (Bridleway 10 and Candlet Track) This is considered a very high priority route.	This route is to be upgraded to a cycle/pedestrian track with a firm, compacted, unsealed, slightly cambered, free draining aggregate surface treatment throughout its length at a width of 3m. It should be well connected to the SCLP12.65 Land at Howlett Way site allocation and Bridleway 10 (Thurmans Lane, for the North Felixstowe Garden Neighbourhood).
F26	Thurmans Lane	Thurmans Lane and the underpass that connects Bridleway 10 (F25) and Candlet Track (F44) currently have poor surface quality and lacks lighting for pedestrian and cyclist use outside of daylight hours. This is considered a very high priority route.	The section of Thurmans Lane that connects Bridleway 10 (F25) and Candlet Track (F44) via the underpass is to be resurfaced, and the underpass fitted with lighting to improve its usability outside of daylight hours.
F27	Thurmans Lane green space	The land parcel just south of Thurmans Lane has a bridleway (Bridleway 32) that runs across it, and a footpath (Footpath 11) around its eastern boundary, which runs continuously to The Josselyns; this area was previously allocated for housing development in the Felixstowe Area Action Plan and has previously had outline permission for up to 50 homes. This land is accessible via Thurmans Lane/Bridleway 10/A14 underpass/Candlet Track and Bridleway 5/Church Lane/the Land at Howlett Way site's proposed public open	Segregated cycling and walking infrastructure is recommended to run through this area and down to Thomas Avenue, where a new shared path is recommended to run along one side to connect down to Faulkeners Way. A parallel crossing is recommended over Faulkeners Way to connect this route to the improvements recommended under F29 (new shared path along Faulkeners Way), which are intended to be delivered along the highway side that runs closest to Trimley St Mary Primary School. An appropriate and accessible cycling and

Map Reference	Location	Description	Recommendation
		space area. If developed for housing, this site would be in the catchment area of the Trimley St Mary Primary School, and would therefore have enhanced need for segregated cycling and walking infrastructure to support families cycling or walking children to school safely. To the west of the site, via Mariners Way, is the Woodland Avenue play area which could be made more accessible through improvements to routes across this land and a more accessible connection point created. Footpath 11 would be difficult to upgrade due to width restrictions and hedgerow that the community is keen to protect; finding an alternative means of connecting down to Faulkeners Way to the south is therefore necessary. Thomas Avenue is likely to be the most efficient connection point to Faulkeners Way. This is considered a high priority route.	walking connection to Mariners Way is also recommended for added permeability and to Woodland Avenue play area more accessible.
F28	Faulkeners Way	Faulkeners Way provides access to various secondary residential streets within Trimley St Mary, commencing and terminating on High Road. Faulkeners Way has sufficient width to accommodate shared paths throughout its length, and offers a good opportunity to provide improved cycling and walking connections between Trimley St Mary and Trimley St Martin via Saint Mary's Green, Thurmans Lane, Bridleways 10 and 5 (F25 and F22), and the Land at Howlett Way site allocation (SCLP12.64; F131 and F17). This recommendation specifically includes the section of Faulkeners Way that connects Saint Mary's Green (via Thomas Avenue) and the High Road, where another recommended improvement (F2) commences. This is considered a high priority route.	A new shared path is to run along one side of Thomas Avenue and down to High Road via Faulkeners Way. A parallel crossing from Thomas Avenue over to Faulkeners Way is also recommended.
F29	Faulkeners Way	Faulkeners Way provides access to various smaller secondary residential streets within Trimley St Mary, commencing and terminating on High Road. Faulkeners Way has sufficient width to accommodate shared paths throughout, and offers a good opportunity to provide improved cycling and walking connections from Trimley St Mary to Trimley St Martin via Saint Mary's Green, Thurmans Lane and the Land at Howlett Way site allocation (SCLP12.64). This recommendation covers the main section of Faulkeners Way. This is considered a medium priority route.	A new shared path is recommended to be installed along the full length of one side of Faulkeners Way, upgrading the current footpaths. The side which runs closest to Trimley St Mary Primary School throughout Faulkeners Way's length is likely to be the most deliverable, as there tends to be more verge space available on this side (side swapping should be avoided, though where this is necessary parallel crossings should be provided).
F32	Gosling's Farm track down to Grimston Lane	This section between High Road and Grimston Lane via Goslings Farm connects the Ipswich to Felixstowe Key Corridor with the route between the Trimleys (via the PROWs that run along the Trimleys' western edge) and the Port of Felixstowe. This route is intended to	This section between High Road and Grimston Lane via Goslings Farm track is recommended to be designed as an all-user track suitable for bi-directional travel. This route is recommended to be designed to a standard that supports commuter cycling; ideally a surface treatment

Map Reference	Location	Description	Recommendation
		become a commuter corridor, and should therefore be delivered at a quality suited to use by cyclists on commuter bikes. Most of the Trimleys to Port route is intended to be made up of all-user tracks that through their design and use of appropriate surfacing meet the needs of commuter cyclists, pedestrians and horse riders. This is considered a very high priority route.	<p>solution for all users should be found, otherwise a grass strip should be provided and maintained for horse riders. The route this section forms part of is intended to operate as a suitable alternative to High Road for active travel between the Trimleys and the Port area.</p> <p>The route is recommended to be created through the creation of a new all-user track that commences at Gosling's Farm track, crosses Grimston Lane, runs via the Poppyfield development's southern boundary, heads south on Footpath 32 (to be upgraded and improved) towards Gun Lane, over the bridleway bridge, heads east at Grimston Hall to Keeper's Lane, then heads south and east along Keeper's Lane, continues up Cordy's Lane and then runs along Blofield Track, terminating at Nicholas Road (as an entry point to The Port). Together with widening and surfacing, effective signage and cohesive design along this new route will be vital to ensuring users stay en route, as there are many different PROW route options in this area.</p>
F35	Bridleway bridge	Bridleway bridge over the railway line to access the PROWs that continue towards the Port via Cordy's Lane (RB28, BW22, FP1, BW12, BW14). This is considered a very high priority route.	The existing bridleway bridge must be well connected to the PROW routes to the north and south of it that form the Trimley to Port route. All-user track improvements to the existing PROWs to the west of the bridge that then head towards the Port are recommended. The connection between the bridge and Gun Lane should also be improved.
F36	Grimston Hall to Cordy's Lane via Keeper's Lane	This section of the Trimleys to Port route provides an important connection opportunity between the bridleway bridge and Cordy's Lane. The route heads east from Grimston Hall and then south towards Keeper's Lodge before emerging onto Cordy's Lane via Bridleway 22 and Footpath 1. This part of the route is currently a combination of 2m/2.5m wide loose sand/earth surfaced bridleway (prior to Keeper's Lane) and narrow unsurfaced earth track, for which there is available space for widening and surfacing throughout. Surfacing and widening Keeper's Lane may incur some natural habitat/biodiversity loss that would need to be appropriately mitigated. This is considered a very high priority route.	Widen and surface using a surface treatment that is safe and accessible for use by cyclists, pedestrians and horse riders, and that is appropriate for this location. Any habitat/biodiversity loss from surfacing Keeper's Lane must be adequately mitigated.
F38	PROW Footpath 30	Footpath 30 connects the High Road to Blofield Track. If upgraded to a bridleway, Footpath 30 has the potential to be an important cycling and walking connection between the Trimleys to Port route and the High Road for access to Spriteshall Lane and over the A14 crossing to	Footpath 30 is to be upgraded to bridleway status, widened and surfaced as a new all-user (cyclists, pedestrians and horse riders) track between the High Road and the Trimleys to Port Route, connecting directly to Bridleways 12 (Blofield Track) and 14 for Nicholas Road. A

Map Reference	Location	Description	Recommendation
		the North Felixstowe Garden Neighbourhood (SCLP12.3). This is considered a very high priority route.	parallel crossing over High Road to improve safe connectivity between Spriteshall Lane and this new track is recommended.
F128	PROW 32	This route follows Footpath 32 to connect High Road with recommendation F10 via the new development (Poppyfield Green) on this site. The opportunity for upgrading Footpath 32 to a bridleway and surfacing it as a track has passed, as post-development there is not sufficient space available for appropriate widening of the route. Similarly, the pavements along the access routes are unable to be widened to shared paths, though the non-through road nature of the site reduces the need for total segregation. Use of a carriageway and a connection across the green space (with a modal filter to prevent vehicle use) on site is therefore the only feasible remaining option. This is considered a medium priority route.	A new connection between High Road and recommendation F10 via Footpath 32 and the Poppyfield Green development, using a combination of on-carriageway cycling and a modally filtered connection across to F10.
F131	Howlett Way to Church Lane/Bridleway 5	This route through the Land at Howlett Way allocated site (SCLP12.64) would provide a direct connection between Howlett Way's recommended infrastructure (F17) and Church Lane/Bridleway 5, for onward travel towards the North Felixstowe Garden Neighbourhood via recommendations F22, F25 and F26. This is considered a very high priority route.	Create a segregated route through the Land at Howlett Way site allocation (SCLP12.64) to connect Howlett Way to Church Lane/Bridleway 5 for access to the North Felixstowe Garden Neighbourhood.
F137	SCLP12.65 Land adjacent to Reeve Lodge, High Road, Trimley St Martin - Connection to Gun Lane	This is a proposed connection through the Land Adjacent to Reeve Lodge allocated site (SCLP12.65) to Gun Lane, providing access for onward travel to the Trimleys to the Port route via the bridleway bridge, and access to the public open space that is set to be provided on the land to the south-east of Gun Lane. This is considered a very high priority route.	Appropriate cycling and walking infrastructure is to be provided through the site to connect the primary access route to Gun Lane.
F141	Trimley St Martin Play Area, off Goslings Way	The Trimley St Martin Play area currently prohibits cycling through it. This is considered a medium priority route.	It is recommended that cycling is allowed through the park.
F142	Gun Lane to High Road	Gun Lane is currently only partly surfaced with hard surfacing to provide vehicular access to the properties off Gun Lane; the rest of the lane maintained as an earth byway (some vehicular access allowed). A connection into the Land Adjacent to Reeve Lodge (SCLP12.65) using an appropriate surfacing treatment for cyclists, pedestrians and horse riders, is recommended. This is considered a high priority route.	Create a cycling and walking connection between the improvements to Gun Lane and the bridleway bridge over the railway line and High Road.

North Felixstowe



Map Reference	Location	Description	Recommendation
F1	F1 High Road (Section 1)	<p>'High Road' (High Road/High Street/High Road West/High Road East) is an important commuting and leisure route for cyclists and pedestrians, providing a continuous route from the north-western edge of Trimley St Martin to Old Felixstowe.</p> <p>High Road's physical and functional constraints to being adequately widened to accommodate continuous physically segregated cycling infrastructure makes worthwhile improvements difficult to achieve. This is due to: (1) sections of narrow carriageway where fully-segregated cycle lanes cannot be fitted without obstructing vehicular traffic (particularly larger vehicles) due to immediate property boundaries preventing widening of the highway; (2) significant sections having high demand for on-street parking, and nowhere off the High Road for this parking to be reasonably re-accommodated, and; (3) the significant number of adjoining side roads and vehicle accesses/driveways to properties, meaning it's necessary to have very frequent breaks in the segregation, therefore opening it up to being parked over/vehicle incursion, etc. The pavements are generally adequate quality in terms of surfacing, but are not consistently wide enough to be inclusive, as per the requirement for a minimum width of 1.5m. This is considered a very high priority route.</p>	<p>An 'interrupted' segregated cycle lane scheme is recommended to run along both sides (uni-directional) of the High Road between Church Lane (Trimley) and King Street, with as much continuity as possible. The segregated cycle lanes scheme should include a physical means of preventing vehicles from parking over it, meaning a managed approach to accommodating the need for on-street parking on High Road is required. This scheme is therefore recommended to include organising and concentrating on-street parking provision into limited bay parking 'blocks' where it is needed along High Road, therefore freeing up protracted sections of High Road for segregated cycle lanes. The potential to accommodate all parking on one side of the highway is recommended to be explored.</p> <p>To support successful implementation, the need for on-street parking along High Road (and any adjoining roads that rely on on-street parking on High Road), will need to be fully assessed in terms of quantity and the value of different locations; this will need to take into account any disabled parking needs, where access will need to be more immediate to the residential/non-residential use it serves.</p> <p>Once the baseline need and appropriate organisation of the on-street parking provision along High Road has been determined, necessary gaps in the scheme will need to be determined, i.e. for access to drive ways, business/retail uses, adjoining roads, or any carriageway sections where there isn't enough width available for segregation. Following this, the remaining sections can be fitted with fully segregated cycle lanes (in both directions).</p> <p>The existing refuge and non-signalised crossings/islands provided along this stretch are recommended to be reviewed and, where possible and appropriate, replaced with signalised crossings. The need for the existing turning boxes into side roads should also be reviewed and either removed (if considered to not be needed) or their depth reduced where this would be effective in providing more carriageway space for the segregated cycle lane scheme.</p>

Map Reference	Location	Description	Recommendation
			Lastly, if along the stretch where F1 and F2 run parallel to each other, it is considered undeliverable for both cycle lanes (F1) and a shared path (F2) to be provided, then the cycle lanes are recommended to stop, so a shared path can be created immediate to the school, as a shared path immediate to the school is higher priority. Once past the identified stretch (F2), the cycle lanes should resume.
F2	High Road via Trimley St Mary Primary School	High Road' (High Road/High Street/High Road West/High Road East) is an important commuting and leisure route for cyclists and pedestrians, providing a continuous route from the north-western edge of Trimley St Martin almost to Felixstowe seafront (where High Road East then becomes Cliff Road). The pavements are generally adequate quality, but are not consistently wide enough to be inclusive, where the minimum width should be 1.5m. However, its physical and functional constraints to being widened enough to accommodate continuous, adequate-quality segregated cycling infrastructure makes substantive improvements challenging. This short section between the western arm of Faulkeners Way and Black Barns via Trimley St Mary Primary School covers a relatively wide section of pavement which has the potential to be converted into a shared path for families to use to access the primary school's High Road entrance on foot or by bike. Reducing the number of cars stopping here on High Road to drop primary school children at the gates will likely have positive impact on traffic flows along High Road at peak times, as well as all of the general benefits of modal shift. This is considered a very high priority route.	<p>A new shared path is to be installed between Faulkeners Way's western arm and the footpath that runs close to the eastern boundary of Trimley St Mary Primary School (for access to Black Barns). This is recommended to utilise and improve the existing short stretch of shared path that runs along the High Road and provides a roundabout bypass for the mini Faulkeners Way/High Road roundabout. If possible, the shared path is to be designed in a way that communicates and supports its principal role, which is to serve the Trimley St Mary Primary School and therefore families with small children so that they are further incentivised to walk, scooter or cycle their children to school (i.e. incorporate appropriate placemaking features, such as 'play on the way' features).</p> <p>Additional means of physical segregation should be used to prevent any parking over the shared path, such as bollards or planters. Refuge crossings/traffic islands etc. and street features should be removed or moved where they would otherwise reduce the width available for a shared path to be installed; refuge crossings in this area should be replaced by parallel crossings (for safe cyclist and pedestrian use). The crossing over Burwood Place should be made into a pedestrian/cyclist priority crossing, with the junction narrowed and visibility in and out maximised to increase cyclist and pedestrian safety.</p> <p>Improvements to the route are to include the existing footpath between High Road and Black Barns (roughly opposite Station Road for Trimley railway station) which would need to be upgraded, widened and resurfaced to make it suitable for cyclist use, too.</p>

Map Reference	Location	Description	Recommendation
			Additional cycle parking at the school should be included if the school does not currently have sufficient cycle/scooter parking facilities.
F4	Walton High Street/High Road West	'High Road' (High Road/High Street/High Road West/High Road East) is an important commuting and leisure route for cyclists and pedestrians. However, its physical and functional constraints make fitting a continuous means of segregation for cyclists challenging. This is considered a very high priority route.	Improve cycling and walking infrastructure along the north side of Walton High Street from Recreation Lane to the Garrison Lane/High Road crossroads. Junctions with side roads should be narrowed, made more perpendicular, and visibility in and out maximised to increase cyclist and pedestrian safety. Priority crossings over side roads are recommended if shared paths are used.
F5	High Road West	'High Road' (High Road/High Street/High Road West/High Road East) is an important commuting and leisure route for cyclists and pedestrians, providing a continuous route from the north-western edge of Trimley St Martin almost to Felixstowe seafront (where High Road East then becomes Cliff Road). The pavements are generally adequate quality, but are not consistently wide enough to be inclusive, where the minimum width should be 1.5m. However, its physical and functional constraints to being widened enough to accommodate continuous, adequate-quality segregated cycling infrastructure makes substantive improvements challenging. This section continues the High Road schemes and connects Garrison Lane 'north' to Beatrice Avenue and Hamilton Road, Felixstowe's town centre. It is very wide to the western end of the section but narrows as it approaches the Beatrice Avenue/Hamilton Road roundabout. A shared path is likely to be achievable here providing all on-street parking can be moved to the south side of the road. This is considered a very high priority route.	Improve cycling and walking infrastructure along the north side of High Road West from the Garrison Lane/High Road crossroads to the Hamilton Road/Beatrice Avenue roundabout. This recommendation is intended to meet the needs of commuters and families accessing Fairfield Infant School on foot or by bike, and therefore may be best suited to a shared path rather than segregated cycle lanes. On-street parking along this stretch is entirely moved to the south side if possible. The scheme's design should appropriately reflect its intended use by families with primary school age children along the stretch most immediate to the school. The scheme should connect to Beatrice Avenue (see F91) at the eastern end and the bi-directional track that is recommended to run up Garrison Lane 'north' (F69) at the western end. If shared paths are used, additional means of physical segregation should be used to prevent any parking over the scheme, such as bollards. Refuge crossings/refuge islands etc. should be removed where they may otherwise reduce the width available for carriageway and infrastructure to co-exist, and should be replaced by formal parallel crossings. The provision of additional cycle/scooter parking facilities at Fairfield Infant School is recommended if supply is insufficient to meet increased demand.
F6	High Road East	'High Road' (High Road/High Street/High Road West/High Road East) is an important commuting and leisure route for cyclists and pedestrians, providing a continuous route from the north-western edge of Trimley St Martin almost to Felixstowe seafront (where High Road East then becomes Cliff Road). The pavements are generally adequate quality, but are not consistently wide enough to be inclusive, where the minimum width should be 1.5m. High Road	Create a shared path to run along High Road East between the Hamilton Road/Beatrice Avenue roundabout and the junction with Looe Road. On-street parking may need to be moved to the other side and any traffic islands removed, and any refuge crossings upgraded to parallel crossings. The value of existing turning boxes should also be evaluated and removed to create

Map Reference	Location	Description	Recommendation
		East is relatively wide and likely to be able to accommodate high-quality infrastructure, particularly if on-street parking can be moved to one side and traffic islands removed, and any refuge crossings upgraded to parallel crossings. The value of existing turning boxes should also be evaluated and removed to create space for cycling and walking infrastructure if not considered necessary. This is considered a very high priority route.	space for cycling and walking infrastructure if not considered necessary. The existing crossings should also be shifted to be in more relevant locations than they are currently for onward travel on foot or by bike. Ideally, junctions with side roads along this stretch should be narrowed, made more perpendicular, and visibility in and out maximised to increase pedestrian and cyclist safety.
F7	Cliff Road	Cliff Road provides an important connection to 'Old Felixstowe' eastern edge, Old Felixstowe's seafront area (for access to the sea wall, a public car park, and the Felixstowe Golf Club clubhouse), and provides for onward travel up to Felixstowe Ferry (also for the foot ferry over to Bawdsey), and down towards the main Promenade and seaside resort area via adjoining streets such as Golf Road and Maybush Lane. Cliff Road is initially quite wide but as it progresses north-east it narrows. There is an existing footpath on the east side that would likely be easy to upgrade, widen and resurface for shared use. This route is also key to a wider ambition of the Strategy to better connect Felixstowe with Felixstowe Ferry for pedestrians and cyclists, providing a safer alternative to use Ferry Road (see F115 which commences where F7 terminates at The Dip). This is considered a very high priority route.	Create a new continuous shared path from the junction with Looe Road up to The Dip for transfer to Footpaths 11, 47 and 62 (all recommended to be upgraded to all-user track status and quality) for onward travel towards Felixstowe Ferry.
F30	PROW 9	This section of route is intended to create an improved and more accessible connection between Spriteshall Lane (which currently has stepped access down to the southern side of the A14/Dockspur roundabout), the A14 lights-controlled crossings, the steps on the northern side of the A14, and the western edge of the North Felixstowe Garden Neighbourhood (SCLP12.3). This route will be particularly useful for connectivity to the proposed new leisure centre. If well designed, this connection between the North Felixstowe Garden Neighbourhood and Trimley St Mary may be able to support a useable horse riding route, despite the presence of the large roundabout. This would allow horse riders to access the various bridleways under F113 by using a combination of F30, F38 and F37 to reach F113. This is considered a very high priority route.	An all-user track is recommended to run north-to-south to connect Candlet Track (see F44) with the new all-user bi-directional track recommended under F51, and then continue south to connect to the north side of the roundabout. There are then two recommended options for improvements: Option A: A new all-user bridge (similar to the design of the Gun Lane bridge over the railway line) is recommended to span between F30 and Spriteshall Lane, therefore entirely avoiding the need to descend down to A14 level for the existing crossings over the Dockspur roundabout completely. Option B: The steps here (on the north-west side of the Dockspur roundabout, at F30's southern end) are recommended to be replaced with a carefully designed all-user ramp down to the A14 to use the existing crossings, which are both recommended to be improved to Pegasus crossing standards. The track should continue via the routing of the existing footpaths at the

Map Reference	Location	Description	Recommendation
			A14/Dockspur roundabout and up to Spriteshall Lane via another all-user ramp that replaces the steps on the south side.
F31	Abbey Walk	This recommendation covers Footpath 7, which has the potential to be upgraded to a bridleway for pedestrian and cyclist use as a shared path. This is a medium priority route.	This route is recommended to be upgraded to a bridleway to allow cyclist and pedestrian use as a shared path.
F33	Land between Cliff Road and Roman Way	This is a footpath that could be upgraded to a bridleway to increase permeability to Cliff Road. This is considered a medium priority route.	The existing path is recommended to be upgraded and improved for pedestrian and cyclists use.
F34	Land adjacent to Cliff Road	This route runs across the Cliff Road Green, following the east to west section of the current car park exit and the footpath to the steps down to the sea wall. Though most connections to the sea wall in this area are step free, this section includes steps, therefore reducing access for cyclists and pedestrians with reduced mobility. This is considered a medium priority route.	A cycling and pedestrian route between Cliff Road and the sea wall is recommended. This route should be made step-free through the installation of an accessible ramp down to and up from the sea wall, that is appropriately designed for use by cyclists and pedestrians with reduced mobility. The access route should be improved to provide a shared path for pedestrian and cyclist use, be continuous in design/dimensions, and any barriers along the route removed.
F44	Kirton Road to Gulpher Road via new track and Candlet Track	This continuous, strategic section of route connects the Martlesham to Felixstowe Key Corridor directly to the western edge of the SCLP12.3 North Felixstowe Garden Neighbourhood area via new infrastructure routes and the upgrading and improvement of existing bridleways from Kirton Road to Gulpher Road, via bridleways 47, 40, 10, 8, 7 and 27. This is considered a very high priority route.	A new continuous bi-directional all-user track (cyclists, pedestrians and horse riders) to run between Kirton Road and the North Felixstowe Garden Neighbourhood's western edge. The surfacing and dimensions designed must meet the needs of all users. This track is recommended to start on the east side of Kirton Road (opposite Roselea Nursery), with a new parallel crossing provided for safely crossing Kirton Road, and then to run south-east parallel to but separate from Kirton Road (as if heading to the Howlett Way roundabout) down to and over Capel Hall Lane, and then continuously along Candlet Track up to Gulpher Road using existing bridleways 47, 40, 10, 8, 7 and 27. The route may benefit from being adjusted from the original routing of the bridleways to optimise directness, where necessary.
F46	Land at SCLP12.3 North Felixstowe Garden Neighbourhood / Footpath 28	This recommendation follows the section of Footpath 28 that runs between Candlet Track and Candlet Road. It is intended to connect to the new residential development currently under construction on land north of Walton High Street located to the south via an appropriate crossing over Candlet Road. This route is primarily intended to connect the North Felixstowe Garden Neighbourhood's communities with Felixstowe Academy, as well as supporting other commutes and active travel journeys. This area of	Create a north-south cycle/pedestrian track between Candlet Track and Candlet Road, with an onward connection (using an appropriate, safe crossing) into the development currently under construction on land north of Walton High Street site. It is expected to follow the existing routing of Footpath 28. The intersection between this section of route and the east-to-west cycle/pedestrian track route that intersects it (F49) must be well

Map Reference	Location	Description	Recommendation
		the North Felixstowe Garden Neighbourhood is also likely to be where the proposed new leisure centre will be situated, which further supports the need for high quality active travel infrastructure in and out of the site, here. This is considered a very high priority route.	designed, with good visibility splays and a clear hierarchy of right of way to ensure the safety of all users. The exact routing of recommendations in this area will need to be developed alongside proposals for the development of the North Felixstowe Garden Neighbourhood's west side and the development on land north of Walton High Street. A suitable crossing must be located over Candlet Road for access into the site on land north of Walton High Street as this route will serve the North Felixstowe Garden Neighbourhood community's access to Felixstowe Academy (as well as other commutes and journeys by active travel).
F47	Land north of Walton High Street	This route runs through the new residential development currently under construction on land north of Walton High Street and is intended to connect the site with the North Felixstowe Garden Neighbourhood, the Felixstowe Academy, the proposed new leisure centre, and onward travel opportunities towards central and coastal Felixstowe. This is considered a very high priority route.	Create a north-to-south cycle and pedestrian route to connect the new community and the western side of the North Felixstowe Garden Neighbourhood with the Felixstowe Academy, the proposed new leisure centre and opportunities for onward travel towards central and coastal Felixstowe. In addition to this, a connection to Gulpher Road via the gap available between Treetops and Ash Tree Close is recommended for access to the central area of the North Felixstowe Garden Neighbourhood.
F49	Land at SCLP12.3 North Felixstowe Garden Neighbourhood	The North Felixstowe Garden Neighbourhood, as a strategic scale site, provides an excellent opportunity to achieve high quality strategic scale active travel infrastructure with the potential to transform active travel movement within the Trimley Villages and Felixstowe area. This will require excellent internal and external connectivity, to provide for journeys north to south and east to west. This section specifically connects journeys originating from the direction of Woodbridge/Martlesham/villages to the west of the river Deben and the western and central areas of the North Felixstowe Garden Neighbourhood; this route is likely to directly connect journeys to the proposed new leisure centre and provide connectivity for onward travel to a proposed primary school in the central area of the site. This is considered a very high priority route.	An east to west cycling and pedestrian track is recommended through the western North Felixstowe Garden Neighbourhood area from F44 or F30 to F58 via the cycling and walking connection to Gulpher Road. All internal routes are recommended to be highly permeable (easy to join) and intuitive to the most likely origins and destinations within and into/out of the North Felixstowe Garden Neighbourhood. The location is indicative on the map, and can be adjusted within reason (without reducing directness or functionality) as required by the infrastructure layout of development proposals that come forward.
F50	Land between Western Avenue and Cliff Road	This route between Western Avenue and Cliff Road provides a green, off-carriageway connection through this residential area to the improvements recommended along the sea front for northward travel to Felixstowe Ferry or southward travel towards the main sea front area. This route is already of decent quality, and is already moderately walked and cycled by Strava users, meaning it may only need to be upgraded for lawful use by cyclists; its	Route be widened and upgraded to bridleway status for use as a shared path, and made recognisable as such with appropriate signage.

Map Reference	Location	Description	Recommendation
		relevance as a useful connection for cyclists as well as pedestrians is likely to increase following improvements to the wider cycling and walking infrastructure network in the Old Felixstowe and the sea front areas. This is considered a medium priority route.	
F51	Land at SCLP12.3 North Felixstowe Garden Neighbourhood	The North Felixstowe Garden Neighbourhood (SCLP12.3), as a strategic scale site, provides the rare opportunity for strategic scale active travel infrastructure to be delivered that could have a transformative effect on active travel movement to, from and within Felixstowe and the Trimley villages. A series of strategic routes through the site, including a route running along the southern boundary, could provide for minimal or vehicle-free travel between Trimley St Martin (F44), Felixstowe Academy (F47, F51, F46), Felixstowe's retail centre (F51, F69 and F91), key primary schools (F51 and F54), its different communities (F58 and F55), and onward travel towards the coastline. F51 has already been partly delivered through the Land at Candlet Road site, between the main access point and the Grove Road/Beatrice Avenue roundabout. The remaining section of the route to the west is envisaged to be delivered as a shared path up until just before the Dockspur roundabout is reached, at which point it is recommended to break away into a track that runs north-westward up to F30 where it should then terminate. This is considered a very high priority route.	<p>The remaining section of the route to the west is envisaged to be delivered as a shared path up until just before the Dockspur roundabout is reached, at which point it should convert into a fully segregated bi-directional cycling and walking track that runs north-westward up to F30 where it then terminates. This is considered a very high priority route. It will be expected that where the route intersects with other cycling/walking routes (e.g. F46, F49), the track will be appropriately designed for optimising legibility and safe visibility splays for approaching cyclists and pedestrians.</p> <p>The section of shared path that has already been delivered along F51's route is recommended to be fitted with a further means of physical segregation from vehicles such as bollards or a well designed barrier rail to enhance pedestrian safety and encourage greater usage of the route.</p>
F52	Gulpher Road to Back Lane (Falkenham)	This suggested route essentially follows a series of existing PROW footpaths (45, 14, 13 and 8) between the North Felixstowe Garden Neighbourhood's northern edge and Falkenham, with the potential to make adjustments to the original PROW routing to both optimise its directness, and to avoid creating inconvenience for agricultural operations in this area. This route is intended to provide an off-carriageway option for bypassing the Trimley villages (and avoids use of High Road) when travelling between the North Felixstowe Garden Neighbourhood and Falkenham, Kirton or beyond. It is also intended to equally meet the needs of cyclists, pedestrians and horse riders, and as such the design and surface treatment should reflect all-user needs. The route crosses both Falkenham Brook and Kingsfleet via existing footbridges, which will need to be confirmed as adequate for all users. This is considered a medium priority route.	It is recommended that this existing PROW-based route (following adjustments to the routing) is appropriately surfaced to create an all user (cyclists, pedestrians and horse riders) track between Back Lane (for Falkenham/Kirton) and Gulpher Road's most north-western point (for the North Felixstowe Garden Neighbourhood). Though the routing should be designed to minimise necessary incursion onto it by agricultural vehicles (i.e. by being routed around field perimeters rather than heading through the middle of them), it will likely still need to be designed to be resilient to being crossed at least occasionally by agricultural vehicles.
F54	Colneis Road	Colneis Road currently has no cycling infrastructure despite providing access to two primary schools – Kingsfleet Primary School and Colneis Junior School - and being relatively wide. Improvements to Colneis Road will help to more generally improve east to west connectivity within Old Felixstowe, and into	Create a new shared path along one side of Colneis Road; the south side is likely to be most appropriate as more space is generally available and is where Colneis Junior School is located, however, weight should be given to whichever side minimises the loss of trees along this route.

Map Reference	Location	Description	Recommendation
		the North Felixstowe Garden Neighbourhood via successful connection to F51, F55 and F56. This is considered a very high priority route.	<p>Create a new shared path is around the junction with Ferry Road and over to Elmcroft Lane via a new parallel crossing.</p> <p>At the western end the shared path must directly connect to the shared paths recommended for installation around the southern arm of the roundabout (Beatrice Avenue arm), and into the Beatrice Avenue scheme (see F91).</p> <p>Street furniture or other design elements that indicate Colneis Road's provision of access to two primary schools should be considered as part of a wider and more comprehensive redesign of Colneis Road to improve its safety and environmental quality (e.g. as a School Zone; 'play on the way' design elements).</p>
F55	Hyem's Lane, Land at SCLP12.3 North Felixstowe Garden Neighbourhood	Hyem's Lane is a moderately well-used PROW for leisure walking and cycling which, together with the PROWs indicated under F58, is recommended to be retained in the North Felixstowe Garden Neighbourhood development in its current location, unless it is in the interests of the wider masterplan of the site for such a north to south connection to be somewhat re-routed. Its main strategic value arises from it being part of a potential strategic east to west movement corridor through the site from the western edge (F30) to the eastern edge (Ferry Road), and, the direct connection this route gives to Colneis Junior School, which will likely meet the primary education needs of some residents of the new communities (though a new primary school is expected to come forward for the North Felixstowe Garden Neighbourhood). Hyem's Lane also leads into Quinton's Lane, which is also recommended for improvement, providing an extra connection to High Road and the opportunity for onward travel to the coast/The Promenade via the Brackenbury Sports Centre site allocation (Policy SCLP12.5). This is considered a very high priority route.	Improvement the surfacing of Hyem's Lane to make it safe and accessible for cyclist and pedestrian use. Integration of this existing route as a segregated continuous north to south route through the site, connecting to Colneis Road to the south and Gulpher Road to the north, is also recommended. Appropriate measures to avoid vehicular use of Hyem's Lane (with the exception of access to Park Farm) are recommended.
F56	PROW 12 and PROW 13	Footpaths 12 and 13 are currently in need of re-orienting, merging together, upgrading and appropriately re-surfacing to create a connection between Hyem's Lane (F55, for access into the wider North Felixstowe Garden Neighbourhood area) and Colneis Road (for access to the primary schools). This is considered a high priority route.	Footpaths 12 and 13 to be upgraded and merged into a single continuous bridleway, and appropriately re-surfaced for cyclist and pedestrian use. The new merged bridleway should be well connected to Hyem's Lane (F55), Ferry Road (via the southern boundary of the Policy SCLP12.4 Land North of Conway Close and Swallow Close site allocation) and Colneis Road (for Colneis Junior School and Kingsfleet Primary School).

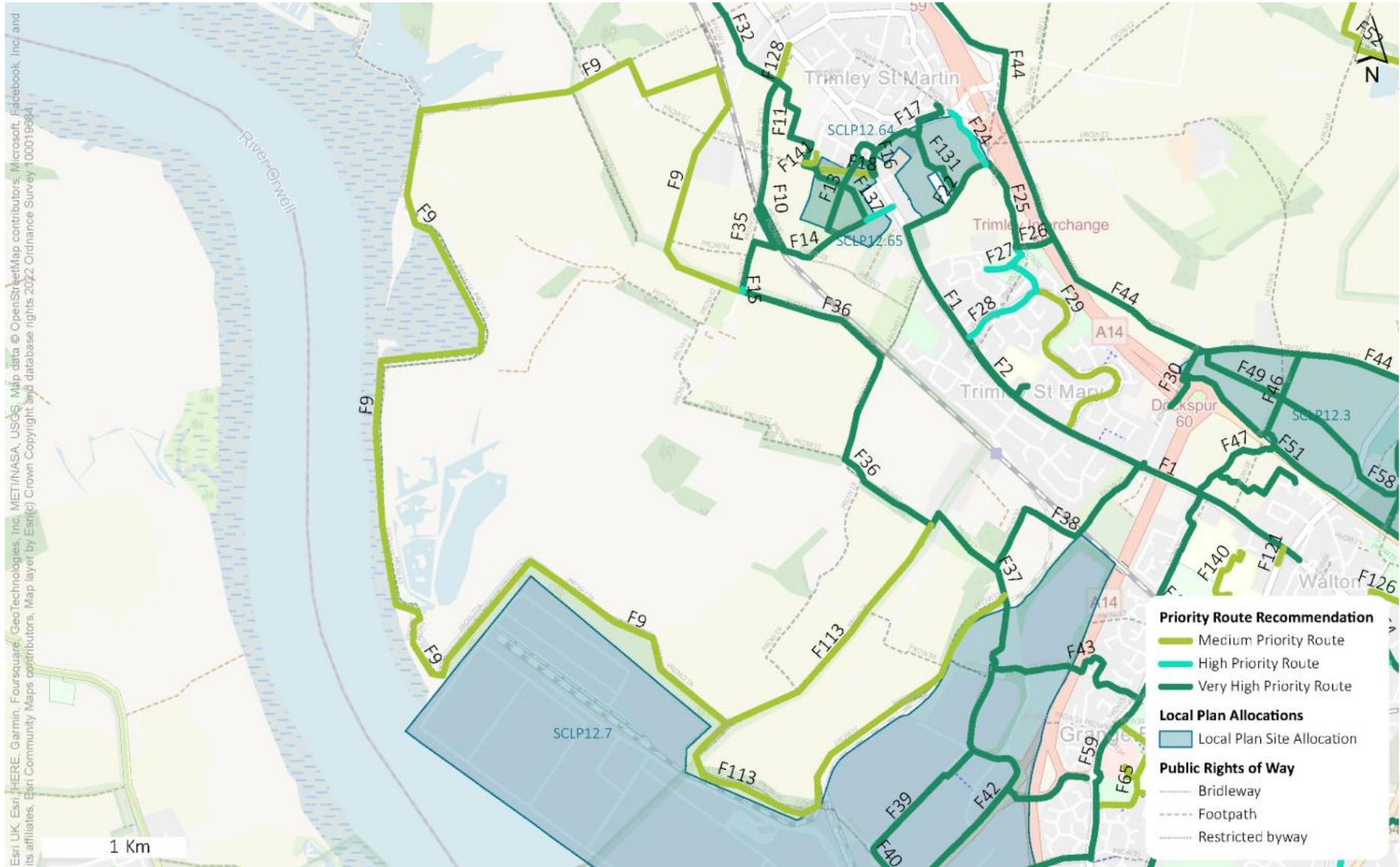
Map Reference	Location	Description	Recommendation
F57	Ferry Road and Church Road	Ferry Road and Church Road together connect the eastern-most edge of the North Felixstowe Garden Neighbourhood and the Policy SCLP12.4 Land North of Conway Close and Swallow Close site allocation with the High Road to the south. This route also connects directly to Kingsfleet Primary School, and the eastern end of Colneis Road, which has been recommended (see F54) to be fitted with a shared path to aid safe active travel to and from Colneis Junior School to the west. This is considered a very high priority route.	A new continuous shared path is recommended to run along the western edge of Ferry Road and Church Road, from the entrance to the Policy SCLP12.4 Land North of Conway Close and Swallow Close site allocation until High Road East is reached. At pinch points, particularly around St Peter & St Pauls' church, consider removal of the existing narrow pavement on the eastern side of the road which is likely to be of little value to retain for pedestrians (i.e. doesn't provide access to properties or side roads; the section immediate to the church entrance could be retained) and is too narrow to be accessible (<1.5m).
F58	Land at SCLP12.3 North Felixstowe Garden Neighbourhood	<p>The North Felixstowe Garden Neighbourhood site (SCLP12.3) is 143 hectares in area, and is intended to be masterplanned as a comprehensive, mixed-use development delivering up to 2,000 dwellings It is also expected to include a new leisure centre, 630 primary school places and early years settings, open space, and a community hub - all of which require a safe and efficient active movement network.</p> <p>The site's central area has already received planning permission for up to 560 dwellings, which will come forward in phases. Segregated internal and onward travel routes for pedestrian and cyclist use are critical to the integration of the new communities, and for safe and direct onward travel to places of employment, education, leisure and for retail/day-to-day trips in Felixstowe and the Trimley villages.</p> <p>Internal connections within the central area of the SCLP12.3 North Felixstowe Garden Neighbourhood are required for policy compliance with criterion (m), which states "A network of pedestrian, cycling and vehicular routes that provide connectivity and movement across the Garden Neighbourhood and with adjacent areas whilst protecting and enhancing local Quiet Lanes".</p> <p>An indicative network of routes is included under this recommendation reference which are based on the site layout of the central permitted section and the existing Public Rights of Way (PROW) on this site. The existing PROW routes on the site are expected to be preserved and enhanced, as per criterion (e) of the Suffolk Coastal Local Plan, though the exact routing may need to be changed through the wider masterplanning for the site as a whole. These are considered very high priority routes.</p>	<p>Internal connections within the central area of the North Felixstowe Garden Neighbourhood site allocation are required for policy compliance with criterion (m) of Policy SCLP12.3 North Felixstowe Garden Neighbourhood to ensure a high level of permeability, safety, accessibility and convenience for the future communities of the site.</p> <p>The recommended internal network includes north to south routes, east to west routes and connections through The Grove woodland. Connections through The Grove woodland must be appropriately designed to minimise impact on wildlife and amenity.</p> <p>Connections at the southern boundary of the site should be provided for onward travel into Felixstowe and the Trimleys via the development under construction on land north of Walton High Street site (for Felixstowe Academy), Ataka Road, Gulpher Road (for High Street/High Road), access to F51, Colneis Road and Grove Road (for Garrison Lane and Beatrice Avenue/Hamilton Road).</p>

Map Reference	Location	Description	Recommendation
F69	Garrison Lane 'north' and the Garrison Lane / Candle Road / Grove Road / Cowpasture Allotments access roundabout	Garrison Lane is an important north to south arterial route through central Felixstowe. Most of Garrison Lane's length is heavily used by Strava Metro users (cyclists and pedestrians), and scores highly on the Propensity to Cycle Tool. The exception to this is Garrison Lane 'north' – the section between High Road West and the Candle Road/Grove Road roundabout – is currently used very little by cyclists or pedestrians due to the lack of infrastructure, speed of the road, and until recently (with development coming forward to the north), minimal demand for active travel along this stretch. Garrison Lane feeds into a number of side roads, including those for alternative access to Felixstowe railway station and Hamilton Road for the town centre. Garrison Lane's width varies throughout its length, becoming more constrained south of High Road (Garrison Lane 'south'). However, the section north of High Road, Garrison Lane 'north', benefits from significant grass verges, which presents a prime opportunity for a segregated bi-directional cycling and walking track. The Garrison Road/Candle Road/Grove Road/Cowpasture Allotments access roundabout currently has no infrastructure for crossing it, despite its proximity to Cowpasture Allotments, a large allotment site that there is likely to be some demand to walk or cycle to. The roundabout has a significant amount of highway land surrounding it that's made up of grass verges and islands - with the western arm's island being particularly wide and sweeping, providing plenty of space for a parallel crossing over to the northside. The new roundabout infrastructure can connect directly to the bi-directional track recommended under reference F51. This is considered a very high priority route.	<p>A bi-directional cycling and pedestrian track is recommended along the eastern side of Garrison Lane 'north'. A new parallel crossing over to Taunton Road is recommended, upgrading the existing pedestrian crossing. Consideration should be given to whether this crossing would benefit from being moved southwards (further from the roundabout) to improve visibility of the crossing.</p> <p>At the roundabout at the northern end of Garrison Lane 'north', the track is recommended to continue around the roundabout's western side and up to an appropriate point for a parallel crossing over Candle Road. This crossing should then connect directly to the new shared path that has been delivered along the north side of Candle Road (see F51).</p> <p>The Garrison Lane 'north' section of track should connect with the existing Grove Road scheme (to the east, along the south side) and the infrastructure improvements to Beatrice Avenue and the Beatrice Avenue/Grove Road/Colneis Road/Links Avenue roundabout (see F91 and F54).</p>
F70	Garrison Lane 'north' connection into Fairfield Avenue	In combination with a new fully segregated bi-directional track along Garrison Lane 'north', it is recommended that the existing footpath connection into Fairfield Avenue is upgraded and the existing cycle barrier removed. This represents a 'quick win' for improving permeability between central Felixstowe and Old Felixstowe. This is considered a high priority route.	It is recommended that this footpath be upgraded and improved to make it suitable and accessible for cycling and walking, which should include the removal of the existing cycle barriers.
F71	Garrison Lane/High Road crossroads	The current large crossroads creates an intimidating and highly 'car-dominated' feel to what could otherwise be an area with higher levels of cycling and walking activity – there are surrounding shops, nearby is the Felixstowe railway station, and the town centre (Hamilton Road). Following the delivery of the North Felixstowe Garden Neighbourhood (SCLP12.3), Garrison Lane 'north' will likely be one of the main routes used to the proposed new leisure centre. The North Felixstowe Garden Neighbourhood is likely to be a significant contributor to improvements to this crossroads due to likely increases in vehicle movements resulting from the development. As	It is recommended that the crossroad junction is reworked to improve the pedestrian crossings over each arm and to install cycle-only filter lights that allow on-carriageway cyclists to get 5+ seconds head start over vehicles. Good connectivity between the north arm and east arm crossings to the bi-directional track along the east side of Garrison Lane 'north' (F69) will be critical to support its useability.

Map Reference	Location	Description	Recommendation
		part of the capacity increasing works, it is recommended that the minimum improvements should be to improve the safety and useability of pedestrian and cyclist crossings and to install cycle-only filter lights. This is considered a medium priority route.	
F91	Beatrice Avenue	Beatrice Avenue is important for connecting the SCLP12.3 North Felixstowe Garden Neighbourhood with the town centre (Hamilton Road). Beatrice Road is unable to be fitted with shared paths due to the presence of large mature street trees, and the incursion of their roots into what needs to be a smooth, flat surface. Beatrice Road is not wide enough for segregated on-carriageway cycle lanes, and necessary vehicular accesses to properties' driveways are numerous and relatively close together, making a substantively segregated scheme unworkable here. This is considered a high priority route.	<p>Beatrice Avenue to be made into a Cycle Street, whereby the design brings cyclists into the primary position using narrow lanes, cycle priority signage, and a tactile surface in the central area, meaning vehicles must drive more slowly and it is more difficult to overtake. On-street parking is controlled using bays and additional landscaping is typically also added to the street. The key UK example of a Cycle Street is the Taff Embankment in Cardiff.</p> <p>The scheme must connect well to the existing infrastructure along the south side of Grove Road and the improvements to Colneis Road (F54), including an improved crossing over the Beatrice Avenue arm of the Grove Road/Colneis Road/Links Avenue/Beatrice Avenue roundabout.</p>
F92	High Road / High Road East / Hamilton Road / Beatrice Avenue roundabout	The High Road/High Road East/Hamilton Road/Beatrice Avenue roundabout is an important connection point between Walton, Old Felixstowe, the North Felixstowe Garden Neighbourhood area and the Hamilton Road area for the town centre. However, the High Road/High Road East route is heavily used by vehicles as the main east to west route through Felixstowe. The current crossings over each arm of this roundabout are not adequate for pedestrians or dismounted cyclists to use to safely cross this road. . This is considered a medium priority route.	The existing refuge crossings over each of the roundabout's arms are to be upgraded to parallel crossings.
F93	Railway Approach/High Road West (for Felixstowe railway station)	The Railway Approach/High Road West lights-controlled junction is a very busy and congested section of the High Road route into Felixstowe due to the demand for access to Felixstowe railway station and the large car park to the rear of Great Eastern Square/the Coop, which is also used for accessing the wider Hamilton Road area for the town centre. It is further congested by the petrol station, garage and the police and fire service stations located between the Railway Approach junction and the roundabout for Beatrice Avenue (for access onto a main route out of Felixstowe via Candlet Road and the A14) and Hamilton Road. Improving access to Railway Approach for Felixstowe railway station is important for supporting greater modal shift to sustainable travel with journey 'tranches', i.e. to cycle to the railway station and then take a train. This is considered a very high priority route.	The traffic lights system at the Railway Approach/High Road West junction is to be upgraded with a cycle-only filter lights that provides cyclists with 5+ seconds of head start over vehicular traffic so that they have time to safely cross the junction without the risk of conflict with vehicles.

Map Reference	Location	Description	Recommendation
F102	Rosemary Avenue	A straight and relatively wide residential street that heads northwards towards the North Felixstowe Garden Village allocation (SCLP12.3). This is considered a high priority route.	New shared path along western side of Rosemary Avenue.
F105	Elmcroft Lane / Footpath 8 (West)	Elmcroft Lane provides access to Kingsfleet Primary School and a small number of homes; it is not a vehicular through route, though does currently provide pedestrian access to Westmorland Road and Windermere Road via the western-most section of PROW Footpath 8. It is not currently useable by cyclists due to barriers and prohibitive signage. There is capacity for this route to be widened (and the barriers removed) to make this route suitable for cyclists as well as pedestrians. This is considered a high priority route.	This route is to be upgraded to a bridleway to allow cyclist and pedestrian use.
F106	Westmorland Road	This section of Western Avenue and Westmorland Road connects the High Road with both entrances/exits for Kingsfleet Primary School (the main entrance being accessible via Elmcroft Lane) and Cliff Road (for access to the sea front and Felixstowe Ferry). This is considered a high priority route. This route benefits from a wide carriageway and in places ample grass verge space for an infrastructure scheme suitable for families cycling or walking with children on bikes/scooters to and from school. The pavements along this route are generally not of an accessible width and therefore shared paths are likely to be the most suitable and beneficial infrastructure option. This is considered a high priority route.	New shared path to be created along one side of this route. A further short stretch along Cliff Road to connect to the sea front via F34 is also recommended. An appropriate crossing should be provided over Keswick Close.
F111	Cliff Road (cycle parking)	The Dip (the café and public toilets in this area), off Cliff Road, is an important rest and refreshment stop for leisure cyclists and walkers though currently lacks cycle parking. This is considered a medium priority route.	Cycle parking to be installed at The Dip.
F115	The Dip to Felixstowe Ferry via PROW Footpath 62	This route seeks to provide a vital cycling and walking connection between Old Felixstowe and Felixstowe Ferry via the sea front, providing a safer alternative to Ferry Road, which has poor visibility. Footpath 62 varies in terms of width, surfacing type and quality, and surface water drainage capacity, which would ideally need to be improved and made consistent throughout; at the very least sections of gravel would need to be replaced with smooth bound surfacing. This route is part of the Strategy for Felixstowe's wider ambition to create a safe and convenient high quality cycling and walking route between Landguard Point and Felixstowe Ferry via the sea front that is as vehicle-free as possible (F119, F118, F117, the Promenade, F116 and F115). This is considered a high priority route.	The coastal footpath and sea wall between Felixstowe Ferry and The Dip (Footpaths 62 and 65) should be upgraded to bridleway status and the surfacing re-laid/replaced with smooth bound surfacing suitable for cycling. Where the route is currently exposed to unprotected steep sides, appropriate barriers are recommended to keep cyclists safe.
F126	Land between Recreation Lane and Plymouth Road	This route seeks to provide a traffic free connection between Plymouth Road and Recreation Lane. This is considered a medium priority route.	Widen, resurface and allow cycling over the footpath to the south of Walton Recreation Park between Recreation Lane and Plymouth Road.

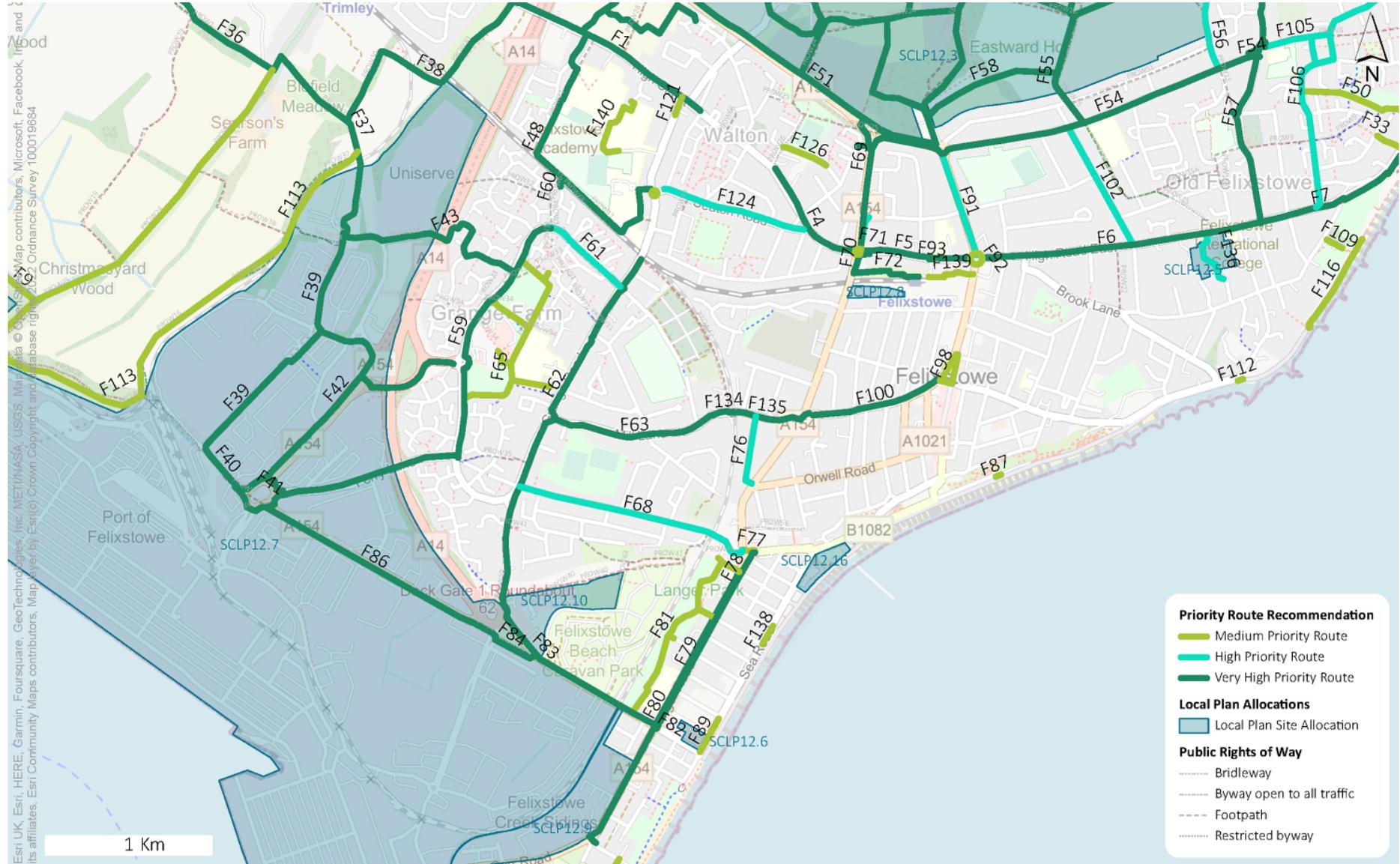
Felixstowe Leisure Loops



Map Reference	Location	Description	Recommendation
F9	Trimley Marshes Nature Reserve circular route	This route is intended to function as a circular, almost entirely off-carriageway leisure route for safe extended cycling, walking and horse riding activities for fitness and recreation purposes, rather than for 'A to B' travel. The route this recommendation follows does not include any use of the Orwell river wall, which is only suitable for low levels of pedestrian use due to the protected habitat for ground nesting birds along the Orwell Estuary areas. This route instead follows the bridleways that are set back from the river wall, therefore minimising the direct disturbance (mostly from dogs off leads) and 'skylining effect' (casting shadows that frighten birds that are feeding, nesting or resting on the river side) arising from activity on top of the river wall. Instead, this route includes improvements to the following existing bridleways and highways: 27A, 36, 45, 49, 3, 48, Thorpe Lane, Grimston Lane and then down to bridleway 57/Gun Lane, Bridleways 22 and 1 to Cordy's Lane for bridleway 24. It is expected that the improvements to these bridleways would be delivered alongside effective means of preventing cycling and horse riding along the river wall, and encouraging pedestrians with dogs to keep their dogs on a lead. This is considered a medium priority route.	<p>This route includes improvements to the following existing bridleways and highways: 27A, 36, 45, 49, 3, 48, Thorpe Lane, Grimston Lane and then down to bridleway 57/Gun Lane, Bridleways 22 and 1 to Cordy's Lane for bridleway 24. These improvements should include widening of the bridleways and the use of location-sensitive and an all-user appropriate surfacing treatment; it is especially important that the infrastructure is high quality, as it must be effective in incentivising its use, therefore providing no reason to transfer up to the river wall when on horse back/cycling. If possible, the designed scheme should help to improve surface water drainage performance in key areas along this route.</p> <p>Signage and any other appropriate and effective means of deterrents from cycling or horse riding on the river wall, and the off-lead walking of dogs, should be considered. Improved way finding signage in this area is also required, including maps and local information/educational signage.</p>
F113	Circular leisure route - bridleways 24, 25, 26, 18, 16, 37	This route consists of a series of bridleways that have the potential to be improved to create a small circular leisure loop suitable for use by cyclists, pedestrians and horse riders for fitness and recreation purposes. The site has some surface water drainage issues which ideally need to be addressed through any improvements works to increase year-round useability of the loop. The surfacing treatment used should be as permeable as possible, and be incorporated into the overall design to sustainably drain the field and provide a drier, less muddy route. The location is rural and is expected to be provided with a surface treatment that is sensitive to its location, whilst also able to accommodate the needs of cyclists using off-road bikes, pedestrians and horse riders equally. This route overlaps with the England Coast Path and Stour and Orwell Walk at bridleways 25 and 26. It is unknown whether there is an intention to improve the safety and accessibility of the level crossing at bridleway 17 (Fagbury Road) over the freight railway lines for the Port via the England Coast Path. It would realistically require a bridge over the freight railway lines to meaningfully improve pedestrian safety and accessibility across this level crossing. However, the cost of delivering such a bridge versus other priorities for the district's movement network means it is unlikely to be delivered in the medium-term., A precautionary	This all-user track is to be improved through widening and surfacing using location appropriate surfacing treatments that meets the needs of cyclists using off-road bikes, pedestrians and horse riders, and improves the overall surface water drainage performance of the route for year-round useability. Improvements to this route should neither prohibit nor encourage increased use of the level crossing through the Port over to Fagbury Road due to concerns about the safety and accessibility of the crossing without the provision of a new bridge to replace the level crossing, which is unlikely to be deliverable. Improvements to the F113 routes must include way finding signage to help direct all users to their intended exit points; signage is recommended to provide information on alternative means of accessing western Felixstowe on foot or by bike than use of the Fagbury Road crossing (e.g. see F43).

Map Reference	Location	Description	Recommendation
		approach has therefore been taken to instead provide improvements to an alternative route between the leisure loop and west Felixstowe, via F37. This is considered a medium priority route.	

Port and West Felixstowe and Central Felixstowe



Map Reference	Location	Description	Recommendation
F37	PROW Bridleways 12 and 14	Bridleways 12 (Blofield Track) and 14 are key traffic-free connecting points into the Port, originating on Cordy's Lane and terminating on Nicholas Road. However, the surfacing on both bridleways is currently in poor condition with deep potholes, and has general surface water drainage issues. This is considered a very high priority route.	Improvements to the surfacing and general drainage capacity along Blofield Track is recommended to improve its useability as an all-user (cyclists, pedestrians and horse riders) track.
F39	Nicholas Road and Parker Avenue	The route via Nicholas Road and Parker Avenue to Fagbury Road is a key route into the western side of the Port. This is considered a very high priority route.	Improve Nicholas Road's existing short section of shared path up to the track (Bridleway 14/12), and extend it all the way down to the roundabout with Blofield Road (the dense hedging at this point will need to be removed). From the Blofield Road roundabout create a new shared path using the existing short stretch of footpath initially, and then extend the new shared path to Fagbury Road.
F40	Fagbury Road from junction with Parker Avenue to Dock Gate 2 roundabout	Fagbury Road is a key route through the western side of the Port with pavements on both sides, and an existing shared path on the north side that could be widened and improved for safer cycling and walking. To the north of Fagbury Road is an existing level crossing over multiple freight railway tracks, for which access over is carefully managed. Substantive improvements to the crossing's safety are not likely to be able to be achieved without the addition of a bridge, which would be very expensive and is unlikely to come forward in the medium term. It is therefore not appropriate to encourage increased use of the crossing by cyclists and pedestrians at this time. Improvements should therefore be limited to improving access to Parker Avenue only. This is considered a very high priority route.	Widen and improve Fagbury Road's existing shared path and connect to Walton Avenue's recommended new bi-directional track on the south side. Unless there is scope for a bridge over the railway tracks, no improvements are recommended to the existing level crossing over the freight tracks to the north.
F41	Dock Gate 2 roundabout	The Dock Gate 2 roundabout is a key connecting point between Walton Avenue (F86) to the east, Fagbury Road (F40) to the west and Ferry Lane (F42) to the north-east. However, the Dock Gate 2 roundabout has limited existing infrastructure for movement around its southern and eastern arm. The existing infrastructure has the potential to be improved in terms of width, surfacing and signage, and could be extended north-eastwards for safe passage over to Ferry Lane. This is considered a very high priority route.	It is recommended that the existing footpaths and shared paths around the Dock Gate 2 roundabout are widened, resurfaced and extended to create full circulation. Upgrade all crossings over the various roundabout arms to parallel crossings.
F42	Ferry Lane (Option 1) OR Trinity Avenue/Blofield Road (Option 2)	There are two main route options for directly connecting more cycling and walking journeys from western and central Felixstowe to the employment uses in the western area of the Port, and to onward travel options there via F39, F40, F41 and F86, etc. Option 1 consists of improvements to Ferry Lane, a one-way road which connects directly to F41 (Dock Gate 2 roundabout) and F59 (Wesel Avenue/Grange Farm Avenue); Option 2 consists of two shared paths,	Option 1: It is recommended that a continuous shared path along Ferry Lane is created if possible, or, if this isn't found to be able to be accommodated due to the narrow width available, that the eastern section (east of the Orwell House turning) is modally filtered so that only pedestrians and cyclists may use it (on a bi-directional basis), though this is contingent on being feasible for adjoining properties' access. If a shared path is able to be accommodated, as Ferry Lane is

Map Reference	Location	Description	Recommendation
		<p>Shared Path A and Shared Path B, which connect into the Port via the connector road to the A14 and Trinity Avenue and then either Blofield Road to Parker Avenue (Shared Path A) or heads south on Trinity Avenue to Dock Gate 2 roundabout (Shared Path B). It is not intended that both options will be delivered, but instead one option or the other.</p> <p>The Ferry Lane option for improvements to connect to the western area of the Port are covered under Option 1 of the recommendations. Ferry Lane is a one-way vehicular route out of the Port into west Felixstowe. According to Strava Metro Ferry Lane is heavily used by both pedestrians and cyclists as one of the key routes to/from the Port into west Felixstowe, though it is unknown to what extent it is used for bi-directional travel by cyclists. In places it is quite restricted in width, and combined with Ferry Lane's use by large commercial vehicles (limited in size to 3.5T, as signed before by the overpass) this poses a safety risk.</p> <p>The Blofield Road (Shared Path A) and Trinity Avenue (Shared Path B) option for improvements to connect to the western area of the Port are covered under Option 2 of the recommendations. The connecting road from the Grange Farm Avenue/Morrisons supermarket roundabout to Trinity Avenue features an existing shared path, though its width varies and would need widening to become LTN 1/20 compliant. Upon reaching Trinity Avenue the existing route crosses over (via a pedestrian crossing) to the west side and downgrades to a footway with restrictive bollards. Although there is sufficient space on the eastern side of Trinity Avenue to create a shared path, there is the engineering challenge of levelling an adequate area of the mounds that exist at the more northern end. There is also no additional access to the employment use units to be gained from extending the route along the eastern side, though there is on the western side. It is therefore recommended that the west side be upgraded, widened and the surfacing improved. The existing bollards could be recycled for added physical segregation benefits in the new scheme if they are found to be in adequate condition for reuse. Similarly the northern arm of the route over to Blofield Road currently downgrades to footways after crossing over via the</p>	<p>one-way for vehicles, it should be made clear with signage that cycling in both directions is allowed on the shared path. At the western end the route must connect well to the new shared paths that have been recommended to fully circumnavigate the Dock Gate 2 roundabout (see F41). At the eastern end, this route must connect well to a suitable parallel crossing over Grange Farm Avenue/Wesel Avenue (see F59).</p> <p>Option 2: Two sections of shared path are recommended: Shared Path A and Shared Path B, with an initial common section based on the existing shared path in this location. Common to both sections is the route between the Grange Farm Avenue/Morrisons roundabout and Trinity Avenue's eastern boundary via an existing shared path along this A14/Trinity Avenue 'connector road'. Measures to claim carriageway, such as a priority 'give way' system, under the A14 overbridge should be considered to create more width for an LTN 1/20 compliant shared path. Shared Path A is recommended to continue over Trinity Avenue using the existing south-east to north-west crossings over Trinity Avenue to head up to Parker Avenue via Blofield Road, terminating where it reaches F39. Shared Path B is recommended to continue along the eastern boundary until the existing informal crossing point is reached over Trinity Avenue, which is recommended to be upgraded to a parallel crossing - signalised if possible. Shared Path B is then recommended to run along the western boundary of Trinity Avenue via the existing footway's routing. The existing bollards along this section are recommended to be recycled for added physical segregation benefits in the new scheme if they are found to be in adequate condition for reuse. Once almost at the Dock Gate 2 roundabout, Shared Path B is then recommended to connect to the improvements recommended to Dock Gate 2's circulation route (F41) and the improvements at Walton Avenue (F86).</p>

Map Reference	Location	Description	Recommendation
		pedestrian crossings. Though there are width restrictions created by hedgerow and the fencing of boundaries present along this route, these are likely to be resolvable through being moved back 1-3 metres; negotiation with the Port Authority will be key to all improvements within the Port area. For the section of the route that passes under the A14 via an overbridge, a 'give way' with a priority hierarchy system may be useful for reducing carriageway space so that an LTN 1/20 width compliant shared path can be created. This is considered a very high priority route.	
F43	PROW Footpath 32B to Rendlesham Road and Hintlesham Drive	The existing footpath provides a pedestrian connection between Nicholas Road/Clicket Hill Road and west Felixstowe via Rendlesham Road/Hintlesham Drive and Grange Farm Avenue. The route includes a pedestrian bridge over the A14. The route is indicated as a footpath in the definitive map though signage on the ground provides mixed messaging - there is shared path status signage through the mid-section of the route, but the barriers have 'cyclists dismount' signage attached to them immediately before (i.e. either intending for the barriers to be negotiated on foot, or for the route to be continued on foot only, as per the PROW status). The eastern most section of the route has poor surfacing that needs to be replaced. This connection is of strategic value due to the connectivity it provides between residential areas to the west and the employment sites to the north and south. There are vehicle barriers along this route that may also be restrictive to wider/longer bikes, such as cargo bikes or adapted bikes, that may be better to be replaced with bollards. This is considered a very high priority route.	Upgrade the footpath sections of this route to bridleway status throughout, replace the barriers with bollards, consolidate the signage, and resurface the sections of the route with poor surfacing. This route should be well connected to Nicholas Road (F39), Clicket Hill Road and Bridleway 12/Blofield Track (F37) for safe and convenient onward travel.
F48	Walton Hall Drive / Footpath 31 / Railway bridge / Runnacles Way / Footpath 43 / Maidstone Road	This route follows Walton Hall Drive's existing shared path to the main entrance to Felixstowe Academy, then follows Footpath 31 to the bridge over the railway line (to Runnacles Way). It then continues eastward to Maidstone Road 'south' (Footpath 43) via the rear of the former Maidstone Infant School site. There is a currently inaccessible (due to the fencing around the Felixstowe Academy site) stretch of path left over from the former Orwell High School site that runs behind properties at Maidstone Road 'south' and up to the Felixstowe Academy/Felixstowe Community Hub roundabout. Improving and opening access up to the Orwell High School paths would provide access to Maidstone Road 'north' and Seaton Road. This route is of strategic value for encouraging children (particularly from	It is recommended that the section of Footpath 31 that runs between the main entrance to the Felixstowe Academy site to the railway bridge, and the section of Footpath 43 that runs between the railway bridge and the path up to the Felixstowe Academy roundabout is upgraded to bridleway status, widened and suitably re-surfaced for safe cycling and walking. Appropriate lighting should also be added. The fencing around Felixstowe Academy's boundary will need to be moved at some points along the route to provide adequate space for route widening, to reduce the acuteness of sharp corners where they occur, and to reduce the sense of enclosure created by narrow paths and tall fences. The potential connection point between the Footpath 43 section of the route to

Map Reference	Location	Description	Recommendation
		Felixstowe's western area, i.e. south of Runnacles Way) to walk and cycle to school, due to the route's potential to connect them with Felixstowe Academy and the Maidstone Infant & Causton Junior School. The existing footpath is currently too narrow for shared path standards, is not lit, has high and tightly enclosing fencing (along the Felixstowe Academy's site boundaries), and has tight corners that currently makes cycling the route difficult. The surfacing treatment is also relatively loose and the footpath is not well maintained. The current footpath also lacks formal connections into the Walton Gate development immediately to the west, despite opportunities for cycling and walking connections at various points. It appears that residents have created an informal 'gap in the fence' desire line connection onto the footpath in the south-eastern corner of the development. As well as increasing permeability and useability having more access points to and from the route may also make the route feel safer, as there will be opportunities to exit the route (which is otherwise constrained on both sides) if needed. The route runs close to Hawkes Lane, which is locally considered to be unsafe for school children to use. The railway bridge (see F60) is currently neither suitable for cycling over (unlike the bridleway bridge at Gun Lane, for example) nor does it provide a wheeling ramp for cyclists (that are physically able) to guide their bikes over the bridge whilst dismounted. This is considered a very high priority route.	<p>the Maidstone Road/Seaton Road roundabout (via the Felixstowe Academy paths and internal roundabout) currently has restricted access, meaning access would need to be negotiated.</p> <p>The potential and existing informal connection points into the Walton Hall Drive development should be created (or formalised where created by desire lines) through improvements. Appropriate lighting (for wildlife and residential amenity) should be added to the route for its use outside of daylight hours.</p> <p>Access from Walton Hall Drive/Felixstowe Academy to Hawkes Lane is recommended to be closed.</p>
F59	Runnacles Way, Grange Farm Avenue/Wesel Avenue to Ferry Lane and Grange Road	Grange Farm Avenue is an important section of cycling and walking infrastructure in the west of Felixstowe network as it directly provides access to the local centre (supermarket, pharmacy, health centre, skate park) and connects to routes for onward travel to Grange Community Primary School, Felixstowe Academy (via the railway bridge and Footpath 31, see F48), the Port (Ferry Lane, F42 or over to Nicholas Road via the A14 bridge, see F43), Haven Exchange and the sea front via Grange Road/Peewit Hill. Grange Farm Avenue has an existing shared path section that runs beside the green space on the eastern side. There is significant capacity along this route to extend the existing shared path to Ferry Lane and Grange Road (see F62). Though Grange Farm Road is a secondary road, it is a through road and it does link directly to the A14, and provides access to a supermarket, so is likely to experience significant vehicle flows. The quality of the infrastructure delivered should reflect this, which may	A continuous shared path along the eastern side of Grange Farm Avenue is recommended to be created by extending and improving (resurfacing and widening, where needed) the existing shared path along this route. It is also recommended to be delivered in conjunction with new parallel crossings over Grange Farm Avenue at logical points for onward travel - the key points being for transition to/from: Runnacles Way for Felixstowe Academy, Hintlesham Drive for the Port, Ferry Lane (west) for the Port and Peewit Hill for the Port/Haven Exchange/onward travel to the sea front. The existing shared path along this section should be improved to LTN 1/20 standards, as appropriate to the traffic levels of this route.

Map Reference	Location	Description	Recommendation
		mean it is appropriate to add additional means of segregation, such as through the use of regular bollards. This is considered a very high priority route.	
F60	Railway bridge between Runnacles Way and Hawkes Lane	The railway bridge is currently not suitable for directly cycling over (unlike the bridleway bridge at Gun Lane, for example), and neither does it provide wheeling ramps for cyclists (that are physically able to) to guide their bikes over it whilst dismounted. For most cyclists, this will dissuade them from using the railway bridge, despite the strategic value of this route. This is considered a very high priority route.	In the short term, a wheeling ramp is to be fitted to both flights of steps. In the longer term this bridge is recommended to be upgraded to a bridleway bridge, that allows cyclists to directly ride over the bridge, or at least more accessibly guide their bikes over it whilst dismounted. The barriers on the Runnacles Way side of the bridge are recommended to be removed.
F61	Grange Farm Avenue	This section of Grange Farm Avenue already has shared paths, though they are not up to LTN 1/20 cycle infrastructure design standards of quality. This is considered a high priority route.	Improve the existing shared paths to LTN 1/20 standards and create safe crossings and connections to Maidstone Road/Grange Road (F62) and the shared path along the main section of Grange Farm Avenue (F59).
F62	Maidstone Road/Grange Road	Maidstone Road and Grange Road provide an important commuter-cyclist route by connecting High Road to the Port and sea front. This section of the route also provides direct access to Grange Community Primary School. The route also connects with other major commuter cyclist routes to other relevant destinations such as Mill Lane to Garrison Lane for Felixstowe train station and Hamilton Road (for the town centre). Though the northern section of Maidstone Road (F121) is very constrained, the section south of the railway bridge is wider and has more scope for improvements; this is the case for Grange Road also. This is considered a very high priority route.	Create a new shared path along one side of Grange Road between Cloncurry Gardens and the Maidstone Road railway bridge.
F63	Mill Lane	Mill Lane is a key east to west connection between west Felixstowe's most important commuter routes (Grange Road/F62; F65, Grange Farm Avenue/F59 and Ferry Lane/F42 or F43 over the A14 footbridge) and the Hamilton Road area for the town centre. Mill Lane also provides connectivity to Grange Community Primary School (approx. 180 primary school places). The existing footpaths are too narrow in places to be inclusive for people with limited mobility. There are some sections of allocated on-street parking, as well as evidence of parking on the grass verges on both sides. This is considered a very high priority route.	A shared path is provided on one side of Mill Lane. The Wadgate Road/Stour Avenue roundabout is to be redesigned for cyclist and pedestrian priority crossings over each arm.
F65	Cavendish Park	This recommended route is intended to extend and improve the existing shared paths that are routed through Cavendish Park, the large open green space to the east of Grange Farm Avenue, and to the west of Grange Community Primary School. This green space	The existing shared paths running through Cavendish Park are to be widened to a consistent adequate width for bi-directional travel throughout. A section of wider, smooth and flat area is recommended to be added off from a section of shared path near

Map Reference	Location	Description	Recommendation
		<p>features Felixstowe Skatepark, and connects via the Grange Farm local centre - made up of a supermarket, pub, service station, pharmacy, medical centre, and a basketball court. The recommended extensions and improvements intend to better utilise and place-make this park as a space for children and young people to exercise and socialise - building on the social infrastructure already provided for young people in Cavendish Park, and the enhanced connections to and from it arising from other improvements suggested in the Strategy (e.g. F48, F60 and F59 for connectivity from Felixstowe Academy). The extensions of the existing shared paths within the park are intended to allow for near full circulation around and through the park from different origins to different key local destinations, such as Grange Community Primary School and onward to the Port and central Felixstowe. Extensions to the existing shared paths provide a safe off-carriageway space for children to learn to ride a bike, and for children and young people to exercise, play, meet and socialise. This is considered a medium priority route.</p>	<p>the skatepark in order to provide an appropriate space for rollerblading, skateboarding (on flat), and scootering activities. New shared paths are recommended to extend from the existing east to west shared path that runs via the skate park to run along the inside eastern edge and up and around the perimeter of the park, following the desire lines, to connect into the park's adjoining streets, and connects back to Grange Farm Avenue at the north-western corner. Cycle parking should be added to key locations within Cavendish Park. The existing and new sections of shared paths are recommended to be appropriately lit to increase safety whilst managing amenity and environmental impacts. These recommendations are intended to increase cycling and walking permeability to and within the park, diversify the range of activities that can be accommodated within the park space, and as importantly, to provide a safe space for leisure cycling for its own sake, particularly for children (e.g. a space for children to learn how to ride bikes) without the threat of vehicles.</p> <p>Cycle parking for use of the Grange Farm local centre and Cavendish Park should also be provided along this route. The shared paths are to be lit. A hard surfaced section that is deliberately widened for rollerblading, skateboarding and associated activities is also recommended. These paths are to be appropriately lit to manage amenity/environmental impacts.</p>
F68	Coronation Drive to Garrison Lane 'south'	<p>Coronation Drive provides an important connection between Grange Road/Maidstone Road and the southern end of Garrison Lane, both of which are key arterial commuter routes between the Port/sea front (via Ferry Lane or the Dock Gate 1 roundabout for Walton Avenue) and central Felixstowe (various east-to-west connections to Hamilton Road are available), and the forthcoming North Felixstowe Garden Neighbourhood (via Gulpher Road or Candlet Road). The southern part of Garrison Lane is too restricted to retrofit with high quality cycling infrastructure (pavements are generally adequately wide, though some resurfacing may be appropriate), so opportunities to provide alternative routes for cyclists to use instead of Garrison Lane is appropriate. One such option is through the use of Coronation Drive as a bypass from Garrison Lane onto Grange Road - which essentially runs parallel with Garrison Lane - for journeys to the north</p>	<p>The existing footpath on one side of Coronation Drive is to be upgraded, widened and resurfaced as a shared path. The footpath connecting Coronation Drive to Garrison Lane 'south' is recommended to be widened (where possible), resurfaced and the multiple sets of barriers along its length removed.</p>

Map Reference	Location	Description	Recommendation
		or west. Coronation Drive itself is relatively wide with wide pavements and verge areas. However, the footpath connecting Coronation Drive to Garrison Lane is relatively narrow and has multiple sets of cycle barriers which will need to be removed. This is considered a high priority route.	
F72	Land to the south of High Road West, between Garrison Lane and Railway Approach (for Felixstowe railway station)	This is a potentially important connection for journeys that have originated in the SCLP12.3 North Felixstowe Garden Neighbourhood, Walton or Fairfield area or have otherwise used the recommended infrastructure around and along Garrison Lane 'north' (F69, F51, etc.) to access Felixstowe railway station to the south, and the town centre (Hamilton Road) via Great Eastern Square. This route passes through railway land. This could potentially be teamed with a wider initiative to better utilise this land for the public good or biodiversity benefit, providing it can be made safe from the railway line/improved in this way. This is a very high priority route.	A new shared path is recommended to run from the High Road West crossing point over the crossroad's eastern arm, along Garrison Lane 'south', before then turning to head east through the railway land (parallel to the southern boundary of the permissioned site on the High Road West/Garrison Lane 'south' corner) and through to Railway Approach. Co-delivery of landscaping/biodiversity benefits is also recommended.
F75	Mill Lane/Garrison Lane crossroads	This crossroads is currently difficult to safely navigate by cyclists and pedestrians. This is considered a very high priority route.	Mill Lane/Garrison Lane's staggered crossroads section is recommended to be redesigned to truncate the overall length of the stagger, bringing forward the north/south stop lines, narrow the wide mouth Mill Lane junctions and provide parallel crossings over each arm, with shared paths leading to/from each one to allow for full off-carriageway circulation of the crossroads. This will have the effect of reducing the depth of the turning boxes and making the junctions perpendicular, and more tightly angled. This is intended to increase visibility of traffic movements and slow traffic that is turning into or out of one of the Mill Lane junctions; this is principally intended to support the safety of cyclists that are cycling on-carriageway mixed with traffic by increasing their sightlines, increasing vehicle drivers' visibility of them, and reducing traffic speed. The traffic lights for each arm could also be provided with cycle-only filter lights, and an algorithm to provide cyclists with a 5+ second head start ahead of vehicles. Provision should be made for pedestrians and other cyclists (i.e. those who are not wishing to cycle on-carriageway mixed with traffic) to fully circulate the crossroads using the new shared paths and parallel crossings.
F76	Chaucer Road	This improvement recommendation is focused on the junctions between Chaucer Road, Garrison Lane 'south' and Orwell Road. Chaucer Road is a relatively quiet residential street and a valuable connection between Orwell Road, Garrison Lane 'south' and Mill	Junction with Chaucer Road to be reworked to include a cycle-only filter light from Garrison Lane into Chaucer Road, and to permit cyclists to cycle north-bound on it (contraflow). It should be designed for a safe transition from both Garrison Lane and Orwell

Map Reference	Location	Description	Recommendation
		Lane. Chaucer Road can help to bring cyclists that have been heading north on Garrison Lane 'south' or that have come across from Orwell Road to head north off Garrison Lane 'south' sooner or avoid it altogether. Chaucer Road is currently 'no entry' at the southern end, though this is understood to apply to vehicles only. This is considered a high priority route.	Road. Ideally a new toucan crossing over Garrison Lane should be placed here.
F77	Undercliff Road West	To provide a safer east-to-west transition across the roundabout (to avoid on-carriageway circulation of it completely) between Undercliff Road West and the cut through to Coronation Drive a short stretch of shared path and priority crossings are recommended. This is considered a medium priority route.	A short stretch of shared path and priority crossing points should ideally be added over the northern and eastern arms of this roundabout, connecting to the footpath (to be upgraded and improved to a shared path) for Coronation Drive (F68) and to the commencement of the Langer Road scheme (F78).
F78	Langer Road	<p>Langer Road is a key commuter corridor, continuing journeys from the main routes through Felixstowe, e.g. Garrison Lane, Orwell Road, Crescent Road, Grange Road, Undercliff Road West etc., towards the Port of Felixstowe, the main seaside resort area around Sea Road, and other strategic employment areas (e.g. Haven Exchange, Carr Road). It also helps to serve journeys to the current leisure centre, also on the sea front, off Sea Road and Undercliff Road West. Langer Road itself serves commercial, industrial and residential uses as well as key social infrastructure such as Langer Primary Academy, a church, a community centre, Langer Park and a holiday park to the south. The presence of a school along this route creates a strong incentive for a shared path to be created, as a more appropriate infrastructure typology for families cycling with small children; widened and resurfaced pedestrian infrastructure will also help to support social journeys for school children and their families travelling via the shared path on foot. Shared paths also have the added benefit over single-lane cycle lane of being bi-directional, though they typically necessitate slower cycling speeds (or even occasional dismounting) to negotiate pedestrians during busy times.</p> <p>Strava Metro data suggests that Langer Road is often selected for routes towards the Port from various points of origin within the Trimleys and Felixstowe regardless of whether it's the most direct route to take, and this is likely down to the straightness, width, continuity and lack of obstructions (Langer Road has yellow lines along most of its length, preventing on-street parking on it) meaning cyclists can maintain higher speeds and have fewer stops and starts;</p>	It is recommended that a shared path extend from the western end of Undercliff Road West and along the eastern side of Langer Road to just south of the junction with Russell Road; raised plinth cycle/pedestrian priority crossings are recommended to be fitted over each side road junction along this route. The traffic island on Undercliff Road West's arm of the roundabout is likely to need to be removed to accommodate infrastructure improvements in this location. A signalised crossing should be provided over Langer Road at the shared path's termination point to allow cyclists and pedestrians to then transfer over to the west side of Langer Road.

Map Reference	Location	Description	Recommendation
		<p>this is particularly relevant to commuter cyclists that want to keep their commute duration as short and convenient as possible. Langer Road is relatively wide throughout its length with some central hatching that could be absorbed as space for cycling and walking infrastructure improvements, however this available space is not sufficient to concurrently accommodate cycle lanes in both directions (more suited to commuter cyclists and leisure/fitness cycling) and a shared path (more suited for families, and slower paced leisure/day-to-day cycling). Langer Road is also used by buses, large vehicles and may occasionally be used by HGVs (where not using the A14) and therefore must accommodate sufficient space for them; options for the infrastructure scheme must make best use of the space to meet the different user needs for this important route. At the northern end, there is limited space available on the Garrison Lane 'south'/Langer Road roundabout's circulation area, and insufficient space on the south west side for a short roundabout bypass between the two or the (to be improved) footpath for Coronation Drive (F68). It is for this reason that it is recommended that there be a consolidation of the route into a single shared path around the eastern/south-eastern side of the roundabout until Langer Road widens at Russell Road. This is considered a very high priority route.</p>	
F79	Langer Road (East side, Port bound)	<p>Langer Road is a key commuter corridor, continuing journeys from the main routes through Felixstowe, e.g. Garrison Lane, Orwell Road, Crescent Road, Grange Road, Undercliff Road West etc., towards the Port of Felixstowe, the main seaside resort area around Sea Road, and other strategic employment areas (e.g. Haven Exchange, Carr Road). It also helps to serve journeys to the current leisure centre, also on the sea front, off Sea Road and Undercliff Road West. Langer Road itself serves commercial, industrial and residential uses as well as key social infrastructure such as Langer Primary Academy, a church, a community centre, Langer Park and a holiday park to the south. The presence of a school along this route creates a strong incentive for a shared path to be created, as a more appropriate infrastructure typology for families cycling with small children; widened and resurfaced pedestrian infrastructure will also help to support social journeys for school children and their families travelling via the shared path on foot. Shared paths also have the added benefit over single-lane cycle lane of being bi-directional,</p>	<p>Recommended Langer Road options, in descending order of preference:</p> <p>Option A: A shared path is created on the west side, from opposite Russell Road to opposite Manor Road, using improved crossings over the Beach Station Road crossroads (see F82). In addition to this, a single-direction (Port-bound) cycle lane is to be provided on the east side. Existing pavements on the east side may need to be reduced to minimum accessibility requirements (1.5m) to accommodate this, teamed with more frequent opportunities to safely cross Langer Road, particularly for Langer Park Academy.</p> <p>Option B: A shared path is created on the west side only, from opposite Russell Road to opposite Manor Road, using improved crossings over the Beach Station Road crossroads (see F82). More frequent opportunities to safely cross Langer Road, particularly for Langer Park Academy, are also provided. This is the first fallback</p>

Map Reference	Location	Description	Recommendation
		<p>though they typically necessitate slower cycling speeds (or even occasional dismounting) to negotiate pedestrians during busy times.</p> <p>Strava Metro data suggests that Langer Road is often selected for routes towards the Port from various points of origin within the Trimleys and Felixstowe regardless of whether it's the most direct route to take, and this is likely down to the straightness, width, continuity and lack of obstructions (Langer Road has yellow lines along most of its length, preventing on-street parking on it) meaning cyclists can maintain higher speeds and have fewer stops and starts; this is particularly relevant to commuter cyclists that want to keep their commute duration as short and convenient as possible. Langer Road is relatively wide throughout its length with some central hatching that could be absorbed as space for cycling and walking infrastructure improvements, however this available space is not sufficient to concurrently accommodate cycle lanes in both directions (more suited to commuter cyclists and leisure/fitness cycling) and a shared path (more suited for families, and slower paced leisure/day-to-day cycling). Langer Road is also used by buses, large vehicles and may occasionally be used by HGVs (where not using the A14) and therefore must accommodate sufficient space for them; options for the infrastructure scheme must make best use of the space to meet the different user needs for this important route. At the northern end, there is limited space available on the Garrison Lane 'south'/Langer Road roundabout's circulation area, and insufficient space on the south west side for a short roundabout bypass between the two or the (to be improved) footpath for Coronation Drive (F68). It is for this reason that it is recommended that there be a consolidation of the route into a single shared path around the eastern/south-eastern side of the roundabout until Langer Road widens at Russell Road, and as such the recommendation under reference F78 reflects this. This is considered a very high priority route.</p>	<p>option recommended if space is considered to be too constrained for Option A.</p> <p>Option C: Single direction cycle lanes to be provided on both sides of Langer Road from Russell Road to the Beach Station Road crossroads. A parallel crossing is provided for Langer Park Academy. This is the second fallback option if space is considered to be too constrained for Option A and both-way cycle lanes are considered to be more valuable to the community than a shared path, Option B.</p>
F80	Langer Road (West side, centre bound)	Langer Road is a key commuter corridor, continuing journeys from the main routes through Felixstowe, e.g. Garrison Lane, Orwell Road, Crescent Road, Grange Road, Undercliff Road West etc., towards the Port of Felixstowe, the main seaside resort area around Sea Road, and other strategic employment areas (e.g. Haven Exchange, Carr	<p>Recommended Langer Road options, in descending order of preference:</p> <p>Option A: A shared path is created on the west side, from opposite Russell Road to opposite Manor Road, using improved crossings over</p>

Map Reference	Location	Description	Recommendation
		<p>Road). It also helps to serve journeys to the current leisure centre, also on the sea front, off Sea Road and Undercliff Road West. Langer Road itself serves commercial, industrial and residential uses as well as key social infrastructure such as Langer Primary Academy, a church, a community centre, Langer Park and a holiday park to the south. The presence of a school along this route creates a strong incentive for a shared path to be created, as a more appropriate infrastructure typology for families cycling with small children; widened and resurfaced pedestrian infrastructure will also help to support social journeys for school children and their families travelling via the shared path on foot. Shared paths also have the added benefit over single-lane cycle lane of being bi-directional, though they typically necessitate slower cycling speeds (or even occasional dismounting) to negotiate pedestrians during busy times.</p> <p>Strava Metro data suggests that Langer Road is often selected for routes towards the Port from various points of origin within the Trimleys and Felixstowe regardless of whether it's the most direct route to take, and this is likely down to the straightness, width, continuity and lack of obstructions (Langer Road has yellow lines along most of its length, preventing on-street parking on it) meaning cyclists can maintain higher speeds and have fewer stops and starts; this is particularly relevant to commuter cyclists that want to keep their commute duration as short and convenient as possible. Langer Road is relatively wide throughout its length with some central hatching that could be absorbed as space for cycling and walking infrastructure improvements, however this available space is not sufficient to concurrently accommodate cycle lanes in both directions (more suited to commuter cyclists and leisure/fitness cycling) and a shared path (more suited for families, and slower paced leisure/day-to-day cycling). Langer Road is also used by buses, large vehicles and may occasionally be used by HGVs (where not using the A14) and therefore must accommodate sufficient space for them; options for the infrastructure scheme must make best use of the space to meet the different user needs for this important route. At the northern end, there is limited space available on the Garrison Lane 'south'/Langer Road roundabout's circulation area, and insufficient space on the south west side for a short roundabout bypass between</p>	<p>the Beach Station Road crossroads (see F82). In addition to this, a single-direction (Port-bound) cycle lane is recommended on the east side. Existing pavements on the east side may need to be reduced to minimum accessibility requirements (1.5m) to accommodate this, teamed with more frequent opportunities to safely cross Langer Road, particularly for Langer Park Academy.</p> <p>Option B: A shared path is created on the west side only, from opposite Russell Road to opposite Manor Road, using improved crossings over the Beach Station Road crossroads (see F82). More frequent opportunities to safely cross Langer Road, particularly for Langer Park Academy, are also provided. This is the first fallback option recommended if space is considered to be too constrained for Option A.</p> <p>Option C: Single direction cycle lanes to be provided on both sides of Langer Road from Russell Road to the Beach Station Road crossroads. A parallel crossing is provided for Langer Park Academy. This is the second fallback option if space is considered to be too constrained for Option A and both-way cycle lanes are considered to be more valuable to the community than a shared path, Option B.</p> <p>Carr Road should include a shared path along one side and a parallel crossing over to Manor Road.</p>

Map Reference	Location	Description	Recommendation
		the two or the (to be improved) footpath for Coronation Drive (F68). It is for this reason that it is recommended that there be a consolidation of the route into a single shared path around the eastern/south-eastern side of the roundabout until Langer Road widens at Russell Road, and as such the recommendation under reference F78 reflects this. This is considered a very high priority route.	
F81	Langer Park	Langer Park is an area of green space that lies between Langer Road and the railway line, and offers public toilets a trim trail for adult outdoor gym use , and a play area. It has narrow footpaths running north-to-south through it, and has several entry and exit points, providing a high level of permeability from the eastern side. Langer Park also helps to provide a stretch of vehicle-free route to/from Langer Primary Academy and the supermarket to the north. Cycling is currently expressly prohibited within Langer Park and as such there currently is no cycle parking provision. There are some barriers to entry and exit, particularly across the access point opposite Buregate Road. There is no direct step-free access from the park to the supermarket to the north, meaning some cyclists/pedestrians with reduced mobility would have to travel along Holland Road and Langer Road to access the hardstanding area opposite the bus stop in order to access the supermarket or to access the shared path that leads to Coronation Drive. A route through Langer Park is intended to be used as a leisure walking and cycling route and a route for families with children; it is not intended to meet the needs of commuter cyclists, of whom are intended to be better catered for via recommendations F78, F79 and F80 directly on Langer Road. This is considered a medium priority route.	The footpaths within the park are to be upgraded and widened to allow cycling through Langer Park; this is intended to be used as a leisure cycling cut through rather than a (fast) commuter cycling route and the design scheme should be led by this. Entry/exit barriers are recommended to be removed where they block access. Cycle parking provision (and seating) is recommended to be added around the trim trail and play area to allow trim trail users and parents and children to securely park their bikes and spend more time in the park. Cycle parking is also to be provided next to the public toilets. A ramp is recommended to be installed for direct access from the northern end of Langer Park up to the supermarket site to provide step-free access to this important retail provision and to allow for use of the site to access the existing shared path over to Coronation Drive or Garrison Lane/Undercliff Road West (see F77).
F82	Langer Road Junction	This recommended improvement covers the Langer Road/Beach Station Road crossroads. This is an important vehicular route for connecting to the Port where the A14 has not been used, and a key cycling and walking route, particularly from central and eastern Felixstowe. This is considered a very high priority route.	The existing signalised pedestrian (pelican) crossings over the Langer Road/Beach Station Road crossroads are to be upgraded to signalised cyclist and pedestrian crossings (toucan). It is recommended that the transition between the F80 (Langer Road, west side) and F83 (Beach Station Road 'west', north side) shared paths is well designed to ensure that what could otherwise be a dangerous tight turn and blind corner for cyclists/pedestrians travelling quickly on either section is made as safe as possible; this may require reworking the western arm of the crossroads to provide more pedestrian and cyclist space, whilst still maintaining adequate

Map Reference	Location	Description	Recommendation
			<p>carriageway space for larger vehicles to turn left out of Beach Station Road 'west' onto Langer Road 'north'. To support on-carriageway cyclists making right turns at the crossroads (which puts them in a vulnerable position), it is recommended that cycle-only filter lights are added to each arm of the crossroads to give cyclists a 5+ seconds head start over vehicular traffic.</p> <p>The feasibility and value of operation of the cycle-only filter lights at all times versus just at peak travel times should be assessed and options explored.</p>
F83	Beach Station Road ('west')	Beach Station Road 'west' is a wide, relatively busy 'A' road (A154) that runs parallel to the Port and nearby employment and retail uses, running between the Beach Station Road/Langer Road crossroads to the east and the Port's Dock Gate 1 roundabout to the west. There is an existing shared path along its northern edge, but its width varies and the overall quality and legibility (particularly as there is no signage) is low; this path connects to routes around the Dock Gate 1 roundabout for access to the more western end of the Port operations, other employment uses, and over to Haven Exchange and western Felixstowe via the eastern arm and Peewit Hill/Grange Road. This is considered a very high priority route.	<p>East of the railway line crossing, a new section of shared path is to be created from the existing pavement between the Beach Station Road crossroads (see F82)/end of Langer Road and the railway line crossing. The existing pavement is already quite wide, though relocation of the on-street car parking spaces that are provided here should be considered if more space is needed.</p> <p>West of the railway line crossing, the existing shared path that runs along the northern side of Walton Avenue 'south' should be widened, resurfaced and the junctions with adjoining businesses reworked to improve visibility of cyclists and pedestrians traversing vehicular accesses. Signage should be added to make clear that this is a shared path. A signalised pedestrian and cyclist crossing is recommended over Walton Avenue to facilitate a safe transition over to the existing cycle/pedestrian track that runs around the south side of the Dock Gate 1 roundabout. The existing island refuge crossing point over Haven Exchange is recommended to be replaced with an improved crossing moved further east. The upgrade and improvement of the connection into Glemsford Close should also include dropped kerbs.</p>
F84	Beach Station Road ('west')/Walton Avenue	Beach Station Road 'west' is a wide, relatively busy 'A' road (A154) that runs parallel to the Port and via its associated employment and retail uses, running between the Beach Station Road/Langer Road crossroads to the east and the Port's Dock Gate 1 roundabout to the west. There is an existing cycle/pedestrian track that runs around over the southern and south-western arm of the Dock Gate 1 roundabout to connect with Walton Avenue. The existing shared path which is of acceptable quality but could be further improved.	A signalised crossing is to be provided over Walton Avenue where the existing south-side pedestrian/cycle track meets it, over to the north-side shared path. The existing south-side cycle/pedestrian track is to be improved with resurfacing.

Map Reference	Location	Description	Recommendation
		Similarly, there is an existing crossing over Beach Station Road that could be further improved to parallel crossing quality. This is considered a very high priority route.	
F86	Walton Avenue	Walton Avenue is a busy 'A' road (A154) that runs through the Port and via its associated commercial/industrial uses, running between the Dock Gate 1 and Dock Gate 2 roundabouts. There is an existing shared path around the southern arm of the Dock Gate 2 roundabout for transference over to an existing shared path on Fagbury Road, but no infrastructure for transfer over to Ferry Lane, which is potentially an important connection into western Felixstowe (see F42). This is considered a very high priority route.	New segregated bi-directional cycle lane on the south side of Walton Avenue, made from absorbing the grass verges (and moving the street lights). Dimensions will be tight, and the junctions over entrances into the path will need to remain as wide as they are now due to HGV use.
F87	Undercliff Road West (Spa Pavilion Theatre/Promenade) (cycle parking)	This recommendation covers the area immediately around the Spa Pavilion Theatre and this section of the Promenade, where there is currently no cycle parking, despite being a major entertainment venue in Felixstowe, and central to a key tourism and leisure area. Here there are landscaped gardens (Hamilton Gardens, Spa Gardens, Felixstowe Sea Front Gardens), the sea front Promenade walk, public toilets, beach huts, and various restaurants and bars. There is no known cycle parking available along this stretch of Undercliff Road West and the Promenade, with the nearest being the leisure centre. This is considered a medium priority route.	New cycle parking provision to be installed around the Spa Pavilion Theatre and on the Promenade at this location. Innovative designs that are secure, space efficient and that help to boost the place-identity (i.e. the Felixstowe brand) of this key tourist and leisure area are encouraged .
F89	Sea Road (cycle parking)	Sea Road is a key sea-side entertainment and tourism location, rich with bars, cafes, restaurants, arcades, side show attractions, independent shops and services, a market space (at Mannings) hotels, etc, as well as running parallel to the sea front and Promenade. There are also public toilets, sheltered areas, seating, beach huts (on the Promenade) the Felixstowe & Suffolk Bowling Club and various other social infrastructure of value at this location. The nearby site allocation SCLP12.6 Land at Sea Road has received permission for a mixed-use development consisting of housing above ground floor commercial uses. There is currently a significant lack of suitable cycle parking along Sea Road, which is particularly acute in the high activity area around the site. This is a medium priority route.	New cycle parking provision to be installed on Sea Road at this location. Removal/re-location of the wall section around the shelter opposite Mannings to accommodate cycle parking should be considered. Innovative designs that are secure, space efficient and that help to boost the place-identity (i.e. the Felixstowe brand) of this key tourist and leisure area are encouraged. The quantum should be relevant to the location and availability (now and anticipated in the future) of cycle parking in this area.
F98	The Triangle	The Triangle is a small pedestrianised park with seating and formal flower beds and trees in tree pits. It is located to the north of the main Hamilton Road town centre area. Immediately to the east is a small stretch of one-way street (also Hamilton Road) with disabled parking, a taxi rank, and a number of shops/commercial units. The	The recommended options for reworking the Triangle to create more pedestrianised/cycle-friendly space and more high quality open space are as follows:

Map Reference	Location	Description	Recommendation
		Triangle and these units are somewhat disconnected from the main town centre area as the pedestrian route space is bisected by Cobbold Road, which is a deterrent to the flow of pedestrian movement up and down the full length of Hamilton Road (as is also the case with Orwell Road and the section of Hamilton Road to the south of it). The current shared space design of the Cobbold Road/Hamilton Road crossroads is confusing as it mimics a zebra crossing but is not intended to be a formal crossing - it is intended to be a negotiation space where pedestrians have priority. This is considered a medium priority route.	<p>Option A: Extend the Triangle southwards, absorbing the short section of Cobbold Road located between Crescent Road and Hamilton Road. This can be achieved by extending the pedestrianised and landscaped area southwards, creating a larger park space and better connecting the Triangle with the main Hamilton Road shopping area. This is intended to have the effect of increasing pedestrian safety, legibility, continuity, footfall to the retail units on the northern section, and overall quality of place. More cycle parking should be included in this extension. The crossroads between Crescent Road and Cobbold Road would need to be adapted accordingly. Vehicles heading east on Cobbold Road that would have previously continued straight over can be re-directed via Gainsborough Road/St Andrews Road; Constable Road can then be used to re-access Cobbold Road, if needed.</p> <p>Option B: Extend the Triangle eastwards to absorb the short section of Hamilton Road that runs between York Road and Cobbold Road. Relocate the disabled parking and taxi tank – possibly on Cobbold Road or at the Crescent Road car park. This would allow Cobbold Road to remain a through-route and still provide an increase in green open space provision and cycle parking, but would not provide the continuous pedestrianisation benefits of Option A.</p>
F100	Crescent Road	Crescent Road is an arterial east-to-west road through Felixstowe town centre. Towards the centre it is quite wide with excess hatched mid-carriageway space, though it tapers as it heads west to Mill Lane. This is considered a very high priority route.	<p>Segregated cycle lanes are recommended for installation in both directions along Crescent Road and Mill Lane (east). It may be appropriate for Mill Lane to include sections of scheme ‘interruption’ for marked bay on-street parking (similar to F1) where provision is deemed necessary. Crescent Road has excess pavement in some sections that can be utilised to create more space for the cycle lanes. The absorption of the central hatching and the right turn turning box for access to Highfield Road should be considered. The traffic flow impact of the right turning box removal to be fully assessed at design stage.</p> <p>Consideration should be given to redevelop Crescent Road car park as an underground/multi-storey car park so that on-street parking pressure in this area can be reduced and streets made safer and more attractive. The traffic lights at the crossroads should be fitted</p>

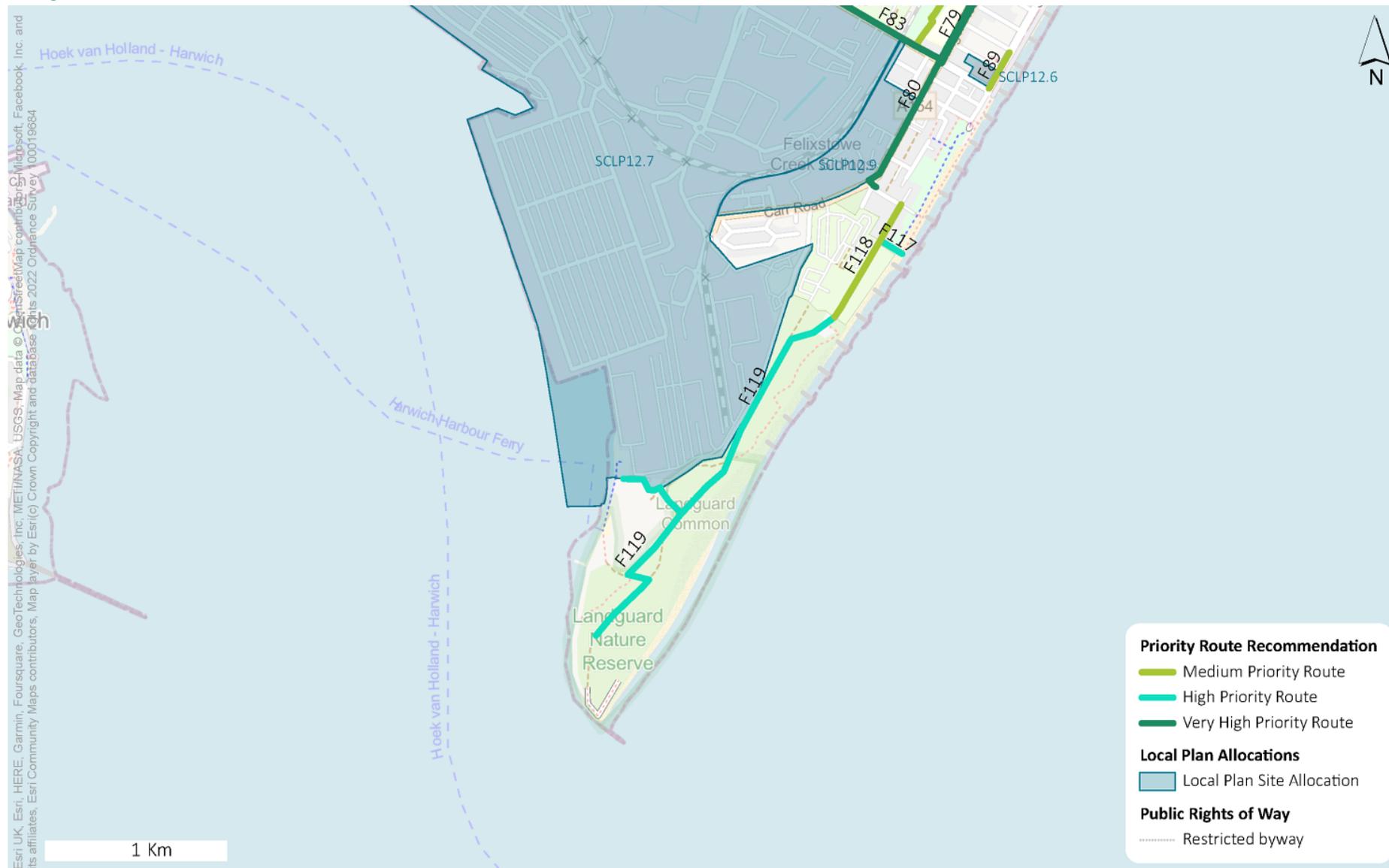
Map Reference	Location	Description	Recommendation
			with cycle-only filter lights to allow on-carriageway cyclists to have 5+ seconds head start over vehicles.
F109	Golf Road (cycle parking)	Golf Road is close to the sea front and has a public car park and toilet facilities, and a pedestrian connection down to the sea wall. It currently has no cycle parking facilities. This is considered a medium priority route.	Cycle parking stands and lockers to be installed in Golf Road car park.
F112	Undercliff Road East (cycle parking)	The area west of Cobbolds Point is a popular spot for sea swimming, and is also the inception/end point of the main Promenade stretch, which some leisure cyclists/walkers may be attracted to cycling/walking the full length of. It is therefore likely to be a useful location to provide some cycle parking. This is considered a medium priority route.	Cycle parking to be installed at the eastern end of the Promenade, close to Cobbolds Point.
F116	Gap in the sea wall (east of Martello Lane)	This route covers the current 'gap' in the Promenade, which has the effect of precluding the opportunity for people to cycle or walk continuously between Felixstowe Ferry and the Landguard Nature Reserve/Landguard Point via the sea front, which is an attractive prospect for many local people (as well as tourists) for leisure/fitness/recreation purposes. This route is part of the Strategy for Felixstowe's wider ambition to create a safe and convenient high quality cycling and walking route between Landguard Point and Felixstowe Ferry via the sea front that is as vehicle-free as possible (F119, F118, F117, the Promenade, F116 and F115). This is considered a medium priority route.	The gap in hardscape coastal defences and sea wall that occurs in this location is to be closed by installing a new connecting section. It is critical that this new section cycling functions as both sea defence infrastructure as well as cycling and walking infrastructure.
F121	Maidstone Road	Maidstone Road (north and south sections) is an important destination within the Walton area due to it being the only means of accessing the Maidstone Infant & Causton Junior School. Maidstone Road is a narrow residential street with car parking on both sides, making it hazardous for pedestrians and cyclists, particularly during peak school drop off and pick up times. Maidstone Road also handles vehicular traffic from the Grange Farm area, though Grange Farm is also well served by the A14, Mill Lane and Seaton Road, the latter of which provides alternative access to Walton High Street. The recent Maidstone Infant & Causton Junior School merger will likely be putting even more vehicle pressure on the north section of Maidstone Road than before, which is the more constrained section	Maidstone Road's junction with the High Street is to be significantly narrowed to slow vehicle entry and exit into and out of Maidstone Road, and give pedestrians and cyclists less carriageway distance to cross over when walking along the High Street. Install a cyclist and pedestrian priority crossing across this junction.

Map Reference	Location	Description	Recommendation
		<p>of the two. Without removing on-street parking on at least one side, cycling and walking infrastructure directly along Maidstone Road cannot be improved.</p> <p>The junction between High Road and Maidstone Road (north section) is very large, and may be encouraging fast entry into Maidstone Road, which creates a more dangerous cycling and walking environment. Restructuring this junction to be tighter (to slow entry) are likely to be the only direct improvement options for Maidstone Road's north section. This is considered a medium priority route.</p>	
F123	Maidstone Road roundabout	Maidstone Road (north and south sections) is an important destination within the Walton area due to Maidstone Infant & Causton Junior School being located there. Seaton Road provides a connection to Maidstone Road from further east of the High Road (High Road West), and from various side roads between the High Street and Cornwall Road, respectively. Maidstone Road also handles vehicular traffic from Grange Road/the Grange Farm area, though Grange Farm is also well served by the A14 and Mill Lane. The roundabout between Maidstone Road and Seaton Road is a mini roundabout with excess space around it and no formal crossings over any of the arms. This is considered a medium priority route.	Roundabout to be redesigned to make it safer to cross each of the arms, and to make the space more attractive and safe for walking and cycling.
F124	Seaton Road	Seaton Road provides a connection between Walton High Street/High Road West, the various adjoining roads, and Maidstone Road - from which the Maidstone Infant & Causton Junior School are accessed. Seaton Road is a residential road with car parking currently on both sides. Seaton Road is relatively wide throughout its middle section but tapers and becomes narrower at either end. This is considered a high priority route.	If following assessment it is found feasible that on-street parking can be removed from the north side, create a shared path to run along the north side of Seaton Road.
F134	Mill Lane (Railway bridge)	This short section of Mill Lane significantly narrows as it passes over the railway bridge, where it becomes one way for traffic, with east-bound traffic given priority. As the railway bridge is quite long, it is likely that cyclists (cycling on-carriageway) are not well seen by traffic at the other end, and therefore may not be given priority to cycle over the bridge when it is their turn; this creates potential for accidents that could be avoided with adequate segregation. There is currently a footway on both sides of the carriageway over the bridge, which could be consolidated onto one side to create a shared path over the bridge section. This is considered a very high priority route.	The two sections of footway are consolidated onto one side (i.e. one stretch removed and that space reallocated to a widened remaining footway) and constructed to a shared path standard. This new shared path should be well connected to the cycling and walking infrastructure on Mill Lane to ensure safe onward travel to the east and west.

Map Reference	Location	Description	Recommendation
F135	Mill Lane (East)	Mill Lane is an important east to west connection between west Felixstowe's most important commuter routes (Grange Road/F62; F65, Grange Farm Avenue/F59 and Ferry Lane/F42 or F43 over the A14 footbridge) and the Hamilton Road area, Felixstowe's town centre. Mill Lane also provides connectivity to Grange Community Primary School. This section has advisory cycle lanes and adequately wide pavements, and most properties appear to have off-street parking provision, meaning a high-quality scheme is likely to be deliverable and cause little disruption through reduced on-street parking space provision. This is considered a very high priority route.	Shared path to be installed between the Mill Lane railway bridge and crossroads with Garrison Lane.
F136	SCLP12.5 Brackenburg Sports Centre	A route through the Brackenburg Sports Centre already exists, though there are barriers at the southern end. Improvements to this route are expected to come forward through the redevelopment of this site for housing. This is considered a high priority route.	The existing cycle/pedestrian track to be improved/re-located as appropriate in the redevelopment of this site to ensure a north-to-south cycling and walking connection is maintained between High Road East and High Row Field. The existing cycle barriers to be removed.
F138	The Promenade (Cycle parking)	The Promenade provides a vehicle-free sea front and seaside resort cycling and walking experience for residents and tourists. It also provides vehicle-free connectivity between Old Felixstowe (north), the Felixstowe Leisure Centre (east) and the Landguard Nature Reserve (south). Its width and surface quality varies considerably throughout its length, however at this point it is very wide and there is plenty of capacity for cycle parking. It is intended that the green space here is to be redeveloped to include bouldering walls, chess/draught tables, table tennis and a petanque area, therefore potentially increasing demand for cycle parking in this area. This is considered a medium priority.	New cycle parking provision to be installed on the Promenade at this location. Innovative designs that help to boost the place-identity (i.e. the Felixstowe brand) of this key tourist and leisure area are encouraged over more standard Sheffield stand designs. The quantum should be relevant to the location and availability (now and anticipated in the future) of cycle parking in this area.
F139	Great Eastern Square	Great Eastern Square provides a through route from Hamilton Road to Felixstowe railway station via a gate to the north of the site which is left open most of the time people are likely to travel, though it provides a relatively narrow space and could be made more clearly a cut through point for pedestrians and cyclists. Removal of the gate would both ensure there is around the clock access to the railway station and increase accessibility by providing a wider space. There is also currently a significant amount of bollarding around the frontage of the square and particularly the entrance to the development site just to the north, which is the only point of entry for cyclists and provides the only drop kerb access for cyclists. This is considered a medium priority.	The gate to the north of the Great Eastern Square shopping centre that provides access to Felixstowe railway station is to be removed; appropriate modal filtering to ensure vehicles do not use this opening is recommended providing the accessibility gains of removing the gate for cyclists and pedestrians is not lost in doing so. All, or the majority, of the bollards around the entrance to the development site are to be removed., Through signage and surfacing, the new permanent access availability communicated to cyclists and pedestrians on Hamilton Road to ensure they are aware of the option to cut through at this point (rather than needing to continue up to High Road West) to access the railway station.

Map Reference	Location	Description	Recommendation
			Increased cycle parking capacity is recommended more generally for Great Eastern Square and the railway station.
F140	Longcroft to Maidstone Road	<p>Maidstone Road (north and south sections) is an important destination within the Walton area due to it being the only current means of accessing Maidstone Infant & Causton Junior School. Maidstone Road is a narrow residential street with car parking on both sides, making it hazardous for pedestrians and cyclists, particularly during peak school drop off and pick up times. Maidstone Road also handles vehicular traffic from the Grange Farm area, though Grange Farm is also well served by the A14, Mill Lane and Seaton Road, the latter of which provides alternative access to Walton High Street.</p> <p>The recent Maidstone Infant & Causton Junior School merger will likely be putting even more vehicle pressure on the north section of Maidstone Road than before, which is the more constrained section of the two. Without removing on-street parking on at least one side, cycling and walking infrastructure directly along Maidstone Road cannot be improved.</p> <p>Alternative access to the school site may be possible via Longcroft and the western boundary of the site (if there is sufficient space to do so and the school is supportive). However this would likely only meet the needs of families that travel to the school via High Street from the west (such as those that will be travelling from the residential development currently under construction on land north of Walton High Street or the Walton Gate development), which is likely to represent a small amount of the school's full catchment, though this would need to be confirmed at the delivery stage. The junction at the northern end of Maidstone Road is very large, and may be encouraging fast entry into Maidstone Road, which creates a more hostile cycling and walking environment. Restructuring this junction to be tighter (to slow entry) and other traffic calming measures are likely to be the only direct improvement options. This is considered a medium priority route.</p>	<p>If there is sufficient space to do so and the school is supportive, it is recommended that a new cycling and walking track is created that runs between the end of Longcroft to the school site via its western border and down to the car park/entrance area.</p>

Landguard Nature Reserve



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Map Reference	Location	Description	Recommendation
F117	Manor Terrace and Promenade	This route seeks to connect Manor Terrace and the Promenade via the stretch of hardstanding which is currently of unknown land ownership but would ideally become an adopted highway. This route forms part of the wider ambition to connect Landguard point and Felixstowe Ferry. This route is part of the Strategy for Felixstowe’s wider ambition to create a safe and convenient high quality cycling and walking route between Landguard Point and Felixstowe Ferry via the sea front that is as vehicle-free as possible (F119, F118, F117, the Promenade, F116 and F115). This is considered a high priority route.	This area of hardstanding is in poor condition and needs complete resurfacing and adoption as a highway.
F118	Manor Terrace	This route follows the adopted highway Manor Terrace through the Suffolk Sands Holiday Park site to better connect Carr Road/Langer Road to the Landguard Nature Reserve, Harwich ferry and Felixstowe Museum. This route is part of the Strategy for Felixstowe’s wider ambition to create a safe and convenient high quality cycling and walking route between Landguard Point and Felixstowe Ferry via the sea front that is as vehicle-free as possible (F119, F118, F117, the Promenade, F116 and F115). This is considered a medium priority route.	Manor Terrace is to have a shared path created along one side to support journeys either to/from the Promenade or to/from Carr Road to/from the Landguard Nature Reserve, Harwich ferry or Felixstowe Museum.
F119	Landguard Nature Reserve / Landguard Point	This route commences at Manor Terrace car park and crosses through Landguard Nature Reserve, terminating close to the pillbox (where the current footpath terminates). As Landguard Point is likely to be the destination or rest point for many leisure cycling and walking journeys to/in Felixstowe, there is likely to be a need for a small amount of cycle parking here to allow cyclists to dismount, secure their bikes and explore Landguard Nature Reserve on foot; more seating is also likely to be beneficial. This route is part of the Strategy for Felixstowe’s wider ambition to create a safe and convenient high quality cycling and walking route between Landguard Point and Felixstowe Ferry via the sea front that is as vehicle-free as possible (F119, F118, F117, the Promenade, F116 and F115). This is considered a high priority route.	The existing cycle/pedestrian track that runs between Manor Terrace car park and Landguard Point should be resurfaced and the surfacing extended to create a continuous route to the current termination point near the pillbox. A small amount of cycle parking is to be installed close to the termination point of the track to allow cyclists to safely secure their bikes and explore the nature reserve/Landguard Point on foot. Additional seating is also required to support Landguard Point as a leisure journey destination/rest point.

Lowestoft Internal Key Corridor



4.25 The Lowestoft Key Corridor seeks to provide a cohesive cycling and walking network, which maximises off-road routes and delivers improvements of the highest quality. The route encompasses the parishes of Oulton, Oulton Broad, Carlton Colville, Gisleham and Lowestoft. Parts of this Key Corridor fall within the Broads Authority area.

- Improvements around Bascule Bridge by providing a segregated cycle path. The bridge is a key pinch point, busy with traffic and requires dismounting.
- A coherent route along the east side of the town along the coast. Comments ranged from improvements to the promenade, a new cliff path in Pakefield and included improvement along the National Cycle Route.
- Improvements needed along the A47. This is a key arterial route that contains some disconnected sections of cycling infrastructure.
- Improvements to the High Street. A number of comments related to improving the High Street particularly the exit to the north of the High Street.
- Crossing Bridge Road and gaining access to Carlton Marshes and Nicholas Everitt Park was raised as a concern.
- Denmark Road is a strategically important road that connects to the train station and town centre with potentially valuable infrastructure being installed with the third river crossing to its west. Improvements to this road was requested.

4.26 Within this Key Corridor the Propensity to Cycle Tool (PCT) map shows the strong travel patterns along the east coast which convene upon Bascule Bridge. It is worth noting however that PCT does not calculate modal shift potential for off-road routes such as Public Rights of Way (PROW). As Strava

Metro compiles data for both on and off-road routes, and shows activity regardless of current PROW status, it provides evidence as to which routes are popular now, and may be more popular/have potential to create modal shift if provided with proper infrastructure for safety, comfort and legibility. There is correlation between the two data sets (PCT and Strava Metro) with high levels of cycling identified along Tom Crisp Way, the seafront, and the A47. There are also clear 'pinch points' at both existing bridges which will hopefully be partially relieved by the forthcoming third river (Gull Wing) crossing.

4.27 The key opportunities that have been identified in support of this Key Corridor include:

- The planned development in the Waveney Local Plan,
- Primary and secondary schools,
- PowerPark (including PowerPark Vision⁵⁷), the Port and the associated employment opportunities,
- Town Centre,
- Transport hubs such as Lowestoft, Oulton Broad North and Oulton Broad South Train Station,
- Normanston Park,
- Retail Parks,
- Leisure and visitor destinations including South Beach,
- Town Centre Masterplan⁵⁸,
- Lowestoft Heritage Action Zones⁵⁹,

⁵⁷ <https://www.eastsuffolk.gov.uk/assets/Business/Regeneration-projects/PowerPark/Powerpark-vision.pdf>

⁵⁸ <https://www.eastsuffolk.gov.uk/business/regeneration-projects/lbcm/>

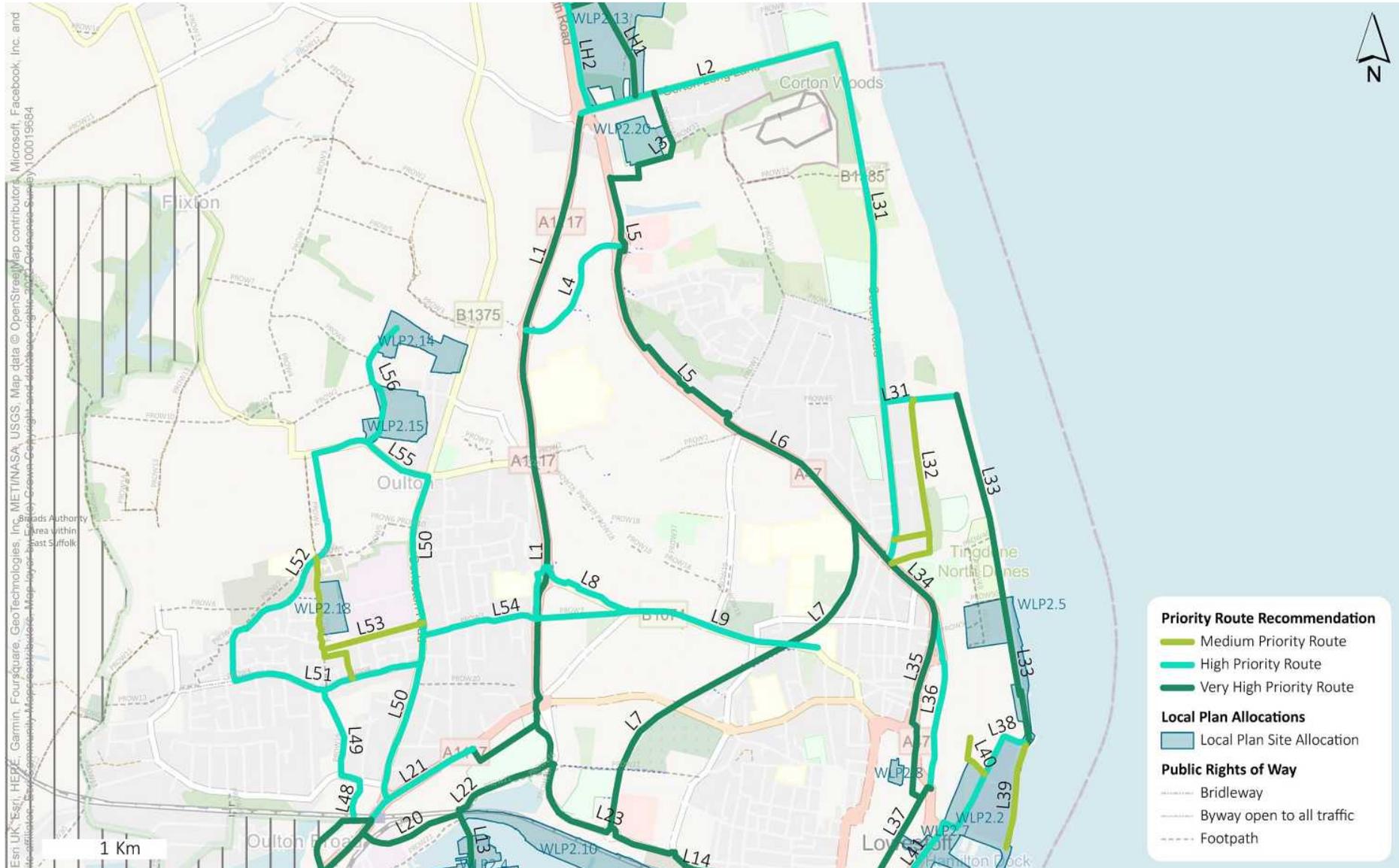
Sustainable Urban Neighbourhood and Kirkley Waterfront development brief⁶⁰, and

The Gull Wing bridge over Lake Lothing (formerly the Third Crossing).

⁵⁹ <https://thinklowestoft.co.uk/regeneration/celebrating-our-culture-and-heritage/>

⁶⁰ <https://www.eastsuffolk.gov.uk/assets/Planning/Waveney-Local-Plan/Supplementary-Planning-Documents/SUN-and-Kirkley-Waterfront/Adopted-Development-Brief.pdf>

North Lowestoft Arterial Route

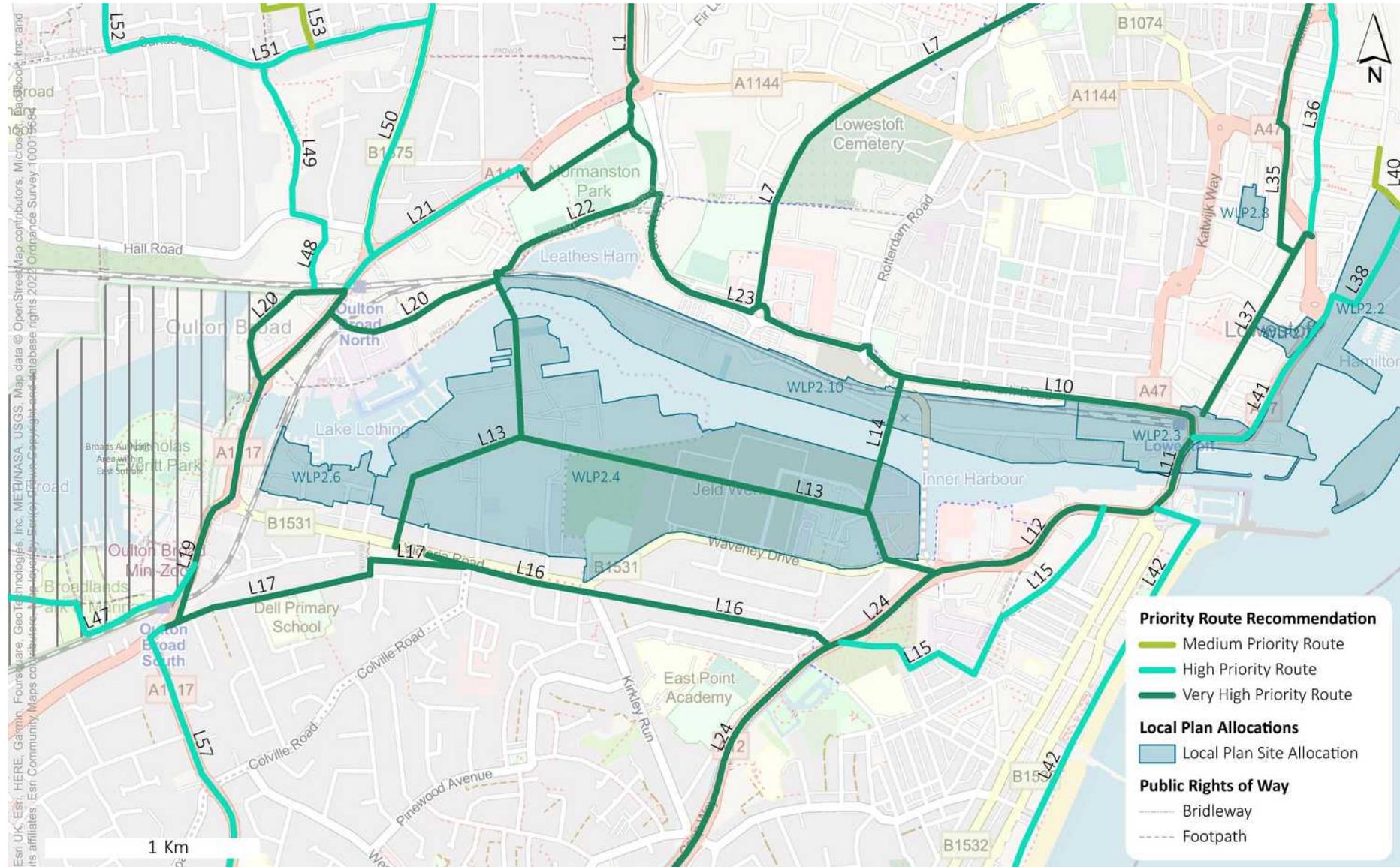


Map Reference	Location	Description	Recommendation
L1	Millennium Way / Peto Way	This section runs centrally to the town and is generally of good quality. It contains a wide shared path that runs adjacent to the public highway. The pathway is direct and well maintained. This is considered a very high priority route.	It lacks the segregation of pedestrian and cyclists (i.e. internal segregation) which could be provided to reduce overall conflict between users. The junction with Grasmere Drive and Somerleyton Road is particularly wide and further junction improvements to prioritise cyclists and walkers could be considered.
L2	Corton Long Lane	This road is a key connection for the Gunton Park (rugby club site) and North of Lowestoft Garden Village site allocations (WLP.20 and WLP2.13). The road contains limited cycle infrastructure to the west and an on-road route to the east. The road itself is narrower to the west but widens eastwards allowing for these on-road options. There is a footpath along its length, however in places this footpath is narrow which can be an obstacle to some users. This is considered a high priority route.	Traffic calming measures could still be added to improve its cycling and walking potential by reducing traffic pressures. In addition, an improved footpath should be considered across the length of the road. Connections from the Garden Village over Corton Long Lane into Old Lane will also be a key consideration. Connections through Old Lane can be encouraged with a good crossing as close to the cycling access from the Garden Village as possible. With wide grass verges to most sides the broader Corton Long Lane/A47 roundabout could be redesigned to improve cycling around its perimeter.
L3	Old Lane/Gunton Park (Lowestoft and Yarmouth Rugby Club site)	The infrastructure on the A47 between Corton Long Lane roundabout south to Gunton Avenue is fragmented. Accordingly, cyclists are directed through Old Lane and Gunton Park then through Gunton Avenue which forms part of the National Cycle Network (NCN). The route through Gunton Park contains a good quality shared path. Whilst Old Lane and Gunton Avenue are quiet cul-de-sacs, they lack specific cycling infrastructure. This is considered a very high priority route.	The Gunton Park allocation (WLP2.20) should retain the existing cycle route and improve where possible with improved access onto the A47. A crossing point connecting the North of Lowestoft Garden Village to the opposite side of Corton Long Lane would aid connectivity of both allocations. The alternative is to complete the cycle infrastructure along the A47 connecting the Corton Long Lane roundabout through to Gunton Avenue. This will allow the current NCR route to be bypassed. There are verges on the western side of the road which could be utilised to expand the existing pavement creating a shared path and the central hatching could be removed. However, the existing cycle infrastructure is on the east side of the road meaning cyclists would have to cross over the road twice. To create the coherent infrastructure on the east side of the road, extensive redevelopment of the road will be required to move it westwards or the purchase of land adjacent so accordingly the preference is to retain the cycle route through Old Lane.
L4	Bentley Drive	This is part of a newer residential estate and connects the A47 and Millennium Way infrastructure together. The existing infrastructure is to a good standard, formed of a shared path with a drainage gully to create segregation. This is considered a high priority route.	At this stage no recommendations are being made here, but this will be assessed in accordance with most recent guidance upon any future review/update of the Strategy.
L5	A47 (to Gunton Church Lane)	The section south of Gunton Avenue up to the access into Gainsborough Drive contains a shared path of good quality. However, the cycle route then takes cyclists south-eastwards towards Gainsborough Drive. Whilst	A continuation of the cycle path along the A47 until it connects to the next formal cycling infrastructure, which commences near Gunton St Peter's Avenue, would be the optimum improvement. The road at Foxburrow Hill is

Map Reference	Location	Description	Recommendation
		Gainsborough Drive is relatively quiet, it lacks formal cycle infrastructure and is indirect, meaning commuters may choose to remain on the A47; cycling on the A47 means cycling on-carriageway on a busy arterial route. This is considered a very high priority route.	constrained by houses to one side and areas of good biodiversity to the other making improvements more difficult to deliver here. A continued shared path along the road should be considered first and foremost as an ancient woodland is present adjacent this recommendation and no work should be undertaken which damages this important biodiversity asset. Beyond Foxburrow Hill there are service roads running parallel to the main road. These quiet roads could provide good opportunities for cyclists to be provided with appropriate wayfinding signage and access improvements. The two service roads are disconnected by a short section. To ensure a cohesive route these should be connected either by utilising the verge or providing a new connection behind the tree line. This may require adoption of a permissive path. An improved crossing point across Gunton Church Lane should be considered which will remain clear of parked vehicles along with a small section of shared path to provide a connection onto Yarmouth Road.
L6	A47 Outside Ormiston Denes Academy	There are shared paths along the majority of this route, however the sections where cycling is and isn't allowed is currently unclear. This section contains relatively wide pavements on both sides of the road so a continuous and clearly signed route is a priority. Where the path crosses other roads (such as Gunton St Peter's Avenue) priority could be given to cyclists. This is considered a very high priority route.	Preferably a shared path to be provided on both sides of the road, but could be utilised on one side only should infrastructure on both sides be unviable. As a minimum, better advisory signage should be considered with some form of segregation between cyclists and pedestrians along the existing shared path sections. Coloured surfaces would not represent an optimum solution, but could also be effective. Cycle priority crossing can also be considered.
L7	Former Railway Line	This section is a high quality off-road, green and attractive route central to Lowestoft. This is considered a very high priority route.	Some connections from the surrounding residential streets onto the 'old railway' could be improved. For example, the barriers off Marham Road are disruptive and could be removed or re-designed to only prohibit vehicle access.
L8	Oulton Road	Propensity to Cycle Tool and Strava data on activity on this route suggest east-west connections across Lowestoft require improvement and Oulton Road offers a good opportunity to achieve this. This is considered a high priority route.	The first section of Oulton Road contains limited pavements that are both narrow and exist only on one-side. However quiet cul-de-sac's runs parallel to the main road. Fastolf Close and Chatsworth Close are connected by pavements that could be widened and upgraded to also be used by cyclists. Around the entrance of Elmore Gardens there is sufficient space for an off-road shared path. Alternatively, the quiet road of Woods Loke East can be used. It is unlikely significant improvements are possible, but it is a quiet road with direct connections over the A1117.
L9	Oulton Road- Church Lane to St Margaret's Academy	Oulton Road offers a good opportunity to increase east to west connectivity. To maximise the benefits and provide modal shift potential the recommended shared pathway on Oulton Road should extend at least as far eastwards as the school. The roads become more constrained	There is sufficient space to widen the existing pavement to create a new shared path to continue recommendation L8.

Map Reference	Location	Description	Recommendation
		further eastwards and it is considered unlikely to be viable extend the path past the school. This is considered a high priority route.	

Lake Lothing Loop



Map Reference	Location	Description	Recommendation
L10	Denmark Road	Parts of Denmark Road have good cycling infrastructure in the form of a shared path, albeit without internal segregation between cyclists and pedestrians. However, the quality dramatically reduces from opposite Hervey Street to the train station. Here the shared path crosses the road to become a narrow dedicated cycle path with at least one significant obstacle. This road is key to connecting the town centre/train station. Furthermore, it will directly interact with the third river crossing/Gull Wing Crossing once built (opening summer 2023). The crossing will provide a unique opportunity for improvements, but some of the potential for creating modal shift could be lost if the entire path eastwards to the town centre is not improved. This is considered a very high priority route.	Widen the dedicated cycle path and remove the obstacles, most notably the large concrete block likely to be for utility purposes. Ensure that the good infrastructure outside the train station and proposed with the third river crossing is optimised by a high-quality path on Denmark Road connecting the two. There are sufficient grass verges here to widen the path significantly.
L11	Station Square/Bascule Bridge	This road, which heads southwards from the town centre across the bridge, is a well-used cycle route with both Propensity to Cycle Tool and Strava showing very high levels of use. This is expected given that currently Lowestoft has just two bridges across the river and the close proximity of the town centre, railway station and seafront. However, this part of the route is poor in quality with a traffic dominated landscape and narrow shared facilities for pedestrians and cyclists. The bridge represents a significant pinch point. The Council-led Lowestoft Town Centre Masterplan-Station Quarter is looking at improvements to this area and the construction of the third river crossing may change the overall level of traffic. This is considered a very high priority route.	To address the poor-quality pinch point that the Bascule Bridge represents, one of its three lanes could be removed to create a specific cycle lane. This would then indirectly improve the pedestrian infrastructure by removing the potential for conflict between cyclists and pedestrians. The third river crossing makes this potential improvement much more viable and the level of improvements here will be subject to the level of traffic reduction potentially created by the new crossing. The suggested remodelling of the bridge is dependent upon sufficient reduction in traffic levels from the third river crossing and is not being recommended if these reduced traffic levels do not materialise. As with all recommendations, further community engagement and detailed design work must be undertaken if the recommendation is taken forward to delivery.
L12	Pier Terrace/Belvedere Road	The infrastructure along Belvedere Road is generally good quality. It is composed of a shared surface on both sides of the road and Propensity to Cycle Tool data suggests that both sides are well used. This section of the route does have two notable weaknesses to be addressed through the Strategy: (1) The first section of poor quality is on the immediate southern exit to Bascule Bridge. Cyclists are currently directed to a cul-de-sac behind Pier Terrace. This is poorly marked and lacks any designated cycle infrastructure. Alternatively, they can continue forward to Pier Terrace, however it appears to be an on-road option only. The pathway is relatively narrow, on a curve in the road with shops directly opening onto the pavement means it could not be upgraded to a shared path without significant improvements. (2) The second issue is outside the Asda superstore. The existing path on the north of Belvedere Road stops outside the Asda store, likely due to the width and the access into the store itself. Cyclists are instead directed around	One option is to improve the cul-de-sac behind Pier Terrace and provide better wayfinding signage. Any improvement could be linked to any future development of South Quay where a waterfront path all the way to the Asda superstore could be considered. Short term minor improvements behind Pier Terrace should also be considered. Another option is to continue the changing distribution of the road space to cyclists after the third river crossing is constructed. If Bascule Bridge is made into two lanes only then this reallocation of road space for cycling infrastructure could be continued southwards to Belvedere Road. This will allow the extension of any proposed cycle path along the bridge which can then join into the existing shared path on Belvedere Road. As a short-term

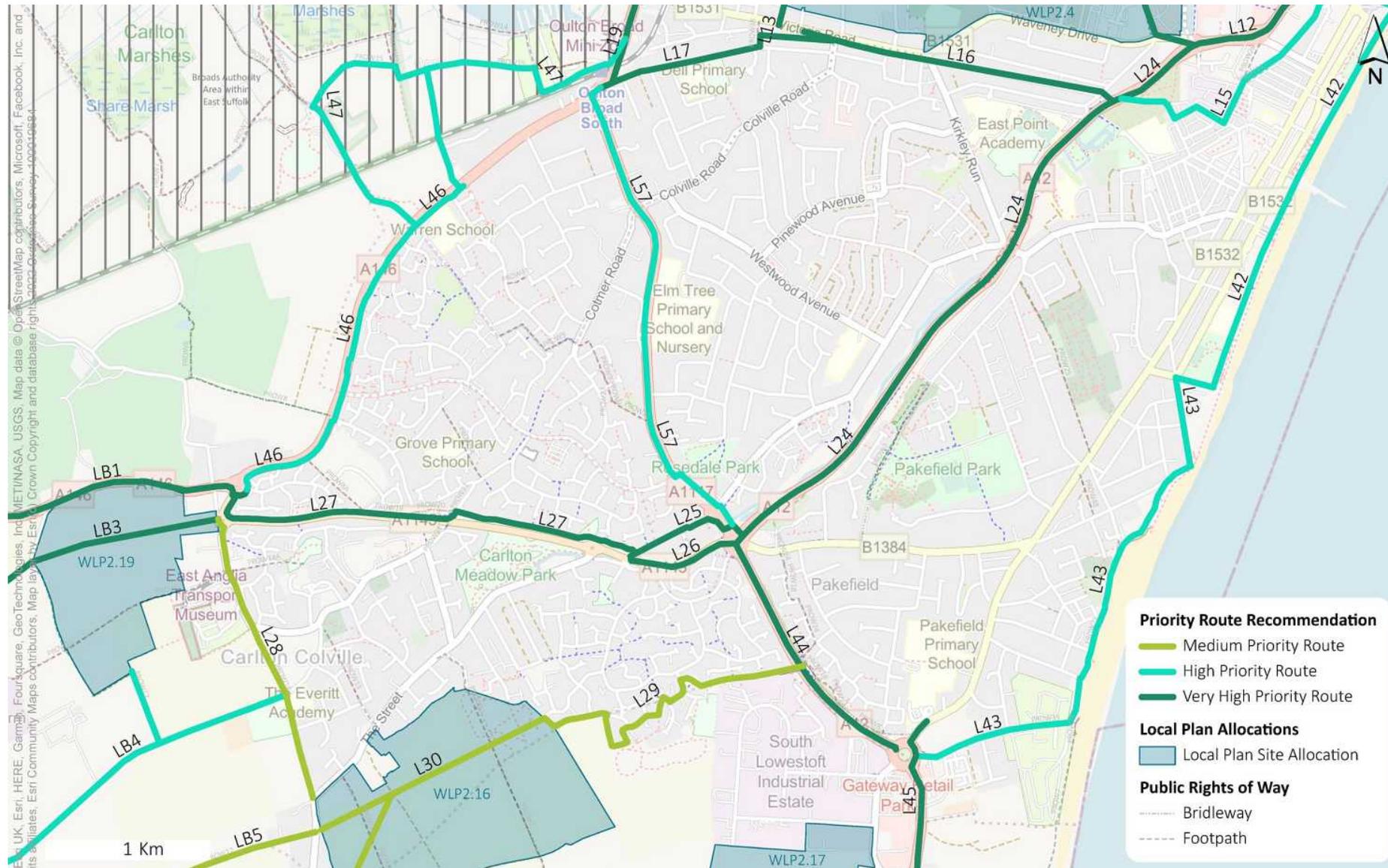
Map Reference	Location	Description	Recommendation
		the store along the waterfront. This area is an attractive destination but indirect and poorly signposted. This is considered a very high priority route.	option, better wayfinding signage should be provided to Belvedere Road to avoid confusion. Whilst the grass verge means the path could be widened to continue the shared surface, the busy access to the superstore would remain an issue and any traffic lights or walking/cycling priority would likely create traffic jams on the roundabout itself. Long term, any development of the South Quay could connect into this pathway creating a cohesive and attractive waterfront route. Neither option is mutually exclusive.
L13	Kirkley Waterfront and Sustainable Urban Neighbourhood (WLP2.4)	The site is in a key location south of Lake Lothing with the ability to provide a continuous east-west route. This is considered a very high priority route.	In accordance with the Local Plan allocation and detailed in the Sustainable Urban Neighbourhood and Kirkley Waterfront Development Brief, a cohesive and high-quality shared path through the site should be provided that creates an attractive, high-quality east west route between Oulton Broad and the town centre. Furthermore it should connect to the proposed cycle/pedestrian bridge which is a policy requirement of policy WLP2.4.
L14	Third River Crossing ('Gull Wing' crossing)	The third river crossing will deliver high quality cycling infrastructure and it is important that this is optimised. This is considered a very high priority route.	At this stage no recommendations are being made here, but this will be assessed in accordance with most recent guidance upon any future review/update of the Strategy.
L15	Kirkley Rise	The former railway line to the rear of St John's Road and Kirkley Rise is a good quality alternative to Belvedere Road. It is already largely available to cyclists and is an off-road and attractive route. Ideally the path would be extended further along the former railway line, but this has been partially developed over by the Business Park. Instead the cyclist currently need to use the roads around Salisbury Road and Bruce Street to reach Tom Crisp Way. Eventually these reconnect to the cycle network and former railway at Kirkley Ham. This is considered a high priority route.	Better wayfinding across most of this section which also directs cyclists on the optimum route once they reach Salisbury Road. If the business park is ever redeveloped the route along the old railway should be extended. Designs should utilise plans shown in the Lowestoft High Street and Seafront HAZ Masterplan.
L16	Tom Crisp Way to path behind Kimberley Road	There is a light controlled crossing point that exits opposite the entrance to the Kimberley Road cycle route. The route south of Kimberley Road represents reasonable quality cycle infrastructure with a narrow road that services garages and the rear of the properties and a well-maintained cycle track that is separated from vehicular traffic. This route is less attractive for pedestrians as it does not appear to have a separate footpath. However, the road is likely to be quiet and alternative dedicated footpaths exist on Kimberley Road. This is considered a very high priority route.	Improved wayfinding signage should be considered.
L17	Victoria Road/Dell Road	Between Kirkley Run and Colville Road the cycle path is of a good quality though contains no physical segregation between users bar the coloured surfacing. This	Whilst the optimum route would be Victoria Road, it is narrow with houses on both sides making new infrastructure unlikely to

Map Reference	Location	Description	Recommendation
		<p>path is separated from the road by a grass verge that declines in width westwards. Following the crossing of the Colville Road arm of the mini roundabout, the quality of the east-west path deteriorates as the cyclist/pedestrian travels further westward on Victoria Road. This is because the shared path continues along Victoria Road, but then suddenly ceases opposite Nelson’s Wharf and forces the cyclist onto the road in a challenging manner. Victoria Road is a busy road particularly during peak commuting times and is relatively narrow. The alternative is to utilise Dell Road, whether via the access servicing residential garages north of Dell Road or utilising Colville Road and then joining Dell Road. Dell Road is a better westward option. It is a quieter road and in sections has good pavement widths and grass verges. However, it lacks any formal cycling infrastructure and will be busier during the school travel times due to the location of Dell Primary School. Suffolk County Council undertook a consultation on improvements to Dell Road/Kirkley Run in 2022. This is considered a very high priority route.</p>	<p>be deliverable. As a key vehicle commuter route, use of any modal filter types are also unlikely to be feasible options. Instead, improvements should be aimed at Dell Road. First, the access road immediately north of Dell Road (running to the rear of the Dell Road houses) and the adjoining footpath between Victoria Road/Dell Road should be upgraded for cycling and improved to as high-quality infrastructure as possible. It is likely that this route has been utilised as a permissive route anyway, but formal designation, surfacing and wayfinding would mean that cyclists could remain off-road for longer. The service road joins a footpath which runs north to Victoria Road and south to Dell Road. Formal infrastructure along Dell Road should be the ambition, particularly from the footpath towards Oulton Broad South train station in the west. This can be achieved by utilising the wide grass verges. A less expensive and easier way to achieve this option would be modal filters to reduce traffic, making on-road cycling easier. Using the railway arch under Bridge Road would allow cyclists to directly enter Oulton Broad South train station without needing to cross a busy road (Bridge Road), however it is currently not accessible. A further alternative option should other options be unavailable would be a cohesive cycle route through the Western End of Lake Lothing allocation (WLP2.6). This would require a master-planned approach but would offer the most direct and potentially high-quality possibility. However, it would still require entrance onto Victoria Road.</p>
L19	Bridge Road/Saltwater Way	<p>Oulton Broad South train station is accessed via Bridge Road, which lies to the west of Dell Road. Currently, there is a footbridge at the end of Dell Road onto Bridge Road which represents a break in the cohesive network and requires cyclists to dismount. The narrow width of the bridge makes shared use unlikely to be viable. The alternative option is to cycle along Bridge Road though this represents a pinch point and a significant hazard over the bridge itself as it is both narrow, busy with the railway crossings creating further disruption, and lacks any verge or footpath either side to use in an emergency. Whilst not ideal, the safest option available currently is for cyclists to dismount at the end of Dell Road and use the footbridge to avoid conflict with pedestrians. This is considered a very high priority route.</p>	<p>A long-term solution would be to widen the footbridge to continue the shared path.</p> <p>Once over the footbridge there are shared paths along Bridge Road and Saltwater Way to the Bridge Road/Commodore Road roundabout. This infrastructure is of good quality and is well maintained. There is an area of vacant land to the east of Saltwater Way which previously had permission which included a wide access onto Saltwater Way – if this land comes forward for development in the future, cycle/walking priority across the access should be considered. A crossing point over Bridge Road to allow better access to Oulton Broad South train station should be considered. This could be close to the junction at Marsh Road to</p>

Map Reference	Location	Description	Recommendation
L20	Bridge Road/Commodore Road/Harbour Road	The cycle infrastructure in this area is currently disjointed, which prevents the establishment of a cohesive route around Lake Lothing. North of the Bridge Road/Commodore Road roundabout the cycling infrastructure ceases, and there is limited cycling and walking infrastructure until Normanston Park/Normanston Drive. Two options present themselves; either (a) staying on the entirety of Bridge Road or Commodore Road before joining Normanston Drive and then through to Normanston Park, or, (b) to turn off at Harbour Road and crossing over the railway line to join into Normanston Park. This is considered a very high priority route.	<p>provide better connections into Carlton Marshes (see L47) and Nicholas Everitt Park. A full Toucan crossing is recommended.</p> <p>Option A – To avoid a large proportion of the busy Bridge Road, cyclists could instead be directed to Harbour Road. At the eastern end of Harbour Road is a footbridge that obtained permission (reference: DC/19/2796/RG3) for a replacement pedestrian/cycle bridge with improvements to the landing areas. This route avoids the busier roads and connects straight onto existing infrastructure to the north. In addition, it connects well with the WLP2.4 Kirkley Waterfront and Sustainable Urban Neighbourhood site’s proposed cycle/pedestrian bridge. However, Harbour Road is relatively narrow and narrows further as you travel eastwards. The road lacks a formal turning head at its end suitable for taller vehicles, which could cause cyclists disruption and potentially compromise their safety if larger vehicles need to turn around at this point. There is limited pedestrian infrastructure on Harbour Road and whilst it is likely to be quiet, it does service industrial uses so the narrow roads can experience heavy goods vehicles. Lighting is a further key consideration.</p> <p>Option B – Bridge Road is a busy pinch point with no current cycling infrastructure. It provides access to both of Oulton Broad’s train stations (Oulton Broad North and Oulton Broad South), forms the main section of the Oulton Broad retail/leisure/tourism area, and features several bus stops for key Lowestoft routes. Improvements are limited given the restricted width of Bridge Road and the constraints to further widening of it. Some of the pavement could be designated for cycle use, but it would be unlikely to meet the current required standard (LTN 1/20) and would be inconsistent throughout its length. The constraints on the road also include a railway crossing where the road once again narrows.</p> <p>Bypassing Bridge Road by directing cyclists towards Commodore Road should be considered as an alternative. Making Commodore Road one-way and improving its wayfinding signage should be considered. Commodore Road can be directly accessed via existing cycling infrastructure along Freshwater Way; Commodore Road exits onto Bridge Road just prior to the railway crossing. It is unlikely that significant improvements could be made to this</p>

Map Reference	Location	Description	Recommendation
			section due to the constraints of the railway line and the crossing infrastructure. Instead, consideration should be given to improving the rail underpass between Commodore Road and Holly Road before heading eastwards on Normanston Drive or northwards along Gorleston Road.
L21	Normanston Drive	Normanston Drive is relatively wide with a central hatched area. Once the third river crossing has been constructed there is potential for lower traffic numbers heading towards Mutford Lock Bridge. Use of this surplus road space/ hatching area for a shared path should be considered. This is considered a high priority route.	A shared path either side should be aimed for, however lower quality options may need to be explored if sufficient width cannot be secured. More generally, if the road becomes quieter it could be considered for a down grade in highways status from an A-road to a B-road.
L22	Normanston Park	The park provides an attractive off-road option with formal infrastructure in the form of a shared path along its southern edge. Furthermore, there are routes through the park north to south from Normanston Drive. Whilst the infrastructure is in good condition the shared paths can produce conflict with pedestrians particularly when dark as there is no lighting within the park. This route connects to the Peto Way shared path south of the signalled crossing. This is considered a very high priority route.	The pathways through the park to be widened and lit. The lighting should be of a good quality and could provide a form of segregation between pedestrians and cyclists.
L23	Peto Way	This section is generally of good quality with an existing shared path and elements of segregation, though there is scope for further improvement. This is considered a very high priority route.	Further improvements can be delivered through the creation of shared paths on both sides of the road. The existing path is of a good quality but the surfacing could be improved.

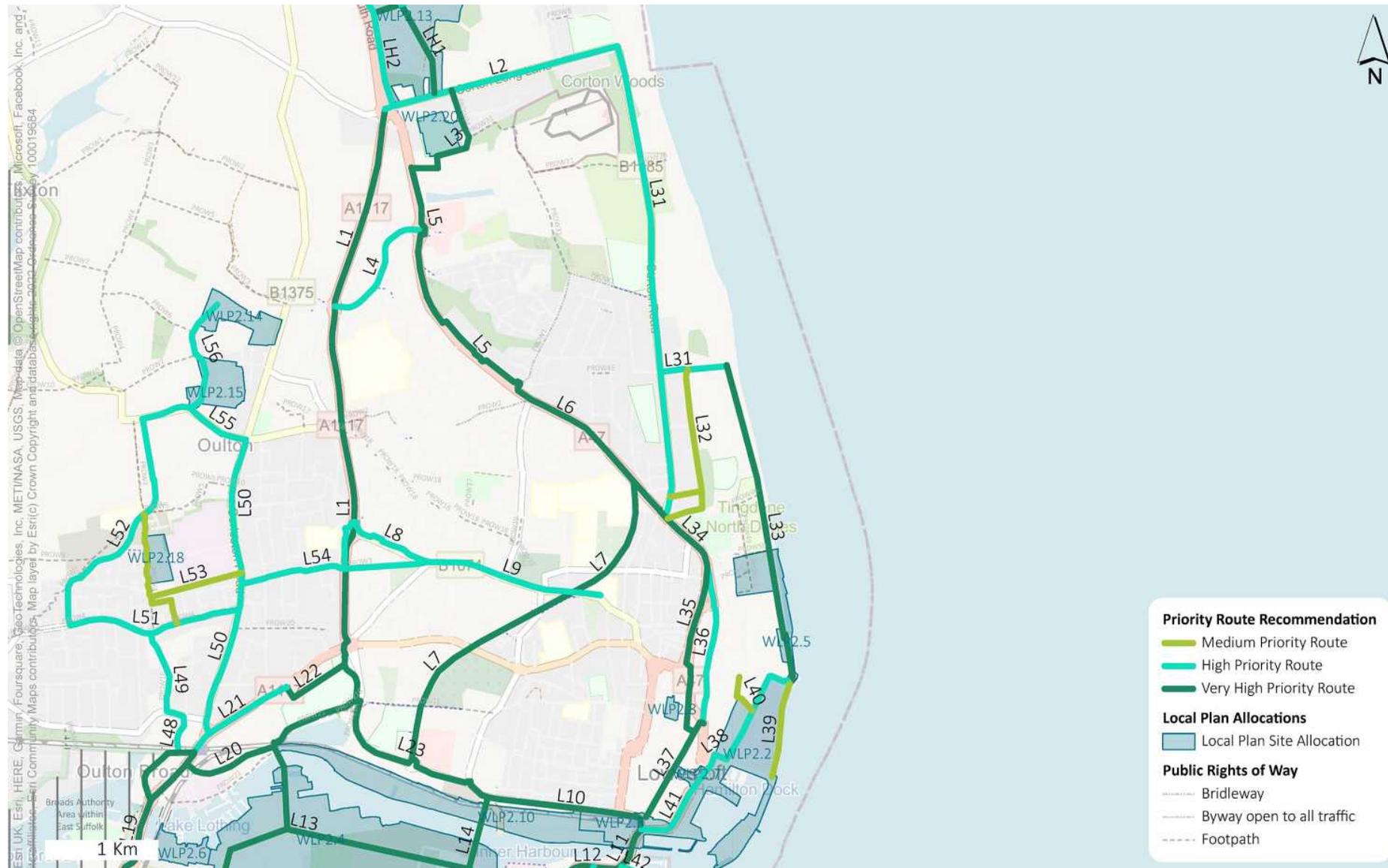
South Lowestoft Arterial Route



Map Reference	Location	Description	Recommendation
L24	Tom Crisp Way	This route acts as a spinal route through South Lowestoft. It is formed of well maintained, shared paths that, although lacking segregation, are of a relatively high quality. Consideration of signage (re)positioning should be given to avoid obstacles and facilitate future upgrades to add ‘harder’ segregation. This is considered a very high priority route.	To bring the path up to full LTN 1/20 compliance segregation between cyclists and pedestrians could be considered. Explore further crossing points to ensure optimum safety for students using the adjacent schools. An ancient/veteran tree is adjacent this route and no improvements should damage this important biodiversity asset.
L25	Long Road (west of Elm Tree Road)	This section represents a break in the continuous cycle path from Tom Crisp Way to Castleton Avenue. As the cycle path does not continue to Castleton Avenue it requires a short diversion north-west along Bloodmoor Road, crossing over Bloodmoor Road, and then along Long Road to continue southwards. The section of Long Road to the west of the Elm Tree Road/Bloodmoor Road roundabout contains reasonable pedestrian footpaths but requires on-road cycling. This part of Long Road is not a through-road, and although it has a relatively high number of properties, it is unlikely to be significantly busy with vehicular traffic. The road is wide with parking bays set into the side meaning there is limited scope for the on-road parking to pose a safety risk to cyclists (i.e. from opening doors, swinging out, etc). This is considered a very high priority route.	Remove the grass verge from the north-west side of the road to create a high-quality shared path. This would help create a cohesive route using a type of infrastructure similar to Tom Crisp Way and Castleton Avenue. If this proves unviable then traffic filters could be applied with removal of parking considered along one side.
L26	Castleton Avenue	Continuing the infrastructure along the A1145 would be the most comprehensive and direct approach and there appears to be sufficient grass verges for continuation of the shared path. This is considered a high priority route but could be considered very high priority route should the constraints be overcome.	Continue the shared path to connect to Castleton Avenue. In many ways the continuation of the high-quality infrastructure would be the preferable approach and if sufficient funding is available should be considered first. However, the constraints created by the adjacent land levels and SUDS would likely require a greater level of engineering.
L27	Castleton Avenue	This route is a key east-west connection through the south of the greater Lowestoft area. The infrastructure is already of a high standard, being formed of an internally segregated cycle/pedestrian track that is coherent and well maintained. It is elevated and landscaped along the section that runs between Long Road to Uplands Close, and therefore has a high degree of segregation from Castleton Avenue’s vehicular traffic. From Uplands Close/Uplands Road South the track separates to allow the cyclist/pedestrian to either continue on via Hollow Lane (which heads northwards) or continue on westwards via a track that runs adjacent to Castleton Avenue itself. The latter track provides the most direct route out towards Beccles. This is considered a very high priority route.	The track is already of a good quality, however consideration should be given to the provision of a suitable crossing point into the Oakes Farm allocation (WLP2.19). Oakes Farm could be a key part of the key corridor towards Beccles and, as a sports and leisure use site allocation, cycling and walking to and from the site is of enhanced value and relevance.
L28	Chapel Road/Church Lane	Travel between Lowestoft and Beccles is via the busy Barnby Bends (Beccles Road, A146) route or the indirect Mutfordwood Lane approach. Whilst the Barnby Bends has no cycle or walking infrastructure the connections on to it from Castleton Avenue are good. The current National Cycle Network (route 517) is through Mutfordwood Lane using St Peters Road and Church Lane,	Much of St Peters Road and Church Lane contain wide grass verges that may be suitable for improved cycle infrastructure. However, there are pinch points around the church that may render a cohesive path unviable. Instead consideration could be given to an upgrade of PROW 14 to a bridleway(with appropriate improvements to the exits and to the path’s width) with further

Map Reference	Location	Description	Recommendation
		neither of which have formal cycle infrastructure with the exception of the latter part of Church Lane. This is considered a medium priority route.	consideration on whether improvements to or alongside Hall Road are achievable.
L29	Dorley Dale, Gratton Dale and Thixendale	From Cranesbill Road to Dorley Drive over Bloodmoor Road is an overpass that leads to a series of shared paths that are connected by way of quiet residential streets, through to Gratton Dale and then Thixendale. This then leads to a shared path towards Ullswater. The shared paths are largely through attractive green spaces using un-segregated shared paths. The residential streets are shared spaces, but are likely to be relatively slow moving. This is considered a medium priority route.	Significant improvements are unlikely to be viable but wayfinding should be provided to ensure ease of passage. An improvement to the overpass could be explored to allow access onto Bloodmoor Road although it is recognised this will be relatively expensive.
L30	Land South of the Street (WLP12.16)	There is a large, strategic allocation (WLP2.16) south of The Street which this route will connect into. The allocation requires cycle connection to Ullswater and Gisleham Road. Please note that the line is indicative only and shows the need for east/west connections and its role in the wider key corridor. This is considered a medium priority route.	LTN 1/20 compliant cycling infrastructure through the site to connect the east and west. This will connect the future residents of the site and will allow improved access to Carlton Colville Primary School for existing residents. With suitable crossing point over Gisleham Road this could then connect through to Bridleway 12 opposite to continue the key corridor westwards towards Beccles.
L57	Cotmer Road/Elm Tree Road	Elm Tree Road contains a short stretch of off-road cycling infrastructure of reasonable quality from the junction with Tom Crisp Way to Rosedale Park. The cycle route then heads into Rosedale Park indirectly returning back to Elm Tree Road further along the road. However, the road (and associated pavement) are relatively wide meaning potential infrastructure improvements are possible. Whilst the road narrows at the approach to Cotmer Road roundabout it contains a wide grass verge with a pavement segregated from the road. This connects to off-road cycle infrastructure allowing passage around the roundabout. The remaining infrastructure along Cotmer Road is sporadic on-road infrastructure. This is considered a high priority route.	Create a new cycle path through Rosedale Park along its south-west boundary parallel to the road making the route more direct. Utilising road space widen the pavement to allow a shared path with cycle/pedestrian segregation around to Elm Tree Road West. The pavement heading northwards can be widened using the grass verge to continue the off-road infrastructure. As this exists on the opposite side of the road to Rosedale Park a suitable crossing point will be required. The on-road infrastructure on Cotmer Road should be utilised to create an off-road shared surface with appropriate segregation. It is recognised where the road narrows either side of the Conrad Road junction a lower quality infrastructure may have to be utilised, but it is important that cyclists do not have to enter/exit the road. The junction with Beccles Road to the far north will require improvements to allow cyclists to head safely right (connecting to recommendation L17) or left.

Town Centre-Coastal Route



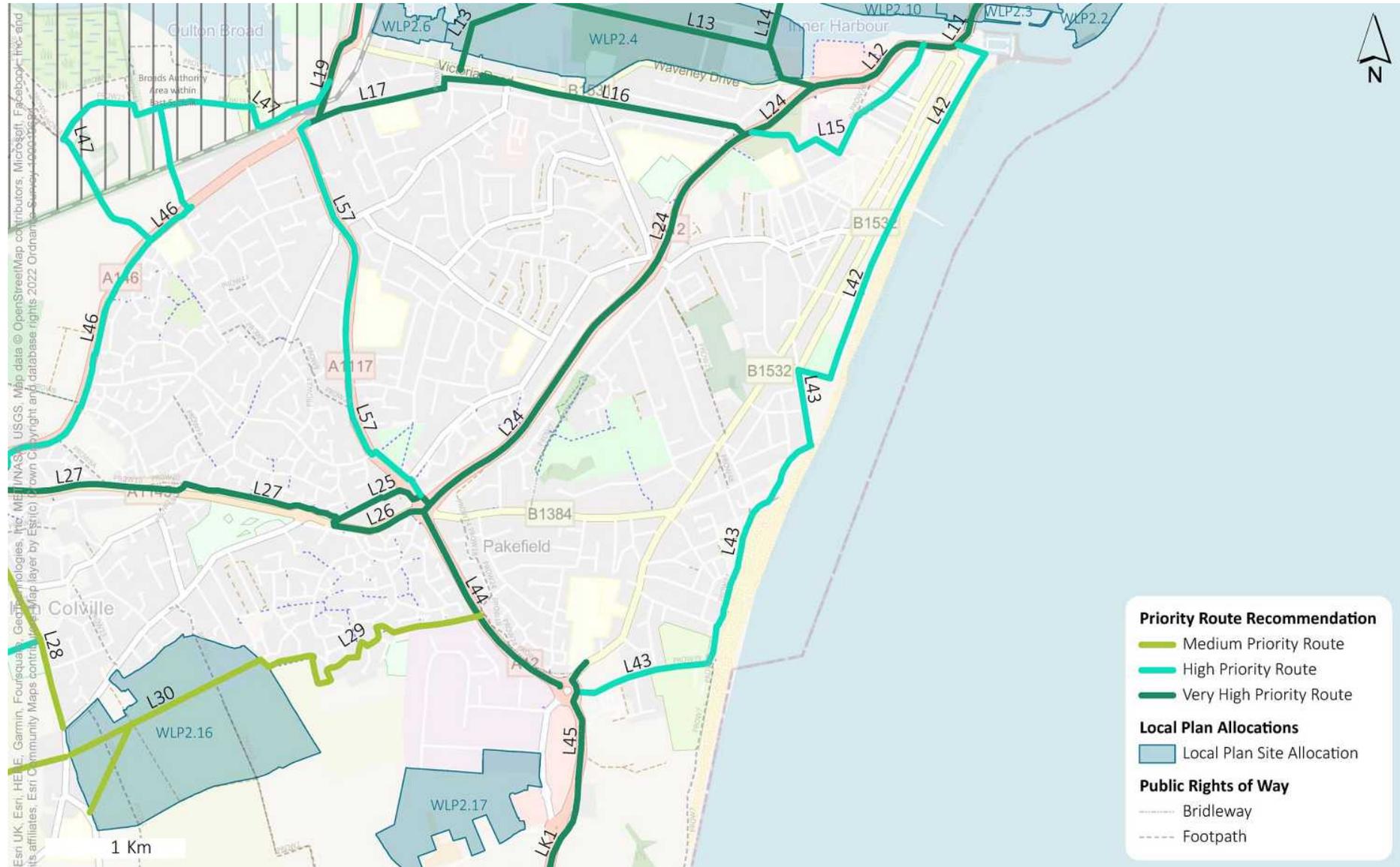
Map Reference	Location	Description	Recommendation
L31	Corton Road/Links Road	This route acts as an secondary route north to Millennium Way/A47. It is a relatively wide, straight residential road with on-road cycle lanes starting from Dene Road with a small section of shared, but unsegregated paths at Corton Playing field. Corton Long Lane is a sporadic mix of on-road markings as it changes character from residential to rural and the speed limit raises to 40mph. Propensity to Cycle Tool shows that this road could experience significant growth in modal shift. Strava Metro data suggests that this road has had high activity over the last year. This is considered a high priority route.	A segregated shared path could be considered on Corton Road. The road is wide and contains significant sections of on-road cycle lanes on both sides of the road. The pavement and cycle lanes could be utilised alongside the grass verges to create space for a good quality shared path. However, it is recognised that the road does narrow in places, notably at either end; whichever infrastructure scheme approach is taken should be as cohesive as possible with safe access to and from the shared surface. The southern end of Corton Road is relatively narrow around a busy junction (Yarmouth Road) where shared paths may not be possible. However, there may be an opportunity to bypass this section utilising North Parade and Lyndhurst Road as a loop with appropriate traffic and modal filters. At the northern end where the road narrows (and if a shared path is not viable) then consideration should be given to an attractive off-road path through Gunton Woods. Limited segregation like Orca wands could be used as a lower-level alternative which may increase cyclists’ safety and reduce the potential for vehicles to park over the infrastructure scheme. Providing a suitable access with appropriate wayfinding to Hubbards Loke should be considered alongside any potential improvements to Hubbards Loke itself.
L32	Gunton Cliff	It is recognised that a cohesive route along the length of Corton Road could be hampered by the relatively narrow entrance from Yarmouth Road. A potential alternative is use of the relatively wide roads of North Parade and Gunton Cliffs instead which offers attractive views eastwards towards the sea. This is considered a medium priority route.	Recommendation Appropriate wayfinding. The road is wide and quiet so appears largely appropriate for cycling. However, to meet latest guidance a segregated cycle track should be considered.
L33	Coastal Path	The sea wall is an attractive route with good leisure benefits. It is a long, straight and off-road route that connects Gunton to the north of the PowerPark – with The Ness Park and Ness Point directly adjoining. It offers attractive views eastwards to the sea and is a visitor attraction as well as being part of the National Cycle Route (NCR). The surface is shared between cyclists and pedestrians with the seawall to the eastern side. The surface is in poor condition with significant cracks. It is recognised that Links Road has a significant gradient which will deter some cyclists. This is considered a very high priority route.	The sea wall requires re-surfacing and would benefit from internal segregation between cyclists and pedestrians. Many pedestrians will be using the path for the sea view and will likely stop or head to the sea wall. To reduce potential conflict between the two users, a marked cycle lane with surface texture and physical segregation could be provided on the western side. The surface would need to be of sufficient quality to ensure high use, and the provision of cycle parking will further broaden its appeal.
L34	Yarmouth Road	It is important to provide a cohesive route to the town centre and Yarmouth Road provides one of the best and most direct opportunities to do so from the north. Yarmouth Road is relatively wide with reasonable quality footpaths	Create a shared path to continue the cycling and walking infrastructure along Yarmouth Road from the junction with Corton Road to the town centre. The path on the east side of the road is relatively wide (except for a

Map Reference	Location	Description	Recommendation
		and a central hatched area. The road is a main arterial route and trunk road so handles high levels of vehicular traffic movement and will be a main route for HGV vehicles. Yarmouth Road, according to Propensity to Cycle Tool, is likely well used by cyclists, however, activity on the route divides with a significant number either heading south on Sussex Road or continuing along Yarmouth Road, despite the cycle infrastructure ceasing past the junction with Corton Road. This is considered a very high priority route.	small section outside the Lowestoft Lighthouse). Ideally the shared path would be located on the west side, as this would continue the existing infrastructure. However, the pavement on the west side is notably narrower, so the eastern side may prove more viable. Additional space could potentially be absorbed for the scheme from the central cross hatching, if needed. Alternatively, a more cost-effective approach could be to utilise a path through Belle Vue Park providing an off-road attractive route; care would need to be taken to avoid disruption to pedestrians. The junction with The Ravine is wide and has poor visibility. For this reason a formal crossing, without junction improvements, may prove unreliable for pedestrians and cyclists' safety.
L35	Jubilee Way	As Yarmouth Road (A47) heads south it eventually forks into Jubilee Way (to which this recommendation relates) and the High Street (see recommendation L36). Jubilee Way features disjointed stretches of poorly marked/sign-posted shared paths along its western edge. At the end of Jubilee Road, the shared path moves southwards on Katwijk Way. To continue directly to the town centre the cyclist/pedestrian would need to use Artillery Way; to bypass the pavement-free sections of the road pedestrians can use Arnold Street. Upgrading the existing path to shared path status and quality (width, surfacing, internal segregation), together with improvements to London Road North recommended within the strategy, could provide an alternative direct route to the town centre and Lowestoft train station. Propensity to Cycle Tool shows this route is well used and has good potential for creating modal shift. However, it isn't a cohesive route and is difficult to understand where cycle paths start and finish. Furthermore, the proximity to a main road makes it less attractive. This is considered a very high priority route.	Improved signage on Jubilee Way is key. Coloured surfacing to create internal segregation is also recommended to better denote where cyclists can cycle. Traffic calming measures and well designed on-street parking bays (if needed at all, otherwise remove the ability to park) are also recommended improvements.
L36	High Street	The High Street route provides clear access to the a number of retail and hospitality units. However, the road is narrow due to historic reasons, and whilst it is one-way and is used relatively little by vehicles, it lacks any kind of cycle infrastructure in the northern section. The central section does contain an on-road marked lane, and further south the High Street becomes a shared space with traffic restrictions. The narrow nature of the road means significant improvements will be more difficult. This is considered a high priority route.	Just prior to the northern-most entry point to the High Street (where Yarmouth Road forks into Jubilee Way and High Street), where the pavement narrows, an on-road cycle lane is marked out allowing cyclists to leave the A47. This path appears to allow reasonable access onto the High Street from the north, however it offers a poor exit from the High Street onto the A47 where cyclists will swing round directly entering a busy road or must cross a busy road without a formal crossing. The High Street is one-way but at this point (the stretch adjacent to the Shell garage) has two lanes; consideration should be given to whether it needs two lanes, particularly as one lane turns northwards back towards the original direction of travel. If one lane could be removed it would provide potential

Map Reference	Location	Description	Recommendation
			for a more substantive cycle infrastructure scheme to be installed here. Along with suitable crossing points this could provide a better alternative traveling northwards from the High Street.
L37	London Road North	This road is pedestrianised and is the main town centre area for Lowestoft, and cycling is restricted in places. This means that cyclists do not have direct and consistent access to the town centre and are forced to travel southwards via Battery Green Road or Katwijk Way – neither of which are attractive routes for cyclists being busy trunk roads. Opening London Road North to greater through-cycling could provide an economic benefit, but it is recognised it must be carefully considered to not create conflict with pedestrians; special consideration must be given to more vulnerable pedestrians in this area, such as those with limited vision. This is considered a very high priority route.	A comprehensive approach with local interested parties in the design of the town centre should be undertaken. Any cycle lane should be attractively marked and ensure that potential conflict is reduced. Associated infrastructure like cycle parking should also be provided where cycling is allowed.
L38	PowerPark	Currently the marked cycle route to connect to the Sea Wall is through Wilde Street. This is one of only two viable routes into the PowerPark and then onto the sea wall – the other being Hamilton Road. Wilde Street is wide, slightly winding and has reasonable pavement provision. It leads the pedestrian/cyclist through to Gas Works Road then onto the sea wall. Hamilton Road is similarly wide with reasonable pavement provision, however travelling via Hamilton Road means cyclists/pedestrians will likely use Newcombe Road to connect to Gas Works Road, and this road is narrow and highly constrained. This is considered a high priority route.	The PowerPark Design Vision provides an opportunity for a continuous, high-quality cycle path through the employment park. The current route from Battery Green Road/Whapload Road/Wilde Street/Gas Works Road has the most merit with the existing urban form. However, there is significant opportunity for this to be shifted to Newcombe Road as the Design Vision sets out improvements that would create a 'Green Link'. The Green Link will improve the overall quality of the public realm in this location by providing green-space wellbeing benefits to cyclists and pedestrians, and potentially wider benefits to wildlife. Furthermore, the proposed improvements to Gas Works Road is to create an attractive public area that supports active travel by creating good onward connections to the sea wall.
L39	Coastal Path (continuation)	Alternative route to recommendation L38 is to allow, cyclists/pedestrians to go through to the end of Hamilton Road onto the sea wall, but this is currently difficult to access with ramps and steps. Whilst the sea wall is an attractive and useful north/south connection it is poorly connected at either end, particularly to the south. This is considered a medium priority route.	Access to the sea wall from Hamilton Road is currently restricted for most cyclists as well as those with poor mobility or prams due to the steps. Subject to approval from the Environment Agency a ramp could be installed allowing cyclists to remain on the sea wall for longer instead of travelling through the PowerPark.
L40	Wilde Street – Rant Score	Currently the marked cycle route to connect to the sea wall is Wilde Street. This is one of only two viable routes into the PowerPark and then onto the sea wall – the other being Hamilton Road. Wilde Street is wide, slightly winding with reasonable pavement provision that leads through to Gas Works Road then onto to the sea wall. This is considered a medium priority route.	Wilde Street contains a wide pavement that could be considered for upgrading to a shared path that leads around to Gas Works Road. In its current form Gas Works Road will be difficult to improve, however, if the former Gasworks land is available then sufficient space for a comprehensive cycle route through from Wilde Street to the sea wall is possible. Whapload Road is wide enough and with sufficient pavement width to continue the shared path northwards to at least opposite Rant's Score,

Map Reference	Location	Description	Recommendation
			which has value as it will allow easier access into the old historic High Street.
L41	Battery Green Road	Battery Green Road is a wide, busy trunk road, it contains three wide lanes with reasonably width pavements. It is not an ideal cycle route due to heavy traffic levels, but there are currently limited options in connecting from the PowerPark into the town centre or round to the south via Bascule bridge – particularly as there are cycling restrictions around the town centre. At present the cyclist would follow Battery Green Road around Waveney Road and enter a busy junction before heading south on Bascule Bridge, which is a significant pinch point (please note improvements to the bridge are covered under recommendation L11). This is considered a high priority route.	Battery Green Road roundabout is a wide, busy roundabout where vehicles exit at speed. This roundabout needs to be used to access Whapload Road or Hamilton Road and the crossing points around the edges require improving. The radial routes would need to be significantly improved to allow free flowing of cyclists and pedestrians around the edges. A Dutch-style roundabout or another alternative could be considered. If London Road North is changed to permit through-cycling, this would likely reduce cycling activity on Battery Green Road and would therefore reduce its delivery priority status to medium priority. Battery Green Road is wide enough to support a shared path, but the level of road re-purposing will be dependent on the traffic numbers and the extent of the impact of the third river crossing (i.e. in relieving traffic pressure in this area). The junction where Waveney Road meets Station Square could be improved alongside wider town centre improvements outlined in the Town Centre Masterplan . Should traffic flows dramatically decrease due to the third river crossing more radical improvements to the roundabout could be considered.

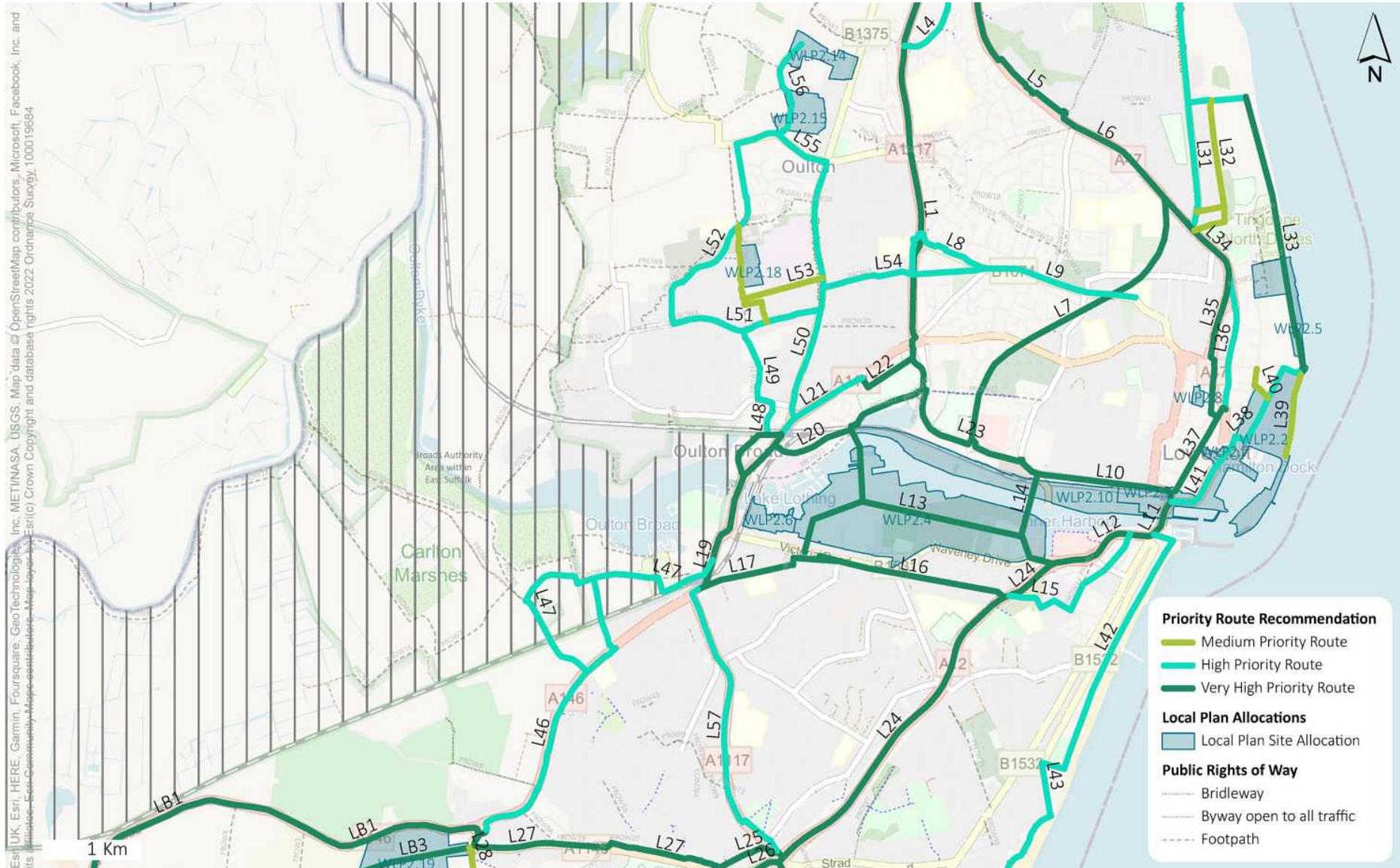
South Coast Route



Map Reference	Location	Description	Recommendation
L42	South Beach	<p>The Strategy has identified The Parade/Promenade or Kirkley Cliff for heading directly south of Bascule Bridge.</p> <p>The route along The Parade is accessed through Royal Terrace (south of Bascule Bridge), and is the only off-carriageway option; The Parade is therefore the preferred option for delivery. Strava suggests this is a popular route to travel. This is considered a high priority route. The route along The Parade is attractive and a destination in its own right. However, the paths are poorly marked with inconsistent signage. The painted lines used to create cycle 'lanes' encourage cyclists to go quickly, but without additional measures to visually indicate their presence or create more segregation they are ineffective in alerting pedestrians of the lane.</p> <p>The other option identified is to improve existing infrastructure on Kirkley Cliff which covers Royal Terrace, Marine Parade, Wellington Esplanade, Kirkley Cliff and Kirkley Cliff Road. This is an attractive route overlooking The Parade with several grand sea-fronting buildings. The road is narrow with on-street parked cars either side but is one-way with traffic calming measures and a south-bound on-road marked advisory cycle lane. It is unlikely significant improvements could be provided here without the removal of car parking. However, the parking likely serves both the seafront, a number of hotels and residential properties so would likely be unpopular to remove. At the southern end of Kirkley Cliff the cyclist/pedestrian can either follow the road around onto Kensington Road or can continue south via Pakefield Road. This is considered a high priority route if an alternative to the Promenade is required.</p> <p>It should be noted that there are currently cycle markings providing access to a local high street along London Road South. However, this route is not supported under this strategy as, whilst London Road South up to Carlton Road contains some on-road cycle markings and is one-way, continuing southwards on London Road South beyond Carlton Road is currently inadequate and a risk to safety as cyclists would need to travel contra-flow to the traffic on a relatively narrow section of the road.</p>	<p>New, attractive surfacing with different materials on the Promenade will create that awareness alongside use of attractive signage. The cycle lane could be moved to the western edge to reduce cyclists/pedestrian conflict, however, there may be some visibility issues from people entering from the west. The ability to cycle on the Parade could be extended through to the lower promenade provided this does not adversely impact the tourism offer. In addition to any improvements additional cycle parking should be provided as well as improve wayfinding signage.</p> <p>To provide better connections to the seafront some of the streets that head westwards into the town should be made more cycle friendly with aesthetic landscaping that improves the visitor offer. These roads could include Claremont Road and could also include additional cycle parking. Designs should utilise plans shown in the Lowestoft High Street and Seafront HAZ Masterplan.</p>
L43	The Cliffs	<p>There is an existing footpath close to All Saints Green that continues south to Arbor Lane along the cliffside. This footpath is for pedestrians only but provides an attractive and direct opportunity to get to the south of Lowestoft almost entirely off-road. If the path could be widened and made into a shared cycling/pedestrian track this would offer significant benefits. It is recognised</p>	<p>The cliff-top path to be widened and internally segregated if possible, providing a cohesive route southward towards Arbor Lane. There is a small path that bisects Arbor Lane, and its current status is unclear so negotiation with third party maybe required.</p>

Map Reference	Location	Description	Recommendation
		<p>that close to the cliffs it could be liable to coastal erosion so this will need to be carefully considered.</p> <p>Kensington Road/Pakefield Road provide access to this path but would require some on-road cycling. A shared path should be considered firstly but traffic filters could be utilised otherwise. Arbor Lane can subsequently be used for travel eastwards to join onto the A12/B1532 roundabout. Arbor Lane is partly a narrow access road to the caravan parks and partly a residential street but is likely to be low enough traffic for most cyclists to consider acceptable for cycling on until the shared path for circulation around the roundabout is reached. This is considered a high priority route.</p>	
L44	Bloodmoor Road	<p>Bloodmoor Road has existing cycling infrastructure of reasonable quality. It consists of a shared path along one side, although lacks internal segregation between pedestrians and cyclists. The roundabout between Arbor Lane and Bloodmoor Road is large and heavily used as a connecting point between the A12 and the B1532, though has acceptable quality cycling and walking infrastructure around the edges. This is considered a very high priority route.</p>	<p>Whilst the roundabout is of a reasonable quality for cyclists, a full Dutch-style roundabout re-design should be considered. The shared paths around the roundabout should be extended to connect to Pakefield High School.</p>
L45	London Road	<p>This section has a key role in connecting to the Lowestoft–Kessingland Key Corridor. Both Tower Road and London Road can be improved to increase connectivity to the adjacent retail and employment areas and as through-routes to the south. Between the two roundabouts there is an existing shared path, but it is relatively narrow and does not have internal pedestrian/cyclist segregation. This is considered a very high priority route.</p>	<p>Widen and improve the quality of the existing shared path to LTN 1/20 standard. Ensure the improved infrastructure extends to the entrance to Pakefield High School.</p>

Western Route



Map Reference	Location	Description	Recommendation
L46	Beccles Road	Beccles Road is a key arterial route, providing a north-south connection that runs along the edge of the western extent of Carlton Colville. The route’s existing cycling/walking infrastructure is of mixed quality. The development at Anchor Way has provided a shared cycle path for a small section northward. This path comes to a sudden stop at the end of Cabin Close where it merges onto a footpath. A second cycle path exists on Harebell Lane; however, this finishes short of Warren School. The wide grass verges alongside the Beccles Road (south) carriageway provide improvement opportunities up to Burnt Hill Lane for access to the Carlton Marshes Suffolk Wildlife Trust site. Accordingly, there is potential for a significant and cohesive shared pathway along much of Beccles Road. Eventually the properties begin to directly adjoin the highway further northwards, which limits the potential for improvements through to Lake Lothing. The remainder of Beccles Road through to Normanston Drive has been covered under recommendations L19 and L20. This is considered a high priority route.	Much of Beccles Road is likely to be suitable for new shared path infrastructure. Beccles Road already contains sections of shared path which should be joined up using new sections of infrastructure. At least, a connecting section between Cabin Close and Hollow Lane should be installed. Outline permission for this site was originally obtained in 1996 and has been developed in sections since then. If it is not possible to complete the path through the development site itself then the end point of the path should be reconsidered to allow safe and easy access back onto Beccles Road. From Hollow Lane there are opportunities due to the wide grass verges facing Beccles Road to extend the shared path yet further until the point where the dwellings directly front the road. There are three properties that directly adjoin the road which hinder the construction of a shared path as the pavement fronting these properties is relatively narrow and there is limited scope to absorb carriageway space. A small section of on-road cycling would not be ideal as it would mean the cyclist would need to enter and exit the highway in quick succession. Widening the path to create a small section of substandard shared path should be considered although it is recognised that it is unlikely the path could be widened to full LTN 1/20 standard. Regarding a connection to the Warren School site, the school has a reasonably wide path to its front with grass verge meaning the pathway could be extended yet further past Burnt Hill Lane. At this point the properties once more directly adjoin the road making it difficult to form new infrastructure. Upgrading the remaining pavements northwards to shared paths could be considered even if they are unable to provide the width required under LTN 1/20.
L47	Behind Beccles Road	The northern extent of Beccles Road is more constrained than the south meaning a quality cycle route may be difficult to achieve. An alternative route is to utilise the PROW routes around Nicholas Everitt Park. This route has the benefits of being attractive in outlook with connections to open space and the Carlton Marshes Suffolk Wildlife Trust site, whilst has the disadvantage of being less direct. This is considered a high priority route.	This alternative route can be achieved by using either Burnt Hill Lane or Ivy Lane to travel westwards. This leads to Footpaths 14 and 15 which if upgraded to cycleway status would allow cyclists to avoid much of Beccles Road. Footpath 14 is adjacent the river and achieving the desired width maybe difficult making Footpath 15 a better option. Whether by way of Marsh Road or the river path cyclists could then travel via Nicholas Everitt Park and then onto Freshwater Way. This would eventually connect to Commodore Road which continues the western route northwards. There exists a permissive path adjacent to Burnt Hill Lane from Beccles Road to just south of the railway crossing

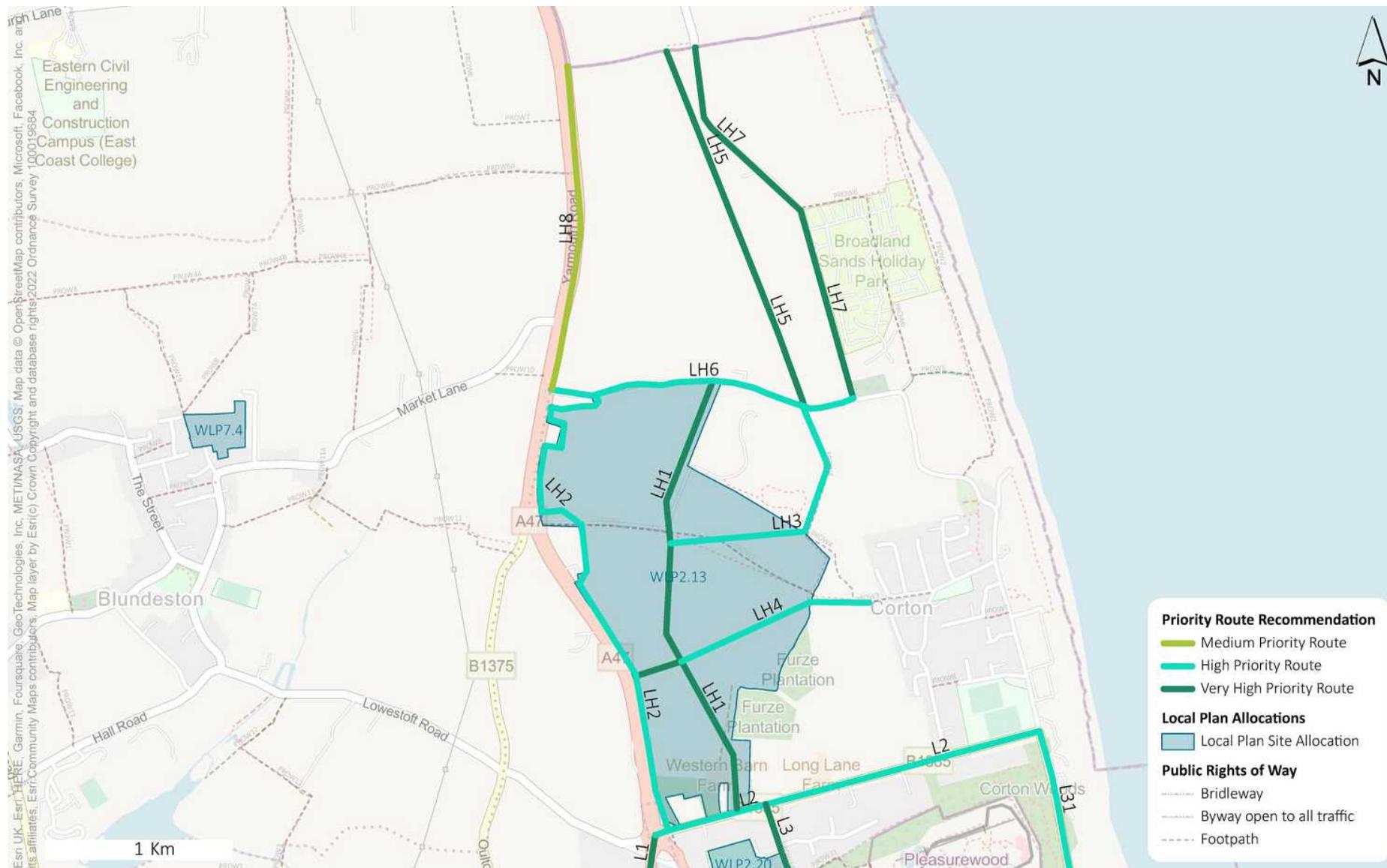
Map Reference	Location	Description	Recommendation
			that if upgraded would allow cyclists to avoid more of the road. Burnt Hill Lane is also fully surfaced meaning it is the higher priority.
L48	Holly Road	It is important to note that the section between Beccles Road and Holly Road is covered under recommendation L19 and L20. This recommendation continues after the recommendation to widen and improve the underpass between Commodore Road and Holly Road. Holly Road is a relatively quiet road with access to either Gorleston Road or Hall Road to continue onwards. This is considered a high priority route.	The road is likely quiet, but modal filters and traffic filters could be applied to Holly Road if required.
L49	Bonds Meadow	Bonds Meadow offers the opportunity for a relatively direct and attractive off-road route northwards avoiding the busy Gorleston Road. A small section of cycling on Hall Road would be required to join Holly Road to Bonds Meadow. This is considered a high priority route.	Upgrade Footpath 16 to allow for cycling through Bonds Meadow. Provide a small section of off-road cycle path alongside Hall Road with appropriate crossing point to allow cohesive movement. This recommendation is subject to appropriate biodiversity considerations.
L50	Gorleston Road	Gorleston Road is a busy 'B' road route northwards. In most places it is relatively wide with reasonable pavement and central hatching on the road. It contains an on-road painted cycle lane that extends to Hall Lane. This is considered a high priority route.	First it should be explored whether the central hatching and painted cycle lanes can be consolidated onto a cycle path segregated from the road. However should this not be viable then a cheaper option of applying Orca Wands should be considered.
L51	Sands Lane	This road provides direct access to the new and good quality infrastructure along Lime Avenue. Sands Lane is relatively wide with significant grass verges. It contains on-road painted cycling infrastructure on both sides of the road. This is considered a high priority route.	An off-road cycle route should be explored first with consideration given to Orca Wands or other forms of segregation if it is not viable.
L52	Lime Avenue	The extension of Lime Avenue to a large new residential site and country park (Woods Meadow) has provided significant new cycle infrastructure meaning this route offers a high-quality route northward. Whilst not as direct as Gorleston Road it provides a better journey overall. The older section of Lime Avenue to the south does not benefit from the new infrastructure. This is considered a high priority route.	Extend the new infrastructure southwards creating a cohesive high-quality route.
L53	Mendip Road	Mendip Road represents an alternative option to recommendation L52 as a quiet residential road which connects onto the new infrastructure on the eastern side of the Woods Meadow development. A further option is to utilise a path to the south of Mobbs Way. This is considered a medium priority route.	Either upgrade the footpaths alongside Mendip Road or provide wayfinding to signpost cycling on Mendip Road itself. Upgrade and formally adopt an off-road route to the south of Mobbs Way.
L54	Woods Loke West	This quiet road provides a direct connection through to Oulton Road. Combined this helps address some of the limits traveling east-west across Lowestoft. This is considered a high priority route.	Traffic calming measures should be applied along Woods Loke West. If viable a segregated cycle path should be considered.

Map Reference	Location	Description	Recommendation
L55	Woods Meadow Development/Hall Lane	The new Woods Meadow development has created high quality shared infrastructure along the western edge of Oulton. This leads to Hall Lane, a relatively wide B-road. It contains a reasonable pavement, but this is largely limited to one side of the road. The character of the road becomes increasingly rural westwards. The road provides a key connection between the new infrastructure along Woods Meadow back into the settlement. It also provides valuable connections to two new allocations north of the road (WLP2.14 and WLP2.15). This is considered a high priority route.	The good quality infrastructure should be extended northwards to continue through to Hall Lane. A further extension of the off-road path along Hall Lane to Gorleston Road should be considered.
L56	WLP2.14 Land North of Union Lane, Oulton and WLP2.15 Land Between Hall Lane and Union Lane, Oulton	Hall Road contains the entrance into site allocation WLP2.15 to the south of Union Lane. This presents a good opportunity to seek further improvements in creating a comprehensive route and connecting new residents into the sustainable network particularly as Oulton Street/Parkhill (which allocation WLP2.14 adjoins to) is fast moving and lacks infrastructure of its own. Cyclists should be directed onto the new infrastructure on Woods Meadow as opposed to Oulton Street/Parkhill. This is considered a high priority route.	Following the exit of the new cycling infrastructure onto Hall Lane, a continuation of the path along a short stretch of Hall Lane to the entrance to allocation WLP2.15 should be considered along with a suitable crossing point. A master planned approach will be required with a cycle lane bisecting WLP2.15 onto Union Lane and then through Union Lane to allocation WLP2.14. This approach is outlined in the allocation with both polices stating the need to provide cycle links onto Union Lane. Union Lane itself is likely relatively quiet and if the connections of the two allocations into Union Lane are at its far west the traffic is likely to be lower still. Consideration could also be given to connecting the two sites through a footpath to the west of Union Lane (Footpath 6). Whilst cyclists could then travel the length of Union Lane and exit onto Oulton Street it would still deposit the cyclist onto a busy road with limited improvement potential due to its narrow form. If a cycle lane could be provided to WLP2.15's primary access point on Hall Lane it could then connect through to the new infrastructure on Lime Avenue. A crossing point on Hall Lane would be beneficial to facilitate this. To help access eastwards back into Lowestoft once more a new crossing point on Park Hill should be provided to allow cyclists either onto Oulton Road North or The Pastures. A cycling and walking track should be introduced from the Union Lane connection to the north east corner of WLP2.14 and north along Parkhill to the footway connecting The Pastures to Footpath 9, which should be upgraded to a bridleway.

Lowestoft to Hopton Key Corridor



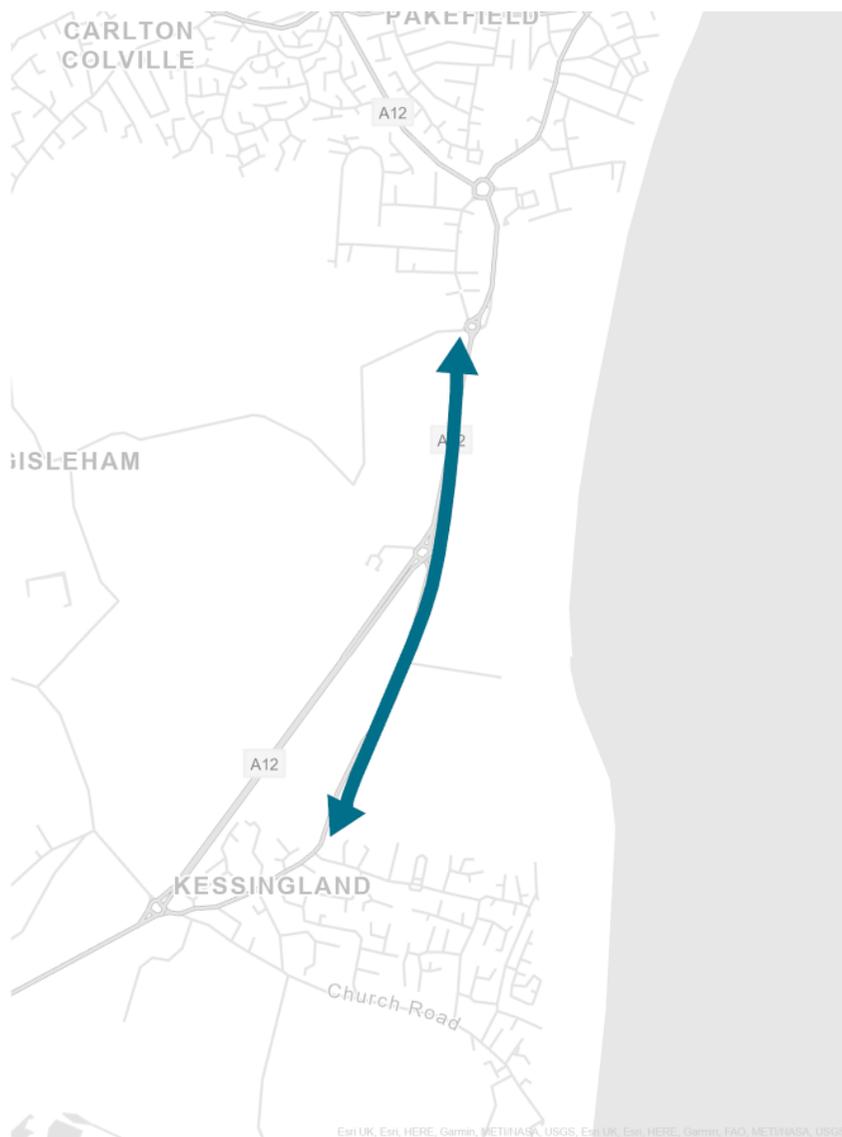
- 4.28 The Lowestoft to Hopton Key Corridor seeks to provide a cohesive cycling and walking network, which maximises off-road routes and delivers improvements of the highest quality. The route encompasses the parishes of Corton and Lowestoft. This route requires close co-operation with Norfolk County Council alongside Suffolk County Council to ensure a cohesive route through to Gorleston in the north.
- 4.29 Within this Key Corridor the Propensity to Cycle Tool (PCT) map shows the importance of the connection to Hopton which is capable of significant modal shift for commuters. It is worth noting however that PCT does not calculate modal shift potential on off-road routes such as Public Rights of Way (PROW) nor leisure trips. However, Strava Metro compiles data for both on and off-road routes and this supports the evidence from the PCT map. Strava data also highlights that Coast Road is well used and that the A47 is also used albeit less regularly.
- 4.30 The key opportunities that have been identified in support of this Key Corridor include:
- The planned development in the Waveney Local Plan,
 - Primary and secondary schools,
 - Potential connections beyond Hopton to Beacon Park with associated employment opportunities, and
 - Potential connections beyond Hopton to James Paget Hospital.



Map Reference	Location	Description	Recommendation
LH1	WLP2.13 North of Lowestoft Garden Village	The cycling and walking infrastructure in North Lowestoft adjoining the south west point of the North of Lowestoft Garden Village (allocation WLP2.13) is of a reasonable quality. Allocation WLP2.13 offers a unique opportunity to address a serious shortfall in the infrastructure provision between Lowestoft and Great Yarmouth by creating a new and high-quality cycle route through the allocation itself. This will allow walkers and cyclists heading north from Millennium Way to move continually northwards without the hazards of the A47 or the detour through Corton. It is recognised that the site only extends to Stirrups Lane meaning that for a full cohesive route further improvement to the north would need to be sought, but it does offer a good start. This is considered a very high priority route.	A new route should be constructed through the allocation forming an intrinsic part of the masterplan. A cycle path should provide connections as close to the A47/B1385 junction as possible to ensure optimum connections through to the Millennium Way shared path as well as ensuring easy access to the cycle route on Old Lane. To the north, its exit onto Stirrups Way needs to be considered as options both east and west are available to continue the path onto Hopton. Creating the exit further eastwards will lead to easier access to the Coast Road and potentially using a track to the east of the sewage treatment plant will minimise on-road cycling further. Exiting westwards on Stirrups Lane would benefit any future projects along the A47.
LH2	A47 (adjacent)	A shared path could be provided along either the grass verge adjoining the A47 or achieved through the WLP2.13 North of Lowestoft Garden Village allocation. Providing a shared path also provides an opportunity to extend the Millennium Way cycle/walking path northwards using the highest quality infrastructure. This is considered a high priority route.	An extended shared path from Millennium Way up to Stirrups Lane offers the most direct route. With the allocation immediately adjacent there is the potential to utilise this allocation to achieve such a path. In order to retain biodiversity assets, the pathway could be constructed on the other side of the existing hedgerows. However a route adjacent a busy A-road would be less attractive which would impact its value accordingly a route through the allocation away from the A47 would be preferable.
LH3	Sewage Treatment Works	The track around the sewage treatment works could provide an opportunity to connect any cycle route in the allocation closer to Coast Road. This is considered a high priority .	Upgrade and enhance Footpath 4 to allow cyclists.
LH4	WLP2.13 North of Lowestoft Garden Village	It is important that the facilities (including the school, playing field and local shopping centre) provided by the allocation are accessible to the residents of Corton. This is considered a high priority route.	Explore options to improve connections between the North of Lowestoft Garden Village and Corton. Consideration could be given to the upgrade and improvement of Footpath 3.
LH5	Off-road route (line indicative)	The most beneficial route is a fully off-road route. This would be a compromise between the directness of the A47 and the attractiveness of the Coast Road route. It provides a fully off-road route between Lowestoft to Hopton when utilised with improvements through the WLP2.13 North of Lowestoft Garden Neighbourhood. Its drawback is that brand-new bridleways would need to be created both physically and legally, but much of the land is under public ownership. The location of the old railway line is still present with a defined line of trees with sufficient width to create a good quality pathway so could be one possible route, but other pathways should also be considered. There has been some loss of the former railway line further north. This is considered a very high priority route.	A new pathway along the old railway line provides a good opportunity for an off-road route in an attractive location. It is relatively direct and appears to contain sufficient space for high quality infrastructure. Given its rural nature the trees would need to be retained and an appropriate surface selected. Should the removal of significant biodiversity assets be needed then a route along field edges could be considered. The exact route will need to be discussed with Suffolk County Council who are land owner of much of the land related to this recommendation.
LH6	Stirrups Lane	Depending on which of the various options (LH1, LH2 and LH3) both north and south are utilised will determine the extent to which a cyclist or walker will need to use Stirrups Lane. This is considered a high priority route.	Small sections of shared path could be constructed to join north-south cycle routes. Alternatively, as the road is likely quiet modal filters could be explored

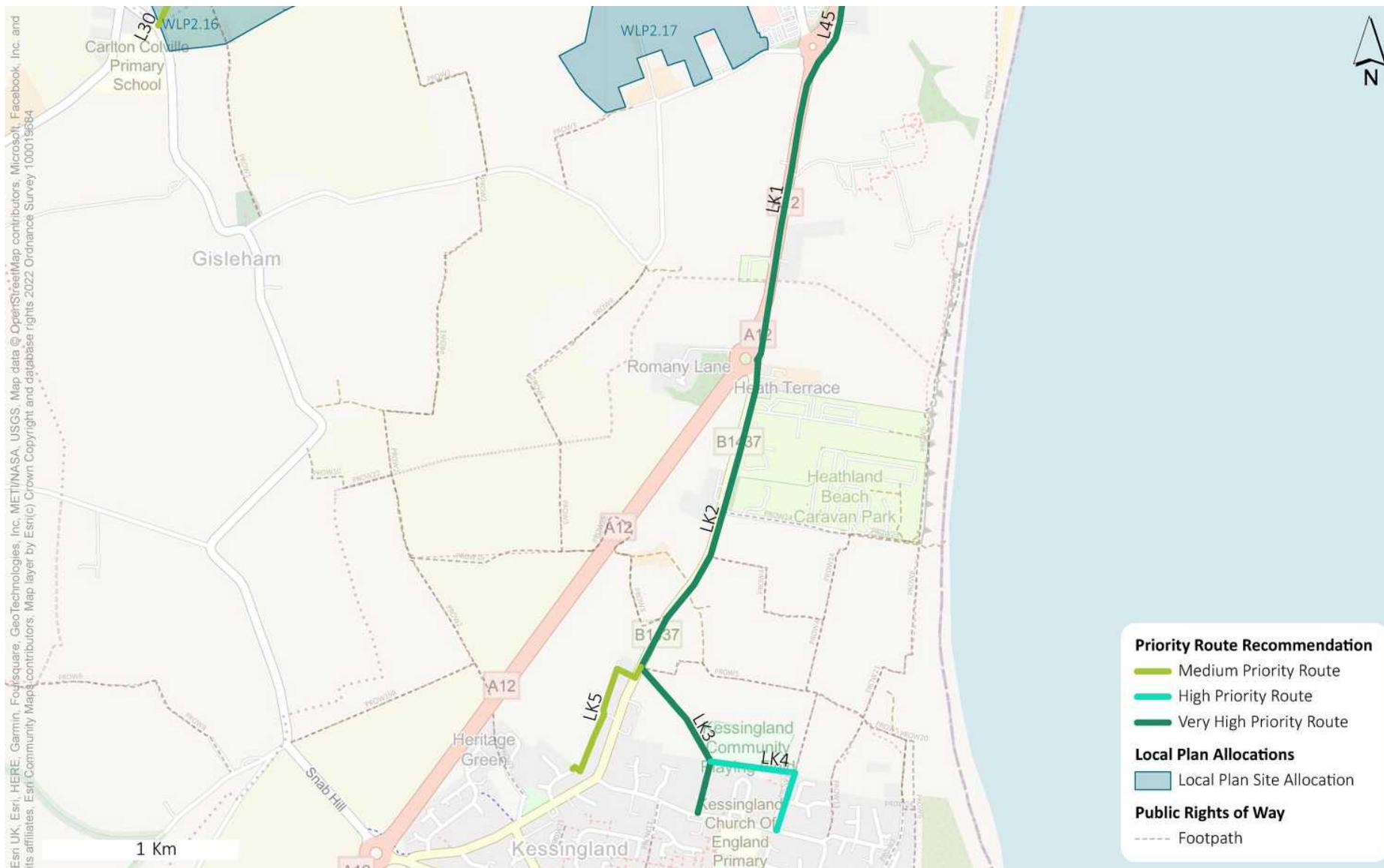
Map Reference	Location	Description	Recommendation
			to reduce its value as a vehicular cut-through to either Hopton or Corton subject to appropriate traffic modelling.
LH7	Coast Road	Coast Road forms part of the NCR. It is an attractive route formed of rural roads with vistas to the sea. The road is of a reasonable width and relatively quiet with vehicular traffic, but its curves and vegetation mean visibility is poor in some parts. Compared to the A47 it is indirect and currently requires the use of Corton Long Lane to access the best cycling infrastructure on Millennium Way (but this could be resolved through the allocation WLP2.13 North of Lowestoft Garden Village. Coast Road connects into Hopton directly. Better signage has recently been erected. This is considered a very high priority route.	Small sections of shared path (with appropriate segregation) could be constructed to join north-south cycle routes. Alternatively, as the road is likely quiet modal filters could be explored to reduce its value as a vehicular cut-through to either Hopton or Corton subject to appropriate traffic modelling.
LH8	A47 (north of Stirrups Lane)	Cyclist numbers are unlikely to be significant, although some commuters are likely use this road due to its speed and directness compared to the Coast Road. To address this a new shared cycling/walking path alongside the A47 could be considered. This would have the merits of creating the most direct route and would connect into the good infrastructure in north Lowestoft and the existing infrastructure between Gorleston and Hopton essentially completing the missing segment. The shared path should be located to the east side of the road where it can adjoin the existing infrastructure and enter Hopton without requiring crossing the road. This is considered a medium priority route.	A shared pathway should be considered from Stirrups Lane through to either the shared pathway north of Hopton or to adjoin into Lowestoft Road to the south of Hopton. The shared pathway should be of the highest possible standard and should have a degree of separation from the road whether by way of a grass verge or vegetation. The A47 contains an existing grass verge alongside the road which could be subsumed into a cycle path. The width of the verge for sections of the road is unlikely to be wide enough for the full-sized shared path and appropriate separation although a path similar to that of north Hopton could be provided despite not being LTN 1/20 compliant. To provide a high-quality shared path it is likely that land to the side of the road would need to be used requiring close working with Suffolk County and private land owners.

Lowestoft to Kessingland Key Corridor



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- 4.31 The Lowestoft to Kessingland Key Corridor seeks to provide a cohesive cycling and walking network, which maximises off-road routes and delivers improvements of the highest quality. The route encompasses the parishes of Gisleham, Kessingland and Lowestoft.
- 4.32 Within this Key Corridor the Propensity to Cycle Tool (PCT) map shows the importance of the A12/London Road for significant modal shift for commuters. It is worth noting however that PCT does not calculate modal shift potential on off-road routes such as Public Rights of Way (PROW) nor leisure trips. However, Strava Metro compiles data for both on and off-road routes and this supports the evidence from the PCT map, showing the popularity of the route. Furthermore, Strava data highlights that the route contains less permutations and choice of route than most other key corridors.
- 4.33 The key opportunities that have been identified in support of this Key Corridor include:
- The planned development in the Waveney Local and Neighbourhood Plans,
 - Primary and secondary schools,
 - South Lowestoft Industrial Estate and associated opportunities, and
 - Leisure and visitor destinations in both Lowestoft and Kessingland.



Map Reference	Location	Description	Recommendation
LK1	A12 (between Tower Road and London Road)	Both Strava and Propensity to Cycle Tool (PCT) suggests this road is already highly used. PCT suggests that if this route is upgraded to the highest possible standard, it would yield significant modal shift. Commuters from Kessingland could achieve a safe and direct route into Lowestoft with direct access to the employment areas to the south. This section of the road contains off-road cycle infrastructure already. Along its eastern edge is a shared path with no segregation between any cyclists and walkers. Whilst the surface appears largely in good condition the path falls well short of the latest best practice. Its width would be prohibitive to shared use and as it is bi-directional it could present a conflict for those travelling opposite in directions. This is considered a very high priority route.	The width of the path needs to be greatly expanded, perhaps doubled at its narrowest points to make it a suitable shared path for bi-directional travel. In many places it would need to incorporate grass verges. The highway boundary suggests some of the path may have been lost to foliage growth so removal of some overhanging foliage would be an inexpensive way of widening the path. The improvements may be difficult to achieve due to the constrained nature of the road however, the high PCT potential for growth here could be justification for a higher quality and higher cost improvement in this location so its improvement should be explored. Situated adjacent this route is allocation WLP2.17 Land at South Lowestoft Industrial Estate which allocates a significant area of land (20 hectares) to employment use. The provision of improvements here will help connect commuters in Kessingland. Improve the crossing points around the edges of the A12/Tower Road/Church Road roundabout to improve accessibility.
LK2	London Road	Propensity to Cycle Tool data shows a high level of use alongside London Road with a significant potential for modal shift and Strava also shows high use. The shared path from the A12 continues around the eastern edge of the London Road roundabout southwards along London Road. London Road is the former A12 which means it is wider than currently needed which opens the possibility of repurposing some of the road surface. This is considered a very high priority route.	On London Road the path should be widened and upgraded to the boundary of Kessingland Neighbourhood Plan allocation SA1 Former Ashley Nurseries site to allow bi-directional travel. To achieve this there is some space in the adjacent verges, but this is unlikely to be wholly sufficient so a consolidated approach could be considered whereby the existing on-road cycle path could be removed in favour of more shared path space on the eastern side. The improvement would not only provide a benefit to the key corridor but also help connect the holiday parks on London Road to Kessingland creating potential economic benefit. An alternative improvement could be the use of Orca Wands to physically segregate the existing on-road route from the road.
LK3	Kessingland Neighbourhood Plan Allocations (SA1 Former Ashely Nurseries site, SA2 Land at Laurel Farm West and SA3 Land at Laurel Farm East)	The position of the Kessingland Neighbourhood Plan allocations (SA1, SA2 and SA3) provides the opportunity to create sustainable connections for residents of these allocations and a wider cohesive network to the benefit of Kessingland's existing residents. Creating a good quality off-road route will have the greatest modal shift potential. The cyclist can then enter Kessingland through Francis Road which is a straight residential street although on-street parking can create obstacles. This is considered a very high priority route.	An off-road cycle and walking path through the site allocations (SA1, SA2 and SA3) should be sought in accordance with the neighbourhood plan and achieved through the development itself. A master-planned approach would ensure a cohesive path. The path could connect to the playing field giving the new residents sustainable access. Consideration can be given to widening a path on Francis Road to allow shared use, but it is unclear how much of the grass verge is under highway ownership.

Map Reference	Location	Description	Recommendation
LK4	Clare Road	An alternative to using Francis Road and to continue any high-quality infrastructure provided through the Kessingland Neighbourhood Plan allocations (SA1, SA2 and SA3) is to continue the cycle path eastwards along the boundary edge of the playing field. This will eventually reach Footpath 12 within an adjacent field. Footpath 12 heads southwards along Clare Road and would require both legal and physical upgrading. Much of the footpath appears to be on highway land which will reduce the number of landowners impacted, however it is noted that to the south the line of the footpath is disrupted by residential properties. The footpath runs along the eastern extent of Clare Road and is segregated by an area of thick foliage. This is considered a high priority route.	This path will eventually reach Footpath 12. A small part of this route is within a field so access and improvements would require negotiation with private landowners to progress this improvement. Footpath 12 then heads southwards into Kessingland. There is ample space to the east of Clare Road to form new infrastructure. There is an option to either upgrade and widen the footpath itself or widen and upgrade the path along the eastern side of the road. This may provide an opportunity to consolidate the footpath and pavement together as the public footpath appears to be fractured.
LK5	London Road – High Street	Should the allocations in the Kessingland Neighbourhood Plan not be developed an alternative route is to continue along London Road onto the High Street. The potential to create new infrastructure on London Road itself becomes less likely to the south with the presence of residential properties on either side of the road. London Road leads to an on-road cycle route along the western side of the High Street. This is considered a medium priority route.	Improvements alongside the remainder of London Road will be difficult to achieve due to constraints. A low-cost step to improve its potential for cyclists would be to stop vehicular parking on the road which serves to narrow the road and reduce visibility. However, this may prove locally unpopular as London Road contains a number of terrace properties without driveways meaning they are reliant on on-road parking. Parts of the path on the western side of the road are relatively wide, but other parts are narrow meaning a cohesive shared path would be difficult to achieve. A potential alternative is a new bridleway. It could start by upgrading Footpath 3 to the north and continue along the field edge to eventually adjoin to Dowson Drive. This route could also utilise a compacted gravel path that services garages to the rear of London Road if this possible it reduces the land required from the field. Negotiation with private landowners to progress this improvement would be required.
<p>The key corridor takes the cyclists/walker to the northern edges of the settlement, below are some routes for potential improvement to take them into or through Kessingland.</p> <p>High Street A wide, but relatively busy road that contains fragmented cycle infrastructure including an on-road path to the north and south and with a central section that appears suitable for a shared path. The on-road options could be upgraded with orca wands to avoid vehicles parking inside them. The junction between the High Street/Whites Lane could be upgraded using the grass verge to provide a shared cycle surface. Appropriate crossing points on both the High Street and Whites Lane will ensure good links to the Africa Alive park.</p> <p>Footpath 11 The definitive maps show a footpath bisecting Kessingland providing direct access through the settlement. The definitive map shows the footpath combines with the road network to the north on Lloyds Avenue and then through a series of residential pathways and a more traditional footpath to the south. A cohesive route could be created subject to surface and legal upgrades.</p> <p>Wash Lane This road heads north to south along the eastern side of the settlement. The road is relatively wide with good grass verges meaning a shared path could be explored here.</p>			

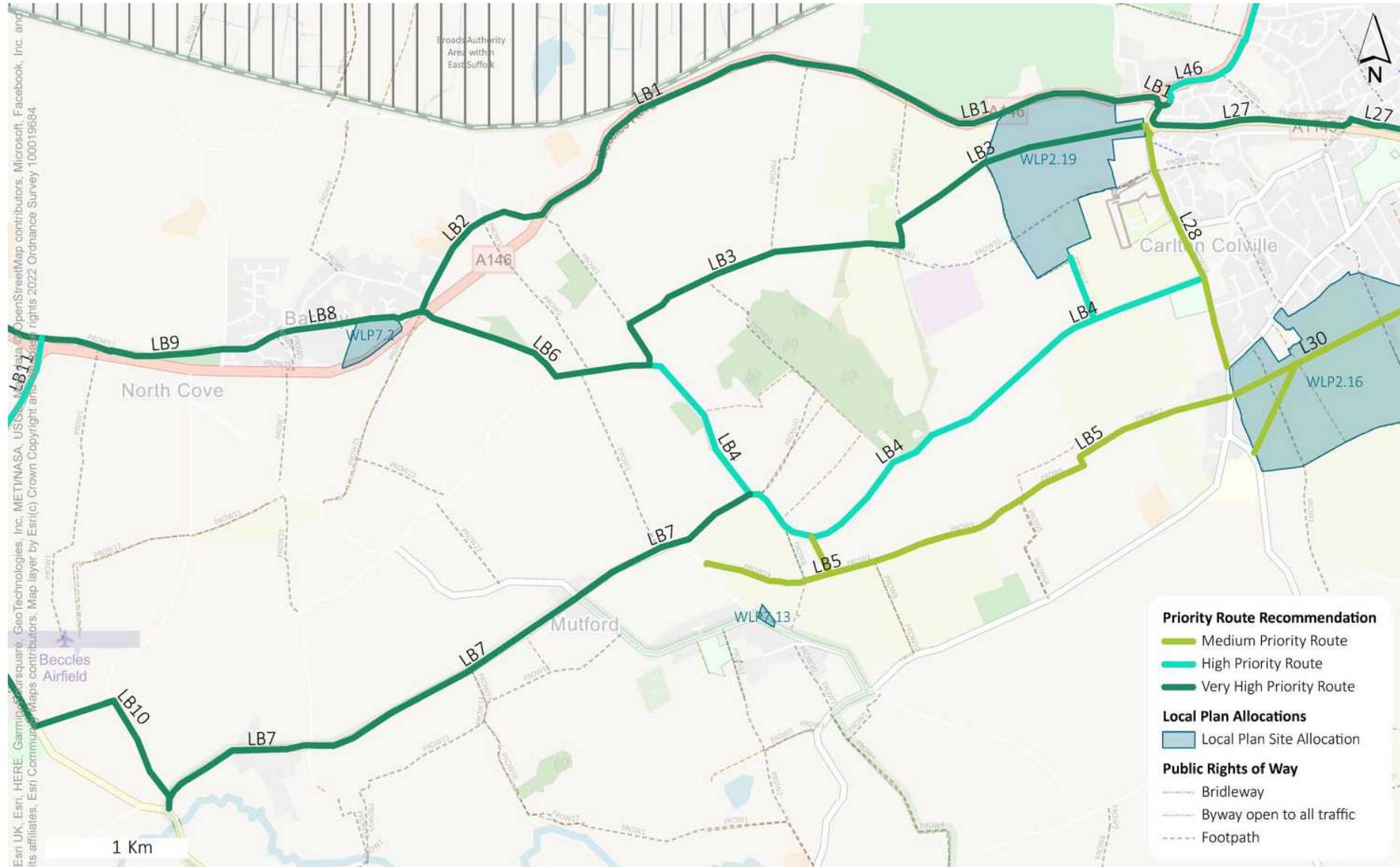
Lowestoft to Bungay Key Corridor



- 4.34 The Lowestoft to Bungay Key Corridor seeks to provide a cohesive cycling and walking network, which maximises off-road routes and delivers improvements of the highest quality. The route encompasses the parishes of Carlton Colville, Barnby, Mutford, North Cove, Ellough, Worlingham, Beccles, Weston, Barsham, Ringsfield, Shipmeadow, Ilketshall St Andrew, Mettingham, Ilketshall St John and Bungay. Parts of this Key Corridor fall within the Broads Authority area.
- 4.35 The Propensity to Cycle Tool (PCT) map shows the importance of Beccles Road (A146) for cyclists despite the hazards it presents. There exists a high potential for significant modal shift here. There is also strong potential through Beccles. It is worth noting however that PCT does not calculate modal shift potential on off-road routes such as Public Rights of Way (PROW). However, as Strava Metro compiles data for both on and off-road routes, it provides evidence as to which routes are popular. There is correlation between the two data sets with high levels of travel between these settlements, but with greater emphasis on quieter routes south of Beccles Road.
- 4.36 A cohesive route through the centre of Beccles was considered, however there are difficulties in achieving this without significant modal filters. Routes to the town centre are instead proposed through the Common and Puddingmoor/Ballygate. A more direct route into the town centre will remain the ambition, but safety and cycle awareness initiatives may provide an alternative approach.
- 4.37 The key opportunities that have been identified in support of this Key Corridor include:
- The planned development in the Waveney Local Plan,

- Primary and secondary schools,
- The cultural offer of two market towns.
- Beccles Town Centre, and
- Beccles Train Station.

Lowestoft to North Cove Route

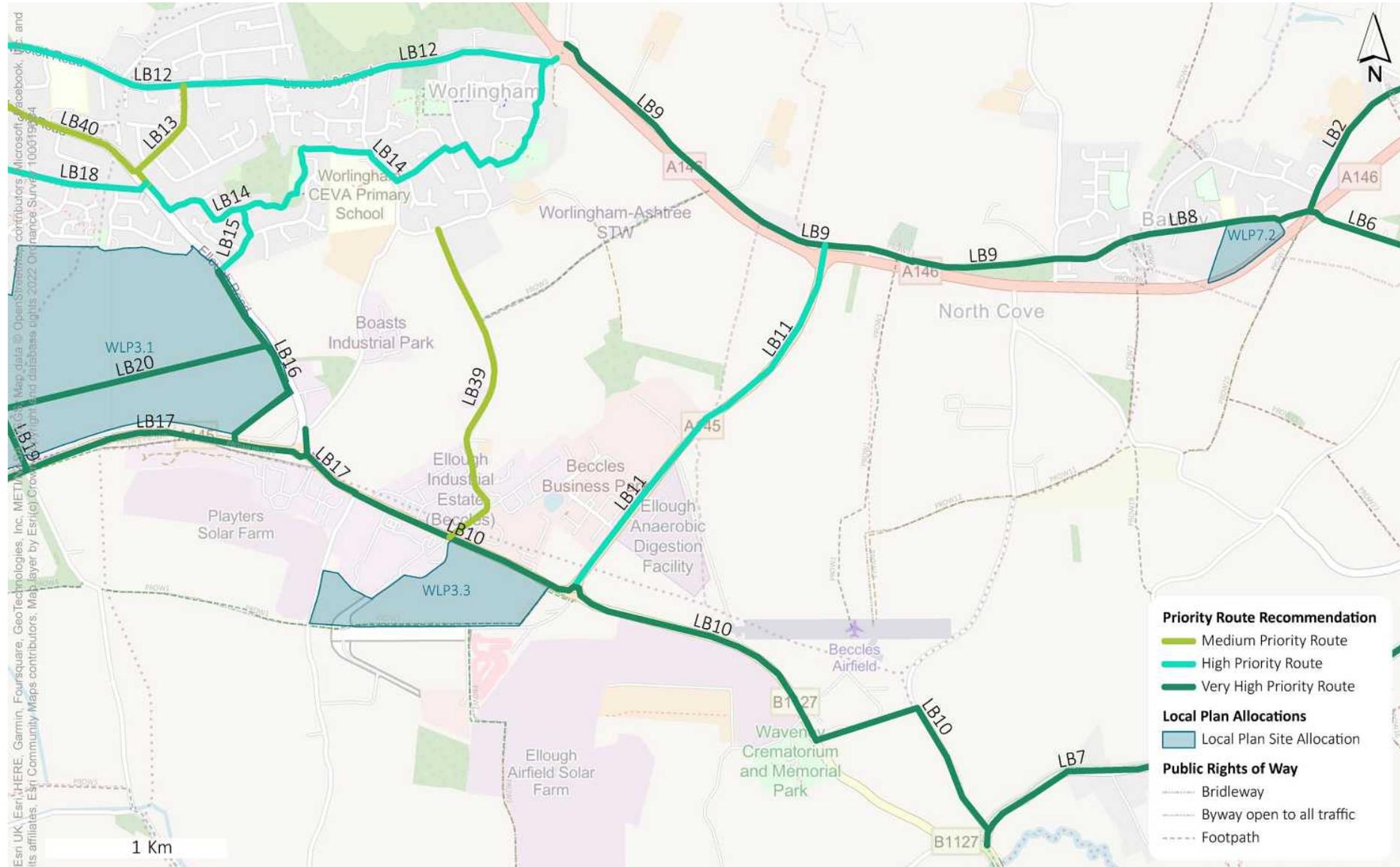


Map Reference	Location	Description	Recommendation
LB1	Beccles Road (A146)	This is the main road between Lowestoft and Beccles and Propensity to Cycle Tool data suggests there is potentially use here. The road is 50mph, both winding, undulating and whilst relatively wide it experiences a high volume of traffic meaning it is a poor route to cycle on currently. Despite its deficiencies it remains the most direct route. This is considered a very high priority route.	Considered in isolation improvements to Beccles Road could be costly due to the road's length and any infrastructure being close to a busy road reduces its value. However, a unique opportunity could be presented should Suffolk County look to construct a bypass to the Barnby Bends. This could then allow for new cycle infrastructure either alongside the new stretch of road or in utilising the existing road once the bypass is complete. A close working relationship between Suffolk County Council (SCC) and East Suffolk Council (ESC) is paramount. Whilst the other options between Lowestoft and Beccles offer opportunities and benefits, Beccles Road/Barnby Bends remains the most direct route and would most likely be the most used for commuters so provide the greatest level of modal shift. Any potential bypass should aim to link sufficiently into the WLP2.19 Oakes Farm sport and leisure allocation. Should the bypass not come to fruition then the Oakes Farm allocation offers an opportunity to start a cycle route alongside Beccles Road as a long term ambition. Should this approach occur care must be taken not to direct cyclists onto Beccles Road until completion of the wider route.
LB2	Swan Lane	Should a bypass to the Barnby Bends be deemed viable the transition into Barnby/North Cove would optimally be at The Street. However, an alternative option requiring less new cycling/walking infrastructure is Swan Lane. This is a relatively narrow road, but relatively quiet. This is considered a very high priority route.	It is unlikely that significant infrastructure will be viable on Swan Lane given its narrow width and strong biodiversity assets either side. A footpath to connect to the public house could be explored, but it is unlikely the footpath could be built to a suitable width. Traffic calming measures and signage could be an alternative. Ideally any potential works to the Barnby Bends would extend beyond Swan Lane. It is important to consider safe access onto and off any new infrastructure to ensure cyclists aren't directed back onto the main road (Beccles Road, A146).

LB3	Mutford Wood	<p>Currently this route has limited use according to Propensity to Cycle Tool data whilst Strava shows moderate use, it is notably less used than the other options. This is most likely because of the use of this route is currently dependent on using a small stretch of Beccles Road to the east. The sport and leisure allocation at Oakes Farm (WLP2.19) may offer an opportunity to address this issue.</p> <p>Mutford Wood contains no cycle infrastructure being a country lane and its unlikely to be cost efficient to create a new cycle path, however this road is likely to be quiet as it serves only a few isolated dwellings and farms. It should be noted that it reaches a potential hazard at the end of New Road where cyclists are required to cross Beccles Road; this issue will be considered under recommendation LB6as both join New Road so both have a similar problem. This is considered a very high priority route.</p>	<p>The Oakes Farm allocation provides an opportunity to create a direct link onto Mutford Wood through the site without the need to use Beccles Road. This will then create a safe and direct means of reaching Mutford Wood. Mutford Wood could then be made a candidate for a quiet lane designation should it be better accessed. Given the limited number of houses and businesses, restrictions for through traffic could also be considered further improving the road.</p>
LB4	Mutfordwood Lane	<p>This route is a National Cycle Route (NCR) and is currently the most suitable for non-confident cyclists. The connection onto this route from Lowestoft is through Church Lane.</p> <p>Mutfordwood Lane is a narrow country road, both long and winding. However, it is likely to be quiet as it serves only a few properties, businesses, and farms. It is also likely that most traffic will use the main roads, however it can be used as a bypass to parts of the A12 and may experience traffic from Mutford itself. Propensity to Cycle Tool data shows some potential use, but not a significant level of cyclists, however Strava shows it is an important route perhaps suggesting it is a route used more for leisure cyclists than commuters.</p> <p>It is inherently indirect which reduces its value but does provide an attractive route with leisure benefits. This is considered a high priority route.</p>	<p>It is unlikely a new track alongside the long Mutford Wood/Mutfordwood Lane/New Road route will be viable despite representing a good solution. However, the road is relatively quiet and could be considered as a quiet lane. The biggest weakness with this route is its eventual connection to North Cove by crossing Beccles Road (A146) via New Road which is noted under LB6 It should be further explored whether connections into the WLP2.19 Oakes Farm sports and leisure allocation can be achieved via improvements to Bridleway 17. An ancient/veteran tree is adjacent this route and no improvements should damage this important biodiversity asset.</p>
LB5	Bridleways 12, 6, 4, 8 and 24	<p>An alternative route to LB4 that could be considered is to utilise bridleways south of Mutfordwood Lane. Cycling is allowed on this route and it provides an attractive, traffic free alternative. This is considered a medium priority route.</p>	<p>The surface would need improving to ensure it is accessible to a wider range of users.</p>
LB6	New Road	<p>New Road heads northwards towards North Cove and is a relatively narrow rural road, both attractive in outlook and relatively quiet with traffic. New Road then meets Beccles Road (A146) requiring cyclists to cross over into North Cove or continue along Beccles Road. This crossing point involves a busy A-road with a 50mph speed limit and utilising a traffic island. This represents a potential hazard that would reduce this routes desirability for non-confident cyclists and families. This is considered a very high priority route.</p>	<p>The biggest weakness with this route is its eventual connection to North Cove by crossing Beccles Road. As a functioning A-road, the extent of traffic calming measures that can be utilised may be limited. However lower speeds and traffic calming measures should be considered.</p>

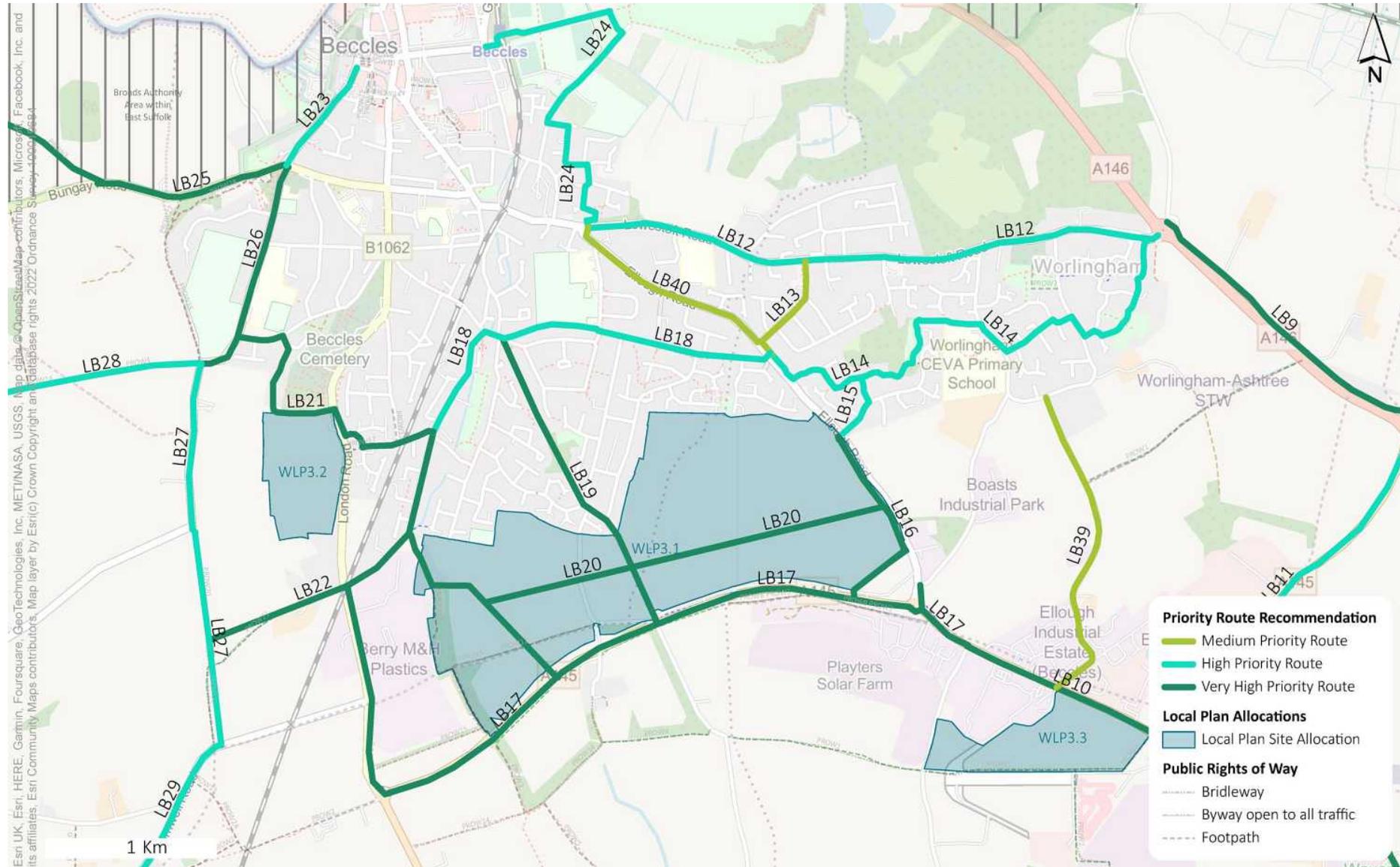
LB7	Church Road/Hulver Road	<p>Propensity to Cycle Tool (PCT) data suggests limited cyclists use this route and also shows a lower growth potential. The PCT data suggest that this has limited commuting potential, but it does connect directly into a key employment area at the Beccles Airfield/ElloUGH industrial Estate which is allocated for more growth (WLP3.3) so it may have greater potential for commuter growth in the future. Strava in contrast shows the route is well used.</p> <p>Church Road is likely to be relatively quiet, but it does go through the village of Mutford so will likely experience higher levels of traffic than Mutford Wood/Mutfordwood Lane.</p> <p>This route has definite merits in offering an alternative and utilising potential growth south of Beccles. This is considered a very high priority route.</p>	<p>A new track alongside the long Church Road/Hulver Road route would represent the best solution, but the roads are constrained, and such a route would have a high expense due to its length. Instead, improved signage and wayfinding with traffic calming measures could be considered.</p> <p>A potential improvement could be the extension of the small section of pavement alongside Hulver Road in Mutford to encompass the remaining properties on Church Road using the existing grass verge. This will provide the benefit of connecting the properties together and connecting the houses on Church Road to the bus stop.</p>
LB8	North Cove	<p>The Street is the main road through the village of North Cove. On-road cycling is required but the road is limited to 30mph (and 20mph outside the school) and will be relatively quiet and an NCR. Eventually this joins a pathway at the western extent of the village. This is considered a very high priority route.</p>	<p>A shared path could be considered alongside The Street and would be the optimum solution so should be considered first. However The Street does narrow westwards and a cohesive shared path across its entire length may not be viable. An alternative solution are modal filters applied to The Street to reduce through traffic. A traffic model would be needed, but it would potentially displace traffic onto Beccles Road (A146) and remove them from the village centre.</p>

North Cove/Mutford to Beccles Route



Map Reference	Location	Description	Recommendation
LB9	Lowestoft Road	At the end (western side) of The Street/Marsh Road is a path that runs parallel to Beccles Road to Lowestoft Road. The legal status of this path is unclear from the definitive map, however there are signs stating footpath and on the western extent signs suggesting cyclists can use it also. In addition, it is part of the NCN. This route does offer a chance to avoid the more hazardous A146 Beccles Road and is off-road and attractive. This pathway eventually joins Lowestoft Road. Lowestoft Road appears to have been a main road before the creation of Beccles Road, now it runs parallel to the Beccles Road and its eastern extent is a dead-end. The road has no cycle infrastructure, but it is quiet, relatively wide and 30mph. It contains a narrow pedestrian footpath. Given its width there is potential for a new cycle path here. As an alternative or addition to this approach modal filters could be applied. Modal filters would restrict the road as a through-road and potentially free more space for cycling. This is considered a very high priority route.	Ensure the status of the path between The Street and Lowestoft Road allows for cycling and provide sufficient wayfinding. Lowestoft Road represents a good cycling route due to its width and quiet nature. However it could be improved with a segregated cycle lane or modal filters to cut through traffic. Ensure that any improved cycling route extends around the roundabout to Marsh Lane.
LB10	Benacre Road	The recently constructed Beccles southern bypass offers good quality infrastructure but this ceases opposite Church Road meaning it does not connect through to the rest of the employment area nor as far as Hulver Road. The B1127 (Benacre Road) is a poor road to cycle on due to the traffic speeds and HGV use and should not be encouraged without an extension of the good quality cycle path. This is considered a very high priority route.	The cycle path should be extended through to Copland Way as a minimum and further if possible. This will provide good connections for the residents of Beccles and Worlingham to the employment area. However to ensure a cohesive key corridor an off-road path would be required with connections to Hulver Road. The line on the map is indicative but the poor junction between the B1127 and Hulver Road needs to be avoided in its current layout.
LB11	Copland Way	This road provides an opportunity to connect the two identified recommendations on Lowestoft Road to the north and Benacre Road to the south. The road is relatively wide with reasonable visibility. It contains large, albeit sloped, grass verges either side. The road services employment uses so experiences HGV traffic. This is considered a high priority route.	Consider an off-road cycle route along the length of the road with a suitable crossing point to join Lowestoft Road. Adjacent PROW routes provide some pedestrian north-south movement, but as a minimum a pedestrian footpath/shared path should be provided to the entrance to Anson Way.
LB39	Sandpit Lane	Sandpit Lane is a relatively quiet road, rural in character connecting employment uses to the south and a residential area to the north. The road is narrow and largely single lane with high vegetation to the north. It becomes marginally more open and wider towards the south. This is considered a medium priority route.	A modal filter can be applied to ensure it is not used as a cut-through into Worlingham. This will potentially reduce traffic and make it safer to walk or cycle. There does not appear to be residential properties on this road and the employment sites should avoid use through the residential streets to the north anyway. Furthermore access between Worlingham and the employment site would not be significantly impacted as College Lane provides an alternative. The usability of Sandpit Lane for cyclists/walkers declines to the south where heavier vehicles will be present so ensuring signposting and visibility will be key here.

Beccles Route



Map Reference	Location	Description	Recommendation
LB12	Lowestoft Road	<p>This route is the most direct route through Worlingham and Beccles and brings the traveller close to Beccles town centre. Lowestoft Road is a wide, busy residential road running through Worlingham and into Beccles. It contains reasonable footpath provision on both sides of the road. The road widens opposite Glenwood Close and then provides an on-road cycle route marked by white lines through to the Ingate/Ellough Road junction. Both Strava and Propensity to Cycle Tool data show this to be the most popular route through Worlingham/Beccles.</p> <p>As the route progresses westwards the constraints increase making it difficult to obtain the best quality infrastructure. The railway line bisects Beccles creating a pinch point at Ingate which is also highly constrained with properties hard to the boundaries of the pavement meaning improvements are limited. Accordingly despite the route's existing popularity the key corridor is diverted northwards to allow direct access to the town centre, but not a direct route through Beccles. Clearly this does not stop cyclists from continuing through, but as high-quality improvements are not likely to be viable this would appear a better use of future resources. This is considered a high priority route.</p>	<p>Provide a cohesive shared path along Lowestoft Road before utilising wayfinding to direct cyclist's northwards at Brick Kiln Avenue. Wayfinding will be required to direct cyclists through a number of quiet residential streets to the Common's cycle/pedestrian route. An ancient/veteran tree is adjacent this route and no improvements should damage this important biodiversity asset.</p>
LB13	Hillside Avenue	<p>The road provides a useful connection between Lowestoft Road and Ellough Road. It is relatively quiet although subject to some through traffic. It contains reasonably wide grass verges and provides connections to a small secondary cluster of shops. This is considered a medium priority route.</p>	<p>The grass verges mean a widened path for shared use remains a possibility. If this is not considered viable an alternative would be to use modal filters to restrict use of the road for through traffic. This could be applied at the most appropriate location on the road.</p>
LB14	NCR (various) Part 1	<p>This route through the built-up areas of south Beccles and Worlingham is less direct and more complex than the central route. Furthermore, it bypasses the town centre with its retail and cultural offer. However, it has a significant advantage in that it utilises existing infrastructure and avoids on-road cycling to a greater degree; potentially making it a safer route. This route is the current NCR so contains reasonable wayfinding signage despite the complexity.</p> <p>Propensity to Cycle Tool data shows sections of the route are reasonably well used (notably Darby Road and Coney Hill) but other sections are poorly used, particularly eastwards. Strava data shows some use, but not to a significant degree. Overall, despite better infrastructure and quieter roads, it is far less used than the central route through the town. This is considered a high priority route.</p> <p>Pains Close – Accessed through a shared path off Lowestoft Road. The road is a quiet cul-de-sac.</p>	<p>Suttons Road /Rectory Lane/All Saints Green - Improve wayfinding signage and crossing points along the route. Whilst functional cycle route signs are present they are sometimes difficult to see and obstructed so, subject to highway consent, more noticeable or even themed signs could be utilised. Extend the shared path along Garden Lane to the off-road cycle and walking route heading westwards.</p>

Map Reference	Location	Description	Recommendation
		<p>Pepys Avenue – A reasonably wide residential road, but unlikely to be a high priority for improvement. Prior to accessing the cul-de-sac a shared path could be created with the space available to connect Pains Close to Pepys Avenue cul-de-sac. However, this would create an incoherent route as cyclists would go from on-road cycling to shared path and off again. The roads width and relatively quieter nature means it is unlikely to require significant improvement.</p> <p>Shared Path between Pepys Avenue to Janet Hadenham Close – An attractive route of good quality. Janet Hadenham Close is a quiet cul-de-sac.</p> <p>Suttons Road /Rectory Lane/All Saints Green – Whilst these roads do not appear to be significantly busy, they are likely busier roads than much of this route so far and it involves several turns and crossings. The grass verges may provide some opportunity for an off-road path, but again it would not create a coherent route with a mix of route typologies. Whilst the route is signposted, improvements could be made in this regard to make it more visible as well as improvements to crossing points with toucan crossings.</p> <p>Wainford Close – There is a shared path from All Saints Green to Wainford Close. Wainford Close is likely to be relatively quiet and there is limited potential for a new path.</p> <p>Garden Lane – There is a small section of shared path from the end of Wainford Close onto Garden Lane to a formalised crossing point and then a short section through to the primary school. Clearly this was created for the safety of school children accessing the school; given the grassed area in front of the school it would appear possible to extend this shared path further through to the entrance of the off-road cycle route to the north. This would allow cyclists to remain off-road for longer on one of the busier stretches of road.</p> <p>Garden Lane to Holly Close – An attractive off-road route of good quality that is unlikely to be a priority for improvement.</p> <p>Holly Close/Rowan Way – Holly Close is a narrow-shared surface with no distinction between cars and cycles. It is a quiet cul-de-sac with limited potential for improvement. Rowan Way is wider and with reasonable pathways. A shared surface along the north edge to join into the shared path on Ellough Road should be considered. It is, however, unlikely to meet the best practice width.</p>	

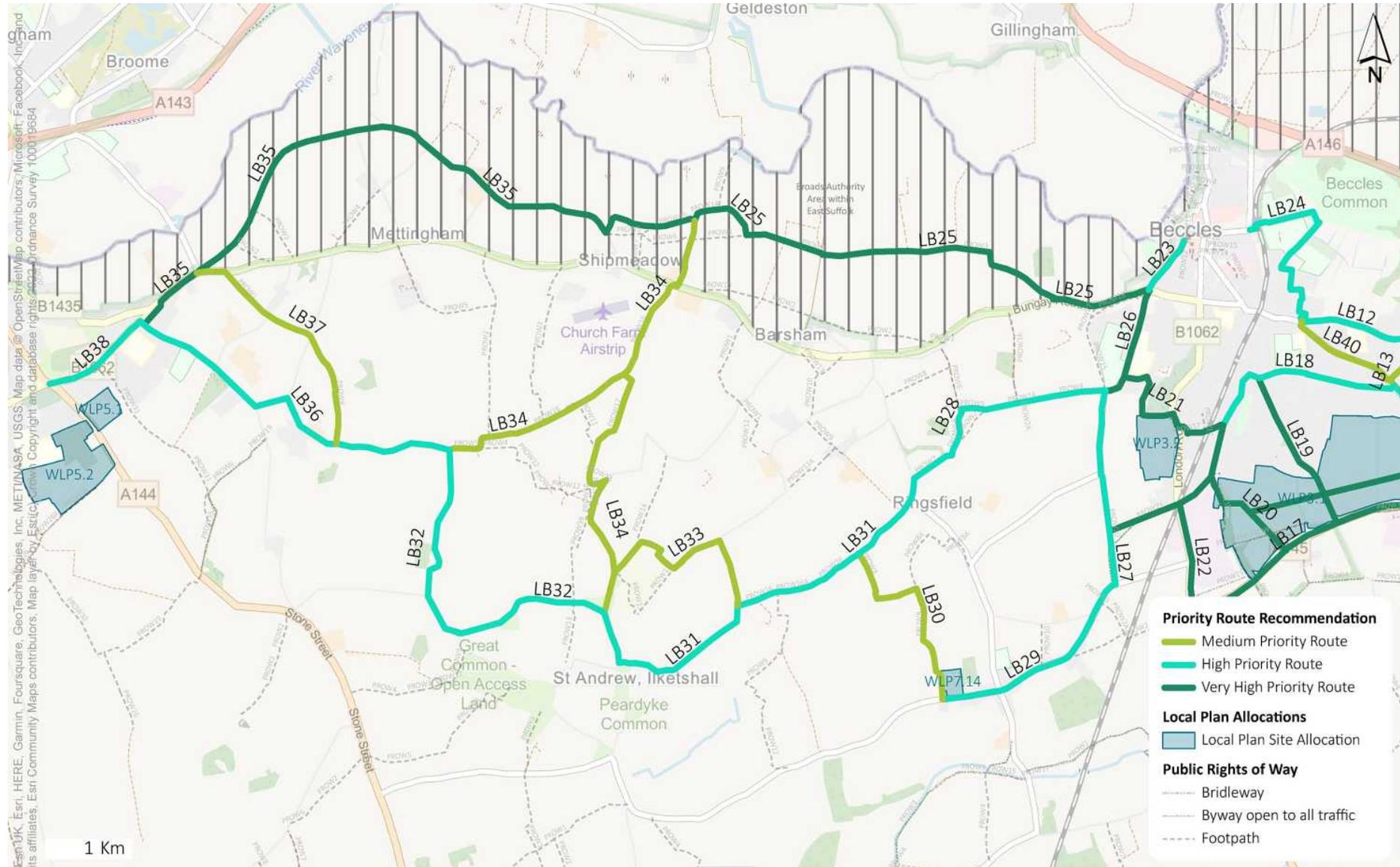
Map Reference	Location	Description	Recommendation
		Ellough Road – This section uses a mixture of shared path infrastructure and uses the quiet service road set parallel to Ellough Road. This represents a small, but decent section for both cyclists and pedestrians before crossing onto Coney Hill using a toucan crossing.	
LB15	Cedar Drive/Rowan Way	Cedar Drive/Rowan Way takes the traveller off the National Cycle Route and allows them to approach the Beccles and Worlingham Garden Neighbourhood avoiding the busier Ellough Road. This is considered a high priority route.	<p>Cedar Drive is relatively wide at its western entrance off of Ellough Road and there is potential to widen the pavement to form a shared path, however the roads do narrow eastwards. There is a pinch point just east of Sycamore Close where the road narrows further and there are no grassed areas. A shared path here would limit road space, but this may provide a traffic calming measure by allowing only one car to pass.</p> <p>Create a crossing point from Cedar Drive over Ellough Road and into the WLP3.1 Beccles and Worlingham Garden Neighbourhood allocation. A lighted crossing could be considered as if the allocation is built this section of Ellough Road will feel more urbanised where a lighted crossing is more appropriate.</p>
LB16	Adjacent Ellough Road	Ellough Road is an important connection to the WLP3.1 Beccles and Worlingham Neighbourhood allocation and employment area. Strava suggests it currently experiences high levels of use. This is considered a very high priority route.	Provide a link alongside Ellough Road through the allocation to connect into the southern bypass infrastructure. This will require a suitable crossing point potentially near Cedar Drive.
LB17	Beccles Southern Bypass	The southern bypass represents a section of high-quality infrastructure and it is important to maximise the use of it via the WLP3.3 Beccles and Worlingham Garden Neighbourhood allocation. This is considered a very high priority route.	The allocation states that ‘Pedestrian and cycle links should be provided to Bluebell Way, Cucumber Lane, Darby Road, Nicholson Drive, Oak Lane, Field View Gardens/Foxglove Close and Cedar Drive’ and ‘A cycle path should be provided along the boundary of the site with Ellough Road’. The high quality infrastructure alongside the southern bypass should be extended to College Lane utilising the wide grass verge to allow safe access onto College Lane. Ensuring safe connection onto and off the dedicated infrastructure will be key.
LB18	NCR (various) Part 2	<p>Coney Hill – This is a long, relatively straight residential road that is likely to experience a higher level of traffic. The road does not contain any cycling infrastructure, however it has relatively wide pavements and wide grass verges meaning the road could be upgraded to a shared path which could directly enter the existing cycle infrastructure at either end. If the traveller heads south from Coney Hill they will eventually join the Beccles and Worlingham Garden Neighbourhood allocation (WLP3.1) and connect through to Route 3.</p> <p>Between Coney Hill/Castle Hill and Rigbourne Hill – The route then connects onto a shared off-road path of reasonable quality.</p>	<p>Coney Hill - New quality shared path along the south side of Coney Road.</p> <p>Rigbourne Road/St Georges Road - Create path along side of the roundabout to connect cycle path St Georges Road/Darby Road with good crossing points.</p> <p>Darby Road - Create shared path if possible, alternatively look at whether a route through Darby Park can be achieved. Ensure the access onto Darby Road is a sufficient width.</p>

Map Reference	Location	Description	Recommendation
		<p>Rigbourne Hill, St Georges Road to Darby Road – The shared path exits near to the roundabout which represents an obstacle for cyclists. However, along the southern side of the roundabout there are sections of open space. These areas of managed grass contain sufficient space to allow a continuation of the shared path with good crossing points from Rigbourne Hill to Darby Road. Ideally to avoid re-entering the road prematurely this would be extended onto Darby Road itself, but this may require private land requisition.</p> <p>Darby Road – This road is a relatively long residential road that heads towards the south of Beccles. The road is relatively wide for much of its length with relatively wide pavements and towards the south, reasonable grass verges. Overall, it may be suitable for a shared path, but it would represent a significant level of work and as there are limited grass verges to the north where the constraints may render this approach difficult. An alternative could be to upgrade the pathway through Darby Park play area, but this would add an additional complexity to an already complex route. This is considered a high priority route.</p>	
LB19	Rigbourne Hill	The is an attractive green corridor running northwards from the WLP3.1 Beccles and Worlingham Garden Neighbourhood allocation which provides a pleasant and direct route towards the town centre. This is considered a very high priority route.	Improve the surface width to reduce conflict between cyclists and pedestrians and ensure the route is available to cyclists as its status is not clearly marked.
LB20	WLP3.3 Beccles and Worlingham Garden Neighbourhood	The Beccles and Worlingham Garden Neighbourhood is an allocated site (WLP3.1) and the policy requires good cycle links throughout the site. This allocation should provide high-quality infrastructure that must be maximised by suitable connections into the site. This is considered a very high priority route.	Suitable infrastructure should be provided in accordance with the allocation policy. The lines on the Cycling and Walking Strategy map are indicative only, however a Beccles and Worlingham Garden Neighbourhood Masterplan Report has been produced.
LB21	NCR (various) Part 3	<p>Kemps Lane – This a narrow stretch of road without a footpath, however it represents one of the few crossings over the railway line so it is a necessity to use. It is likely relatively quiet, and cyclists are aided across the narrow ridge of the bridge by the provision of an on-road marked cycle route. It is unlikely significant improvements could be made here. The narrow width of the bridge may render Orca Wands too prohibitive although any larger vehicles should be directed to another crossing point over the railway line. Traffic restrictions could also be considered.</p> <p>Kemps Lane to London Road to Meadow Gardens – There is an off-road path between Kemps Lane to London Road which then adjoins the shared path along London Road itself representing a section of good quality infrastructure. Via a crossing point with traffic island this then leads to a</p>	<p>London Road to Meadow Gardens - Improve crossing point to create new path through allocation WLP3.2</p> <p>South Road/Ringsfield Road - Create cycle path to link the NCR with the key corridor suggestions westwards to Bungay.</p>

Map Reference	Location	Description	Recommendation
		<p>shared path on towards Meadow Gardens. The crossing point should be upgraded to encourage greater use. The adjacent allocation, WLP3.2 Land West of London Road which is currently under construction, provides an opportunity to improve the crossing and provide a new and improved pathway through to Meadow Gardens.</p> <p>Meadow Gardens – Meadow Gardens is a quiet residential cul-de-sac. It is unlikely significant improvements could be undertaken here. The width of the road and lack of grass verges is not conducive to a shared path, however as a relatively quiet road it is likely to be an acceptable surface to cycle on.</p> <p>South Road/Ringsfield Road – South Road runs to the south side of the high school. It contains no cycle infrastructure but is relatively wide with some grass verges. Its width is likely needed due to the parking caused by the school. Ringsfield Road which exits Beccles to the southwest contains no cycle and walking infrastructure. It is likely that a shared pathway is possible on South Road, in addition a shared path is possible along Ringsfield Road as south of the playing field there are wide grass verges. These improvements have limited value on their own, but with wider improvements towards Bungay they become much more valuable. This is considered a very high priority route.</p>	
LB22	Wash Lane	<p>Wash Lane provides an opportunity to provide further connections into the WLP3.1 Beccles and Worlingham Garden Neighbourhood allocation. The road is relatively quiet and an attractive location in which to walk and cycle. The road services a few properties only and as you travel westwards vehicles can no longer pass. Wash Lane continues westwards and merges with Bridleway 21. Its surface is relatively good where it forms an access to a few properties, but it does degrade further westwards. This is considered a very high priority route.</p>	<p>The exit from Wash Lane to London Road should be widened where possible so cyclists can remain mounted (this will be restricted by the positioning of the Pill box), the width of Wash Lane should also be improved through foliage management. A cycle path should then be extended to the nearby employment premises.</p> <p>Consideration should be given to extending it further still to the southern bypass. However a similar route could be achieved through the garden neighbourhood allocation itself. Bridleway 21 could have its surface upgraded where required to allow continuous access for cyclists. It is important that any route between Lowestoft to Bungay also provides adequate connections into Beccles town centre. For infrequent cyclists or those making service trips completing the whole key corridor from Lowestoft to Bungay is unlikely. More likely are trips from either end or the surrounding villages to use the services present in Beccles. A route using the most central arterial routes were explored first, however these roads are busier with traffic and contain significant pinch points. It was considered that without significant restructuring of its main road's improvement would be difficult to achieve.</p>

Map Reference	Location	Description	Recommendation
			Whilst it is recognised that these two options put forward do not represent the most direct nor even the most well used routes to the town centre itself, they are considered quieter, more attractive and with greater potential for improvement.
LB23	Ballygate or Puddingmoor	To access the town centre from the western side would likely be best utilised by either Ballygate Road or Puddingmoor. Ballygate is an attractive route and relatively wide to the south although it does narrow northwards, some of the roadside gullies may also dissuade cyclists. Puddingmoor is narrower, but likely quieter and likewise attractive. However Puddingmoor is at a lower level than the main town centre with narrow steps up to Ballygate unsuitable for cyclists. This is considered a high priority route.	Better wayfinding to help direct cyclists to the optimum route. Consideration should be given to traffic filters where one-way traffic could free up road space on Ballygate with consideration for vehicle weight restrictions on Northgate providing this does not adversely impact delivery vehicles.
LB24	Common Lane	Access to the town centre from Lowestoft Road could be achieved through an existing connection onto Brick Kiln Avenue then utilising a series of quiet residential streets to Common Lane. Common Lane is a long, narrow road with a rural character. The south of Common Lane contains a narrow pavement. The remainder of the road leads to the playing field, which provides a route back into town and with suitable connections onto Gaol Lane which provides access to the town centre. This is considered a high priority route.	Provide suitable wayfinding and potentially traffic controls through the residential streets to Common Lane. Widen the existing pavement on Common Lane and provide traffic controls and wayfinding to the playing field (dismounting over the rail bridge will be required. Provide suitable connections onto Gaol Lane using recently installed crossing point.
LB40	Ellough Road	Ellough Road a wide, residential street and one of the busier roads within the area. It contains a pavement to either side with wide grass verges between Hillside Avenue and Beccles Primary Academy. There are sporadic on-road cycle markings from Lowestoft Road to past the Castle Hill junction. Alongside the on-road section there appears to be a small section on shared pathway. This is considered a medium priority route.	A shared pathway with an element of cycle/pedestrian segregation should be sought from the junction of Lowestoft Road to the junction with Hillside Avenue with suitable toucan crossing points. Parts of this route have sufficient grass verges that likely allow required widths. The north side of the road is more constrained. Road space currently used for cycle lanes could be subsumed into any new infrastructure. It is unlikely a shared path could be achieved on both sides of the road.

Beccles to Bungay Route



Map Reference	Location	Description	Recommendation
LB25	Beccles to Shipmeadow	<p>This route to Shipmeadow is formed of footpaths and as such is not assessed under Propensity to Cycle Tool (PCT) data and there is limited use on Strava. Cyclists are not currently able to use this route hence its low numbers, however it does represent the most direct route between Beccles and Bungay which avoids the busy and ill-suited B1062.</p> <p>Strava shows the B1062 is well used despite its deficiencies whilst PCT shows modest use with high modal shift potential. However, this route currently involves using a busy, fast moving and undulating road with its length making high-quality improvements expensive. It is hoped that a route north of this will offer a viable alternative and take at least some of the cycle traffic currently using the B1062.</p> <p>The first section of this route is Bungay Road exiting from Beccles at its western extent. This road is busy where cars accelerate to high speeds. The road contains a pavement up to the entrance with Roos Hall, but this ceases further westwards and does not connect to Footpath 3. This currently limits the wider effectiveness of this route both for cyclists and pedestrians.</p> <p>Footpath 3 appears already suitable for cycling. It is partially metalled and wide enough to accommodate both pedestrians and cyclists. It appears to be used as an access for farming vehicles and its wider use for cyclists should be explored. As a farming track it is unlikely to be busy but passing bays should be considered as a potential improvement. This is a quiet, rural road so a crossing point is unlikely to be a priority although improved visibility splays could be considered.</p> <p>Unfortunately, the remainder of the footpaths are of a poorer surface quality. The first section is a grassed track which then becomes a barely defined path along the north edge of the field. The route is blocked to cyclists by stepped turnstiles. Whilst the route eventually opens up and widens, surfacing improvements would be required all the way through to Locks Lane. Locks Lane is another quiet rural road which forms the connection between Footpath 3 to Footpaths 9, 14 and 15. These footpaths form a very attractive route; however, they are again unsurfaced so require upgrading. Footpaths 9 and 14 appear relatively wide and are an un-made track used for farm vehicles. Footpath 15's width is considerably narrower due to the position of drainage ditches either side. In addition, the land becomes boggy so these considerations will impact the type of surface used.</p>	<p>A wide grass verge alongside the start of Bungay Road until the access to Footpath 3 provides opportunity for a shared path. This would have the additional benefit of connecting Footpath 27 which heads southwards. Upgrade Footpath 3 to bridleway status. Upgrade the remaining footpaths to bridleways or cycleways with improved surfacing and widening where possible to create an accessible surface. Whilst full asphalt surfacing would form the highest quality for the cyclists it is likely that a compacted surface would be more acceptable and more in keeping with the character of the area particularly as this section is within the Broads Authority area. Better wayfinding would also be required and the alteration to barriers to better allow cycling.</p>

Map Reference	Location	Description	Recommendation
		Alternatively, Footpaths 5, 6 and 8 could be used. These are more direct and avoids the boggy parts. However, these are largely set through fields (as opposed to around the edges) so surfacing may prove more difficult. This is considered a very high priority route.	
LB26	Ringsfield Road (north)	This road contains two schools so would benefit from better cycle links. It has reasonable grass verges (particularly outside Sir John Leman High School) and a relatively wide pavement on its western side until Nelson Way. Connecting the schools to a cycle path would have significant benefit. This is considered a very high priority route.	Use the grass verges to create a shared path. It is likely that any path would have to be on the western side of the road which is opposite the schools. Accordingly suitable crossing points will be needed. The road narrows outside St Benet’s Catholic Primary School so consideration could be given to bypassing this section utilising and improving existing pathways through the adjacent wooded area (The Dell).
LB27	Ringsfield Road (south)	A relatively straight, quiet but fast flowing road which has limited potential for improvement but provides a first step towards Ringsfield. Ringsfield Road leads to Bridleway 20. The northern part of the bridleway (Primrose Lane) is narrow, but quiet and metalled and a reasonable place to cycle. The southern part contains a poorer surface. This is considered a high priority route.	Wayfinding, warning signage and speed restrictions could be used to make the road a safer experience. The southern part of the bridleway could have its surface upgraded to provide better access for all cyclists. An ancient/veteran tree is adjacent this route and no improvements should damage this important biodiversity asset.
LB28	Bridleways between Ringsfield Road and Church Road	An alternative, less direct route from Beccles and Bungay which also avoids the B1062 is via public rights of way in Ringsfield and Ilkeshall St Andrew. The existing NCR between Beccles and Bungay takes a similar approach, but heads further south potentially making it even less direct, and the NCR is largely on-road. This route offers a compromise between the NCR and the other potential routes. Strava shows this route to be relatively well used although not as well used as the less direct on-road options. This is perhaps a reflection of the poor quality of the surface. Exiting from Beccles on Ringsfield Road, south of the playing field, takes you to the entrance to Bridleways 4/14/5. This creates a direct route westward; however, the surface is largely compacted dirt so would need upgrading to be attractive to road cyclists. The surface improves where it meets Lodge Farm Lane/Bridleways 6a/7. This surface is partially metalled and although not smooth so still has potential for improvements. The connection through to the next bridleway (8) requires travel along a short stretch of public highway on Church Road. This short stretch of road is likely quiet; it is relatively straight although curves where it meets Bridleway 8. There is space sufficient for a small, shared path to connect them together, but this may not meet LTN 1/20 standards. As a minimum	Whilst the route uses existing bridleways large sections require an improved surface to open them out to a wider array of cyclists, otherwise the less direct NCR is preferable. Notably bridleways 4/14/5 and 8/16 should be upgraded with a firmer, all-weather surface. Bridleways 6a/7 are more accessible but would also benefit with improvements. Where the bridleways meet the highway on Church Road the visibility splays need to allow a safe access.

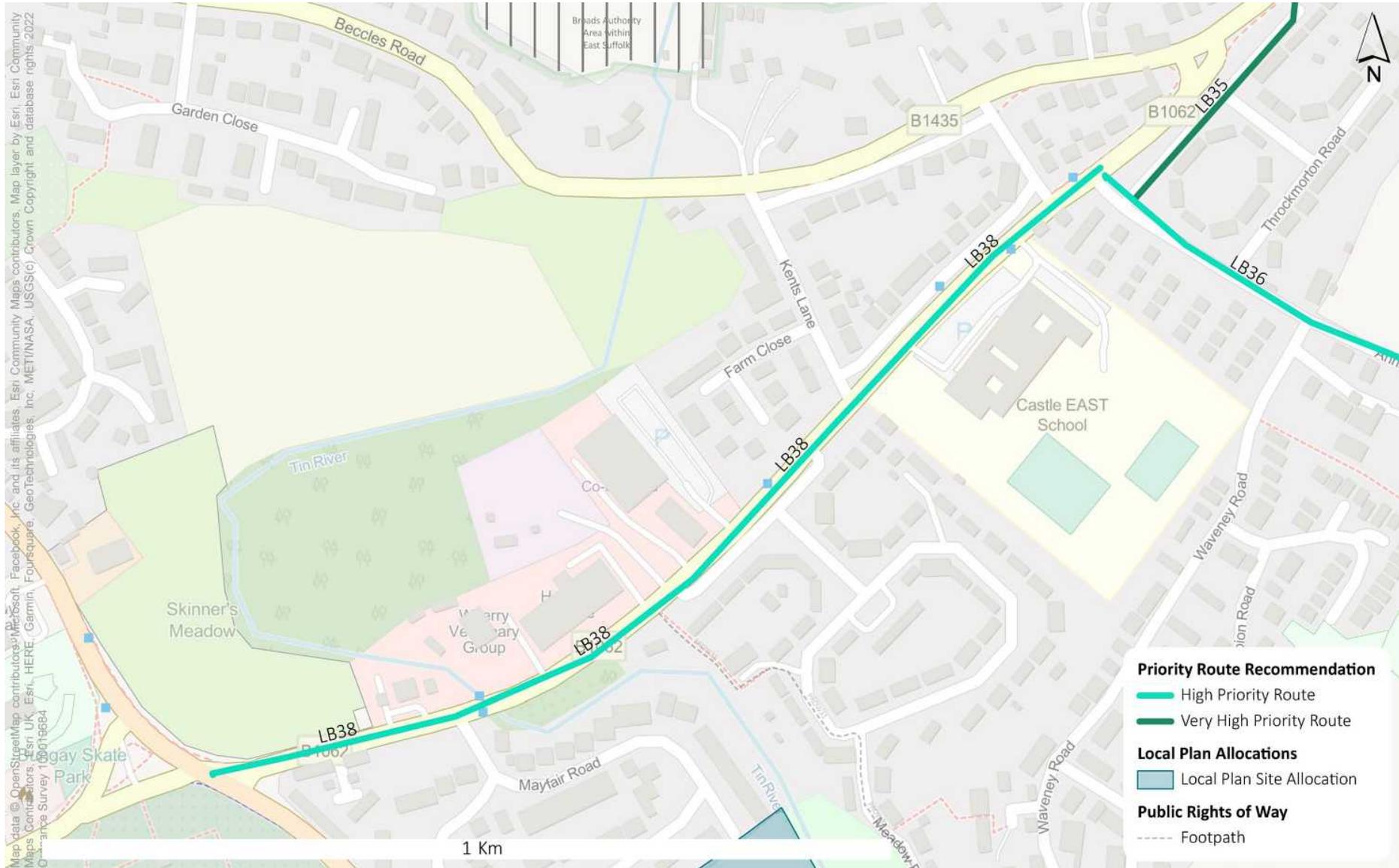
Map Reference	Location	Description	Recommendation
		visibility should be sufficient to allow safe entrance and exit onto the public highway. This is considered a high priority route.	
LB29	Ringsfield	School Road provides the main arterial route through Ringsfield. It contains a pavement for pedestrians and the road speeds reduce to 20mph outside the primary school. The pavement ceases at the school and does not extend to the entrance to Bridleway 8. This is considered a high priority route.	Whilst an off-road cycle route would be most beneficial it is unlikely that significant improvements will be viable for much of Church Road and Ringsfield Road. Along School Road the pavement stops short of the access to Bridleway 8. Extending the pavement and providing a formal crossing point will better connect Ringsfield to a potentially useful cycling and walking route. This can be achieved in conjunction with allocation WLP7.14 Land North of School Road. An ancient/veteran tree is adjacent this route and no improvements should damage this important biodiversity asset.
LB30	Bridleway 8	Bridleway 8 then connects back onto route 3. The bridleway offers an attractive route through the fields; however it is formed of an un-made surface. This means that whilst cyclists can use this path, it is unsuitable for most users. The pathway is tree lined, but its boundary with the fields becomes less distinguishable in the central section. This is considered a medium priority route.	Bridleway 8 should be resurfaced to better allow all cyclists as well as retaining its use for horses. A full hard surface may not be suitable particularly as there appears to be some farm vehicles crossing this path. However, some form of upgraded compacted surface is recommended.
LB31	Bridleways 6 to 16	Both Bridleway 8 and Bridleway 16 are of better quality at either end, however the majority of these bridleways to the centre are unmade meaning resurfacing would be beneficial. This need not be asphalt but instead a compacted surface would making the bridleway accessible to a wider range of cyclists and people with mobility issues and retain its core use for horses. These bridleways exit upon Banter's Lane which is a long, narrow rural road that leads into Ilketshall St Andrew. The road is relatively quiet and provides an attractive route. However, it is in places winding and the hedgerows either side do remove some visibility. This is considered a high priority route.	Improve the surfacing along the majority of the route. Banter's Lane is particularly winding with lessened visibility which could be improved. Opportunities to improve Banter's Lane should be explored. However, it may be that an alternative route could ultimately be deemed more appropriate.
LB32	Ilketshall St Andrew	This route brings the traveller through the village of Ilketshall St Andrew. This village is dispersed into clusters and the route skirts along the east/north boundary. Cyclists are required to use Chapel Road, Tooks Common Lane, Mill Lane and Manor Farm Road. In character these roads are rural and relatively attractive, but they do contain regular accesses through to residential properties. Chapel Road is a narrow road and closely bordered on either side by trees and hedgerows. It contains no significant bends, but visibility within its slight curves is reduced by its constrained nature. It lacks regular passing bays, and the verges and hedges mean it has little space to move off-road. Despite being within a village there is no pedestrian footpath.	It is unlikely that significant improvements will be viable given its rural nature and length. However, Strava shows this route is relatively well used so where the curves in the road provide limited visibility the turns could be widened to make safer for cyclists. Traffic calming measures could also be considered.

Map Reference	Location	Description	Recommendation
		<p>Mill Lane is similar in character to Chapel Road in that it is rural in character, narrow and with residential properties accessing off it. Whilst Mill Lane is similarly constrained in places, the majority of the road is more open with wider grass verges and shrubs as opposed to thick hedgerows. This road also contains gentle curves but again visibility is varied.</p> <p>Manor Farm Lane likewise is narrow and of a rural character, but unlike Mill Lane and Chapel Road this road lacks residential accesses. This road leaves the village northwards and becomes increasingly agricultural with wider vistas and wider, but unkept, grass verges. Visibility is better, but the same constraints do exist in places. There are a set of sharper turns central to the road. This is considered a high priority route.</p>	
LB33	Hall Lane, Footpaths 11 and 17	<p>Banters Lane represents a poorer road for cyclists to utilise due to its winding nature, accordingly an alternative could be sought. Using Hall Road to head northwards provides access to Footpath 11 leading to Footpath 17 could be one option. Footpath 11 is unmade along field edges. However there are a number of other tracks (not adopted) within this area which could also be utilised. Another option is a metalled track that services a substation which has a more solid surface offering cycling benefit. This is considered a medium priority route.</p>	<p>Upgrade both the legal status and surfacing of Footpath 11. Assess the potential for new cycle routes from Hall Lane to Clarke's Lane. Consider similar upgrade to Footpath 17 or whether the substation access could be utilised.</p>
LB34	Clarke's Lane, Byways 5, 16 and 17	<p>Whilst it is indirect this route has the benefits of using existing off-road infrastructure and provides a connection between the north (LB25 and LB35) and south (LB31 and LB32) key corridors. Strava suggests limited cyclists are currently using this route hence why this is considered as an alternative only, but this may also be due to surface quality.</p> <p>Clarke's Lane is a narrow road, rural in character that is likely to be relatively quiet in terms of vehicular traffic. To the south it is straighter and less enclosed so offers good visibility. To the north up to where it meets Bridleway 16 it becomes enclosed by hedgerows and the road becomes more winding.</p> <p>Bridleways 16 and 17 are mostly unmade and largely formed of narrow compacted dirt tracks. Whilst this will likely be acceptable to pedestrians and many off-road cyclists it will not be accessible to all.</p> <p>To the north it is possible to connect into another section of the key corridor, however, to do so does involve crossing the B1062. The B1062 is the main road connecting Beccles and Bungay and it is busy with vehicular traffic travelling at speed. The crossing point onto Lock's Lane has reasonable visibility and is positioned on a gentle slope.</p>	<p>Bridleways 5, 16 and 17 (potentially bridleway 3 also) should be resurfaced to better allow all cyclists as well as retaining its use for horses. The bridleway would need extending to Clarke's Lane as it is currently connected by a short stretch of footpath.</p> <p>An improved crossing point to the highest standard between the bridleway and Lock's Lane should be the aim as it would not only help the key corridor but also provide a benefit to existing users. However, it is recognised that the top standard such as a lighted crossing would likely be unviable given the infrequency of use and the speed of the road. As there are wide verges either side a traffic island could be considered with appropriate signage. Furthermore, to ensure people cross at the correct location and use the B1062 as little as possible a small stretch of pathway allowing the bridleway and Lock's Lane to align should be provided.</p>

Map Reference	Location	Description	Recommendation
		Lock's Lane itself is a narrow rural lane through heavily wooded areas and with limited verges. However, it is a vehicular cul-de-sac serving one dwelling, agricultural fields and a water pump so is unlikely to be busy. This is considered a medium priority route.	
LB35	Low Road	<p>Low Road provides an alternative route to the more hazardous B1062. Low Road is a quiet, narrow country lane. Strava suggests strong use on the west side of Low Road due to the connections through Mill Pool Lane to Norfolk. Propensity to Cycle Tool data suggests limited use with moderate potential for modal shift gain.</p> <p>The road surface is poorer to the east where there are significant potholes which would require repair to optimise the road's overall use. Visibility is reasonable overall although where curves in the road limit visibility these should be improved.</p> <p>This route takes you to the eastern extent of Bungay. The junction where Low Road, Wainford Road and Beccles Road meet represents a hazard and is reasonably busy with traffic movement. A formalised crossing point should be considered. The south side of Beccles Road contains a narrow pathway of low quality, but reasonably wide grass verges so the path is suitable for expansion to a shared surface. Furthermore, a quieter residential street (Bigod Road) running parallel just south of Beccles Road would offer a potential safe haven from the busier road. This is considered a very high priority route.</p>	<p>Low Road - Low Road is notably quieter than the B1062 and could benefit less confident cyclists furthermore it is not significantly less direct. However, the road to the east requires maintenance and where possible visibility increased on some of the tighter turns.</p> <p>Low Road, Wainford Road and Beccles Road junction - Junction improvements needed. This could be the addition of a crossing point on Wainford Road allowing cyclists and pedestrians to traverse Wainford Road safely onto Low Road. A formalised crossing point should be considered, this would likely require a small, shared path from Low Road northwards along Wainford Road to provide a crossing point a safe distance from the junction. A second crossing point could then be provided further west along Beccles Road to the south side of the road.</p> <p>Beccles Road contains reasonable space to allow a shared path along its south side creating a safer entrance into the town. This could then connect onto Bigod Road allowing cyclists to remain off Beccles Road for longer. Bigod Road is notably elevated above Beccles Road so would require a ramp to access the shared path. If this is not viable the route could be continued along Beccles Road instead of using Bigod Road.</p>
LB36	Castle Road/Annis Hill	<p>There are multiple ways to enter Bungay itself, the most direct of which is through Annis Hill and Strava shows this route is well used. This route uses the public highway (as opposed to bridleways) so contains better surfacing and allows for greater speed, but also the greater potential of conflict with vehicular traffic. This route passes Mettingham Castle offering unique leisure benefits.</p> <p>Castle Road/New Road is narrow and rural in character. It is an attractive road to cycle and its visibility is reasonable in most places, but gentle curves and hedgerows limit visibility in others.</p> <p>Annis Hill is likely a busier road with vehicular traffic than either Castle Road or New Road but appears less busy than the B1062 to the north and St John's Road to the south. The road is largely straight with good visibility although its form may encourage greater speeds than the more winding rural roads.</p> <p>The road is wider than Castle Road, giving more potential space to cyclists. The grass verges are relatively wide to the east and gradually narrow</p>	<p>A new cycle path alongside Annis Hill would be the highest quality improvement and should be the ambition, however this may prove unviable given its rural nature and length. Strava shows this route is relatively well used so as a minimum where the curves in the road provide limited visibility the turns could be widened to make safer for cyclists. Traffic calming measures could also be considered. An ancient/veteran tree is adjacent this route and no improvements should damage this important biodiversity asset.</p>

Map Reference	Location	Description	Recommendation
		before the entrance to Bungay itself. Once the cyclist/pedestrian enters the town, Annis Hill becomes more urban and widens considerably. This is considered a high priority route.	
LB37	Bridleways 1 and 8	<p>This route offers the benefit of remaining off-road and provides good access to Mettingham Castle. Bridleway 8 connects Castle Road to Rectory Lane. It is a relatively narrow path that gently curves northwards. It bisects a field and is lined by a high level of foliage. The path is formed of compacted dirt. Whilst it is a designated bridleway its width and dense vegetation either side would appear prohibitive to horses and the surface would also be prohibitive to casual cyclists.</p> <p>Connecting Bridleway 1 and 8 appears to be a permissive path as it is not marked on the definitive map. This may be an error as it is signposted as a county walk and appears more suitable for a public right of way than the following Bridleway 1. It is relatively wide and formed of compacted gravel and in places grass.</p> <p>Bridleway 1 bisects a field but has no vegetation acting as a boundary so is less clearly marked, and it is likely that farm traffic will cross this path. Its width is not restricted by vegetation, but by desire lines through the grass. This is considered a medium priority route.</p>	<p>Bridleways 1 and 8 should be resurfaced to better allow all cyclists as well as retaining its use for horses. Both face separate constraints which may limit the extent of resurfacing. Bridleway 8 is narrow and lined with vegetation either side. Increasing the width will likely require some removal of vegetation which will have a character and biodiversity impact. Bridleway 1 by contrast is not constrained by vegetation, but it is open to the field which presents different challenges as it should be assumed that farm vehicles will cross it and a full hard surface would be subject to heavy vehicles.</p> <p>The central section appears to be used as a public right of way but its status should be made clear. Conversely it is of a reasonable width and partially surfaced compared to the bridleways. Like the bridleways once formally made a public right of way it should have its surface upgraded.</p>

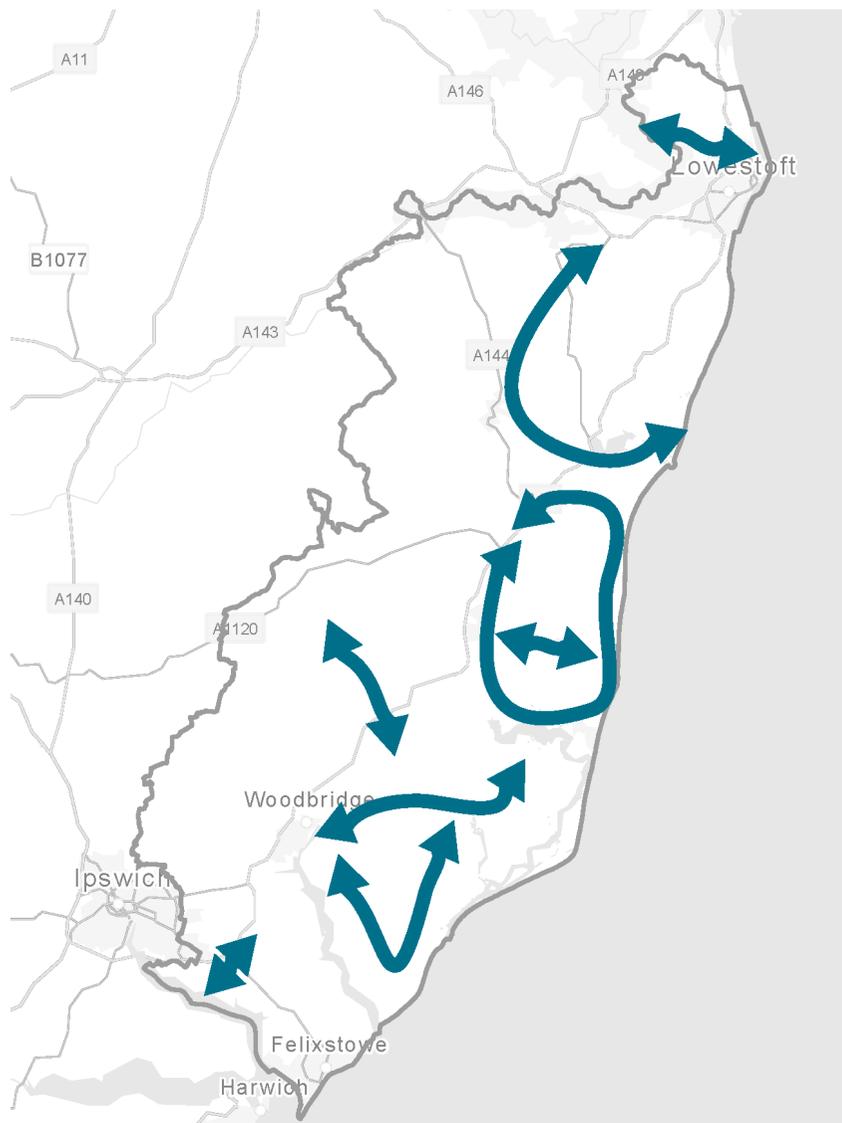
Bungay



The key corridor currently finishes at the eastern extent of Bungay, however for many travellers this will not represent the end-point so it is important to consider how cyclists or walkers could then progress to the town centre. Potential options that could be explored have been outlined below:

Map Reference	Location	Description	Recommendation
LB38	Hillside Road East	This road forms one of the key arterial routes into the town with access to the school. Along the majority of the road there exists relatively wide grass verges, but also a number of residential service roads running parallel to the main road itself. This is considered a high priority route.	Options exist here to provide a comprehensive shared path, or a less expensive approach of utilising the quiet residential service roads to allow a safer route. Hillside Road East-Hillside Road West-St John’s Hill junction – The junction is very wide and difficult to cross with 3-lanes entering/exiting the junction. Opposite is a triangular section of highway. Road space should be reallocated here with part of the triangle (the southern section) potentially closed to vehicular traffic. Suitable crossing points should also be considered. Hillside Road West - Similar to Hillside Road East its wide grass verge and quieter service road does provide potential for either a comprehensive shared path or better use of the residential streets.
<p>Northwards to the Town Centre</p> <p>Along the western side – There are a number of quiet residential roads that can be connected through wayfinding to provide an inexpensive and attractive route. Castle Lane, Castle Orchard and the connections between Castle Lane and Boyscott Lane all appear suitable cycling locations. To access these roads either the path opposite from St John’s Road to Upper Olland Street could be upgraded and utilised with suitable crossing point or the residential roads of Tower Mill Road and Laburnum Road could be used.</p> <p>St John’s Road – A relatively wide road (particularly to the south), but also relatively busy being a central arterial route through the town. It is unlikely that significant alterations could facilitate this route, potentially the one-way streets around the centre could be extended to make Upper Olland Street and St John’s Road one-way, but this would involve significant alterations to existing traffic flows.</p> <p>Beccles Road – A relatively wide road to the south that connects the eastern outskirts to the town centre. There is potential room for a shared path for the first section, but this becomes less viable further north. A path through the grass/pasture to the south of the road to join Garden Close could provide a route off the busier road. Garden Close is a quieter residential street that connects to a play area and can through this connect to Pilgrim’s Way.</p>			

Leisure Routes



4.38 The Key Corridors identified elsewhere in the Strategy are mostly focused on the more urban parts of the district. Understandably these areas present greater opportunities to enable modal shift towards more sustainable transport modes, often represent a greater benefit to cost ratio, and will be more closely tied to delivery opportunities in the form of development sites. However, consideration has also been given to cycling and walking opportunities within rural communities and, in particular, the leisure and tourism benefits that such opportunities can facilitate.

4.39 The following Leisure Routes are expanded upon in the following pages:

- Lowestoft – Somerleyton
- Halesworth – Beccles
- Halesworth – Southwold
- Darsham Railway Station – Walberswick – Southwold
- Minsmere Area
- Darsham Railway Station – Leiston
- Kelsale – Darsham Railway Station
- Benhall – Saxmundham – Kelsale
- Leiston – Saxmundham
- Leiston – Aldeburgh
- Thorpeness – Aldeburgh
- Snape – Benhall
- Aldeburgh – Snape
- Framlingham – Wickham Market Railway Station/Wickham Market
- Woodbridge – Orford – Snape
- Woodbridge – Bawdsey – Orford
- Nacton Beach – Ipswich

4.40 These leisure routes proposed within the Strategy do not comprise every cycling and walking leisure opportunity throughout East Suffolk, but have been identified through community engagement at both the informal and formal consultation stages. In addition to the community's helpful feedback the following evidence sources have been used in the creation of the Leisure Routes:

- Public Rights of Way network,
- Sustrans National Cycle Network,
- Suffolk Coast Path and the emerging England Coast Path,
- Suffolk Cycle Route,
- Suffolk Coast and Heaths AONB Walking and Cycling Guides,
- Designated and proposed Quiet Lanes,
- Various Neighbourhood Plans and neighbourhood level transport strategies and plans,
- Key tourism and leisure destinations,
- Trip data in the form of Strava Metro data,
- The quality of existing cycling and walking routes, and
- ESC and SCC officer feedback.

4.41 It is important to recognise that the detailed design of the Leisure Routes will need to be sensitive to their natural and historic contexts, which is often the very reason the routes are popular. Particular attention will need to be paid to the need for and detailed design of surfacing and lighting solutions.

LR1 Lowestoft – Somerleyton

4.42 This route seeks to provide a cycling and walking route between Lowestoft and Somerleyton along traffic free or low traffic routes as far as possible, with potential to connect into Blundeston. Waddling Lane provides a good traffic free route between Somerleyton and Flixton Marsh Lane. However, challenges exist beyond Waddling Lane in creating a cycling and walking route east of Oulton and Lowestoft whilst avoiding adverse effects on the environmental quality of the area and avoiding cycling and walking along the B1074. In taking forward the recommendation consideration of and consistency with the Lound with Ashby, Herringfleet and Somerleyton Neighbourhood Plan, and emerging Oulton Neighbourhood Plan will need to be demonstrated. Consideration will need to be given to ensuring the route is sensitive to the Broads National Park, Flixton Decoy, Flixton Decoy Meadows, and Blundeston Prison Lake and Woods County Wildlife Sites.

LR1.1

Resurface Waddling Lane with a smooth hard surface that avoids harm to the qualities of the area.

LR1.2

Introducing a cycling and walking route between Flixton Marsh Lane and Oulton, connecting into the Woods Meadow development and/or the WLP2.14 and WLP2.15 Oulton site allocations, that avoids the B1074 while also avoiding harm to Flixton Decoy should be further explored.

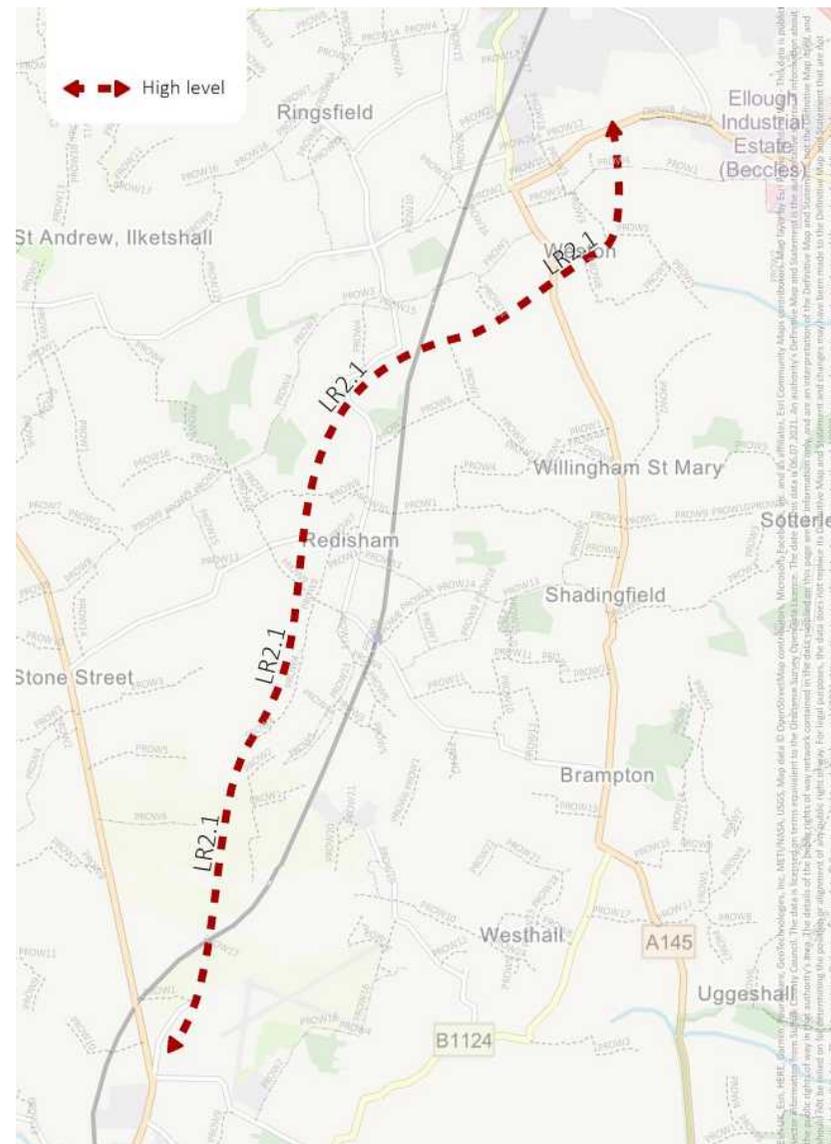


LR2 Halesworth – Beccles

4.43 This route seeks to provide a cycling and walking link between Halesworth and Beccles, and the wider Lowestoft to Bungay Key Corridor. In taking forward the recommendation consideration of and consistency with the Beccles Neighbourhood Plan, emerging Halesworth Neighbourhood Plan, emerging Shadingfield, Sotterley, Willingham and Ellough Neighbourhood Plan, and emerging Mettingham, Barsham, Shipmeadow, Ringsfield and Weston Neighbourhood Plan will need to be demonstrated. Consideration will also need to be given to ensuring the route is sensitive to the Halesworth Conservation Area, Fairview Farm Meadow County Wildlife Site (CWS), Moat Yards Scheduled Monument, Sparrow’s Thicks CWS and Ancient Woodland, Weston Crossing Meadow CWS, Weston Crossing Railway Line CWS, Great Wood CWS and Ancient Woodland, Titsal Wood Site of Special Scientific Interest and Ancient Woodland, and Beccles Conservation Area.

LR2.1

Introduce a cycling and walking route between Halesworth and Beccles, making best use of existing Public Rights of Way and Quiet Lanes, including Butts Road and Wangford Road and the proposed Quiet Lanes of King’s Lane, Church Lane and Cucumber Lane, throughout the area and connecting into Brampton Railway Station where possible.



LR3 Halesworth – Southwold

- 4.44 This route seeks to connect Halesworth and Southwold via an attractive cycling and walking route. This route will have the opportunity to connect the parishes of Blythburgh, Wenhaston and Walberswick via a combination of on-road and off-road cycling and walking. In taking forward the recommendation consideration of and consistency with the emerging Halesworth Neighbourhood Plan, Wenhaston Neighbourhood Plan, and Southwold Neighbourhood Plan will need to be demonstrated. Consideration will need to be given to ensuring the route is sensitive to the Mill Heath County Wildlife Site (CWS), Bicker’s Heath CWS, the Area of Outstanding Natural Beauty, Minsmere-Walberswick Ramsar, SPA, SSSI, and SAC, Suffolk Coast National Nature Reserve, Outer Thames Estuary SPA, Big Common and Haw Woods CWS, and Walberswick Conservation Area.

LR3.1

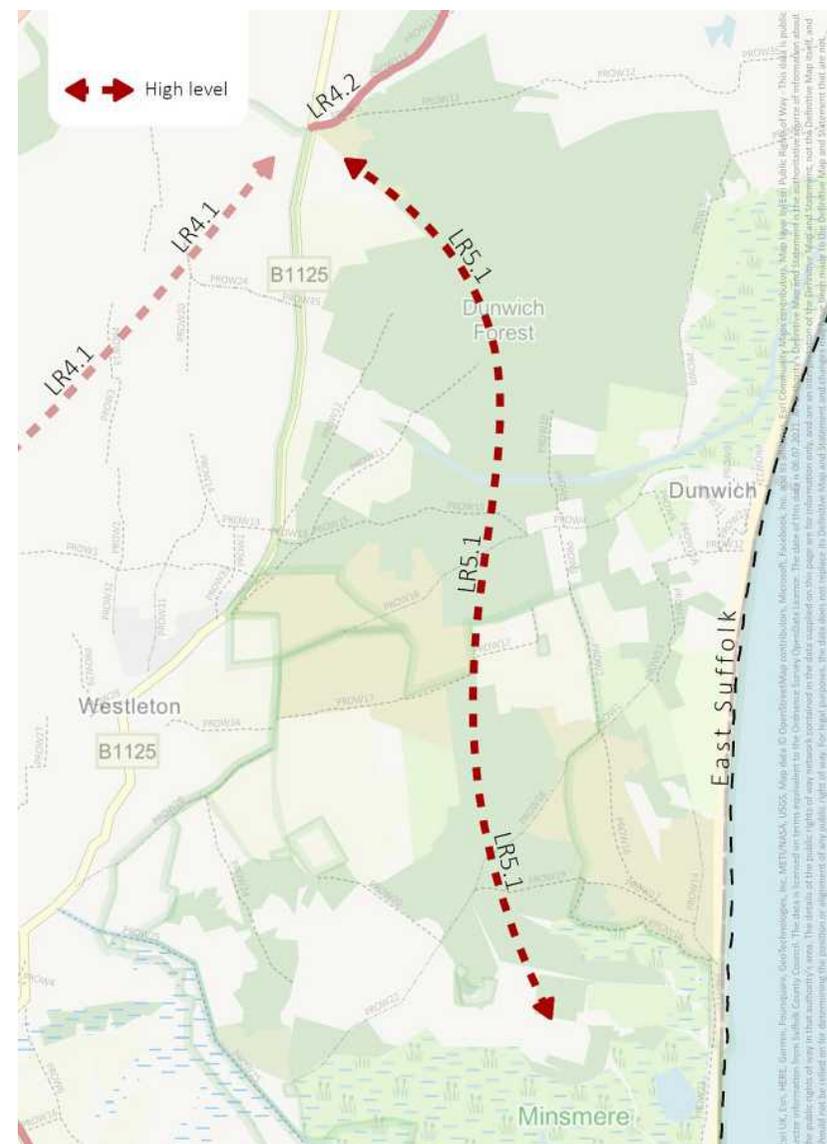
Introduce a cycling and walking route between Halesworth and Southwold, via Wenhaston and Blythburgh. National Cycle Route 1 provides a high-quality cycling and walking route between Halesworth Millennium Green and Heath Road. Heath Road offers an attractive and low traffic volume route between Halesworth and Wenhaston. Further consideration should be given to cycling and walking infrastructure opportunities between Wenhaston and Blythburgh, paying particular attention to safely crossing the A12. Public Rights of Way, including Bridleways 1 (Blythburgh), 9 (Blythburgh), 28 (Walberswick), 29 (Walberswick), 5 (Walberswick), and 25 (Southwold), offer opportunities and connecting these in a comprehensive route should be further explored.

LR5 Minsmere Area

4.46 This route seeks to facilitate walking and cycling opportunities within the Minsmere area that will not have a harmful impact on the sensitive landscape. Consideration will also need to be given to ensuring the route is sensitive to the AONB, Minsmere-Walberswick Ramsar, SPA, SAC, and SSSI, Dunwich Conservation Area, Greyfriars Wood CWS and Ancient Woodland, Greyfriars Scheduled Monument, Dunwich Valley Woods and Grassland CWS, Dunwich Forest CWS, Westleton Heath National Nature Reserve, Pottton Hall Fields CWS, and Suffolk Coast National Nature Reserve.

LR5.1

Due to the sensitive landscape throughout this area consideration should be given to making best use of existing Suffolk Coast and Heaths AONB walking and cycling guides, Public Rights of Way and low traffic roads, rather than the creation of new cycling and walking routes.



LR6 Darsham Railway Station – Leiston

4.47 This route seeks to provide a traffic free cycling and walking route between Darsham Railway Station and Leiston, making best use of the infrastructure to be delivered by Sizewell C. In taking forward the recommendation consideration of and consistency with the Leiston Neighbourhood Plan and Leiston Transport Strategy⁶¹ will need to be demonstrated. Consideration will need to be given to ensuring the route is sensitive to Yoxford Conservation Area, Rookery Park Historic Parkland, Cockfield Hall Historic Parkland, Minsmere Valley Reckford Bridge to Beveriche Manor CWS, Leiston Abbey Scheduled Monument and Listed Buildings, Minsmere-Walberswick Heaths and Marshes SSSI, SPA, Ramsar, SAC, the AONB, and Heritage Coast.

LR6.1

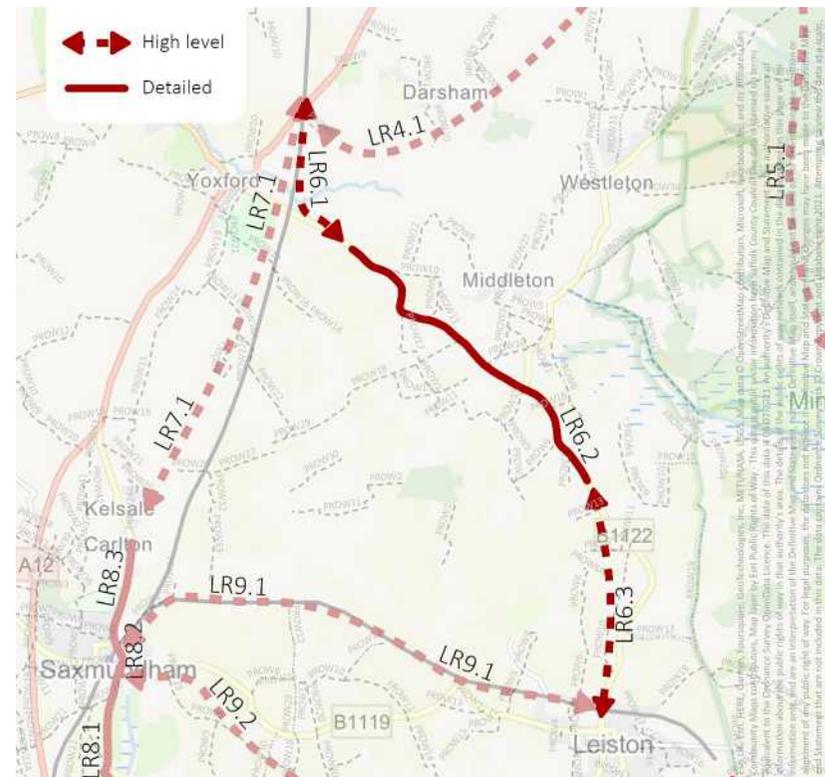
Opportunities to connect the recommended repurposed B1122 (LR6.2) to Darsham Railway Station to the north, as well as connecting to the villages of Middleton and Kelsale, should be further explored.

LR6.2

The B1122 should be repurposed to provide a comprehensive cycling and walking route between the proposed northern and southern B1122/Sizewell Link Road junctions. Opportunities to limit vehicular access along the repurposed B1122 should be considered, with potential for vehicular access to be permitted for residents, refuse collection, and emergency vehicles only, through the use of modal filters.

LR6.3

Opportunities to connect the recommended repurposed B1122 (LR6.2) and Leiston to the south, as well as to the villages of Theberton and Eastbridge, should be further explored.



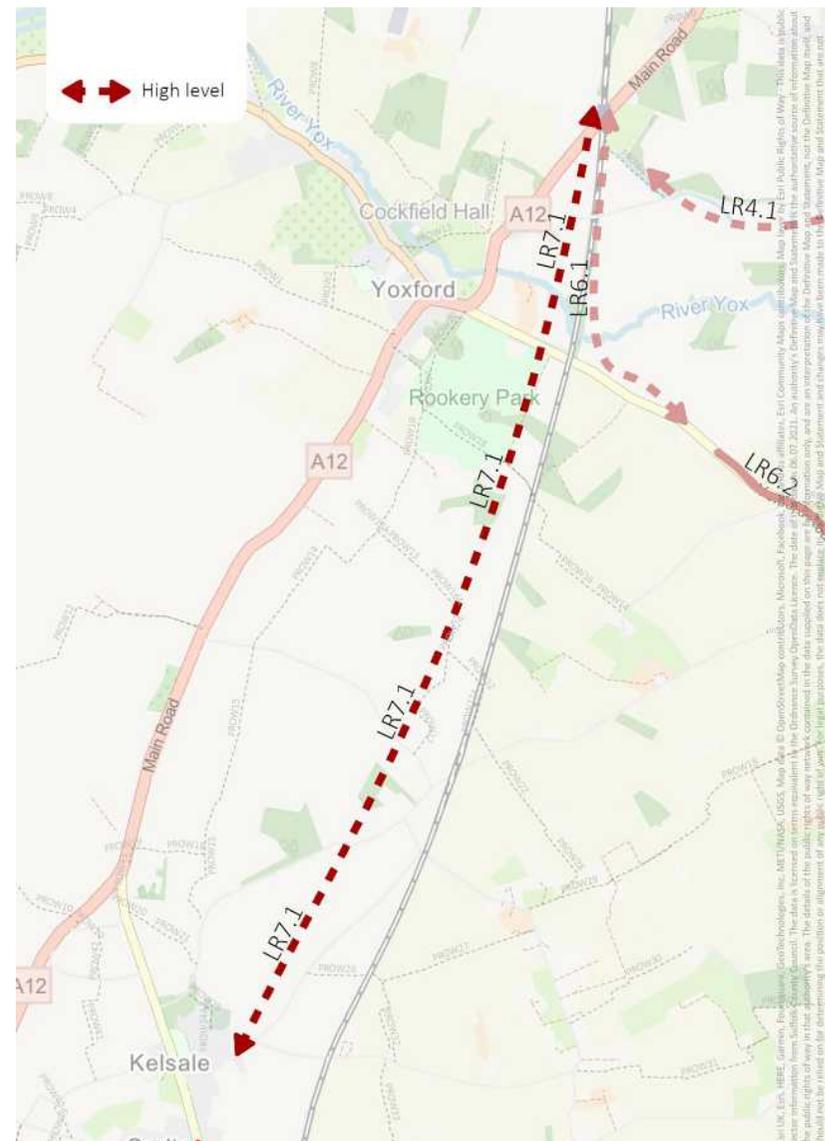
⁶¹ <https://www.leistontowncouncil.gov.uk/leistontransportstrategy/>

LR7 Kelsale – Darsham Railway Station

4.48 This route seeks to provide a low traffic route between Kelsale and Darsham Railway Station and connect to other leisure routes. Consideration will need to be given to ensuring the route is sensitive to the Yoxford Conservation Area, the three Historic Parks that encompass Yoxford (Rookery Park, Grove Park, and Cockfield Hall Park), and Minsmere Valley County Wildlife Site.

LR7.1

Introduce a cycling and walking route between Kelsale and Darsham Railway Station. Quiet Lanes, including those of Butchers Road, North Green, and Town Farm Lane, and Public Rights of Way offer opportunities. Connecting these in the form of a comprehensive route should be further explored. Consideration should be given to upgrading Footpaths 15, 14 (Kelsale cum Carlton), 19, and 20 (Yoxford) to bridleways, and widening and resurfacing accordingly. Consideration should also be given to an alternative option; a cycling and walking track alongside the railway line between Saxmundham and Darsham Railway Stations.



LR8 Benhall – Saxmundham – Kelsale

4.49 The route seeks to provide a traffic free (as far as possible) cycling and walking route between Benhall, Saxmundham, and Kelsale. The community of Benhall, Saxmundham and Kelsale has planned such a route (The 3 Communities Link) and this is broadly followed in the following recommendations. In taking forward the recommendations consideration of and consistency with the emerging Saxmundham Neighbourhood Plan will need to be demonstrated. Consideration will need to be given to ensuring the route is sensitive to Saxmundham Conservation Area, the various listed buildings along the route, and Carlton Park Historic Parkland.

LR8.1

Introduce a cycling and walking track along the B1121, segregated from the road by the existing hedgerow, and upgrade Footpath 19 to a bridleway, widen and resurface accordingly. Introduce a cycling and pedestrian crossing point on the B1121 to access School Lane. Make School Lane one way for vehicles, while retaining two way movement for pedestrians, cyclists and equestrians, and extend the School Safety Zone along the entire length of School Lane to reduce the speed limit from the current 60mph to 20mph. Alternatively, introduce a modal filter to School Lane and a turning place on either side of the modal filter.

LR8.2

Consider reallocating road space along the High Street and North Entrance for cycling and walking.

LR8.3

Introduce a segregated cycling and walking track along the B1121 to Kelsale and introduce a connection onto Low Road.



LR9 Leiston – Saxmundham

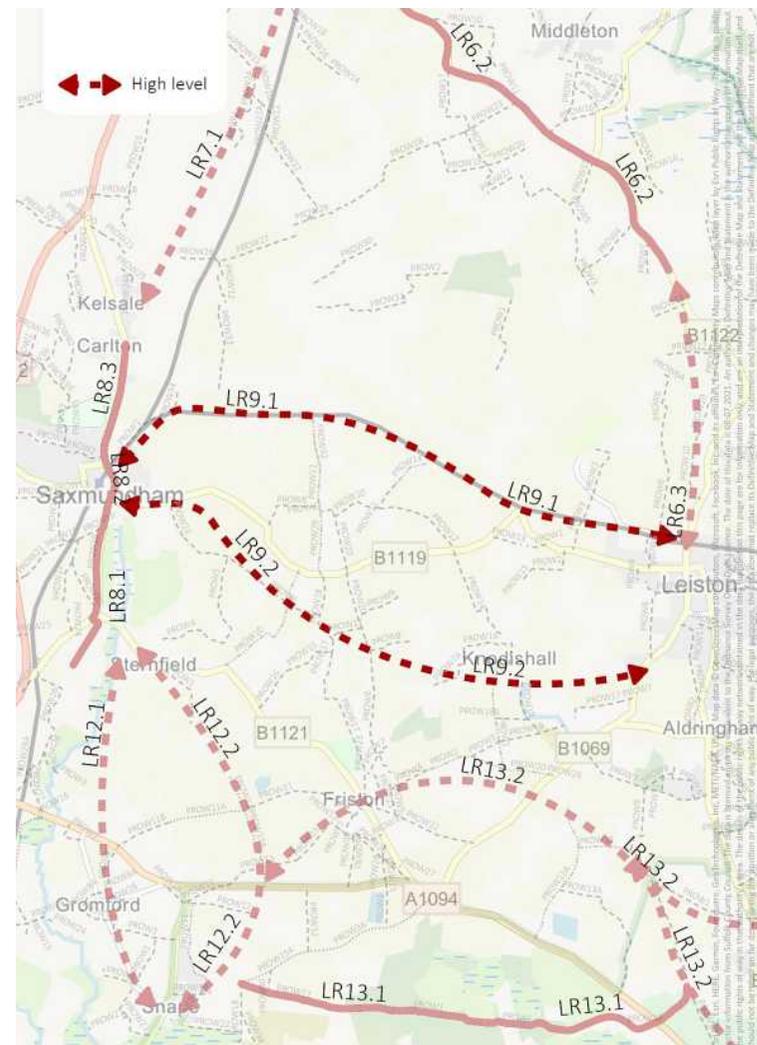
4.50 This route seeks to create a link between Saxmundham and Leiston for walking and cycling that avoids the B1119 as far as possible. The existing public rights of way network between Saxmundham and Leiston predominately run north to south and therefore offer little opportunity to create a suitable comprehensive route between the two towns. In taking forward the recommendation consideration of and consistency with the emerging Saxmundham Neighbourhood Plan, Leiston Neighbourhood Plan and Leiston Transport Strategy⁶² will need to be demonstrated.

LR9.1

Introduce a cycling and walking route between Leiston and Saxmundham. Consideration should be given to introducing a cycling and walking track alongside the railway line as this could potentially provide a direct route into the heart of Saxmundham and Saxmundham High Street and be traffic free throughout.

LR9.2

A cycling and walking route arcing to the south could be introduced. Such a route between the two towns should make use of the existing smaller roads and public rights of way. Opportunities to make use of Footpaths 6 (Sternfield) and 23 (Saxmundham) should be further explored. Cycling and walking along the B1119 should be avoided where possible as this road is currently unsuitable for walking and cycling.



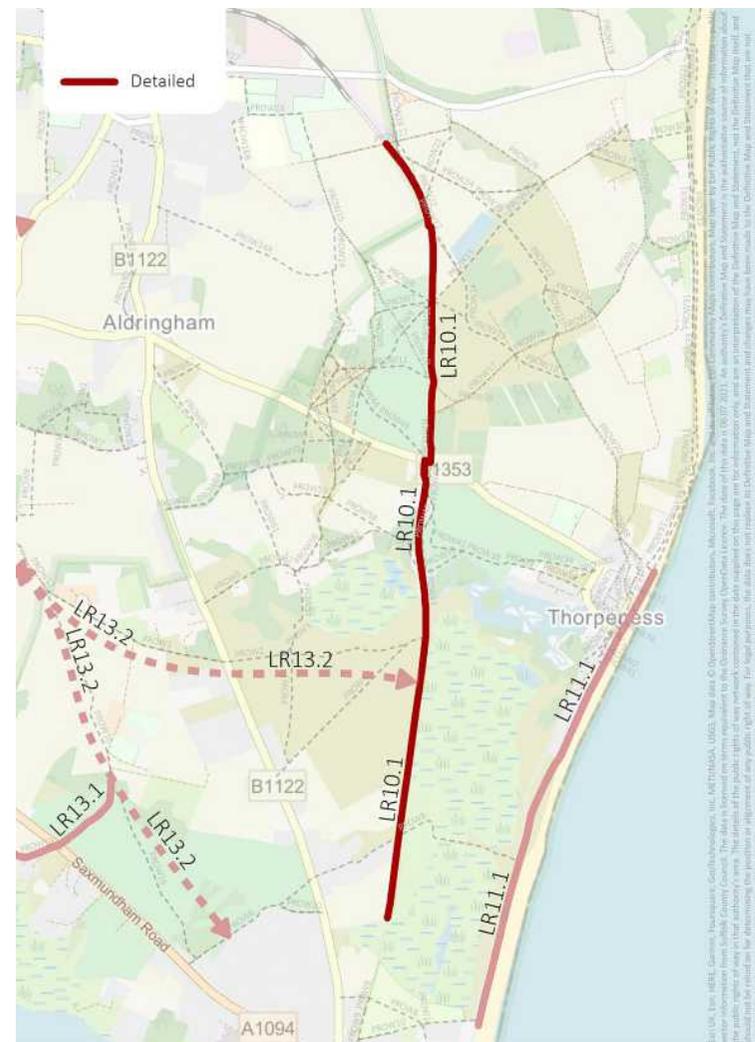
⁶² <https://www.leistontowncouncil.gov.uk/leistontransportstrategy/>

LR10 Leiston – Aldeburgh

4.51 This route seeks to provide a predominantly traffic free cycling and walking connection between Aldeburgh and Leiston. In taking forward the recommendation consideration of and consistency with the emerging Aldringham Cum Thorpe Neighbourhood Plan, Leiston Neighbourhood Plan and Leiston Transport Strategy⁶³ will need to be demonstrated. Consideration will need to be given to ensuring the route is sensitive to the AONB, Aldringham to Aldeburgh Disused Railway Line CWS, Leiston-Aldeburgh SSSI, and Sandlings SPA.

LR10.1

Re-purpose the existing disused railway line to create a cycling and walking route between Aldeburgh and Leiston, whilst avoiding loss of and harm to existing vegetation where possible. This area benefits from a good public rights of way network that link to the former railway line, which present opportunities for further cycling and walking routes.



⁶³ <https://www.leistoncouncil.gov.uk/leistontransportstrategy/>

LR11 Thorpeness – Aldeburgh

4.52 This route aims to create an attractive cycling and walking route between Aldeburgh and Thorpeness situated close to the beach. There is an existing path that runs from Aldeburgh to the edge of the Aldringham-cum-Thorpe parish boundary. Further north of this path it is unsuitable for cycling due to the uneven terrain. Cyclists instead use Thorpe Road which while of a low traffic nature can be busy in peak tourist season and has a national speed limit and is therefore not suitable for cycling. In taking forward the recommendation consideration of and consistency with the emerging Aldringham-Cum-Thorpe Neighbourhood Plan will need to be demonstrated. Consideration will need to be given to ensuring the route is sensitive to the AONB, The Haven Local Nature Reserve, Leiston-Aldeburgh SSSI, Suffolk Shingles Beeches CWS, Thorpeness Conservation Area, and the Outer Thames Estuary SPA. Consideration will also need to be given to ensuring the below potential infrastructure solutions are appropriate in respect of matters of coastal change.

LR11.1

In improving cycling and walking between Aldeburgh and Thorpeness a number of solutions are possible. Potential solutions include:

- Resurface and extend the existing shared path to Thorpeness to accommodate both cycling and walking.
- Reduce the speed limit on Thorpe Road to facilitate safe on road cycling.
- Introduce a modal filter on Thorpe Road to restrict vehicle through traffic but allow cycling and walking. Consideration will need to be given to the suitability of this alternative recommendation in light of the resultant vehicle diversion between Aldeburgh and Thorpeness.



LR12 Snape – Benhall

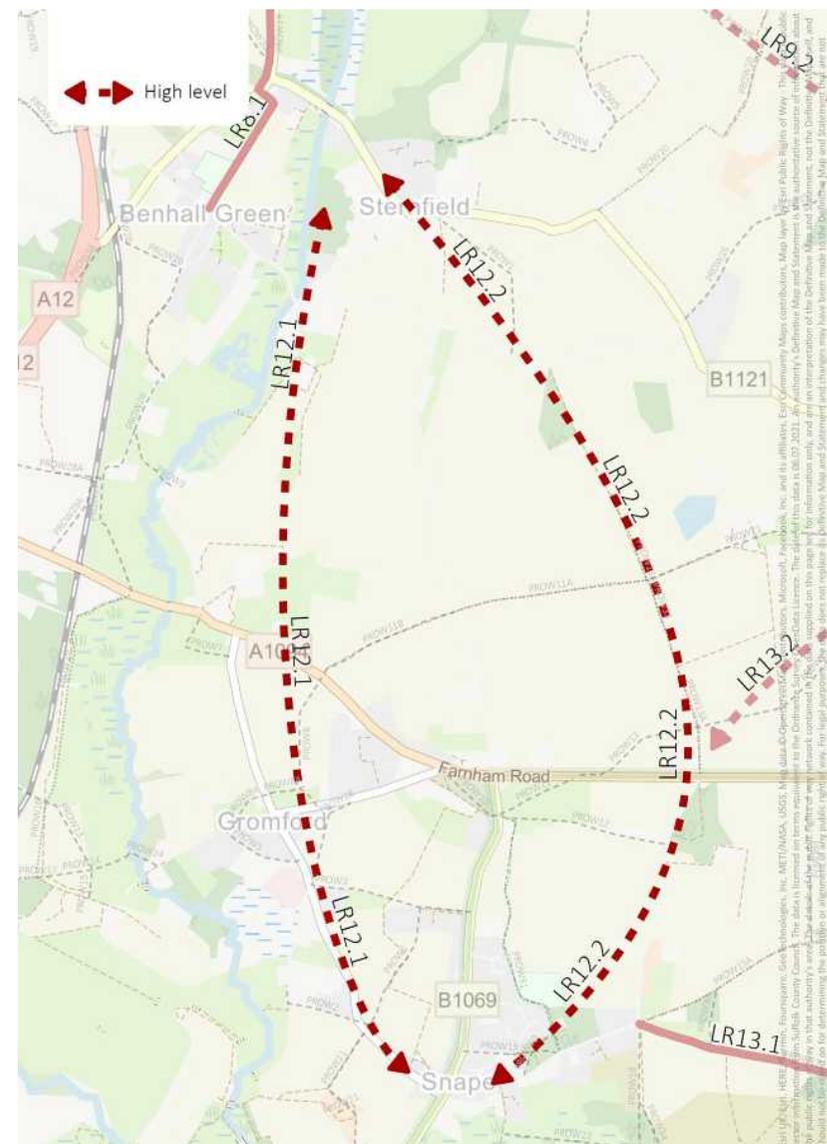
4.53 This route seeks to link Snape Maltings, the internationally renowned music and cultural venue, to the routes planned between Benhall and Kelsale, and Aldeburgh and Snape. Safely crossing the A1094 presents challenges to a safe cycling and walking route. Consideration will need to be given to ensuring the route is sensitive to the AONB, Heritage Coast, Manor Farm Meadows CWS, and Benhall Green Meadows CWS.

LR12.1

Introduce a cycling and walking route between Snape and Benhall. Quiet Lanes, including those of Hulver Lane, Wadd Lane, and Gromford Lane, and Public Rights of Way offer opportunities and connecting these in the form of a comprehensive route should be further explored.

LR12.2

Introduce a cycling and walking route between Snape and Benhall. A route arcing to the east of Benhall presents opportunities. Priory Lane Quiet Lane, Restricted Byway 13A and Redbarn Lane offer low traffic and traffic free opportunities and connecting these in the form of a comprehensive route should be further explored.



LR13 Aldeburgh – Snape

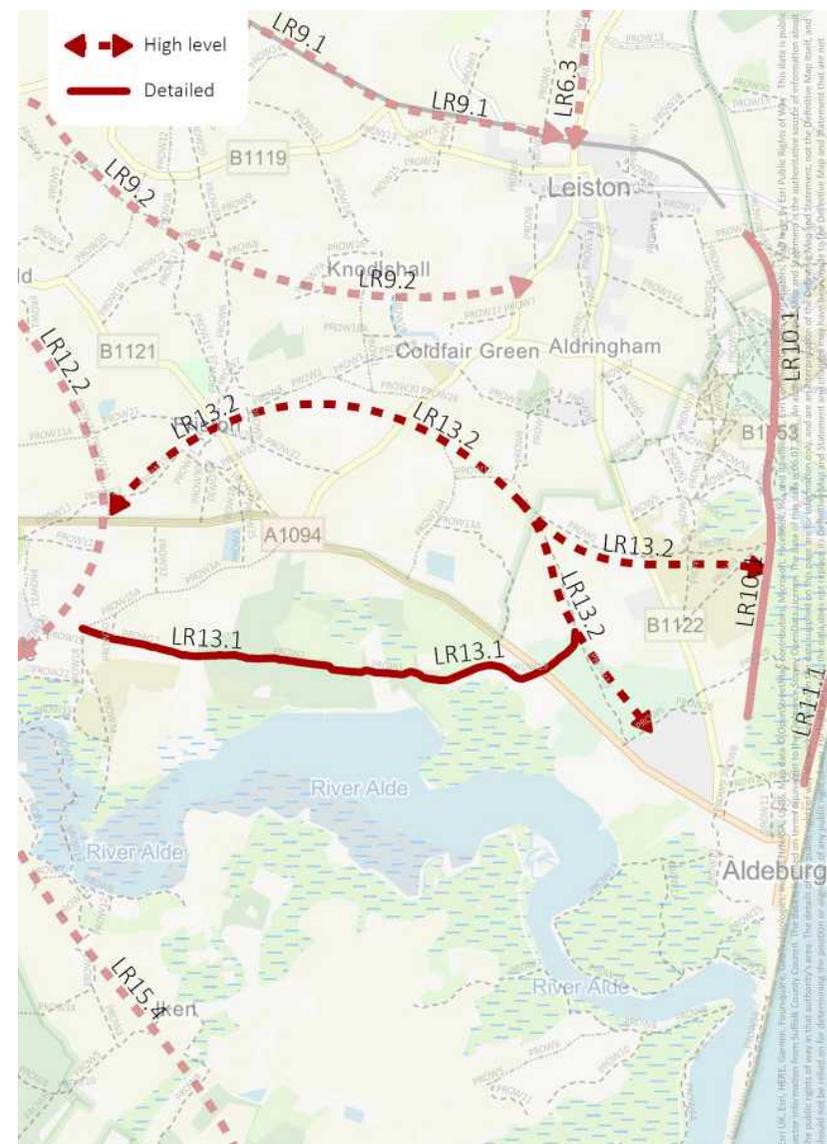
4.54 This route aims to create a cycling and walking connection between Aldeburgh and Snape, making best use of the Sailor’s Path (Footpaths 19 [Aldeburgh], 1 [Friston], and 17 [Snape]) whilst acknowledging the walking only nature of the Path. One of the main barriers to this route, for cycling in particular, is crossing the A1094, which is highly unsuitable for cycling and walking. Consideration will need to be given to ensuring the route is sensitive to the AONB, Aldeburgh Golf Course CWS, Alde-Ore Estuary Ramsar, SPA, SSSI, and SAC, Snape Warren SSSI, Sandlings SPA, Snape Marshes CWS, and Snape Conservation Area.

LR13.1

Consideration should be given to the possibility of upgrading Sailor’s Path (Footpaths 19 [Aldeburgh], 1 [Friston], and 17 [Snape]) to a bridleway status, making the necessary improvements in order to accommodate walking and cycling and crossing the A1094.

LR13.2

An alternative option to LR13.1 is to retain the Sailor’s Path as a walking route, and a cycling route connecting Aldeburgh and Snape via Friston should be introduced. Public Rights of Way, including Bridleways 2, 20, 12A, 12 (Friston), and Footpaths 16, 1 and 2 (Aldeburgh), and Priory Road Quiet Lane offer opportunities and connecting these in the form of a comprehensive route should be further explored.

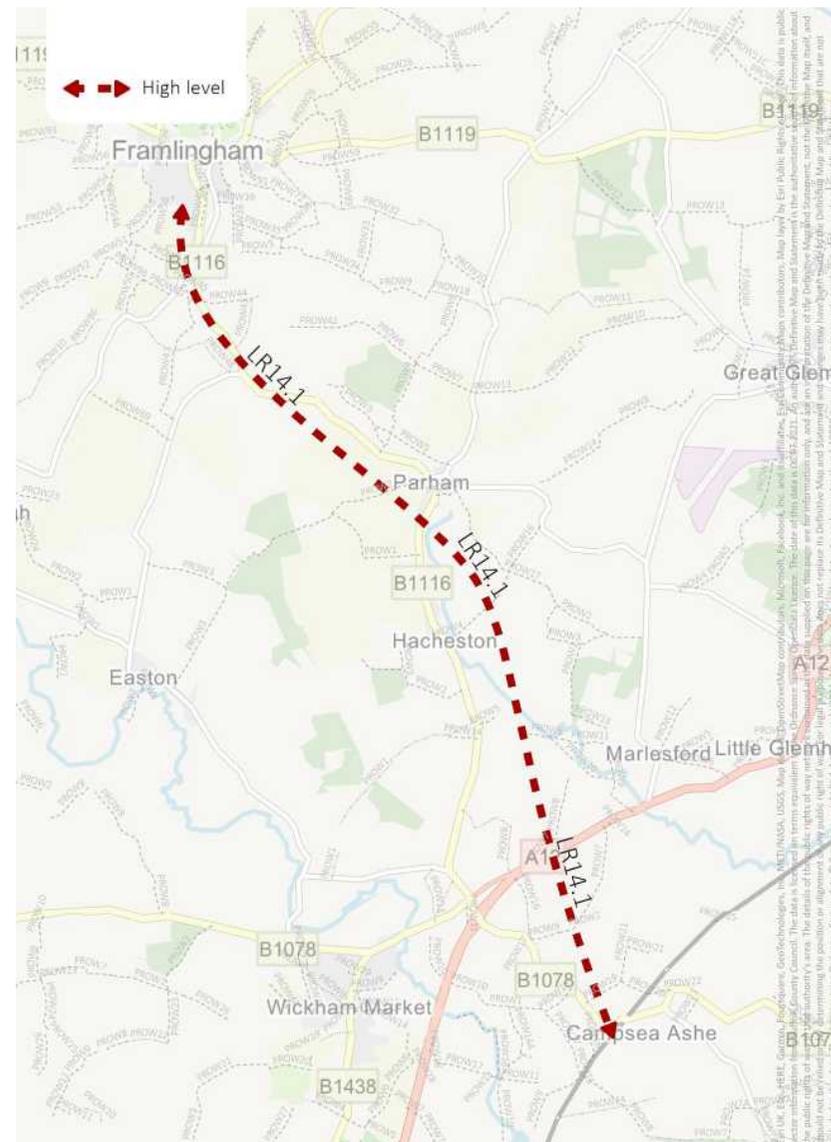


LR14 Framlingham – Wickham Market railway station/Wickham Market

4.55 The Framlingham Branch Line was a passenger and freight rail service between Wickham Market Railway Station and Framlingham Railway Station. The Line operated between 1859 and 1952 for passengers, and 1963 for freight, with stops at Marlesford, Hacheston and Parham. In taking forward the recommendation consideration of and consistency with the Framlingham Neighbourhood Plan, emerging Easton Neighbourhood Plan, and emerging Wickham Market Neighbourhood Plan will need to be demonstrated. Consideration will need to be given to ensuring the route is sensitive to Parham Woods Ancient Woodland and County Wildlife Site, Maids/Brockley Woods Ancient Woodland and County Wildlife Site, Glevering Hall Historic Park, Great Wood Ancient Woodland and County Wildlife Site, Catt’s Wood Ancient Woodland and County Wildlife Site, Marlesford Hall Historic Park, Marlesford Conservation Area, and Wickham Market Conservation Area.

LR14.1

Introduce a cycling and walking track along the route of the dismantled Framlingham Branch Line. It will not be possible to follow the exact route of the Branch Line, particularly where physical barriers exist, to avoid unnecessary harm to environmental designations, to avoid flood risk areas, and/or to connect the route into Wickham Market and other leisure opportunities.



LR15 Woodbridge – Orford – Snape

4.56 This route seeks to link the settlements of Woodbridge, Orford, and Snape through off road and low traffic cycling and walking routes. The Ipswich to Melton Key Corridor seeks to provide segregated and low traffic cycling and walking routes between Ipswich and Melton, connecting to Sutton Hoo and Common Lane Quiet Lane in the east. The Ipswich to Melton Key Corridor therefore provides a comprehensive starting point for cycling and walking to Orford and Snape. In taking forward the recommendation consideration of and consistency with the Rendlesham Neighbourhood Plan will need to be demonstrated. Consideration will need to be given to ensuring the route is sensitive to all affected natural and historic environment designations, including: the Deben Estuary Ramsar, SPA, and SSSI, Bromeswell Green CWS, Bromeswell Conservation Area, the AONB, RAF Bentwaters CWS, Sandlings SPA, Sandlings Forest SSSI, Staverton Lake Marshes and Alder Carr CWS, Tunstall Forest CWS, Alde-Ore Estuary Ramsar, SPA, SSSI, and SAC, Sudbourne Historic Park, Sudbourne Park Pit SSSI, Orford Conservation Area, Watling and Oakyard Woods Ancient Woodland and CWS, Tunstall Forest CWS, Iken Wood Ancient Woodland and SSSI, Captain’s Wood Ancient Woodland and CWS, Sudbourne Great Woods Ancient Woodland and CWS, and Snape Conservation Area.

LR15.1

Introduce a cycling and walking route that utilises designated and proposed Quiet Lanes and the PROW network between Melton and Orford. These could include Common Lane, Summer Lane, Ufford Road, Castle Hill, Low Road, White Womans Lane, and Cracks Lane Quiet Lanes. The route could also follow the continuous Bridleway along the south of Bentwaters Park (Bridleways 8, 6 [Rendlesham], 32, and 33 [Wantisden]) as well as, upgraded to a bridleway and resurfaced, the continuous footpath between Chillesford and Orford (Footpaths 22, 21, 16 [Chillesford], 18, and 3 [Orford]).

LR15.2

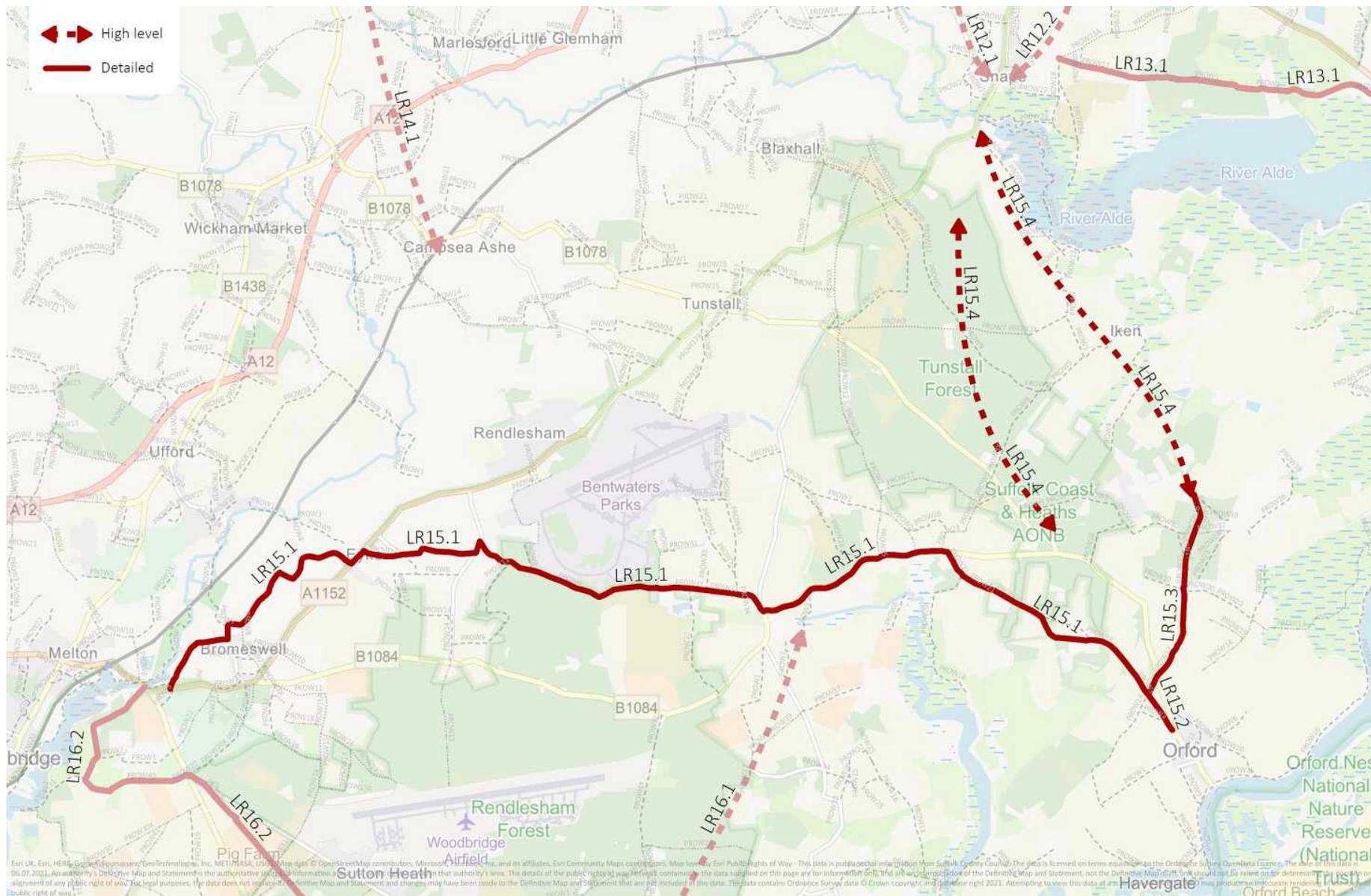
Upgrade Footpath 3 and 14 (Orford) to bridleways and resurface to accommodate cyclists.

LR15.3

Resurface Bridleways 17 (Orford), 16, 25, 26, 40 (Sudbourne) to accommodate cyclists.

LR15.4

Consideration should be given to the opportunities to enhance the cycling and walking routes within Tunstall Forest, and new routes through Sudbourne and Iken to connect into a wider Woodbridge-Orford-Snape route.



LR16 Woodbridge – Bawdsey – Orford

4.57 This route seeks to connect Woodbridge, Bawdsey and Orford for cycling and walking, utilising traffic free routes as far as possible. The Ipswich to Melton Key Corridor seeks to provide segregated and low traffic cycling and walking routes between Ipswich and Melton, connecting to Sutton Hoo and Common Lane Quiet Lane in the east. The Ipswich to Melton Key Corridor therefore provides a comprehensive starting point for cycling and walking to Bawdsey and Orford. Consideration will need to be given to ensuring the route is sensitive to the AONB, Deben Estuary Ramsar, SPA, and SSSI, Bromeswell Golf Course CWS, Rendlesham Forest CWS, SPA, and SSSI, Sandlings SPA, Sutton and Hollesley Heaths SSSI, Crag Pit SSSI, Henge and associated barrow cemetery south of Home Whin Farm Scheduled Monument, Shottisham Heath CWS, Shottisham Conservation Area, Bawdsey Manor Historic Park, Sandlings Forest SSSI, Oak Wood Ancient Woodland and CWS, Water Wood Ancient Woodland and CWS, Carmen’s Wood Ancient Woodland and CWS, Alde-Ore Estuary Ramsar, SPA, SSSI, and SAC, Neutral Farm Pit SSSI, Chillesford Marsh CWS, Sudbourne Historic Park, Sudbourne Park Pit SSSI, and Orford Conservation Area.

LR16.1

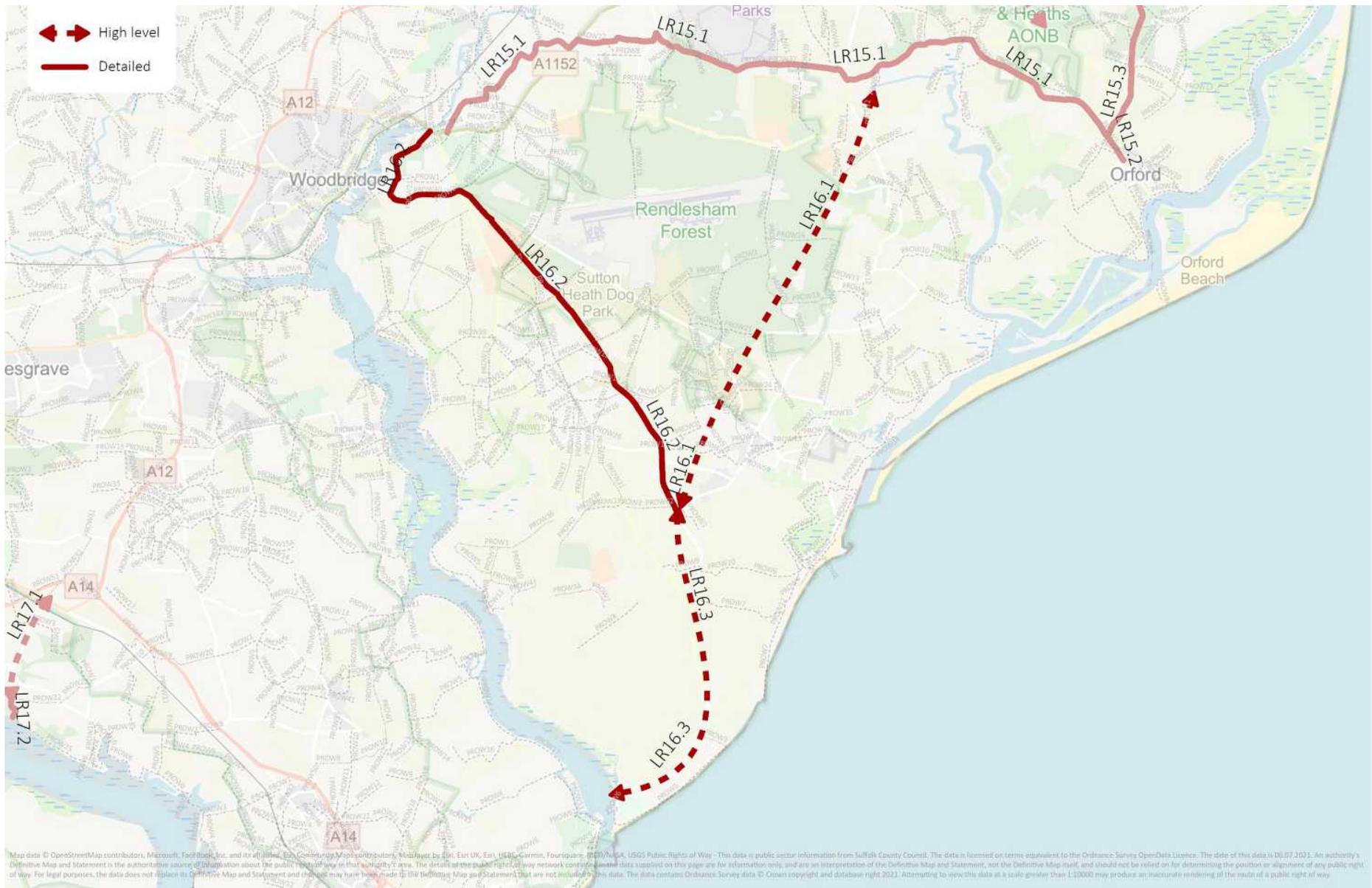
Connecting from the Ipswich to Melton Key Corridor recommendations around Sutton Hoo, consideration should be given to creating a traffic free cycling and walking route utilising the dense PROW network around Sutton, Shottisham, and Hollesley. The direct route along Bridleway 6 (Sutton) and Byway 1 (Sutton) offers a desirable traffic free route.

LR16.2

Opportunities to introduce traffic free cycling and walking routes between the Sutton, Shottisham, Hollesley dense PROW network and Bawdsey are severely limited due to flood risk and environmental constraints, and thus use of the B1083 and Ferry Road is the only realistic route for cyclists and pedestrians.

LR16.3

Introduce a low traffic and traffic free cycling and walking route between Chillesford and Orford. Consideration should be given to the route along Mill Lane Quiet Lane, and the continuous footpath between Chillesford and Orford (Footpaths 22, 21, 16 [Chillesford], 18, and 3 [Orford]).



LR17 Nacton Beach – Ipswich

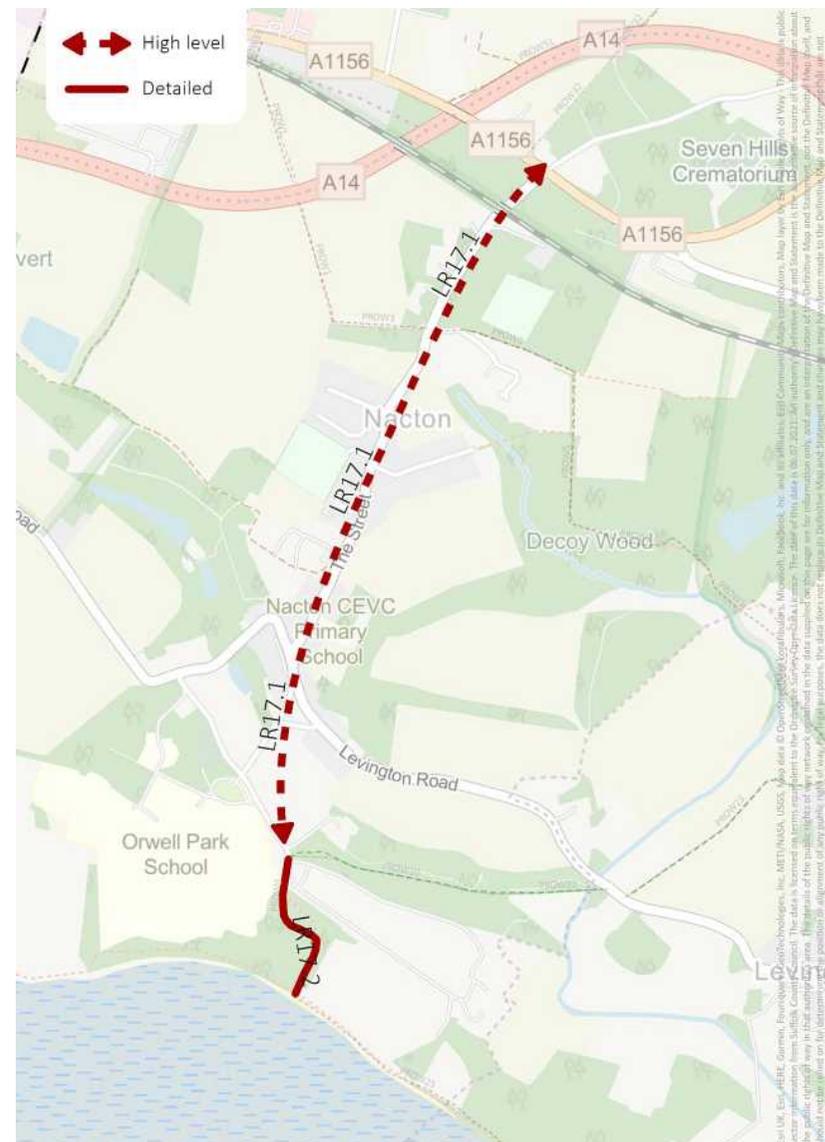
4.58 This route seeks to link the urban area of Ipswich to the scenic beauty and leisure opportunities of the river Orwell. Consideration will need to be given to ensuring the route is sensitive to the AONB, the Historic Parks of Orwell Park and Broke Hall Park, and the Stour and Orwell Estuaries Ramsar, SPA and SSSI.

LR17.1

Upgrade Footpath 9 to a bridleway to allow cycling. Footpath 9 has a hard smooth surface and so does not require resurfacing or widening to accommodate cyclists. The road network north to Felixstowe Road links into the proposed Ipswich to Felixstowe Key Corridor, at which point segregated cycling and walking infrastructure is recommended. The Ipswich to Felixstowe Key Corridor will provide a safe cycling and walking route into Ipswich and Felixstowe.

LR17.2

Introduce cycle parking at the end of Footpath 9.



Site Allocation Recommendations

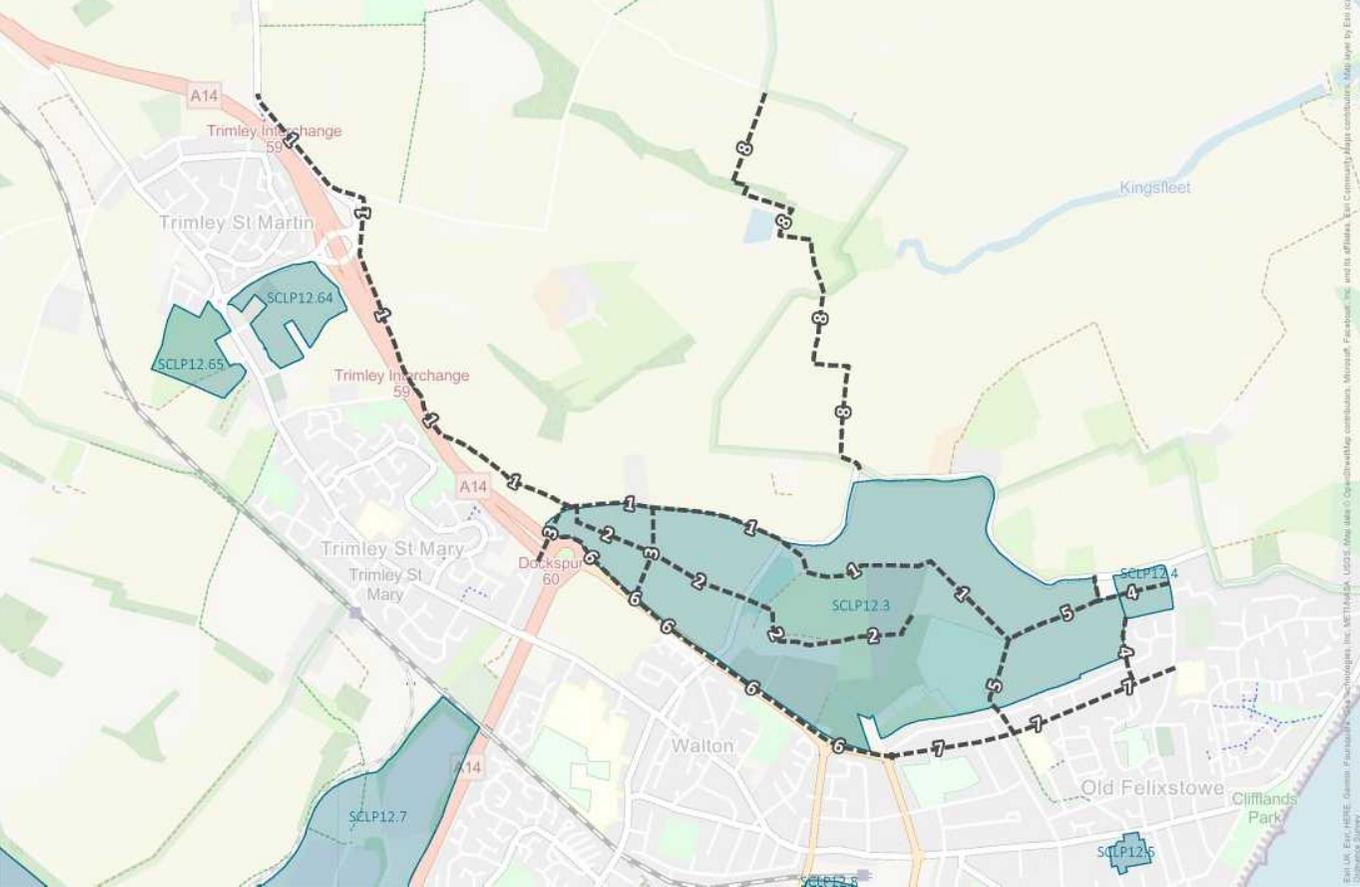
- 4.59 The adopted Suffolk Coastal and Waveney Local Plans contain site allocation policies. These allocate land for development for a particular use including residential, employment or mixed use.
- 4.60 The Strategy provides cycling and walking infrastructure recommendations that add value to the site allocation policies and policies WLP8.21 (Sustainable Transport) and SCLP7.1 (Sustainable Transport) which support development that integrates with and enhances the cycling and walking network. These recommendations address both on and off-site opportunities and must be considered when designing development proposals and when determining planning applications. The council expects

the provision of cycling and walking infrastructure through the delivery of site allocation to be in line with the Strategy.

- 4.61 In some instances, site allocations have already received planning permission and/or commenced construction, and therefore the recommendations cannot be secured through a planning permission. However, the recommendations have been drafted with consented plans in mind and may be delivered via other funding and delivery opportunities available in the area, as set out in the 'Implementation and Monitoring of the Strategy' section.
- 4.62 The recommendations for the site allocations are set out in the table below. Further information in relation to the planning policies for each of the site allocations can be found in the Suffolk Coastal and Waveney Local Plans.⁶⁴

⁶⁴ <https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/local-plans/>

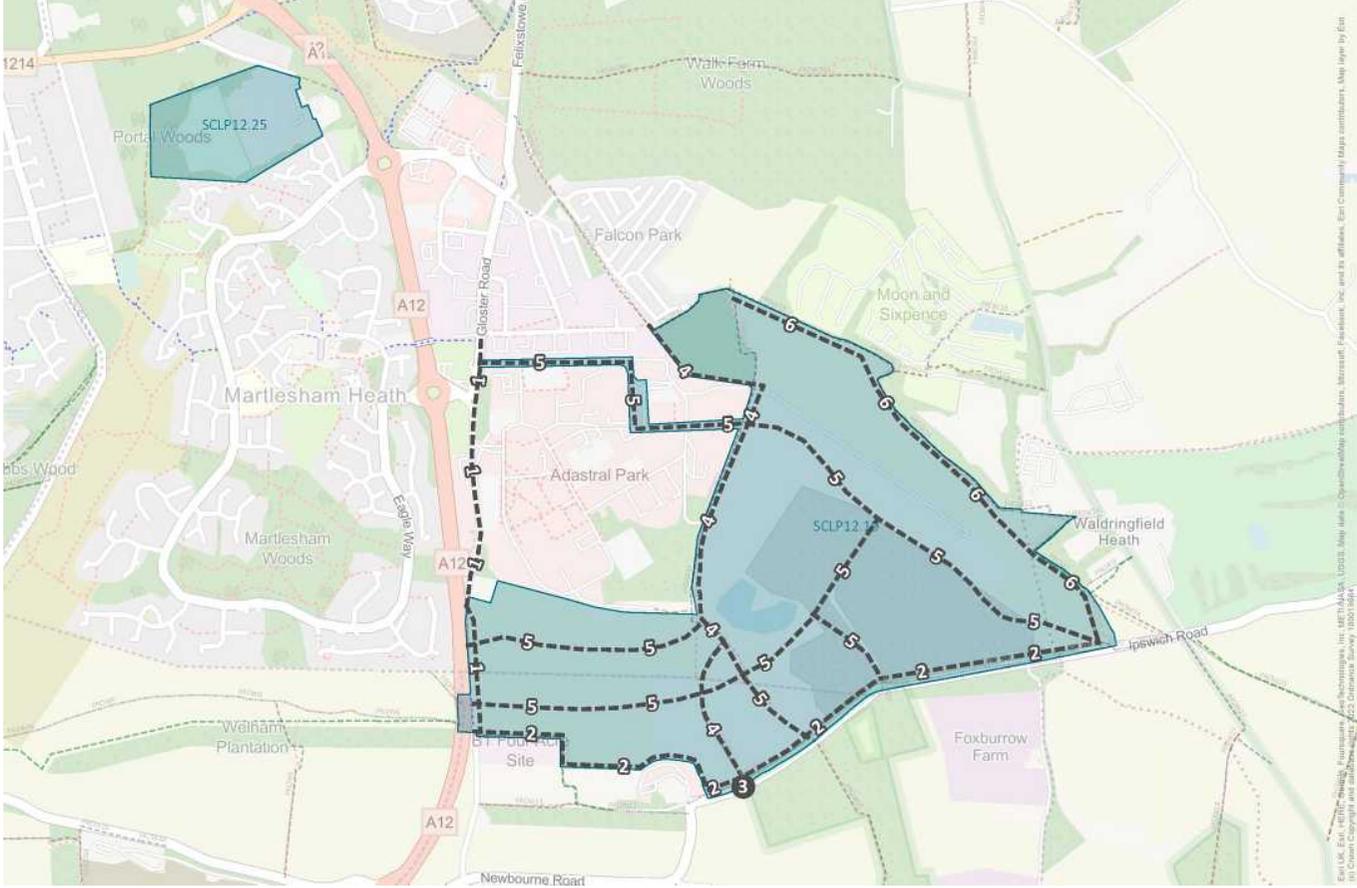
Table of Site Allocation Recommendations

Parish	Policy	Recommendations
Felixstowe	SCLP12.3 North Felixstowe Garden Neighbourhood	 <p>1 – Provide a bi-directional cycling and walking track between Kirton Road (commencing opposite the Roselea Nursery) and the crossroads between Gulpher Road and Hyems Lane (see F44).</p> <p>2 – Provide bi-directional cycling and walking tracks that run east to west, connecting Candlet Track to Hyems Lane.</p> <p>3 – In the western quadrant, provide bi-directional cycling and walking tracks that run north to south, connecting Candlet Track to the recommended infrastructure along Candlet Road (see F51), connecting Candlet Track to Spriteshall Lane via a new bridge over the Dockspur roundabout to replace the existing level crossings (see F30), and connecting Candlet Track to the development on land north of Walton High Street</p>

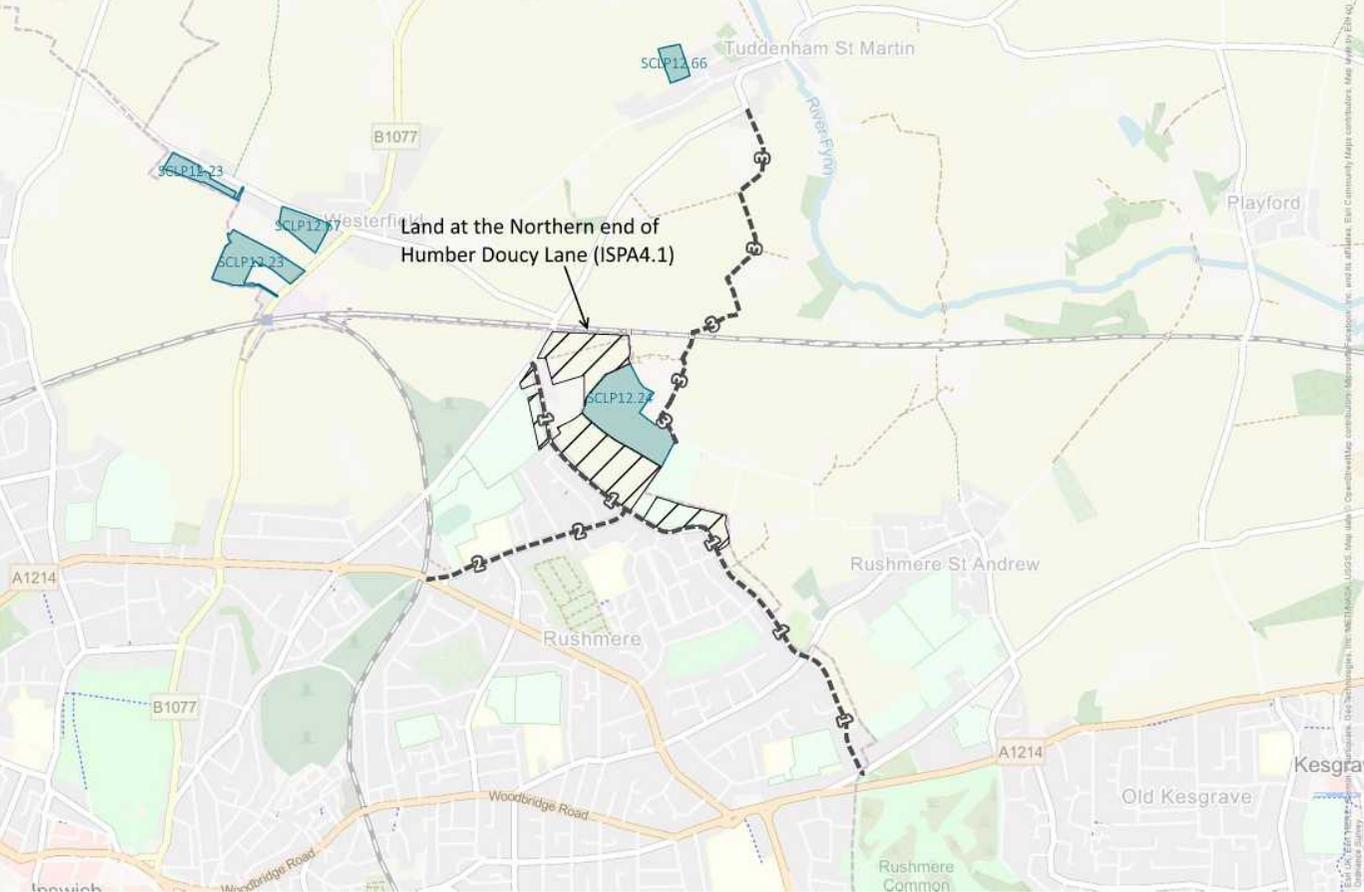
Parish	Policy	Recommendations
		<p>site, via the new Candlet Road access roundabout (see F47 and F46). An appropriate parallel crossing must be provided over Candlet Road to ensure user’s visibility and safety.</p> <p>4 – In the eastern quadrant, provide cycling and walking infrastructure that runs between Hyems Lane, Colneis Road and Ferry Road through the merger and upgrade of Footpath 13 and a cycling and walking track through SCLP12.4 (see F56).</p> <p>5 – In the eastern quadrant, provide a bi-directional cycling and walking track that runs north to south between Gulpher Road and Colneis Road via Hyems Lane and over Links Avenue (see F55).</p> <p>6 – Introduce segregated cycling and walking infrastructure along the southern site boundary between the Dockspur roundabout crossings (for a connection up to Candlet Track and Spriteshall Lane) and the Grove Road/Beatrice Avenue/Colneis Road/Links Avenue roundabout (see F51 and F30).</p> <p>7 – Provide cycling and walking infrastructure along Colneis Road for improved access to Colneis Junior School and Kingsfleet Primary School (see F54)</p> <p>8 – Provide a leisure cycling and walking connection to Lower Falkenham for open countryside cycling and walking activities (see F52).</p>
Felixstowe	SCLP12.4 Land north of Conway Close and Swallow Close	<p>1 – Provide a cycling and walking connection into the North Felixstowe Garden Neighbourhood (see F56).</p> <p>2 – Provide cycling infrastructure along Ferry Lane for improved access to Kingsfleet Primary School (see F57).</p>
Felixstowe	SCLP12.5 Land at Brackenbury Sports Centre	<p>1 – Remove barriers to walking and cycling on the existing route through the site.</p> <p>2 – Introduce appropriate and sensitive signage to clearly indicate the route through the site as a cycling and walking route.</p> <p>3 – Widen and resurface the existing route through the site, to accommodate cycling and walking. The route should also be connected to the cycling and walking infrastructure on High Road East.</p> <p>4 – Introduce a cycling and walking crossing point of High Road East at the site frontage.</p> <p>5 – Provide cycling and walking infrastructure along the more immediate sections of High Road and Cliff Road (see F6 and F7)</p>
Felixstowe	SCLP12.6 Land at Sea Road	<p>1 – An area of cycle parking should be provided opposite the site, in addition to on-site provision, to meet the needs of tourists, customers and visitors to the development and surrounding area. Removal of the section of wall immediately opposite the site to accommodate more cycle parking and improve access to the Promenade should be considered.</p> <p>2 – A parallel crossing should be delivered over Sea Road, aligned with an existing access point onto the Promenade and with the pedestrian desire line from the site.</p>

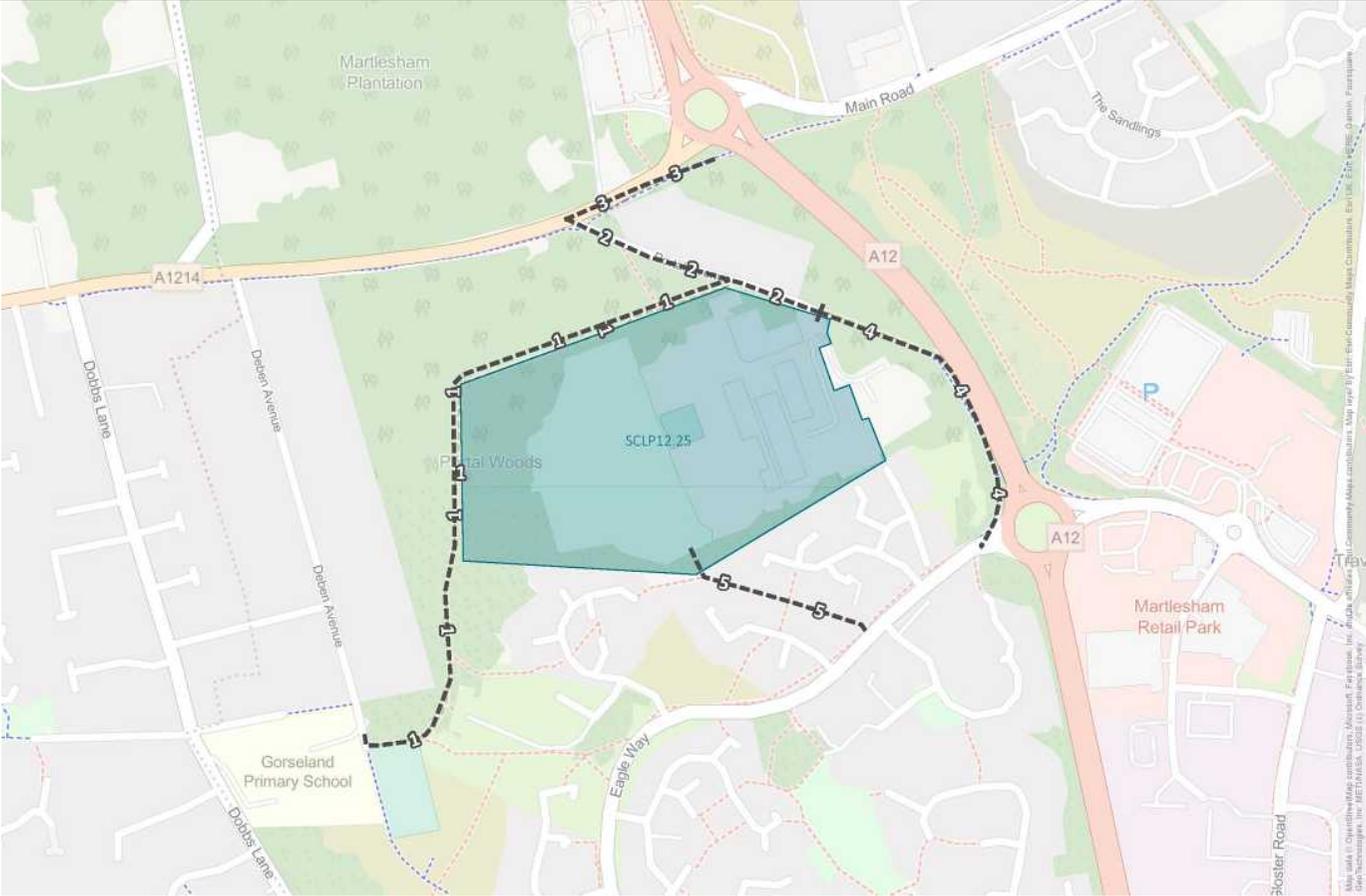
Parish	Policy	Recommendations
Felixstowe	SCLP12.7 Port of Felixstowe	 <p>1 – Improve Nicholas Road's existing section of shared path up to the track (Bridleway 14/12). Extend the shared path down to the roundabout with Blofield Road/Parker Avenue, then create a new shared path (using the existing short stretch of footpath initially), and then extend the new shared path to Fagbury Road (see F39).</p> <p>2 – Improve Fagbury Road's existing shared path and connect to Walton Avenue's new bi-directional track on the south side (see F86). Unless there is scope for a bridge over the railway tracks, no improvements are recommended to the existing level crossing to the north (see F40).</p> <p>3 – Widen and resurface the existing footpaths and shared paths around the Dock Gate 2 roundabout. Extend a new section around the northern side to create full circulation of the roundabout. Upgrade all crossings over the various roundabout arms to parallel crossings (see F41).</p> <p>4 – Provide a segregated cycling and walking connection to Grange Farm Avenue via Ferry Lane, or shared paths along Trinity Avenue and the A14</p>

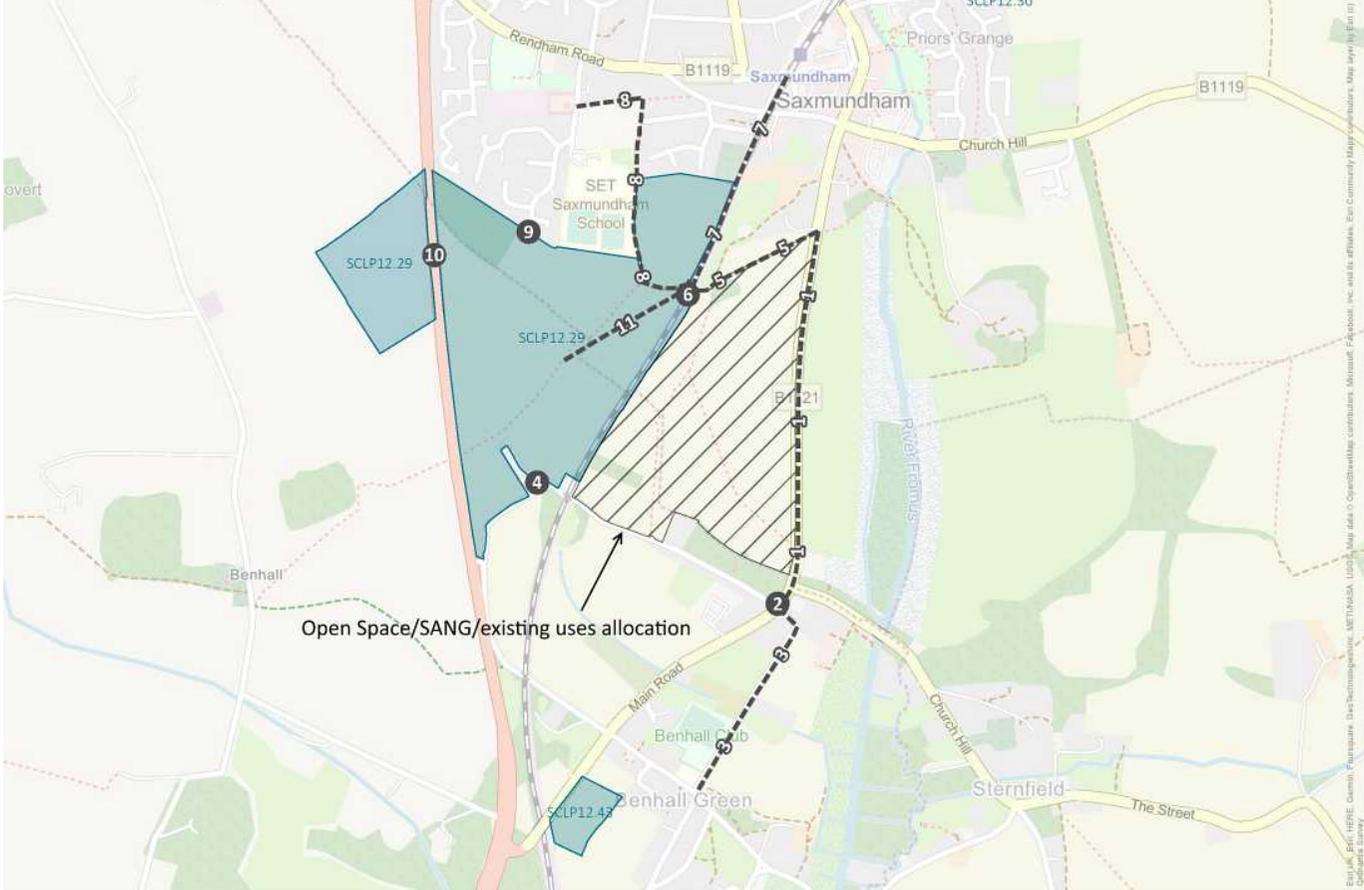
Parish	Policy	Recommendations
		connector road (A) and Parker Avenue via Blofield Road (B) (see F42). 5 – Create a cycling and walking connection between Cordy’s Lane and Hintlesham Drive for access to Grange Farm via Bridleway 12/Blofield Track Bridleway 14 (see F37) and Footpath 32B (see F43). Upgrade the footpath sections of this route to bridleway status throughout, replace the existing barriers with bollards, consolidate the signage, and resurface the sections of the route with poor surfacing. This route should be well connected to Nicholas Road (F39), for safe and convenient onward travel. 6 – Provide cycling and walking infrastructure along Langer Road (see F78, F79 and F80). 7 – Provide cycling and walking infrastructure along Walton Avenue to Dock Gate 2 (see F84 and F86).
Felixstowe	SCLP12.8 Land at Bridge Road	1 - Bridge Road should be resurfaced, widened where possible, and made one way for cars/vans etc, and both ways for cyclists and pedestrians.
Felixstowe	SCLP12.9 Land at Carr Road/Langer Road	1 – Provide cycling and walking infrastructure along Carr Road. Provide a parallel crossing for cyclists and pedestrians to cross over Carr Road and access Manor Road (see F80).
Felixstowe	SCLP12.10 Land at Haven Exchange	1 – Provide cycling and walking infrastructure along Walton Avenue to Dock Gate 1 and Peewit Hill (see F83).
Felixstowe	SCLP12.16 Felixstowe Leisure Centre	1 – Ensure easy and legible access onto the Promenade. 2 – Introduce accessible and secure cycle parking and storage. 3 – Provide a shared path connection between Undercliff Road West and Coronation Drive, including a crossing over Garrison Lane (south) (see F77). 4 – Provide a shared path along one side of Coronation Drive to Grange Road; connect into the Grange Road shared paths (see F68 and F62).

Parish	Policy	Recommendations
Brightwell / Martlesham	SCLP12.19 Brightwell Lakes	 <p>The following recommendations align with the cycling and walking infrastructure to be delivered as part of the consented Brightwell Lakes development.</p> <ol style="list-style-type: none"> 1 - Move the existing cycling and walking track along the eastern side of the A12 further east, appropriately segregated from the A12. Widen this track to accommodate the desired number of cycling and walking trips from the Brightwell Lakes development. If vehicle trip and speed data suggest cycling and walking infrastructure is required introduce cycling and walking infrastructure along Barrack Square and Glouster Road to the Brightwell Lakes northern access point. 2 - Introduce a cycling and walking track from the recommended cycling and walking track along the western site boundary, east through the site along the southern site boundary.

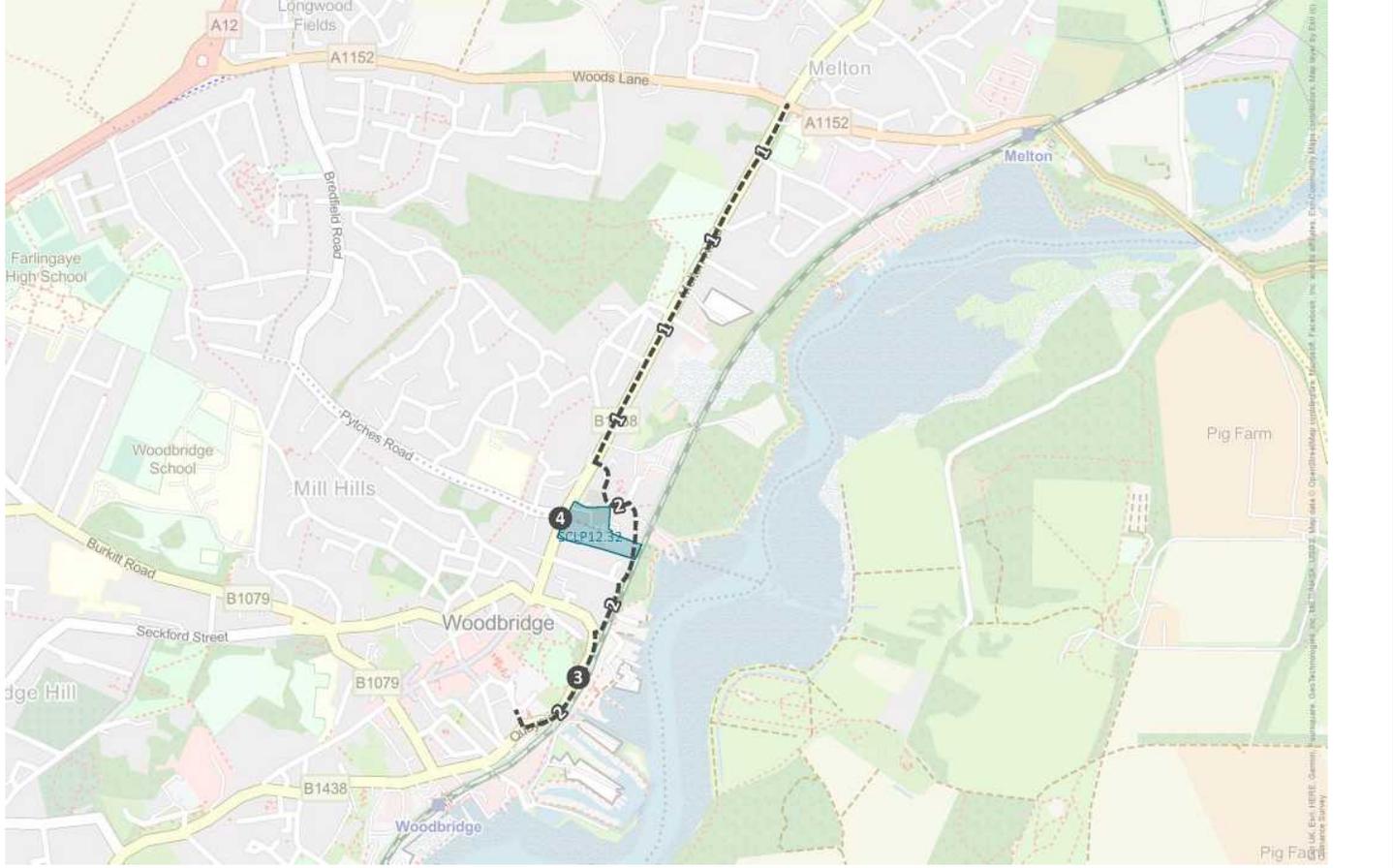
Parish	Policy	Recommendations
		<p>3 - Introduce a cycling and walking crossing point on Ipswich Road where Footpath 10 meets Ipswich Road.</p> <p>4 - Introduce a cycling and walking track through the site, along Footpaths 10, 30, 51, the southern and western sides of Spratt’s Plantation to Betts Avenue. Upgrade Footpaths 10, 30, and 51 to bridleways up to the southern edge of Spratt’s Plantation, widen and resurface accordingly.</p> <p>5 - Introduce cycling and walking infrastructure through the site between the southern A12 and northern Gloster Road main access points (see IM31).</p> <p>6 - Introduce cycling and walking infrastructure along the eastern edge of the site, connecting recommendations 2 and 4.</p>
Nacton	SCLP12.20 Land at Felixstowe Road	<p>1 - Introduce a segregated cycling and walking track along the southern side of Felixstowe Road, from Ipswich to the southwest corner of the site (see IF11).</p> <p>2 - Introduce a signalised cycling and walking crossing point on Felixstowe Road, at the southwest corner of the site.</p> <p>3 - Introduce a cycling and walking track along the northern side of Felixstowe Road, through the site and segregated from the road by existing vegetation, from the recommended signalised cycling and walking crossing point east along Felixstowe Road.</p> <p>4 - Introduce cycling and walking connection points from the site onto the recommended Felixstowe Road cycling and walking track.</p>
Nacton Heath	SCLP12.21 Ransomes	<p>1 - Reallocate central hatching and central turning lanes along Felixstowe Road to create a segregated cycling and walking track along the southern side of Felixstowe Road (see IF7).</p> <p>2 - Introduce a signalised cycling and walking crossing point where Bridleway 51 meets Felixstowe Road.</p> <p>3 - Introduce a signalised cycling and walking crossing point where the Felixstowe Road service road switches from north to the south side of the road to enable continuous cycling and walking segregated from vehicles.</p> <p>4 - Widen and resurface Bridleways 51, 32, and 31.</p>
Westerfield	SCLP12.23 Land off Lower Road and Westerfield Road	<p>1 - Introduce a cycling and walking track to connect from the southern site boundary of SCLP12.67, along Westerfield Road (segregated from the road by existing vegetation), west through the Country Park, along Footpath 18, and west through the Country Park.</p> <p>2 - Footpath 18 should be upgraded to a bridleway, widened and resurfaced accordingly.</p>

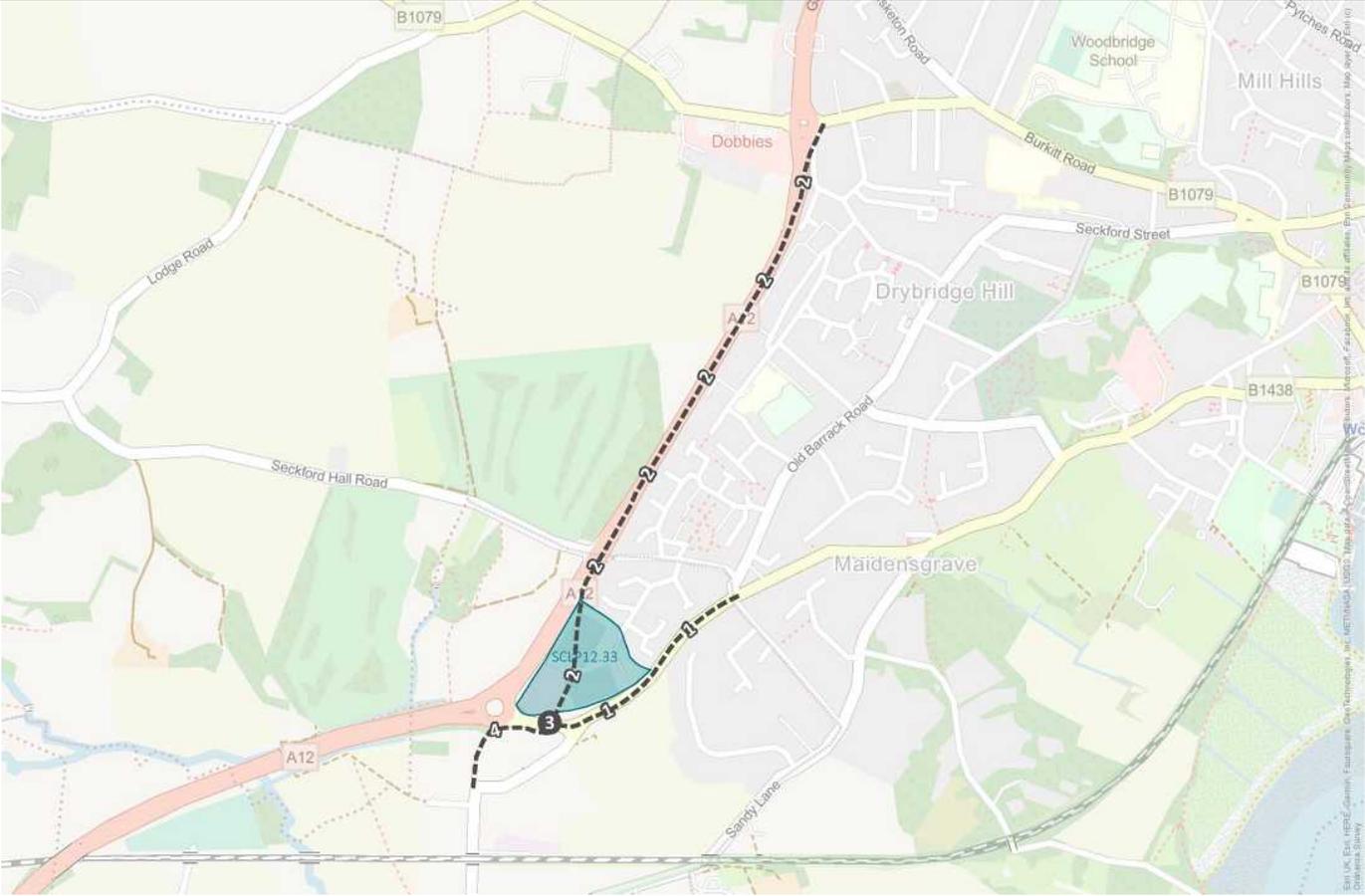
Parish	Policy	Recommendations
Rushmere St Andrew	SCLP12.24 Land at Humber Doucy Lane	 <p data-bbox="613 1153 2022 1348"> 1 - Introduce a segregated cycling and walking track along Humber Doucy Lane, segregated from the road by existing vegetation. This segregated track should run all the way along Humber Doucy Lane and across the area of land between Playford Road and Woodbridge Road, becoming an on road facility in the form of a Cycle Street between the Humber Doucy Sports Centre vehicle access and Playford Road. Introduce cycling and walking crossing points at appropriate intervals along Humber Doucy Lane. 2 - Introduce a shared cycle/footway along Sidegate Lane. 3 - Introduce a cycling and walking connection onto Tuddenham Lane and Bridleway 1. Widen and resurface Bridleways 1, 15, and 2 to accommodate cyclists and pedestrians. </p>

Parish	Policy	Recommendations
Martlesham Heath	SCLP12.25 Suffolk Police HQ, Portal Avenue	 <p data-bbox="611 1153 2022 1355"> 1 - A track runs just beyond the northern and western edges of SCLP12.25. The track surface should be improved to accommodate cyclists and pedestrians from Deben Avenue to Portal Avenue, as well as being sensitively lit, in a sympathetic way having regard to the aviation heritage of the route and existing surface. Connections should also be introduced between the track and the site. Alternatively, a segregated cycling and walking track should run through SCLP12.25. (see IM9) 2 - Introduce cycling and walking infrastructure along Portal Avenue without or with limited tree removal. (see IM27) 3 - At the junction of Portal Avenue and the A1214 the existing footway that runs towards the A12 underpass should be widened to accommodate cyclists as well as pedestrians. (See IM10) </p>

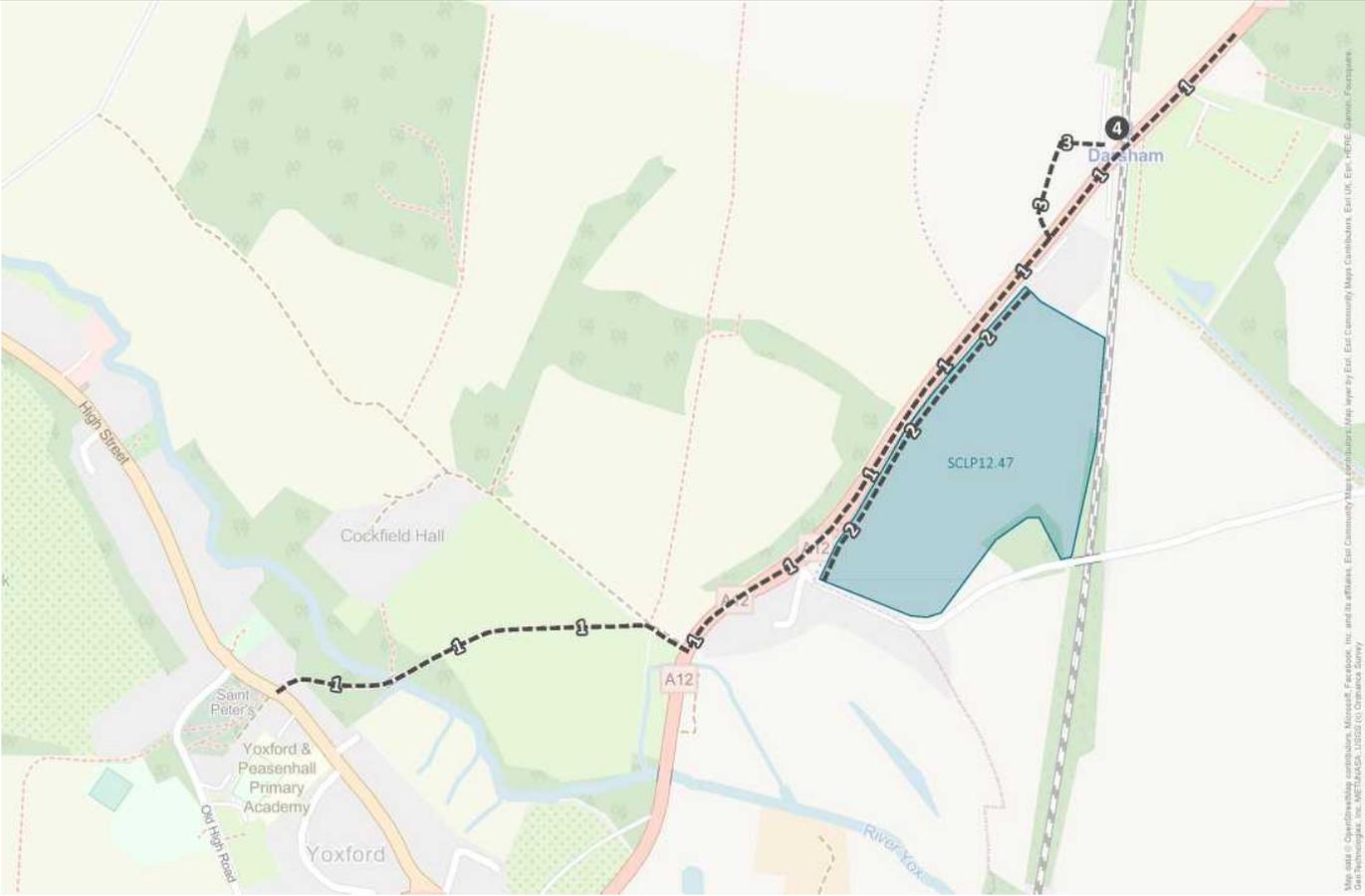
Parish	Policy	Recommendations
		<p>4 - A car free cycle and pedestrian track should be introduced from Portal Avenue to Eagle Way (connecting into the existing cycling and walking infrastructure along Eagle Way), between the A12 and Police Investigation Centre. This track should connect into the A12 underpass at the A12/Anson Road junction.</p> <p>5 - Introduce a footpath between the site and the existing footpath south of the site leading to the Eagle Way bus stop.</p>
Aldeburgh	SCLP12.27 Land Rear of Saxmundham Road	<p>1 - Introduce a cycling and walking track along the access road if space permits.</p> <p>2 - Footpaths 6 and 20 should be upgraded to bridleway status and resurfaced.</p>
Saxmundham / Benhall	SCLP12.29 South Saxmundham Garden Neighbourhood	 <p>1 - Introduce a cycling and walking track along the B1121, segregated from the road by the existing hedgerow, and upgrade Footpath 19 to a bridleway, widen and resurface accordingly.</p>

Parish	Policy	Recommendations
		<p>2 - Introduce a signalised cycling and pedestrian crossing point on the B1121 to access School Lane.</p> <p>3 - Make School Lane one way for vehicles, while retaining two way movement for pedestrians, cyclists and equestrians, and extend the School Safety Zone along the entire length of School Lane to reduce the speed limit from the current 60mph to 20mph. Alternatively, introduce a modal filter to School Lane and a turning place on either side of the modal filter.</p> <p>4 - Maintain and improve cycling and walking connection from the site onto Kiln Lane.</p> <p>5 - Upgrade Footpaths 16 and 17 to bridleways, widen and resurface accordingly.</p> <p>6 - Upgrade the existing railway bridge to accommodate cyclists and pedestrians.</p> <p>7 - Introduce a cycling and walking track from the existing railway bridge, north along the western side of the railway line, the eastern edge of the cemetery, Park End, Alma Place, Station Approach, and into Saxmundham Railway Station.</p> <p>8 - Introduce a pedestrian and cycle connection onto Footpaths 15 and 13 to Seaman Avenue, which should be upgraded to a bridleway.</p> <p>9 - Introduce a pedestrian and cycle connection onto Lincoln Avenue.</p> <p>10 - Introduce cycling and walking connection between the employment and residential development on either side of the A12.</p> <p>11 - Introduce a segregated cycling and walking track from the railway bridge to the required on-site community hub.</p>
Saxmundham	SCLP12.30 Land North-East of Street Farm	<p>1 - Explore the potential to connect the site to the agricultural track adjoining the southeast corner of the site.</p>

Parish	Policy	Recommendations
Melton	SCLP12.32 Former Council Offices	 <p>1 - Introduce a segregated cycling and walking track along Melton Road, between Melton Road/Woods Lane crossroads and Melton Hill.</p> <p>2 - There is not sufficient space to introduce any cycling or further walking infrastructure on Melton Hill, between the site frontage and Old Maltings Approach due to the narrow road. Therefore, a cycling and walking track should be introduced along Old Maltings Approach through the former East Suffolk Council offices site, between the railway line and dwelling off Deben Road, along Deben Road and Quayside to the Hamblin Road car park. (See IM20)</p> <p>3 - Remove railings along Quayside fronting Elmhurst Park entrance.</p> <p>4 - Replace the island on Melton Hill fronting the site with a zebra crossing.</p>

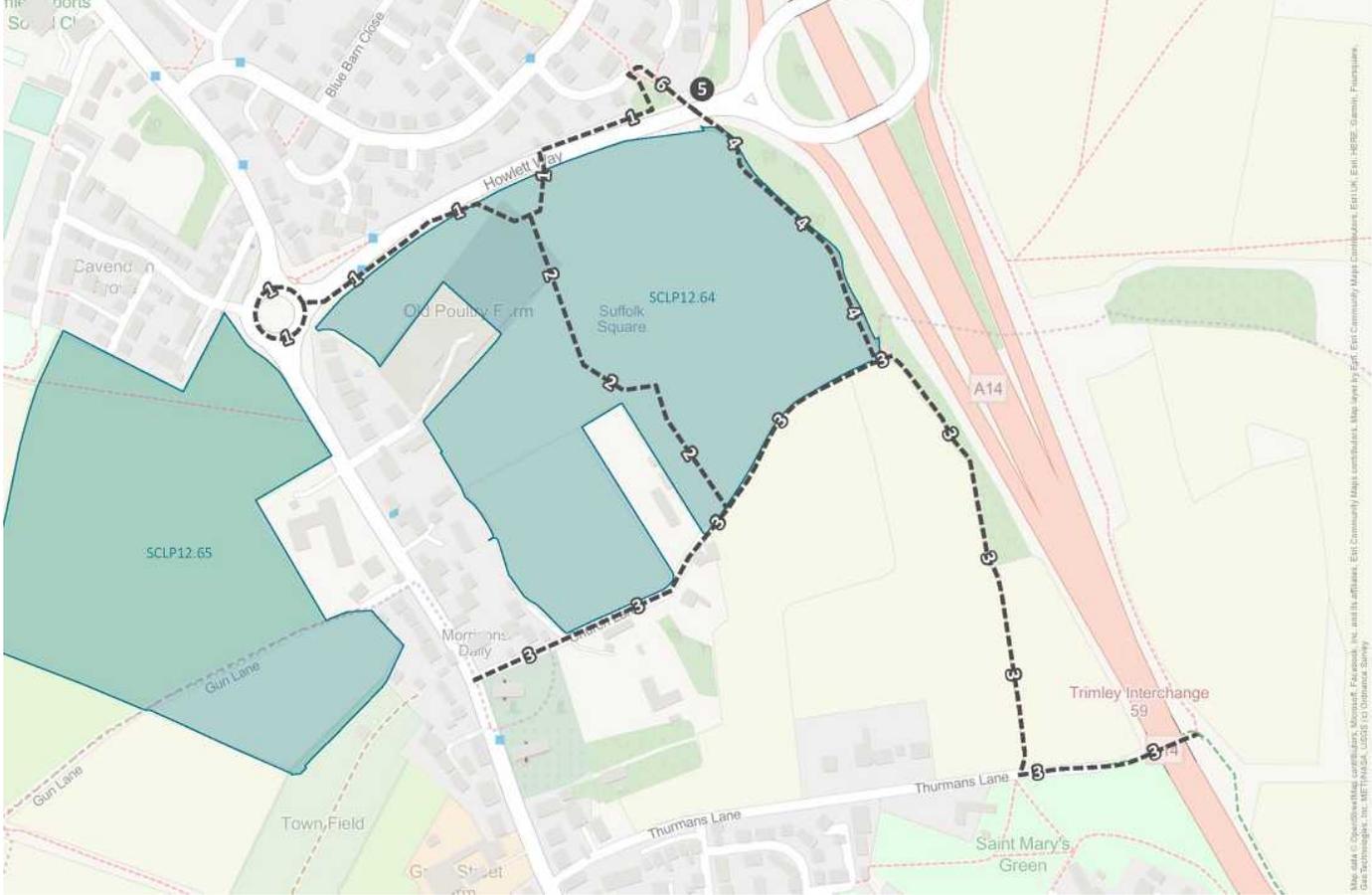
Parish	Policy	Recommendations
Woodbridge	SCLP12.33 Land at Woodbridge Town Football Club	 <p>1 - Widen the B1438 footway if possible.</p> <p>2 - Introduce a segregated cycling and walking track from the proposed B1438 cycling and walking bridge, through the allocation, alongside the A12, to Grundisburgh Road, using Bilney Road and the track to the rear of properties along Peterhouse Crescent. (see IM17)</p> <p>3 - Introduce a cycling and walking bridge over the B1438. Link the B1438 footway to this bridge.</p> <p>4 - Introduce a cycling and walking track from the recommended B1438 cycling and walking bridge south to the Top Street roundabout.</p>
Debach	SCLP12.35 Former airfield Debach	No recommendations due to the highly rural location and significant distance to any services and facilities.

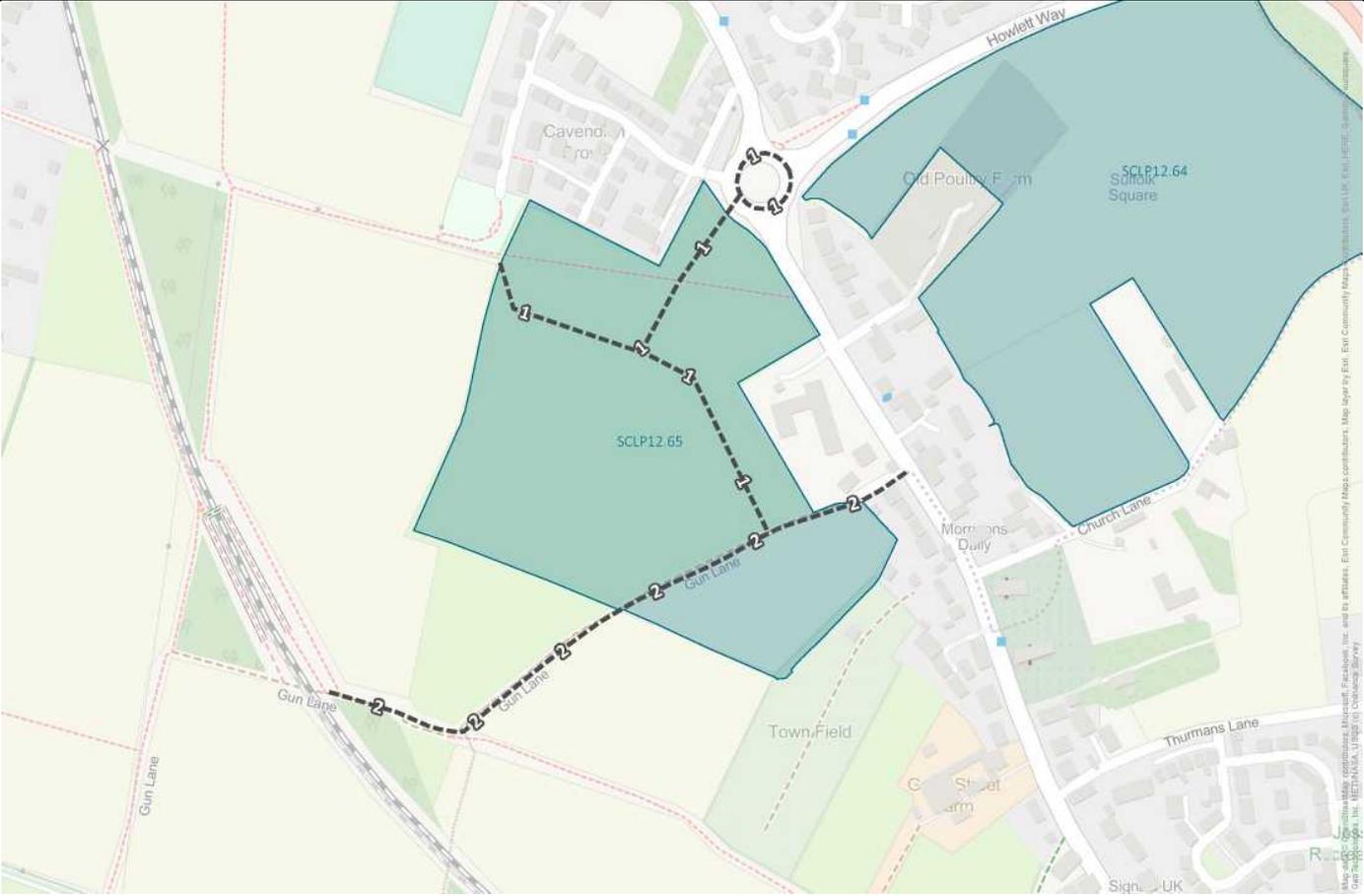
Parish	Policy	Recommendations
Kelsale cum Carlton	SCLP12.36 Carlton Park, Main Road	1 - Introduce a segregated cycling and walking track along the site frontage, north to Carlton Road, and south to Saxmundham Sports Club.
Levington	SCLP12.37 Levington Park	1 - Introduce a segregated cycling and walking track along the site frontage, south to the built edge of Levington, and north to the railway bridge.
Parham	SCLP12.38 Land at Silverlace Green (former airfield)	1 - Upgrade Footpaths 16 & 17 to bridleways, widen and resurface accordingly.
Parham	SCLP12.39 Former airfield	No recommendations due to the highly rural location and significant distance to any services and facilities.
Rendlesham	SCLP12.40 Bentwaters Park	1 - Upgrade the A1152/B1069 roundabout to a Dutch-style roundabout. 2 - Introduce a segregated cycling and walking track to the Bentwaters Park access road, from the recommended Dutch-style roundabout. 3 - Introduce a segregated cycling and walking track along the A1152 and B1069 from the B1069/Ivy Lodge Road to Ash Road via Rendlesham Mews. This may require road realignment and some tree removal.
Wickham Market	SCLP12.41 Riverside Industrial Estate, Border Cot Lane	1 - Introduce a cycling and walking track along Border Cot Lane from the employment site west to Bridleway 7. 2 - Introduce secure cycle storage on the employment site. 3 - The Border Cot Lane/High Street junction could be reconfigured to slow traffic and widen the existing footway to enhance the pedestrian environment. 4 - On Border Cot Lane, west of the Wickham Market built edge, a village gateway could be introduced to slow traffic entering the village. This should not undermine the ability to introduce a cycling and walking track along Border Cot Lane.
Aldringham	SCLP12.42 Land to the East of Aldeburgh Road	1 - Introduce a cycling and walking track and enhance the existing footway along the western side of Aldeburgh Road from SCLP12.42 and leading to Leiston.
Benhall	SCLP12.43 Land South of Forge Close between Main Road and Ayden	1 - Introduce a cycling and walking track along the B1121, on the SCLP12.43 side of the existing hedgerow in order to enhance walking and cycling connectivity and safety into Footpath 26 and along the B1121. 2 - Introduce a crossing point on the B1121 to enable safe access to the bus stop and the B1121 footway into Saxmundham. 3 - Footpath 26 (between the B1121 and Mill Lane) should be upgraded to a bridleway, widened and resurfaced to accommodate pedestrians and cyclists.
Bucklesham	SCLP12.44 Land to the South East of Levington Lane	1 - Introduce a footpath from the site to the allotments.
Campsea Ashe	SCLP12.45 Land to the South of Station Road	1 - Widen the existing footway fronting the site by removing vegetation on the footway.
Charsfield	SCLP12.46 Land behind 15 St Peters Close	1 - Introduce a walking track through the allocation, connecting St Peters Close with the playing fields to the east.

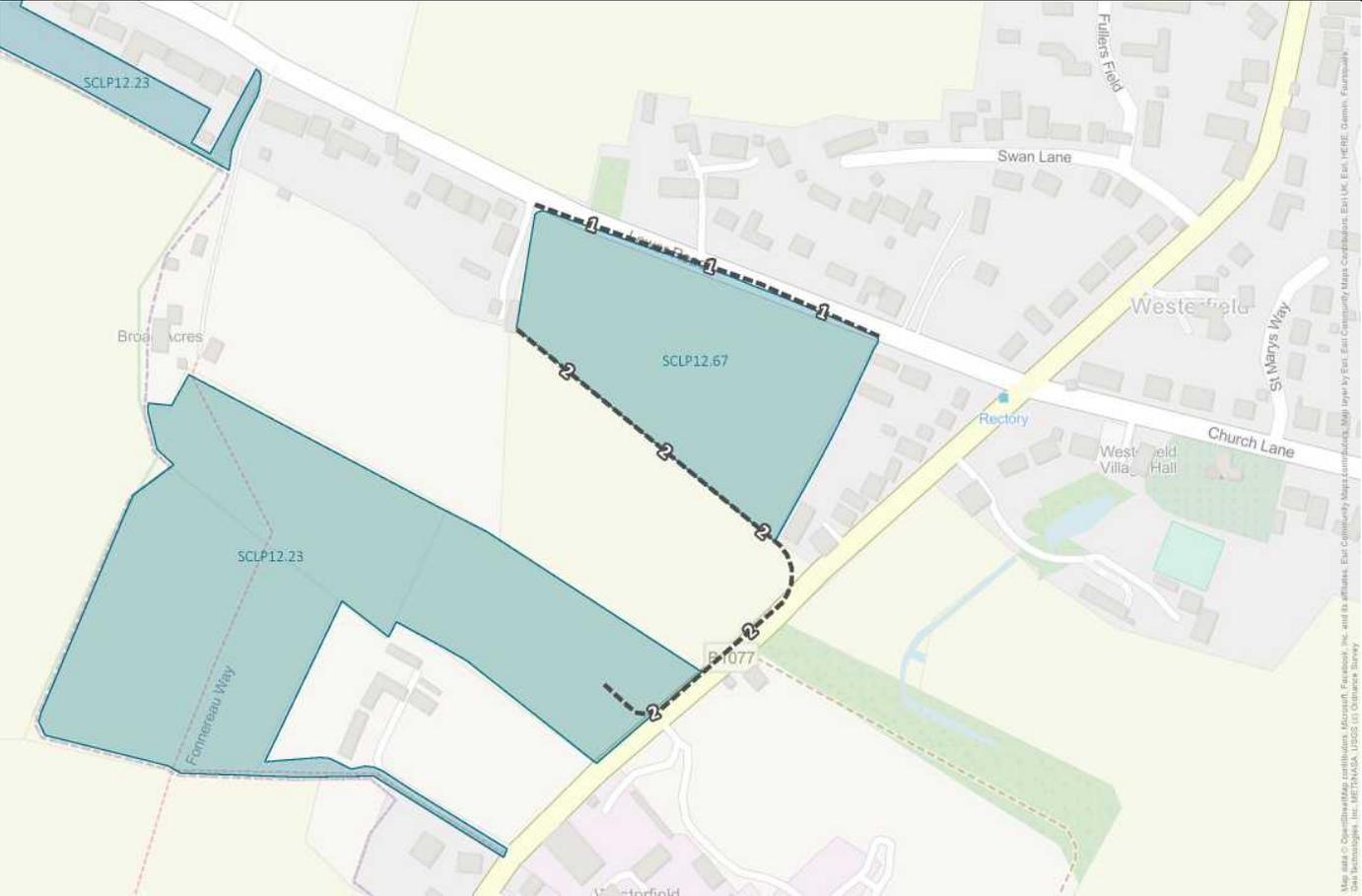
Parish	Policy	Recommendations
Darsham	SCLP12.47 Land to the South of Darsham Station	 <p>The map displays the site SCLP12.47 in blue, situated south of Darsham Station. It shows the A12 road, the River Yox, and local landmarks like Cockfield Hall and Yoxford Primary Academy. Two recommended routes are marked: Route 1 (dashed black line) and Route 2 (solid red line).</p> <p>1 - A crossing point on the A12 and a cycling and walking track along the western side of the A12 is required to ensure safe pedestrian and cycle access from the site into the existing footway network and to Darsham Railway Station and Darsham Service Station. To enable safe cycling and walking into Yoxford, Footpath 13 should be upgraded to a bridleway and connected to the cycle and pedestrian infrastructure recommended along the A12. Once upgraded to a bridleway, Footpath 13 should be widened and resurfaced to accommodate pedestrians and cyclists. This should be achieved in a manner sensitive to the historic significance of Cockfield Hall Historic Park, the Grade I Cockfield Hall, other nearby listed buildings, and the Yoxford Conservation Area.</p> <p>2 - A cycle and pedestrian track should be introduced along the western edge of the site, segregated from the A12 by retaining the existing vegetation along the A12. This route should connect with the cycle and pedestrian crossing point on the A12.</p>

Parish	Policy	Recommendations
		<p>3 - Immediately south of Darsham Railway Station it is recognised that property boundaries are tightly bound to the A12. Opportunities to route cycle and pedestrian infrastructure behind these properties should be explored.</p> <p>4 - Cycle parking and storage should be introduced at Darsham Railway Station.</p> <p>5 - Ensure cycling and walking infrastructure aligns with and does not prohibit future Sizewell C development.</p>
Darsham	SCLP12.48 Land North of The Street	1 - Introduce a footway along The Street, between the site allocation and Footpath 9.
Dennington	SCLP12.49 Land off Laxfield Road	1 - Introduce a footway along the site frontage with Laxfield Road.
Eyke	SCLP12.50 Land to the South of Eyke CoE Primary School and East of The Street	<p>1 - The pedestrian infrastructure outside the site should be improved and connected into the wider network. This could involve widening the path on the west side of the road with an appropriate crossing or a new path within the site that connects into the existing network through the adjacent car park.</p> <p>2 - As the Policy identifies that this site allocation provides an opportunity for a green open space to be used by the community, it may be necessary to also provide cycle parking facilities.</p> <p>3 - If space permits, providing off-road cycling infrastructure between the site/school and village shop/Church/bus stop will provide a sustainable travel opportunity and start a wider route through to Rendlesham.</p> <p>4 - Consideration should be given to providing a cycling and walking connection between the site and Church Lane.</p>
Grundisburgh	SCLP12.51 Land to the West of Chapel Road	<p>1 - Resurface Footpath 20.</p> <p>2 - Consider implementing at either end signage marking the crossing point from Footpaths 20/21. Consider improving the connection to Post Mill Orchard to allow safe access into the village.</p> <p>3 - Introduce a cycling and walking connection between the site and Chapel Road.</p>
Kelsale cum Carlton	SCLP12.52 Land South of Ambleside, Main Road	1 - Introduce a segregated cycling and walking track along the B1121 from the allocation into Saxmundham.
Kettleburgh	SCLP12.53 Land North of the Street	<p>1 - Any path created to the front of the allocation should be extended westwards to the existing infrastructure at the entrance to Lings Field thus creating a more continuous route that connects the residents of the allocation to the public house.</p> <p>2 - Footpath 19 should be easily accessible to residents of the site and upgrading Footpath 19 to a bridleway to allow cycling should be explored.</p>
Kirton	SCLP12.54 Land to the rear of 31-37 Bucklesham Road	<p>1 - Introduce a cycling and walking track between Footpaths 50 and 41, to the rear of properties along Bucklesham Road, and along the western site boundary. (see MF2)</p> <p>2 - Footpath 41 should be upgraded to a bridleway, widened and resurfaced accordingly.</p>
Knodishall	SCLP12.55 Land at School Road	1 - A cycling and walking track should be introduced along the site frontage, segregated from the road by the existing hedgerow.
Levington	SCLP12.56 Land at Bridge Road	<p>1 - Introduce a segregated cycling and walking track along the site frontage, north to the railway bridge.</p> <p>2 - Introduce a pinch point on Bridge Road at the site frontage to narrow the carriageway and enable safer crossing of Bridge Road.</p>
Orford	SCLP12.57 Land North of Mill Close	<p>1 - Upgrade Footpath 3 to a bridleway and resurface to accommodate pedestrians and cyclists, whilst retaining its rural character.</p> <p>2 - Introduce cycle parking within the village centre, sensitive to the Conservation Area.</p>
Otley	SCLP12.58 Land adjacent to Swiss Farm	<p>1 - Introduce a shared cycling and walking path along Chapel Road, between SCLP12.58 and the Village Shop. If space allows, introduce segregation between the shared cycling and walking path and Chapel Road.</p> <p>2 - Introduce cycle parking at services and facilities on Chapel Road.</p>

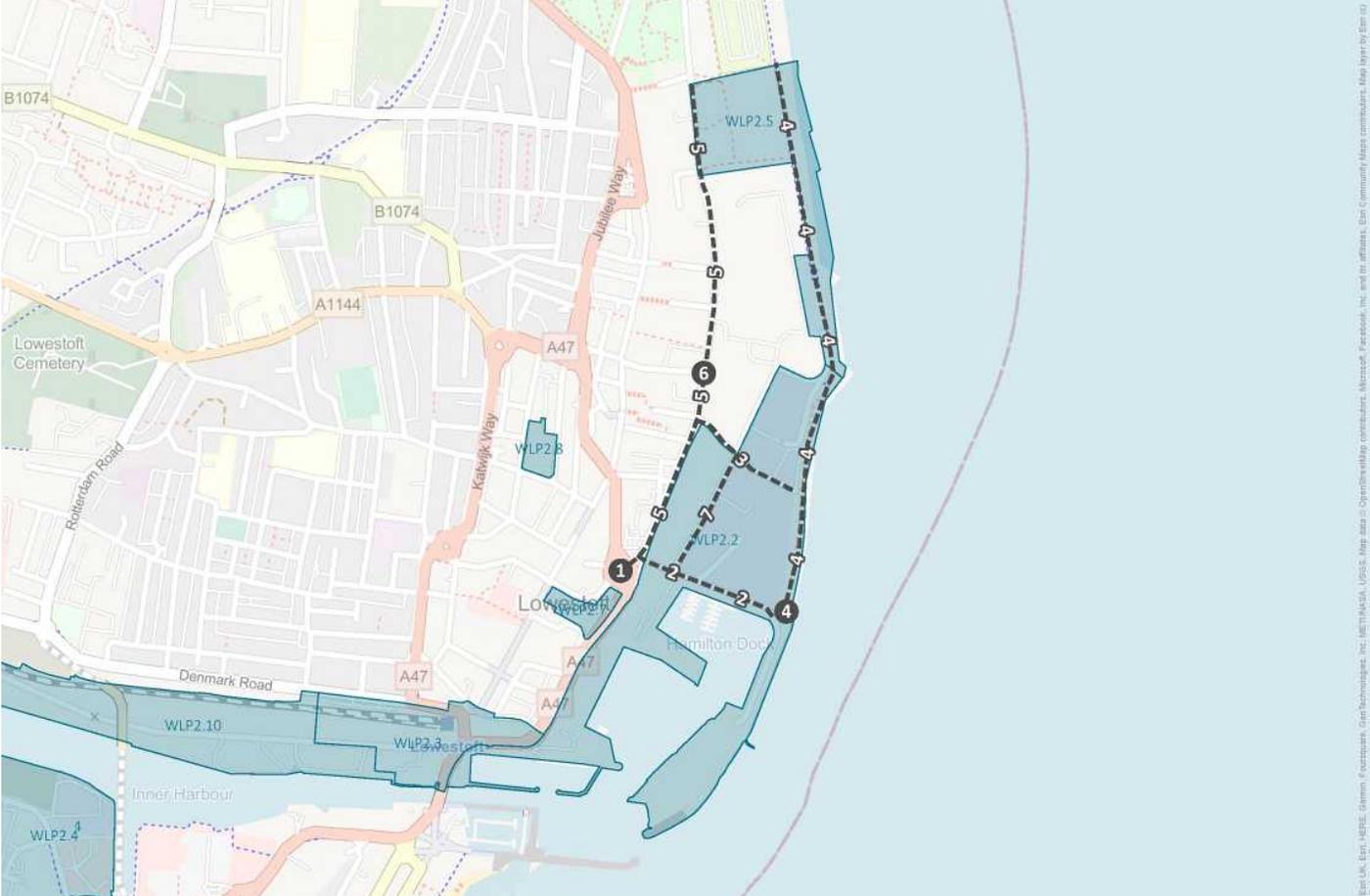
Parish	Policy	Recommendations
		<p>3 - Upgrade Footpaths 56 and 58 to bridleways, widen and resurface. This will provide a cycling and walking connection between Bridleway 28, the site and Chapel Road as well as a circular cycle route onto Bridleway 72.</p> <p>4 - Introduce a cycling and walking crossing point on Chapel Road.</p>
Peasehall	SCLP12.59 Land adjacent to Farthings, Sibton Road	1 - Introduce a modal filter to Sibton Road.
Pettistree	SCLP12.60 Land between High Street and Chapel Lane (adjoining Wickham Market)	<p>1 - Introduce a footway along the site frontage, connecting to the existing footway.</p> <p>2 - Introduce a cycling and walking connection from the site onto Chapel Lane.</p>
Rendlesham	SCLP12.61 Land West of Garden Square	<p>1 - Deliver PROW cycling and walking infrastructure as set out in the planning permission.</p> <p>2 - Introduce cycling and walking connection point onto Garden Square.</p>
Rendlesham	SCLP12.62 Land East of Redwald Road	<p>1 - Introduce a cycling and walking track parallel to Redwald Road, with a crossing point into the site.</p> <p>2 - Introduce a cycling and walking track along the A1152 from the A1152/B1069 roundabout to Ash Road via Rendlesham Mews. This may require road realignment.</p> <p>3 - Deliver PROW cycling and walking infrastructure as set out in the planning permission.</p>
Shottisham	SCLP12.63 Land opposite The Sorrel Horse, The Street	1 - Introduce cycling and walking connection points from the site onto Restricted Byway 37.

Parish	Policy	Recommendations
Trimley St Martin	SCLP12.64 Land off Howlett Way	 <p>1 - Introduce a segregated cycling and walking track alongside and segregated from Howlett Way, and around the Howlett Way/High Road roundabout. (see F17)</p> <p>2 - Introduce a segregated cycling and walking track between Howlett Way and Church Lane (Bridleway 5). (see F131)</p> <p>3 - Church Lane, Bridleway 5 and Bridleway 10 should be resurfaced and widened. Appropriate lighting should be introduced to the Bridleway 10/A14 underpass. (see F22-F25)</p> <p>4 - Retain Footpath 26 as a walking route. However, if the recommended cycling and walking track along Howlett Way, through the allocation, and along Bridleway 5 and Church Lane is not of a high enough standard and segregated from vehicles, a segregated cycling and walking track will be expected along the eastern site boundary. This track should link into the Howlett Way cycling and walking track and Bridleway 5. (see F24)</p>

Parish	Policy	Recommendations
Trimley St Martin	SCLP12.65 Land adjacent to Reeve Lodge, High Road	<p>5 - Undertake a safety audit of the Footpath 26 crossing of Howlett Way and implement measures identified in audit.</p> <p>6 - Upgrade the section of Footpath 26 between Howlett Way and Ash Ground Close to a bridleway and widen to accommodate cyclists.</p>  <p>1 - Introduce a segregated cycling and walking track from the Trimley St Martin play area to and around the High Road/Howlett Way roundabout, and from the Trimley St Martin play area to Gun Lane. (see F13-F137)</p> <p>2 - Widen and resurface Gun Lane (Restricted Byway 3) and Restricted Byway 28 (up to the railway bridge). (see F14)</p>
Tuddenham	SCLP12.66 Land off Keightley Way	<p>1 - Introduce a cycling and walking route from the site, through the playground and east along the field boundary to Bridleway 10, which should be resurfaced accordingly.</p>

Parish	Policy	Recommendations
Westerfield	SCLP12.67 Land South of Lower Road	<p data-bbox="613 225 2029 284">2 - Introduce a cycling and walking track from the site west, along the northern built edge of the village, to Footpath 8, which should be upgraded to a bridleway, widened and resurfaced accordingly.</p>  <p data-bbox="613 1225 2029 1284">1 - Introduce a footway along the site frontage with Lower Road, segregated from the road by the existing trees. The footway should connect into the existing footway along Lower Road from the eastern corner of the site.</p> <p data-bbox="613 1284 2029 1337">2 - Introduce a cycling and walking track from the allocation, along the southern site border, connecting to Westerfield Road, and south alongside Westerfield Road (and segregated from the road by the existing vegetation) to and through the Ipswich Garden Suburb Country Park (SCLP12.23).</p>

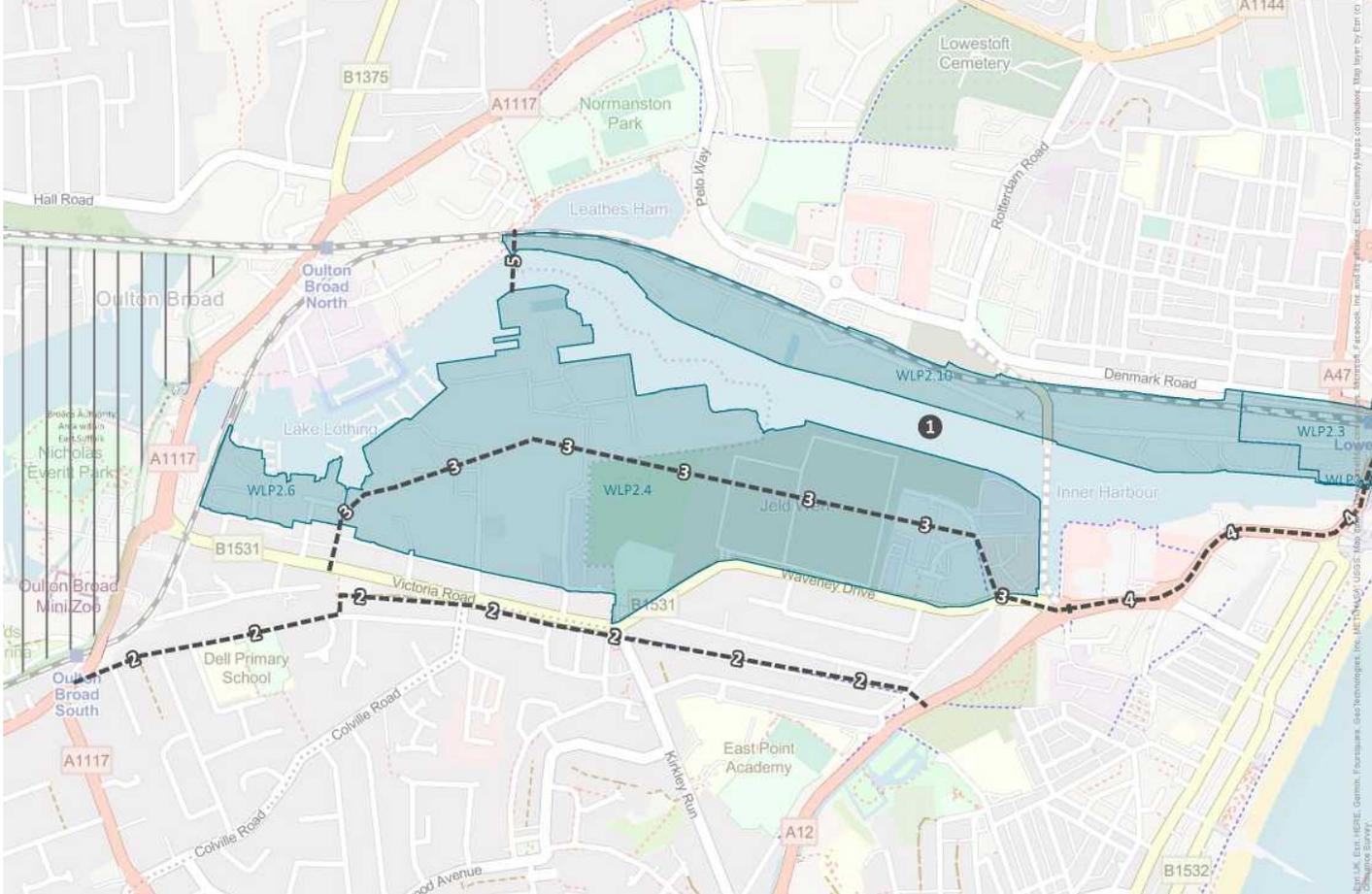
Parish	Policy	Recommendations
Westleton	SCLP12.68 Land West of the B1125	1 - The existing footway along the B1125 should be widened, provided there is sufficient space to do so. 2 - Introduce a footway along the B1125 site frontage, segregated from the B1125 by the existing Hedgerow. 3 - Introduce a chicane to the B1125, to slow traffic entering the village and provide a short crossing point for pedestrians.
Westleton	SCLP12.69 Land at Cherry Lee, Darsham Road	1 - Resurface Bridleway 32 and widen where appropriate. 2 - Due to the limited space along Darsham Road opportunities to provide a cycling and walking route from Bridleway 32, through the site, to Bridleway 31 should be explored, although this may not be possible due to potential land ownership issues.
Witnesham	SCLP12.70 Mow Hill	1 - Introduce a footway along the western edge of the site, east of the existing vegetation which is to be retained except where required for access. 2 - A stile is situated where Footpath 26 meets Mow Hill. This should be removed and/or replaced with a gate that provides greater accessibility to all users.
Witnesham (Bridge)	SCLP12.71 Land at Street Farm	1 - Ensure pedestrians can safely cross the B1077 from the site, thereby accessing the village facilities from the existing B1077 footway. 2 - Resurface Bridleway 30 to better accommodate pedestrians and cyclists.

Parish	Policy	Recommendations
Lowestoft	WLP2.2 PowerPark	 <p>1 - Upgrade Battery Green Roundabout. The radial routes should be significantly improved to allow free flowing of cyclists and pedestrians around the edges. A Dutch-style roundabout or another alternative could be considered.</p> <p>2 - Introduce a shared cycle/footway along Hamilton Road.</p> <p>3 - Introduce a shared cycle/footway along Wilde Street. (see L40)</p> <p>4 - Upgrade the Hamilton Road connection to the seafront to ensure easy access for cyclists and pedestrians between Hamilton Road and The Ness Park. (see L39)</p>

Parish	Policy	Recommendations
		<p>5 - Widen the existing Whapload Road footway to create a shared cycle/footway and connect into planned Scores improvements as set out in North Lowestoft Heritage Action Zone⁶⁵.</p> <p>6 - Introduce cycling and walking crossing points on Whapload Road.</p> <p>7 - Provide an improved cycling on Newcombe Road reflecting the design vision⁶⁶ for the PowerPark. (see L38)</p>
Lowestoft	WLP2.3 Peto Square	<p>1 – Increase the number of cycle parking spaces by introducing a cycle storage facility.</p> <p>2 – Reallocate the third vehicle lane along Denmark Road, Station Square, Waveney Road, Bascule Bridge, Pier Terrace, and Belvedere Road to segregated cycling and walking infrastructure. (see L11)</p> <p>3 - Denmark Road should be reconfigured to reflect its importance as an east-west connection for cycling and walking to the town centre. The central hatching and islands should be removed, and segregated cycling and walking infrastructure introduced alongside appropriate crossings. Where Denmark Road narrows with on-street parking, the shared cycle/footway should be expanded onto land south of Denmark Road to create a bi-directional cycling and walking track. (see L10)</p>

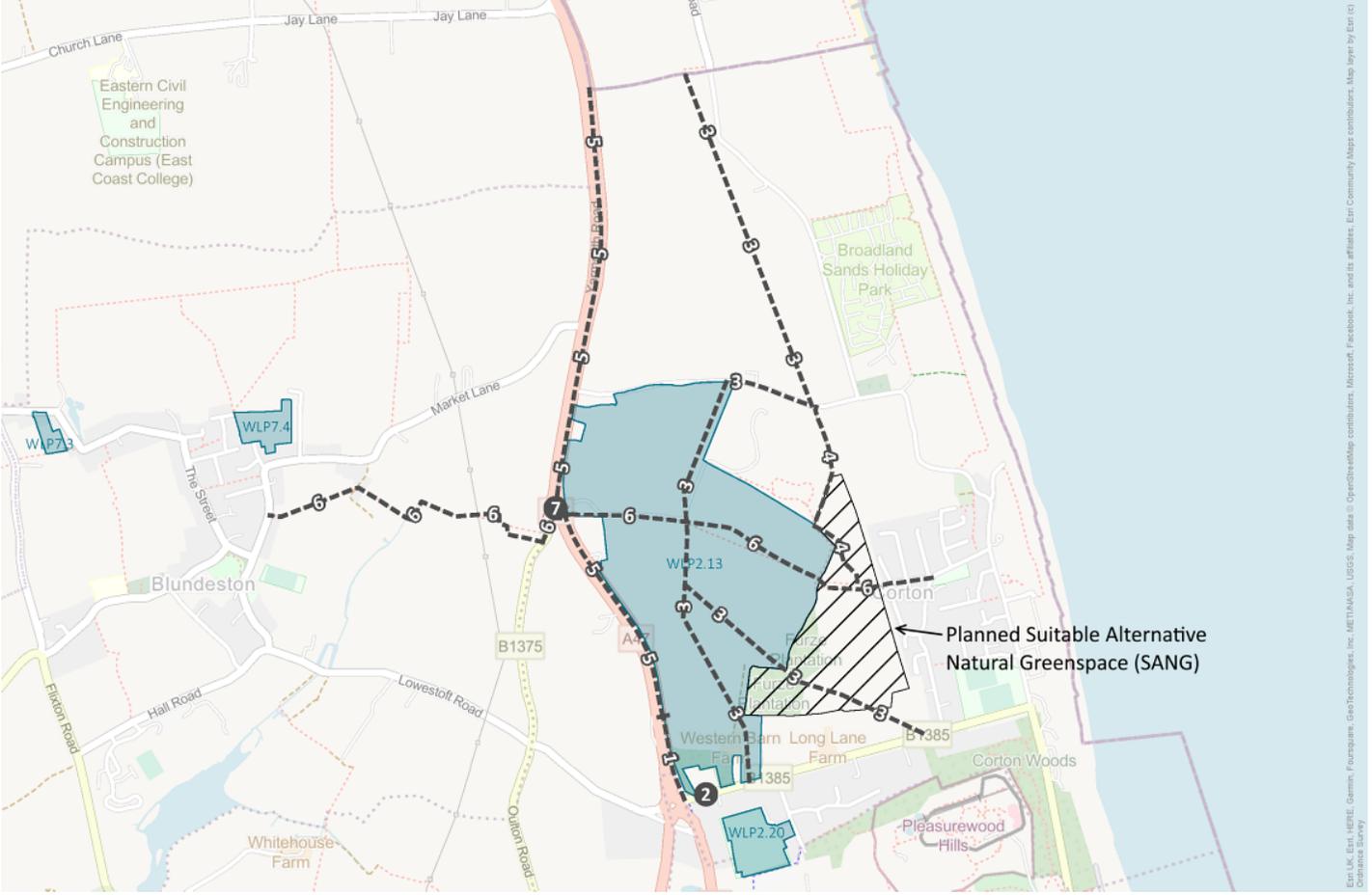
⁶⁵ <https://thinklowestoft.co.uk/regeneration/celebrating-our-culture-and-heritage/haz/>

⁶⁶ <https://www.eastsuffolk.gov.uk/business/regeneration-projects/powerpark/>

Parish	Policy	Recommendations
Lowestoft / Oulton Broad	WLP2.4 Kirkley Waterfront and Sustainable Urban Neighbourhood	 <p>1 - Ensure the cycling and walking infrastructure delivered as part of the Gull Wing crossing connects into the site and north of the river into Denmark Road and surrounding areas, with associated improvements. (see L14)</p> <p>2 - The existing cycling and walking route south of and parallel to Victoria Road and Kimberley Road should be enhanced and connected into the site. Cycling and walking crossing points should be introduced along Waveney Drive and Victoria Road. How this Connects into Oulton Broad South railway station and Nicholas Everitt Park is a key consideration. (see L16-L17)</p> <p>3 - Introduce cohesive and segregated cycling and walking routes throughout the site, ensuring east-west links throughout and connections to the existing infrastructure south of and parallel to Victoria Road and Kimberley Road. (see L13)</p>

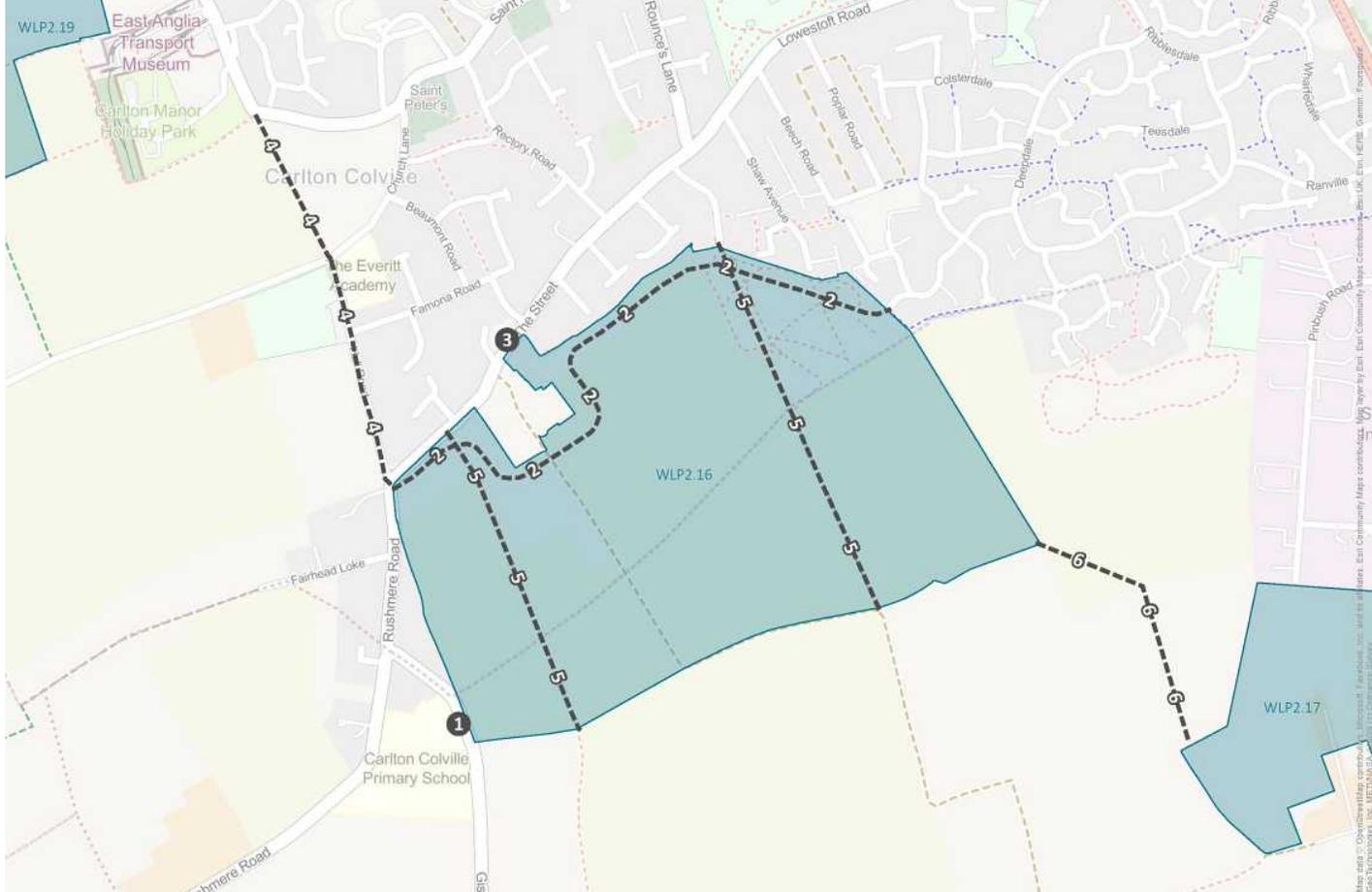
Parish	Policy	Recommendations
		<p>4 - Introduce a cycling and walking track along the north of Belvedere Road and Horn Hill, between Dunelm and Asda, and introduce cycling and walking priority over junctions. This will improve the connections to the town centre/train station. (L12)</p> <p>5 – Opportunity to introduce a cycling and walking bridge from the site at the Brooke Peninsula to Normanston Park. (see L13)</p> <p>6 – Cycling and Walking improvements should be in accordance with the Sustainable Urban Neighbourhood and Kirkley Waterfront Development Brief.⁶⁷</p>
Lowestoft	WLP2.5 East of England Park (The Ness)	<p>1 - Resurface the seafront cycling and walking track and where possible introduce segregation between cyclists and pedestrians. (see L33)</p> <p>2 - Introduce a cycling and walking track along Whapload Road, segregated by existing vegetation on the East of England Park. Ensure the cycling and walking track preserves the significance of the historic net drying racks.</p> <p>3 - Introduce cycling parking at Ness Point.</p>
Oulton Broad	WLP2.6 Western End of Lake Lothing	<p>1 - Introduce a cycling and walking track west to east through the site connecting to the Kirkley Waterfront and Sustainable Urban Neighbourhood (WLP2.2) to the east and, with an appropriate crossing, connect through the land to the west to Saltwater Way.</p>
Lowestoft	WLP2.7 Former Battery Green Car Park	<p>1 - Upgrade Battery Green Roundabout. The radial routes should be significantly improved to allow free flowing of cyclists and pedestrians around the edges. A Dutch-style roundabout or another alternative could be considered.</p> <p>2 - Remove the central cross hatching and central turning lane and introduce a segregated cycling and walking track along Battery Green Road. (see L41)</p> <p>3 - Introduce a modal filter to the eastern end of Marina, subject to necessary delivery access and disabled parking.</p> <p>4 - Introduce more cycle parking to the High Street.</p> <p>5 - Introduce a traffic free east-west route through the site, connecting the High Street to Battery Green Road/Gordon Road.</p>
Lowestoft	WLP2.8 Former Lowestoft Hospital	<p>1 - The constraints on streets around the site make the introduction of cycling and walking infrastructure challenging. If the area between Katwijk Way, Regent Road, London Road North, Artillery Way, and St Peter’s Street suffers from significant through traffic, it may be appropriate to introduce a Low Traffic Neighbourhood between these streets or other forms of modal filters.</p>
Lowestoft	WLP2.10 Inner Harbour Port Area	<p>1 - Introduce a segregated cycling and walking track along Commercial Road. This may require the removal of on-street parking, which could be accommodated by the introduction of a multi-storey car park for employees on the port site.</p> <p>2 - Upgrade Footpath 21 to a bridleway, widen and resurface accordingly with appropriate access provided into the allocation from the west.</p> <p>3 - Introduce a cycling and walking bridge over the railway line at Footpath 21.</p> <p>4 - Cycling and walking infrastructure delivered through the Gull Wing crossing should be connected into the cycling and walking infrastructure delivered on the allocation.</p>

⁶⁷ <https://www.eastsuffolk.gov.uk/assets/Planning/Waveney-Local-Plan/Supplementary-Planning-Documents/SUN-and-Kirkley-Waterfront/Adopted-Development-Brief.pdf>

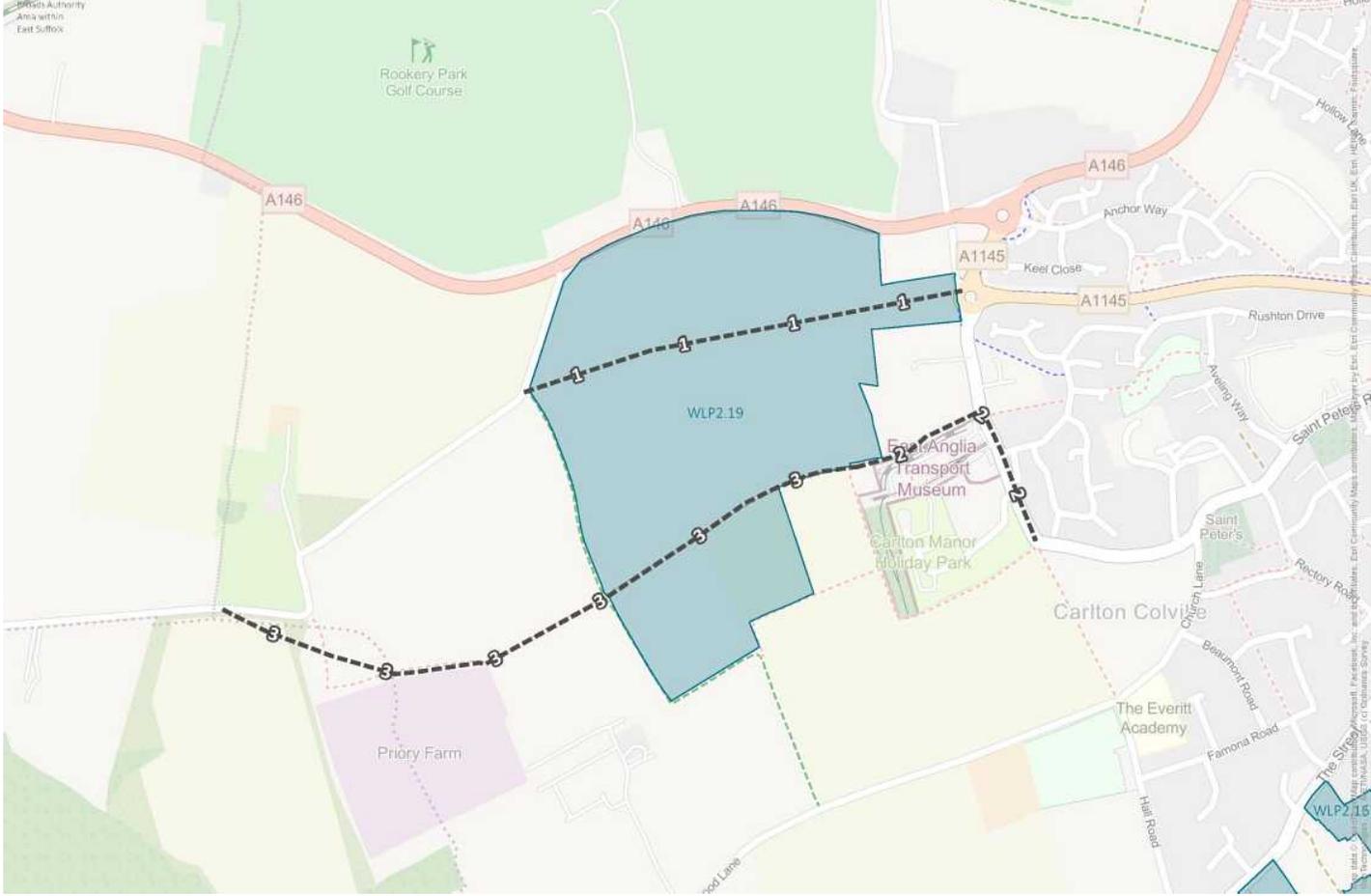
Parish	Policy	Recommendations
Corton	WLP2.13 North of Lowestoft Garden Village	 <p>1 - Open the existing dead-end road off Corton Long Lane just east of the junction with the A47 to cyclists and pedestrians only. Extend this route into the site whilst retaining as many trees as possible.</p> <p>2 - Introduce a cycling and walking crossing point on Corton Long Lane, just east of the A47 junction.</p> <p>3 - Introduce a segregated cycling and walking track through the site and north to the Suffolk/Norfolk border along the disused railway line or as part of the Broadland Sands Holiday Park expansion. (see LH1 to various)</p> <p>4 - Upgrade Footpath 4 to a bridleway, widen and resurface accordingly. (see LH3)</p>

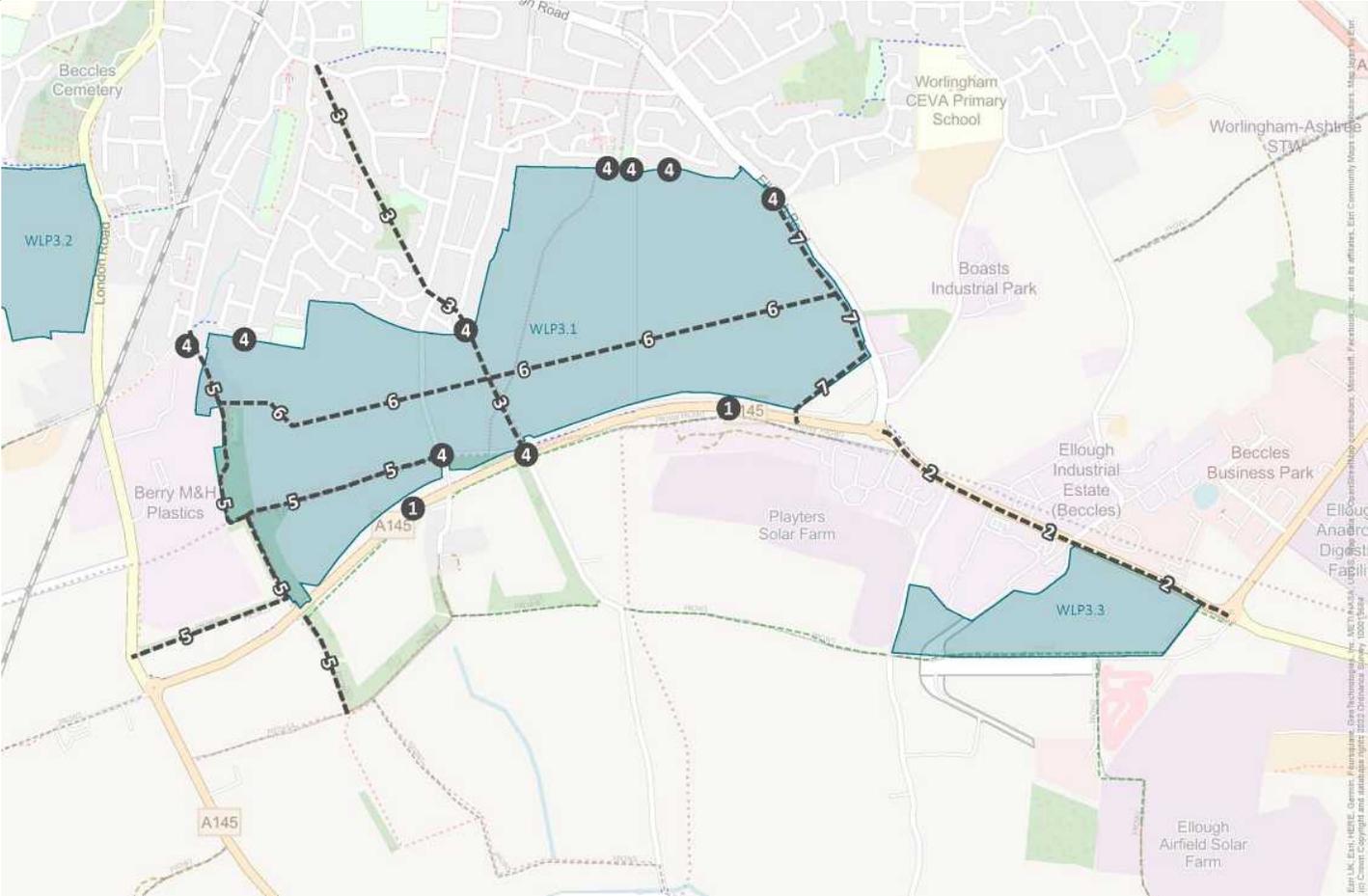
Parish	Policy	Recommendations
		<p>5 - Introduce a segregated cycling and walking track along the A47 between Corton Long Lane and Hopton, to the standard delivered along Millennium Way. (see LH2 to LH8)</p> <p>6 - Introduce a cycling and walking track along Footpath 3 (Corton), across the A47 and Gorleston Road, Footpath 11 (Blundeston), to Market Lane.</p> <p>7 - Introduce signalised cycling and walking crossing points over the A47 at Footpath 3 (Corton), and over Gorleston Road to access Footpath 11 (Blundeston).</p>
Oulton	WLP2.14 Land North of Union Lane	<p>1 – Introduce cycling and walking infrastructure in accordance with the Residential Development Brief (2021)⁶⁸.</p> <p>2 - Introduce a footway along Parkhill, from Union Lane and along the site frontage.</p> <p>3 - Make Union Lane and Flixton View a Cycle Street. Any vehicle accesses from Union Lane and Flixton View onto the site should be for emergency vehicles only.</p> <p>4 - Introduce a cycling and walking track between WLP2.15 (Land Between Hall Lane and Union Lane) and WLP2.14, via Footpath 6, which should be upgraded to a bridleway, widened and resurfaced accordingly. (see L56)</p> <p>5 - Introduce a cycling and walking track from the northeast corner of the site along Parkhill to Footpath 9 (which should be upgraded to a bridleway alongside Footpath 3) and The Pastures cut through.</p>
Oulton	WLP2.15 Land Between Hall Lane and Union Lane	<p>1 - Introduce a cycling and walking track along Hall Lane from the likely site entrance to the roundabout with the B1375, segregated from the road by the existing vegetation north of Hall Lane. (see L55)</p> <p>2 - Make Union Lane and Flixton View a Cycle Street. Any vehicle accesses from Union Lane and Flixton View onto the site should be for emergency vehicles only.</p> <p>3 - Introduce a cycling and walking crossing point on Hall Lane.</p> <p>4 - Introduce a segregated cycling and walking track along the southern side of Hall Lane from the recommended cycling and walking crossing point (at point 3 above) west to Footpath 4. (see L55)</p> <p>5 - Introduce a cycling and walking track between WLP2.14 (Land North of Union Lane) and WLP2.15, via Footpath 6, which should be upgraded to a bridleway, widened and resurfaced accordingly. (see L56)</p>

⁶⁸ <https://www.eastsuffolk.gov.uk/assets/Planning/Planning-Policy-and-Local-Plans/Supplementary-documents/Residential-development-Union-Lane-Oulton/Residential-Development-Brief-for-WLP2.14-Land-North-of-Union-Lane-Oulton.pdf>

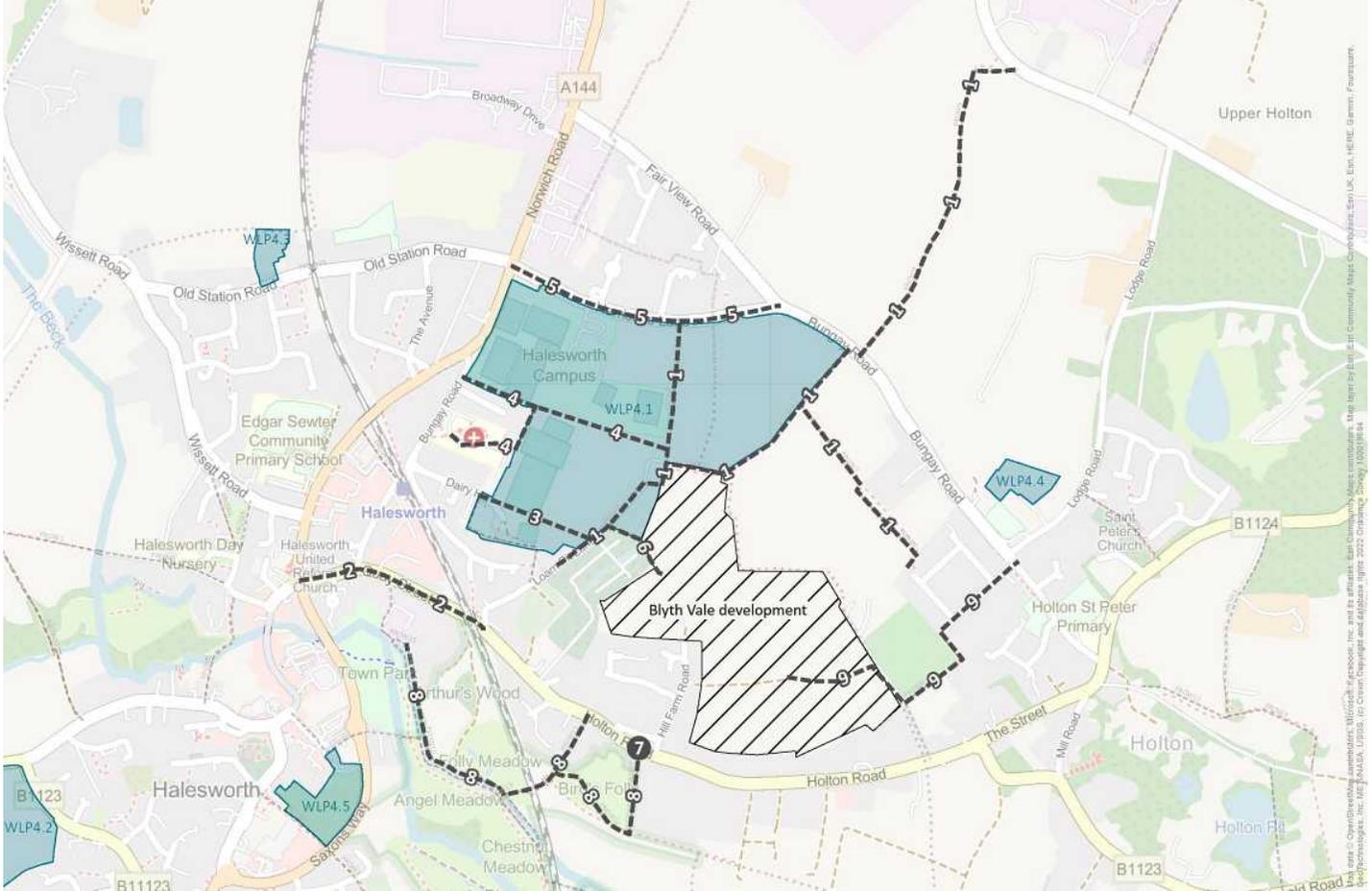
Parish	Policy	Recommendations
Carlton Colville / Gisleham	WLP2.16 Land South of The Street	 <p>1 - Introduce a cycling and walking crossing point on Gisleham Road, from the southwest corner of the site to Carlton Colville Primary School.</p> <p>2 - Introduce a segregated cycling and walking track along the northern site boundary.</p> <p>3 - Introduce cycling and walking crossing points along The Street.</p> <p>4 - Consider cycle improvement along Hall Lane. Connect the northwest corner of the site to the Hall Road cycling and walking track via two cycling and walking crossing points on The Street and Hall Road. Connect improvements to the Carlton Colville recreation ground and over Mutfordwood Lane via a cycling and walking crossing point and along Footpath 14, which should be upgraded to a bridleway, widened and resurfaced. (see L28)</p> <p>5 - Improve existing PROW routes internal to the site to allow cycling.</p>

Parish	Policy	Recommendations
		6 – Create cycling/walking connection between allocation WLP2.16 and WLP2.17 (Land at South Lowestoft Industrial Estate) ensuring access to employment opportunities.
Gisleham	WLP2.17 Land at South Lowestoft Industrial Estate	<p>1 - Introduce a cycling and walking track between WLP2.16 (Land South of the Street, and the wider network) and WLP2.17 through to Church Road.</p> <p>2 - Widen the existing shared cycle/footway around the A12/Church Road roundabout and where possible narrow road arms to allow safer crossing.</p> <p>3 - Widen the existing shared cycle/footway along Tower Road, and ensure cyclists and pedestrians have priority over side streets.</p> <p>4 - Introduce a segregated cycling and walking track along the northern site boundary, connecting into Tower Road in the east and along field boundaries west of the site to WLP2.16. A cycling and walking crossing point will be required over any vehicle access off Pinbush Road. This track should connect onto Church Road via a comprehensive cycling and walking route.</p>
Oulton	WLP2.18 Land at Mobbs Way	<p>1 - Introduce a cycling and walking track along the southern site boundary, between the Woods Meadow shared cycle/footway and Gorleston Road. Vegetation removal should be limited to that which is necessary to create the cycling and walking track. The track should be designed to make the most of natural surveillance. (see L53)</p> <p>2 - Introduce a segregated cycling and walking track along the western side of Gorleston Road, between the recommended cycling and walking track south of Mobbs Way employment area and Dunston Drive.</p> <p>3 - Introduce cycling and walking connections from the site onto the Woods Meadow shared cycle/footway.</p>

Parish	Policy	Recommendations
Carlton Colville	WLP2.19 Oakes Farm, Beccles Road	 <p data-bbox="609 1161 2029 1329"> 1 - Introduce a segregated cycling and walking track through the allocation, east to west, from the Chapel Road/A1145 roundabout to Mutford Wood. The Chapel Road/A1145 roundabout crossing points should be upgraded to allow continuous cycling and walking over the roundabout arms. (see LB3) 2 - Introduce a cycling and walking track from the south east corner of the site, south and east along the western built edge of Carlton Colville and connect into Footpath 14. Footpath 14 should be upgraded to a bridleway, widened and resurfaced accordingly. 3 - Upgrade Footpath 15 to a bridleway, widen and resurface accordingly. </p>
Corton	WLP2.20 Gunton Park, off Old Lane	<p data-bbox="609 1334 2029 1385"> 1 - Upgrade Old Lane to a Cycle Street. 2 - Widen the shared cycle/footway between Old Lane and Gunton Avenue and introduce sensitive lighting. </p>

Parish	Policy	Recommendations
Beccles / Worlingham / Ellough / Weston	WLP3.1 Beccles and Worlingham Garden Neighbourhood	<p>3 - Upgrade Footpath 31 to a bridleway, widen and resurface accordingly. Introduce appropriate lighting along the route.</p>  <p>1 - The southern relief road cycling and walking track should be connected into the cycling and walking routes within the allocation. 2 - The Benacre Road cycling and walking track should be continued east to the Benacre Road/Copland Way roundabout. (see LB17) 3 - A traffic free cycling and walking track connecting Oak Lane (or alternatively Cucumber Lane) and the southern relief road should be introduced leading to an attractive green corridor through to Rigbourne Hill which should be improved and widened where possible to encourage cycling. (see LB19)</p>

Parish	Policy	Recommendations
		<p>4 - As required by Policy WLP3.1, cycling and walking connections should be made between the allocation and Bluebell Way, Cucumber Lane, Darby Road, Nicholson Drive, Oak Lane, Field View Gardens/Foxglove Close and Cedar Drive.</p> <p>5 - Footpath 18 should be upgraded to a bridleway, widened and resurfaced accordingly. Bridleways 12 and 15 should be widened and resurfaced and connect onto the southern relief road south of Marlborough Cottage. Footpath 3 should be upgraded to a bridleway, widened and resurfaced accordingly and connect the southern relief road to Bridleway 15.</p> <p>6 - Ensure cycling and walking connections east-west through the site. (see LB20)</p> <p>7 - Provide a link alongside Ellough Road through the allocation to connect into the southern bypass infrastructure. This will require a suitable crossing point potentially near Cedar Drive. (see LB16)</p>
Beccles	WLP3.2 Land West of London Road	<p>1 - Introduce a cycling and walking route alongside and segregated from London Road, between the cycling and walking route north of the site and the A145, where possible.</p> <p>2 - Connect the site to the existing cycling and walking route along the northern site boundary and continue cycling and walking routes through the site.</p> <p>3 - The London Road crossing point adjacent the northeast corner of the site should be upgraded to a parallel or signalised cycling and walking crossing.</p>
Ellough	WLP3.3 Land South of Benacre at Ellough Airfield	<p>1 - The Benacre Road cycling and walking track should be continued east to the Benacre Road/Copland Way roundabout. (see LB10)</p> <p>2 - Introduce a cycling and walking track along the western site boundary, segregated from Church Road by the existing vegetation.</p> <p>3 - Introduce a segregated cycling and walking track along Copland Way, connecting into Lowestoft Road with a suitable cycling and walking crossing point of the Copland Way/A146 roundabout. (see LB11)</p> <p>4 - The Copland Way/Benacre Road roundabout should be upgraded to a cycle and pedestrian friendly roundabout, thereby connecting the recommended segregated cycling and walking tracks along Benacre Road and Copland Way.</p> <p>5 - Bridleways 1 and 2 that run along the eastern and southern site boundaries should be widened and resurfaced to accommodate cycling and walking.</p>

Parish	Policy	Recommendations
Halesworth / Holton	WLP4.1 Halesworth/Holton Healthy Neighbourhood	 <p>1 - Upgrade Footpaths 7 (Halesworth), 16 (Halesworth), 7 (Holton), 8 (Holton) and 9 (Holton) to bridleways, widen and resurface to accommodate cycling and walking.</p> <p>2 - Widen the footway along Quay Street to create a shared foot/cycleway with on-road parking retained where possible to ensure suitable parking provision for residential properties. Quay Street will need to be narrowed at the junction with Loam Pit Lane to accommodate the shared cycle/footway. The narrowing of the road will not only allow for the introduction of a shared cycle/footway, but also slow traffic on Quay Street and give priority to vehicles in the westbound direction over vehicles eastbound. This provides a connection between the Healthy Neighbourhood and the town centre by connecting to Loam Pit Lane.</p>

Parish	Policy	Recommendations
		<p>3 - Introduce a cycling and walking connection between Loam Pit Lane and Dairy Hill.</p> <p>4 - Widen the existing footway between Bungay Road and the site (behind the tree line) to allow cycling with appropriate segregation and extend the route to connect to Footpath 7 (Halesworth).</p> <p>5 - Continue the existing Harrisons Lane shared cycle/footway along the site frontage with Harrisons Lane, segregated from Harrisons Lane by the existing vegetation.</p> <p>6 - Introduce a cycling and walking track from Loam Pit Lane, along the northern boundary of the cemetery, east along the existing field boundary and into the Hill Farm Road development.</p> <p>7 - Introduce a cycling and walking crossing point on Holton Road where Footpath 21 meets Holton Road.</p> <p>8 - Upgrade Footpaths 21, 22, 11, 10, and 9 (Halesworth) to bridleways, widen and resurface accordingly, improve the exit of Footpath 11 onto Holton Road.</p> <p>9 - Introduce walking connections between the open space route of the Hill Farm Road development onto Footpath 6, to allow safe off-road access to Holton St Peter Primary School.</p>
Halesworth	WLP4.2 Land Adjacent to Chediston Street	<p>1 - Introduce cycling and walking infrastructure along the site frontage of the B1123 (Chediston Street and Roman Way).</p> <p>2 - Introduce a cycling and walking crossing point on Roman Way.</p> <p>3 - Ensure suitable internal connections that provide cycling and walking benefits to the residents with preference to connecting to Allington Road, subject to land ownership.</p>
Halesworth	WLP4.3 Land North of Old Station Road	<p>1 - Introduce a segregated bi-directional cycling and walking track through the site, from Old Station Road to the northern site boundary. This is to future proof cycling and walking infrastructure along the railway line heading north.</p>
Holton	WLP4.4 Land West of Lodge Road	<p>1 - It should be explored whether a pedestrian connection can be made between the eastern corner of the site and Footpath 5.</p>
Halesworth	WLP4.5 Land at Dairy Farm, Saxons Way	<p>1 - Introduce cycling and walking track along the WLP4.5 frontage of Saxon's Way with preference given to improvements to the east side of Saxons Way.</p> <p>2 - Replace the existing island crossing on Saxons Way at Swan Lane with a zebra crossing with consideration for a toucan crossing should recommendation 2 and 3 both be carried out.</p> <p>3 - Upgrade Footpath 3 to a bridleway where possible.</p> <p>4 - Introduce cycle parking, close to recommended cycling routes, community centre, and/or White Swan pub.</p> <p>5 - Upgrade Footpath 9 to a bridleway, widen and resurface. Connect the bridleway with the cycling and walking track recommended in point 1.</p>
Halesworth / Spexhall	WLP4.6 Broadway Farm, West of Norwich Road	<p>1 - Introduce a segregated cycling and walking track along Norwich Road, from the existing cycling and walking infrastructure at Zemke Way to and through the site.</p> <p>2 - Introduce a cycling and walking crossing point on Norwich Road, just south of Zemke Way.</p> <p>3 - Introduce cycling and walking crossing points to all four arms of the A144/Sparrowhawk Road roundabout.</p>
Bungay	WLP5.1 Land East of St Johns Road	<p>1 - Introduce Toucan crossing to St John's Road to enable cycling and walking connection from the site to existing cycling and walking infrastructure along the west of St John's Road as well as the bus stops.</p> <p>2 - A cycling and walking track should run along St John's Road and the northern site boundary and be connected to Mayfair Road and Meadow Way. The cycling and walking track along St John's Road should be segregated from the road.</p> <p>3 - Between Joyce Road and Hillside Road East, upgrade Footpath 31 to a bridleway, and widen and resurface accordingly.</p> <p>4 - Introduce a segregated cycling and walking track to Hillside Road East, with appropriate crossing points to ensure safe cycling and walking to and from the former Bungay Middle School site.</p> <p>5 - Widen existing cycling and walking track along the west of St John's Road, whilst ensuring segregation from St John's Road.</p>

Parish	Policy	Recommendations
Bungay	WLP5.2 Land West of St Johns Road	<ul style="list-style-type: none"> 1 - Widen the Kings Road footway to create a shared cycle/footway, whilst retaining grass verges where possible. 2 - Upgrade Footpath 15 to a bridleway, widen and resurface accordingly. 3 - Introduce a cycling and walking crossing point where Manor Road meets Hillside Road West. 4 - Reconfigure St John's Road/Kings Road junction to slow traffic speeds. 5 - Introduce a segregated cycling and walking track along St John's Road, along the site frontage, the swimming pool site frontage, and as far north into Bungay as feasible given the available road widths. 6 - Introduce cycling and walking connections onto Ethel Mann Road and Thomas Bardwell Drive.
Reydon	WLP6.1 Land West of Copperwheat Avenue	<ul style="list-style-type: none"> 1 - Upgrade Footpaths 1 (north of Footpath 2 connection) and 2 to bridleways, widen and resurface accordingly. 2 - Introduce a cycling and walking connection from Footpath 2 to Kingfisher Crescent. 3 - Introduce a segregated cycling and walking track along The Drive and Nightingale Avenue, between the A1095 and Wangford Road.
Barnby	WLP7.2 Land Between The Street and A146	<ul style="list-style-type: none"> 1 - The existing footway along the north of The Street between the Primary School and Swan Lane should be widened. 2 - A modal filter should be introduced on The Street, outside the Primary School, accompanied by planting and space for children to wait safely at school drop off and pick up times.
Blundeston	WLP7.3 Land South of Lound Road	<ul style="list-style-type: none"> 1 - Pedestrian access to Footpath 2, which runs along the southern site boundary, should be provided alongside improvement to the existing footpath access. Appropriate lighting should be introduced to Footpath 2 to ensure safety whilst respecting the rural location.
Blundeston	WLP7.4 Land North of Pickwick Drive	<ul style="list-style-type: none"> 1 - Introduce pedestrian connection to Footpaths 8 and 19 and provide a walking route through the site for onward travel.
Somerleyton	WLP7.5 Land North of The Street	<ul style="list-style-type: none"> 1 - Introduce a cycling and walking connection through the site, from The Street to the allotments north of the site.
Somerleyton	WLP7.6 Mill Farm Field	<ul style="list-style-type: none"> 1 - Introduce a cycling and walking track along the site frontage with Station Road, segregated from the road by the existing vegetation. 2 - Connect the recommended cycling and walking track into Footpath 5. 3 - Upgrade Footpath 4 to a bridleway, widen and resurface accordingly.
Wangford	WLP7.7 Land North of Elms Lane	<ul style="list-style-type: none"> 1 - Introduce a footway along the site frontage with Elms Lane, segregated from the road by the existing hedgerow.
Wrentham	WLP7.8 Land North of Chapel Road	<ul style="list-style-type: none"> 1 - Introduce a cycling and walking track along the western site boundary. The track should link into Footpath 6 to the south, which should be upgraded to a bridleway to enable cycle access. 2 - Introduce a cycling and walking crossing point on Chapel Road, preferably where the recommended cycling and walking track along the western site boundary meets Footpath 6 provided appropriate visibility splays can be achieved. 3 - Introduce a village gateway on Chapel Road, where Footpath 6 meets Chapel Road, provided appropriate visibility splays can be achieved. The village gateway should be on the eastbound carriageway, thereby primarily slowing traffic entering the village from a national speed limit. The village gateway will also make crossing Chapel Road from WLP7.8 into Footpath 6 much safer and more comfortable. 4 - Explore opportunities to introduce a cycling and walking track between the north west corner of the site and Footpath 4. Upgrade Footpath 4 to a bridleway to enable cycle access. As the site boundary does not reach Priory Road and as a stream appears to run west to east just north of the site, a pedestrian and cycle crossing point of the stream may be required.
Brampton	WLP7.9 Land South of Southwold Road	<ul style="list-style-type: none"> 1 - Introduce a segregated cycling and walking track along the site frontage of the residential areas of the site, segregated from Southwold Road by the existing vegetation. 2 - Connect the allocation to Woodside opposite by a cycling and walking track, running between the car park and playing pitch, as per the WLP7.9 indicative masterplan.

Parish	Policy	Recommendations
Brampton	WLP7.10 Land at Toodley Farm	1 - Introduce a cycling and walking track from the site to Bridleway 23.
Ilketshall St Lawrence	WLP7.11 Land South of Hogg Lane	1 - As per Policy WLP7.11 the existing footway on Hogg Lane will require extending to the site. This will facilitate pedestrian access along Hogg Lane and to Footpath 14 which adjoins Hogg Lane.
Lound	WLP7.12 Land East of The Street	1 - Upgrade Footpath 11 to a bridleway, widen and resurface accordingly. 2 - Introduce a footpath through the hedgerow along the southern site boundary to connect to the footpath on the village green.
Mutford	WLP7.13 Land North of Chapel Road	1 - Introduce a footway along the site frontage, east of the retained hedgerow.
Ringsfield	WLP7.14 Land North of School Road	1 - Widen the existing School Road footway. 2 - Introduce walking and cycling connections from the site to Bridleway 8, which runs along the western site boundary.
Rumburgh	WLP7.15 Land East of Mill Road	1 - Introduce a footway along the site frontages with Mill Road and The Street, segregated from the roads by the existing hedgerow. 2 - Explore the possibility of introducing a footway along The Street to the Rumburgh Allotments.
Willingham	WLP7.16 Land East of Woodfield Close	1 - Introduce a footway along the site frontage with Sotterley Road, segregated from the road by the existing vegetation and connected into the footway along Sotterley Road to the west of the site and into the recreation ground to the north. 2 - Introduce a cycling and walking track from Sotterley Road, through the recreation ground, north to Footpath 2 along the rear property boundaries off London Road.
Westhall	WLP7.17 Land West of Lock's Road	1 - Introduce a cycling and walking track from Lock's Road, through the site, through the recreation ground, and onto the car park off Wangford Road.

Community Recommendations

- 4.63 797 comments were submitted to the initial map-based consultation (19 October 2020 to 7 December 2020). These comments identified cycling and walking relevant issues and opportunities across East Suffolk. After assessing these comments against a methodology created in collaboration with Suffolk County Council (SCC), explained in more detail below, they form part of the Strategy as Community Recommendations. Officers considered the comments received following the formal consultation and amended the scores where appropriate.
- 4.64 The list of the Community Recommendations and their assessments can be viewed in the separate 'Community Recommendations' document (Appendix 1)⁶⁹.
- 4.65 Comments made on SCC specific matters (e.g. speeding, highways maintenance, and Quiet Lanes) have been shared with SCC for their consideration as the Highways Authority.

Methodology

- 4.66 A Multi-Criteria Assessment Framework (MCAF) has been used to assess the comments.
- 4.67 An MCAF assessment sets out a criteria in which to score each comment. The categories that have been used include: Connectivity and Growth, Modal Shift, Optimisation, Safety, Biodiversity and Leisure. The chosen

criteria largely reflect that used by SCC. Providing continuity between the district council and county council should provide some consistency in determining value.

- 4.68 East Suffolk was keen to shape the categories to be more specific to East Suffolk itself and provide a criterion more in keeping with local values. Accordingly, leisure was included to show the value that the visitor economy has to the district. Furthermore, providing more leisure-based infrastructure can provide significant benefits to local residents.
- 4.69 The scores range from -3 which represents a significant adverse impact through to 3 which represents a significant benefit. A score of 0 means it represents a neutral impact either because the improvement will have little impact to that particular criterion or the positive and negative impacts balance each other out. Given there are 6 categories a maximum score of 18 is possible, but none of the improvements identified achieved such a high score.
- 4.70 A summary of each category is listed below, along with a brief explanation of scoring considerations. It is important to remember that each improvement is subject to an individual set of circumstances and whilst efforts have been made to ensure as much consistency as possible each will be subject to planning judgement.

⁶⁹ <https://www.eastsuffolk.gov.uk/planning/planning-policy-and-local-plans/supplementary-planning-documents/>

Table 1 MCAF category and scoring explanation

Name	Category Explanation	Scoring explanations
Connectivity and Growth	This criterion determines whether the improvement will result in a better-connected locality and encourage sustainable growth.	Higher scores will be given to new infrastructure that provides connectivity to isolated developments/settlements to key services. The better connected a settlement/development already is or the less valuable the settlement or services that the connection is to will the lower the score. Locations already well connected may not score in this category.
Modal Shift	This criterion rates the improvement in terms of encouraging users of motor vehicles to taking more trips by sustainable means.	Propensity to Cycle Tool and Datashine ⁷⁰ websites are used to gauge potential modal shift. Significant growth in modal shift will result in a high score whilst limited growth will result in no score. The amount of people who benefit and the extent and quality of the infrastructure will have an impact. High-quality well-connected infrastructure will have a bigger impact than low quality or infrastructure poorly connected.
Optimisation	This criterion provides a rating of how it improves existing infrastructure.	Improvements that provide significant upgrades to existing infrastructure will score highly. Minor improvements will not score as highly and brand new infrastructure is unlikely to score.
Safety	This criterion considers whether the improvement will result in a safer network for cycling and walking. This criterion considers the uplift on the existing situation and does not determine whether the existing situation is safe or unsafe, but how much the suggestion makes it safer.	Improvements to routes which contain high speed traffic movements and take the cyclists off-road will score highly. Low speed roads will either score lower or be deemed neutral. The quality of the proposed infrastructure will be a factor.
Biodiversity	This criterion assesses whether the improvement can result in biodiversity gains or losses.	Proposals which result in significant new planting will score highly, whilst development which results in the loss of high value biodiversity assets will likely receive a significant minus score.
Leisure	This criterion assesses whether the improvement encourages leisure use. These routes are likely to be used less frequently by individuals and offer scenic routes and attractive locations.	Improvements that provide connections to key tourist attractions or are a high value attraction in itself will score highly. Improvements which provide more 'everyday' services connections or provide an unattractive route will score lower or be neutral.

Please note these are broad explanations and each recommendation will be subject to planning judgement in the scores given

⁷⁰ <https://datashine.org.uk/#table=QS502EW&col=QS502EW0010&ramp=YIOrRd&layers=BTtT&zoom=12&lon=-0.1500&lat=51.5200>

Table 2 MCAF scoring explanation

-3	-2	-1	0	1	2	3
Significant Adverse Impact	Adverse Impact	Slight Adverse Impact	No Discernible Impact or effects uncertain	Slight Positive Impact	Positive Impact	Significant Positive Impact

Glossary

A

Advisory cycle lane

A dashed white line marked on the carriageway denoting an area for cyclists for use. Motor vehicles should try not to enter this area unless unavoidable, but parking and loading is allowed unless specific restrictions forbid it.

Advanced stop line

A stop line specifically for cyclists placed ahead of the stop line for motor vehicles.

Area of Outstanding Natural Beautiful (AONB)

An area designated at a national level because of its outstanding landscape quality. Development within these areas is tightly controlled.

All-user track

A route designed to be suitable and safe for use by cyclists, pedestrians and horse riders. Their design takes into account the specific needs of horses in terms of width and surface treatments to ensure their and their rider's safety.

B

Bi-directional cycle track/path

A segregated cycle path/track where cyclists are allowed to travel in both directions.

Bridleway

A publicly available route first created for equestrian use but now extended to pedestrians and cyclists. Cyclists should give way to horse riders and pedestrians.

Bollard

A vertical post that is used to restrict and manage the movement of vehicles and people.

Buffer/Segregation

Often a strip that provides additional space to reinforce the need for motor vehicles to give safe space to cyclists.

Byway

A highway over which the public have a right of way (can be restricted) for vehicular and all other kinds of traffic, but which is used by the public mainly for the purposes for which footpaths and bridleways are used.

C

Cambridge kerb

An angled kerb designed to provide segregation between cyclists and motorists whilst enabling smooth transition for cyclists between cycle infrastructure and the carriageway.

Carriageway

Part of the street used for motor vehicles.

Coloured surfacing

A surface material different in colour to a standard surface often used to denote areas intended for cyclists.

Community Infrastructure Levy (CIL)

This is a standard fee that is applied to new development to pay for infrastructure that supports new development within the District.

Contraflow

A lane or facility that allows a user (often cyclists) to travel against the normal flow and direction of traffic.

County Wildlife Site (CWS)

Areas of important biodiversity that sit outside the nationally protected areas.

Cycle bypass

A separation that allows the cyclist to avoid restrictions placed on other road users.

Cycle-only filter lights

A filter light added to traffic light systems that provide cyclists with a number of seconds head start over motor vehicles to cross the junction safely.

Cycle street

A street open to all users where cyclists are prioritised and vehicles must give way.

Cyclops crossing

A series of signalised crossings for cyclists in orbit around a junction.

D

Desire line

A route taken by significant numbers marking the desired route by people that is not necessarily on formal infrastructure.

Dutch entrance kerb

A type of ramped kerb that allows cyclists and pedestrians to remain at a continuous flat level.

Dutch-style roundabout

A form of roundabout first seen in the Netherlands that gives greater priority to cyclists. The cyclists are separated from other road users with orbital cycle tracks.

E

E-bike

A bike that can be powered by electricity to assist with pedalling.

F

Footpath

A legally designated Public Right of Way for use on foot only.

G

Greenways

An off-road route which often utilises pleasant and ‘green’ foliage.

H

Habitats Regulation Assessment

The purpose of the Habitats Regulations Assessment (HRA) is to identify potential to cause a likely significant effect on Natura 2000 or European sites (Special Areas of Conservation (SACs), Special Protection Areas (SPAs) and Ramsar sites), (either in isolation or in combination with other plans and projects), and to identify appropriate avoidance and mitigation strategies where such effects were identified.

I

Infrastructure Development Framework (IDF)

The Infrastructure Delivery Framework sets out in detail all of the infrastructure required to support the growth outlined in the Local Plan, the timescale for delivery, likely cost and funding options (where known).

J

Junction table/Raised table

A raised area within the carriageway used to control vehicle speed, often at junctions.

L

Low traffic neighbourhood

An area closed off to through vehicular traffic, which facilitates safer cycling and walking.

M

Mandatory cycle lane

Part of the carriageway marked with a solid white line for use of cyclists during the relevant times of operation. Motor vehicles should not use the cycle lanes and should not park in these areas unless there are site specific exemptions.

Modal filter

This is usually a physical feature (often bollards or planters) that blocks motor vehicle access to a carriageway (typically a residential street) to reduce the number of vehicle movements or prevent access completely. Typically they restrict motor vehicles, but allow cyclists, pedestrians and emergency vehicles. They can be permanent fixtures, collapsible/removeable for emergency vehicles or temporary (for example, for use during School Streets operation times). Modal filters can also be camera technology based, such as Automatic Number Plate Recognition (ANPR) system filters where unauthorised vehicles or vehicles travelling along a route at a prohibited time of day are identified on camera and fines are subsequently issued.

Modal shift

To change from one transport mode to another. In the context of this Strategy it is the aim to create new infrastructure that encourages a shift from using motor vehicles to cycling or walking.

N

Nationally Significant Infrastructure Projects

Major infrastructure developments that bypass normal local planning requirements due to their strategic importance and are dealt with at Government level. They are major infrastructure projects such as new harbours, roads, power generating stations (including offshore wind farms) and electricity transmission lines, which require a type of consent known as ‘development consent’. Development consent, where granted, is made in the form of a Development Consent Order (DCO). For such a project, the Planning Inspectorate examines the application and will make a recommendation to the relevant Secretary of State, who will make the decision on whether to grant or to refuse development consent.

National Cycle Network (NCN)

A national cycle network created by Sustrans denoting areas that are preferable for cyclists to use.

O

Orca Wands

Flexible and highly visible bollards to indicate presence of cycle lane.

P

Passenger car unit (PCU)

A method that distils all users into a specific unit to measure traffic flow.

Pegasus crossing

A form of crossing used by cyclists, pedestrians and horse riders. Pegasus crossings typically have two points of operation to meet the needs of pedestrians, cyclists and mounted horse riders. Similar to a puffin crossing Pegasus crossings have a same-side push operated pedestrian/horse rider control box which lights up showing a 'red man/horse and rider' or 'green man/horse rider' to indicate whether or not it is safe for users to cross. Pegasus crossing may or may not emit sound when it is safe to cross, and may or may not have a rotating cone provided at the base of the control box for pedestrians who are visually or hearing impaired.

Pelican crossing

A signal-controlled pedestrian crossing. The pelican crossing is characterised by a same-side push operated pedestrian control box and the appearance of a 'red man' or 'green man' on the opposite side of the crossing to indicate to the pedestrian when it is safe to cross. The control box will indicate in the interim that the pedestrian is required to wait using an illuminated 'WAIT' message or small red light. Pelican crossings will usually emit sound when it is safe to cross, and may or may not have a rotating cone provided at the base of the control box for those who are visually or hearing impaired. The 'green man' is followed by a flashing 'green man' for pedestrians and a flashing amber light for vehicles.

Pinch point

A location where the usable space is limited and where higher numbers of road users are travelling often through a narrow piece of infrastructure. Pinch points can be designed as traffic calming measures.

Play street

Temporary closures of a road to allow for play on the street.

Public Right of Way (PROW)

Public Rights of Way (PROW) allow the public the legally protected right to: walk (footpaths); walk, cycle or horse ride (bridleways), or; walk, cycle, horse ride or drive (byway) over land that is not otherwise publicly accessible, such as privately-owned land. PROW designations are usually no more than 2-3 metres wide, meaning users are required to keep to the mapped routes PROWs in Suffolk are identified in Suffolk County Council's 'Definite Map' series, and are signed on the ground using finger posts or signage on boundary treatments to the land being crossed (fence posts, gates, etc.).

Puffin crossing

A signal-controlled pedestrian crossing that indicates when it is safe for pedestrians to cross using a same-side push operated pedestrian control box which lights up showing a 'red man' or 'green man' to indicate whether or not it is safe for the pedestrian to cross. They differ from pelican crossings in that they are more inclusive for people who are visually impaired, and can be programmed to emit or not emit sound when it is safe to cross (making them more appropriate in residential areas). They may or may not have a rotating cone provided at the base of the control box for those who are visually impaired. . Detectors can also be present to allow the crossing period to be extended or cancelled as required.

Q

Quiet lane

A Quiet Lane is a nationally recognised designation of single-track road (i.e. no line markings), typically with less than 1000 vehicles using it per day. The designation does not prohibit vehicular traffic, but instead seeks to encourage considerate use.

R

Ramsar site

A term adopted following an international conference, held in 1971 in Ramsar in Iran, to identify wetland sites of international importance.

Refuge island

An area within the carriage way used either to provide safe places to stop for pedestrians crossing the road or allow for vehicles to turn right against the flow of traffic.

S

Scheduled Monument

A form of heritage protection, Scheduling is the selection of nationally important archaeological sites.

School street

A School Street is a road outside a school with a temporary restriction on motorised traffic at school drop-off and pick-up times. The restriction applies to school traffic and through traffic.

Section 106

A legal agreement between the Council and a developer to, for example, provide affordable housing and infrastructure needed to support a new development. These have been largely replaced by the Community Infrastructure Levy.

Segregated cycle lane

Cycle infrastructure within a carriageway that is physically separated from vehicles by way of kerbing, orca wands or similar design features that prevent the lane being accessed or parked over by vehicles.

Segregated pedestrian and cycle track

Cycling and walking infrastructure that is physically completely separated from carriageways, either through being routed through open/vehicle-free spaces or, where placed close to a carriageway, through the use of a significant means of physically dividing users, such as through a wide strip of grass or planted/tree-lined verge, railings or other means of creating significant physical segregation from vehicles. Unlike shared paths or cycle lanes, tracks running close to carriageways do not have immediate access to the carriageway.

Shared path

A wide foot and cycleway that runs adjacent to the carriageway and elevated from it (like a pavement). Shared paths are useable by both pedestrians and cyclists with or without internal segregation between the two. The safety of shared paths may be enhanced through the use of minor further physical means of segregation such as bollards or knee railing.

Shared surface/street

A type of street that allows multiple users to share and negotiate safe use of the space, reducing the dominance of motor vehicles.

Sheffield cycle stand

A form of tubular cycle stand used for cycle parking.

Site allocation

An area of land allocated for a specific type of development within the Local Plan or a Neighbourhood Plan.

Sites of Special Scientific Interest (SSSI)

A formal designation of land that usually contains particular flora or fauna of scientific interest.

Special Area of Conservation (SAC)

This is an area designated under the European Habitats Directive to give special protection to plants, animals and habitat

Special Protection Area (SPA)

This is an area identified as being of value for the feeding, breeding, migrating and wintering of threatened bird species. These sites are identified under the European Wild Birds Directive and receive enhanced protection.

Stepped track

A cycle track that is raised above the carriageway, but below the pedestrian footpath with no other form of segregation often incorporating a Cambridge Kerb (an angled kerb).

Strategic Environmental Assessment

A systematic process for evaluating the environmental implications of a proposed policy, plan or programme which provides a means for looking at cumulative effects to appropriately address them at the earliest stage of decision making alongside economic and social considerations.

Suitable Alternative Natural Greenspace (SANG)

Greenspace suitable as mitigation to offset development.

Surface treatment

The material used for surfacing a highway, for example use of hoggin or rubber crumb to surface a bridleway or footpath.

T

Tiger/Parallel crossing

A crossing that is shared between both pedestrians and cyclists it is similar in look to a 'zebra crossing' but wider.

Toucan crossing

A signal-controlled crossing very similar to the puffin crossing that can be used by both pedestrians and cyclists with a larger and shared crossing area, and a same-side push operated pedestrian/cyclist control box which lights up showing a 'red man/red cycle' or 'green man/green cycle' to indicate whether or not it is safe for the pedestrians and cyclists to cross. Toucan crossings may or may not emit sound to indicate when it is safe to cross, and may or may not feature a rotating cone at the base of the control box to indicate to visually impaired pedestrians when it is safe to cross.

Transport assessment

A comprehensive and systematic process that sets out various transport issues relating to a proposed development. It identifies what measures will be taken to deal with the anticipated transport impacts of the scheme in relation to all forms of travel.

Transport statement

A simplified Transport Assessment, used in some cases where transport issues arising out of development proposals may not require a full Transport Assessment i.e. smaller scale developments where the traffic impact is limited in both volume and area impact.

Travel plan

Travel Plans are long-term management strategies for integrating proposals for sustainable travel into the planning process.

V

Village gateway

A form of traffic calming involving a physical build out in the carriageway at approaches a settlement, thus forcing drivers entering the settlement to slow down, give way to oncoming traffic before crossing over to the other side of the carriageway to continue into the settlement. Often accompanied by settlement name signs and speed limit signs.

Z

Zebra crossing

A pedestrian crossing distinguished by white strips and orange beacons where users of the carriageway are required to give way to pedestrians to cross.

Email us 

Planning Policy and Delivery Team (Local Plans)
planningpolicy@eastsoffolk.gov.uk

Development Management (Planning Applications)
planning@eastsoffolk.gov.uk

Call us 

Planning Policy and Delivery Team (Local Plans)
01394 444557

Development Management (Planning Applications)
01502 523100

Write to us 

East Suffolk Council
Planning Policy and Delivery Team
Riverside, 4 Canning Road,
Lowestoft, NR33 0EQ

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Appendix 1

Community Recommendations

East Suffolk Cycling and Walking Strategy

October 2022



Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
Aldeburgh	62	Thorpe Rd Aldeburgh, the full length of this road between Aldeburgh and Thorpeness.	Many cyclists use this road as it is difficult to cycle all the way to Thorpeness along the beach/foreshore, both because of the terrain and the number of people using the footpath. This road has a 60mph speed limit and because it is straight many people drive fast. It is therefore a dangerous road for cyclists and families to use. It should also be noted that this road runs along side a nature reserve and the risk to wildlife is significant. Deer are also a danger to drivers.	Get the speed limit reduced to 30mph so that it becomes safer and links the 30mph limits in Aldeburgh and Thorpeness together.							N/A	This comment is in relation to speed and should not be scored, but rather passed on to SCC.
Aldeburgh	172	Aldeburgh...et al	Like many of our towns Aldeburgh high street is often full of cars...especially during holiday seasons...making life difficult for pedestrians, cyclists and mobility scooter users.	Promote the idea of regular car free days across the district...where cars are banned from the centre of towns such as Aldeburgh, Woodbridge, Southwold, Framlingham, Halesworth, Beccles, Bungay etc...Maybe one Sunday per month...in support of World Car free day...it works in London why not in Suffolk							N/A	The comments raised have been considered in the formation of the strategy, the creation of car free days is beyond the scope of the strategy and cannot be scored under the MCAF system. However modal filters and barriers to traffic have been considered.
Aldeburgh	346	Between Aldeburgh and Thorpeness	As in a previous comment, the road is unsuitable for riding a bike comfortably, safely and pleasantly. The path is really a footpath not a cycle path. Shared use paths are against LTN 1/20 so the best thing to do is build a new cycle only path. This will be welcomed by people who walk and cycle there.	So that the new cycle path has greater currency, there is a need to link with cycle routes at either end. If there aren't any, then either build them or designate a new route using existing infrastructure.	1	0	0	3	-1	3	6	Connectivity and Growth – Although the proposal will likely have more leisure benefit than connectivity benefit, it is likely that there may be some commuting for the services provided in Aldeburgh. A score of 1 is considered reasonable. Modal Shift – It is unlikely that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal offers an alternative to the use of Thorpe Road, which is relatively narrow and has a national speed limit, therefore a score of 3 is considered reasonable under safety. Biodiversity – A small negative score is deemed reasonable as the implementation of a cycleway will likely result in the removal of foliage. Leisure – As the existing footway is situated along the coast between Thorpeness and Aldeburgh, the addition of a cycleway adjacent it will have significant leisure benefit.
Aldeburgh	474	The old railway track bed between TM 4601 5745 and TM 4622 5945.	This forms part of much walked circular routes taking in Aldeburgh, Thorpeness, the Aldringham Fen and Aldringham Walks. It also presents for walkers and cyclists a safe alternative to the B1122 which is a fast and extremely dangerous road and the only other direct link between Aldeburgh and Leiston. Much of the track bed appears to be in private ownership but is open, presumably as a permissive path. Permissive paths are unsatisfactory because the permission can be withdrawn at any time.	Creation Agreements or Orders should be funded to secure the route as a permanent public right of way. An ideal solution would be for a bridleway to be created over the track bed as this would provide a multi-user facility for walkers, horse riders and cyclists.	0	0	0	3	-1	3	5	Connectivity and Growth – The proposal does help towards a connection to Thorpeness and Aldringham, however this route will likely have more leisure benefit than connectivity benefit as the route does not directly connect into either settlement but connects to PROWs which, in turn, connect to Thorpeness and Aldringham. It is considered, therefore, that a neutral score is reasonable. Modal Shift – No evidence to suggest that the proposal would lead to a significant modal shift. Optimisation – The proposal is for new infrastructure and it is not considered, therefore, an optimisation. Safety – This route could be used as an alternative to Thorpe Road and the B1122, which have a national speed limit and likely have high volumes of traffic, therefore a score of 3 is considered reasonable. Biodiversity – A small negative point is deemed reasonable as the proposal will likely result in the removal of foliage in order to have access for both pedestrians and cyclists. Leisure – Not only would the proposal help create a connection to Aldeburgh, which is a coastal town, from Thorpeness and Aldringham, but it would also connect multiple attractive PROWs. Therefore, a score of 3 under this category.
Aldeburgh	476	Verge of the A1094 near Aldeburgh Golf Course forming part of “the Sailors’ Path”	Until recently there was no safe link at the Aldeburgh end between the small car park at TM443581 and the footway at TM448577. Walkers were expected to walk in the carriageway of a fast and dangerous road after it leaves the 30mph limit. Verges are narrow, sloping and uneven with drainage channels - totally inadequate. SCC has secured a licensed path but this is understood to be a ten-year agreement only.	A permanent right of way is required over this licensed path. The verges on the southern side of the road fronting the gardens between the Golf Club and the small car park also need dedicating.	0	0	0	3	0	2	5	Connectivity and Growth – The proposal will likely have more leisure benefit than that of connectivity, hence a score of 0 under this category. Modal Shift – There is no evidence to suggest that the proposal will result in a significant modal shift. Optimisation – The proposal would create new infrastructure and is not considered, therefore, an optimisation. Safety – Currently, it is necessary for pedestrians to utilise Saxmundham Road, which is a busy ‘A’ type road with a national speed limit, therefore implementing a permanent right of way connecting the two PROWs has safety benefits. Biodiversity – No biodiversity impacts. Leisure – The proposal would connect Sailors Path, which is a particularly attractive PROW route, to the PROW network residing within Aldeburgh. It is considered, therefore, that a score of 2 is deemed reasonable.
Aldeburgh	508	Pier Avenue and Station Road Junction -- this roundabout has heavy traffic in all directions and there is no dedicated crossing area which is safe for pedestrians	A safe crossing point. This will become even more important as the west side of Station Road and Mights Road are developed with new housing, community facilities, and employment space.		1	0	0	1	0	0	2	Connectivity and Growth – The road appears to be a modest barrier between those situated on either side, but as a 30mph road it is crossable. As a food shop is located nearby, a score of 1 is considered reasonable. Modal Shift – There is insufficient evidence to suggest that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The road is a 30mph road, but relatively busy and as a food shop and restaurant is located nearby, a score of 1 is deemed reasonable. Biodiversity – No biodiversity impacts. Leisure – The suggestion provides limited leisure benefit.
Alderton	308	Alderton Road/Hollesley Road between the two villages (60mph section).	This is, not unreasonably, a 60mph stretch of road, so has fast cars upon it. It is, however, too narrow in all places to allow vehicles to pass at speed, let alone for cyclists to be/feel safe.	There appears to be significant potential on farmland on the east side to both expand the road and to add a cycle/footpath adjacent to the road.	2	0	0	3	-1	3	7	Connectivity and Growth: These are two small villages, and there is likely to be limited demand for walking and cycling between them as they both have basic services and no particular draw between them - it is therefore likely to be most notably scoreable under the leisure category. However, due to the absolute lack of connection between them in terms of active travel infrastructure, a higher score of 2 is given. Modal Shift: Principally a leisure route and does not have significant modal shift potential. Optimisation: New infrastructure so not scored under this category. Safety: Full segregation earns a full score. Biodiversity: Likely to be loss of green

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
												space, and potentially biodiversity valuable plants. A score of -1 is given. Leisure: Full score given.
Alderton	503	B1084 between Bawdsey and Alderton	We need a continuous footpath from Bawdsey through to Alderton for walkers to feel safe. Currently there are 4 isolated sections of path that don't join up. Its already a 30mph road, and we dont expect nor do we want street lights, but we do need somewhere safe to step back clear from 2 way passing traffic on narrow roads.	A new 120m section of footpath (with elevated or rumble strip kerbing edging) should be created on the west side of the road to join up between the exit from the new Orwell Housing Development, and Pitcairn Cottage where the next section begins. If the road needs to be widened to accomodate it, then extend it into the verge/bank /hedge on the east side so that pedestrians dont have to keep crossing from one side to the other.	1	1	1	2	-2	1	4	Biodiversity: This is a sensitive area, and creation of the path would require some removal of mature hedgerow which would take a while to re-establish from replacement plantings.
Alderton	797	Wilford Peninsula	I have completed the map based consultation with several suggestions on the Felixstowe peninsular, but I also wonder about a possibility on a rather grander scale... It all rather depends on how ambitious you want this cycling and walking strategy to be!!	What are the barriers to creating a ground-breaking "Cycle Country" on the Wilford Peninsular? Starting at Wilford Bridge, bounded by the River Deben to the south/south-east; the coast line from Bawdsey to Aldeburgh; the A1094 to Snape Crossroads; and the B1069/A1152 back to Wilford Bridge. I appreciate this is a massive area, but it would create an equally massive leisure and tourist attraction in what is arguably some of the best Suffolk countryside. Other than agriculture and the Bentwaters Airfield business park (accessed via the A1152), there is precious little industry requiring fast-moving access within this area. It could perhaps comprise of a 30mph designation for classified roads within the area (eg on the B1063, B1078 & B1084) and quiet lane status with a 20mph recommendation elsewhere.	1	0	1	0	-2	3	3	Connectivity and Growth: Depending on the route taken there maybe some connectivity and growth value if it connects settlements as a co-benefit of creating a leisure route - however - routes are likely to be indirect, and are unlikely to be suitable for commuting. Modal Shift: As above. Optimisation: Likely to be new routes, with some PROW routes upgraded and surfaced. Score of 1 is given for the latter. Safety: These routes would be fully segregated, however theres no uplift in safety as these routes don't currently exist or are already segregated PROW routes. Biodiversity: In the AONB/European Sites/Ramsar areas that come into contact with this route, there may be some disturbance from cyclists and pedestrians - particularly the latter walking dogs that may disturb ground nesting birds and their habitat. Leisure: Full score for leisure.
Aldringham Cum Thorpe	51	the entire A1094 from Friday street to Aldeburgh but especially the stretch between Frisyon and Alfeburgh.	fast road with cars doing 60mph, having to brake heavily when coming upon bikes. road is often busy both ways and insulates meaning it becomes difficult to pass the cyclists safely.with the increase in hgvs traffic expected for the wind farm installation something needs to be done to protect the cyclists	I have no solution but as a motorist I'm petrified of slow moving cyclists going up.hill and meeting them before I've been able to brake sufficiently.	2	1	0	3	-3	3	6	Connectivity and Growth – the proposal would connect Snape to the market town Aldeburgh, which provides some key services. The route will, however, likely have more leisure value, therefore a score of 2 is considered reasonable. Modal Shift – According to PCT, the road is currently poorly used, however if segregated off-road infrastructure is deliverable PCT suggests there will be a small uplift, thus a score of 1 is considered reasonable. Optimisation – the proposed improvements are new and do not optimise the existing, hence a score of 0 under optimisation. Safety – the majority of the A1094 has a NSL, is unlit, and is an 'A' type road, which means volume and speed of traffic is likely high. With consideration to the road conditions, taking cyclists/pedestrians off this road is beneficial and receives the highest score under safety. Biodiversity – the proposal will result in potential significant loss of wild growth and hedges which have high biodiversity value meaning a significant minus score. Leisure – the proposal will have a significant Leisure benefit as not only will it provide cohesion of a number of PROWs but will also connect to Aldeburgh beach and the River Alde, which are leisure attractions.
Aldringham Cum Thorpe	243	This whole redundant railway line should be surfaced and rebuilt as a cycleway between Leiston and Aldeburgh	Could be a dedicated cycleway with funding from the windfarms perhaps? You know - like a proper dedicated route like they have in other parts of the country.		0	0	0	3	-1	3	5	Connectivity and Growth – The proposal does help towards a connection to Thorpeness and Aldringham, however this route will likely have more leisure benefit than connectivity benefit as the route does not directly connect into either settlement but connects to PROWs which, in turn, connect to Thorpeness and Aldringham. It is considered, therefore, that a neutral score is reasonable. Modal Shift – No evidence to suggest that the proposal would lead to a significant modal shift. Optimisation – The proposal is for new infrastructure and it is not considered, therefore, an optimisation. Safety – This route could be used as an alternative to Thorpe Road and the B1122, which have NSL and likely have high volumes of traffic, therefore a score of 3 is considered reasonable. Biodiversity – A small negative point is deemed reasonable as the proposal will likely result in the removal of foliage in order to have access for both pedestrians and cyclists. Leisure – Not only would the proposal help create a connection to Aldeburgh, which is a coastal town, from Thorpeness and Aldringham, but it would also connect multiple attractive PROWs. Therefore, a score of 3 under this category is deemed reasonable.
Aldringham Cum Thorpe	485	Sizewell Cliffs- Cliff-top path Thorpeness to Sizewell (Aldringham FP 31) - serious incidents of erosion along this path which have caused the Suffolk Coast Path recreational route to be re-routed. The path affords outstanding beautiful views	Strengthening work needs urgently to be carried out just to the south of the junction with footpath 32 (TM475616) where the path edge is falling away. Footpath 31 seems now to have been lost between points TM474599 (Old Homes Road) and approximately TM476604. The footpath below the cliffs (footpath 33) is also impassable at high tide in the vicinity of TM475601 where gabions have been installed.	This part of the problem is eased by the fact that people have for many years been able to walk freely over the grassland between Thorpeness Common and the cliffs and along the existing tracks to reach Byway 20 or North End Avenue, Thorpeness. However, this area is not recorded as Access Land nor are there any public rights of way over it recorded on the Definitive Map. Creation of permanent rights of way over these tracks should be funded to enable signage to be installed and them to become part of the Suffolk Coast Path recreational route.	0	0	0	0	0	2	2	Connectivity and Growth – The proposal does not create any new connections and will likely have more leisure benefit than connectivity. Modal Shift – There is insufficient evidence that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – No significant safety benefit. Biodiversity – As it is an existing track, it is unlikely going to have a significant biodiversity impact. Leisure – The proposal will create another attractive route along the coast and connect two PROWs, therefore a score of 2 is considered reasonable.
Aldringham Cum Thorpe	551	Old rail line running between aldeburgh and	Restore old rail line route from Aldeburgh to leiston (crown farm junction) a hard surfaced cycle route for	Suffolk's own cinder track for cyclists. Smooth hard surface available to all and not just hardcore 'off roaders'	0	0	0	3	-1	3	5	Connectivity and Growth – The proposal does help towards a connection to Thorpeness and Aldringham, however this route will likely have more leisure benefit than connectivity benefit as the route does not directly connect into either

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
		crown farm, lovers Lane, leiston, sizewell	tourists. This could then be extended through to Southwold									settlement but connects to PROWs which, in turn, connect to Thorpeness and Aldringham. It is considered, therefore, that a neutral score is reasonable. Modal Shift – No evidence to suggest that the proposal would lead to a significant modal shift. Optimisation – The proposal is for new infrastructure and it is not considered, therefore, an optimisation. Safety – This route could be used as an alternative to Thorpe Road and the B1122, which have NSL and likely have high volumes of traffic, therefore a score of 3 is considered reasonable. Biodiversity – A small negative point is deemed reasonable as the proposal will likely result in the removal of foliage in order to have access for both pedestrians and cyclists. Leisure – Not only would the proposal help create a connection to Aldeburgh, which is a coastal town, from Thorpeness and Aldringham, but it would also connect multiple attractive PROWs. Therefore, a score of 3 under this category.
Aldringham Cum Thorpe	654	B1353 running from Aldringham to Thorpeness	This road is heavily used by families to cycle to and from Thorpeness. The speed of traffic combined with the ever reducing width of the road makes this activity very dangerous.	A new cycle path/footpath linking these two villages would reduce the ever increasing risk to cyclists and pedestrians.	2	0	0	3	-3	3	5	Connectivity and Growth – Thorpeness and Aldringham are both small settlements with limited services, therefore connecting them would likely have moderate connectivity benefits as it will allow an element of service pooling. However, it is likely that the proposal will have more leisure benefit than connectivity benefit. A score of 2 is considered reasonable. Modal Shift – No evidence that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The B1353 has a NSL and, as a 'B' type road, speed and volume of traffic is likely high, therefore removing cyclists and pedestrians off the road has safety benefits. A score of 3 is considered reasonable. Biodiversity – The proposal will likely have a resultant loss of established hedgerows and trees adjoining the B1353, therefore a significant negative score is deemed reasonable. Leisure – The proposal has clear leisure benefits as the proposal connects to Thorpeness which, as a beach, is likely a significant leisure attraction.
Ashby, Herringfleet And Somerleyton	606	Between Somerleyton and Blundeston	I live in North Oulton Broad, and would love to go on country bike rides with my young children, but cannot risk them biking on the country roads. This said, although the villages of Blundeston, Somerleyton etc are very close, it is near impossible for us to bike there.	To expect a change in road infrastructure is impractical, therefore I can only suggest that a review of public footpaths in this area (as well as other similar areas) are made in view of bolstering these to provide the potential to cycle along them. This may require some compulsory purchase to widen footpaths, and a form of deterrent for motorcycles, but I believe it would be an excellent means of safely connecting the local villages and allowing families a better means of exploring these areas (which in itself can help with increasing trade/footfall in local businesses)	3	0	0	3	-2	2	6	Connectivity and Growth - Providing good cycle connections into Oulton (and then Lowestoft) provides Somerleyton and Blundeston access into the main town centre. Modal Shift - The roads through to Somerleyton show little potential modal shift growth which then suggests the improvements have limited potential for modal growth overall. Optimisation - This creates a new route albeit using existing footpaths. Safety - Cyclists currently use the B1074 or country roads. Although relatively quiet these can be winding. Biodiversity - Using the existing paths would limit biodiversity impact, however widening the footpaths would result in some biodiversity loss, particularly at Fp20. Leisure - Creating an attractive cycle route that utilises the countryside and where possible its proximity to the river creates a good leisure destination in its own right, but also links to the attractive village of Somerleyton.
Ashby, Herringfleet And Somerleyton	667	Between Haddiscoe and Reedham via Somerleyton	The marshes between Haddiscoe and Reedham via Somerleyton involves cycling along 'car fast' narrow lanes which have no provision whatsoever for cyclists, and any attempt to avoid fast roads involves miles of detours with in real terms no real gain in safety.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.	1	0	0	3	-2	3	5	Connectivity and Growth - Whilst the proposal connects 2 villages and then potentially a third in Reedham both Somerleyton and St Olaves have limited facilities. The access to the school in Somerleyton is a benefit. Modal Shift - PCT shows limited potential for modal shift growth along the B1074 Optimisation - This represents a new cycle route. Safety - The B1074 is a busy and well used road, bypassing this road scores highly. Biodiversity - The exact biodiversity impact is unknown and could be high or lower depending on the route. Given the proximity to the broads and other important habitats a score of -2 is considered reasonable, but this could rise to a -3. Leisure - A route alongside the river and an attractive location linking attractive villages is considered a high scoring proposal.
Barnby	65	New Road	A general issue that reports of road problems which affect cyclists are not taken seriously by the highways department. At this location there is a big dip in the road where the telegraph line crosses the road. It is a downhill stretch and if you do not know about it then it could lead to a cyclist being dismounted or coming off the road (this has happened).	The highways department to take cycling issues seriously and fix accordingly.							N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Barnby	99	New road junction.	A very ill thought out cycle path. If coming towards the a146 down new road you have to cross over the road to get onto the cycle path. You have to look out for drivers turning right onto new road, and left onto new road. Visibility is poor to see if a driver is turning left off a146.	Extend cycle path up new road so you can get on it before the junction or a new path and crossing on the left of the road.	3	0	1	2	-1	1	6	Connectivity and Growth - This is currently the main route between Lowestoft and Barnby for cyclists and walkers which avoids and Barnby Bends. This junction represents a key issue for the wider route. Modal Shift - PCT suggests the wider route has modest potential growth for commuter use and the improvement represents a small section of this. Optimisation - The improvement represents a small section of the wider route from Lowestoft to Barnby, but such is the potential impact of this junction a point is deemed worthy. Safety - As a road of speeds of 50mph improvements to this crossing could achieve a 3 if to a high standard. However it is unlikely a top quality crossing such as a lighted crossing or bridge is possible here so a score of 2 has been provided. Biodiversity - A small amount of unmanaged and managed verged may be required giving a small minus score. Leisure - This route may have a bigger leisure draw than commuters so a score of 1 is deemed appropriate here.
Barnby	197	Barnby bends	The road is far too narrow and winding and it needs a cycle path/lane that follows the same route but takes cycles off the main road as it is dangerous and causes huge tailbacks. The only cycle route takes cyclists so far off this route that they just don't use it! I would not dare cycle to work because it is just dangerous and any other route is far too far round (via Mutford)	Totally bypass the Barnby bends and include a cycle path - this has been needed for decades! At least widen the road to include a proper cycle path on each side of the road	3	2	0	3	-1	1	8	Connectivity and Growth - This route lies on a key corridor and directly connects Barnby/North Cove to the main town of Lowestoft. Modal Shift - PCT suggests a high modal shift that just falls under the threshold for a top score. Optimisation - Whole new infrastructure so no optimisation benefit. Safety - A busy, winding and undulating road with speed limits between 40 and 50mph so getting cyclists off-road would score highly. Biodiversity - The area to the south is largely managed grass although there are

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												sections that are not managed which may have a greater value. Leisure - This route is largely beneficial to service users and commuters as opposed to leisure cyclists, whilst recognising there is some benefit connecting to Lowestoft and Beccles so a modest score is given.
Barnby	382	There need to be a safe cycle track from Carlton Colville to Beccles on the A146	Several people cycle the A146 and it is very dangerous especially by the Barnby Bends. The back rounds are hazardous in the dark morning and evening so there is no safe route. If there was a cycle track I'm sure more people would cycle rather than use cars.	Decent cycle track to link towns and villages	3	2	0	3	-1	1	8	Connectivity and Growth - The connection between the main town of Lowestoft and Barnby/North Cove is a key corridor that connects villages to a key service centre. Modal Shift - PCT suggests a high modal shift that just falls under the threshold for a top score. Optimisation - Whole new infrastructure so no optimisation benefit. Safety - A busy, winding and undulating road with speed limits between 40 and 50mph so getting cyclists off-road would score highly. Biodiversity - The area to the south of the A146 is largely managed grass although there are sections that are not managed which may have a greater value. Leisure - This route is largely beneficial to service users and commuters as opposed to leisure cyclists, whilst recognising there is some benefit connecting to Lowestoft and Beccles a modest score is given.
Barnby	610	Barnby Bends	Large dip on westbound although road surface not broken. Possibility of dismounting cyclist since it is downhill and cyclists could be travelling at reasonable speed. Almost dismounted cyclist in front of me yesterday - I am aware of dip so can avoid								N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Beccles	104	Heavy traffic down Northgate	In order to access the proposed cycle path along the disused railway line from the opposite bank (as identified in this strategy and on the interactive map) all walkers and cyclists would need to use Gillingham Dam and Northgate, where their safety is an issue due to lack of pavements and the narrowness of the roads	Link with the highways strategy. Consider linking bus and rail services and redirect the heavy traffic away from this area to make it safer and more accessible for Walkers and Cyclists	2	1	0	2	0	1	6	Connectivity and Growth – Gillingham Dam connects Beccles and Gillingham it is National Speed Limit but is likely to be relatively quiet given the A146 runs parallel. Beccles contains a number of important services, but a modal filter to direct traffic away from this route will not remove traffic entirely unless the road is closed so a score of 2 is considered reasonable. Modal Shift – Assuming any improvement also redirects cyclists from using the A146 the improvement could score a 2 at the highest standard. However, the route is unlikely to be fully traffic free so the modal shift to the lower standard doesn't represent a significant gain. A score of 1 is considered reasonable. Optimisation – This doesn't optimise existing cycle infrastructure nor provide improvements to the pavement. Safety – The road is NSL, but the suggestion is not to remove traffic all together. Accordingly, a full score has not been provided, but a score of 2 is deemed reasonable. Biodiversity – There are no Biodiversity Impacts Leisure – The improvements will help connect PROW routes within the Norfolk County area alongside the river. However, records suggest it is a footpath as opposed to a bridleway meaning cycle improvements won't provide significant connections to these. Beccles is an attractive visitor location in itself so a score of 1 is deemed acceptable.
Beccles	106	Between Suffolk town centre of Beccles and Suffolk town centre of Bungay (in partnership with Norfolk).	Having no direct route between the Suffolk towns and having the old railway route unused.	Between Suffolk town of Beccles and Suffolk town of Bungay (in partnership with Norfolk). Reconnect the town's by making use of the old railway route as a new cycle path. This would be away from roads, existing infrastructure (bridges, embankments and cuttings), minimal / no gradients, countryside views, direct route between town centres and for the majority of their route likely to be unused and already furnished with trees, hedges and the odd bit of history along the way.							N/A	The suggestion has not been scored as creating connections between Beccles and Bungay is part of a Key Corridor and an important ambition of the strategy. The use of the old railway line has been considered and discussed with NCC.
Beccles	488	This used to be a road. It is now a very important green corridor. Whole length of Rigbourne Hill Lane	The surface needs updating. The hedges need cutting back. The bank needs taking back. Important cycling/walking link from the new garden community.	This will be a main route from new Garden Community into town. We need to encourage walking and cycling and this is an existing safe route that needs upgrading, rather than a new route putting in.	2	1	1	1	0	1	6	Connectivity and Growth - Whilst some connectivity does already exist through on-road cycling using the National Cycle Route. It does provide a direct route to a large allocation which has significant benefit, but this is tempered by the number of potential routes the allocation will provide so a score of 2 is deemed reasonable here. Modal Shift - PCT suggests that the roads around this route are well used, particularly Banham Road and Darby Road. Creating a off-road cycle route is of a high standard. It is not expected that this improvement will take them all off the roads as it will be dependant on the cyclists direction of travel and destination. However, some would likely be taken off road so a score of 1 is deemed reasonable. Optimisation - Re-surfacing and moving back the banks will provide optimisation benefits to an existing path. Safety - Taking cyclists off the road will have some safety benefit. The roads are 30mph and residential in nature so the safety benefit will be modest. Biodiversity - Re-surfacing the existing path with some modest widening would be unlikely to have a significant biodiversity impact, however if the path requires significant widening this score could change. Leisure - Providing an attractive, green off-road route could have modest leisure benefit. Whilst it improves links to Beccles centre, which also has leisure benefit, the connection would not be wholly complete limiting its score.
Beccles	664	London Road, from Wash Lane to the new bypass	With respect to the proposed routes, it was considered that urgent consideration be given to new cycles path from Wash Lane to the new bypass.		1	1	0	3	-1	0	4	Connectivity and Growth - The proposed route would provide connection to the new infrastructure along the southern bypass and access to employment areas, however some connectivity does already exist and in addition the proposed path is one a several connections proposed through the garden neighbourhood allocation. Overall a score of 1 is deemed appropriate. Modal Shift - The potential for modal shift growth on Wash Lane is good whilst the potential for modal shift growth on Cucumber Lane is modest. The proposed route would be expected to take some, but not all of this potential due to its position between the two. Accordingly a score of 1 is deemed reasonable. Optimisation - This represents a new cycle route. Safety - Wash Lane is a relatively busy and fast flowing road with HGV use so removing cyclists off this road scores highly. Biodiversity - Without a defined route this category is difficult to assess. It could utilise the existing footpath, but would likely result in the removal of some foliage whilst passing over what is currently a field would have a lower impact. A score of -1 is deemed appropriate at this stage. Leisure - The connections into the

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												employment areas suggests this is more of a day-to-day route meaning limited scoring on Leisure.
Beccles	677	River Waveney, Beccles	Lack of cycle / walking access from Beccles towards Burgh St Peter, Aldby peninsula	acquire and restore the former railway bridge over the Waveney, that used to carry trains towards Haddiscoe. Work jointly with South Norfolk Council to create a walk/cycle way, and also protect the corridor for possible future rail service	3	1	0	3	-3	3	7	Connectivity and Growth - The proposed route will allow Aldeby which has limited services to connect into Beccles. Modal Shift - PCT suggests limited potential for modal shift gain judging by surrounding roads. Datashine suggests limited walking for commuting purposes in Aldeby so a score of 1 has been given for a potential modest gain here. Optimisation - This is a new route and does not represent an optimisation. Safety - The current route for cyclists to get between Beccles and Aldeby is to use the A143 and A146 which are relatively busy and fast flowing roads so getting cyclists off these roads creates a high score. Biodiversity - This road would likely require significant foliage removal some of which directly adjacent the river itself. Whilst the full biodiversity impact is unknown at this stage it is considered likely to be high. Leisure - Creating an attractive route that encompasses the Broads and provides connections into the Beccles Heritage offer scores highly.
Benacre	112a	Kessingland to Southwold	To make this journey by bike you have to go inland through Henstead to avoid the A12. A long way out of your way.	Provide a cycle route between Kessingland beach to Benacre village or a cycle route beside the A12 between Kessingland Wildlife Park roundabout to the Benacre turn on the A12.	2	1	0	3	-2	1	5	Connectivity and Growth - The suggested improvement connects 2 settlements to together, one of which (Benacre) has limited services. However a score of 3 was not considered suitable due to the very low population numbers so limited growth potential is available. Modal Shift - PCT suggests a small number of cyclists may use the A12 currently and has the potential for a reasonable level of growth. Optimisation - A new path so no optimisation. Safety - This proposal takes cyclists off the A12 which is a main road at national speed limit. There are limited alternatives currently between Benacre and Kessingland. Biodiversity - The A12 contains grassed verged to the side of the road which would need to be utilised. These do not appear regularly cut. Further vegetation may require removal to get the appropriate width so this score may grow to -3 if more established foliage requires removing. Leisure - This route connects to important tourist locations. Alongside the A12 would not form an attractive route so a score of 1 is deemed sufficient, however a more attractive path would potentially score a 3.
Benacre	112b	Kessingland to Southwold	To make this journey by bike you have to go inland through Henstead to avoid the A12. A long way out of your way.	This is an alternative suggestion made by an officer of East Suffolk Council in exploring whether there is potential along a more coastal path.	2	1	0	3	-1	3	8	Connectivity and Growth - The suggested improvement connects 2 settlements to together, one of which (Benacre) has limited services. However a score of 3 was not considered suitable due to the very low population numbers so limited growth potential is available. Modal Shift - PCT suggests a small number of cyclists use the A12 currently and has the potential for a reasonable level of growth, some of this would be transferred to a coastal path. Optimisation - A new path so no optimisation. Safety - This proposal takes cyclists off the A12 which is a main road at national speed limit. There are limited alternatives currently between Benacre and Kessingland. Biodiversity - There appears to be a path already along this route formed of desire lines. Any attempt to surface and formalise this path would result in the loss of some wild grass. Leisure - This route connects to important tourist locations and would form a highly attractive destination in its own right.
Benhall	193	A12 to the west of Saxmundham	safe crossing for cyclists and walkers	The local plan proposes a new housing development of 800 homes on the easdtern side of the A12 just south of Saxmundham. it also proposes development of an employment area just north of the A 12. There must be a safe crossing for cyclists and walkers between the new housing development and the employment area. preferably in the form of either a footbridge or underpass.	3	0	0	3	0	1	7	Connectivity and Growth – the A12 will be a significant barrier between the mixed-use allocation SCLP12.29 to the east of the A12 and the employment allocation SCLP12.29 to the west of the A12, therefore the provision of a footbridge for use by both cyclists and walkers receives a high score. Modal Shift – currently low numbers along the A12 on PCT, therefore there is insufficient evidence that the proposal would lead to a modal shift. Optimisation – Providing new infrastructure does not represent an optimisation. Safety – This section of the A12 is wide, straight, and has an NSL; therefore, the suggestion has a significant safety benefit as it will be removing cyclists and walkers off the road. Biodiversity – there are no significant biodiversity impacts. Leisure – The suggestion has a small leisure benefit as there are a couple PROWs on both sides of the road, therefore a bridge or underpass would connect them. This proposal will likely have more connectivity value than leisure value.
Benhall	324	A safe cycle crossing to the path on west side of A12 at Aldburgh/Friday St junction would enable cyclists to access roads on this side from the Snape Rd.	The path needs to be kept clear of vegetation and allocated as a shared use path. It is currently overgrown and not fit for purpose. Cyclists frequently cross here to cycle either north or south to access the roads to Ben hall and other villages west of the A12.	As above	1	0	0	3	-2	0	2	The commenter proposes a crossing to the west of the A12/A1094 junction in order to access the footway north of the A12, which should be widened to become a cycleway. Connectivity and Growth – Cycleway would connect into Benhall which is a small, isolated village. The A12 is a significant barrier between those situated on either side, therefore the proposal has modest connectivity benefits. Modal Shift – According to PCT, it is unlikely that the proposal – even if delivered to the highest standard – will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – This section of the A12 is a busy dual carriageway with a national speed limit to the north and a 50mph speed limit to the south. With consideration to this, the A12 represents a significant barrier to those situated on either side. Providing a safe crossing and widening the existing footway to include a cycleway will have safety benefits. Biodiversity – A negative score of -2 is given under this category due to the likelihood of the removal of the managed green verges and foliage adjoining the path. Leisure – No significant leisure benefit.
Benhall	412	A12 Saxmundham bypass.	It is extremely unsafe at present for cyclists and pedestrians to cross the A12 bypass e.g. to roads, bridle paths or footpaths on the west side. Safe crossings are essential. This is all the more so given the Local Plan	We need underpasses, effective pedestrian crossings, or even step-free bridges at all relevant crossings. The attached photo showing a footpath crossing was taken in full lockdown when, almost uniquely, there was zero	3	0	0	2	0	1	6	Connectivity and Growth – The A12 will be a significant barrier between the mixed-use allocation SCLP12.29 to the east of the A12 and the employment allocation SCLP12.29 to the west of the A12, therefore the provision of a footbridge for use by both cyclists and walkers receives a high score.

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			designation of the South Saxmundham Garden Neighbourhood which will, amongst other matters, mean that local residents will need to cross the A12 to access rural areas, as the existing much-used rural paths to the east of the bypass will become semi-urbanised.	traffic - usually going 60 mph. Impossible for people who cannot move fast to cross without extreme danger. These crossings become even more essential if Garden Neighbourhood proceeds.								Modal Shift – There is insufficient evidence that the proposal would lead to a modal shift. Optimisation – Providing new infrastructure does not represent an optimisation. Safety – This section of the A12 is wide, straight, and has a national speed limit; therefore, the suggestion will likely have a modest safety benefit. However, a crossing point does not completely address the concern raised, therefore a score of 2 is considered reasonable. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion has a small leisure benefit as there is a network of PROWs on either side of the road, which is currently a barrier, and a crossing point would provide cohesion of these footpaths and bridleways. However, the crossing point may not provide direct cohesion between PROWs, thus a score of 1 is considered reasonable.
Blundeston	222	Lowestoft road coming into Blundeston Village	The walking/cycling links into and out of the village are awful, especially for kids who frequently use this road to access the skate park in the summer and vice versa with those venturing out. A pathway along the entire road would vastly improve access out of the village for those of all ages. There is a large development of houses about to be built near that road, meaning this worse is even more essential.	Investigate the safety of pedestrians in Blundeston entering and existing the village, especially children. Think about how it could improve social isolation. Also factor in this matter when giving permission to large housing developments.	3	2	0	2	-1	0	6	Connectivity and Growth - This route connects a larger settlement to a main town where limited connections currently exist. Modal Shift - Datashine suggests low commuter walking currently, however as a large settlement close to Lowestoft this could be improved. A score of 2 is deemed reasonable for modal shift for every day users. Optimisation - A new route so no optimisation benefit. Safety - The road is 30mph, but it is winding and as a main access into Blundeston likely to be busy so a score of 2 is deemed reasonable. Biodiversity - The route is large, bordered to the south by managed grass, but there are sections where it appears to be unmanaged. Leisure - This route appears more beneficial for everyday use by residents of Blundeston.
Brampton With Stoven	76	Footpath marking around Stoven Wood , Brampton, also North Green and also footpath from Stoven to North Green	Several years ago I walked these paths with an 'official footpath lady' I think from Ipswich. She undertook to get new wayposts installed and direction markers replaced. This never happened.	Replace defective waymarks, put official direction posts at North Green and mark the path from Stoven to North Green. The marker on the map is indicative only as there are several issues.	0	0	1	0	0	0	1	Connectivity and Growth: No significant connectivity and growth benefit Modal Shift: No significant modal shift Optimisation: Wayfinding signs will improve quality of existing route Safety: No significant benefit Biodiversity: No effect Leisure: No significant effect
Bredfield	201	Junction of A12 and New Road between Melton and Bredfield	At busy times it is very difficult and hazardous for cyclists to cross the A12 when travelling between Melton and Bredfield. The A12 carriageway is very wide at this junction	Provide central reservation for cyclists and pedestrians. This could also make the junction safer for motorists.	1	0	0	2	0	0	3	Connectivity and Growth – the suggestion provides limited connectivity opportunities to services or employment, however the A12 is likely a significant barrier when travelling between Melton and Bredfield, thus the suggestion has modest connectivity benefits. Modal Shift – The numbers using this route is unlikely to lead to a modal shift. Optimisation – This does not improve existing infrastructure. Safety – the A12 is a busy straight road with an NSL. The proposal will have modest safety benefit, however a central reservation is unlikely going to completely address the issue raised. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion provides limited leisure benefits.
Bredfield	215	Junction of New Road (Saddlemakers Lane) with the A12 North of Melton Roundabout	Crossing the A12 by Bike at this junction to access the road to Bredfield & Boulge is perilous, especially at weekends when the A12 is busy with 'Holiday' traffic. This junction is on a convenient quiet route for cyclists from Melton (& Woodbridge) to Bredfield, Debach, Charsfield & beyond)	Some sort of formal cycle crossing maybe just south of the junction to allow cyclists to cross the A12 to the footpath on the west side of the A12. Upgrade this footpath to a combined cycle/footpath to remove the need for cyclist to use the 'slip lane' off the A12 to access the road to Bredfield.	1	0	0	2	0	0	3	Connectivity and Growth – the suggestion provides limited connectivity opportunities to services or employment, however the A12 is likely a significant barrier when travelling between Melton and Bredfield, thus the suggestion has modest connectivity benefits. Modal Shift – The numbers using this route is unlikely to lead to a modal shift. Optimisation – This does not improve existing infrastructure. Safety – the A12 is a busy straight road with an NSL. The proposal will have modest benefit but may not completely address this. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion provides limited leisure benefits.
Bredfield	216	Bridleway 'crosses' A12	There is a bridleway at this point that 'crosses' the A12, there is no provision for Walkers, Cyclists, Horse Riders to cross the A12 safely and continue along its route toward/from Bredfield. There is no path on the east side of the A12 to allow users to travel either north or south. The only option is to cross the A12 to the path on the other side of the road.	Some sort of improved road markings/crossing point/signage and widening of paths	0	0	0	2	0	2	4	Connectivity and Growth – any crossing point would provide cohesion to PROW routes but offers limited connectivity opportunities to residential areas, services, or employment. Modal Shift – there is insufficient evidence to suggest any significant modal shift. Optimisation – the crossing point does not appear to improve existing infrastructure. Safety – This stretch of the A12 has a NSL, straight, and is considerably busy but a crossing point will not completely address this. Therefore, a score of 2 under safety is considered reasonable. Biodiversity – There are no significant biodiversity impacts. Leisure – The PROWs on either side of the A12, which is a significant barrier, are likely used for leisure purposes and Strava suggests that PROW 33 has reasonable use. There are limited crossing points along this stretch of the A12 and the proposed crossing point will likely also benefit a handful of PROWs east of the ones in discussion.
Bredfield	275	Pavement through Bredfield	Much of the "pavement" is now too broken or overgrown for safe walking, particularly for anyone with a buggy, a wheeled walker. or a wheelchair People are forced to walk in the road.	The "pavement" needs to be resurfaced and parts of it need to be remade.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Bredfield	375	the thoroughfare woodbridge.	walking/shopping on this street at times when motorised vehicles have unrestricted access can be a very unpleasant experience, it becomes a noisy, dangerous and polluted area, and pavement parking further limits the safe public space, forcing vulnerable pedestrians/ shoppers onto the space remaining to compete with powerful industrial machines. this is in complete contrast to the safer, relaxed, more sociable atmosphere that prevails when motorised vehicle movement is restricted.	consider making this street safe for shoppers/ walkers / cyclists / vulnerable people like children, elderly and disabled at all times, not just for a few hours each day. if you need to know how its done look at other towns and cities, much bigger and more complex than Woodbridge, that confronted and resolved this conflict years ago. this has to be considered low hanging fruit for any council developing a cycling and walking strategy.	0	0	1	1	0	0	2	Connectivity and Growth – The Woodbridge Thoroughfare is a pedestrian zone and restricts vehicular access between 10am-4pm on Mon-Sat, therefore the connection already exists so the suggestion does not score in this category. Modal Shift – The road is relatively quiet on PCT, but busy on Strava Metro. Even if improvements are provided, it is unlikely to provide significant modal shift, hence a score of 0. Optimisation – The proposal does provide moderate improvements to a cyclist/pedestrian priority route as it will restrict vehicular traffic, therefore a score of 1 is considered reasonable. Safety – The Thoroughfare is a narrow road with a 30mph speed limit, and the proposal would restrict further vehicular access providing safety benefits for both cyclists and pedestrians, therefore a moderate score of 1 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – The Thoroughfare is a key strategic location and includes an array of shopping, eating,

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												and drinking establishments, however as an existing pedestrian zone the proposed improvement will not have a significant impact.
Bredfield	501	A12 between Ufford Road junction to Bredfield and Woods Lane roundabout	There is only a pedestrian path alongside the main road, not authorised for cyclists.	1. Authorise making this a shared user (pedestrians & cyclists) and thus legitimise current practise.2. Widen the path	3	1	0	3	-1	1	7	The commenter proposes a shared path adjoining the A12 between Ufford Road junction and the A12/Woods Lane roundabout, however a segregated cycle track may be viable along this section of the A12. Connectivity and Growth – The proposal would create a cycle route to a small handful of villages, which include Bredfield and Ufford, to Woodbridge/Melton. Although Bredfield has a small food shop within the village, it is likely the villages would rely on Woodbridge and Melton for key services – including the primary schools and the high schools. Therefore, a score of 3 under 'Connectivity and Growth' is considered reasonable. Modal Shift – According to PCT, the A12 is currently moderately used and, if infrastructure can be delivered to the highest standard, the proposal will likely result in a small modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore optimise the existing. Safety – The A12 has a national speed limit and as a straight 'A' type road, volume and speed of traffic is likely high. With consideration to the road conditions, infrastructure that removes cyclists off the road scores significantly under 'Safety'. Biodiversity – The proposal would result in the loss of grassed areas that are likely regularly cut and of limited benefit, but over a significant length therefore the proposal warrants a small negative score under this scoring category. Leisure – The proposal would connect a handful of PROWs warranting a small score; however, this route likely has more commuter benefit rather than leisure benefit.
Bredfield	502	Woodbridge Road, Bredfield, between pump at junction with Scott's Lane, and A12	This stretch of road is busy and highly frequented by HGV traffic. It is made hazardous by the presence of several blind bends. There is no safe and separate path for cyclists & pedestrians.	With landowner permission create a shared user path of about 900m to the A12. Surface a strip of the track eastwards from Pump Corner past Blue Barn Farm (picture 1) and extend it (picture 2) alongside and past Horse Close Wood (aka Jubilee Wood) to meet the path running alongside the A12	2	0	0	3	-1	0	4	The commenter proposes implementing a shared path that runs just south of Blue Barn Farm and Horse Close Wood joining the existing footway adjacent the A12. Currently, there are no footways or cycleways that provide a direct route into Woodbridge. Connectivity and Growth – The proposal would connect into an existing footway which provides a direct route into Woodbridge, a key service centre, however this is limited to pedestrians only. As the proposal only provides a small section of a wider route into Woodbridge for cyclists, a score of 2 is considered reasonable. Modal Shift – The proposal is for a shared path, therefore, PCT suggests that it is unlikely that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Woodbridge Road predominantly has a national speed limit. Removing cyclists off this road has clear safety benefits and it is considered, therefore, that a score of 3 is reasonable. Biodiversity – A small negative point is deemed reasonable as the proposal will likely result in the removal of some foliage. Leisure – Although the proposal connects into the PROW network through PROW25, a connection between Woodbridge and Bredfield would be considered more a commuter route than leisure, any leisure benefits would be relatively modest giving a neutral score.
Bredfield	591	Saddlemakes Lane /A12 junctio a GR 278514	Dangerous to cross A12 from cycle way to Saddle Makers lane	A Toucan Crossing. Also resurface & remove foliage from cycle way	1	0	0	2	0	0	3	Connectivity and Growth – The A12 has a NSL and is a modest barrier for those situated on either side and there does not appear to be a pedestrian crossing along this stretch of the A12, however there is a limited number of destinations on either side of the road. Therefore, a score of 1 is considered reasonable. Modal Shift – there is insufficient evidence that the proposal would lead to a modal shift. Optimisation – the proposal does not improve existing infrastructure. Safety – This stretch of the A12 has a NSL, straight, and is considerably busy but a crossing point will not remove pedestrians/cyclists off the road. Therefore, a score of 2 under safety is considered reasonable. Biodiversity – the proposal will not have a significant biodiversity impact. Leisure – the proposal has limited leisure benefit.
Brightwell	529	A12 crossing out of Brightwell Lakes.	I fully endorse comment 278 relating to connections for new development. The opportunity should be taken to view the whole area from Brightwell Lakes/Martlesham to the hospital/Ipswich as a single cycle friendly zone containing housing, employment, retail, educational facilities etc ideal for developing cycling priority routes	Safe crossing under A12, upgrades to existing bridleway from crossing into Kesgrave, linking with cycleways to hospital and Ipswich and national cycle network	3	3	0	3	-2	2	9	Connectivity and Growth: This option is made difficult by the Ipswich Heaths SSSI. This is specifically due to the use of bridleway 6 - in situ or re-routed to 'snap' to the southern boundary of Martlesham Heath, the latter of which would be the preferred option for ease of onward travel - as both options cross the SSSI. It is for this reason that the Strategy recommends transitions through Martlesham Heath to access Dobbs Lane and Longstrops Bridleway, instead. With that said, this connection would be extremely valuable for future commuters and leisure cyclists residing in the Brightwell Lakes development. From a C&G perspective, this scores a full score of 3. Modal Shift: As above - Brightwell Lakes is set to house over 5,000 people - direct connections for cycling/walking to and from Martlesham, Woodbridge, Ipswich and Felixstowe are critical for ensuring meaningful alternatives to private car access to employment, retail, services and leisure opportunities in these locations are accessible. Optimisation: Entirely new infrastructure so cannot be scored under this category. Safety: Total segregation, so full score. Biodiversity: -2 given rather than -3 because the option to route Bridleway 6 around the SSSI designated site is there, however, it would likely be close and may still have development impact depending on the level of modal shift success on this route. Higher numbers of pedestrians and cyclists around the SSSI are more likely to cause disturbance to protected flora and fauna of the site. Leisure: Scored 2 as some leisure value - principally a commuter corridor, though.
Brightwell	597	GR 248 447	Brightwell's bway12 cross A12 to 6 unusable for years by all except at night. When safe, day-time crossing for ATs is provided, then Brightwell bridleway 6 needs connect to safe cycle & walkway to Ipswich Hospital,	Brightwell Lakes coming Pegasus Crossing of A12: although a bridge like that at GR 246453 (I find fully acceptable unlike 169) would be better, as doubt any horse & rider will use and many ATs will be reluctant to	0	0	0	-3	0	0	-3	Connectivity and Growth: No connectivity and growth benefit at current as Bridleway 6 (For onward travel from the crossing) is not surfaced or well maintained, making it currently unsuitable for cycling and walking. Modal Shift: No anticipated modal shift benefit.

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			town, buses, coaches & rail NB: two way requirement Quiet Lanes Suffolk point to need to encourage the 200,000 living Ipswich & suburbs, to benefit by AT on PROWs in countryside, without needing to come by car.	stop busy & fast traffic. But if the smart lights & vehicles in platoons system are adopted in lieu of widening the 4 roundabouts, then the Pegasus crossing will probably be best								Optimisation: N/A Safety: A pegasus crossing of the A12 could be problematic due to the speeds the vehicles are travelling at. A new bridge is a more likely possibility for future permeability enhancements; the Strategy does not currently include it as a recommendation due to constraints (cost, habitat/conservation impact of cutting through Martlesham Heath/Ipswich Heaths SSSI) meaning the enhancement of the existing ped/cycle bridge and Broomfield alleys are a more deliverable option, at least in the short/medium term. A bridge or underpass at this location would be much more appropriate. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Brightwell	598	GR 239432 and 238431	Foxhall' footpaths #18, #26 and #27 have been severed by the A12 although #18's finger posts are still in place. #26 has a car-sized culvert through which a stream flows. Also the A12 (T) has severed the #27/#25 crossing, which has an AT suitable road to the west and #25a lane to houses beside a track to the east.	It could have an inexpensive walkway through, but H&S will probably veto. But #27/#25 seems very suitable for a Toucan crossing which would provide an attractive and relatively direct route for ATs in both directions. Indeed this and the Bucklesham/Levington bridleway #21 crossing of the A14 could provide a good AT route	0	0	0	-3	0	0	-3	Connectivity and Growth: No connectivity and growth benefit at current as Bridleway 6 (For onward travel from the crossing) is not surfaced or well maintained, making it currently unsuitable for cycling and walking. Modal Shift: No anticipated modal shift benefit. Optimisation: N/A Safety: There should be no signalised crossings over this section of the A12 due to the speed of travel at this point and visibility issues for vehicles. Likewise, BW21 (Levington Lane) should not have a level signalised crossing, though a fully segregated means of crossing the A14 in this location, i.e. a bridge, would have value. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Bromeswell	20	Wilford Bridge round about up to entrance of Sutton Hoo	The footpath is getting smaller as the hillside is slowly creeping over on to the path Not only that but excessive amount of weeds growing on the curb The main issue - the footpath needs widening and allowing cyclists - many want to cycle to woodbridge from the peninsula but dont due to this bottle neck on the hill and the roundabout is dreadful and is desperate for an up grade	cut back into the side of the 'hill' to widen the footpath split the footpath with markings to allow cyclists and people and then make clear signage from the railway station to sutton hoo of a cycle path Engage with National trust to see if they can help - we need a better sustainable travel option to a world heritage site	0	1	0	3	-2	2	4	Connectivity and Growth – This section provides limited connections to other villages and services. Modal Shift – Using PCT, the development of a cyclist and pedestrian shared pavement will achieve a small modal shift, therefore scoring it a 1. Optimisation – the proposed improvements are new and do not optimise existing infrastructure. Safety – The current footway is narrow and the road it adjoins has both national speed limited and 40mph speed limit. Given the speed limit and that the proposal allows cyclists off the road, it has a high potential for safety improvements. Biodiversity – The proposal will likely result in the removal of hedges and trees. The resultant loss means it has a somewhat high negative impact. Leisure – The proposal will connect the village of Bromeswell to Sutton Hoo and to multiple PROWs, therefore a score of 2 is considered reasonable.
Bromeswell	38	B1083 from Wilford roundabout up towards Sutton Hoo	Tarmac footpath is often overgrown + narrowed due to bank subsiding. Road busy with traffic. Insufficient room to pass each other on path or for the less fit cyclist to walk a bike up in order to prevent cars trying to overtake on this steep, blind hill. This is a popular area for walkers + cyclists accessing Deben, Rendlesham forest, National Trust and coast.	Either 1. Provide a cross country path linking the roundabout with the extensive bridleway network in this area (so it can be used by cyclists too) or 2. Widen path and reinforce bank to provide safer access up hill. I dislike cycling to shops in town as it feels too dangerous.	0	1	0	3	-2	2	4	Connectivity and Growth – This section provides limited connections to other villages and services. Modal Shift – Using PCT, the development of a cyclist and pedestrian shared pavement will have small modal shift, therefore scoring it a 1. Optimisation – The proposed improvements are new and do not optimise existing infrastructure. Safety – The current footway is narrow and the road it adjoins has parts that are national speed limited and other parts at 40mph speed limit so removing cyclists off the road has high potential safety benefits. Biodiversity – The proposal will likely result in the removal of hedges and trees. The resultant loss means it has a somewhat high negative impact. Leisure – The proposal will connect the village of Bromeswell to Sutton Hoo and to multiple PROWs, therefore a score of 2 is considered reasonable.
Bromeswell	166	Road between Sutton Hoo and Rock Barracks	No pavement or cycle lane - vehicles travel extremely fast on this road (60mph) and yet there is no cycle lane or pedestrian route from the barracks into Woodbridge. Many people walk this route (especially from the Travellers Site) and it is very dangerous - especially in the dark. There should be a safe cycle route from all the villages into Woodbridge to enable people to commute by bicycle instead of driving, especially as the bus services are so infrequent and do not connect with trains.	Cycle lane from villages into Woodbridge plus pavement/pedestrian footpath between Barracks and Melton.	1	0	0	3	-3	2	3	Connectivity and Growth – the proposal would connect the MoD site to Woodbridge. The MoD site does appear to be well established in terms of it having a food shop and primary school, therefore it is unlikely the proposal will have significant daily use. The proposal will likely have more leisure benefit than connectivity benefit, therefore a score of 1 under 'connectivity and growth' is considered reasonable. Modal Shift – the proposal would unlikely result in significant modal shift. Optimisation – the proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Heath Road, which is situated just south of the Mod site, and the B1083 have a national speed limit, therefore removing cyclists and pedestrians off the road warrants a significant score. Biodiversity – the proposal will result in significant biodiversity losses including the loss of wild verges and established hedgerows. Leisure – the proposal connects to Sutton Hoo and highly attractive PROW routes, which include those that go through Sandlings Forest and Sutton and Hollesley Heaths. Therefore, a score of 2 is considered reasonable.
Bromeswell	255	Wilford Bridge Melton	This is a dangerous road to cross for pedestrians using the footpaths either side of the river and also bad for cyclists too.	Slowing traffic down so pedestrians get a chance to cross the road, or narrow the road to slow traffic down and widen the pavements which could then accommodate a bike lane.	1	0	0	3	-2	2	4	The commenter proposes the speed along this road to be reduced, however this is outside the remit of the project and should be passed through to SCC. However, the commenter also suggests an off-road cycle lane along Wilford Bridge Road. Connectivity and Growth – the proposal provides a connection to a small handful of PROWs and to Melton railway station; however, it provides limited connections to other villages and services. Therefore, a score of 1 is considered reasonable. Modal Shift – As a leisure route without significant connectivity it is not considered that there will be significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal has safety benefits. Wilford Bridge Road has a NSL and, as a b-type road, volume and speed of traffic is likely high, therefore the highest score under this category is considered reasonable. Biodiversity – In order to develop the proposed infrastructure, the removal of vegetation that adjoins the footway would be necessary – vegetation will likely include a cut verge and unkept shrubs, therefore a score of minus 2 is considered reasonable. Leisure – The proposed route will connect the village of Melton to Melton Riverside,

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												which contains walks along the River Deben, and a handful of other PROWs including both bridleways and footpaths; therefore, a score of 2 is considered reasonable.
Bromeswell	429	Walking path required along side Orford Road besides Woodbridge Rugby Club to provide safe walking from path between path emerging opposite from Eyke Road to track to Potter's Woodyard.	The Orford Road is a busy road with fast traffic and at times lorries. There is a path which links the Eyke and Orford Road's which emerges opposite the Club but to reach the path opposite one has to walk up the busy road side. This is far from safe.	Clear a passable footpath in the grass verge alongside the Rugby Club	0	0	0	2	-1	1	2	Connectivity and Growth – No significant connectivity benefits. Modal Shift – It is unlikely that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – This section of Orford Road, or the B1084, is straight with a 30mph speed limit, however it is likely that speed approaching this section of the road will be high as a national speed limit is situated just east of the Woodbridge Rugby club. Therefore, a score of 2 is considered reasonable. Biodiversity – The proposal will likely result in the removal of the green verge (and potentially some other foliage) situated between the road and the hedgerow adjoining the rugby field. Leisure – As the proposal connects to existing infrastructure to the rugby field, a score of 1 is deemed reasonable.
Bromeswell	430	Orford Road opposite Bromeswell School Lane	There is no defined path from the bridle way over Woodbridge Golf Club to the Bus Stop. This is part of the Sandlings Way and yet is not a well defined path and is very dangerous given the speed of traffic on this busy road	A very short well defined path to connect the Sandlings way on the Bridleway over Woodbridge Golf Club to the bus stop opposite School Lane	0	0	0	3	-1	1	3	Connectivity and Growth – The proposal would connect Sandlings walk to the bus stops and to other PROWs within the network, however the proposal will likely have more leisure benefit than connectivity benefit and there are existing connections (including PROW28). A score of 0 is considered reasonable. Modal Shift – The proposal will likely have more leisure benefit and it is not expected, therefore, that the improvements will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – This section of the B1084, or Orford Road, has a national speed limit. Removing pedestrians off this road has safety benefits, therefore the highest score under this category is considered reasonable. Biodiversity – The implementation of a path along this section of the B1084 would result in the loss of the managed grass verge that adjoins the road over a significant length, hence a small negative score. Leisure – The proposal would connect PROW23 and PROW23X, which are byways/bridleways residing in the Sandlings Walk, to the bus stop and to other PROWs. However, connections, although a little more indirect, do already exist. A score of 1 is considered reasonable.
Bromeswell	624	Wilford Bridge - Access to the peninsula/ Suffolk Coast AONB	The Suffolk Coast AONB is becoming more and more popular for cycling, both on trail and the road. There is minimal signage and road marking to highlight or protect the cyclists on this network of roads and trails.	Additional signage on the main routes onto the peninsula, Wilford Bridge being one, to warn motorists that they are entering a high cycle area. Motion activated signage akin to the speed warning signs that are prevalent on entry to low speed limit areas. Central Bedfordshire Council have used Swarco Ltd signs of this nature. There are also many "high risk" sections of road that comments have already been placed on. eg uphill stretches, entry into wooded sections, blind summits and corners. Again, road markings or signage to highlight additional awareness for bikes would be of real benefit. Finally, as has been noted in other comments, the villages on the Suffolk AONB lack a safe / marked cycle route on the main roads such as B1083, B1084, Heath Road for commuting cyclists. These users may be distinctly different from recreational users and travel at slower speed and so require better protection.	0	0	0	1	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – No significant modal shift benefit. Optimisation – No significant optimisation benefit. Safety – As a road with no sustainable travel infrastructure and with a national speed limit, a guidance sign may have partial benefit, although whether any sign makes significant difference is unknown. Biodiversity – No significant biodiversity benefit. Leisure – No significant leisure benefit.
Bucklesham	249	Levington Lane & crossing the A14 at this point	There is a public right of way that crosses the A14 (Levington Lane) at this point via a gap in the central reservation. It is possible to get across without being killed but you have to be quick.... The A14 verges are often over grown....	Tidy verges so that there is better visibility of the crossing.							N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Bucklesham	272	Seven Hills Road A14 Junction	There are no footpaths or designated cycle lanes at this junction and on the A1156 into Ipswich...This precludes cycling and walking from (& to) Bucklesham, Kirton, Waldringfield and beyond into SE Ipswich and the Ransomes Euro park areaAccess to the newly built crematorium is only possible by car....As an experienced cyclist it is possible to negotiate this junction on the carriageway but it is not safe due to the speed of the traffic.	Provide some sort of path/cycle path as per the Nacton and Claydon Junctions of the A14 connecting with the existing Ipswich to Felixstowe cycle route	0	0	2	1	0	0	3	Connectivity and Growth: Given a 0 as connectivity already exists via Straight Road, and this route is only really valuable to cyclists travelling between Woodbridge/Brightwell Lakes/Bucklesham and the southern/south-eastern (mostly industrial, large retail) area of Ipswich. Felixstowe-bound cyclists from Ipswich would use the Ipswich to Felixstowe Key Corridor (Felixstowe Road) and Felixstowe bound cyclists from Woodbridge/Brightwell Lakes would travel either via Brightwell Lakes, Newbourne and Kirton (Felixstowe to Woodbridge strategic route) or via Bucklesham and Kirton. It would be of most relevance if teamed with a parallel cycle track along the A12 between the Foxhall Road roundabout and the Seven Hills roundabout.
Bucklesham	599a	GR 242407	Bucklesham/Levington bridleway #21 has been severed by the A14. As the only safe AT crossing for 6.5 Kms between Trimley pedestrian bridge and the A1156 road bridge at GR 223433, this seriously deters AT	1: As per reference 272, provide a safe way across the A12/A14 junction at Seven Hills. 2: Provide a pedestrian/cycle bridge at #21. 3; Negotiate with the owner of Walk Farm for ATs' to use the tunnel at GR 252396. This might need traffic type lights at each end so that farm vehicle drivers are forewarned	0	0	2	1	0	0	3	Connectivity and Growth: Given a 0 as connectivity already exists via Straight Road, and this route is only really valuable to cyclists travelling between Woodbridge/Brightwell Lakes/Bucklesham and the southern/south-eastern (mostly industrial, large retail) area of Ipswich. Felixstowe-bound cyclists from Ipswich would use the Ipswich to Felixstowe Key Corridor (Felixstowe Road) and Felixstowe bound cyclists from Woodbridge/Brightwell Lakes would travel either via Brightwell Lakes, Newbourne and Kirton (Felixstowe to Woodbridge strategic route) or via Bucklesham and Kirton. It would be of most relevance if teamed with a parallel cycle track along the A12 between the Foxhall Road roundabout and the Seven Hills roundabout.
Bucklesham	599b	GR 242407	Bucklesham/Levington bridleway #21 has been severed by the A14. As the only safe AT crossing for 6.5 Kms between Trimley pedestrian bridge and the A1156 road bridge at GR 223433, this seriously deters AT	1: As per reference 272, provide a safe way across the A12/A14 junction at Seven Hills. 2: Provide a pedestrian/cycle bridge at #21. 3; Negotiate with the owner of Walk Farm for ATs' to use the tunnel at GR 252396. This might need traffic	2	1	3	3	0	1	10	Connectivity and Growth: In tandem with the infrastructure improvements recommended for the Ipswich to Felixstowe Key Corridor along Felixstowe Road (both of them), re-connecting both halves of Levington Lane and installing a new cycle/pedestrian bridge would be highly effective in opening up active travel to and from Bucklesham which is currently cut off for those that will not ride bikes on-

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				type lights at each end so that farm vehicle drivers are forewarned								carriageway, and therefore are unwilling to ride towards Ipswich via Bucklesham Road (Seven Hills roundabout is highly unsuitable so not an option or improvements included in the Strategy) or Felixstowe via Brightwell Road/Innocence Lane; this connection is most relevant for those wishing to access south-east Ipswich's more industrial areas, and those travelling towards Felixstowe. A score of 2 is given. Modal Shift: No modal shift data as non-existent route. Score of 1 is given an estimate of the impact. Optimisation: Full score given as a fully segregated scheme. Safety: As above. Biodiversity: No anticipated negative biodiversity affects. Leisure: Some leisure value, score of 1 given.
Bucklesham	599c	GR 242407	Bucklesham/Levington bridleway #21 has been severed by the A14. As the only safe AT crossing for 6.5 Kms between Trimley pedestrian bridge and the A1156 road bridge at GR 223433, this seriously deters AT	1: As per reference 272, provide a safe way across the A12/A14 junction at Seven Hills. 2: Provide a pedestrian/cycle bridge at #21. 3: Negotiate with the owner of Walk Farm for ATs' to use the tunnel at GR 252396. This might need traffic type lights at each end so that farm vehicle drivers are forewarned	2	0	0	0	0	1	3	Connectivity and Growth: If a bridge at Levington Lane is not installed, this is the next best option for connecting Brightwell Lakes and the villages to the west of the Deben with the Levington/Stratton Hall/Nacton area, otherwise they must head over to the Nacton Heath/Warren Heath area of Ipswich via Bucklesham as there is no earlier opportunity due to the severance caused by the A14. Score of 2 given. Modal Shift: Considered unlikely to create modal shift on its own. Optimisation: As this would be creating a new PROW over private land, this cannot be scored under optimisation. Safety: No uplift in safety. Biodiversity: No foreseen biodiversity impact. Leisure: Low leisure uplift on its own.
Bungay	350	The A144 between the Bungay Bowling Club and through St Mary's Street, Bungay	Lower Olland Street, Bungay is two way with on street parking. As a result it is often congested and dangerous. It is unpleasant for all users (including motorists) but especially for pedestrians and cyclists. There is a notice telling motorists to "consider pedestrians" and "courtesy crossings" with a 20 mph speed limit. So cycling is often subjected to intimidatory driving, the speed limit is not enforced and nobody knows where it is safe to cross the road.	Make Lower Olland Street one way northbound with Beccles Road one way southbound both with a contraflow cycle lane. Dual use pavements even widened ones, turn cyclists into a hazard. Enforce a 20mph speed limit by camera if need be. Clearly mark and identify pedestrian crossings. The roads in the centre of Bungay were built as multi use roads for pedestrians and horse drawn traffic. To make them more pleasant (and IMPROVE the sacred cow of traffic flow) you need the courage to reallocate some road space. The alternative is doing nothing or demolishing half the town to improve traffic flow.	2	0	0	2	0	2	6	Connectivity and Growth - The improvement would aid connection from the key corridor through to the town centre. Modal Shift - PCT suggest limited modal shift potential if improved to a lower standard (such as using markings), should full cycle paths be possible a higher score could be given. Optimisation - This is new infrastructure so does not represent an optimisation. Safety - This is a busy, 30mph street, where parking can create an obstacle, given its importance a score of 2 is deemed reasonable. Biodiversity - There are no biodiversity impacts. Leisure - This would provide a connection into the historic town centre.
Bungay	482	Footpath/cycleway, bridge and roundabout	There is an informal footpath around the edge of this field which allows pupils from the High School to access East Bungay without going along the busy main road. Turn this into a legal right of way with footpath and cycle way and a bridge over the Tin River. Also to enable safe crossing of the main road put a round about or at least a median island at the junction of Kings Road and St Johns road		1	1	0	1	0	1	4	Connectivity and Growth - The connections for pedestrians already exist using Hillside Road East albeit slightly less direct. Cycling provision in this area is generally poor and it will help connect residents in east Bungay to the school and playingfield so a score of 1 is deemed acceptable. Modal Shift - There is potential for a modest amount of modal shift as it could remove some of the cyclists of Hillside Road East which PCT suggests has decent modal shift potential. Optimisation - This would be a new formal bridleway. Safety - The formalisation of the pathway would have some safety benefit by removing some cyclists off road from Hillside Road East, whilst the crossing will provide benefit across a wide road in St John's if people are currently using this route anyway. Both roads are 30mph and relatively straight with reasonable visibility so a score of 1 is deemed reasonable. Biodiversity - If upgraded to allow cyclists then a new surface would be required, however the loss would be of farmland which is of lower biodiversity value. Leisure - This would create an attractive route that is currently rural in nature. However, it should be noted that the land is allocated and this will potentially lower its leisure value. Providing connections to the playingfield and swimming pool for those in the east means a score of 1 is deemed reasonable.
Burgh	184	B1079, Grundisburgh to Otley	This particular section of the B1079 is a narrow, windy and undulating road and poses a real safety challenge to anyone wishing to walk, mobility Scoot, cycle or ride a horse along it. Its common to see organised 'charity' rides using it as part of their route planning to/from Woodbridge, which further puts cyclists at risk as well as making overtaking difficult for following vehicles.	1. Create one continuous 30mph speed limit along its length, Otley to Woodbridge. 2. Develop an alternative 'cycle' route via the parallel smaller lanes. 3. Encourage organised rides not to use this part of the B1079.	2	0	0	3	-3	1	3	The commenter proposes cycle route between Otley and Grundisburgh. Stoney Road, Charity Lane, and PROWs 35,30, 28, 56, and 58 provides a safer alternative route. Connectivity and Growth – The proposal would connect Grundisburgh to Otley whilst also connecting into Otley College. Grundisburgh and Otley have similar levels of services and it is not likely, therefore, that there would be significant 'everyday' use – this would usually warrant a single point under this category, however as it also connects into Otley College, a score of 2 is considered reasonable. Modal Shift – According to PCT, the proposal will unlikely result in a modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will provide a safer alternative to the B1079, which contains bends, has a NSL, and is likely particularly busy, therefore a score of 3 under this category is considered reasonable. Biodiversity – In order to implement segregated infrastructure adjoining the roads and widening the PROWs to create bridleways, there will likely be significant biodiversity losses. Currently, Stoney Road and Charity Lane have established hedgerows that will likely need to be removed and it is likely that widening FP35, 31, 30, 56, and 58 will result in foliage loss. Leisure – The proposal would connect into the PROW network in and around Otley and, although these PROWs do not extend through designated areas, they are particularly attractive, therefore a modest score under this category is warranted.
Butley	795	Butley	What is not shown are the number of footpaths in existence. Surely if you want to get people to get out walking and use the footpaths you need to identify them! In the EADT last week it commented that 1904 miles of footpaths had been lost in SUFFOLK alone. They could not have just disappeared! There has been an erosion of the rights of walkers by farmers ploughing up the ways. Establish where these paths are and get them re-established.	As chairman of Butley PC I have raised the issue of farmers ploughing up paths and never even received any answer from Suffolk CC. So lets have some joined up thinking and action. Otherwise this is all a waste of time and money.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.

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Campsea Ashe	401	Mill Lane Campsea Ashe	Narrow road, high hedges, no footpaths, heavy traffic from agriculture	Mark as unsuitable for cyclists/walkers	0	0	0	1	0	-2	-1	Connectivity and Growth – Removing access to this road is unlikely to have a significant ‘Connectivity and Growth’ impact as the road does not connect to any key services, however it is likely that the proposal will have a ‘Leisure’ impact. Modal Shift – No modal shift impact. Optimisation – Not considered an optimisation. Safety – Restricting access would remove potential conflict between cyclists / pedestrians and vehicles; however, Mill Lane is a minor road with a 30mph SL containing a number of passing places, therefore a score of 1 is considered reasonable. Biodiversity – Suggested improvement is unlikely to have an impact on biodiversity. Leisure – Removing cyclists and pedestrians from using this route would restrict access to a handful of PROWs.
Campsea Ashe	496	Marlesford Lane dips beneath railway line at Bucks Head bridge.	Road often floods after rain in winter and from irrigation run-off in summer. Existing drain usually blocked. Water depth often sufficient to prevent access by walkers and cyclists - sometimes deep enough to cause abandonment of motor vehicles.	New drainage works.							N/A	This is a highways issue and should be passed to SCC
Campsea Ashe	498	Blackstock Crossing	Register as quiet walking and cycling route between Wickham Market and Blaxhall and on to Snape.		2	0	0	2	0	3	7	Connectivity and Growth – The proposal will connect Lower Hacheston, Blaxhall, and Snape. All three settlements have limited services, which will allow an element of service pooling, and Blaxhall is within the Snape primary school catchment area so there may be ‘everyday’ use of the infrastructure. A score of 2 is considered acceptable. Modal Shift – PCT suggests that even if infrastructure is delivered to the highest standard, the proposal will not have a resultant significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The road has a national speed limit and is particularly narrow so the proposal will have safety benefit, however as it is unlikely that the road can be made completely traffic free a score of 2 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – As the proposal connects into Snape, which is situated by the River Alde and has a multitude of attractive PROW routes, it is likely there will be significant leisure benefit. A score of 3 is deemed reasonable.
Campsea Ashe	499	Ashe Road between Campsea Ashe and Eyke / Rendlesham	Register as a quiet walking and cycling route between Campsea Ashe station and Eyke or Rendlesham. Give priority to walkers and cyclists.		1	0	0	1	0	1	3	Connectivity and Growth – The proposal will connect Eyke to Campsea Ashe and, as both settlements have limited services, the connection will allow an element of service pooling. As a quiet lane is not considered high-quality infrastructure, a modest score is considered reasonable. Modal Shift – PCT suggests that Ashe Road Road is not currently well used, and infrastructure will unlikely result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Ashe Road has a national speed limit and is particularly narrow. Creating a quiet lane may reduce conflict between vehicles and pedestrians/cyclists, however they do not introduce hard safety measures. A score of 1 under this category is considered reasonable. Biodiversity – No biodiversity impact. Leisure – The proposal will connect into a handful of attractive PROWs; however, the proposal will likely have more connectivity and growth benefit. A score of 1 is considered reasonable.
Campsea Ashe	500	Ivy Lodge Road between Campsea Ashe and Rendlesham / Bentwaters	Register as a quiet cycling route.	Frequently used as a short cut by lorries accessing Bentwaters from the A12. Road not suitable for HGVs and potentially dangerous for walkers and cyclists Prohibit HGVs from using this route (with exception of agricultural vehicles).	1	0	0	1	0	1	3	Connectivity and Growth – The proposal will connect Rendlesham to Campsea Ashe. Both settlements have limited services and the connection will allow an element of service pooling and Campsea Ashe has a train station, however a quiet lane is not high-quality infrastructure reducing the benefit, a score of 1 is warranted. Modal Shift – PCT suggests that Ivy Lodge Road is not currently well used and infrastructure will unlikely result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Ivy Lodge Road is likely busy and has a national speed limit. Creating a quiet lane will reduce conflict between vehicles and pedestrians/cyclists, however it doesn’t introduce any hard safety measures. A score of 1 under this category is considered reasonable. Biodiversity – No biodiversity impact. Leisure – The proposal will connect into a handful of attractive PROWs; however, the proposal will likely have more connectivity and growth benefit. Therefore, a score of 1 is considered reasonable.
Campsea Ashe	578	Public footpath from Mill Lane Wickham Market to Mill Lane Campsea Ashe	The path between the bridges gets very muddy and when the river is in flood mode the bridge closest to Wickham Market can become unreachable due to high water levels. This route could also provide a good cycle route from the centre of Wickham Market to the railway station	Improve the entrance to the bridge. Provide a decent surface along the public footpath.	2	0	1	2	0	1	6	Connectivity and Growth – The proposal will connect Wickham Market to Campsea Ashe, which allow an element of service pooling and create a connection to a train station. A score of 2 is considered reasonable. Modal Shift – The alterations would not be expected to create significant modal shift although it will create better availability for some users. Optimisation – The improvements will help make the path more inclusive. This will provide an improvement to a path that is already off-road meaning it is considered one point. Safety – The proposal offers a safer route between the two villages than the B1078, however as the route will not be completely traffic free, a score of 2 is considered reasonable. Biodiversity – No significant biodiversity impact. Leisure – These paths are particularly attractive PROWs as they reside along the River

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												Deben and, as the improvements will provide leisure access to a wider range of people, a score of 1 is deemed acceptable.
Campsea Ashe	678	B1078 between Campsea Ashe and Five Ways / Lower Hacheston	very dangerous conditions for pedestrians and cyclists trying to access the key bus stops at Five Ways from Campsea Ashe	Pavement / footway-cycleway; some can be done as pavement adjacent to kerb (e.g., in front of houses and Lower Hacheston) some as segregated track parallel to the road, behind hedgerows	2	0	0	3	-3	1	3	The commenter proposes reducing the speed on The Hill, Wickham Market, however this falls outside the remit of the project and should be passed through to Suffolk County Council (SCC). The commenter also proposes a 'shared space' Connectivity and Growth – The proposal would connect Lower Hacheston and Campsea Ashe, which are both relatively small settlements. Both settlements have limited services, but the connection would allow an element of service pooling. Therefore, a score of 2 is considered acceptable. Modal Shift – According to PCT, Ash Road (B1078), is relatively quiet, therefore it is unlikely that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – Ash Road, is a 'B' type road with a NSL. Getting cyclists and walkers off road will have significant safety benefit. Biodiversity – The proposal will likely result in the removal of the well-established hedgerows that adjoin the road. Leisure – The proposal will likely have small leisure benefit as it will help in connecting a small handful of PROWs, therefore a modest score is considered reasonable.
Carlton Colville	121	Bridleway at Carlton Marshes (Suffolk Wildlife Trust) ends in the middle of a field	The bridleway ends in the middle of the field. This could be extended at the bottom of the flood wall to the river.	By extending the bridleway at the base of the flood wall there will be no risk of injury to walkers and still allows cyclists to be able to ride from Oulton across the Waveney and on towards Norwich..	0	0	0	0	0	3	3	Connectivity and Growth – Carlton Marshes doesn't provide connectivity in terms of settlement, population or the built environment. Whilst a key corridor does exist nearby this proposal extends outwards towards the Marshes. Modal Shift – As an extension to a leisure route there is unlikely to be significant modal shift. Optimisation – This would represent a new route for cyclists as opposed to an optimisation. Safety – Extending a footpath has limited safety potential. Biodiversity – Path appears a reasonable size currently so unlikely to need direct biodiversity removal, however increased cyclists to important natural area would need to be considered. Leisure - This could become an attractive leisure route extension that encompasses an important visitor attraction. The route represents a strong Leisure route adjacent the river and adjoining the Carlton Marshes with its new visitor centre. The attractiveness of the route means it is considered a full score.
Carlton Colville	405	The cycle access at Bloodmoor Road bridge	No cycling access from under the footbridge onto the cycle path. To use a cycle you need to either carry up steps to access or ride cycle over bridge to other side of the road which does not have a cycle path.	Place cycle path from Dale End area of estate on the cycle path of A12 which will take children to Pakefield High School Main entrance. Or place cycle path alongside A12 on School side of the road. Children walk to school because the only other cycle route is from Bloodmoor roundabout which when coming from the Dales housing estate doubles the journey	1	0	2	0	-1	0	2	Connectivity and Growth - Connectivity does exist, however the lack of ability to get onto the cycle bridge without a significant alternative from the western side of the A12 means this improvement will provide some additional connectivity. Modal Shift - PCT has limited data on getting east to west across the A12, but with alternative crossings to the north and south it is not considered to create significant modal shift growth. Optimisation - An additional ramp provides good optimisation of the existing cycling and walking infrastructure. Safety - There is a shared path along the A12 with crossing points at either end so a safe crossing is available albeit slightly less direct. Biodiversity - The proposed ramp would result in the loss of managed grass. It isn't clear what tree removal, if any, would be required so it is given a minus 1 score, but this could become a high minus number should significant foliage removal be required. Leisure - This is likely to be predominantly used for day-to-day use over leisure use.
Carlton Colville Madison	8	Footpath between Elmdale Drive and Wannock Close	Metal railings obstructing the footpath, slowing down cyclists and making it difficult for people with mobility issues to get through.	Remove railings. These are not required as they are approximately 10 metres from either Elmdale Drive and Wannock Close so do not help with safety. Also, there are many other similar footpaths in the area without these.	0	0	1	0	0	0	1	Connectivity and Growth - Provides a modest short cut, but alternative routes are available which are not indirect so this limits the connectivity and growth score. Modal Shift - This improvement is not expected to create significant modal shift. Optimisation - Removing the barrier will improve the use of this section of shared path providing a modest benefit. Safety - Barriers are likely present to stop vehicular traffic so an alternative should be discussed with SCC. No score has been given in this category. Biodiversity - There is no significant biodiversity impact. Leisure - This route is within a residential area and is not considered to significantly benefit leisure users.
Clopton	177	B1078 junction with Manor Road at Clopton IP13 6QN	Traffic coming up the hill in Easterly direction is often speeding and also often overtakes on the brow of the hill where the driver can have no view of road ahead. At the top of the hill is a road junction, a blind corner, a village hall, a childrens' play area and a bus stop. Cycling and walking along this stretch of road is made suicidal by speeding traffic, and HGVs. It is necessary to cross this road to access local footpaths, the childrens play area and the village hall.	A speed limit through the village of 30mph would be a good idea to start with. At the very least, double white lines (no overtaking) up the hill to prevent blind overtaking would be a step forward.							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Clopton	178	B1079 between Grundisburgh and Otley	Twisty narrow road with considerable lorry traffic is not safe for cyclists or walkers.	Newly developed cycling routes should avoid this road.	2	0	0	3	-3	1	3	The commenter proposes cycle route between Otley and Grundisburgh. Stoney Road, Charity Lane, and PROWs 35,30, 28, 56, and 58 provides a safer alternative route. Connectivity and Growth – The proposal would connect Grundisburgh to Otley whilst also connecting into Otley College. Grundisburgh and Otley have similar levels of services and it is not likely, therefore, that there would be significant 'everyday' use – this would usually warrant a single point under this category, however as it also connects into Otley College, a score of 2 is considered reasonable. Modal Shift – According to PCT, the proposal will unlikely result in a modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will provide a safer alternative to the B1079, which contains bends, has a NSL, and is likely particularly busy, therefore a score of 3 under this category is considered reasonable. Biodiversity – In order to implement segregated infrastructure adjoining the roads and widening the PROWs to create bridleways, there will likely be significant

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												biodiversity losses. Currently, Stoney Road and Charity Lane have established hedgerows that will likely need to be removed and it is likely that widening FP35, 31, 30, 56, and 58 will result in foliage loss. Leisure – The proposal would connect into the PROW network in and around Otley and, although these PROWs do not extend through designated areas, they are particularly attractive, therefore a modest score under this category is warranted.
Cookley	742	Blyth Valley towards Walpole	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Explore the possibility of the above linking to a footpath along the Blyth valley west towards Walpole. (flood risk may make this unviable and land ownership not known).	3	0	0	0	-2	1	2	Connectivity and Growth - Creating a walking and cycling route between Walpole and Halesworth would be a significant connectivity improvement for the area. Modal Shift - No effect. Optimisation - This improvement looks to create a new piece of infrastructure. Safety - No effect. Biodiversity - The proposed route location is close to the River Blyth which is a sensitive area. Any works close to the river will likely result in a negative impact to biodiversity. Leisure - This route will provide modest leisure benefits in itself.
Corton	188	Hopton to North Lowestoft lack of a cycle route either along the A47, the coast road from Corton to Hopton or on bits of the old railway line.	There is no dedicated cycle route from north Lowestoft to Gorleston or Yarmouth. There is a dedicated cycle path alongside the A47 in Norfolk, from Gorleston to Hopton, after that there is nothing. Cyclists either have to go along the busy A47 or the coast road, which has high hedges, sharp bends and adds distance to the journey. This road is used by tourists staying at facilities in Corton and Hopton, who are not used to tight bends and cyclists. It is a real health and safety issue.	The options are either a continuation of the cycle path alongside the A47 from Hopton to the Corton Long Lane roundabout and possibly a spur off to Oulton Broad or a dedicated cycle route alongside the coast road.	3	3	0	3	-2	0	7	Connectivity and Growth – The current route is indirect, but by creating a more direct route it provides connections between Lowestoft and Gorleston which are both sizeable towns meaning it receives the top score. Modal Shift – Using PCT it shows that upgrading the A47 or the current route will have significant modal shift. Considered together it gives the highest score. Optimisation – This does not optimise existing infrastructure Safety – This will ensure that cyclists either are taken off the A47 (PCT suggests some but not a significant number use this route) or off Coast Road. Getting people off the A47 by providing a more direct route gives this a top score. Biodiversity – The exact placement of the route is not clear, the comment suggests the route should be alongside the A47. Such a route would likely involve some vegetation removal whether cut verge which could score a minus 1 or trees which could score minus 3. A minus 2 is considered a reasonable score. Leisure – A connection between Hopton to Lowestoft would be considered a more commuter route than leisure, any leisure benefits would be relatively modest giving a neutral score.
Corton	391	Church Road and the Coast Road from Corton to Hopton	This is a dangerous stretch of road for cyclists and walkers as it is narrow and has several blind corners	A dedicated cycle/footpath would improve it immensely. Some years ago Sustrans proposed using the old railway lines but it never happened, this would be a good solution, if that is not possible then creating a separated route along the road would help	3	3	0	3	-3	1	7	Connectivity and Growth – The current route is indirect, but creating a more direct route it provides connections between Lowestoft and Gorleston which are both sizeable towns meaning it receives the top score. Modal Shift – Using PCT, it shows that upgrading the A47 and coast Road will have significant modal shift. Considered together it gives the highest score. Optimisation – This does not optimise existing infrastructure Safety – This will ensure that cyclists are either taken off the A47 (PCT suggests some although not a significant number use this route) or off Coast Road. Getting people off the A47 by providing a more direct route gives this a top score. Biodiversity – Using the old railway would likely involve vegetation removal. The railway is now heavily overgrown and contains areas of standing water. Leisure – Unlike other comments relating to a connection between Hopton and Lowestoft using the old railway will create an attractive route with some leisure potential.
Cransford	211	Bannocks Lane Cransford	This is on a marked cycle route. When the road was resurfaced pot holes were not filled prior to coverage with chippings. This makes the the pot holes more dangerous as it is much more difficult to see them. This applies in many other areas of the region and is potentially very dangerous both to cycles and cyclists.	All pot holes should be repaired prior to any surface dressing being applied. Contractors work needs to be thoroughly checked by council officials.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Darsham	108	A 12 cycle path from Kelsale to Hinton is not maintained and is largely therefore unsafe to use.	Both the surface and surrounding hedgerows etc are not maintained and the cycle path in many places isn't usable, so you have to cycle on the A12, which is often quite unpleasant on a bike among fast, heavy traffic	Maintain the cycle paths							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Darsham	230	Junction of 'The Street' Darsham, with A12	When cycling from Darsham village up to this A12 junction its not obvious that there is a short cycle path on the righthand pavement. This is effectively on the wrongside of the road and as a cyclist you have to cross the opposite carriageway of the 'Street' at its junction with the A12 to get to it. Which is putting yourself at conflict with vehicles turning off the A12 into 'The Street'. Its a similiar situation at the Willow Marsh Lane Junction opposite.	Extend the 'cyclepath' around the corner of the verge into 'The Street', make it a decent width and not just footpath sized. Do a similar thing to the one at the Willow Marsh Lane Junction opposite. Some A12 roadside bollards and improved signage to show a 'cycle crossing' would make it 'more obvious' to A12 drivers.	0	0	1	3	0	3	7	Connectivity and Growth - There is an existing footway meaning that this category scores zero as no new connection will be made with this improvement. Modal Shift - PCT uplift of 25 meaning that this category scores zero. Optimisation - There is an element of optimisation in the use and upgrading of the existing cycle paths, but much of this will likely be new infrastructure so it scores modestly in this section. Safety - Removing cyclists off the A12 provide a high potential for safety benefits. Biodiversity - Existing grass verge would be removed to accommodate a path wide enough to cycle on however it would only be a small section. Leisure - This area would link into the Tourism and Leisure key corridor.
Darsham	338	Junction of A12 and The Street, Darsham	Twice we have used the train from/to Ipswich to/from Darsham Station to ride out to the coast. We used the cycle path beside the A12 to get to 'The Street'. It was extremely difficult to cross the A12, traffic in both directions was continuous and travelling fast (possibly faster than the 40mph speed limit) and we had to wait for a considerable time for a gap in both directions before being able to cross SAFELY. My suggestions for improvement are shown below. Not safe for adults let alone children	1. A signalised crossing for pedestrians and cyclists 2. Lower speed limit on the road at this point 3. Advance signs warning of cyclists and/or pedestrians crossing. 4. Painting SLOW PEDESTRIANS/CYCLISTS CROSSING on the road in each direction. 5. Install a speed camera at this location. 6. Install a central refuge to allow the road to be crossed in two stages.	0	0	3	3	0	3	9	Connectivity and Growth - New crossing will not create a new route as such but instead make use of the existing infrastructure. Modal Shift - PCT uplift of 25 meaning that this category scores zero. Optimisation - The new crossing will improve the existing footways to allow pedestrians to access the Street from Darsham Station safely. Safety - Providing a safe crossing on the A12 will be beneficial and score maximum points. Biodiversity - No effect on biodiversity. Leisure - scores maximum as it will link into the tourism and leisure key corridor.
Darsham	366	Footpath entrance adjacent to the railway crossing at Darsham station	The public footpath exit on to the A12 is dangerous. It opens directly onto the A12 with poor steps, no visibility or waiting place for crossing. The pavement is the other side of the road with no direct means to access it other than either go back up the road or over the banked verge. The exit has been marked as closed for some time but needs to be re-opened to allow access to the station and the shop at the garage.	Work needs to be done to the steps, waiting area, visibility for crossing the road and allowing access onto the pavement the other side of the road. or investigate a pavement in front of Darsham Nurseries leading back towards the garage and shop where visibility may be better.	1	0	1	2	0	1	5	Connectivity and Growth - Although the crossing is for the A12, this section has a 30mph limit with speed cameras in place meaning that, although it is still a busy road, cars will be travelling relatively slowly. Modal Shift - No effect. Optimisation - Improving the steps and increasing the waiting area will allow greater access to users. Safety - Due to the nature of the A12, a suitable crossing point and waiting area will have safety benefits, however maximum points is not awarded due to the 30mph speed limit.

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												Biodiversity - No effect. Leisure - This improvement will be a small on road connection to the Tourism and Leisure route.
Darsham	367	between the A144/A12 junction and the Hinton lane/A12 junction (in front of the 2 Magpies bakery)	The formal footpath ends opposite the A144 junction with no where to walk safely next. It is dangerous to walk or cycle to the bakery beside the A12. Create a new stretch of path from the end of the existing path to the Hinton turn off to access the bakery and High Lodge. This would also create a safer link out to Dunwich, Walberswick (and then Southwold via the Bailey Bridge) along the Hinton Road. This could link in with the cycle routes from Willow Marsh Lane.	Create a new stretch of foot and cyclepath from the end of the existing path to the Hinton turn off to access the bakery. There is a wide verge between the end of the existing path in front of the bakery to the Hinton lane turn off. It is only a very short distance and would make the existing footpath very useful.	2	0	0	3	-1	3	7	Connectivity and Growth - Extending the existing footway will create a new connection to Darsham shop and the facilities at High Lodge from Darsham Station. Modal Shift - No effect. Optimisation - No applicable. Safety - Currently pedestrians must walk along the A12 as the path ends before Darsham shop and High Lodge, extending the path will allow pedestrians to remain off road and would provide benefits. Biodiversity - the improvement would require the removal of a grass bank which would have a minor effect on biodiversity. Leisure - Access will be available to High Lodge and this infrastructure could feed into the Tourism and Leisure route.
Darsham	408	Darsham Station	Lack of connecting cycle/footpath to/from Darsham station towards Westleton, towards Yoxford	With land allocated for development why not include a dedicated cycle/foot path connecting Darsham Station with Westleton Road through this development and Darsham Station to Yoxford by widening the A12 footpath to cycle/footpath specification	3	0	0	3	0	3	9	Connectivity and Growth - Connecting the train station with the allocation will provide a high quality new connection. Modal Shift - PCT score below 30. Optimisation - No existing infrastructure. Safety - Redirecting pedestrians and cyclists away from the A12 and through the development will have pedestrian benefits. Biodiversity - No effect as this land is allocated for development. Leisure - Pathway could be linked into the Tourism and Leisure route. If this route is not achieved then widening the existing path could be considered as a fall-back approach.
Dunwich	223	Westleton Road, Dunwich between access tracks to Mount Pleasant and Raceground Housee.	Walking on a busy road makes this circular walk dangerous.	Create a short footpath along the edge of the National Trust field to link the two existing footpaths.	2	0	0	2	-2	3	5	Connectivity and Growth - Would join up PROWs to connect Dunwich with Westleton Modal Shift - No significant Modal Shift increase Optimisation - No existing infrastructure Safety - Taking pedestrians off a straight, narrow and potentially fast road has benefit Biodiversity - Loss of established hedge would score a -3, however there is potential to situate a path behind the hedgerow improving the score to -2. Leisure - Key link to existing leisure routes and increased access to Dunwich.
Easton	323	Easton, Suffolk	The roads out of Easton to surrounding villages do not have pavements and the increasing through traffic in Easton, particularly at rush hour and during school run means it is increasingly unsafe to walk/cycle. Neither Wickham Market or Framlingham is far from Easton and would be easily walkable if it weren't for the danger of the roads. While it is not possible to provide pavements, I suggest that permissive paths on the edge of farmland could be instigated which link public rights of way.	Pursue a series of permissive paths on the edge of farmland that link the village with Wickham Market and Framlingham and public rights of way so providing a safe walking network in and around the village separated from roads. Such paths would not need to be wide - possibly only 1-2m wide and once created could be maintained by footfall. This idea is not applicable just to Easton it could be rolled out across many rural villages to encourage walking.	1	0	0	3	-1	1	4	Connectivity and Growth - Easton is a relatively small settlement with limited services with the exception of a primary school, connecting it to either Framlingham or Wickham Market would allow an element of service pooling. However, the proposal will likely result in a relatively indirect route and will likely have more leisure value than that of connectivity. With consideration to the previous, a score of 1 is deemed reasonable. Modal Shift - There is insufficient evidence to suggest that the proposal would lead to a significant modal shift. Optimisation - The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety - If viable, the proposal will provide an alternative route to that of along Wickham Market Road, which has a national speed limit and appears relatively narrow in some sections, and Framlingham road, which also has a national speed limit. Getting pedestrians off this road has significant safety benefits. Biodiversity - The proposal will likely result in the loss of managed grassed areas edging the agricultural fields, therefore a score of -1 is considered reasonable. Leisure - The proposal will likely result in attractive PROW routes, hence a score of 1 under this category.
Ellough	21	Ceder drive towards new roundabout	No cycling or walking path connecting the Ellough Road with the new Beccles bypass	Install a cycle/walking path.	2	1	0	3	-2	0	4	Connectivity and Growth - Connects through to isolated employment uses and would benefit the proposed Garden Neighbourhood. The employment land isn't a key service so 2 points have been given. Also benefits from connecting 2 identified key corridors. Modal Shift - PCT shows the road is poorly used currently, there are other routes south onto the new infrastructure and the allocated Garden village that may also provide additional connectivity, however Datashine shows no walking to work in this area, as an employment area, albeit isolated some gain could be made here. Overall a score of 1 is deemed reasonable. Optimisation - The proposed improvements are new and do not optimise the existing. Safety - The road is narrow and NSL, removing cyclists/walkers off this road would provide safety benefits scoring it a 3. Biodiversity - The grass verges would have to be removed and they are currently largely wild meaning in the short term at least there would be a negative biodiversity impact so minus 2 has been given. Leisure - There are limited leisure routes nor does it connect leisure attractions so it scores 0. It should be noted that if an alternative connection is provided through the proposed Garden Neighbourhood this could lower the connectivity and growth and modal shift scores.
Ellough	321	From Church Rd, Ellough left to Mor Business park.	It's impossible to walk safely from Church Rd Ellough to the Moor Business park. Theoretically you would need to cross over to Walkway/cycle way towards roundabout but cannot cross over Benacre Rd again opposite entrance to Moors Business park as there is a ditch to traverse. There is enough space on the side of the road as the business park to provide a walkway/cycle way.		3	1	0	3	0	0	7	Connectivity and Growth - This cycle/walking path extension lies on a key corridor and provides a full connection from Beccles into the employment zone. Modal Shift - PCT suggests limited growth, however it is based on census data and may not factor the new infrastructure alongside the southern bypass nor the garden village so a score has been provided here. Optimisation - This represents new infrastructure and not an optimisation. Safety - This is a national speed limit road, busy and with a likely high level of HGV traffic, getting cyclists and walkers off the road has a high safety benefit. Biodiversity - This will result in a modest section of well managed grass verge only. Leisure - The connections to employment areas suggests a day-to-day use over a leisure use.

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Elloough	669	Lowestoft to Elloough	Cycling to the Elloough farmers market from Lowestoft. The majority of this route is currently satisfactory despite no obvious provision for cyclists once out of Lowestoft, but at the end cyclists are deposited onto a very fast busy B road with no provision for cyclists.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.	3	1	0	3	-3	1	5	Connectivity and Growth - This path exists on one of the key corridors and helps connect into the major settlement centre of Lowestoft and the larger market town of Beccles. Furthermore it connects a large employment area and to a large allocation in the Garden Neighbourhood. Modal Shift - PCT suggests some modest potential for modal shift. It is recognised that PCT uses census data that may not factor in the rest of the relief road, but a score of 1 is deemed reasonable. Optimisation - This would represent new cycling infrastructure. Safety - The B1127 is a busy, fast flowing road with HGV use so removing cyclists off this road would be of benefit. Biodiversity - With trees and hedgerow close to the road boundary any new cycle path would likely have a high biodiversity impact. Leisure - Whilst this route will likely be for more day-to-day use with connections to the employment area by expanding the existing path to the farmers market and then to surrounding villages and the wider Beccles Cultural offer has some leisure benefit.
Eyeke	626	The corners and ascent into the forest at Spratt's Street	High risk point for cyclists: fast driven corners meeting slow moving cycles and change in light conditions as a result of the trees.	Signage or road markings to highlight this would be of benefit.	0	0	0	1	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – No significant modal shift benefit. Optimisation – No significant optimisation benefit. Safety – As a road with no sustainable travel infrastructure and with a national speed limit, a guidance sign may have a partial benefit, although whether any sign make a significant difference in reality is unknown. Biodiversity – No significant biodiversity benefit. Leisure – No significant leisure benefit.
Felixstowe	52	Old Felixstowe, walk to Felixstowe Ferry	The pathway by the sea down to Felixstowe Ferry is hard core or gravel, which makes walking difficult and renders it almost impossible for wheelchair users or buggies to complete the walk to the ferry and the cafes at Felixstowe Ferry.	To replace the rough walking surface with a smooth surface to encourage walkers to reach Felixstowe Ferry.	0	0	1	0	0	1	2	Connectivity and Growth – No significant connectivity benefit. Modal Shift – The alterations would not be expected to create significant modal shift although it will create better availability for some users. Optimisation – The improvements will help make the pathway more inclusive. This will provide an improvement to a path that is already off-road meaning it is considered 1 point. Safety – The issue raised is a matter of access and usability over safety. Biodiversity – No significant biodiversity benefit. Leisure – These paths represent high value leisure links alongside the river and coast and will provide leisure access to a wider range of people and improved surfaces for all meaning it scores a point in this category.
Felixstowe	64	Footpath leading to steps to the beach at the end of Martello Lane, Felixstowe. Known as Jacobs Ladder I believe	The footpath is overgrown. You need to weave your way along avoiding weeds, plants, dead foliage etc along with overhanging branches from neighbouring houses								N/A	This proposal has been scored '0' in each of the MCAF categories because it relates to an issue more appropriately dealt with directly by the Highways Authority (e.g. highway maintenance, speed reductions), rather than through the Strategy.
Felixstowe	116	High Road East, Felixstowe	Very poor road surface in cycle lane	Road needs resurfacing, not just another top dressing, which makes matters worse for cyclists	0	0	0	1	0	0	1	Connectivity and Growth - No significant connectivity and growth benefit. Modal Shift - No significant modal shift Optimisation - No likely optimisation Safety - scored '1' under safety respectively for improving cycling and walking experience and safety. Biodiversity - No significant biodiversity impact. Leisure - No significant Leisure impact.
Felixstowe	118	No entry in to th ASL from 2 directions	The Garrison lane traffic lights has no entry lane into the box either from the south bound direction or the west bound	Your the engineers work it out. Last time I commented on the west bound and you removed the north bound. The whole system needs a rethink. Painted advisory cycle lanes are continually parked on rendering them useless, they are often not wide enough especially when they contain drains							N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Felixstowe	119	School traffic	At school start time there is a lot of contention when parents park on the double yellow lines across the cycleway or crisscrossing the cycle way to drop off kids.	Why can't they use the drop off circle that was designed for this within the school freeing up the high road . And the school should reopen the Maidstone entrance for cyclist	2	0	0	1	0	0	3	Connectivity and Growth – Regarding the re-opening of the Maidstone Road entrance to the school point: this would have added connectivity and safety benefits, meaning children may not have to cycle up to the High Road (which is a busier road than Maidston Road) to reach school by bike, as this is currently the only entrance. 2 points. Modal Shift – No PCT score available for Maidstone Road entrance re-opening. Moving vehicles from the High Road's cycle lanes will improve the road's cycling potential, however it is unlikely that any new dedicated infrastructure could be created meaning no score for modal shift can be created. Optimisation – No change in infrastructure quality Safety – The road appears to be very busy with high levels of parking that will only increase during the school times. It is not a narrow road, but with vehicles parked either side it does essentially become a single lane meaning cyclists have to mix with traffic so it has scored 1 point. Biodiversity – No significant biodiversity benefit Leisure – The road appears to have limited leisure potential.
Felixstowe	137	Felixstowe, Undercliffe Rd at the Leisure Centre car park	Section of road (part of national cycle route 51) extremely dangerous for cyclists due to uncontrolled parking along the road on the Leisure Centre car park side.	Double yellow lines along this section of road on the car park side. Could provide some 30 minute free parking spaces in the nearby leisure centre and Convalescent Hill car parks to mitigate any impact on the businesses facing the leisure centre car park.	0	0	0	1	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The removal of the cars off the road does not create new infrastructure and is not considered to create a significant modal shift to warrant score here. Optimisation – There is no existing cycling or walking infrastructure which this optimises. Safety – The road is relatively wide outside the leisure centre car park, but regardless the parked cars do create an obstacle. A cycle path does exist off the road and through the car park, but this is unlikely to be useful for those travelling past the leisure centre/pier. Biodiversity – No significant biodiversity benefit Leisure – No leisure impact.

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Felixstowe	174	The bridleway which passes Hill House Cottages and Candlet Farm between Gulpher Road and Thurmans Lane	This bridleway is a perfect route to take cyclists off the High Road and High Street through the Trimleys. There has already been comment on the issues facing cyclists travelling along High Road and High Street where they have to move in and out of moving traffic because of parked cars in the dedicated cycle lane.	Improve the bridleway surface and provide adequate signage to divert cyclists onto this route. This would greatly improve the safety and encourage more people to use their cycles when travelling to work and for pleasure.	1	2	3	3	0	3	12	Improving Bridleway 10 to LTN 1/20 standards is critical to access to the North Felixstowe Garden Neighbourhood (NFGN) from the Trimley Villages or Kirton, and potentially the main route in for cyclists/pedestrians originating from Ipswich (west) or Woodbridge (north) way. It needs consistent smooth surfacing throughout to be accessible to road bikes and pedestrians with reduced mobility. Connectivity and Growth: 1 - This route is already accessible to off-road cyclists and already well used, according to Strava Metro data, however opening it up to all active user types in tandem with the NFGN development coming forward will provide some additional connectivity and growth benefits. Modal Shift: 2 - No PCT data, but bridleway 10 is considered to be of relatively little commuting, but may be of school travel value by giving Trimley-based pupils of Felixstowe Academy a traffic-free route via the site know as Land North of Walton High Street. Optimisation - 3 Safety - 3 Ideally, post development bridleway 10 should have no vehicle use at all, and would therefore qualify as a cycle track. Biodiversity - 0 Leisure - 3
Felixstowe	258	A154 Candlet Road between Garrison Lane Roundabout and Gulpher Road overbridge	The improvement required is a segregated cycle lane - an essential component for a continuous safe route between Hamilton Road (Town Centre) and the new North Felixstowe Garden Village Development and planned new leisure centre	There is ample room on both sides of the A154 Candlet Road for a segregated cycle lane between the locations suggested, but preferable on the south west side. This would link in with the existing cycle/pedestrian crossing across Garrison Lane, to link with the existing Grove Road cycle path to the Grove Medical centre, access to the Town Council's Cowpasture Allotments and my proposed segregated cycle lane alongside Garrison Lane (east side) from this point to Fairfield Avenue.	3	3	0	2	-1	3	10	Connectivity and Growth: A cycle/pedestrian parallel to Candlet Road, irrespective of side (adequate crossings/joining points from either side would need to be included), will be critical for east to west movement across the North Felixstowe Garden Neighbourhood (NFGN) and accessing the NFGN from the south (e.g. from Garrison Lane/Grove Road/Spriteshall Lane). Modal Shift: As above - as the NFGN is an entirely new community and this scheme would predominantly serve their needs, the MS score is estimated based on the uplift in the level of cycling anticipated with it compared to without it. Optimisation: New infrastructure so not scored under this category. Safety: Intended to be fully segregated from vehicles, though with some inevitable crossing points of vehicle accesses. Score of 2 given. Biodiversity: Negative biodiversity score due to loss of mature trees, however it is intended that over the long term these trees would be replaced on the NFGN site. Leisure: High leisure value, particularly for older children and young people that might enjoy playing on the track as a safe space from vehicles.
Felixstowe	259	A154 Garrison Lane (from Fairfield Avenue northbound to Grove Road roundabout) - segregated cycle lane	The suggested IMPROVEMENT is a segregated cycle route alongside the southbound side of the A154 Garrison Lane, between the Grove Road roundabout and the pedestrian entrance to Fairfield Avenue.	A safe cycle route is desperately needed between Hamilton Road (Felixstowe Town Centre and Railway Station) to the new North Felixstowe Garden Village Development and proposed new leisure centre. Part of this could be a segregated cycle lane, which is possible on the east side of the A154 between Fairfield Avenue and the Grove Road roundabout, which would link in with the signalled crossing to Taunton Road, the crossing to Cowpasture Allotments and the cycle way along Grove Road to the medical centre, Eastward Ho sports facilities and Abbey Grove woodland	3	3	3	3	0	1	13	Connectivity and Growth: Connecting the NFGN to the Grove Road roundabouts with cycling and walking infrastructure, and (at least) a bi-directional track along Garrison Lane's east side to the High Road crossroads, is critical for sustainable onward travel and integration with existing Felixstowe. Full score of 3 given. Modal Shift: Modal shift score of 3 was given due to the importance of these improvements to connecting future residents/visitors (particularly as the NFGN will include a new leisure centre) of the NFGN with the town's employment/retail/services, and other residential areas. Optimisation: A score of 3 is given as currently there is a poor quality informal footpath (not a PROW) in this location. Safety: 0 as no anticipated significant green space loss. Leisure: A score of 1 is given due to the connection facilitating movement between the Primary Shopping Area (Hamilton Road), the new leisure centre and the train station.
Felixstowe	260	Between Glenfield Avenue and Fairfield Avenue	Signposting a cycle route	A safe cycle and walking route is desperately needed between Hamilton Road/Town Centre, the railway station and the new North Felixstowe Garden Village development and proposed new leisure centre. This is possible by using the route: Hamilton Road (Great Eastern Square) to the Railway Station, thence Station Approach, across High Road West into Glenfield Avenue, left into Fairfield Avenue. At the northern end of Fairfield Avenue, open up existing pedestrian access onto a segregated cycle route alongside the A154 Garrison Lane northbound to the Grove Road roundabout, linking in with the existing signalled pedestrian crossing to Taunton Road/Candlet Road (with proposed segregated cycle lane as far as Gulpher Road overbridge) - also linking in with the crossing to the Cowpasture Allotments and existing Grove Road segregated cycle lane to the medical centre, Eastward Ho and Abbey Grove.	3	3	3	3	0	1	13	Connectivity and Growth: Connecting the North Felixstowe Garden Neighbourhood (NFGN) to the Grove Road roundabouts with cycling and walking infrastructure, and (at least) a bi-directional track along Garrison Lane's east side to the High Road crossroads, is critical for sustainable onward travel and integration with existing Felixstowe. Full score of 3 given. Modal Shift: Modal shift score of 3 was given due to the importance of these improvements to connecting future residents/visitors (particularly as the NFGN will include a new leisure centre) of the NFGN with the town's employment/retail/services, and other residential areas. Optimisation: A score of 3 is given as currently there is a poor quality informal footpath (not a PROW) in this location. Safety: 0 as no anticipated significant green space loss. Leisure: A score of 1 is given due to the connection facilitating movement between the Primary Shopping Area (Hamilton Road), the new leisure centre and the train station. The use of Eastern Square shopping centre to access the train station and onward travel is undesirable as it necessitates dismount, and (unconfirmed) probably means access is limited to operational hours.
Felixstowe	312	Traffic light controlled cross roads of Langer Road and Beach Station Road, Felixstowe.	The traffic lights are activated by sensors in the road. However, they are not activated by cyclists. If a cyclist approaches the junction during quiet times, they face the choice of either waiting for a car to come along and activate the sensor, or jumping red lights. It is incredibly frustrating watching the lights on the intersecting road change through multiple cycles of green orange and red whilst the lights controlling your own progress remain fixed on red.	The sensors need either to be adjusted to ensure that a lone cyclist will be detected and will activate the traffic lights, or the whole system needs to be changed to a simple timer with the requirement for a vehicle to activate a sensor being dispensed with completely.	0	0	1	1	0	0	2	Optimisation: A score of 1 for optimisation was given as this would represent an improvement to the current (lack of) infrastructure for cycling - in this case on-road cycling. Safety: A rating of 1 for safety is given as it reduces the temptation for on-road cyclists to jump red lights during quieter periods.
Felixstowe	313	Cross roads controlled by traffic lights, at High Road West and Garrison Lane, Felixstowe	The traffic lights are activated by sensors in the road. However, they are not activated by cyclists. If a lone cyclist approaches the junction during quiet times, they face the choice of either waiting for a car to come along and activate the sensor, or jumping red lights. It is incredibly frustrating watching the lights on the	The sensors need either to be adjusted to guarantee that a lone cyclist will be detected and will activate the traffic lights, or the whole system needs to be changed to a timer with the requirement for a vehicle to activate a sensor being dispensed with completely.	0	0	1	1	0	0	2	Optimisation: A score of 1 for optimisation was given as this would represent an improvement to the current (lack of) infrastructure for cycling - in this case on-road cycling. Safety: A rating of 1 for safety is given as it reduces the temptation for on-road cyclists to jump red lights during quieter periods.

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			intersecting road change through multiple cycles of green orange and red whilst the lights controlling your own progress remain fixed on red.									
Felixstowe	315	The bridleway which passes Hill House Cottages and Candlet Farm between Gulpher Road and Thurmans Lane	Someone else has suggested diverting cyclists from the High Road to this bridleway. This would be a significant and grossly unreasonably lengthy diversion for cyclists needing to transit between eastern Felixstowe and Trimley. That said, the improvement of the bridleway is a good idea to benefit cyclists who already use it, but it should not be on condition that cyclists who would otherwise use the High Road being expected to divert, as the likely net result would be a reduction in cycling.								N/A	This proposal has been scored '0' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Felixstowe	317	Crossroads of Mill Lane and Garrison Lane, Felixstowe.	The placements of the pedestrian crossings force pedestrians to make a significant detour from the natural line particularly if trying to cross Garrison Lane on either side and either direction.	Locate an additional crossing point to allow pedestrians to cross directly from the NW corner to the SE corner to enable a more direct approach for pedestrians travelling along Mill Lane to cross Garrison Lane in both directions.	0	0	3	3	0	0	6	Connectivity and Growth: No foreseen connectivity and growth benefits. MS: No anticipated modal shift benefit. Optimisation & Safety: If the design can be made to work so that a central 'island' area can be added for crossing the crossroads diagonally, this would offer a significant optimisation benefit for both cyclists and pedestrians. A foreshortening and circulatory approach has been recommended in the Strategy due to the anticipated design difficulties of a central island, however this may be possible to achieve at a detailed level of design (by Highways Engineers). Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	322	High Road East, Felixstowe, & out through Trimleys	Cars regularly parked in cycle lanes	Change from dotted to continuous white line and enforce no parking in bike lanes.	0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be adequate to meet LTN 1/20 Cycle Infrastructure Design standards in this location.
Felixstowe	341	NCN 51 link between Manor Road and the southern end of Promenade, Felixstowe.	The gravel surface of the link between Manor road and the promenade (part of NCN 51) is unsuitable for cycling. The surface is uneven and the gravel is deeper in places and difficult to ride through and could be dangerous for inexperienced cyclists, especially children. After rain there are a number of deepish puddles. This would certainly not be acceptable as a promoted cycle route in the Netherlands and nor should it be in the UK! Parked cars can also obstruct the track.	Provide a suitable surface on one side of the path, clearly marked for cycles and on which car parking is banned. The promenade and path across Landguard Common provide one of the few offroad routes available for parents to introduce their children to cycling and this poorly surfaced link needs improvement.	1	0	3	2	0	1	7	Connectivity and Growth - Score of 1 given as Manor Road is a common point to transfer off the Promenade, as this is effectively where the Promenade ends as beyond this is private land (Suffolk Sands Holiday Park) so the relevance of its improvement is enhanced, even though other connecting points up to Carr Road/Langer Road/Sea Road are available. Modal Shift: Improvements not likely to have any modal shift value. Optimisation: Again, due to Manor Road's relevance as a cycle/pedestrian route, its improvement is important - particularly for cycling. Resurfacing at least a moderate strip of it would be a significant improvement, if the whole section cannot be resurfaced. Safety: Resurfacing in this location, given how bad the quality is of the surfacing at Manor Road currently, could provide a significant uplift in safety. However, Manor Road is still accessible by vehicles, and therefore is not technically segregated (despite it being a small number likely to travel down the dead end road). Score of 2 is given. Biodiversity: No anticipated biodiversity effects. Leisure: Low leisure impact.
Felixstowe	345	Ferry Road to Felixstowe Ferry	This route is popular with cyclists and is part of the NCN with the ferry link across the River Deben. The C class road is quite narrow, twisting and tightly hemmed by the golf course on each side. It is quite scary being overtaken by close passing and relatively fast moving motorised traffic (cars have grown in size over the years).	A 20mph speed limit would be more appropriate for this road which forms a dead end for motor traffic. The road could be marked with cycle lanes each side and a central lane for motor vehicles with drivers having similar to Felixstowe Road between Anson Road and Main Road at Martlesham.	2	1	0	2	-1	3	7	Connectivity and Growth: A 20mph speed limit and segregated cycle lanes to Felixstowe Ferry would, if possible, be likely to improve safety and cycling rates, and open up Felixstowe Ferry to more leisure tourism. However, speed limit changes are not covered by the Strategy and requests must be passed to the Highways Authority. The upgrading and surfacing of Footpath 62 is likely to be the better and cheaper alternative, though segregated cycle lanes along Ferry Road to Felixstowe Ferry could be a viable option, too. Modal Shift: Score of 1 is given as likely to be minimal. Safety: A score of only 2 (rather than 3) is given for safety, as even with segregated cycle lanes, lighting and a 20mph speed limit, as Ferry Road's overall form may still result in speeding. Optimisation: Score of 0 given as its new infrastructure. Biodiversity: -1 for biodiversity given due to damage to golf course fringe areas, which may be valuable for wildlife. Leisure: Full score for leisure is given as Strava Metro shows a strong desire for cycling between Felixstowe and Felixstowe Ferry via Ferry Road.
Felixstowe	365	Ferry Road from Golf Club to Gulpher Road	Cars travelling too fast, particularly at the sharp bends, dangerous for both cyclists and walkers. Road is too narrow for increased volume of traffic.	Speed limit 20mph, warning signs, possibly cycle & foot priority in the area.	2	1	0	2	-1	3	7	Connectivity and Growth: A 20mph speed limit and segregated cycle lanes to Felixstowe Ferry would, if possible, be likely to improve safety and cycling rates, and open up Felixstowe Ferry to more leisure tourism. However, speed limit changes are not covered by the Strategy and requests must be passed to the Highways Authority. The upgrading and surfacing of Footpath 62 is likely to be the better and cheaper alternative, though segregated cycle lanes along Ferry Road to Felixstowe Ferry could be a viable option, too. Modal Shift: Score of 1 is given as likely to be minimal. Safety: A score of only 2 (rather than 3) is given for safety, as even with segregated cycle lanes, lighting and a 20mph speed limit, as Ferry Road's overall form may result in speeding. Optimisation: Score of 0 given as its new infrastructure. Biodiversity: -1 for biodiversity given due to damage to golf course fringe areas, which may be valuable for wildlife. Leisure: Full score for leisure is given as Strava Metro shows a strong desire for cycling between Felixstowe and Felixstowe Ferry via Ferry Road.
Felixstowe	370	Pedestrian-only junction of Upperfield Drive and Links Avenue, Felixstowe.	This is currently only for the permitted use of pedestrians, however Links Avenue and Upperfield Drive could form a quiet and suitable alternative route for cyclists travelling between Ferry Road and Beatrice Avenue avoiding Colneis Road.	If the junction of Upperfield Drive and Links Avenue could be upgraded to a full cycle link as well as pedestrian link, whilst maintaining the barrier to through-traffic by motor vehicles, this could create an additional option for cyclists travelling in this part of town.	0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Small optimisation benefit in the context of the new shared path recommended for Colneis Road. Safety: No added safety benefit over current footpath. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	373	Junction of Chaucer Road and Garrison Lane	Cyclists travelling northward along Garrison Lane wishing to then head towards Western Felixstowe are compelled to continue along the busy Garrison Lane all the way to the crossroads with Mill Lane to turn left onto Mill Lane. There is a junction however with Chaucer Road which is exit only to all traffic including cyclists.	Alter the junction between Chaucer Road and Garrison Lane to permit cyclists bound for Western Felixstowe to turn left from Garrison Lane onto Chaucer Road so that they can avoid the busy part of Garrison Lane approaching the crossroads. Chaucer Road is much quieter and suitable for cycling as well as slightly shortening the distance travelled. The junction would	0	2	1	1	0	0	4	Connectivity and Growth: No significant connectivity and growth benefit - mainly a minor opportunity to increase permeability and get cyclists heading north on Garrison Lane 'south' off Garrison Lane 'south' earlier so they can avoid the Mill Lane/Garrison Lane crossroads, which is not currently suitable for cyclists. Modal Shift: PCT identifies moderate modal shift value, suggesting the Mill Lane/Garrison Lane crossroads may be actively avoided by cyclists. Strava Metro shows average use of Chaucer Lane and heavy use of Garrison Lane, which may be more reflective of

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				require physical work to safely permit cyclists, but not motorists, to enter from Garrison Lane. It should also permit cyclist travelling south along Chaucer Road to turn right onto Garrison Lane or straight over onto Orwell Road.								Chaucer Lane being 'no entry' at the Garrison Lane end, which may be where it would otherwise be more useful for ingress by cyclists if they were allowed. Score of 2 given. Optimisation: Score of 1 given under both optimisation and safety categories on the basis of extra permeability for cyclists being given by making it only 'one way' for vehicles. Safety: From a safety point of view, it would need to be designed and confirmed that it would not actually reduce cyclists' safety using this diversion, which is a high risk with any contraflow cycling infrastructure. Biodiversity: No anticipated biodiversity value. Leisure: No anticipated leisure value.
Felixstowe	381	Gulpher Road, Felixstowe	Provide an improved surface and access to create an accessible cycleway which would link Gulpher Road and the bridleway to provide an effective High Rd bypass for cyclists		2	0	2	0	0	3	7	Connectivity and Growth: A score of 2 is given as improvements to bridleways 10 and 27, in isolation, would be vital for connectivity and growth - though less important to connectivity and growth (Still important as a leisure route) if a bi-directional track parallel to Candlet Road is able to come forward.
Felixstowe	389	walkway/promenade from Cobbolds Pt to Felixstowe Ferry (especially from the Dip toilets northwards) .	This can be an ideal shared use route for cyclists to reach the Ferry off-road, avoiding fast-moving traffic and other hazards (!) on the road through the golf course. Cycle access easy at the Dip.	As with the prom south of Cobbolds Point, more clear signage is needed to ensure safety and consideration of all users, especially cyclists being considerate of and giving way to walkers, but also walkers looking carefully when joining prom or changing direction while walking.	2	1	1	3	-1	3	9	Connectivity and Growth: Connecting up the Promenade would be ideal, though expensive, and would likely incur the need to incorporate coastal defence infrastructure into the design. If fundable, a fully connected, uninterrupted, traffic-free and cycle-able sea-front route between Felixstowe Ferry and Martello Park would be an excellent leisure and tourism asset. Currently Felixstowe Ferry is not safely accessible by cyclists, as Footpath 62 obviously excludes cycling and Ferry Road is known for vehicle speeding and poor visibility. The necessary scheme to achieve this - which would need to upgrade and surface Footpath 62 at least/or achieve the equivalent - would therefore have high connectivity value. However, Felixstowe Ferry has a small population, and the route would predominately be of leisure value, so score is adjusted to 2. Modal Shift: As this would be principally a leisure route, and the population of Felixstowe Ferry is quite small, a modal shift score of 1 is given. Optimisation: Score of 2 given for the improvements to the existing sections, which in places have poor surfacing, though are already segregated from vehicles. Score of 0 given for entirely new sections. 1 overall. Safety: Score of 3 given as the route is full segregated from vehicles throughout its length. Biodiversity: A cautious score of -1 is given for biodiversity, as the biodiversity impacts of creating a new section of sea wall where none currently exists are unknown. Leisure: Full score for leisure.
Felixstowe	425	Entrance to Peewit Caravan site to former Beach Station (past Felixstowe Beach Holiday Park)	Unclear as to whether cycling is allowed on the "footway"	Cycling is allowed on the footway between McDonalds/Dock Gate 1, in front of Lidl's supermarket, the JW Kingdom Hall, up as far as Peewit Caravan site approach road. It is then unclear whether cycling is allowed alongside Beach Holiday Park, although there is no difference in the width of the footway. Solution: clarification/additional signage needed	0	0	0	0	0	0	0	Connectivity and Growth – No significant connectivity benefit.Modal Shift – Better advertising that the shared cycle path has ceased does not provide modal shift benefit.Optimisation – The path, though better signed, is not optimised.Safety – Whilst the safety issue is likely to be modest the poor clarity does create the risk of conflict occurring.Biodiversity – No significant biodiversity benefit. Leisure – The proposal links through to the coast to the east, but on its own is unlikely to have a significant leisure benefit.
Felixstowe	426	Walton Avenue (A154) between a point SE of Dooley Road NW towards Dock Gate 2 roundabout	For no apparent reason, the ability to cycle on the footway stops just short of Dooley Road (in front of Wincanton depot), along the frontage of China Shipping House, as far as just before Dock Gate 2 roundabout. No significant change in the width of the footway	Link up these two sections of cycleroute, to avoid having to cycle on the highway between these two points: Walton Avenue (A154) is heavily trafficked with HGVs and other Port related traffic (but very few pedestrians). This (and my other proposals) would lead to a continuous off-road cycleway all the way from the railway crossing at the NW end of Fagbury Road through to the former Beach Station and Beach Station road, around the busy environs of the Port.	3	3	3	2	0	0	11	Connectivity and Growth: Though this route currently has some cycling and walking infrastructure, the quality is generally poor and it is not continuous, meaning some cycling must either be on the carriageway or (illegally) on the footways. This route is likely to be used by any Port workers in central/east Felixstowe, and is therefore of high connectivity and growth (and modal shift) value. Modal Shift: See above. Full score of 3 given. Optimisation: See Connectivity and Growth - existing infrastructure optimised. Safety: Full segregation apart from crossing over the Dock Gate 1 roundabout arms when heading east. Score of 2 given. Biodiversity: Some greenspace (green verges) lost, however these appear highly managed in an urban environment. Leisure: No anticipated leisure value.
Felixstowe	437	Area bounded by Candlet Rd, Gulpher Rd, The Grove	This area is the subject of a major planning application for 560 houses, ref DC/20/1002/ARM, containing significant walking & cycling proposals Although the formal comment period for that is closed, those interested in this area may wish to look at that for information, and possibly also add a comment there.	All Walking and cycling matters in this area and those to West and East planned for development in the East Suffolk Local Plan should be considered in the context of the entire area.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. The strategy does consider new development in making its recommendations.
Felixstowe	438	Area bounded by Candlet Rd, Gulpher Rd and approximately the track to Candlet Farm	This area is the subject of major proposals for development of housing and a sports centre in the East Suffolk Local Plan containing significant walking & cycling proposals. Those interested in this area may wish to look at that for information. The relevant policy is at pages 215-221.	All Walking and cycling matters in this area and those to the East planned for development in the East Suffolk Local Plan should be considered in the future context of the entire area.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.

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Felixstowe	440	Area bounded by Links Avenue, Upperfield Drive, Ferry Rd, Gulpher Rd to The Grove	This area is the subject of major proposals for development of housing in the East Suffolk Local Plan containing significant walking & cycling proposals. Those interested in this area may wish to look at that for information. The relevant policy is at pages 215-221.	All walking and cycling matters in this area and those to the East planned for development in the East Suffolk Local Plan should be considered in the future context of the entire area							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Felixstowe	443	Mill Lane into town centre - lack or safe cycle route	there is no safe cycle route into town centre from the Coronation Park / Wesel Ave / Grange Road area - one of the most deprived areas of the town. Such areas have been shown to have far lower than average access to a private car. e.g. in poorest areas of Lowestoft up to 48% of households have no access to private car (2011 Census.)	create / build a high quality cycle route connecting Grange Farm / Coronation park area to town centre, potentially via Mill Lane. Due to the high prevalence of on-road parking on Mill Lane, it may be necessary to utilise the existing pavement(s) to allow shared or dual use between cyclists and pedestrians.	0	2	2	2	0	0	6	Connectivity and Growth: Painted cycle lanes exist, so cannot be scored under this category. Modal Shift: PCT shows some moderate and high scores for improvements to Mill Lane and Grange Road, so a score of 2 is given. Optimisation: Full score cannot be given due to interruptions necessary for some level of on-street parking. Safety: As above. Biodiversity: Little to no impact. Leisure: Little to no impact.
Felixstowe	547	Mill Lane railway bridge	The carriageway here is restricted to a single lane where traffic heading east has priority over traffic heading west. However many westbound motorists do not give way to eastbound cyclists when the cyclist has priority and this has the potential for head-on collisions, I personally find this junction scary to approach on a cycle with the right of way as you never know if the oncoming motorist will or will not respect your right of way.	Signage facing west-bound traffic reminding them of the need to give way to oncoming cyclists.	0	0	0	2	0	0	2	Connectivity and Growth: No impact. Modal Shift: No impact. Optimisation: No impact. S: Score of 2 given as this suggestion is likely to reduce the likelihood of accidents, providing the signage does not create "signage overload", which leads to signage being ignored (there appears to be two signs there already). Total signage in the area may need to be reviewed to optimise the desired effect. Biodiversity: No impact. Leisure: No impact.
Felixstowe	549	South Hill, Felixstowe	Due to parking of cars on both sides the width of carriageway available on South Hill is limited and it is not possible for a car to pass a cyclist safely, and many motorists especially those descending refuse to slow down or wait for cyclists and pass dangerously, there is the risk that a speeding motorist coming down the hill will have a head on collision with a cyclist climbing the hill.	Make South Hill one way for motor vehicles, I suggest this should be uphill only (and retain two-way passage for cyclists) reflecting the solution arrived at for Bent Hill several years ago as a response to a serious accident. Convalescent Hill is the only one of the three roads ascending the cliff in this area between Sea Road and the Spa Pavilion that is suitable for through motorised traffic.	0	2	1	2	0	0	5	Connectivity and Growth: No Connectivity and Growth value. Modal Shift: PCT shows a moderate uplift potential for South Hill if good improvements on Princes Hill can be achieved. This assumes more than modal filtering. Score of 2 given. Optimisation: Score of 1 given for the modal filter. Safety: Score of 2 given for modal filter at the top of South Hill so vehicles can only travel up the hill (i.e. make 'one way'). Biodiversity: No foreseen biodiversity impact. Leisure: Moderate leisure benefit due to access to the leisure centre and coast line.
Felixstowe	605	Colneis Road from Ferry Road to Beatrice Avenue	Parked cars on both sides, especially near Kingsfleet and Colneis schools, also high speed of traffic at any time. Children, from the expanding Laureate Fields development, will be in danger when cycling to the Academy.	Mandatory cycle lanes would improve safety for all cyclists.	1	1	2	2	0	0	6	Connectivity and Growth: Improvements to Colneis Road are relevant to C&G due to the growth planned around Ferry Road and in the NFGN. However, the NFGN development is intended to include a total of 630 primary school spaces and early years provision, so the benefit of the connection is likely to be limited for the future NFGN community. Modal Shift: PCT data suggests a significant but not high increase in cycling for school travel along Colneis Road.
Felixstowe	611	Langer road safety issues for cyclists and pedestrians alike	Langer rd is a straight length of road with a primary school & playgroup located on it. Due to the lack of any speed restrictions, traffic calming or cycle lanes, children cycling to school & workers cycling to & from work at the port are at risk on this road. Drivers consistently speed leaving cyclists at risk and forced to use the paths. The schools lollipop lady is in constant fear of speeding motorists. Residents are blighted by speeding cars & children travelling to school will be hurt.	The road should be a 20's plenty as a minimum!!! The safety of the children is most at risk. Most schools have this measure but Langer Academy on Langer road does not. Speed cameras should be installed or police monitoring increased. One side of the road has a wider pedestrian path than the other. It should become a mixed cycle/pedestrian path to aid children in their travel to school, this could then should be connected to Langer park's path via marina gardens to encourage walkers and cyclists to stay away from the dangerous roads and use the facilities on Langer park which is looking to be improved by the council.	3	2	0	3	0	0	8	Connectivity and Growth: A score of 3 is given as this is a central location without any infrastructure that serves as a route towards employment sites (particularly The Port) and a primary school. Modal Shift: High PCT uplift, however without a segregated cycle lane as well, the shared path may not create high levels of modal shift - the average commuter cyclist would prefer a segregated cycle lane over a shared path so that they can travel faster with lower potential conflict with pedestrians than on a shared path, even when generous in width and internally segregated. Score of 2 given. Optimisation: Optimisation score is 0 as no infrastructure for cycling currently exists on Langer Road. Safety: Full score of 3. Biodiversity: No foreseen biodiversity impact. Leisure: Not considered a leisure route on its own, though may be used as an alternative route to Sea Road, which is not set to be improved beyond more cycle parking added and improved crossing points for pedestrians.
Felixstowe	612	Felixstowe Promenade	Lack of continuation of cycling and walkway connecting Felixstowe to Old Felixstowe	The promenade should be continued for the full length of the coast line between Felixstowe and old Felixstowe encouraging runners Walker and cyclists.	2	1	1	3	-1	3	9	Connectivity and Growth: Connecting up the Promenade would be ideal, though expensive, and would likely incur the need to incorporate coastal defence infrastructure into the design. If fundable, a fully connected, uninterrupted, traffic-free and cycle-able sea-front route between Felixstowe Ferry and Martello Park would be an excellent leisure and tourism asset. Currently Felixstowe Ferry is not safely accessible by cyclists, as Footpath 62 obviously excludes cycling and Ferry Road is known for vehicle speeding and poor visibility. The necessary scheme to achieve this - which would need to upgrade and surface Footpath 62 at least/or achieve the equivalent - would therefore have high connectivity value. However, Felixstowe Ferry has a small population, and the route would predominately be of leisure value, so score is adjusted to 2. Modal Shift: As this would be principally a leisure route, and the population of Felixstowe Ferry is quite small, a modal shift score of 1 is given. Optimisation: Score of 2 given for the improvements to the existing sections, which in places have poor surfacing, though are already segregated from vehicles. Score of 0 given for entirely new sections. 1 overall. Safety: Score of 3 given as the route is full segregated from vehicles throughout its length. Biodiversity: A cautious score of -1 is given for biodiversity, as the biodiversity impacts of creating a new section of sea wall where none currently exists are unknown. Leisure: Full score for leisure.
Felixstowe	651	from the Dip northwards to Felixstowe Ferry along sea wall/ promenade	to be consistent with Prom south of Cobbolds Point, allow cycling access as shared use with pedestrians along prom/sea wall north off Dip. This will mean children / families won't have to use fast section of Ferry Rd through golf course if they wish to get to Fx Ferry - a popular spot for families. Also, Fx Ferry as a dead end, has a traffic and parking congestion problem, which improved cycle access to the hamlet would help mitigate.	Give permission for considerate cycling, while maintaining pedestrian priority. Narrow stretch near Cliff car park may need widening or signs for cyclists to dismount for this short stretch.	0	0	1	3	0	2	6	As noted by the respondent, this is a popular location for leisure cycling, as can be seen clearly in StravaMetro data between June 2019-2021. Ferry Road is more popular to date, though this is suspected to be because cycling is prohibited and cycling is awkward along the off-road 'Dip' to Felixstowe Ferry section at present; Ferry Road is quite unsuitable for family cycling at least at present, due to its narrowness and relatively heavy use. The 'Dip to Felixstowe Ferry' section needs upgrading and improving (widening, proper surfacing and 'shared path' signage) for access and usability. Connectivity and Growth: 0 - Not a connectivity route. Modal Shift: 0 Optimisation: 1 - Unlikely to cause a significant uplift in commuter cycling,

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												though may support greater leisure engagement, as it creates a totally segregated routes section that is suitable for short distance cycling within a much larger leisure route (leisure circular - yellow line on map). Safety: Safer than Ferry Road as it is totally segregated from traffic. As a bonus, it also moves the cyclist away from the golf course, which Ferry Road careers through. Safety: 3 - Full score given for safety as it completely segregates cyclists from cars for the full length between The Dip and Felixstowe Ferry. Leisure: 3 - A score of 2 was given as, though it plugs into a larger leisure route, on its own its limited in length and therefore meets only a sub-set of users' needs.
Felixstowe	683	North of Felixstowe	See attached.	See attached.	1	0	0	0	-1	1	1	Connectivity and Growth: Suggestions included in the respondents plan that differ from existing intentions are minor in added connectivity benefit overall in Felixstowe but does have some connectivity benefit locally Modal Shift: 0 Optimisation: 1 Safety: 0 Biodiversity: -1 as rural routes used Leisure: 1 as creates greater connectivity for leisure cycling
Felixstowe	689	Felixstowe	See attached.		0	0	0	0	0	0	N/A	Proposals made by made by Felixstowe Town Council have been separated out.
Felixstowe	691	Foxgrove Lane / High Rd (Walking)	Poorly signposted, heavily overgrown, poor surface		0	0	1	0	0	0	1	Connectivity & Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Score of 1 given for the improvement in legibility/wayfinding. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	693	Brook Lane / Park Avenue (Walking)	Signposting, maintenance		0	0	1	0	0	1	2	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Signage proves a low uplift on optimisation of a route. Score of 1 given. Safety: No added safety benefit arising from addition or improvement of signage in this location. Biodiversity: No anticipated biodiversity benefit. Leisure: Low uplift in leisure by directing cyclists/pedestrians towards the coast.
Felixstowe	696	Church Rd / St. Georges Rd / Western Ave / Roman Way / Cliff Rd (Walking)	Signposting, maintenance (not bad) Is this cable of upgrading to Cycle Route?		1	0	1	0	0	0	2	Connectivity and Growth: Low uplift in connectivity through to the coast. Modal Shift: No anticipated modal shift benefit. Optimisation: Low score for optimisation as it utilises an existing footpath. Safety: No uplift in safety anticipated. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	697	Martello Lane / beach (Walking)	Signposting, maintenance		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	699	Quintons Lane Ferndown Rd / Colneis Rd (Walking)	Poorly signposted, heavily overgrown, poor surface capable of upgrade to cycling?		1	0	2	1	-1	1	4	Connectivity and Growth: A useful connection between Colneis Road and the High Road, particularly for access to Colneis Junior School. Modal Shift: No PCT data as off-carriageway; Strava Metro data shows some but low usage, which may be mostly attributable to the issues identified in the comment - overgrown and unsurfaced - and probably unlit too, rather than through lack of demand for a connection between Colneis Road and High Road. Optimisation: A score of 2 is given due to the fact the bridleway is already fully segregated, but surfacing and clearing overgrowth would make it considerably more useable. Safety: A moderate increase in safety from its current status as unsurfaced - particularly if redesign also includes appropriate lighting of the route. Biodiversity: Likely to be at least a moderate reduction in biodiversity value of the route due to the necessary cutting back of overgrowth/bound surfacing over earth. Leisure: Unlikely to be of leisure value most of the time (main function likely be would be for school travel and access to the High Road for onward commuter travel) though may be used as a connection down through Brackenbury Sports Centre site towards the coastline.
Felixstowe	700	Ferndown Rd / Gosford Way (Walking)	Poorly signposted, heavily overgrown, poor surface capable of upgrade to cycling?		1	0	1	0	0	1	3	Connectivity and Growth: No effect. Modal Shift: No PCT data as off-carriageway; Strava Metro data shows some but low usage, which may be mostly attributable to the issues identified in the comment - overgrown and unsurfaced - and probably unlit too, rather than through lack of demand for a connection between Colneis Road and High Road. Optimisation: Signposting represents only a modest uplift in overall quality. Safety: Signposting does not increase safety in this instance. Biodiversity: No anticipated biodiversity impact. Leisure: There may be a mild leisure uplift in adding signage at the Colneis Road end towards the coastline
Felixstowe	701	York Rd / rear St. Felix Church (Walking)	Signposting, maintenance		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	702	Ranelagh Rd Car Park to Spa Pavilion & Garden via steps on Hamilton Gardens (Walking)	Signposting		0	0	0	0	0	1	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.

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Felixstowe	703	Garrison Lane roundabout to Coronation Drive via Railway bridge (Walking)	Signposting, maintenance		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	704	Beach Station Rd through Langer Park (Walking)	Signposting, significant enhancement		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	705	Peewit Hill (Walking)	Poorly signposted, heavily overgrown, poor surface capable of upgrade? Ownership issues?		1	2	1	1	0	0	5	Connectivity and Growth: A score of 1 is given as Peewit Hill is an important connection from Dock Gate 1 roundabout (for The Port) and Grange Road, which (once improved to have shared paths, particularly) acts as a spinal route through west Felixstowe up to Maidstone Road for access to the High Road. Modal Shift: Strava Metro shows clear and defined usage of Peewit Hill to transfer between Grange Road and Walton Avenue (via Dock Gate 1 roundabout). Combined with infrastructure for onward travel north or south, Peewit Hill has moderate/high modal shift value. Optimisation: Peewit Hill is already modal filtered, and therefore segregated, though the surfacing is poor. Low optimisation uplift from resurfacing. Safety: Low safety uplift from resurfacing. Biodiversity: No anticipated biodiversity impact. Leisure: No anticipated leisure impact.
Felixstowe	706	Footpath 41 Haven Exchange to Coronation Drive	Signposting, maintenance Was closed due to slippage. What is current status?		0	0	1	1	0	0	2	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Resurfacing would improve the quality of the route. Score of 1 given. Safety: Moderate safety uplift. Biodiversity: No anticipated biodiversity benefit. Leisure: No anticipated leisure uplift.
Felixstowe	707	Footpath xx Philip Avenue to Coronation Drive	Was closed due to slippage. What is current status?		0	0	1	1	0	0	2	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Resurfacing would improve the quality of the route. Score of 1 given. Safety: Moderate safety uplift. Biodiversity: No anticipated biodiversity benefit. Leisure: No anticipated leisure uplift.
Felixstowe	708	Elmcroft Lane / Colneis Rd / Westmorland Rd x2, Ferry Rd	"No Cycling" sign at Westmorland Rd? No Cycling sign near Whinyard Way. Overgrown, part poor surface. This could surely be a Cycle Route?	See attached map - references W6B	0	0	1	1	-1	0	1	Connectivity and Growth: There would be a slight connectivity and growth benefit arising from upgrading and surfacing the full length of Footpath 8 by making it more accessible for pedestrians, however as the eastern half of the footpath is realistically too narrow for cycling this negates its overall value. Score of 0 given. Modal shift: No foreseen modal shift value. O: Due to the narrowness of the route, the value to cyclists is minimal, and the footpath is useable as it is for pedestrians now, though accessibility would be improved. Overall score of 1. S: Safety slightly increased from surfacing. Score of 1 is given. B: As this is currently a grass/earth route, there would be a biodiversity impact of surfacing it. L: No anticipated leisure uplift.
Felixstowe	709	Elmcroft Lane Western Ave (Walking)	Poorly signposted, heavily overgrown, poor surface Is this cable of upgrading to Cycle Route?		0	0	1	1	-1	0	1	Connectivity and Growth: There would be a slight connectivity and growth benefit arising from upgrading and surfacing the full length of Footpath 8 by making it more accessible for pedestrians, however as the eastern half of the footpath is realistically too narrow for cycling this negates its overall value. Score of 0 given. Modal shift: No foreseen modal shift value. O: Due to the narrowness of the route, the value to cyclists is minimal, and the footpath is useable as it is for pedestrians now, though accessibility would be improved. Overall score of 1. S: Safety slightly increased from surfacing. Score of 1 is given. B: As this is currently a grass/earth route, there would be a biodiversity impact of surfacing it. L: No anticipated leisure uplift.
Felixstowe	710	High Row Field / High Road (Walking)	Status? Created as part of High Row Field development. Signposting, maintenance. NB reference effects of potential redevelopment of Brackenbury Sports Centre site.		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. No anticipated safety benefit. Safety: No anticipated safety benefit. B: No anticipated biodiversity benefits. L: No anticipated uplift in leisure.
Felixstowe	711	College Green / Maybush Lane (Walking)	Status? Created as part of College development. Signposting, maintenance. Ownership & rights complex. Reference correspondence about Planning Application DC/20/4188/FUL		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	712	Quintons Lane Sunray Ave / Links Ave (Cycling)	Signposting, maintenance		0	0	0	0	0	1	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	713	Left turn Chaucer Rd (Cycling)	A short cycle track, c. 3m length across the Chaucer Rd island would allow cyclists to turn left when travelling North West, to access Mill Lane rail bridge, avoiding heavy traffic on Garrison Lane, and traffic lights at Mill Lane junction.		0	2	1	1	0	0	4	Connectivity and Growth: No significant benefit - mainly a minor opportunity to increase permeability and get cyclists heading north on Garrison Lane 'south' off Garrison Lane 'south' earlier so they can avoid the Mill Lane/Garrison Lane crossroads, which is not currently suitable for cyclists. Modal Shift: PCT identifies moderate modal shift value, suggesting the Mill Lane/Garrison Lane crossroads may be actively avoided by cyclists. Strava Metro shows average use of Chaucer Lane and heavy use of Garrison Lane, which may be more reflective of Chaucer Lane being 'no entry' at the Garrison Lane end, which may be where it would otherwise be more useful for ingress by cyclists if they were allowed. Score of 2 given. Optimisation: Optimisation score of 1 given under both optimisation and safety categories on the basis of extra permeability for cyclists being given by making it only 'one way' for vehicles. Safety: From a safety point of view, it would need to be designed and confirmed that it would not actually reduce cyclists' safety using this diversion, which is a high risk with any contraflow cycling infrastructure. Biodiversity: No anticipated biodiversity value. Leisure: No anticipated leisure value.

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Felixstowe	714		Open connection beneath Leisure Centre walkway to promenade between Pier Bight Car Park existing route and the Events Area (Cycling)	Although not obvious, careful informal survey appears to indicate this is feasible. Would need negotiation with Leisure Centre operator. Previously identified by SCC 2015. Also a good principle to establish ahead of potential future development of Leisure Centre site.	0	0	0	-1	0	0	-1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: No optimisation benefit anticipated from the proposal. Safety: May present a safety risk to re-open this walkway, presumably this has been locked for a reason. Cautious -1 given. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Felixstowe	715	Exit Martello Park to Manor Terrace - See also map Cycle Route 51B & Insets	Track ends at boundary of Martello Park development. Cycle Route 51 continues onto Manor Terrace to Landguard via the Car Park. The large area of unmade ground is without known ownership.	This needs to be researched again (ESC did some work c . 1999 as part of South Sea Front project) and ESC should seek to claim it, as was done recently nearby on corner of Manor Road & Terrace. Could then serve as Cycling and Walking Route, and also possibly additional residents parking for Manor Terrace properties, frequently requested. But it is also a critical access route for both ESC and EA for plant access to 2 vehicular flood gates for flood defence maintenance. Protection is believed to be formalised for EA by flood defence regulations. Layout must recognise that. NB the land cannot be built on, for that reason.	0	1	1	1	0	1	4	Connectivity and Growth: Although this road (Orford Road) is in relatively poor surfacing condition, it is still useable and alternatives to its use exist for connections between Langer Road and Sea Road/the Promenade. Score of 0 given. Modal Shift: Modal shift potential is 0 in PCT. Orford Road does appear to have slightly higher activity than Beach Station Road and the other connecting roads, however this is likely to be connected to leisure trips to and from the coast line rather than commuter/school trips/utility trips. Score of 1 given. Optimisation: Score of 1 for optimisation and safety given for resurfacing. Biodiversity: No anticipated biodiversity value. Leisure: No anticipated leisure value.
Felixstowe	716	New recreational Cycle Route The Dip to Felixstowe Ferry	Enable cycling on: A) ESC Coast defence "promenade". B) Environment Agency sea wall adjacent golf course Would require permissions ESC, EA And Golf Club? (own the land on which sea wall is built?) Some improved surfacing required at northern end.	See attached map - references C21A	2	1	1	3	-1	3	9	Connectivity and Growth: Connecting up the Dip and Felixstowe Ferry would be ideal. Currently Felixstowe Ferry is not safely accessible by cyclists, as Footpath 62 obviously excludes cycling and Ferry Road is known for vehicle speeding and poor visibility. The necessary scheme to achieve this - which would need to upgrade and surface Footpath 62 at least/or achieve the equivalent - would therefore have high connectivity value. However, Felixstowe Ferry has a small population, and the route would predominately be of leisure value, so score is adjusted to 2. Modal Shift: As this would be principally a leisure route, and the population of Felixstowe Ferry is quite small, a modal shift score of 1 is given. Optimisation: Score of 2 given for the improvements to the existing sections, which in places have poor surfacing, though are already segregated from vehicles. Score of 0 given for entirely new sections. 1 overall. Safety: Score of 3 given as the route is full segregated from vehicles throughout its length. Biodiversity: A cautious score of -1 is given for biodiversity, as the biodiversity impacts of creating a new section of sea wall where none currently exists are unknown. Leisure: Full score for leisure.
Felixstowe	717	Hawkes Lane / footpath to Maidstone Rd & Runnacles Way via railway foot bridge	The short stretch of Hawkes Lane between High Street and the school entrance road, and its continuation as a footpath along the West and South of the school site to the new railway bridge and beyond is poorly maintained, partially overgrown and has negligible signage.	If also upgraded for cycling use, it could constitute a significant cross town route to the Orwell Green area, the port area and towards Trimley and Ipswich via the A14 footbridge. Additionally it would link with access across the forthcoming Walton North development to Candlet Road, and then to the North Felixstowe Garden Village and the countryside beyond as a major strategic cycle route, potentially from the Deben to the Orwell estuaries. It should also be made accessible directly from the South Eastern corner of the new Walton Hall Drive, giving access from that estate to the south and west as above. (See attached map - references C23B)	3	3	1	2	0	0	9	Connectivity and Growth: A new continuous route from the core of the NFGN through the Land North of Walton High Road, down Hawkes Lane and around the school site to Maidstone Road and the footbridge over to Felixstowe West and into the Port has been included in the Strategy. It will be of high value to future residents of the NFGN for access to Felixstowe Academy, and potentially also school children coming from Kirton via what is currently (to be improved) Candlet Track. Score of 3 is given. Modal Shift: No PCT data, based on judgement. NFGN-based school children being able to walk or cycle to Felixstowe Academy safely via well-designed schemes will make a significant difference to vehicular movements into and around the school. Being able to cycle directly to the Port via the Hawkes Lane footbridge will also provide an opportunity for an uplift in commuting. The Land North of Walton High Road site will hopefully - via a new crossing over Candlet Road and Treetops/Gulpher Road - connect directly into a new bi-directional cycle track that will run parallel to Candlet Road up to The Grove, after which new cycle infrastructure down Garrison Lane (bi-directional track) or Beatrice Avenue (modal filtered on-road) will transfer them to Hamilton Road, the Primary Shopping Area for employment and retail goods and services access. In conjunction with these other routes, the north-south route between Maidstone Road and Candlet Road via Felixstowe Academy and the Land North of Walton High Road site have the potential to create significant modal shift to cycling and walking. Optimisation: Score of 1 given as, though the section through the Land North of Walton High Street site is new, improvements to the existing Hawkes Lane route and railway bridge are included. The respondent is right to highlight how critical maintenance of this route will be to its success, particularly given the green edges around Hawkes Lane that could quickly become overgrown and inaccessible in the summer months. Safety: 2 given as not all of the route is segregated from vehicles due to the need to cross the High Road. Biodiversity: 0, no anticipated significant affects. Leisure: Intended for commuting/school journeys.
Felixstowe	722	Proposed Felixstowe Garden Village Areas	There should be a shared walkway and cycleway connecting all the proposed Felixstowe Garden Village Areas and linking into the town. Kesgrave is an excellent example of what can be achieved. These routes should be wide, well lit, welcoming.		2	2	0	3	-1	1	7	Connectivity and Growth: A basic grid (following existing PROW routes where these occur) to demonstrate how the NFGN should be internally connected through cycling and walking tracks/paths (full segregated wherever possible) has been indicated on the Strategy map. Ideally a more extensive network will be delivered, aligned to anticipated desire lines for onward travel, though maintaining separation from vehicles. However, these will not score highly in the connectivity and growth section as they relate to internal permeability within the overall development rather than connecting different settlements, which score the highest scores. Score of 2 is given. Modal Shift: No PCT data as routes don't exist, but modal shift is likely to have a moderate boost from the routes due to their capacity to connect (a) homes with routes for onward travel from the NFGN to their place of work/education, and (b) homes with employment/services (primary school, new leisure centre etc.) within the site. Score of 2 given. Optimisation: Entirely new infrastructure so cannot be scored under this category. Safety: Full score as full segregation anticipated. Biodiversity: -1 due to loss of former farm land in their creation. Leisure: 1 has been given as not intended for leisure purposes, though the increased permeability will allow for very local cycle trips (e.g. children playing on bikes within the NFGN) and alternatives to

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												use of Gulpher Road for through-trips (East to west most likely, towards the coast line).
Felixstowe	723	The Grove and Abbey Grove	Access to The Grove and Abbey Grove needs to have kissing gates to prevent cycling. Mountain bikes would soon ruin the pathways for walking.		-1	0	-1	0	0	0	-2	The installation of gates was suggested to avoid destruction of the path by cyclists. Connectivity and Growth – The site is positioned on the north edge and does not directly connect to any key services currently. However, it does sit between the proposed North Felixstowe Garden Village allocation and the rest of the town. Removing cycling rights would remove a potential connection between the two, though this will not be the only point of connection so will have a limited impact; a score of -1 was given. Policy SCLP12.3: North Felixstowe Garden Neighbourhood protects Grove Woodland and Eastward Ho, and requires the creation of a network of pedestrian, cycling and vehicular routes that provide connectivity and movement across the Garden Neighbourhood (and with adjacent areas). Paragraph 12.50 also specifically supports off-road cycle routes ("opportunities to provide off road cycle paths are encouraged to increase recreational opportunities for active lifestyles as well as making provision to access employment sites...through sustainable forms of travel"). The installation of kissing gates to block a key entry point into the site, as indicated by the placement of the response on the consultation map, would be incongruent with the policy requirements. If the issue is the degradability of the current surfacing of the paths, rather than the principle of cycling in this area, then resurfacing is a more reasonable and policy compliant response. Modal Shift – Once the NFGN site is developed, there will be multiple entry/exit points, and therefore the closure of this access point - though inconvenient - is unlikely to create modal shift away from sustainable modes of travel; commuters will use a different access point to exit the site, without much added journey time. Optimisation – Reduces the usability of existing infrastructure, and therefore a minus score is provided. Safety – The damage to the pathway is not considered to create a significant safety issue. Biodiversity – The pathway is unlikely to have any significant biodiversity value. Leisure – Whilst removing cyclists may benefit pedestrians on this cycle route, the cyclists themselves would be adversely affected meaning a neutral score was given.
Felixstowe	724	Beatrice Ave, Beatrice/Colneis roundabout and The High Rd/Beatrice Ave/Hamilton Rd roundabout	A safe cycle way along Beatrice Ave is essential. The cycle way must not push cyclists into riding on the camber of the road as is often the case. There also needs to be a safe cycle route around the Beatrice/Colneis roundabout and The High Rd/Beatrice Ave/Hamilton Rd roundabout.		1	0	1	1	0	0	3	Connectivity and Growth: A score of 1 is given for the only reason that Beatrice Avenue is a more straightforward choice than Garrison Lane, particularly from the eastern side of the NFGN for accessing Hamilton Road by bike; improvements to it are therefore significant for overall connectivity and growth. A higher score is not given as it is currently possible to cycle down Beatrice Avenue without significant risk due to relatively low traffic movements for the location, excellent visibility and minimal on-street parking. Modal filtering will help to lower traffic movements further by precluding access to Hamilton Road to the south (i.e. prevents through traffic). Modal Shift: PCT shows no uplift in commuter cycling at 'Gender Equality' standards, which modal filtering at one end is considered to achieve (at most). Score of 0 given.
Felixstowe	725	Hamilton Rd junctions with St Andrews Rd, Cobbold Rd, Orwell Rd and then across Hamilton Gardens and into Bent Hill	A safe crossing with priority for cyclists should be available at Hamilton Rd junctions with St Andrews Rd, Cobbold Rd, Orwell Rd and then across Hamilton Gardens and into Bent Hill. This will be a safe route from Garden Village to the prom. Cyclists can then cycle along the prom to pier and Landguard area.		0	0	2	1	0	0	3	Connectivity and Growth: A set of co-ordinated priority crossings along the full length of this route would not add any additional connectivity and growth. Modal Shift: Priority crossings on their own are unlikely to cause modal shift. Optimisation: A score of 2 for optimisation is given on the basis of creating, in total, a cycle-priority on-carriageway route. However, it still lacks the high scoring element of segregation. Safety: Assuming they are well designed, cycle priority crossings should provide a slight uplift in safety for cyclists and pedestrians. Biodiversity: No anticipated biodiversity value. Leisure: No anticipated leisure value.
Felixstowe	726	Beatrice Ave/Colnies roundabout to Taunton Rd	A high standard cycle path on the verge from Beatrice Ave/Colnies roundabout to Taunton Rd and into Ataka and then Gulper would work very well.		3	3	0	2	-1	3	10	Connectivity and Growth: A cycle/pedestrian parallel to Candlet Road, irrespective of side (adequate crossings/joining points from either side would need to be included), will be critical for east to west movement across the NFGN and accessing the NFGN from the south (e.g. from Garrison Lane/Grove Road/Spriteshall Lane). Modal Shift: As above - as the NFGN is an entirely new community and this scheme would predominantly serve their needs, the Modal Shift score is estimated based on the uplift in the level of cycling anticipated with it compared to without it. Optimisation: New infrastructure so not scored under this category. Safety: Intended to be full segregated from vehicles, though with some inevitable crossing points of vehicle accesses. Score of 2 given. Biodiversity: Negative biodiversity score due to loss of mature trees, however it is intended that over the long term these trees would be replaced on the NFGN site. Leisure: High leisure value, particularly for older children and young people that might enjoy playing on the track as a safe space from vehicles.
Felixstowe	728	Cycle ways in Felixstowe	Many of the so called cycle ways in Fx are too narrow given the road camber and gutter to make for safe and comfortable cycling. Too many allow cars to park in them. Cars expect cyclists to be in the lanes when they are unsuitable.	Maybe the pavement on one side of the road should be a cycle way. Again Kesgrave is very good in this respect. These lanes need to be kept clear of grit and debris that cars push into them. Better signage needed for cyclists and cars. Thought needs to be given at junctions.	0	0	0	0	0	0	0	Garrison Lane in an A Road, and therefore the carriageway needs to be consistently wide enough to accommodate HGVs when the A14 is out of action. This does not leave much space for outward expansion to accommodate an LTN 1/20 compliant shared path, which at an absolute minimum would need to be 2m wide, which is not accommodatable. Garrison Lane's east side (which, from Google Maps, appears to be wider and flatter overall than the west side) pavement averages approximately 1.25m in width, and features poor junctions for pedestrian/cyclists to cross. Though there are sections with absorbable central reservation/turning boxes that could be removed, there isn't sufficient carriageway space consistently to create a consistent shared path. The suggestion must unfortunately therefore be 0 scored across the categories. Garrison Lane also has residential development with off-street parking along both sides throughout, which necessitates a high number of drop kerbs, which would make for a less than smooth cycling experience even if deliverable. Moreover, even if there was enough space to expand to the 2m minimum on the east side, this route is of strategic importance between the Trimleys/Felixstowe west and the Port,

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												meaning a shared path is an undesirable solution in this location, anyway (LTN 1/20 discourages conversion to shared paths, stating "conversion of existing footways to shared use should only be considered when options that reuse carriageway or other, e.g. verge, space have been rejected as unworkable", p.42). Connectivity and Growth: 0 Modal Shift: 0 Optimisation: 0 Safety: 0 Biodiversity: 0 Leisure: 0
Felixstowe	729	Garrison Lane traffic lights	It is dangerous for a cyclist at Garrison Lane traffic lights if a vehicle behind at the lights turns left in front of the cyclists.	There needs to be a period during the light change that is for cyclists only. I realise this would make the lights even slower but if we want more cyclists on the road it is needed.	0	0	0	0	0	0	0	N/A - No traffic lights at this location could be identified. However, more generally, where traffic light controlled junctions and crossroads occur in Felixstowe, they should all be fitted with cycle lights that give on-road cyclists at least a 15 second head start over vehicles, particularly where advanced stop lines are used, so that cyclists have time to safely move from the primary position back into the secondary position safely.
Felixstowe	730	The prom and onto the Landguard Reserve	Cycling on the prom and onto the Landguard Reserve cycle way and onto the viewing area needs to be well signed and the surface maintained.		0	0	0	1	0	0	1	The surfacing in this location is uneven and coarse, so should be re-surfaced, mainly for safety reasons.
Felixstowe	731	Links Avenue and Upperfield Drive	Links Avenue and Upperfield Drive should become cycle ways.	Cars could be confined to Colneis Rd unless for access. This would aid pupils reaching Colneis and Kingsfleet Schools	0	0	0	0	0	0	0	Connectivity and Growth: No foreseen connectivity and growth benefits arising from modal filtering. Modal Shift: No anticipated modal shift benefit. Optimisation: No optimisation benefit anticipated from the proposal. If anything, this is likely to intensify congestion on Colneis Road, if residents were using Links Avenue and Upperfield Drive to park up for school drop off as an alternative to Colneis Road. Safety: No significant anticipated safety benefit. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Felixstowe	732	Quiet lanes	Quiet lanes should have enforceable restrictions placed on them. Motorists do not seem to take any notice in Gulpher Rd.	It needs a mandatory scheme. Many more warning cyclists signs would help, the flashing speedo signs are good. Maybe the tarmac could be a different colour. Could the roads be access only for vehicles to stop the joy riders.	0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because requests for Quiet Lane designations have been dealt with separately.
Felixstowe	758	Bent Hill, Felixstowe	Cyclist riding at speed down the middle of Bent Hill thus risking themselves, walkers and car drivers to injury. An accident waiting to happen (but should it wait?) Incidentally the same goes for skateboarders.		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Felixstowe	759	Hamilton Road shared space	Cycling one way, same as traffic, would help with safety of walkers especially the deaf and poor sighted. Cyclists/skateboarders play in this area.		0	0	-1	0	0	0	-1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: No optimisation benefit anticipated from the proposal. Limiting movement, particularly of pedestrians and cyclists, is contrary to the Shared Space concept and would therefore represent 'harm' to the existing scheme/space. Safety: No significant anticipated safety benefit. Also likely to be ignored as restricting movement of bikes (and pedestrians) is contrary to Shared Space core principles. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Felixstowe	547a	Mill Lane railway bridge	The carriageway here is restricted to a single lane where traffic heading east has priority over traffic heading west. However many westbound motorists do not give way to eastbound cyclists when the cyclist has priority and this has the potential for head-on collisions, I personally find this junction scary to approach on a cycle with the right of way as you never know if the oncoming motorist will or will not respect your right of way.	This is an alternative suggestions made by an East Suffolk Council officer consideration could be given to a shared path along north side of the road	3	3	0	1	0	0	7	Connectivity and Growth - A high priority route within a key corridor. Modal Shift - A quality improvement will have a significant modal shift growth in accordance with PCT. Optimisation - This represents new infrastructure. Safety - The road is a residential street at 30mph is would only represent a modest safety benefit. Biodiversity - No biodiversity impact. Leisure - Limited leisure impact.
Felixstowe	X1	ELMCROFT LANE /WESTMORLAND ROAD TO CLIFF ROAD	FOOTPATH 8 REPLACE STAGGERED BARRIERS WITH BOLLARD AND SIGN ROUTE.		0	0	1	1	-1	0	1	Connectivity and Growth: There would be a slight connectivity and growth benefit arising from upgrading and surfacing the full length of Footpath 8 by making it more accessible for pedestrians, however as the eastern half of the footpath is realistically too narrow for cycling this negates its overall value. Score of 0 given. Modal Shift: No foreseen modal shift value. Optimisation: Due to the narrowness of the route, the value to cyclists is minimal, and the footpath is useable as it is for pedestrians now, though accessibility would be improved. Overall score of 1. Safety: Safety slightly increased from surfacing. Score of 1 is given. Biodiversity: As this is currently a grass/earth route, there would be a biodiversity impact of surfacing it. Leisure: No anticipated leisure uplift.
Felixstowe	X10	FAIRFIELD AVE TO GARRISON LANE/HIGH ROAD WEST JUNCTION	CONVERT FOOTWAY TO CYCLE TRACK REMOVE HOOPED BARRIERS REPLACE WITH BOLLARDS		1	0	1	0	0	0	2	Connectivity and Growth: Score of 1 given in the context of the Strategy's recommendation to provide a bi-directional track up the north side of Garrison Lane; this improvement would increase permeability for use of the track. Modal Shift: Unlikely to have significant modal shift value on its own. Optimisation: Makes best use of existing footpath in the context of the bi-directional track. Safety: No added safety benefit. Biodiversity: No anticipated biodiversity value. Leisure: No anticipated leisure value.
Felixstowe	X11	WALTON AVE EAST	ET06181 EXTEND OFF ROAD CYCLE TRACK FROM CHURCH OF LATTER-DAY SAINTS TO LANGER ROAD		0	2	2	2	0	0	6	Connectivity and Growth: A shared path already exists in this location but it is relatively low quality, narrow and the junctions remain designed for cars' visibility splays, not safe crossing by cyclists/pedestrians. A score of 0 must therefore be given. Optimisation: Score of 3 cannot be given due to the amount of times the shared path is crossed by junctions/vehicles.
Felixstowe	X12	LANGER ROAD	SAFE ROUTE TO SCHOOL CONVERT WESTERN FOOTWAY BETWEEN WALTON AVE TO HOLLAND ROAD		3	2	0	3	0	0	8	Connectivity and Growth: A score of 3 is given as this is a central location without any infrastructure that serves as a route towards employment sites (particularly The Port) and a primary school. Modal Shift: High PCT uplift, however without a segregated cycle lane as well, the shared path may not create high levels of modal shift - the average commuter cyclist would prefer a segregated cycle lane over a shared path so that they can travel faster with lower threat of hitting pedestrians than on a shared

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												path, even when generous in width and internally segregated. Score of 2 given. Optimisation: Optimisation score is 0 as no infrastructure for cycling currently exists on Langer Road. Safety: Full score of 3. Biodiversity: No foreseen biodiversity impact. Leisure: Not considered a leisure route on its own, though may be used as an alternative route to Sea Road, which is not set to be improved beyond more cycle parking added and improved crossing points for pedestrians.
Felixstowe	X15	BEACH STATION ROAD	SIGN AS CYCLE ROUTE TO LANDGUARD & BEACH		0	0	1	0	0	1	2	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Signage proves a low uplift on optimisation of a route. Score of 1 given. Safety: No added safety benefit arising from addition or improvement of signage in this location. Biodiversity: No anticipated biodiversity benefit. Leisure: Low uplift in leisure by directing cyclists/pedestrians towards the coast.
Felixstowe	X16	GARRISON LANE	ADD ADVISORY CYCLE LANES BETWEEN ITS JUNCTION OF UNDERCLIFFE ROAD WEST AND HIGH ROAD WEST.		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be adequate to meet LTN 1/20 Cycle Infrastructure Design standards in this location.
Felixstowe	X17	CLIFF ROAD WEST	SIGN AS CYCLE ROUTE TO PIER		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X18	PRINCES ROAD/ SOUTH HILL	SIGN AS CYCLE ROUTE TO PIER		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X19	CRESCENT ROAD	BETWEEN GARRISON LANE & COBBOLD ROAD EXISTING SIGNED AS NCR51 ADD ADVISORY CYCLE LANES		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be adequate to meet LTN 1/20 Cycle Infrastructure Design standards in this location.
Felixstowe	X2	LOCAL ROUTE 1 COLNEIS ROAD	ADD ADVISORY CYCLE LANES BETWEEN JUNCTION OF CHURCH ROAD AND BEATRICE AVE		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be likely to achieve an uplift across any of the MCAF categories, particularly safety.
Felixstowe	X20	PRIORY ROAD	BETWEEN HIGH ROAD WEST & GOLF ROAD SIGN AS CYCLE ROUTE		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X21	CARR ROAD	BETWEEN BEACH STATION ROAD & DOCK GATES SIGN AS LOCAL CYCLE ROUTE		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X22	ORFORD ROAD	BETWEEN CARR ROAD & SEA ROAD REMOVE NCN SIGN REPLACE WITH LOCAL ROUTE SIGNING		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X23	MANOR ROAD & MANOR TERRACE	REMOVE NCN SIGNAGE BETWEEN CARR ROAD WORK ITEM 13		0	0	0	0	0	0	0	Community and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: No anticipated benefit arising from removal of this signage without re-routing. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X24	MANOR ROAD CAR PARK	ADD CYCLE LOGS (1057) TO HIGHLIGHT ROUTE THROUGH CAR PARK		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X25	COBBOLD ROAD	SIGN AS LOCAL CYCLE ROUTE & ADD CYCLE LOGO 1057		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X26	HIGH ROAD WEST (HOWLETT WAY RNDBT)	EXISTING FACILITY, AT START REQUIRES CYCLISTS DIRECTION ARROW FROM HIGH RD TO FACILITY REQUIRES DROP KERB AND MARKING TO REJOIN HIGH RD ON WESTERN SIDE OF RNDBT		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X27	HIGH ROAD WEST (HOWLETT WAY RNDBT)	EXISTING FACILITY CYCLE TRACK REQUIRES BOLLARDS AND LINE GIVE WAY LINE MARKING.		0	0	0	1	0	0	1	Connectivity and Growth: Any significant improvements to High Road are going to score highly for connectivity and growth. However the proposal is for bollards (alone) and give way lines, which are not conducive to safer crossings in this location compared with the creation of circulatory shared paths and constructed priority crossings over each arm. Score of 0 given. Modal Shift: Any significant improvements to High Road are going to score highly for modal shift. However, bollards and give way lines are unlikely to be effective in creating modal shift. Score of 0 given. Optimisation: The current High Road roundabout arms/crossings over the Howlett Way arm is unnecessarily wide and the crossings could therefore be considerably 'pinched'. More generally, the shared paths around the circulation of the roundabout could also be considerably improved and priority crossings over each arm added.

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												Score of 0 given. Safety: Bollards and give way lines are unlikely to be effective in significantly improving cyclists and pedestrians, particularly as they are already quite well segregated by green verges/plantings as they circulate around the Howlett Way arm. Score of 1 given. Biodiversity: No anticipated biodiversity benefit. Leisure: No anticipated leisure benefit.
Felixstowe	X28	HIGH ROAD WEST	EXTEND ADVISORY CYCLE LANE THROUGH TRAFFIC ISLAND TOWARDS RDNBT TO START OF OFF ROAD CYCLE TRACK.		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be likely to achieve an uplift across any of the MCAF categories, particularly safety.
Felixstowe	X29	WALTON AVE EXTENSION WEST	ET06180		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Felixstowe	X3	CHURCH ROAD	SIGN ROUTE		0	0	0	0	0	0	0	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X31	GRANGE FARM AVENUE	WIDEN FOOTWAY TO EXTEND CYCLE FACILITY BETWEEN WESTLETON WAY TO THE RDNBT JUNCTION WITH WESSEL AVENUE		1	2	2	3	0	0	8	Modal shift: Moderate modal shift potential on Grange Road Avenue from a west side shared paths. Optimisation: There are existing 'patchy' and non-LTN 1/20 standard shared paths on the eastern side (other side) of Grange Farm Avenue which may be better off being downgraded to footpaths and a west-side entirely new path be added. Optimising this west side represents a moderate uplift in optimisation. The east side also has more junctions to cross, which could be mitigated with priority crossings and the restructuring of bell mouth junctions where they occur. Safety: Modest uplift in safety. Biodiversity: No significant biodiversity impact anticipated. Leisure: No anticipated leisure value - would likely only be commuting and utility trips in this location.
Felixstowe	X32	GRANGE FARM AVENUE & WESEL AVENUE	WIDEN FOOTWAY TO EXTEND CYCLE FACILITY ACROSS EASTERN ARM OF RDNBT TO MEET LOCAL ROUTE 6 CYCLE TRACK .		1	1	1	1	0	0	4	Connectivity and Growth: 1 as infrastructure of adequate (but not LTN 1/20 standards) already exists in this location. Modal Shift: PCT suggests a moderate uplift in modal shift, however, the data does not factor in the North Felixstowe Garden Neighbourhood development (expected to deliver 2,000 homes) and the increased importance of this route for access to the Port (particularly the operations based to the west of the Port) for those living in the central area wishing to walk or cycle to work. However, other options are available. Score of 1 given. Optimisation: Existing footpaths on eastern side would be improved, uplift of 1 scored. Safety: Slight uplift on current level of safety with improved paths and crossings. Score of 1 given. Biodiversity: No impact or benefit scored. Leisure: No anticipated leisure value uplift from current shared paths.
Felixstowe	X33	FERRY LANE	FROM END OF OFF ROAD CYCLE FACILITIES ADD ADVISORY CYCLE LANES TO HODGKINSON ROAD/DOOLEY INN PH		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be likely to achieve an uplift across any of the MCAF categories, particularly safety.
Felixstowe	X34	GRANGE FARM AVENUE	BETWEEN LANGLEY AVE & SUDBOURNE RD ADD CYCLE LOGOS AND ADVISORY CYCLE LANES THROUGH ISLAND PINCH POINTS		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be likely to achieve an uplift across any of the MCAF categories, particularly safety.
Felixstowe	X35	GRANGE FARM AVENUE (GFA)	AT CROSS ROADS FORMED BY BRACKLEY & POND CLOSE. TERMINATE CYCLE PATH AT POND CL ADD SPUR TO CROSS GFA WHERE BUILD OUT NARROWS ROAD. CONSTRUCT CYCLE BYPASS TOWARDS BRACKLEY CLOSE AND ADD CYCLE LANE ACROSS ITS MOUTH.		0	0	0	0	0	0	0	Connectivity and Growth: No foreseen connectivity and growth benefits. Likely to have a very low benefit to cost ratio in isolation; Strategy recommends a more comprehensive shared path scheme to run along one side of the full length of Grange Farm Avenue/Wesel Avenue to (at least) Ferry Lane. Modal Shift: No anticipated modal shift benefit. Optimisation: No optimisation benefit anticipated from the proposal. Likely to have a very low benefit to cost ratio done in isolation. Safety: No significant anticipated safety benefit. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Felixstowe	X36	GRANGE FARM AVENUE	EXISTING CYCLE FACILITY ADD GIVE WAYS & SIGNS - DO WHAT TO THEM?		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Felixstowe	X37	LOCAL ROUTE 5 (MORRISONS LAND)	CONSTRUCT NEW OFF ROAD FACILITY ACROSS CAVENDISH PARK NORTH TO MEET CYCLE FACILITY ON CAVENDISH PARK SOUTH.		1	0	0	0	-1	3	3	Connectivity and Growth: Low connectivity and growth value for increasing permeability within west Felixstowe; the new and improved existing stretches of shared paths recommended for Grange Farm Avenue and Grande Road respectively will increase permeability through this area in this area - a route through Cavendish Park's two halves would be effective in increasing internal permeability, as well as providing an off-carriageway stretch of cycle/pedestrian track suitable for leisure cycling with children. Maybe useful for some journeys up to the schools on Maidstone Road. Modal Shift: No PCT or Strava Metro data as route does not currently exist. Unlikely to be used for commuting in this location. Score of 0 given. Optimisation: N/A new route. Safety: No uplift in safety created as it would be a brand new off-road route, and again, unlikely to serve as anything other than a leisure route. Biodiversity: Likely to have some biodiversity effect as it would create a net loss in greenspace, however, as a moved green park, the location of the route (which wouldn't necessitate more than minor loss of hedging/mature trees around the

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												perimeter) would not likely cause the removal of high biodiversity value plants/trees; it would at least have the impact of sealed-surfacing over earth. Leisure: As above - a route through Cavendish Park's two halves would provide an off-carriageway stretch of cycle/pedestrian track suitable for leisure cycling with children.
Felixstowe	X38	CAVENDISH PARK NORTH	CONSTRUCT NEW OFF ROAD FACILITY ACROSS CAVENDISH PARK NORTH TO MEET CYCLE FACILITY ON CAVENDISH PARK SOUTH.		0	0	0	0	0	0	0	Duplicate of previous - no score.
Felixstowe	X39	WESTMORLAND ROAD	SIGN AS LOCAL CYCLE ROUTE		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X4	ROSEMARY AVENUE	REVISED ROUTING OF LOCAL ROUTE 1, SIGN & ADD ADVISORY CYCLE LANES		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be likely to achieve an uplift across any of the MCAF categories, particularly safety.
Felixstowe	X40	GRANGE FARM AVENUE	BETWEEN HINTLESHAM DRIVE & POND CLOSE WIDEN EASTERN FOOTWAY & CONVERT TO SHARED USE WITH PRIORITY CROSSING OF REYNOLDS CLOSE. REMOVE TRAFFIC ISLAND & REPLACE WITH RAISED CROSSING FROM NEW CROSSING WIDEN FOOTWAY TOWARDS BRACKLEY CLOSE. (SEE 35)		1	1	1	1	0	0	4	Connectivity and Growth: 1 as infrastructure of adequate (but not LTN 1/20 standards) already exists in this location. Modal Shift: PCT suggests a moderate uplift in modal shift, however, the data does not factor in the North Felixstowe Garden Neighbourhood development (expected to deliver 2,000 homes) and the increased importance of this route for access to the Port (particularly the operations based to the west of the Port) for those living in the central area wishing to walk or cycle to work. However, other options are available. Score of 1 given. Optimisation: Existing footpaths on eastern side would be improved, uplift of 1 scored. Safety: Slight uplift on current level of safety with improved paths and crossings. Score of 1 given. Biodiversity: No impact or benefit scored. Leisure: No anticipated leisure value uplift from current shared paths.
Felixstowe	X41	NATIONAL CYCLE ROUTE 41	SUFFOLK COASTAL CYCLE ROUTE		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Felixstowe	X42	NORTH SEA CYCLE ROUTE	FORMERLY NCN 1 NOW NCN41 & 51		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Felixstowe	X43	MILL LANE	ADD ADVISORY CYCLE LANES BETWEEN GARRISON LANE AND GRANGE ROAD. AT BRIDGE REDUCE VISUAL RUNNING LANE BY WHITE LINE & HATCHING.		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be adequate to meet LTN 1/20 Cycle Infrastructure Design standards in this location.
Felixstowe	X44	TRIMLEY ROAD KIRTON	ADD CYCLE LOGOS (1057) 100M NORTH OF SCHOOL TO ROSELEA NURSERY		0	0	0	0	0	0	0	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X45	KIRTON ROAD ROAD TRIMLEY	WIDEN FOOTWAY LEADING TO FOOT BRIDGE OVER A14, & CONVERT TO SHARED USE.		2	0	2	1	0	1	6	Connectivity and Growth: The response relates to the short stretch of footpath leading to the bridge, rather than the bridge as well. However, it is critical that the bridge can (legally) actually be reached by bike, and that the width supports bi-directional travel. A score of 2 is given as alternative (well used by cyclists though not safe) access to the High Road is available via Howlett Way roundabout. Modal Shift: The response relates to the short stretch of footpath leading to the bridge, rather than the bridge as well. No modal shift potential score without incorporating the bridge for onward travel. Optimisation: Upgrading the footpath to a shared path legally 'unlocks' the route as a cycling route to Trimley St Martin/Felixstowe. The footpath is unacceptably narrow for bi-directional travel. In terms of segregation, this footpath is already fully segregated with green verges and bollards which means the creation of shared paths doesn't provide a significant uplift on the current standard. Score of 2 is given overall. Safety: A score of 1 is given for safety as upgrading the footpath to a shared path (And widening/resurfacing it) would not increase the current level (full) of segregation from traffic, but would make it safer for cyclists/pedestrians to pass or overtake each other. Biodiversity: No anticipated biodiversity effects. Leisure: Score of 1 for leisure given as a shared path to the bridge is an important component in a larger route more suitable for leisure cycling than the Howlett Way roundabout.
Felixstowe	X46	HOWLETT WAY TRIMLEY ST MARTIN	WIDEN FOOTWAY & CONVERT TO SHARED USE.		2	3	3	3	0	1	12	Connectivity and Growth: This route is highly valuable for permeability to and from the site and east to west connections between the Trimleys (and beyond) and the North Felixstowe Garden Neighbourhood via/to/from the two allocations (SCLP12.64 and SCLP12.65), and over to the west for the route down to The Port. Score of 2 given. Modal Shift: High modal shift anticipated associated with high quality infrastructure between the Land at Howlett Way site and the (relocated) Trimley St Martin Primary School by virtue of the high quality infrastructure to be continuously available between them. This route, the 'Dutch style' roundabout anticipated at Hogh Road and the shared paths through the Land Adjacent to Reeve Lodge site will together provide a safer transition over to the route down to The Port, which provides an opportunity for high levels of modal shift for new residents of both of these sites. Optimisation: 3 given as this is a significant improvement on the current earth desire line. Safety: As above, plus priority crossings are expected over the arms of the two new roundabouts. Biodiversity: No anticipated effects.

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												Leisure: Low anticipated leisure value, as Footpath 26 is anticipated to remain a footpath.
Felixstowe	X47	HIGH ROAD TRIMLEY ST MARTIN	ADD CYCLE LANE BETWEEN MILL LANE & HOWLETT WAY		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be adequate to meet LTN 1/20 Cycle Infrastructure Design standards in this location.
Felixstowe	X48	HIGH ROAD TRIMLEY ST MARTIN	ADD CYCLE LOGOS 1057 FROM EGRESS OF CYCLE PATH TO MILL LANE		0	0	0	1	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X49	HIGH ROAD TRIMLEYS & HIGH ST WALTON	REPLACE THE MISSING SECTION OF ADVISORY CYCLES & ADD NEW TO PROVIDE CONTINUOUS LANES BETWEEN GARRISON LANE AND HOWLETT WAY.		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be adequate to meet LTN 1/20 Cycle Infrastructure Design standards in this location.
Felixstowe	X5	TAUNTON & EXETER ROADS	SIGN ROUTE		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: A score of 1 is given as signage is needed here to alert cyclists to the opportunity to cross Garrison Road or Candler Road towards the end of Taunton Toad, which is the first eastward opportunity to do so after Gulpher Road. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X50	FAULKENERS WAY (EAST) HIGH ROAD JUNCTION	CONSTRUCT CYCLE TRACK PRIORITY CROSSING		0	0	1	1	0	0	2	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: A priority crossing may provide a slight uplift in safety for cyclists and pedestrians in this location if designed well. Score of 1 given for optimisation and safety. Safety: A priority crossing may provide a slight uplift in safety for cyclists and pedestrians in this location if designed well. Score of 1 given for optimisation and safety. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X51	NATIONAL CYCLE ROUTE 51	HARWICH TO CAMBRIDGE		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Felixstowe	X52	MAIDSTONE ROAD & GRANGE ROAD	ADD CYCLE LOGO 1057 BETWEEN RAISED TABLE BETWEEN HIGH ST WALTON AND WESSEL AVE /PEWITT HILL		0	0	0	1	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X53	GRANGE ROAD	ADD CYCLE LANES AT SCHOOL ENTRANCE BETWEEN VICARAGE RD & MILL LANE		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be adequate to meet LTN 1/20 Cycle Infrastructure Design standards in this location.
Felixstowe	X54	SEA ROAD	ADD CYCLE LOGOS BETWEEN UNDERCLIFF ROAD & ORFORD ROAD		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X55	HAMILTON ROAD	CONTRA FLOW CYCLING BETWEEN COBBOLD ROAD & ORWELL ROAD		0	0	-1	0	0	0	-1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: No optimisation benefit anticipated from the proposal. Limiting movement, particularly of pedestrians and cyclists, is contrary to the Shared Space concept and would therefore represent 'harm' to the existing scheme/space. Safety: No significant anticipated safety benefit. Also likely to be ignored as restricting movement of bikes (and pedestrians) is contrary to Shared Space core principles. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Felixstowe	X56	HIGH ROAD EAST	EXTEND ADVISORY CYCLE LANE FROM PRIORY Road TO CLIFF ROAD		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be likely to achieve an uplift across any of the MCAF categories, particularly safety.
Felixstowe	X57(1)	MAIDSTONE ROAD - SEATON ROAD RNDBT	OPTION 1 REDUCE ROAD ENTRY WIDTH OF THE 3 ARMS BY LINING AND HATCHING ADD CYCLE LOGOS.		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X57(2)	MAIDSTONE ROAD - SEATON ROAD RNDBT	OPTION 2 REDESIGN AS SHARED SPACE.		0	0	1	1	1	0	3	Connectivity and Growth: No Connectivity and Growth benefit. Modal Shift: No Modal Shift benefit. Optimisation: Redesigning this area as shared space without teaming it with further infrastructure is unlikely to deliver meaningful changes in the safety of cyclists, though it will likely improve the overall urban design quality of the space. Safety: Shared space would likely improve safety by a small amount but necessitating a slower speed, however this is dependent on the design quality as not all shared spaces are ultimately successful in increasing safety for cyclists and pedestrians. Biodiversity: A positive score of 1 for biodiversity has been scored here as an overall shared space scheme would be highly likely to include a net increase in green infrastructure. Leisure: No anticipated leisure value.
Felixstowe	X58	SEATON ROAD	ADD CYCLE LOGO 1057 BETWEEN HIGH RD WALTON AND MAIDSTONE ROAD		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage. Safety: No anticipated safety benefit.

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												Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Felixstowe	X59	BACK LANE	ADD CONTRA FLOW CYCLE LANE BETWEEN SEATON ROAD AND HIGH ST WALTON		0	0	0	-1	0	0	-1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: No optimisation benefit anticipated from the proposal. Safety: As it is a small stretch with poor visibility, this is considered to represent more of a safety risk than it is worth for the cut-through. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Felixstowe	X6	HIGH ROAD EAST	CONVERT TO SHARED USE BOTH EAST BOUND FOOTWAYS TOWARDS THE EXISTING PED ISLAND. LENGTHEN THE ISLAND AND EXTEND DROP KERBS TO PROVIDE A CYCLE CROSSING BETWEEN ROSEMARY AVENUE & PICKETTS ROAD		2	3	3	3	0	1	12	Connectivity and Growth: High Road East currently has advisory cycle lanes (painted lines) which offer no protection for cyclists, and are not considered LTN 1/20 compliant for meeting the needs of most people due to the speed and volume of traffic in this location. Creating a form of segregation is therefore appropriate, and there are pros and cons to being shared paths or cycle lanes. Shared paths can be argued to be more inclusive than cycle lanes, and more appropriate for connecting families with schools, however shared paths - due to the need to negotiate with pedestrians - are much slower than cycle lanes, and therefore are less suitable for the peak time commuter cyclist. As far as Connectivity and Growth is concerned, a high score of 2 is appropriate. Modal Shift: This response proposes shared paths which, in this location, are likely to be less relevant than cycle lanes as the latter better meet the need of peak time commuter cyclists, however this section in en route (When travelling westwards) for the Fairfield Infants School, and therefore shared paths would be appropriate. Both options therefore have modal shift value. The development of the Land at Brackenbury Sports Centre site (SCLP12.5) is likely to be higher density in nature than surrounding development, and is likely to come forward as predominantly flats, which may bring in more working age households - which further increases the relevance of segregated infrastructure in this location. Score of 3 is given. Optimisation: Full optimisation score as shared paths, particularly if their elevation from the carriageway (i.e. as with normal pavements) is teamed with additional physical barriers (e.g. bollards, knee rails, etc) to prevent pavement parking/vehicle incursion, provide full segregation from cars. Safety: As above. Biodiversity: No foreseen effects. Leisure: Moderate leisure uplift for onward access to the sea front.
Felixstowe	X60	FELIXSTOWE LEISURE CENTRE	CONSTRUCT NEW OFF ROAD FACILITY FROM UNDERCLIFFE ROAD TO SEA ROAD BEHIND SEA FLOOD WALL. SCDC ASPIRATIONAL ROUTE		0	2	2	3	-1	3	9	Connectivity and Growth: No connectivity and growth value due to the immediate alternative of using the Promenade for the same journey. Modal Shift: PCT shows that a scheme along Sea Road would create a significant uplift in commuter cycling, however this is based on the Promenade having not been recognised as a route (as it is not a highway/on-carriageway route). Optimisation: Full segregation from Sea Road by being behind the flood wall provides a high safety uplift whilst not impacting the function of the carriageway. However, from a whole-network point of view, this scheme is not considered to be the best solution for this movement corridor (i.e. parallel to the coast) versus the Promenade. Safety: Full segregation from Sea Road by being behind the flood wall provides a high safety uplift whilst not impacting the function of the carriageway. However, from a whole-network point of view, this scheme is not considered to be the best solution for this movement corridor (i.e. parallel to the coast) versus the Promenade. Biodiversity: A cautious -1 score is added for the likely necessary reduction in some of the Felixstowe sea front green space to achieve the infrastructure, if it were acceptable and possible. Leisure: High leisure value.
Felixstowe	X61	CRESCENT ROAD /HAMILTON ROAD JUNCTION	AT TRFFIC LIGHT INSTALL ADVANCED STOP LINES (ASL)		0	0	1	1	0	0	2	Connectivity and growth: No direct connectivity and growth value to adding advanced stop lines. Modal Shift: Advance stop lines at this junction are unlikely to trigger significant modal shift. Optimisation: A score of 1 is given for optimisation and safety as the advance stop line help cyclists get ahead of vehicles when the lights turn green, however, without a cyclist filter light to give then X seconds head start (as programmed) simply being in front of the traffic is unlikely to really significantly increase the safety of cyclists. Safety: A score of 1 is given for optimisation and safety as the advance stop line help cyclists get ahead of vehicles when the lights turn green, however, without a cyclist filter light to give then X seconds head start (as programmed) simply being in front of the traffic is unlikely to really significantly increase the safety of cyclists. Biodiversity: No foreseen biodiversity benefit. Leisure: No anticipated leisure benefit.
Felixstowe	X7	PICKETTS ROAD	SIGN ROUTE		0	0	1	0	0	1	2	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Signage provides low level optimisation value. Safety: No uplift in safety. Biodiversity: No anticipated biodiversity benefits. Leisure: May have low level leisure benefits if used to signpost the route to the coast. Score of 1 is given.
Felixstowe	X8	A1021 HAMILTON ROAD ROUNDABOUT	CONVERT TO SHARED USE THE FOOTWAYS AND FOUR PEDESTRIAN ISLAND CROSSING AT THE ROUNDABOUT ARMS		0	0	-1	-1	0	0	-2	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: As the High Road is the main east to west arterial route, and at this end of Hamilton Road there is no further (northward, eastward or westward) retail provision, shared space is highly unlikely to function well in this location - flattening the area and removing signage here would be unlikely to have a positive impact on cyclist and pedestrian safety. Safety: In this location this is likely to reduce cyclist and pedestrian safety. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Felixstowe	X9	BETWEEN GARRISON LANE & MAYBUSH LANE	SIGN AS LOCAL ROUTE 7 ST ANDREWS ROAD & FOXGROVE LANE AS CYCLE ROUTE		0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Low uplift in optimisation from signage.

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												Safety: No anticipated safety benefit. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Foxhall	347	Bridleway A12 to Dobbs lane	Surface not suitable for cyclists or mobility scooter users. Possible route for a cycle/footpath from new Brightwell development.	This bridleway could be upgraded to give a cycle/footpath route from the new 'Brightwell' development south of BT towards the centre of Ipswich. Connecting with the one that runs south of Cedarwood Primary School and mentioned by others as being upgradable to allow cycling, mobility scooters and buggies.	3	3	0	3	-3	1	7	Connectivity and Growth: From a connectivity and growth perspective, a new bridge and realigned Bridleway 6 would be worth a score of 3 for its function in adding connectivity to and from Brightwell Lakes. Modal Shift: Although PCT cannot be used for currently non-existent off-road routes, an estimate of a significant uplift (200+ a day) of cyclists between Brightwell Lakes and the east of Ipswich (Heath Road) would be expected if this route was developed instead of the route through the Martlesham Heath woodland (its unlikely they would both come forward, particularly given the SSSI incursion using Bridleway 6 - incursion through the Martlesham Heath woodland as well would be difficult to justify) it would be expected to be well used. Optimisation: N/A Biodiversity: -3 for the SSSI incursion
Foxhall	431	From Elmham Drive, eastwards to Straight Road, north side of A1156.	Cyclists wanting to travel from this part of Ipswich towards Martlesham via Straight Road are meant to cross the A1156 here and then re-cross to access Straight Road or continue along narrow, poorly maintained footway and a short section on the main carriageway.	Either improve and add crossings of A1156 to make it safer or provide quality path/cycle lane between Elmham Drive and Straight Road. This could be continued to the cemetery.	1	0	0	3	0	1	5	Connectivity and Growth: Minor Connectivity and Growth benefit as alternative access to Bucklesham Road. Isolated short section of cycle/pedestrian track between Elmham Drive and Straight Road alone would have very little value. It does have value within the context of the Ipswich to Felixstowe Key Corridor, but the Key Corridor recommends use of the service road that runs along the southside of Felixstowe Road 'west' almost perfectly between these two points, which would if nothing else be a more economical (and almost as safe) solution as a track due to the low vehicle movements anticipated on the service road. Score of 1 given. Modal Shift: A score of 0 given as too small an impact anticipated. Optimisation: New infrastructure so would not be scored under this category. Safety: Score of 3 given for track and crossing. Biodiversity: No affect for biodiversity anticipated. Leisure: Low leisure value.
Foxhall	571	Junction from Felixstowe Road (A1156) and Straight Road	Cyclists seek westbound on the popular A1156 Felixstowe Road seeking to turn north up Straight Road have a limited opportunity to safely merge to the centre of the road with fast moving traffic behind them.	Provision of a cycleway along the A1156 and any additional safety features to enable cyclists to be able to turn right in to Straight Road (and potentially right from Straight Road on to the A1156).	3	3	0	2	0	2	10	Connectivity and Growth: This comment supports the delivery of the main section of the Ipswich to Felixstowe Key Corridor - segregated infrastructure along Felixstowe Road 'west' (A1156). Modal Shift: PCT shows modal shift potential here is high. Optimisation: A shared path to allow a right turn onto Straight Road would require new infrastructure on the northern edge of the main road. Safety: As above; segregated infrastructure throughout most of this length (besides the use of service roads where they occur) provides the high score of 2, but not a full score of 3. Biodiversity: No anticipated effects. Leisure: Considered likely to have high leisure value as the route connects Ipswich and Felixstowe.
Foxhall	252a	Bucklesham to Ipswich, walking / cycling	Negotiating the Seven Hills Road Junction by bike or on foot	1. Make Bucklesham Road a cycle friendly route into Ipswich	0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Foxhall	252b	Bucklesham to Ipswich, walking / cycling	Negotiating the Seven Hills Road Junction by bike or on foot	2. Consider upgrading the Bridleway (just West of the Seven Hills A14 junction) that connects Bucklesham Road with Felixstowe Road to hard surfaced allowing direct access to Felixstowe Road, Warren Heath and Ransomes Europark avoiding the Seven hills A14 Junction.	0	0	1	1	-1	1	2	Connectivity and Growth: No added connectivity as Straight Road can be used to connect the two, and Straight Road is low traffic (only provides access to a small number of properties and a cut through from Felixstowe Road to Bucklesham Road, which is of limited value to local traffic compared to other routes) so the difference is negligible. Modal Shift: Anticipated to be negligible; scored as 0. Optimisation: Low uplift as route is already segregated from traffic, but does not have suitable surfacing for road bike tyres at present. Safety: Low uplift in safety as route is already segregated. Biodiversity: Score of -1 given as understood to be a coarsely surfaced farm track with trees lining the western edge. Scheme would try to minimise the loss or damage to the trees on the western edge. Leisure: Score of 1 given as the route is rural and passes reservoirs, which is attractive blue infrastructure.
Framlingham	194	Framlingham - New Road to B1120 Brabbling Green	Road is crying out to be a Quiet Lane. Heavily used by both cyclists and walkers pretty much the whole length. Also, the 60 mph speed limit should be reduced and appropriate signage installed at each end plus repeaters at appropriate intervals.		1	0	0	1	0	1	3	Connectivity and Growth – The proposal would connect Framlingham, a town, to Brabbling Green. As Brabbling Green has no services and is isolated, connecting into Framlingham, which has numerous key services, will provide connectivity benefits, therefore a quiet lane scores a 1 under this category. Modal Shift – It is unlikely that a quiet lane will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – New Road has a NSL and is particularly narrow. Although quiet lanes raise awareness and modifies driving behaviour, they do not introduce any hard safety measures, hence a modest score under safety. Biodiversity – No biodiversity impact. Leisure – The proposal would connect multiple PROWs and connects into Framlingham Mere, which is likely a major leisure attraction, however a quiet lane will unlikely result in significant leisure benefit. A score of 1 is considered reasonable.
Framlingham	417	Castle Street btw Double Street and Fore Street	Castle Street is one-way eastbound which reduces access to the town centre and church from estates on the east side of the town	Suggested contraflow cycle lane. There wouldn't be any loss of parking as the only parking currently is the widest section - there are two exit / queuing lanes and you only need one. West of Double Street may well be too narrow but not a problem as cycles can turn down Double Street which is 2-way	1	0	0	2	0	2	5	Currently, Castle Street is a one-way road travelling eastbound out of Framlingham town centre. The commenter suggests painting a contraflow cycle lane along Castle Street as to avoid cycling along the one-way system on the 'b' type roads. Connectivity and Growth – Castle Street is on-route to Framlingham town centre, which is a key destination with key services including a food shop, however as the proposal will not directly connect into the town centre and as the proposal will likely have more leisure benefit than connectivity benefit, a score of 1 is considered reasonable under this scoring category. Modal Shift – The proposal will unlikely result in a significant modal shift. Optimisation – Castle Street is not an existing cycle route, so the proposal does not represent an optimisation. Safety – The proposal would allow cyclists to use the minor, safer roads rather than

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												following the one-way system along the B1119, which is a busy 'B' type road with a 30mph speed limit. Therefore, a score of 2 under this category is considered reasonable. Biodiversity – The proposal will not have an impact on biodiversity. Leisure – Again, Castle Street is on-route to Framlingham town centre, which has numerous public houses, restaurants, and shops. Furthermore, west of Castle Street is the entrance to Framlingham castle which is a historical leisure attraction. Despite Castle Street being on-route to the town centre, it does not directly connect into the town, therefore a score of 2 is considered reasonable.
Friston	113	Snape to Aldeburgh	The A1094 is too busy and there is no other way of cycling to Aldeburgh.	Use of the coastal path for cyclists as well as walkers. Surfacing in some places, fencing of livestock and extending from Hazlewood Common into Aldeburgh.	1	0	0	3	-1	3	6	For the purpose of this assessment, footpaths 17 and 1 will be looked at to be upgraded into bridleways which will help in the connection of Snape and Aldeburgh. Connectivity and Growth – The proposal will likely have more leisure benefit than connectivity benefit; however, despite the likely low numbers of 'everyday use', the proposal will create a new connection between Aldeburgh and Snape. Therefore, a score of 1 is deemed reasonable. Modal Shift – As a leisure route, it is unlikely going to result in a significant modal shift. Optimisation – As the proposal will create a new route for cyclists, it is not considered an optimisation. Safety – The proposal will provide an alternative to the utilisation of the A1094, which is a busy 'A' type road with a national speed limit, therefore the proposal will likely be beneficial. Biodiversity – The proposal will likely result in the widening on the existing footpath which, in turn, will result in the loss in small foliage and grassed areas adjacent the path. A small negative score is deemed reasonable. Leisure – The proposal will create an attractive route between Snape and Aldeburgh which, being a beach, is likely a major leisure attraction. Furthermore, PROW 17 and 1 resides within the key corridor leisure route. Therefore, a score of 3 is deemed reasonable.
Frostenden	134	Frostenden Hall	Cyclists using footpaths putting walkers, employees and contractors in danger. It is illegal for a cyclist to cycle along a public footpath without the land owner's permission. Very few cyclists are aware of this.	Educate cyclists. Identification numbers on cycles will help deter persistent offenders. Inform navigation apps that some of their information could be incorrect							N/A	The comments raised have been considered in the formation of the strategy, however the education of cyclists is beyond the scope of the strategy and cannot be scored under the MCAF system.
Frostenden	511	Cycle Rt 31 between Beccles and Southwold in the area between Clay Common and A12	Road is in a poor condition and difficult to cycle on -- dropped drainage, etc. There is insufficient signage on the A12 that this is a cycle route crossing the A12.	Sign on north side of the A12 indicating designated cycle path crossing point. Modest repairs to the stretch of road identified above.	0	0	1	1	0	0	2	Connectivity and Growth – No significant connectivity benefit. Modal Shift – Unlikely to create significant modal shift. Optimisation – Advertising the crossing point for a NCR could provide a modest optimisation boost. Safety – The speed of traffic and the slight curve in the road raises the potential benefit, but signs alone are unlikely to offer a significant safety benefit so a score of 1 appears appropriate. Biodiversity – No significant biodiversity benefit. Leisure – Whilst the path has some leisure benefits, the signage for traffic is not deemed to have a significant score.
Gedgrave	468	River Wall - eastern side of Butley River. The path along the river wall between the points TM 393 505 and TM 396 485	This section of river wall is blocked off to the public by fencing. Its omission from the Definitive map could simply be an anomaly as the route recorded on the Definitive Map as Chillesford Footpath 18 stops abruptly at the Chillesford/Gedgrave parish boundary which is absurd.	This route must be added to the Definitive map by way of a Creation Order or Agreement. The proper recording of this route would enable a fine circular walk linking Chillesford and the Butley Ferry.	0	0	0	0	0	3	3	Connectivity and Growth – This proposal connects two existing footpaths but provides limited connections to other villages and/or services and would not provide significant connectivity. Modal Shift – Insufficient evidence to suggest that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – No safety benefit. Biodiversity – No significant biodiversity impact. Leisure – The route represents a strong leisure route adjacent the river and within the AONB designation. The attractiveness of the route means it is considered a full score.
Gedgrave	471	River Wall – Butley River, The Gull, River Ore. Butley Ferry to Tide Guage (TM393481 to TM415484).	This is another section where there is no apparent reason for the route not to be recorded on the Definitive Map. It is freely used (possibly on a permissive basis) but is another instance where a Creation Order or Agreement should be funded.	Path should be added to the Definitive Map by way of a Creation Order or Agreement.	0	0	0	0	0	3	3	Connectivity and Growth – This proposal connects two existing footpaths, but provides limited connections to other villages and/or services and would not provide significant connectivity. Modal Shift – Insufficient evidence to suggest that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – No safety benefit. Biodiversity – No significant biodiversity impact. Leisure – The route represents a strong leisure route adjacent the river and within the AONB designation. The attractiveness of the route means it is considered a full score.
Gisleham	36	Kessingland to lowestoft	One path to use Only on one side of the road. This is a shared pedestrian and cycle path which is used by people going north and south - it's not enough room. There needs to be a substantial cycle path so that people wishing to cycle to Lowestoft can do so safely.		3	3	3	2	-3	2	10	Connectivity and Growth - Whilst it is noted that a connection already exists which would lower the score it does improve a significant section of a recognised key corridor giving it a maximum score. Modal Shift - PCT suggests that this has potential for significant growth if improved to a top standard. Optimisation - A shared path already exists along the A12, but additional width will improve its use giving a score. Off-road routes along London Road could be improved to a shared path standard. Altogether a score of 3 is deemed reasonable. Safety - Providing a wide and comprehensive route will reduce the potential for cyclists and walkers to use London Road meaning a score of 2 is deemed reasonable. Biodiversity - To widen the path would require the loss of verges and likely the loss of established hedgerow Leisure - Whilst it is unlikely to provide the leisure benefit compared to a more coastal path due to its unattractive aspect it still provides good connections to a number of large holiday camps meaning a score of 2 is deemed reasonable.
Great Bealings	153	Seckford Hall Road (West of A12 Woodbridge)	Consider incorporating this lane into a designated cycle route from woodbridge to the Bealings and out lying villages.	Some sort of protected status such as Green Lane, no HGV' route, reduced speed limit, currently national speed limit status	0	0	0	0	0	0	0	Quiet Lanes are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.

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Great Bealings	224	Footpath Brock Lane Woodridge to Great Bealings	Path is unsurfaced and difficult to walk / cycle on, especially for the less mobile and buggies	This path could be upgraded to a surfaced cycle/footpath connecting Bealings and Grundisburgh with Woodbridge without having to negotiate crossing the A12 dual Carraigeway. There is already an existing Pedestrian tunnel under the A12 Martlesham bypass for this footpath. The route could easily connect with cycle/footpaths to Woodbridge and Martlesham Heath Industrial area / supermarkets.	2	1	2	3	-1	1	8	Connectivity and Growth: Little Bealings and Great Bealings, despite their relatively close proximity to Kesgrave, Martlesham and Woodbridge are quite isolated in terms of active transport due to the absence of suitable routes; though unlikely to be heavily trafficked even at peak times, the rural roads will likely experience speeding vehicles and lower visibility, and are unlikely to have lighting on them in the hours of darkness (not confirmed). Brock Lane also provides a good connection point over to Sandy Lane (which is likely to be improved via the Strategy) for onward travel to Woodbridge, where some people in these villages may work. Score of 2 is given. Modal Shift: As above, score of 1 is given. Optimisation: Score of 2 is given as the route uses and upgrades and improves existing footpaths. Safety: Full score for safety given as entire route is vehicle-free. Biodiversity: Biodiversity impact unknown, a cautious -1 is given. Leisure: A low score of 1 is given for leisure as this route is intended to create a commuter connection and utility trip connection to Martlesham/Woodbridge, rather than leisure route.
Grundisburgh	161	Grundisburgh to Woodbridge	Cycling the B1079 between Grundisburgh and Woodbridge is perilous and not suitable for children, inexperienced cyclists and those using mobility scooters.	Consider creating a cycle friendly route using the back lanes, either via Burgh and Hasketon and the existing A12 crossing, or via Great Bealings and Seckford with a new one at Seckford Hall Road. Ensure 30mph speed limits, restriction of HGV's Cars and suitable signage. Connect with existing Woodbridge Cycle/foot paths on East side of A12	3	1	0	3	-3	2	6	The commenter proposes a cycle route through Great Bealings and Seckford into Woodbridge along the quieter roads. For the purpose of this assessment, providing cycling infrastructure along Chapel Road, Grundisburgh Road, Boot Street, and Rosery Lane will be assessed – this will then connect into Seckford Hall Road. Connectivity and Growth – The proposal would create a new connection between Grundisburgh, Great Bealings, and Woodbridge, which being a town is a key service centre. Connecting into a key service centre warrants a score of 3 under this category. Modal Shift – If infrastructure can be delivered to the LN 1/20 standard, then the proposal will likely result in a small modal shift, hence a score of 1. Optimisation – The proposal is for new infrastructure and is not, therefore, considered an optimisation. Safety – The proposal will not only direct cyclists away from 'B' type roads, but also provide a primarily traffic free route. As the 'B' type roads have a NSL and considerably busy, it is likely that removing cyclists and pedestrians off them will have safety benefits. Biodiversity – In order to develop the proposed infrastructure, there would likely be resultant loss of wild verges, established hedgerows, and grass verges. A score of -3 is considered reasonable. Leisure – The proposal will likely have moderate leisure benefits. Although Woodbridge is key town centre and likely considered a leisure attraction with its provision of comparative shopping, eating establishments, and drinking establishments, the proposal will likely have more connectivity benefit than that of leisure. Providing a cycleway along the quieter backroads will also provide connections into multiple PROWs which, although not generally through designated areas, will likely have some leisure value. A score of 2 is considered reasonable.
Grundisburgh	491	Proposed 80 house development in Grundisburgh	A proposed large housing development accessed only via two minor roads with no direct access to the 'B' road network. Increased motorised traffic during construction and when inhabited will increase the risk factor for cyclists, pedestrians and other vulnerable road users trying to negotiate Park, Chapel, Lower & Ipswich Roads all of which have limited if any pavements. This will actively discourage walking and cycling in the area, particularly with regard to those less abled...	Motorised traffic on these local roads need to be forcefully restricted to allow more vulnerable road users to safely walk, cycle, scoot or trot along them to/from local amenities The developer should be instructed to provide suitable cycle/footpaths along the roadside boundaries of the development and off site connecting with the School and local amenities. There is considerable local opposition to this development as per the comments on the current planning application.	1	1	0	1	0	2	5	Connectivity and Growth – The proposal does not provide significant connectivity within Grundisburgh as it does not connect into any services within the village, however it does provide infrastructure along the roads that currently do not have any which helps in connecting isolated houses into the village. Modal Shift – PCT suggests that Chapel Road, if delivered to a high standard, the proposal could score a 1. PCT suggests that Ipswich Road could provide a more significant modal shift, however it is unlikely that the road could be made completely traffic free. A score of 1 is deemed acceptable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The roads proposed by the commenter are not significantly hazardous, but Park Road, Chapel Road, and Lower Road do not have existing infrastructure, therefore the proposal will likely have modest safety benefits. Biodiversity – No significant biodiversity impact. Leisure – As the proposal will connect into the recreation ground situated within Grundisburgh and a couple of PROWs, a score of 2 is considered reasonable.
Hacheston	349	A12 Loer Hacheston / Wickham Mark Roundabout	Given the likely hood of this being a SXC park and ride facility with increased road traffic, there will be increased risk for local cyclists using the roads, roundabout and crossing the A12	Provide suitable segregated cycle/footpaths to allow cyclists/pedestrians to transit from the B1116 to the B1078 and vice versa.	1	0	0	2	0	0	3	Connectivity and Growth – The B1078 and B1116 currently have limited cycling infrastructure. It is unlikely that improving the roundabout for cyclists and pedestrians is going to have significant connectivity benefits, however the B1116 is likely a significant barrier in order to travel into Wickham Market along the B1078, therefore a score of 1 is considered reasonable. Modal Shift – The improvement will unlikely result in a significant modal shift. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – The roundabout is busy with traffic and national speed limit whilst the improvement would offer benefits to a small section of the road, it is a significant safety improvement. A score of 2 is warranted. Biodiversity – No significant biodiversity impact. Leisure – No significant leisure benefits.
Hacheston	477	B1078 / B1116 junction Lower Hacheston	Very limited pedestrian and no cycling facilities at the B1078 / B1116 junction. Bus stops for the main no. 64 bus and also 963 school bus to Thomas Mills HS	Pedestrian island on SW arm of junction Secure cycle parking at bus stop (next to the shelter) Lay-by where cars can safely pull over and wait, if collecting / dropping people Markings and dropped kerbs to facilitate segregated path between the bus stop and Station Road (the lane just next to the southbound slip road)	1	0	0	1	0	0	2	Connectivity and Growth – The road is likely a small barrier to those that need to access the bus stop from the southern side of the road, however the road is crossable with the majority of it having a 30mph speed limit. Modal Shift – There is insufficient evidence that the proposal would lead to modal shift. Optimisation – Providing new infrastructure does not represent an optimisation. Safety – The B1078 / B1116 roundabout is situated in a national speed limit zone, however the SW exit sits close to the 30mph zone, so traffic is likely slow. Therefore, a score of 1 under safety is considered reasonable. Biodiversity – There are no significant biodiversity impacts. Leisure – The proposal provides limited leisure benefit.

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Hacheston	497	Lane leaving B1078 adjacent to where southbound A12 slip road joins B1078	Register as a quiet cycling route to Campsea Ashe, avoiding the B1078 which can be busy with motor traffic and which, in places, is narrow with high banks. This also gives access at Well Cottage to a lane which crosses the railway line via the Blackstock level crossing to give a quiet cycling route via Station Road to Blaxhall and on to Snape.		2	0	0	2	0	1	5	Connectivity and Growth – Not only does the proposal connect Lower Hacheston and Campsea Ashe, but it also partially connects into Wickham Market. As Campsea Ashe has a train station, connecting other settlements into it will have connectivity benefit, therefore a score of 2 is considered reasonable. Modal Shift – PCT suggests that even if infrastructure is delivered to the highest standard, the proposal will not have a resultant significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The road has a national speed limit and is particularly narrow so the proposal will have safety benefit, however as it is unlikely that the road can be made completely traffic free a score of 2 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – The proposal will connect a small handful of PROWs which, although attractive, do not have significant leisure value. A score of 1 is considered reasonable.
Halesworth	175	Between Halesworth and the sea at Southwold	To deliver a cycle route which is safe for children and adults and would enable them to get from the town of Halesworth and nearby villages to the seaside at Southwold (10 miles). It would avoid busy main roads. It would make use of existing bridleways and would link into the national cycle route 1 at Halesworth Millennium Green. It would benefit from signage and a few improvements in path surface to make the route easy to use. Great for fitness, great for all ages, great for building tourism.	Suggested route: start Halesworth Town Park, take National Route 1 along Millennium Green to rail level crossing on Walpole to Mells road. Follow road to Wenhaston and the then to A12 at Blythburgh 644900 274900. Take Bridleway Blythburgh 1 and 9 eastwards. Take Bridleway Walberswick 28 and 29 eastwards and join Blythburgh to Walberswick road. Leave road on Bridleway Walberswick 5 to the river bridge. Cross river and take Bridleway Southwold 25 to Harbour Inn. Then road to Southwold. Main improvement surface of Bridleway Blythburgh 1 and security of short section adjacent to A12. Survey needed for all bridleway surfaces.Route shown in image file attached.	2	0	3	0	-3	3	5	Connectivity and Growth - This improvement will create a majority off road walking and cycling connection between two market towns in East Suffolk which provides a high score. However it does not score the highest possible value as both settlements have good levels of services so the improvement is unlikely to create significant day-to-day connections. Modal Shift - no likely effect. Optimisation - potential widening and resurfacing of existing bridleways would be a positive improvement to the existing Public Right of Way infrastructure. Safety - no likely significant effect. Biodiversity - This improvement will result in the loss of some biodiversity due to the scale of the improvement and the sensitive area it is located in. Leisure - Both Halesworth and Southwold are considered popular locations for leisure activities and therefore the maximum score is given for this category. Moreover, the route itself will be set in an attractive area for users to enjoy.
Halesworth	281	Lack of connectivity	There is no easy way for cyclist and pedestrians to walk/cycle into Halesworth except along the busy B1123, Holton Road. The new 160 unit housing estate will add pressure to the need for a surfaced track to link this part of Halesworth through the Millennium Green to the town centre thus avoiding the B1123. Such a route will encourage people to cycle/walk along this attractive cross country route.		0	1	2	1	-1	2	5	Connectivity and Growth - This improvement looks to upgrade a number of Public Rights of Way (PROW) to create a cycle connection parallel to the B1123 and therefore will be scored under Optimisation. Modal Shift - Holton Road has a modest modal shift potential along the B1123. Optimisation - Upgrading, widening and resurfacing the existing PROWs to accommodate effectively will be a significant improvement to this area of the Town. Safety - This improvement will divert cyclists off the busy B1123 which will have a positive impact on safety. This stretch of road is 30mph which is reflected in the score for this category. Biodiversity - Potential loss of grassland from widening and resurfacing the existing path. This area consists of a mature trees that could potentially also be affected by an increase in footfall. Leisure - This improvement will increase connectivity to Millennium Park and Halesworth Healthy Garden Neighbourhood as well as some additional connectivity to the town centre so a score of 2 is deemed reasonable.
Halesworth	282	Remove the confusion facing cyclists using Rroute 1 through the Thoroughfare in Halesworth	The Thoroughfare in Halesworth is part of Route 1 but it's a one-way mainly pedestrianised shopping street.	Move Route 1 to the east side footpath of Saxons Way to enable two way cycling. This footpath is little used by pedestrians. This footpath is a little less than 2 metres wide and so will require to be widened.	3	1	2	1	0	2	9	Connectivity and Growth - Saxon's Way (A144) forms of part of the spine road that travels through the Heart of Halesworth and therefore implementing cycling infrastructure along the A144 will provide a key connection to the centre of Halesworth. Moreover, this improvement will link into existing cycling infrastructure further north along the A144. Modal Shift - Score of 1 has been attributed to Modal Shift due the modest potential growth shown by PCT for this stretch of road. Optimisation - This improvement will upgrade an existing footway into a shared pathway allowing use by cyclists in a key location. Safety - small potential benefit, the road is often busy however traffic should be moving at 30mph. Biodiversity - no effect. Leisure - The improvement connects into the centre of Halesworth providing users access to many leisure facilities and therefore is given a high score to reflect this.
Halesworth	285	A footpath along the northern verge of Wissett Road Halesworth	From the rear entrance to Edgar Sewter school and Old Station Road there is not a continuous footpath. Pedestrians have to keep crossing the road and for part of this stretch they have to walk in the road. A continuous footpath alongside the road would greatly increase safety and improve the connectivity of this part of Halesworth with the town centre.		2	0	0	2	-1	1	4	Connectivity and Growth - linking up existing pathways to connect the primary school with existing housing would be a significant improvement to connectivity in this part of the town. Modal Shift - no effect. Optimisation - no impact on existing infrastructure. Safety - Old Station Road is 30mph and receives moderate amounts of traffic on a typical. However, during school drop off and pick up times, there can be a lot of traffic and children will be walking to and from school. Therefore a score of 2 has been given to reflect the benefits. Biodiversity - The potential removal of maintained grass verge would not score significantly, although potential loss over adjacent shrubbery could have a negative score. Leisure - This improvement will create a modest connectivity to the town centre.
Halesworth	286	Lack of connectivity	A short cycle/pathway linking Bramblewood Way with Loam Pit Lane. This short connecting link would enable cyclists/pedestrians to avoid having to go along Holton Road if they were going to the station, surgery, Edgar Sewter school or the north of the town.		1	0	0	0	-1	0	0	Connectivity and Growth - Linking Bramblewood Way with Loam Pit Lane would create a small improvement to the overall connectivity for this part of Halesworth and therefore a score of 1 has been awarded to reflect this. Modal Shift - no effect. Optimisation - the improvement will be a new piece of infrastructure and therefore is not scored under Optimisation. Safety - no significant effect. Biodiversity - The area comprises of a number of existing trees and vegetation which could potentially be effected by the improvements. Leisure - no effect.
Halesworth	287	No continuous footpath along the east side of the A144 from Fair View Road up to the Sparrowhawk Road roundabout	There are short lengths of path which need to be linked up for convenience and safety reasons. There is space along the verge and a path should have been installed at the time of the new housing developments were being built.		2	1	0	1	0	0	4	Connectivity and Growth - Extending the length of the cycle path will improve connectivity for houses in the Northern part of Halesworth and link to the existing employment area. Modal Shift - a potential modest uplift would be achieved with improvements to this road according to PCT. The town centre would be linked to the employment area (WLP4.6) in the North. Optimisation - This comment is focused on connecting the existing pathway with new pieces of infrastructure to create one complete route into Halesworth. Safety - A low score has been given due to the fact that the speed limit along this stretch is 30mph. However, this is a heavily used road with a considerable amount of traffic meaning that a score of one has been given to

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												reflect this. Biodiversity - no effect. Leisure - Whilst some additional connectivity to the town centre is provided the majority of the beneficiaries are from the employment areas meaning no score for leisure has been provided.
Halesworth	289	Upgrade the footpath from opposite the Beech Close junction on Chediston Street through the fields to the end of School Lane.	Improving this unmade path would make a pleasant country walk around the west side of Halesworth. If it can be widened to accommodate cycles it would add an interesting connection for cyclist around the town without having to go through it.		0	0	2	0	-1	1	2	Connectivity and Growth - This comment is in relation to an existing footpath. Modal Shift - no effect. Optimisation - The improvement will upgrade an existing off-road footpath to a bridleway status to accommodate cycling and walking. Resurfacing and widening the route would be needed to accommodate cycling and walking. Safety - No significant effect. B - Potential loss of grassland when widening and resurfacing the existing route. Leisure - The improvement will create an attractive, off-road route that links into the centre of Halesworth.
Halesworth	293	A144 roundabout joining Quay Street and Saxons Way (Hooker House), up to the Triple Plea Roundabout where Sparrowhawk Road joins the A144 Norwich Road	The current main south-north cycle and pedestrian route up Norwich Road to businesses to the north of the town, and importantly to the Edgar Sewter Primary School, is dangerous, too complex (multiple road crossings with varying priorities) and does not serve the primary school for sustainable transport	From the Norwich Road/Quay Street roundabout (A144), move the existing cycle route from the east side of the A144 across to the west. Create a 'Copenhagen' or similar vastly improved crossing at Wissett Road junction, widen what would become the shared pedestrian/cycle path on the west side, remove all existing parking where necessary on the west side (especially near Wissett Road junction, and up A144 past the police station), and replace with single yellow lines with waiting limits of 1 hour (to support school visits and drop-offs). This route must link from the Quay Street Hooker House roundabout up as far as the Sparrowhawk Road roundabout near the Triple Plea Road and pub. Suggest NCR1 route is also amended to utilise this new safer less complex route, once established, and once connected to other proposals entered onto the interactive map. Agreed with the Halesworth NPSG Cycle Advisory Team	2	2	0	1	0	2	7	Connectivity and Growth - Moving and extending the length of the cycle path will improve connectivity for houses in the Northern part of Halesworth. Modal Shift - A reasonable uplift could be potentially achieved with improvements to this road. The town centre would be linked to the employment area (WLP4.6) in the North. Optimisation - This comment is focused on creating a new cycling path on the East of the A144 to extend further North and the removal of the existing path to the East. Safety - A low score has been given due to the fact that the speed limit along this stretch is 30mph. However, this is a heavily used road with a considerable amount of traffic meaning that a score of one has been given to reflect this. Biodiversity - no effect. Leisure - The improvement will link close to the Throughfare in Halesworth which is as well as providing reasonable connections to the Healthy Neighbourhood allocation meaning a good score is given.
Halesworth	294	A144 - East side of Saxons Way and London Road in Halesworth, from the Quay Street/Norwich Road roundabout south to the junction between London Road and Bramfield Road	Current NCR1 cycle route through the town Thoroughfare requires dangerous mixing of cyclists with pedestrians and is too complicated. Importantly it routes through the busy central car park which is hazardous for riders to mix with multiple/reversing parked vehicles. The proposal links safely with the separately proposed shifting of the A144 Norwich Road cycle path to the west of the road, via the use of the existing pelican crossings on Saxons Way and/or Norwich Road	Pavements along Saxons Way, from Quay Street roundabout to the Coop/London Road roundabout should become safe, shared cycle and pedestrian paths. There is adequate council-owned land to provide this on the east side of Saxons Way and east side of London Road. Route should continue along the east side of London Road to the Bramfield Road junction (main route into Halesworth from the A12) This route creates the key movement corridor through the town that enables connections to all major destinations – school, Thoroughfare, Doctors Surgery, Sports Centre (in development), industrial estates, residential areas Suggest rerouting of NCR1 away from the Thoroughfare/Bridge Street between the Quay Street and the entrance to the car park removes a confusing and badly signposted national route. The Saxons Way route would remove the confusing one way cycling in the Thoroughfare and the dismount instruction at the southern end of the Thoroughfare. Agreed by the Halesworth NPSG Cycle advisory group.	3	1	2	1	0	2	9	Connectivity and Growth - Saxon's Way (A144) forms part of the spine road that travels through the Heart of Halesworth and therefore implementing cycling infrastructure along the A144 will provide a key connection to the centre of Halesworth. Moreover, this improvement will link into existing cycling infrastructure further north along the A144. Modal Shift - Score of 1 has been attributed to Modal Shift due to a modest potential shown in PCT. Optimisation - This improvement will upgrade an existing footway into a shared pathway allowing use by cyclists in a key location. Small - small improvement for safety, the road is often busy however traffic should be moving at 30mph. Biodiversity - no effect. Leisure - The improvement connects into the centre of Halesworth providing users access to many leisure facilities and therefore is given a high score to reflect this.
Halesworth	295	A144 Halesworth, Bramfield Road from junction with London/Walpole Road to Blyth Road Industrial Estate, and possibly to existign NCR1 at Mells/Walpole crossroads on A144	No safe cycle path exists at present, meaning cyclists heading along here must use the main busy road up a steep hill if travelling south-north	Create a route along the south-west side of Bramfield Road (A144), making use of Durban Close if required. This would connect to the proposed north/south route on London Road and to Blyth Road and the entrance to the industrial estate and on into the Millennium Green (hence back towards NCR1. Ideally, this should extend slightly further south-east along the A144 just a little way so that it links with NCR1 where it crosses at the Mells/Walpole crossroads (Wenhaston Grange Road - this creates a far safer route into town for neighbouring Walpole cyclists/families, etc). Blyth Road-London Road section agreed by NPSG Cycling Advisory group, with an additional beneficial extension to Mells/Walpole crossroad to the south	0	0	2	1	-1	1	3	Connectivity and Growth - An existing footway is already in place that provides a connection onto London Road and then into Halesworth Town Centre. Modal Shift - no significant effect. Optimisation - Widening and resurfacing existing footway into a shared pathway to accommodate cycling and walking is a significant improvement. Safety - The A144 is a busy road that receives a lot of traffic at peak times however this section is covered by a 30mph speed limit and therefore a score of 1 has been given to reflect this. If the proposed cycle route was to extended further along the A144 into a national speed limit stretch then it would have a greater benefit to safety and would receive a higher score. Biodiversity - Widening and resurfacing the path would result in the loss of the existing grass verge and potential impact on the existing hedge. Leisure - The improvement provides a modest benefit with links into NCR1, the town centre and Millennium Green.
Halesworth	296	Halesworth, existing access route between Chichester Road/Uplands Way housing estate	Current pedestrian-only access between Norwich Road and Uplands Way is narrow and doesn't promote safe cycling of households and children between the Chichester Road estate and town or Primary School. The only current legal cycle route is along busy Wissett Road which is dangerous, has a steep hill for young riders, and has an extremely dangerous junction with Norwich Road (lacking a pushchair/wheelchair width footway). Some young children cycle this route to school but is far from ideal.	Requires possible inclusion of land from Edgar Sewter Primary School to enable wider cycle path. Upgrade the existing footpath between Uplands Way and the Norwich Road alongside the school fence to create a shared pedestrian and cycle path, with signage. Agreed with NPSG Cycle Advisory group.	0	0	2	0	-1	2	3	Connectivity and Growth - Connection already available via existing footpath. Modal Shift - no effect. Optimisation - Widening, resurfacing and upgrading the existing footpath into a shared pathway that can be used by cyclists and walkers will be a significant improvement to this area of the Town. Safety - no effect. Biodiversity - Potential loss of grassland and potential slight impact on existing hedge from widening and resurfacing the route. Leisure - The improvement will link close to the Throughfare in Halesworth which is as well as providing reasonable connections to the Healthy Neighbourhood allocation meaning a good score is given.
Halesworth	297	Halesworth - Loam Pit Lane, cemetery area, Harrisons Lane and Hill Farm development	No cycle route linking current and proposed housing development in Harrisons Lane and Hill Farm/Blyth Vale. This will inadvertently encourage riders to use Holton Road and/or Bungay Road and cross the railway line, and/or Norwich Road which is a longer route for	Loam Pit Lane – include/provide a cycle route so connecting Harrisons Lane to Holton Road, serving the new housing on Harrisons Lane (planning permission given), connecting the new sports centre on the Campus site, with a planned cycle route within it, into	0	0	3	0	0	2	5	Connectivity and Growth - Existing footway (PROW 7) offers an connection for pedestrians to use but is not suitable for cyclists. Modal Shift - No significant effect. Optimisation - Upgrading, resurfacing and potentially widening the existing path way to create segregated walking and cycling paths will be a significant improvement to the existing infrastructure. Safety - No significant effect. Biodiversity - Likely no

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			young riders. When Campus project is delivered for more social and leisure facilities, it's vital that such a safe route exists, and minimises car use.	Loam Pit Lane. This may partially utilise/link into the development intentions by Hopkins Homes Ltd at Blyth Vale (off Hill Farm Road), by linking across the west end of the cemetery and Loam Pit Lane, onto open space areas created by the Hopkins development, and linking towards Holton.								effect however if the path is required to be widened it may have an impact on the surrounding grassland. Leisure - This improvement will link residents into the proposed leisure facilities set out in the allocation site. Moreover, it will improve access to Halesworth Town Centre where a large number of leisure facilities are located.
Halesworth	298	Halesworth - link Briar Close with Quay Street/Holton Road	From Quay Street /Holton Road is currently difficult to ride from the road up to the railway station. There is a pedestrian footpath linking from just beside the railway bridge to the end of Briar Close.	Improve access into Briar Close and the route to the Station by improving the pavement under the railway bridge and its connection to the end of Loam Pit Lane. Convert the existing footpath to a shared cycle/pedestrian path. Consideration will have to be given to negotiating the dangerous traffic flow under the Quay Street/Holton Road railway bridge. Part of the NPSG Cycle Advisory group review.	2	0	2	0	0	1	5	Connectivity and Growth - Improving the pathway to allow cyclists will ensure better access to the train station and a connectivity benefit. Modal Shift - no significant effect. Optimisation - Upgrading existing footway into a shared cycle path would be a significant improvement for this part of the Town. Safety - no significant effect. Biodiversity - no effect. Leisure - This improvement will provide a modest increase connectivity to Halesworth Town Centre where a variety of leisure facilities are located.
Halesworth	299	Halesworth - new link required between Hill Farm Road development and Loam Pit Lane	Currently it is not clear there is any safe cycle/pedestrian link proposed between the new Hill Farm development (Hopkins Homes Ltd), Loam Pit Lane, and the east side of town towards Holton. Without this the natural route will be a less safe one down Hill Farm Road and onto Holton Road, which is busy for younger and other riders, some of which could be to and from the primary schools in Holton and/or Halesworth.	Hill Farm Road development – create a path from this new estate and the proposed playground west into Loam Pit Lane (possibly linking across the north side of the cemetery) to connect to the proposed new path east to Holton Orchards Road so improving cycling access to and from the east of town and from Holton.	3	0	0	0	-1	1	3	Connectivity and Growth - This improvement will create a new off road connection between the East side of Halesworth and Holton. Furthermore, this route will add to the existing infrastructure to create better connectivity between Halesworth Town Centre and Holton. Modal Shift - no significant effect. Optimisation - no existing infrastructure. Safety - no significant effect. Biodiversity - Potential loss of agricultural land/grass land. Leisure - This improvement will provide a modest increase connectivity into Halesworth Town Centre and facilities within the Healthy Neighbourhood.
Halesworth	300	Halesworth - new route between Allington Road and Roman Way to support proposed housing development	The current route into town heading west-east (Chediston, Metfield and Harleston) currently takes a cyclist off Chediston Road and up the steep (HGV route) and often busy Roman Way hill which is a steep climb. The proposed new development at Chediston Street/Roman Way includes suggestion for a cycle route up Chediston Street into the town centre. This is considered a dangerous and narrow street with parking both sides, and an alternative route should be planned starting from Allington Road.	From the estate create a route into either Barley Meadow, Dakings Drift and/or Allington Road connecting into Dukes Drive, cross Roman Way to connect to the existing cycle route in Holmere Drive and into Church Farm Lane. This creates a relatively safe cycling route into the Market Place and town centre via the quiet northern end of London Road around the St Mary's Church yard.	0	0	2	0	0	1	3	Connectivity and Growth - Existing pedestrian footway is in place that provides a connection to the cycling infrastructure along the B1123. Modal Shift - no significant effect. Optimisation - Upgrading the existing pedestrian footway into a shared pathway for cycling and walking would be a significant improvement to the existing infrastructure. This would create a continuous cycling connection between WLP4.2 and Halesworth Town Centre. Safety - There would be a slight improvement to safety with this improvement, however the road speed is 30mph and this stretch of road is quieter than other areas of Halesworth and therefore a neutral score has been allocated to reflect this. Biodiversity - no effect. Leisure - This improvement will create a better cycling connection towards the town centre, however it is not direct so a modest score is deemed reasonable.
Halesworth	301	Halesworth - from Saxons Way through River Lane to the town park and Millenoum Green	Improve cycling connectivity from the town centre to the Millenium Green and east. River Lane (past George Maltings) is currently only a pedestrian footpath (ownership unknown), but this would be a good short cut from the ANgel Link end of town into the park and Millenium Green and east sides of the town, using the existing Millenium Green cycle path. The existing river bridge in the lane is too narrow to accommodate cyclists.	Make River Lane into a cycle route. This would connect the Angel Link roundabout on Saxons Way to the park. The bridge over the river is too narrow at present but could be given pedestrian right of way or cyclist dismount signs until the bridge can be widened. River Lane is a private road and is unregistered on the land registry, so investigations needed to start the process of redesignation.	2	0	0	0	0	2	4	Connectivity and Growth - This improvement will create a link between Millennium Green and Angel Link and then into Halesworth Town Centre. MS - No significant effect. Optimisation - This improvement is related to a new piece of infrastructure. Safety - no significant effect. Biodiversity - no effect. Leisure - This improvement will create a direct link between Millennium Green and Angel Link and then into Halesworth Town Centre where a variety of leisure facilities are located.
Halesworth	302	Halesworth - Blyth Mews link between Quay Street and the town park	Cycling from the east of town (e.g. from Holton Road and Holton village) currently can only use the main Holton Road and Quay Street which links onto Norwich Road (A144) at Hooker House roundabout. This is a dangerous section of road with multiple constricted parking areas (mainly residents), ending in a very busy Hooker House roundabout and confusing pedestrian crossings	Make Blyth Mews off Quay Street into a cycle route with appropriate signage. The bridge over the Patrick Stead Lock at the end of Blyth Mews would benefit from improving. Note - with the addition of 'cyclists give way to pedestrians' signs, the bridge is wide enough in its current form if funding isn't available for widening, until the bridge can be replaced and widened with possible signage giving pedestrians right of way. Review how the Blyth Mews/Quay Street cycle and pedestrian routing could be improved so safer/easier access could be made into Station Road, given giving access to the railway sStation and the 'The Cut'. If the car showroom (currently MR King Ltd) site opposite Blyth Mews was developed this could give an opportunity. Agreed by NPSG Cycle Advisory group	0	0	2	0	0	2	4	Connectivity and Growth - This improvement will look to improve an existing footway and therefore will be scored under optimisation. Modal Shift - No significant effect. Optimisation - Upgrading and widening the existing footway to support cycling infrastructure will be a significant improvement for this area of Halesworth. Safety - There would be a slight improvement to safety with this improvement, however the road speed is 30mph and this stretch of road is quieter than other areas of Halesworth and therefore a neutral score has been allocated to reflect this. Biodiversity - no effect. Leisure - This improvement will increase connectivity to Halesworth Town Centre where a variety of leisure facilities are located.
Halesworth	303	Halesworth - Millenium Green and Folly, joining Holton Road to the town park and centre	Currently the natural cycling route from Holton Road and Holton (east side of town) is via Holton Road and Quay Street, which are dangerous and regularly used by HGVs and emergency service vehicles. This should be relieved such that cyclists can divert away from Holton Road onto a new parallel route	Create a new cycle route through the Folly in and the Millennium Green. This would connect Holton Road opposite the new housing development at Hill Farm Road into the Millennium Green and into the town centre or out on the NCR1. The details of the route have been mapped out by the Millennium Green trustees, who are responsible for much of the land through which the proposed route passes. Footpaths off the Holton Road (towards 'Rails End') would need to be redesignated as cycle routes. The owners would need to be approached. This would then link with the proposed River Lane and Blyth Mews routes into and then through to the town park and Millennium Green, also giving an alternative to the Saxon Way route for less confident cyclists and conversely, a route out to the east of town. Agreed with the NPSG Cycling Advisory group.	0	1	2	1	-1	2	5	Connectivity and Growth - This improvement looks to upgrade a number of Public Rights of Way (PROW) to create a cycle connection parallel to the B1123 and therefore will be scored under Optimisation. Modal Shift - PCT suggests that this improvement will have a modest effect on Modal Shift along the B1123. Optimisation - Upgrading, widening and resurfacing the existing PROWs to accommodate effectively will be a significant improvement to this area of the Town. Safety - This improvement will divert cyclists off the busy B1123 which will have a positive impact on safety. This stretch of road is 30mph which is reflected in the score for this category. Biodiversity - Potential loss of grassland from widening and resurfacing the existing path. This area consists of a mature trees that could potentially also be affected by an increase in footfall. Leisure - This improvement will increase connectivity to Halesworth Town for Holton

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Halesworth	304	Halesworth - provide new 20mp speed limit through town to calm traffic and promote safer cycling and low speed vehicle use	Unlike many Suffolk and National towns and villages, Halesworth has no reduced speed limits to 20mph even outside the Edgar Sewter Primary School. This is creating direct danger to cyclists and pedestrians alike, particularly being combined with very poor parking practices in London Road, Norwich Road, Holton Road and Quay Street.	Halesworth requires traffic calming/slowing measures, and the popular and effective way like other nearby market towns would be to provide 20mph speed limiting as follows: 1. The main A144 north-south route from Bramfield Road/London Road junction (Kerridges garage) all the way along London Road, Saxons Way, and Norwich Road as far north as "The Avenue". 2. Eastwards from the Norwch Road Hooker House roundabout along Quay Street and Holton Road, as far as "Castle House" at the top of Holton Road hill. 3. Westwards from the Angel Link roundabout and London Road (Coop roundabout) to the junction of Roman Way and Chediston Road. 4. Roman Way from its junction at Chediston Road, to the junction at London Road near the Rifle Hall.							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Halesworth	306	Halesworth - Remove parking and apply waiting limits to Norwich Road between its junctions with "Wissett Road" and "The Avenue"	Current unrestricted parking is posing an immediate safety hazard to other road users - cyclists and people/children crossing Norwich Road. It is believed the current prolonged parking may be businesses and Police Station employees. Current parking risks doors being opened into other road users' paths, and pedestrian/children crossing between parked cars onto the main through-route including HGVs, is very dangerous. This is a site of previous cyclists being knocked off cycles by cars.	Provide double yellow lines between Wissett Road junction and opposite Hammonds Ford Garage, and from there northwards to the junction with "The Avenue" provide single yellow line restricted parking for 1 hour to enable school drop-off and school visit parking.	0	0	0	1	0	0	1	Connectivity and Growth - No effect. Modal Shift - No effect. Optimisation - No effect. Safety - The parked cars do pose a potential obstacle as cyclists are required to move closer to the centre of the road. Moreover, the parked cars also reduce the visibility of pedestrians to vehicle drivers. Therefore a score of 1 has been given to reflect this. Biodiversity - No effect. Leisure - No effect.
Halesworth	314	Saxon's Way and Thoroughfare	The Saxon's Way A144 thru road is a very busy road for cyclists and has no cycle path. Cycling is only allowed one way thru the Thoroughfare, which is busy with peds. A cycle path is badly needed on Saxon's Way to connect with Bungay Rd A144 where there is a cycle path. (This one needs extending to the quiet lane at the Triple PLea Roundabout at Sparrowhawk Lane.) It is worth remembering that the Edgar Sewter School is on the A144.		3	1	2	1	0	2	9	Connectivity and Growth - Saxon's Way (A144) forms of part of the spine road that travels through the Heart of Halesworth, therefore implementing cycling infrastructure along the A144 will provide a key connection to the centre of Halesworth. Moreover, this improvement will link into existing cycling infrastructure further north along the A144. Modal Shift - Score of 1 has been attributed to Modal Shift due to the PCT score of 65 for this stretch of road. Optimisation - This improvement will upgrade an existing footway into a shared pathway allowing use by cyclists in a key location. Safety - The road is often busy, however traffic should be moving at 30mph. Therefore, the proposal will provide a small improvement for safety. Biodiversity - No effect. Leisure - The improvement connects into the centre of Halesworth providing users access to many leisure facilities, therefore a high score is awarded to reflect this.
Halesworth	359	Footpath that runs beside the River Blyth from Halesworth to Blythburgh	Someone has suggested turning this into a combined footpath and cycle way. I think that would completely destroy a beautiful piece of countryside. I do not want to walk always having think is there a cyclist hurtling towards me? The hard surface is completely out of keeping with the location. It will ruin it.	Cycle ways should be provided alongside roads, with a hedge inbetween. It is I fact possible to cycle on quiet road between the two places, just not directly.	0	0	0	0	0	0	0	Connectivity and Growth - No change to the path will not create new connectivity. Modal Shift - No effect. Optimisation - No effect. Safety - The improved safety for walkers is minimal in this instance. Biodiversity - No effect. Leisure - The suggested improvement to leisure benefit for walkers is cancelled out by the loss of leisure opportunities for cyclists.
Halesworth	360	Round Halesworth	A Councillor has suggested a list of cycle route round the town. I support all of the councillors ideas and am not going to write all out again on this cumbersome system.	Do, what the Councillor suggests.	0	0	0	0	0	0	0	Support for comments has been noted.
Halesworth	361	Link to bypass Wissett Road by joining Norwich Road and Wissett Road	Currently, Wissett Road is a very hazardous route for all road users, but particularly for cyclists and pedestrians, plus the Edgar Sewter Primary School. It is too narrow even for an acceptable pedestrian path at the end near the Norwich Road junction, yet is a route often used by HGVs and farm traffic. Wissett Road in its current form is a dangerous hazard to all.	Norwich Road and Wissett Road should be linked by a new road AND combined cycle/pedestrian route from Broadway Drive (i.e. off Norwich Road) down across the railway line to Wissett Road on the Wissett/north-west side of Halesworth. This would require funding for a railway crossing, but if the field between Norwich Road, Old Station Road and the railway line is (as believed) to be developed for residential or elderly care, then such a crossing should be made an essential part of the development permission process. As a trade-off, perhaps the Old Station Road Mill Post Crossing could be removed to make this proposal more palatable to Network Rail.	1	1	0	1	-3	1	1	Connectivity and Growth - A new connection will be created to connect the north of the Town to the western edge of the Town. Modal Shift - PCT score of 76 along the A144, diverting people onto a new route, albeit less direct, will have a notable impact on Modal Shift. Optimisation - This comment is related to a new piece of infrastructure and, therefore, does not score under this category. Safety - Diverting users away from the often busy A144 will have a positive effect on safety. Biodiversity - This route is entirely off-road and would result in the loss of agricultural and grassland. Moreover, the proposed route may require the removal of mature trees and/or hedges which would be a significant biodiversity loss. Leisure - This improvement will have a modest improvement in terms of access to leisure facilities. Access to the town centre would not be significantly improved with the main beneficiaries being businesses as opposed to leisure users.
Halesworth	480	The thoroughfare, Halesworth. Between Halesworth Library and the Thoroughfare/London Road junction	Cycling to be allowed in both directions, thus allowing both local and visiting cyclists to travel through The Thoroughfare and use its facilities		1	0	0	0	0	3	4	Connectivity and Growth - Allowing cyclists to travel both in both directions along the Thoroughfare will increase connectivity in the heart of Halesworth Town Centre. Modal Shift - No effect. Optimisation - No effect. Safety - No effect. Biodiversity - No effect. Leisure - The Thoroughfare contains a variety of leisure facilities that are frequently used by residents and visitors to Halesworth, as this suggested improvement is directly within the town centre the highest score is deemed acceptable.
Halesworth	739	Halesworth	I have been looking at the plans for the Cycling and Walking Strategy for Halesworth and I think these are all good ideas.	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the							N/A	Comment noted - see comments 739a, 739b, 739c, 739d, 739e

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				Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).								
Halesworth	740	Town Centre to Millennium Green	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Support the improvement to the routes and connectivity from the Town Centre to the Millennium Green (see Objective 7) so encouraging more use.	2	0	0	0	0	2	4	Connectivity and Growth - This improvement will create a link between Millennium Green and Halesworth Town Centre. Modal Shift - No effect. Optimisation - This improvement is related to a new piece of infrastructure. Safety - No effect. Biodiversity - No effect. Leisure - This improvement will create a link between Millennium Green and Angel Link and then into Halesworth Town Centre where a variety of leisure facilities are located.
Halesworth	741	Green corridor / walking route	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Designate land that would support the creation of a green corridor/walking route around the South/Western edge of the town. This supports Objective 1 (biodiversity) and 5 (protection of entrance views to the town if future development of farming land was permitted). It could run from Chediston St, behind the backs of Dukes Drive, crossing Walpole Road and continuing behind the backs of Kennedy Avenue and Close linking to the proposed nature reserve and footpath to the Basely sports ground.	1	0	0	0	-2	2	1	Connectivity and Growth - A new connection will be created that connects the Eastern edge and Western edge of Halesworth through this improvement. However, this connection takes users South of the Town rather than through the Centre where the majority of services and facilities are located. Therefore, a score of one is given to reflect this. Modal Shift - No effect. Optimisation - This improvement is providing a new piece of infrastructure as opposed to improving existing infrastructure. Safety - No effect. Biodiversity - The proposed route will result in the loss of agricultural land and possibly the removal of existing hedges. Leisure - The proposed route will create a very attractive route for users to for leisure purposes.
Halesworth	743	Halesworth	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Rationalise the walking maps available so they can form a suite of information online and in leaflet form and that reference each other. Some are signposted. Some need updating. Some have a specific historical focus. NB Subsequent agreement to work with Green Access team at SCC to produce a leaflet of circular walks for the Discover Suffolk website and to digitise the other leaflets so they can be accessed on the same website. Erect well designed and coherent signage once the maps and routes are finalised. (not a planning matter but a potential use of CIL money).							N/A	Comprehensive information material that is readily available to the public in regards to walking and cycling routes would be a positive improvement to support future infrastructure improvement.
Halesworth	745	Harrisons Lane to Loam Pit Lane	Hill Farm Road, Fairview Road,(being built) Chediston St/Roman Way, Harrison's Lane/Town Farm (with outline planning) are the new developments. Attention has been given to walking connections from Hill Farm Road (this better connects Halesworth and Holton and tries to ensure children can walk to the two primary schools) and to Fairview (a rather disjointed pavement/cycle track around the corner of Fairview Road that doesn't really connect).	Where gaps exist ensure safe and connected pavements from the main residential areas to the town centre and key destinations. Harrisons Lane housing and sports developments need to connect with Loam Pit Lane and the east west routes. There is concern about the poor considerations given to walking and cycling in the Chediston St development plans.	0	0	3	0	0	2	5	Connectivity and Growth - Existing footway (PROW 7) offers an connection for pedestrians to use but is not suitable for cyclists. Modal Shift - No effect. Optimisation - Upgrading, resurfacing and potentially widening the existing path way to create segregated walking and cycling paths will be a significant improvement to the existing infrastructure. Safety - No effect. Biodiversity - Likely no effect however if the path is required to be widened it may have an impact on the surrounding grassland. Leisure - This improvement will link residents into the proposed leisure facilities set out in the allocation site. Moreover, it will improve access to Halesworth Town Centre where a large number of leisure facilities are located.
Halesworth	746	Allington Road to Dukes Drive	Where gaps exist ensure safe and connected pavements from the main residential areas to the town centre and key destinations.	Routes need to make use of the lie of the land and connect with Allington Road onto Dukes Drive to connect with the bus stop. The proposed cycle route up Chediston Street is strongly opposed by the Cycling Group as the road is too narrow).	0	0	2	0	0	1	3	Connectivity and Growth - Existing pedestrian footway is in place that provides a connection to the cycling infrastructure along the B1123. Modal Shift - No effect. Optimisation - Upgrading the existing pedestrian footway into a shared pathway for cycling and walking would be a significant improvement to the existing infrastructure. This would create a continuous cycling connection between WLP4.2 and Halesworth Town Centre. Safety - There would be a slight improvement to safety with this improvement, however the road speed is 30mph and this stretch of road is quieter than other areas of Halesworth and therefore a neutral score has been allocated to reflect this. Biodiversity - No effect. Leisure - This improvement will create a better cycling connection towards the town centre, however it is not direct so a modest score is deemed reasonable.
Halesworth	747	Wissett Road down to Old Station Road	Where gaps exist ensure safe and connected pavements from the main residential areas to the town centre and key destinations.	Create a new pavement down the northeast side of Wissett Road from the entrance to the Children's Centre down to Old Station Road. This can improve the existing tarmac path at the top end, better connect it to the pavement in Wissett Close which goes down to Chichester Road and then use the wide verge down to Old Station Road. This would give safer walking for the Chichester Road estate and help connect with country footpaths around Wissett.	2	0	0	2	-1	1	4	Connectivity and Growth - Linking up existing pathways to connect the primary school with existing housing would be a significant improvement to connectivity in this part of the town. Modal Shift - No effect. Optimisation - No impact on existing infrastructure. Safety - Wissett Road is 30mph and receives moderate amounts of traffic on a typical. However, during school drop off and pick up times, there can be a lot of traffic and children will be walking to and from school. Therefore a score of 2 has been given to reflect this. Biodiversity - The potential removal of maintained grass verge would not score significantly, although potential loss over adjacent shrubbery could have a negative score. Leisure - This improvement will create a modest connectivity to the town centre.
Halesworth	748	Pavement down Norwich Road	The existing pavement from Norwich Road down to the Children's Centre entrance is very narrow in parts and should be looked at to see if some widening could be done.	Where gaps exist ensure safe and connected pavements from the main residential areas to the town centre and key destinations.	0	2	2	0	-1	2	5	Connectivity and Growth - A new connection is not created through this improvement. Modal Shift - PCT score of 116 is given for this stretch of road. If the widening was enough to accommodate cycling and walking, this would have a significant positive impact on modal shift. Optimisation - Widening the existing pathway to accommodate walking and cycling would be a significant improvement to the existing infrastructure. Safety - Widening the pathway would provide a small improvement to safety for users. Biodiversity - Potential loss of maintained grass verge.

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												Leisure - The route will be used extensively by residents to access the Town Centre for leisure purposes.
Halesworth	749	Entrance to Wissett Road	Make walking, cycling and scootering to Edgar Sewter primary school a safe and healthy option for children and parents. Based on consultation with years 5 and 6 children and with parents in the walking expert group the following are the suggestions as to how to achieve this policy. The planned extension to the school gives an opportunity to make changes. The proposed rerouting of the cycle track up the West side of Norwich Road could support these changes.	Create a safe crossing (zebra, pelican, toucan, Copenhagen) across the entrance to Wissett Road – this is a busy and narrow junction with Norwich Road especially at school start and finish times, with cars backing up down Wissett Road, and cars turning into Wissett Road from both north and south into the very narrow entrance.	2	0	0	1	0	0	3	Connectivity and Growth - The A144 is a 30mph speed limit but is one of the main roads through Halesworth and therefore is subject to a lot of traffic - especially at peak times. Therefore this suggestion would create a connection to the other side of the road for pedestrians and cyclists. A score of 2 has been awarded in this instance due to the importance of creating high quality walking and cycling connections to the primary school. Modal Shift - No effect. Optimisation - No effect. Safety - Small improvement for safety, the road is often busy however traffic should be moving at 30mph. Biodiversity - No effect. Leisure - No effect.
Halesworth	750	Norwich Road School Entrance	Make walking, cycling and scootering to Edgar Sewter primary school a safe and healthy option for children and parents. Based on consultation with years 5 and 6 children and with parents in the walking expert group the following are the suggestions as to how to achieve this policy. The planned extension to the school gives an opportunity to make changes. The proposed rerouting of the cycle track up the West side of Norwich Road could support these changes.	Create a safe crossing on Norwich Road in front of the main entrance to the school – at present the nearest crossings are at the Quay Street roundabout and at Harrisons Lane (installed for the former middle school). This would support walking options from the east of the town and new developments at Harrisons Lane where walkways will enable children to commute onto Bungay Road and up the path just north of the school but on the 'wrong side'.	2	0	0	1	0	0	3	Connectivity and Growth - The A144 is a 30mph speed limit but is one of the main roads through Halesworth and therefore is subject to a lot of traffic - especially at peak times. Therefore this suggestion would create a connection to the other side of the road for pedestrians and cyclists. A score of 2 has been awarded in this instance due to the importance of creating high quality walking and cycling connections to the primary school. Modal Shift - No effect. Optimisation - No effect. Safety - Small improvement for safety, the road is often busy however traffic should be moving at 30mph. Biodiversity - No effect. Leisure - No effect.
Halesworth	751	Wissett Road	Make walking, cycling and scootering to Edgar Sewter primary school a safe and healthy option for children and parents. Based on consultation with years 5 and 6 children and with parents in the walking expert group the following are the suggestions as to how to achieve this policy. The planned extension to the school gives an opportunity to make changes. The proposed rerouting of the cycle track up the West side of Norwich Road could support these changes.	Create a safe crossing across Wissett Road to connect the footpath from Rectory Street to the Children's Centre entrance to school – this makes for a safe and healthier route from the South of the town along the Thoroughfare and connects with the pre-school in School Lane.	2	0	0	1	0	0	3	Connectivity and Growth - The crossing point over Wisset Road will create a new connection for children and parents walking to and from the Edgar Sewter Primary School. Therefore a score of 2 has been awarded due to the importance of having high quality walking and cycling connections to the primary school. Modal Shift - No effect. Optimisation - No existing infrastructure. Safety - The improvement will have a benefit to the safety of people crossing Wisset Road, however a score of 1 has been awarded due to the 30mph limit along this road. Biodiversity - No effect. Leisure - No effect.
Halesworth	752	Norwich Road	Make walking, cycling and scootering to Edgar Sewter primary school a safe and healthy option for children and parents. Based on consultation with years 5 and 6 children and with parents in the walking expert group the following are the suggestions as to how to achieve this policy. The planned extension to the school gives an opportunity to make changes. The proposed rerouting of the cycle track up the West side of Norwich Road could support these changes.	Make a 20mph zone along the Norwich Road in front of the main school entrance preferably from the Quay Street roundabout to The Avenue or beyond.							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Halesworth	753	Thoroughfare	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Pedestrianisation of the Thoroughfare, (Objective 7 and 6).	0	0	0	0	0	0	0	Connectivity and Growth - No new connection created. Modal Shift - No effect. Optimisation - No improvement. Safety - Stopping traffic would improve safety however, traffic is limited and moves very slowly through the Thoroughfare meaning that the current risk is not very high. Biodiversity - No effect. Leisure - No effect.
Halesworth	754	Saxons Way from Lansbury Road estate	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Dangerous crossings identified across Saxons Way from the Lansbury Road estate, which has several homes for elderly people. Make the crossing from Swans Lane, presently a central island, into a zebra or pelican.	0	0	2	1	0	0	3	Connectivity and Growth - No new connections are made with this improvement. Modal Shift - No effect. Optimisation - Upgrading existing pedestrian refuge into a pedestrian crossing will be a significant improvement to the existing infrastructure. Safety - The A144 is a 30mph speed limit but is one of the main roads through Halesworth and therefore is subject to a lot of traffic - especially at peak times. Biodiversity - No effect. Leisure - No effect.
Halesworth	755	Roman Way / London Road	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Dangerous crossings identified across Roman Way where it joins London Road which is the main route out of town to the A143 and on towards the A14. A crossing is needed to help walking from the estates down Walpole Road.	1	0	0	1	0	0	2	Connectivity and Growth - The road represents a modest barrier for pedestrians. The A144 is a 30mph speed limit but is one of the main roads through Halesworth and therefore is subject to a lot of traffic - especially at peak times. Therefore this suggestion would create a connection to the other side of the road for pedestrians and cyclists. Modal Shift - No effect. Optimisation - No effect. Safety - Small improvement for safety, the road is often busy however traffic should be moving at 30mph. Biodiversity - No effect. Leisure - No effect.
Halesworth	756	Halesworth	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Styles, gates and seats – better designs required to make walking in the countryside easier for the less mobile and more seats around town to encourage more walking to shops etc. (advice needed on what a NP can do on this)	0	1	0	0	0	1	2	Connectivity and Growth - No effect. Modal Shift - Small improvement to Modal Shift as these improvements will facilitate walking and cycling for people with mobility constraints. Optimisation - No effect. Safety - No effect. Biodiversity - No effect. Leisure - Improvements will allow people to further utilise the leisure attractions in the Town Centre.
Halesworth	801	footpath between Uplands Way and Norwich Road	Link residential areas to the main town destinations and NCR1 - Upgrade the footpath between Uplands Way and the Norwich Road alongside the school fence to create a shared pedestrian and cycle path, with signage. This would connect the Chichester Road		0	0	2	0	-1	2	3	Connectivity and Growth - Connection already available via existing footpath. Modal Shift - No effect. Optimisation - Widening, resurfacing and Upgrading the existing footpath into a shared pathway that can be used by cyclists and walkers will be a significant improvement to this area of the Town.

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			residential area, support cycling to school, help cyclists coming from the Wissett area to avoid the dangerous and steep Wissett Road.									Safety - No effect. Biodiversity - Potential loss of grassland and potential slight impact on existing hedge from widening and resurfacing the route. Leisure - The improvement will link close to the Throughfare in Halesworth which is as well as providing reasonable connections to the Healthy Neighbourhood allocation meaning a good score is given.
Halesworth	802	Loam Pit Lane	Link residential areas to the main town destinations and the NCR1	Loam Pit Lane - make it into a cycle route so connecting Harrisons Lane to Holton Road, serving the new housing on Harrisons Lane (planning permission given), connecting the new sports centre on the Campus site, with a planned cycle route within it, into Loam Pit Lane. This may partially utilise/link into the development intentions by Hopkins Homes Ltd at Blyth Vale (off Hill Farm Road), by linking across the west end of the cemetery and Loam Pit Lane, onto open space areas created by the Hopkins development, and linking towards Holton.	0	0	3	0	0	2	5	Connectivity and Growth - Existing footway (PROW 7) offers an connection for pedestrians to use but is not suitable for cyclists. Modal Shift - No effect. Optimisation - Upgrading, resurfacing and potentially widening the existing path way to create segregated walking and cycling paths will be a significant improvement to the existing infrastructure. Safety - No effect. Biodiversity - Likely no effect however if the path is required to be widened it may have an impact on the surrounding grassland. Leisure - This improvement will link residents into the proposed leisure facilities set out in the allocation site. Moreover, it will improve access to Halesworth Town Centre where a large number of leisure facilities are located.
Halesworth	803	Briar close	Link residential areas to the main town destinations and the NCR1 - Improve access into Briar Close and the route to the Station by improving the pavement under the railway bridge and its connection to the end of Loam Pit Lane.		2	0	2	0	0	1	5	Connectivity and Growth - Improving the pathway to allow cyclists will ensure better access to the train station and a connectivity benefit. Modal Shift - No effect. Optimisation - Upgrading existing footway into a shared cycle path would be a significant improvement for this part of the Town. Safety - No effect. Biodiversity - No effect. Leisure - This improvement will provide a modest increase connectivity to Halesworth Town Centre where a variety of leisure facilities are located.
Halesworth	804	Hill Farm Road Development	Link residential areas to the main town destinations and NCR1	Hill Farm Road development – create a path from this new estate and the proposed playground west into Loam Pit Lane to connect to the proposed new path east to Holton Orchards Road so improving cycling access to and from the east of town and from Holton.	3	0	0	0	-1	1	3	Connectivity and Growth - This improvement will create a new off road connection between the East side of Halesworth and Holton. Furthermore, this route will add to the existing infrastructure to create better connectivity between Halesworth Town Centre and Holton. Modal Shift - No effect. Optimisation - No existing infrastructure. Safety - No effect. Biodiversity - Potential loss of agricultural land/grass land. Leisure - This improvement will provide a modest increase in connectivity into Halesworth Town Centre and facilities within the Neighbourhood.
Halesworth	805	new development at Chediston Street/Roman Way	Link residential areas to the main town destinations and the NCR1	The proposed new development at Chediston Street/Roman Way includes suggestion for a cycle route up Chediston Street into the town centre. This is considered dangerous and an alternative route should be planned. From the estate a route should be created into Allington Road. This makes best use of the contours of the land and connects into Dukes Drive near to the bus stop. It would then cross Roman Way to connect to the existing cycle route in Holmere Drive and into Church Farm Lane. This creates a relatively safe cycling route into the Market Place and town centre via the quiet northern end of London Road around the St Mary's Church yard.	0	0	2	0	0	1	3	Connectivity and Growth - Existing pedestrian footway is in place that provides a connection to the cycling infrastructure along the B1123. Modal Shift - No effect. Optimisation - Upgrading the existing pedestrian footway into a shared pathway for cycling and walking would be a significant improvement to the existing infrastructure. This would create a continuous cycling connection between WLP4.2 and Halesworth Town Centre. Safety - There would be a slight improvement to safety with this improvement, however the road speed is 30mph and this stretch of road is quieter than other areas of Halesworth and therefore a neutral score has been allocated to reflect this. Biodiversity - No effect. Leisure - This improvement will create a better cycling connection towards the town centre, however it is not direct so a modest score is deemed reasonable.
Halesworth	807	Wissett Road junction	Create a direct and safe 'key movement' cycle route from the Sparrowhawk Road roundabout in the north to the Bramfield Road/London Road intersection in the South	The Wissett Road junction should be made into a Copenhagen style junction giving priority to cyclists and pedestrians. This would encourage safer cycling to the primary school by children and parents.	2	0	0	1	0	0	3	Connectivity and Growth - Implementing a pedestrian prioritised roundabout will create a safe crossing of the often busy A144 for pedestrians. Modal Shift - No effect. Optimisation - No effect. Safety - The improvement will provide a safe crossing of the A144 for pedestrians. This road is 30mph however it is usually busy and therefore a score of 1 is appropriate. Biodiversity - No effect. Leisure - No effect.
Halesworth	808	River Lane	Improve cycling connectivity into the Town Park and the Millennium Green which has NCR1 running through it and out into the countryside beyond.	Make River Lane into a cycle route. This would connect the Angel Link roundabout on Saxons Way to the park. The bridge over the river is too narrow at present but could be given pedestrian right of way or cyclist dismount signs until the bridge can be widened. River Lane is a private road and is unregistered on the land registry, so investigations needed to start the process of redesignation.	2	0	0	0	0	2	4	Connectivity and Growth - This improvement will create a link between Millennium Green and Angel Link and then into Halesworth Town Centre. Modal Shift - No effect. Optimisation - This improvement is related to a new piece of infrastructure. Safety - No effect. Biodiversity - No effect. Leisure - This improvement will create a link between Millennium Green and Angel Link and then into Halesworth Town Centre where a variety of leisure facilities are located.
Halesworth	809	Blyth Mews / Quay Street	Improve cycling connectivity into the Town Park and the Millennium Green which has NCR1 running through it and out into the countryside beyond.	Make Blyth Mews off Quay Street into a cycle route with appropriate signage. The bridge over the Patrick Stead Lock at the end of Blyth Mews would benefit from improving with 'cyclists give way to pedestrians' signs, if funding is not available for widening). Look at how the Blyth Mews/Quay Street cycle and pedestrian routing could be improved so safer/easier access could be made into Station Road, giving access to the railway station and 'The Cut'. If the car showroom site opposite Blyth Mews was developed this could give an opportunity.	0	0	2	0	0	2	4	Connectivity and Growth - This improvement will look to improve an existing footway and therefore will be scored under optimisation. Modal Shift - No effect. Optimisation - Upgrading and widening the existing footway to support cycling infrastructure will be a significant improvement for this area of Halesworth. Safety - There would be a slight improvement to safety with this improvement, however the road speed is 30mph and this stretch of road is quieter than other areas of Halesworth and therefore a neutral score has been allocated to reflect this. Biodiversity - No effect. Leisure - This improvement will increase connectivity to Halesworth Town Centre where a variety of leisure facilities are located.
Halesworth	810	The Folly / Millennium Green	Improve cycling connectivity into the Town Park and the Millennium Green which has NCR1 running through it and out into the countryside beyond.	Create a new cycle route through the Folly which is a part of and the Millennium Green. This would connect Holton Road opposite the new housing development at Hill Farm Road into the Millennium Green and into the town centre or out on the NCR1. Footpaths off the	0	1	2	1	-1	2	5	Connectivity and Growth - This improvement looks to upgrade a number of Public Rights of Way (PROW) to create a cycle connection parallel to the B1123 and therefore will be scored under Optimisation. Modal Shift - Holton Road has a PCT score of 45 which suggests that this improvement will have a modest effect on Modal Shift along the B1123. Optimisation - Upgrading, widening and resurfacing the

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				Holton Road (towards 'Rails End') would need to be redesignated as cycle routes. The owners would need to be approached. Routes into and then through to the town park and Millennium Green also give an alternative to the Saxon Way route for less confident cyclists and a route out to the east of town.								existing PROWs to accommodate effectively will be a significant improvement to this area of the Town. Safety - This improvement will divert cyclists off the busy B1123 which will have a positive impact on safety. This stretch of road is 30mph which is reflected in the score for this category. Biodiversity - Potential loss of grassland from widening and resurfacing the existing path. This area consists of a mature trees that could potentially also be affected by an increase in footfall. Leisure - This improvement will increase connectivity to Halesworth Town for Holton.
Halesworth	811	Roundabout at Quay Street up the Norwich Road to Sparrowhawk Road	Create a direct and safe 'key movement' cycle route from the Sparrowhawk Road roundabout in the north to the Bramfield Road/London Road intersection in the South. This would reduce the 'inconsistent and confused approach for cyclists and pedestrians' and thereby reduce conflict for all users' as they navigate the Town Centre (Waveney Local Plan). Rerouting of NCR1 would be needed.	Cycle route from the roundabout at Quay Street up the Norwich Road should be on the west side of the road. The partial and inadequate cycle route that goes up to Harrison's Lane on the east should be decommissioned as dangerous. The west side of the road would solve some of the issues for children cycling to school. At present they cannot cross safely from the present cycle route to the school. Poor parking on the west side of Norwich Road (from Edgar Sewter Primary School to 'The Avenue'), caused by overspill from the Police Station, businesses in town, and by parents dropping children off at school, would need to be resolved. This route would become a re-routed NCR1 doing away with the confusing route down Harrison's Lane into Holton and then up to Sparrowhawk Road. At Sparrowhawk Roundabout the NCR1 route could go up the road in front of the Triple Plea pub and join the present NCR1 route at Butts Road in a more direct and straightforward route towards the railway Mill Post Crossing.	2	2	0	1	0	2	7	Connectivity and Growth - Moving and extending the length of the cycle path will improve connectivity for houses in the Northern part of Halesworth. Modal Shift - Uplift of 116 would be achieved with improvements to this road. The town centre would be linked to the employment area (WLP4.6) in the North. Optimisation - This comment is focused on creating a new cycling path on the East of the A144 to extend further North and the removal of the existing path to the East. Safety - A low score has been given due to the fact that the speed limit along this stretch is 30mph. However, this is a heavily used road with a considerable amount of traffic meaning that a score of one has been given to reflect this. Biodiversity - No effect. Leisure - The improvement will link close to the Throughfare in Halesworth which is a major leisure feature in East Suffolk and therefore a high score has been given in regards to leisure benefit.
Halesworth	812	Saxons Way	Create a direct and safe 'key movement' cycle route from the Sparrowhawk Road roundabout in the north to the Bramfield Road/London Road intersection in the South.	The pavements along Saxons Way, from Quay Street roundabout to the Coop/London Road roundabout should become safe, shared cycle and pedestrian paths. (the east side of Saxons Way may be the best option as it links with the proposed east side route on London Road and would not impinge on the entrance to the new development on the west side or the entrance to the car park). The Saxons Way route would remove the confusing one way cycling in the Thoroughfare and the dismount instruction at the southern end of the Thoroughfare. The route should then continue along the eastern side of London Road to the turning with Bramfield Road (the main route into Halesworth from the A12)	3	1	2	1	0	2	9	Connectivity and Growth - Saxon's Way (A144) forms of part of the spine road that travels through the Heart of Halesworth and therefore implementing cycling infrastructure along the A144 will provide a key connection to the centre of Halesworth. Moreover, this improvement will link into existing cycling infrastructure further north along the A144. Modal Shift - Score of 1 has been attributed to Modal Shift due to the PCT score of 65 for this stretch of road. Optimisation - This improvement will upgrade an existing footway into a shared pathway allowing use by cyclists in a key location. Safety - Small improvement for safety, the road is often busy however traffic should be moving at 30mph. Biodiversity - No effect. Leisure - The improvement connects into the centre of Halesworth providing users access to many leisure facilities and therefore is given a high score to reflect this.
Halesworth	813	Thoroughfare / Bridge Street	Reroute the NCR1 away from the Thoroughfare / Bridge Street. The rerouting of NCR1 away from the Thoroughfare/Bridge Street between the Quay Street and the entrance to the car park removes a confusing and badly signposted national route from a semi pedestrianised shopping street and allows for the Thoroughfare to become safer and more pedestrianised route.		0	0	0	0	0	-2	-2	Connectivity and Growth - No effect, the loss of the cycle route on the Thoroughfare will be re - routed to ensure the existing connection remains. No new connections will be created. Modal Shift - No effect. Optimisation - No effect. Safety - No effect, this category is primarily concerned with conflict with vehicles. Biodiversity - No effect. Leisure - The Thoroughfare features a large number of services and facilities that attract visitors to Halesworth. Directing cyclists away from the Thoroughfare will have a significantly negative effect on Leisure.
Halesworth	814	Market Place	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance cyclist groups to use Halesworth as a stop off destination.	An option is to provide E-chargers along the wall of the 'Boarding House' café, where there are currently market stallholder electrical outlets already provided. An alternative could be along the wall of the Wine Shop/public toilets on the opposite side of the Market Place.	0	1	1	0	0	2	4	Connectivity and Growth - No effect. Modal Shift - Score of one awarded for Modal Shift, cycle parking provision alone is unlikely to facilitate a large modal shift however it will have a positive impact. Optimisation - The cycle parking provision will facilitate the use of the existing infrastructure within Halesworth. Safety - No effect. Biodiversity - No effect. Leisure - Score of 2 has been awarded for Leisure due to the location of the comment. Halesworth Town Centre is identified as a market town in East Suffolk and provides a range of leisure services and facilities that attract visitors.
Halesworth	815	Central (main) Thoroughfare carpark	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance cyclist groups to use Halesworth as a stop off destination.	There is a substation in the central carpark, plus numerous businesses, that potentially could facilitate E-charging points. Ideally these could be along the river side wall (north) of the car park.	0	1	1	0	0	2	4	Connectivity and Growth - No effect. Modal Shift - Score of one awarded for Modal Shift, cycle parking provision alone is unlikely to facilitate a large modal shift however it will have a positive impact. Optimisation - The cycle parking provision will facilitate the use of the existing infrastructure within Halesworth. Safety - No effect. Biodiversity - No effect. Leisure - Score of 2 has been awarded for Leisure due to the location of the comment. Halesworth Town Centre is identified as a market town in East Suffolk and provides a range of leisure services and facilities that attract visitors.
Halesworth	816	Angel Link carpark	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance	Discussions highlight this car park as considerably underused, and there remains the potential for a bus terminus here, despite past failed attempts (which should be refreshed). In addition there is plenty of scope here for E-chargers to be positioned in numerous places, to attract town centre visitors to use this under-	0	1	1	0	0	2	4	Connectivity and growth - No effect. Modal Shift - Score of one awarded for Modal Shift, cycle parking provision alone is unlikely to facilitate a large modal shift however it will have a positive impact. Optimisation - The cycle parking provision will facilitate the use of the existing infrastructure within Halesworth. Safety - No effect. Biodiversity - No effect. Leisure - Score of 2 has been awarded for Leisure due to the

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			cyclist groups to use Halesworth as a stop off destination.	utilised space. An ideal position might be along the boundary to the Angel Hotel private carpark. Alternatively, there could be scope for E-chargers in what I believe is called 'Angel Lane South' carpark behind the EACH charity shop.								location of the comment. Halesworth Town Centre is identified as a market town in East Suffolk and provides a range of leisure services and facilities that attract visitors.	
Halesworth	817	Bridge Street	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance cyclist groups to use Halesworth as a stop off destination.	Cyclists would benefit from the addition of perhaps a 3-4 cycle toast rack positioned on the town river bridge, which is the widest section of the main street. If carefully positioned on the upstream side of the bridge, it was felt these wouldn't encroach on vehicular flows or the pedestrian access across the bridge and viewing the river.	0	1	1	0	0	2	4	Connectivity and Growth - No effect. Modal Shift - Score of one awarded for Modal Shift, cycle parking provision alone is unlikely to facilitate a large modal shift however it will have a positive impact. Optimisation - The cycle parking provision will facilitate the use of the existing infrastructure within Halesworth. Safety - No effect. Biodiversity - No effect. Leisure - Score of 2 has been awarded for Leisure due to the location of the comment. Halesworth Town Centre is identified as a market town in East Suffolk and provides a range of leisure services and facilities that attract visitors.	
Halesworth	818	Market Place	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance cyclist groups to use Halesworth as a stop off destination.	There is currently a 3-4 bike toast rack store adjacent to the Market Place pump. The storage capacity could be significantly boosted in the Market Place, possibly by taking up a parking bay adjacent to the Wine Shop. This would provide enough space for a 10 (or more) bike toast rack.	0	1	1	0	0	2	4	Connectivity and Growth - No effect. Modal Shift - Score of one awarded for Modal Shift, cycle parking provision alone is unlikely to facilitate a large modal shift however it will have a positive impact. Optimisation - The cycle parking provision will facilitate the use of the existing infrastructure within Halesworth. Safety - No effect. Biodiversity - No effect. Leisure - Score of 2 has been awarded for Leisure due to the location of the comment. Halesworth Town Centre is identified as a market town in East Suffolk and provides a range of leisure services and facilities that attract visitors.	
Halesworth	739a	Halesworth	Comment 306	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).							N/A	See comment 306 for a full assessment	
Halesworth	739b	Halesworth	Comment 303	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).								N/A	See comment 303 for a full assessment
Halesworth	739c	Halesworth	Comment 302	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs								N/A	See comment 302 for a full assessment

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
				20mph zones and traffic calming to make it safer to walk and cycle around (304).								
Halesworth	739d	Halesworth	Comment 480	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).							N/A	See comment 480 for a full assessment
Halesworth	739e	Halesworth	Comment 304	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Hemley	733	Newbourne, Hemley and Waldringfield	The lanes out towards and through Newbourne, Hemley and Waldringfield need to be 'quiet lanes'. Maybe they could be for access only by cars.		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because requests for Quiet Lane designations have been dealt with separately.
Hollesley	69	road from hollesley village (rectory road) , moors farm corner to shingle street.	The road to Shingle Street from Moors farm, which is a minor road, has 5 very dangerous blind corners, yet it is sign posted at national speed limit. This road has become very busy with walkers and cyclists (including many children), horse riders and dog walkers, tourists including campervans, 'boy racers' and large heavy vehicles. It also includes a national cycle way and is used as a Duke of Edinburgh Award walk. Further information on request as I have lived on this road for 35 years.	Reduce speed limit to 30 or less and please look at the corners before their is fatalities							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Hollesley	78	Alderton Road just out side Hollesley	As soon as motorists leave the 30 mph zone they accelerate hard to the full 60 mph. Pedestrians have no protection. There are no pavements, the agricultural vehicles are destroying the verges and there are no footpaths through the fields that could be used as alternatives. The road is so narrow and the vehicles so fast (even the tractors drive at 60mph here and they're HUGE) that we don't dare let our 14 year old out on the road on her bike.	This is your job, not mine. Widen the road? Reduce the speed limit? Ban agricultural vehicles of a certain size or power from public roads? Build pavements?	0	0	0	2	-1	0	1	The provision of a safe refuge area where the speed change occurs has been assessed. Connectivity and Growth - Providing an area of safe refuge where the speed limit changes does not create significant connectivity and growth. Modal Shift - Providing an area of safe refuge where the speed limit changes does not create significant modal shift as it does not provide a cohesive route to important locations. Optimisation - This suggestion does not represent an optimisation. Safety - Providing a area of refuge in a potentially hazardous area scores well for safety, however any refuge is temporary to doesn't obtain the full score. Biodiversity - Any improvement will likely require the removal of unmanaged grass so obtains a modest minus score. Leisure - There are limited leisure benefit.
Hollesley	111	Sutton Hoo to Hollesley Village (Melton Road/Heath Road)	Road is unsafe for cyclists due to large volume of fast traffic. As the road is straight it gives the impression that you can drive fast. It is undulating and very narrow. Alternative routes to Hollesley or Hollesley Common are a long way round.	A separate lane for cyclists. Maybe through the forest or making use of bridleways across Sutton Common (with surface for normal bikes).	1	1	1	2	-1	2	6	Connectivity and Growth - Connecting the villages of Boyton and Hollesley to Melton/Woodbridge with their high levels of services could score highly, but the distance between the villages means it is unlikely to be highly used for day-to-day use so the score should lower to reflect this. Modal Shift - Using Heath Road as a guide, PCT suggests if this road is approved to a high standard there is a modest potential for modal shift and the bridleways/byways provides this as an equivalent. Optimisation - Parts of the forest are already either bridleways or byways (whether available to cyclists needs to be ascertained) so these can be optimised with a mixture of surfacing and legal upgrading. Safety - Heath Road is largely a 40mph albeit straight with reasonable visibility. A score of 2 is considered reasonable. Leisure - Creating an attractive off-road cycle route will provide a leisure destination in its own right.

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
Hollesley	130	Street between Duck Corner and Woodbridge Walk, Hollesley	main road between two parts of the village, but no cycle or footpath. Both parts of the village are within a cycling distance but the 60mph speed limit and no pathways make it too dangerous. Has been spoken about for at least twenty years but no positive outcome.	Some cycle or footpath to allow people to safely walk from one part of the village to another.	2	1	0	3	-3	0	3	Connectivity and Growth - The properties on the junction on Boyton Road and those further eastwards along Woodbridge Walk are significantly cut off from the services in Hollesley, providing these connections should score highly. A score of 2 has been given in recognition that some connectivity, albeit indirect does exist through footpath 37. Modal Shift - PCT suggests limited potential for modal shift for cycling, however a new footpath would allow the small numbers of properties to the north access to regular services so a score of 1 has been given. Optimisation - This would not represent an optimisation. Safety - A fast moving road that necessitates use with high foliage either side means the improvement is beneficial. Biodiversity - Any potential improvement along Duck Corner would result in significant loss of adjacent hedgerows scoring a high minus number here. Better utilisation of footpath 37 provides an alternative, but this is indirect. Leisure - A path along Duck Corner would suggest a more day-to-day route over that of leisure use.
Hollesley	209	The road to Shingle Street	The road is very congested and during the summer a huge number of cars park on the verges, ruining the unique beauty of the beach and marshes. It is difficult and dangerous for walkers and cyclists to navigate the traffic.	The road should be used by vehicles only for access to the homes at Shingle Street. Visitors should be required to park at the Shepherd & Dog pub or the Suffolk Punch Trust and walk or cycle to the beach. Bikes and trailers could be offered for hire to raise funds for the community, and the Trust, village shop and pub would also benefit from increased footfall in the village.	0	0	0	2	0	2	4	Connectivity and Growth - Closing Shingle Street to all but access only will help sustainable connectivity for the residents. However Shingle Street has a low population and closure of the Shingle Street Road will not create a full route to nearby services. Modal Shift - This category is concerned with everyday trips to which there will be a limited number and again the improvement will not provide a cohesive route to the services. Optimisation - This does not represent an optimisation of the existing cycling or walking infrastructure. Safety - The road is national speed limit, although likely quiet outside of peak times. A score of 2 is deemed reasonable here by significantly reducing car numbers at peak times. Biodiversity - No significant biodiversity impact. Leisure - Providing a safer and attractive route to the coastal village is considered to have a good leisure impact.
Hollesley	307	The entire stretch of 'The Walks' plus Sutton Road to Wilford Bridge roundabout.	Very busy, fast, unsafe traffic, yet this is one of two main access routes to/from the peninsula for cyclists.	With a large proportion of the land to the north of The Walks being publicly-owned (Forestry Commission), there is surely an opportunity to establish a safe all-season paved cycle (and walking) way through the forest between the peninsula villages (notably Boyton/Hollesley) and Melton. This would encourage commuting to Melton/Woodbridge/the stations by bicycle, and would also increase recreational cycling by families daunted by the busy main road.	1	1	1	2	-1	2	6	Connectivity and Growth - Connecting the villages of Boyton and Hollesley to Melton/Woodbridge with their high levels of services could score highly, but the distance between the villages means it is unlikely to be highly used for day-to-day use so the score should lower to reflect this. Modal Shift - Using Heath Road as a guide, PCT suggests if this road is approved to a high standard there is a modest potential for modal shift and the forest path provides this as an equivalent. Optimisation - Parts of the forest are already footpaths and bridleways so these can be optimised with a mixture of surfacing and legal upgrading. However, it has not scored higher as the full route would require new footpaths/bridleways. Safety - Heath Road is largely a 40mph albeit straight with reasonable visibility. A score of 2 is considered reasonable. Biodiversity - Leisure - Creating an attractive off-road cycle route utilising the forest will provide a leisure destination in its own right.
Hollesley	398	The level of traffic on the small lane to Shingle Street	It is dangerous to walk down this lane to Shingle Street in the summer months because of the number of visitor cars to the area. It is a popular route for walkers, local families, rambler groups, D of E groups to visit Shingle Street. The large volume of cars using the lane makes it very dangerous for non-vehicle users because it is narrow, with unmarked 90 degree bends and there is nowhere to escape if a car travelling too fast or misjudges the space available to safely pass	Register the lane under the Quiet Lane Scheme. Mark out on the road surface a lane for walkers/cyclists to reduce the speed of the cars by highlighting the lack of space for the cars to pass other users Ban cars parking from the bridge down to Shingle Street, except resident vehicles during the summer months.							N/A	Quiet Lanes are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Hollesley	625	At Red Lodge, where the road becomes bordered by the wood	As noted in other comments, this is a very fast section of road, popular with cyclists. I have been witness to near misses with cars on a number of occasions. The change in light as a result of coming into or leaving the trees, leaves cyclist or other road users extra vulnerable.	Signage or road markings to highlight this would be of benefit.	0	0	0	1	0	0	1	Connectivity and Growth - The addition of advisory signage is not considered to create significant connectivity or growth. Modal Shift - The addition of advisory signage is not considered to create significant modal shift. Optimisation - The addition of signage will not optimise existing cycling infrastructure. Safety - The provision of an advisory sign will have a modest safety benefit. Biodiversity - This would not have a significant biodiversity benefit. Leisure - No significant leisure benefit
Holton	198	There is currently no safe or semi-direct safe route for cyclists or walkers between Halesworth and Walberswick/Southwold	There exists currently an 'unsurfaced' footpath running in most parts alongside the River Blyth from Halesworth to Walberswick and then on to Southwold via the river 'Bailey Bridge'. This tends to become overgrown in spring and summer months. It follows a similar path to the ex-Southwold railway track bed (disused and removed early 1900's).	It is suggested that this route be the basis for an improved combined cycle and walkway between these two market towns. This would provide such benefits as alleviating considerable traffic and parking from Southwold and Walberswick, and sharing the abundant tourist and leisure opportunities available at these and along the whole route as it passes through beautiful Suffolk countryside and wildlife. An additional significant benefit is that Halesworth already lies on the Sustrans NCN route 1, plus the benefit of the national rail network, and so passing cycle and rail traffic can detour easily towards the coast. This would require safe provision of a crossing of the A12 at Blythburgh.	0	0	3	0	-3	3	3	Connectivity and Growth - although there is an existing connection between Halesworth and Southwold via a PROW footway, it is currently not complete whilst completing the path will provide additional connectivity the distance between the 2 settlements means day-to-day trips are unlikely. Modal Shift - no significant effect. Optimisation - Upgrading the existing PROW to a bridleway to accommodate cycling and walking. Furthermore, the route would require widening and resurfacing to support cycling effectively. Safety - no significant effect. Biodiversity - This improvement will result in the loss of some biodiversity due to the scale of the improvement and the sensitive area it is located in. Leisure - This improvement will create an attractive route between two market towns in the District and therefore will provide a significant benefit to leisure.
Holton	309	Holton - Triple Plea road from Sparrowhawk Road/Norwich Road A144 roundabout, towards Butts Road (NCR1)	The NCR1 route from Halesworth heading north through Holton, currently is quite complex in places, and if other suggested improvements to north-south routes through Halesworth take place, NCR1 would need slight re-routing from Sparrowhawk Road (Triple Please roundabout) to link up to Butts Road where NCR1 then heads north via the railway Mill Post Crossing towards Westhall and Bungay.	Suggest a crossing from Sparrowhawk Road near the Triple Plea pub to safely cross/cycle onto Triple Plea Road, then signing Triple Plea Road as NCR1 cycle route to the junction with existing NCR1 at Butts Road heading north. This would link the proposed Halesworth area cycle way improvements back onto NCR1 heading north towards Bungay, and vice versa improve cyclist access south to the business and industrial areas at the north end of the town.	2	0	0	1	0	0	3	Connectivity and Growth - The crossing point over Sparrowhawk Road will create a better connection for cyclists to access the A144 and into Halesworth Town Centre. Modal Shift - No effect. Optimisation - This comment is in relation to a new piece of infrastructure. Safety - This improvement will create a safe crossing over Sparrowhawk Road and divert cyclists away from the Sparrowhawk roundabout. This would result in a positive impact to pedestrian safety. Biodiversity - No effect. Leisure - No effect.
Holton	311	Halesworth - suggested new waymarked county cycle loop (Halesworth, Beccles and Bungay)	This suggested loop follows all back/minor roads and links three prominent market towns, plus would join the route from Beccles to Southwold at Stoven/Sotterley. It would enable joining the loop by train links at either Halesworth, Brampton or Beccles	The originator has navigation files that could be used to illustrate and publicise this route which is a family-safe and beautifully scenic route that can be done in parts or as a whole (total 35-40 miles). Heads north from Halesworth through Holton, Brampton, Stoven, Sotterley, Ellough, Beccles, Ringsfield, Ilketshall St Andrews, Mettingham, Bungay, St Peters, St Margarets,	0	0	0	0	0	0	0	The issue and recommendation provided has been considered in the creation of the strategy, however it is too broad in scope to be realistically and effectively scored against the methodology

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				Rumburgh and back to Halesworth. Granting of a formal route number and signage would be required - navigation files are available for this very safe route that also piggy-backs a part of NCR1.								
Hoo	168	Chimer Lane/Hall Lane/Honeypot Lane junction near Charsfield	This whole area not just this confluence of c -roads is an exceptionally rich completely rural area which offers outstanding cycling. The nature of the roads is that of restricted width and with many blind bends. Unfortunately motorists seem to think it is a racetrack and often are moving at unsafe speeds for cyclists. At least once in last month I have been almost brushed by a passing car at speed, unsafe for him/her and me	The diversity of nature is outstanding in this area. Just today cycling that route I encountered a young stag with approximately 8 points on his antlers, several buzzards, hunting; various other birds and rabbits. An upper speed limit of 40mph on such roads whilst not making them safe would reduce some of the risk. Could we have a countryside limit please in Suffolk or lobby for such nationally on roads of a diminished width?							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Iken	472	Alde River wall east of Iken Church (TM412567 - TM443556)	This is another section of river wall that should be opened to the public as a public footpath to link Iken Church with Public Footpath Iken 7. We are recommending to Natural England that it becomes part of the England Coast Path.	A Creation Order or Agreement is needed.	0	0	0	0	0	3	3	Connectivity and Growth – This proposal will have more leisure benefit than that of connectivity. Although the proposal will connect two existing footpaths, it provides limited connections to other villages and services, hence a neutral score. Modal Shift – Insufficient evidence to suggest that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – No safety benefit. Biodiversity – No significant biodiversity impact. Leisure – The route represents a strong leisure route adjacent the river and within the AONB designation
Ilketshall St Lawrence	481	The high street and the A143 junction	We live between bungay and spexhall,we have no pathways at all,it would be fantastic to have a walkway or cycle path put in between bungay where we do our shopping and spexhall where our local public house is situated that we use for social events,I cycle but feel very unsafe riding on the main road as it is very dangerous,my partner has a mobility scooter that she could never use between these two points on the map,so we have to always use the car but would much rather use our cycle and scooter	Pathway or cycle lane from bungay to spexhall along the A143	2	1	0	3	-3	1	4	Connectivity and Growth - The improvement connects Ilketshall St John, Ilketshall St Lawrence and Spexhall to the services in Bungay giving villages with limited services to a market town. A score of 2 is deemed reasonable as the long distance (particularly for Spexhall) means many cyclists/walkers would be dissuaded from its use. Modal Shift - Datashine suggests limited pedestrian commuting. It is considered the path would get modest use so 1 point is deemed reasonable. Optimisation - No optimisation benefit. Safety - A narrow rural road at National Speed Limit means a full score is awarded here. Biodiversity - Any new pathway alongside the road would result in significant foliage removal including trees, hedgerows and unmanaged verges. Leisure - Providing connections between the villages and the attractive market town of Bungay would have some leisure benefit. However, the route itself is not considered attractive. A score of 1 is deemed reasonable.
Kelsale Cum Carlton	227	A12 Saxmundham, Carlton Lane junction	There is a cycle path across the A12 at this junction however it is not very wide and not very well laid out, it is just a path really and not suitable for cycles / mobility scooters. It is not that visible to traffic on the A12. Again crossing the A12 is perilous for experienced adult riders let alone young people wishing to cycle into Sax from the villages.	Upgrade the path, make it wider and more pronounced, improve the A12 road markings and signage to show that there is a 'cycle crossing' at this junction.	0	0	2	2	0	0	4	Connectivity and Growth - This comment is in relation to existing infrastructure so does not provide significant connectivity. Modal Shift - No significant effect. Optimisation - Widening and resurfacing the crossing would be a significant improvement to the existing infrastructure. Safety - The A12 is one of the main roads in the district and therefore is subject to high levels of traffic which is often moving at high speeds. Upgrading this crossing would provide a significant benefit to pedestrians attempting to cross the A12. Biodiversity - No effect. Leisure - No significant effect.
Kelsale Cum Carlton	362	Yoxford to Saxmundham	Cycleway alongside A12 from Yoxford to the B1121 turnoff to Saxmundham is poorly maintained or non-existent. This could provide a direct route to access important local services in Saxmundham such as the medical centre, shops and pharmacy for cyclists from Parishes to the north								N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Kelsale Cum Carlton	469	Clayhill Road, Kelsale – between the points TM 3924 6410 and TM 3965 6416 (between Saxmundham Footpths 34 and 33).	Safe connectivity is required for walkers along this road between the points TM 3924 6410 and TM 3965 6416 so that they can walk safely between Saxmundham Footpths 34 and 33.	Creation of a new footpath between these points.	2	0	0	3	-3	3	5	Connectivity and Growth - Connects PROW 33 and 34 which completes the connection for residents at East Green to access Saxmundham Town Centre. Modal Shift - No effect. Safety - National speed limit, no road markings, rural road, narrow road, and tight bend. A score of 3 is considered reasonable. Biodiversity - The proposal will result in the loss of a number of mature trees and an established hedge. Leisure - Links to Saxmundham town centre through attractive rural fields.
Kesgrave	29	Main road kesgrave	Cycle track not fit for purpose, especially around Windrush Road where potholes on road are dangerous. Very uneven and old cycle track surface, many cyclists forced to use Road.	Resurface section from police station to Kesgrave fisheries.	0	3	1	1	0	0	5	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – Improving the pathway here to the highest standard (segregated cycle lane) will provide a significant modal shift and would score 3 points. Optimisation – Moving from a shared path to a segregated cycle track from pedestrians is deemed to provide 2 points. Safety – The cyclists are already separated from the road and whilst the comment suggests it is in a poor condition this is more of a maintenance issue, improving the pathway doesn't significantly improve safety. Biodiversity – No significant biodiversity benefit. Leisure – The pathway exists already and whilst it connects into Ipswich which has leisure benefits it is a long path and appears largely for commuter purposes, so no score is given.
Kesgrave	63	Main road Kesgrave from Martlesham to Ipswich hospital	You talk about cycling strategies to improve access- I have reported this many times over the years about the poor state of the cycle path and poor condition potholed surface on Kesgrave to Ipswich main road cycle path. It's simple- improve cycling numbers by providing Dutch style standard surfaces to cycle on. No more cycle repairs due to rubbish poorly maintained cycle paths like this one!!!!	I've mentioned this as above							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Kesgrave	67	Grange Farm Cycle way	Very poorly maintained and by end of summer is badly overgrown. Additionally people enter the combined Cycle / walkway from hidden junctions.	Need a better maintenance and clearance so its possible to see people entering the cycle track.							N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.

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Kesgrave	129	Footpath between Longstrops and Dobbs lane	Increase and improve cycle network	Turn footpath into bridleway and if need be turn bridleway into footpath - suitability is the opposite of designation.	2	2	0	3	-1	1	7	Paths 49 and 50 are already bridleways, the commenter states in some cases where unsuitable reversing bridleways into footpaths could be considered. Footpath 43 and 23 are footpaths only. For the purposes of this assessment changing footpath 23 and 43 into bridleways have been considered. Connectivity and Growth – The alterations would allow cyclists north and bypassing much of Dobbs Lane which is not a suitable cycle route. Most people using this path for connectivity purposes will be within the residential areas in south Kesgrave. The alternative is to use the residential streets to reach the north of Dobbs Lane and the school. This means there are some connections available despite the high use according to PCT limiting the score to 2. Modal Shift – PCT suggests that Dobbs lane would experience significant Modal Shift Growth should in be improved to a high standard. It appears to be a strong commuter route from Ipswich via Foxhall Lane. If using Bridleway 49 all the way through to the north of Dobbs Lane could be seen as a viable alternative it would score highly here. However, much of the route is already a bridleway so it is unclear whether improvements to the final section would attract new users onto this path. On balance it is considered a high score could be given here, but a full score may be unfeasible. Optimisation - This is a new route for cyclists so not an optimisation. Safety – Removing cyclists off Dobbs Lane has safety benefits, this is balanced against the potential for the use of other residential streets anyway. However a top score is deemed reasonable in this instance. Biodiversity – A modest minus point is deemed reasonable due to any widening of the path will likely remove some foliage. Leisure – The improvement of this section would link important leisure route to the south of Kesgrave for cyclists. Due to the other options to get to this destination a score of 1 is deemed acceptable.
Kesgrave	231	A1214 Kesgrave, Junction with Dr. Watsons Lane to Playford.	Having negotiated the Bell Lane traffic lights cyclists then have to make an unprotected right turn across traffic on this busy A road into Dr. Watsons lane when travelling to Playford and beyond.	Consider creating a short piece of cyclepath using the existing footpath' from Bell Lane at the Traffic lights, along the side of the A1214 to opposite Dr. Watsons Lane.	3	3	0	2	0	0	8	Connectivity and Growth – Despite only being a small section of road, this section does reside in the Ipswich to Melton key corridor and has, therefore, significant value. A score of 3 is considered reasonable. Modal Shift – According to PCT, if this section of the road is delivered to the highest standard, it will likely result in a significant modal shift hence a score of 3 under this category. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Despite only being a 30mph road, the A1214 is a busy road so removing cyclists off the road has safety benefits hence a score of 2. Biodiversity – The proposal will likely result in the loss of the managed grass area adjacent to the existing footpath however the loss of a small section of managed grass if not considered a significant impact. Leisure – The route will likely have more connectivity benefit than that of leisure.
Kesgrave	236	Cycle path Kesgrave Grange Lane to Bell Lane	An amazing Cycle/footpath that runs from Grange lane to Bell Lane completely traffic free, flat and well surfaced with plenty of space for both Walkers and Cyclists. An exemplar of how combined walking and cycling provision should be in modern housing developments	...Continue the off road segregated cycle path idea towards Ipswich across Rushmere heath. The current Ipswich route follows roads and requires some mixing with cars and buses and a very hilly bit near Brendan Drive.	3	3	0	2	-1	1	8	Connectivity and Growth – The proposal will likely have significant connectivity benefit - not only would the proposal connect into the existing cycling and walking infrastructure, which provides a route through Kesgrave to Martlesham, but the proposal also resides in the Ipswich to Melton key corridor. A score of 3 is considered reasonable. Modal Shift – The proposal would provide a safe off-road route which can be used as an alternative to the A1214 which, according to PCT, would have a significant modal shift if cycling infrastructure is delivered to the highest standard. Therefore, a score of 3 is considered reasonable. Optimisation – Despite Rushmere Heath already containing existing footpaths, the proposal will result in a new route for cyclists, therefore it is not considered an optimisation. Safety – The proposal can be used as an alternative to the use of the A1214, or Woodbridge Road, which, despite being a 30mph road, is busy. A score of 2 is considered, therefore, reasonable. Biodiversity – There are existing footpaths through Rushmere Heath (Rushmere golf course), therefore it is unlikely that the suggested improvements will result in significant biodiversity loss. However, as these footpaths will need to be widened and resurfaced, a small negative score is deemed reasonable. L – The proposal would connect to the existing leisure route south of Kesgrave and a route through the heath will likely be considered attractive, therefore a small score of 1 is considered reasonable under this category.
Kesgrave	290	The service road/cycle lane that runs the southern length of Main Road A1214 along the settlement boundary of Kesgrave.	The cycle path was created from a service road with pedestrian access to shared cycle use. Due to neglect it is unfit for purpose and is dangerous and therefore unused. The surface is poor and the many side roads are hazardous. Cars frequently drive straight out over the cycle path exiting shops/garages. Give Way signs have worn away or are non-existent. Cars park on it (esp near shops and school) again making the case for cyclists to choose the main road.	This is a golden opportunity to do something to put cycling and walking at the centre of transport policy for the future while not actually preventing other road users having access. The land is there to be properly utilised and turned into a modern cycling freeway on a major through route into Ipswich. It needs real imagination and investment.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Kesgrave	291	Long Strops Bridleway, Kesgrave	This is a 2.2km bridleway and walking route with rough surface cycle tracks. This could provide an opportunity for a major cycling through route path to Ipswich.	This is an opportunity to provide a cycling route along the length of Kesgrave which if coordinated with neighbouring villages could be part of a through route from Martlesham to Ipswich.	0	0	1	0	0	1	2	The commenter states that Long Strops has rough surfacing, therefore, for the purpose of this assessment, resurfacing with a high-quality hard surface will be assessed. Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The alterations would not be expected to create significant modal shift although it will create better availability for some users. Optimisation – Resurfacing will help make the pathway more inclusive. This will provide an improvement to a route that is already off-road meaning it is considered 1 point. Safety – The issue is a matter of access and usability over safety. Biodiversity – No biodiversity impact. Leisure – This bridleway represents a route with moderate leisure value and

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												improved surfacing will likely improve access, therefore a score of 1 is deemed reasonable.
Kesgrave	342	Roundabout too narrow for cars and bikes	Rushmere Road/Colchester Road Roundabout is too narrow at peak time to allow safe cycling. The junction needs improvement								N/A	Not within the East Suffolk Area and has been given to the appropriate council.
Kesgrave	343	Cycle lane along Woodbridge road east	The cycle path/lane on the pavement along woodbridge road is a joke: it is old, raid surface is terrible, too narrow and occupied by pedestrians, blocked by driveways making it very dangerous and cars d not stop		0	3	2	0	-1	0	4	For the purpose of this assessment, upgrading the existing shared cycle/pedestrian infrastructure to a segregated bi-directional cycle track will be assessed. Connectivity and Growth – The proposal is regarding the existing cycling/pedestrian infrastructure along the A1214, or Woodbridge Road, and does not represent, therefore, a new connection. Modal Shift – According to PCT, the A1214 has high cycling traffic and the widening and resurfacing of the cycling infrastructure to the highest standard will likely increase this. The proposal will result in a significant modal shift, therefore a score of 3 under this category is considered reasonable. Optimisation – The proposal will upgrade the existing infrastructure from a shared path to a segregated cycle track. This optimisation warrants a score of 2. Safety – Off-road cycling infrastructure already exists, therefore the proposal will not have significant safety benefit. Biodiversity – The proposal will likely result in the loss of adjoining managed grassed areas, but across a relatively large area. Leisure – No leisure benefit.
Kesgrave	371	Bus stop opposite Penzance Road in Bell Lane Kesgrave	there is a sign here stating pedestrians and cyclists allowed. Cyclists assume they are able to cycle from here to Foxhall Road on the pavement as they have been allowed so to do from the Woodbridge Road end of Bell Lane. Pedestrians are of a different opinion, and there is contention	If cyclists are allowed to cycle all the way to Foxhall Road from the last sign at the junction of PenzanceRd/Bell Ln then more signs are needed. If they are not then a sign saying cycling ceases/stops/not permitted is needed to stop confusion and a likely future accident	0	0	1	0	0	0	1	The commenter proposes further signage along Bell Lane to better inform cyclists where they can and cannot cycle. Connectivity and Growth – No connectivity or growth benefit. Modal Shift – This change is not considered to create significant modal shift. Optimisation – Although the route is not improved, the addition of signage represents a modest optimisation so scores 1 point. Safety – Whilst the safety issue is modest, the poor clarity does create the risk of conflict occurring. Biodiversity – No biodiversity impact. Leisure – No leisure benefit.
Kesgrave	390	Main Road Kesgrave	the cycling path which runs along Main Road is an asset to Kesgrave. The High School, which is located along the Main Road has one of the highest amount of pupils who cycle to school in the County. This cycle path is in great need of repair. the markings,signage and surfacing all need updating, re instating and re tarmacking. If ESC wish to encourage cycling and walking in East Suffolk then these issues need to be addressed ASAP.	As above.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Kesgrave	419	Cycle path A1214 Kesgrave Road	A typical example of a 'stop start' cycle path where motor vehicles are given priority at each minor road junction and property driveway entrance, hence impeding the steady progress of cyclists and pedestrians	Consider giving cyclists & pedestrians the right of way at minor junctions by removing the 'giveaway' from the cyclepath and moving the road 'giveaway' lines back from the junction to before where the cycle path crosses it.Also where a cyclepath crosses the front of a property entrance put the giveaway lines across the entrance to ensure that anyone leaving the property gives way to the cyclist, rather than relying on the cyclist having to dodge vehicles sticking their nose out onto the cycle path.This is common practice in countries where cyclists are given priority over vehicles, rather than in the uk where vehicles are given priority over cyclists (and pedestrians, mobility scooter users etc).	0	0	1	0	0	0	1	Connectivity and Growth – The proposed alteration does not create additional connectivity.Modal Shift – The existing infrastructure remains so no modal shift. Optimisation – Currently cyclists are regularly forced to stop to give way to motorists so whilst it is not improving the type of existing infrastructure, it will optimise its use, therefore a score of 1 is deemed reasonable. Safety – No significant safety benefit. Biodiversity – No biodiversity impact. Leisure – No leisure benefit.
Kesgrave	458	Brendan Drive	NCN 1 & the cycle route into Ipswich is via an estate road at this point and sections are cluttered with parked cars, and a couple of short hilly sections where less abled and older riders have to get off and push.	It would make sense to upgrade the footpath that runs across Rushmere Common to Heath Road to a Cycle/footpath there by giving cyclists a section of the route that is traffic free and relatively flat. It would also connect in the other direction with the bridle way that runs east towards Bell lane and beyond... giving a continuous traffic free cycle route from the Hospital to almost the Brightwell Development Area.	3	3	0	2	-1	1	8	Connectivity and Growth – The proposal will likely have significant connectivity benefit - not only would the proposal connect into the existing cycling and walking infrastructure, which provides a route through Kesgrave to Martlesham, but the proposal also resides in the Ipswich to Melton key corridor. A score of 3 is considered reasonable. Modal Shift – The proposal would provide a safe off-road route which can be used as an alternative to the A1214 which, according to PCT, would have a significant modal shift if cycling infrastructure is delivered to the highest standard. Therefore, a score of 3 is considered reasonable. Optimisation – Despite Rushmere Heath already containing existing footpaths, the proposal will result in a new route for cyclists, therefore it is not considered an optimisation. Safety – The proposal can be used as an alternative to the use of the A1214, or Woodbridge Road, which, despite being a 30mph road, is busy. A score of 2 is considered, therefore, reasonable. Biodiversity – There are existing footpaths through Rushmere Heath (Rushmere golf course), therefore it is unlikely that the suggested improvements will result in significant biodiversity loss. However, as these footpaths will need to be widened and resurfaced, a small negative score is deemed reasonable. Leisure – The proposal would connect to the existing leisure route south of Kesgrave and a route through the heath will likely be considered attractive, therefore a small score of 1 is considered reasonable under this category.

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Kesgrave	517	Full Length of Main Road Kesgrave	The main road is too narrow to take both cycles and cars safely. The cycle path along the length of the road is also extremely uneven and crosses to many road to make it a practical through cycle route. This makes it unsuitable as a safe/fast through route into Ipswich.	The cycle path needs improving (levelling and better signage) and an alternative through route needs providing through Ksgrave - this could be along long strops bridle way. The only other way would be to provide a cycle route along the northern side of the main road - but assume this is not practical due to all the land that would need to be purchased. Pilboroughs Walk is too busy and has too many junctions to make it a viable through route either.	1	3	2	1	-2	2	7	The commenter proposes improving the existing Long Strops and Dobbs Wood bridleways and creating new bridleways along Rushmere Heath. Connectivity and Growth – The proposal would connect the existing bridleway into Ipswich; however, the proposal will likely have more leisure benefit than connectivity benefit. Modal Shift – The proposal will provide an alternative to the A1214 which, according to PCT, would result in a significant modal shift if infrastructure is delivered to the highest standard. Therefore, a score of 3 is considered reasonable. Optimisation – The proposal will optimise the existing bridleway to include a segregated cycleway – this warrants a score of 2 under optimisation. Safety – The proposal would provide an alternative to the A1214. The A1214, despite having a 30mph speed limit, is a busy fast road, although with existing infrastructure along some stretches of the road. A score of 1 is deemed reasonable. Biodiversity – Widening of the bridleway to implement a segregated cycleway will likely result in the removal of wild verges and small immature trees, therefore a score of -2 is deemed acceptable. Leisure – The proposal will create a particularly attractive route for leisure cycling, therefore a score of 2 is deemed reasonable.
Kesgrave	518	Longstrops, Dobbs Wood and Foxhall Heath Bridleway - (Sandlings Walk)	This bridleway can be used as a cycle way through Kesgrave but is currently grass / soil so isn't fast. It is also not lit. It is also narrow across Foxhall Heath. If the route was upgraded it could help relieve through cycling along the main road which isn't safe.	If a suitable surfaced cycleway was laid along the length, with possibly lighting, it would provide a fast, safe, traffic free route for cycling through Kesgrave. It would however need to be joined up at the Rushmere and Martlesham ends to make it a continuous fast route into Ipswich.	2	3	2	1	-2	2	8	The commenter proposes improving the existing Long Strops and Dobbs Wood bridleways and creating new bridleways along Rushmere Heath whilst also connecting it to the existing cycling infrastructure through Martlesham. Connectivity and Growth – The proposal, which also resides within the Ipswich to Melton key corridor, would create a connection through Ipswich, Kesgrave, and Martlesham and will, therefore, provide considerable connectivity benefit. However, the route, being situated to the south of Kesgrave, will likely have more leisure benefit than connectivity benefit and there are existing connections, although poor, along the A1214. A score of 2 is deemed reasonable. Modal Shift – The proposal will provide an alternative to the A1214 which, according to PCT, would result in a significant modal shift if infrastructure is delivered to the highest standard. Therefore, a score of 3 is considered reasonable. Optimisation – The proposal will optimise the existing bridleway to include a segregated cycleway – this warrants a score of 2 under optimisation. Safety – The proposal would provide an alternative to the A1214. The A1214, despite having a 30mph speed limit, is a busy fast road, although with existing infrastructure along some stretches of the road. A score of 1 is deemed reasonable. Biodiversity – Widening of the bridleway to implement a segregated cycleway will likely result in the removal of wild verges and small immature trees, therefore a score of -2 is deemed acceptable. Leisure – The proposal will create a particularly attractive route for leisure cycling, therefore a score of 2 is deemed reasonable.
Kesgrave	600	GR 242 464 to GR 198 453	The A1214 Woodbridge Road's cycle way is reasonable except:1. For most of its length, vehicles joining from side roads tend to halt on the cyclists' way crossing that side road before the junction. 2. Where it passes alongside the Rushmere Golf Course, it co-uses the narrow pavement and the kerbstone prevents cyclists getting on/off to avoid walkers.	1. Side roads surfaces should be painted with 'zebra crossing patches' and maybe a warning sign2. Widen the foot & cycle way	0	3	2	0	-1	0	4	The commenter proposes giving cyclists and pedestrians right of way at junctions through the implementation of zebra crossing whilst also widening the existing shared paths to allow segregation between cyclists and pedestrians. Connectivity and Growth – The proposed alteration does not create additional connectivity as there is existing infrastructure. Modal Shift – Although the zebra crossings will not result in a significant modal shift in itself, according to PCT, the widening of the existing infrastructure to the highest standard will result in a significant modal shift. Therefore, a score of 3 is deemed reasonable. Optimisation – Currently, cyclists are regularly forced to stop to give way to motorists so, whilst it is not improving the type of existing infrastructure, it will optimise its use. In terms of the improvements to the existing infrastructure, this warrants a score of 2. Safety – Off-road cycling infrastructure already exists, therefore the proposal will not have significant safety benefit. Biodiversity – Widening the existing infrastructure will likely result in the loss of adjoining grassed areas across a significant length; therefore, a small negative score is deemed reasonable. Leisure – No leisure benefit.
Kesgrave	628	The A1214 between Ipswich and the A12 junction and the cycle footways alongside the A1214 that's used for Kesgrave High School access	1) The A1214 between Ipswich and the A12 junction is a key route for everyday transport cycling but is congested/polluted and on-road improvements are needed. 2) The design of the cycle/footways by Kesgrave Fisheries and Kesgrave High School are not fit for purpose and also need repair/resurfacing 3) Damage to the cycle/footways is exacerbated by vehicles driving and parking on them and vehicles also cause obstructions 4) The side road cycle priority crossings have also deteriorated.	1) Make the whole of the A1214 between Ipswich and the A12 junction a 20mph zone with priority for cyclists. It runs past a school and residential housing and lower speeds would make it safer / more attractive for cyclists/pedestrians 2) Widen the road across Rushmere Heath to create dedicated cycle lanes on either side, separated from the footway. And plant Oak/Birch etc trees along the Heath edge 3) Turn the sections of shared cycle footway by Kesgrave Fisheries, Kesgrave High School etc into wide attractive pedestrian-only routes - they are too narrow /dangerous for shared use by cycles/pedestrians/mobility scooters/wheelchairs/buggies 4) Where space allows e.g. by KHS the new pedestrian-only route could be designed and built as a wide and pleasant tree-lined boulevard to accommodate the very high level of foot traffic at school times including buggies, dogs etc. Trees would also help soak up some of the traffic pollution and help improve health, the environment and visual amenity.	0	0	-1	-1	2	0	0	The commenter proposes reducing the speed limit along the A1214 to 20mph, however this is a highways matter and should be passed onto SCC. For the purpose of this assessment, the proposal of making the road cyclist priority with on-road cycle lanes whilst making the existing shared path pedestrian only will be assessed. Connectivity and Growth – As there is existing cycle infrastructure along the A1214, the proposal will not result in additional connectivity, hence a neutral score. Modal Shift – No significant modal shift impact. Optimisation – The proposal will result in removing cyclists from off-road infrastructure to on-road infrastructure, which is considered a downgrade despite the existing infrastructure being in poor quality, therefore a small negative score of -1 is deemed reasonable. Safety – Although the commenter proposes cycle lanes, the A1214 is a busy 'A' type road. By downgrading the existing infrastructure from off-road to on-road, it is increasing the hazard for cyclists, hence a score of -1. Biodiversity – The commenter proposed planting trees alongside the existing pedestrian infrastructure, therefore a score of 2 under this category is deemed acceptable. Leisure – No leisure impact.
Kesgrave	629	A1214 Kesgrave especially its junction with Bell Lane and the section up to All Saints Church and Ropes	1) The cycle/footway is too narrow on south side of A1214 and at Bell Lane junction and is heavily used for walking and cycling to/from Kesgrave High School 2) There is no pedestrian crossing of the A1214 and this is	Redesign A1214 corridor as safe and attractive for people to walk, cycle and use a bus. Helps address the climate emergency and public health crisis (reduces NHS burden if people can choose active travel). Make	0	0	-1	-1	2	0	0	The commenter proposes reducing the speed limit along the A1214 to 20mph, however this is a highways matter and should be passed onto Suffolk County Council. Also, the proposals for bus use and free bike repairs are outside the remit of this project. For the purpose of this assessment, the proposal of making the road cyclist

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		Drive West roundabout and in the other direction going to Heath Road roundabout	needed to enable people to cross the road from All Saints Church to access the Cemetery, Carpet Cuts and the bus stop 3) High level of air pollution by The Bell caused by traffic congestion and queuing here which creates health risks for everyone- especially car drivers and occupants	the A1214 a priority route for cyclists, buses and disabled users who need to use their cars. It's a key bus route and First Bus have previously asked for improvements to A1214. In return, ask them - with support from local councils/central government funding - to offer free bus use for a month (+ ongoing offers) to persuade people out of cars e.g. The Park and Ride bus service is excellent but few people have tried it. More bus use = less single occupancy car use +less congestion and pollution. Turn A1214 into a 20mph road to encourage cycling, offer free cycle training and bike repairs locally. Redesign the cycle/footway on the south side of A1214 as a pedestrian-only route with pedestrian crossing of A1214 and ped/cycle/bus friendly redesign of the Bell Lane/a1214 junction.								priority with on-road cycle lanes whilst making the existing shared path pedestrian only will be assessed. Connectivity and Growth – As there is existing cycle infrastructure along the A1214, the proposal will not result in additional connectivity, hence a neutral score. Modal Shift – No significant modal shift impact. Optimisation – The proposal will result in removing cyclists from off-road infrastructure to on-road infrastructure, which is considered a downgrade despite the existing infrastructure being in poor quality, therefore a small negative score of -1 is deemed reasonable. Safety – Although the commenter proposes cycle lanes, the A1214 is a busy 'A' type road. By downgrading the existing infrastructure from off-road to on-road, it is increasing the hazard for cyclists, hence a score of -1. Biodiversity – The commenter proposed planting trees alongside the existing pedestrian infrastructure, therefore a score of 2 under this category is deemed acceptable. Leisure – No leisure impact.
Kesgrave	410a	Kesgrave School	Doesn't appear to be a safe route for children and other cyclists to get to Dr. Watsons Lane (to Playford) and Hall Road (to Bealings) from the Northern (School) side of the road or indeed the existing cycle path on the South side. Hence limiting the opportunity for children and parents from the villages to cycle to the school in safety.	1). Provide a proper crossing and short section of cycle/footpath on the northside of the road where the central refuge is on the A1214 at Hall Road.	0	0	1	1	0	0	2	The commenter proposes a toucan crossing to replace the central refuge, which is situated just west of the Hall Road/A1214 junction. Connectivity and Growth – The proposal does not create additional connectivity or growth. Modal Shift – Insufficient evidence to suggest that the proposal will result in a modal shift. Optimisation – The proposal is improving the existing crossing point, which is currently a central refuge, therefore the proposal is considered an optimisation. The proposed optimisation warrants a score of 1 under this category. Safety – The A1214, despite having a 30mph speed limit, is a busy road. As the existing crossing point is of poor quality, the proposal will likely provide moderate safety benefit. Biodiversity – No biodiversity impact. Leisure – No leisure benefit.
Kesgrave	410b	Kesgrave School	Doesn't appear to be a safe route for children and other cyclists to get to Dr. Watsons Lane (to Playford) and Hall Road (to Bealings) from the Northern (School) side of the road or indeed the existing cycle path on the South side. Hence limiting the opportunity for children and parents from the villages to cycle to the school in safety.	1). Extend the existing cycle path beyond the Bell Lane traffic lights past the Doctor Watsons lane junction and provide a separate crossing integrated with the existing traffic lights.2) This would also help all cyclists wishing to travel from the Kesgrave development north into the villages and beyond.	0	3	2	2	-2	1	6	The commenter proposes extending the cycle path along the A1214 beyond Bell Lane, however, there does appear to be an existing cycleway here. As the existing cycleway is shared pedestrian/cyclist path, for the purpose of this assessment improving the existing infrastructure to a segregated bidirectional cycleway will be explored instead. Connectivity and Growth – As there is existing infrastructure, no new connections are created, therefore the proposal scores a 0 under this category. Modal Shift – According to PCT, if the proposal is delivered to the highest standard, the route will have a significant modal shift. Therefore, a score of 3 is considered reasonable. Optimisation – The proposal will improve a shared cyclist/pedestrian path to a segregated cycle track, therefore a score of 2 is deemed acceptable. Safety – The A1214, despite being a 30mph road, an 'A' type road and speed, and volume of traffic is often high. Removing cyclists off the road has safety benefits, therefore a score of 2 is considered reasonable. Biodiversity – In order to achieve infrastructure to the highest standard, removal of the managed green verges and some hedges adjacent to the road may be necessary. With consideration to the previous, a score of -2 is deemed reasonable. Leisure – The proposal would connect into Rushmere Heath, which contains attractive PROW routes, however, despite the infrastructure being poor, the heath is already well connected with the existing infrastructure. A score of 1 is deemed reasonable.
Kessingland	546	the Denes to Kessingland	Great to see this subject being considered, particularly at a time when cycling & walking are likely to play more important roles in all our lives. Being a keen cyclist, I've always been impressed with the amount of cycling paths and lanes but, understandably, a number of these were put in place probably decades ago and the town has changed around them.	The Third Crossing will obviously impact traffic volumes and flows, and hopefully be one factor in providing opportunities for improving cycling and walking paths, particularly where these can be provided alongside, rather than necessarily sharing, the same road as vehicles. In that respect, there could be an opportunity to join up, or create, a coastal cycle & walking path, running from the Denes to Kessingland? That would potentially allow people to travel safely from one end of town to the other, mostly away from traffic. And something to support the promotion of the Sunrise Coast, too.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. The potential of the third river crossing is being considered in the formation of the strategy.
Kessingland	638	Kessingland + A12 going south	There is no cycle route at all. There is no way for cycles to travel safely along the A12. How do we even get to Benacre from Lowestoft? Why no cycle way along the A12?	Cycle way along the A12. At present no way of getting to Lowestoft until Kessingland is reached (and then it's not very good)	3	3	3	2	-3	0	8	Connectivity and Growth - Whilst it is noted that a connection already exists which would lower the score it does improve a significant section of a recognised key corridor giving it a maximum score. Modal Shift - PCT suggests that this has potential for significant growth if improved to a top standard. Optimisation - A shared path already exists along the A12, but additional width will improve its use giving a score. Off-road roads along London Road could be improved to a shared path standard. Altogether a score of 3 is deemed reasonable. Safety - Whilst it is recognised that the width of the path along the A12 could cause disruption there nominally exists an off-road route so would not normally score. However such is the narrow width that users may be forced to use the road giving a score here, in addition the path does not continue to Kessingland and a comprehensive route will get people off London Road meaning a score of 2 is deemed reasonable. Biodiversity - To widen the path would require the loss of verges and likely the loss of established hedgerow Leisure - This is predominantly seen as a commuter route and an unattractive route meaning its unlikely to provide significant leisure benefit compared to a more coastal path.

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Kettleburgh	253	Easton to Kettleburgh Road, big dip in road about 0.75m from verge going up the hill into Kettleburgh, catches cyclists and motorbiked out.	Raise grate and level road								N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Kettleburgh	520	The Street, Kettleburgh	It is a fairly well used road by all manner of vehicles. It is also a well used cycle route but alas not ideal for walkers as there is no path and no street lighting. I was saddened three weeks ago, whilst I was walking down the road in the early evening when I lost my footing and fell to the ground, sprained my ankle very badly and hurt my left knee and arm. I noted exactly where this happened and have attached photographs of the damaged road there and further unacceptable and unsafe areas.	Please try to address this road safety situation as a matter of urgency as I believe it is only a matter of time before a more serious incident could occur to cyclist and walkers alike. I know the government is encouraging more activity in these areas so safety has got to be the priority.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Kettleburgh	520A	The Street, Kettleburgh		New pedestrian path alongside The Street joining the existing pavements either side.	1	0	0	1	0	1	3	Connectivity and Growth - This path will connect both sides of the village, however it should be noted that a number of PROW footpaths do provide some connectivity albeit less directly. In terms of services Kettleburgh has limited services in which to connect to, it would provide some benefit in providing connection to the public house. A score of 1 is deemed reasonable. Modal Shift - There is unlikely to be significant modal shift growth as this would not create significant connections to day-to-day services and need. In addition the low numbers of likely users means it scores 0 here. Optimisation - This would represent new infrastructure as opposed to an optimisation. Safety - The section is a short stretch at 30mph. The condition of the road is a maintenance issue so does not factor in this scoring. A score of 1 is deemed reasonable. Biodiversity - There is limited space in which to create a path so use of some of the road space may be required. A small grass verge may also need to be used. Leisure - The proposal has some leisure benefit with connections between a number of guest houses to the public house.
Kirton	572	Kirton Village Green to Reeve Lodge Trimley St Martin	Trimley St Martin Primary School is being moved from its present position on Kirton Rd Trimley to a piece of land by Reeve Lodge SCLP 12.65. This school is attended by children from Kirton many of whom do not have cars. There needs to be a safe segregated cycle path from Kirton to the new site.	The land opposite Kirton Village Green is owned by Trinity College as is the land where the new school is to be built. If land could be acquired from Kirton Green crossing Croft Lane and beyond it would be possible to put in a new segregated cycle path virtually up to the existing footbridge over the A14. There is a wide footpath past Roselea Nursery which could easily be increased in width. The path would then link into Old Kirton Road. There would have to be some kind of crossing to get children to the new school over Many adults cycle over the footbridge as a means to get to Felixstowe. This could be a very valuable route to decrease road traffic and meet East Suffolk's climate change Greener agenda. It also connects to other major cycle routes in the area.	3	2	0	1	-3	1	4	Connectivity and Growth – The proposal will likely have significant connectivity benefits as it will connect into site allocation SCLP12.65 and the route proposed also resides within a key corridor, therefore a score of 3 is warranted. Modal Shift – Along some sections of the proposed route, specifically Kirton Road, PCT suggests that the proposal would result in a somewhat significant modal shift if infrastructure were delivered to the highest standard. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will remove cyclists off Trimley Road, Old Kirton Road, and Kirton Road, which are both reasonably busy roads with a 30mph speed limit. As a 30mph road, it does not represent a significant hazard, however the proposal will still have modest safety benefits. A score of 1 is awarded. Biodiversity – In order to implement the infrastructure, the removal of established hedgerows that adjoins the roads will be necessary. The removal of established hedgerows warrants a score of -3 under this category. Leisure – The proposal will likely have more connectivity benefit than leisure, however the proposal does connect into Kirton Village green, which likely has small leisure value. A score of 1 is considered reasonable.
Kirton	636	Between Kirton village and the site adjacent to Reeve Lodge, High Rd, Trimley St Martin	Trinley St Martin Primary School is currently located in Kirton Rd, in easy walking distance of Kirton village. In 2023, or thereabouts, it will be relocating to a site on the opposite side of the A14 adjacent to Reeve Lodge, High Rd, Trimley St Martin which is much further away.	A safe, segregated cycle track is needed to enable Kirton children to cycle to the new location.	3	2	0	1	-3	1	4	The commenter proposes a cycleway to connect Kirton into SCLP12.65. For the purpose of this assessment, a cycle track along Trimley Road, Kirton Road, and Old Kirton Road will be assessed. Connectivity and Growth – The proposal will likely have significant connectivity benefits as it will connect into site allocation SCLP12.65 and the route proposed also resides within a key corridor, therefore a score of 3 is warranted. Modal Shift – Along some sections of the proposed route, specifically Kirton Road, PCT suggests that the proposal would result in a somewhat significant modal shift if infrastructure were delivered to the highest standard. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will remove cyclists off Trimley Road, Old Kirton Road, and Kirton Road, which are both reasonably busy roads with a 30mph speed limit. As a 30mph road, it does not represent a significant hazard, however the proposal will still have modest safety benefits. A score of 1 is awarded. Biodiversity – In order to implement the infrastructure, the removal of established hedgerows that adjoins the roads will be necessary. The removal of established hedgerows warrants a score of -3 under this category. Leisure – The proposal will likely have more connectivity benefit than leisure, however the proposal does connect into Kirton Village green, which likely has small leisure value. A score of 1 is considered reasonable.
Leiston Cum Sizewell	105	On the shared use cycle path along Lovers Lane towards Sizewell.	The cycle path is great but in a few places there are bollards on the pavement which encroach on the space and make it impossible for a cyclist to pass a pedestrian or other cycle on the path. This shared use path is well used by walkers and cyclists but we repeatedly have to join the road here as it is not possible to pass others. It is particularly awkward as this is really well used by families and children.	The bollards just need removing! I am not sure why they are there. Also, perhaps a guide line on the path for pedestrians/cyclists half of the path?	0	0	1	0	0	1	2	Connectivity and Growth – The path connects Leiston to a key employment area in Sizewell, and whilst the barriers may reduce the worth of the connection, it does remain connected so receives a neutral score. Modal Shift – The removal of the barrier is unlikely to create a significant modal shift. Optimisation – Removing the barriers won't improve the overall infrastructure, but would provide a modest optimisation benefit scoring 1 point. Safety – Whilst there is a potential benefit to removing barriers to the pathway, the barriers likely perform a safety role themselves so highway input is needed. A neutral score has been given. Biodiversity – No significant biodiversity benefits Leisure – There may be a modest leisure benefit to

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												this route, but it is not clear that the removal of the barriers will provide a significant benefit so a score of 1 is deemed reasonable.
Leiston Cum Sizewell	444	B1122 Abbey Road / Lovers Lane junction to Valley Road. All offroad.	Safe route, avoiding Abbey Road, bringing workers into Town. Legacy route for residents and tourists accessing Aldhurst and route onto Suffolk Coast Path. 3. Links to route 2 and on to all other proposed routes.		1	0	0	1	-1	0	1	Connectivity and Growth - New off road connection created into Halesworth. Modal Shift - Small uplift in modal shift but not enough to be scored. Optimisation - No existing infrastructure. Safety - Although its 30mph, this is the main road through Leiston and receives a lot of traffic, therefore a score of 1 has been given in regards to safety. Biodiversity - Potential removal of vegetation to accommodate off-road path. Leisure - No effect on use of route for leisure.
Leiston Cum Sizewell	445	LOVERS LANE - VALLEY ROAD - ALLOTMENTS - EXITING AT SIZEWELL ROAD/KING GEORGES AVENUE.	Lovers Lane via EDF route. Close part of Valley Road to sewage works. Then on road via Valley Road to allotments. Then across allotments on FW and across private land to King George Avenue. Route 2b from allotments to High Street Closure of Valley Road will facilitate safe route from camp site for construction workers. Legacy route for residents and tourists accessing Aldhurst and route onto Suffolk Coast Path to Aldeburgh via new tourist cycle route along the old railway line.		1	0	0	0	0	3	4	Connectivity and Growth - The proposed route will link the centre of Leiston onto Lovers Lane with connections into Sizewell. Modal Shift - No effect. Optimisation - No effect. Safety - No effect. Biodiversity - No effect. Leisure - This route will have a positive impact on Leisure as it could form part of the East Suffolk Tourism and Leisure route.
Leiston Cum Sizewell	446	LOVERS LANE - SIZEWELL ROAD - KING GEORGES AVE - EXITING AT GRIMSEY ROAD	Sizewell Gap / Lovers Lane Junction Via King Georges Avenue to Sizewell Road / Grimsey Road junction. Off road cycleway on south side of King George Avenue as far as eastern entrance to Sports Field/Recreation ground. Then private tracks / footways behind houses. Links back to King George Avenue with off road cycleway on Sylvester Road.		1	0	0	1	0	3	5	Connectivity and Growth - The proposed route will link Leiston into Sizewell. Modal Shift - No effect. Optimisation - No effect. Safety - King Georges Avenue is a busy road with traffic travelling at 30mph. The proposed off-road cycle track will divert users off this road which will provide a slight improvement to safety, therefore a score of one has been given to reflect this. Biodiversity - No effect. Leisure - This route will have a positive impact on Leisure as it could form part of the East Suffolk Tourism and Leisure route.
Leiston Cum Sizewell	447	CROWN FARM JUNCTION - NEW TOURIST ROUTE - GRIMSEY LANE EXITING AT LEISURE CENTRE	Sizewell Gap via track south to join Grimsey Lane. West via Grimsey Lane to the Leisure Centre. Off road (tracks) but on road from Leisure Centre along Red House Lane to Poppy Way. More direct cycle access for workers to the Leisure Centre. Legacy route for residents and tourists accessing new tourist route to Aldeburgh and route to Sizewell.		3	0	0	0	0	3	6	Connectivity and Growth - This suggestion will create a connection between Sizewell and Leiston Leisure Centre. It could also link into the proposed East Suffolk Tourism and Leisure route and therefore, this proposal will have a significant benefit to connectivity in this area. Modal Shift - No effect. Optimisation - No effect. Safety - No effect. Biodiversity - No effect. Leisure - This improvement will create an attractive route for users to access Sizewell Beach and Leiston Leisure Centre whilst also potentially connecting to the East Suffolk Tourism and Leisure route.
Leiston Cum Sizewell	448	GRIMSEY ROAD (Sylvester Road?) - THROUGH TOWN CENTRE - CROSS STREET - VICTORY ROAD - WATERLOO AVENUE	King George Avenue / Sylvester Road junction via Sizewell Road, Cross Street and Victory Road (all on street) then via public footway to Waterloo Avenue (off road) Main route through town linking east with west, avoiding busy/unsafe routes; Haylings Road, Park Hill and White Horse junction.		3	2	1	1	-2	3	8	Connectivity and Growth - This suggestion will create a new connection for cyclists to travel east to west through the centre of Leiston. This is a key connection for users as the town centre contains a majority of the key services and facilities for residents. Modal Shift - Cross Street recorded a PCT score of 100 which suggests that any improvement along this route will have an effect on modal shift. Optimisation - This score is in relation to PROW 8 which would need to be widened to support both cycling and walking. Safety - Although most of the route is on road and along 30mph speed limits, a score of one has been allocated to reflect the busy nature of the Town Centre. Biodiversity - The widening of PROW 8 may require the removal of existing vegetation along this section of the route. The loss of this vegetation would result in a negative impact to biodiversity. Leisure - Leiston Town Centre contains a variety of leisure facilities that would be more accessible to residents and visitors as a result of this improvement.
Leiston Cum Sizewell	449	GOLDINGS LANE - ALDEBURGH ROAD - THROUGH TOWN CENTRE TO WHITE HORSE (WATERLOO AVENUE/STATION ROAD JUNCTION)	B1069 Haylings Road via Goldings Lane (part on, part off road) to B1122 Aldeburgh Road the north on Aldeburgh Road, High Street and then west to Waterloo Avenue / Station Road junction. On road with short diversion onto service road. Main route through the town from south to north. Route from Knodishall into Town or to Leisure Centre via 6b or Sizewell via route 4.		3	2	0	1	0	3	9	Connectivity and Growth - This suggestion will connect the South of Leiston into the Town Centre via a combination of off-road and on-road cycle infrastructure. Modal Shift - The B1122 received a PCT score of 138 which suggests that improvement along this route would result in a degree of modal shift. Optimisation - No effect. Safety - Although parts of the route are on-road and most of the route is within 30mph speed limits, a score of one has been allocated to reflect the busy nature of the road. Biodiversity - No effect. Leisure - Leiston Town Centre contains a variety of leisure facilities that would be more accessible to residents and visitors as a result of this improvement.
Leiston Cum Sizewell	450	ALDEBURGH ROAD - HOPKINS ESTATE - LEISURE CENTRE AND ALDE VALLEY ACADEMY	B1122 Aldeburgh Road via track to Daisy Drive, then on road via Foxglove End and Prettet Way to Red House Lane. Safer route avoiding traffic in Red House Lane. Links to route 8		1	0	1	0	-1	3	4	Connectivity and Growth - This proposal will improve connectivity between the South of Leiston and the East of Leiston avoiding the Town Centre. Modal Shift - No effect. Optimisation - In relation to PROW 14a which is an existing footpath, this will need to be widened and potentially resurfaced to accommodate cycling effectively. Moreover, it will have to be upgraded to bridleway status to support cycling legally. Safety - No effect. Biodiversity - The potential need for widening the path would require the removal of grassland and would result in a small loss to biodiversity. Leisure - This route will link a large number of houses to Leiston Leisure Centre which will be a significant benefit to leisure.
Leiston Cum Sizewell	451	ALDEBURGH ROAD - SEAWARD AVENUE - SYLVESTER ROAD	Off road cycleway from Aldeburgh Road along Seaward Avenue to Sylvester Road. Then on road (contra flow) on Sylvester Road north to join route 3 south of Sizewell Road. Extension 7b on Seaward Avenue to Alde Valley Academy and route 8. Safe route to travel to Alde Valley Academy (Secondary School) and Avocet Academy (Primary School) avoiding Town centre. Important link for route from south to north of town for workers and residents/tourists.		0	3	3	1	-1	2	8	Connectivity and Growth - existing connection in place for walking but not for cycling, this comment focuses on upgrading existing infrastructure and therefore will be scored under optimisation. Modal Shift - Seaward Avenue received a PCT score of 234 which suggests that improvement along this road would result in significant Modal Shift. Optimisation - Upgrading the existing footpath to an off road cycle path would provide a significant improvement to the existing infrastructure. Safety - Although this is a 30mph road, it does receive a high level of traffic at peak times and therefore a score of one has been allocated to reflect this. Biodiversity - The widening of the existing footpath would require the removal of existing grassland.

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												This would result in a small loss to biodiversity. Leisure - This route would connect a large number of house close to the Town Centre. A score of two has been given to reflect the fact that the route would not directly link to the Town Centre.
Leiston Cum Sizewell	452	LEISURE CENTRE- ALDE VALLEY ACADEMY - AVOCET ACADEMY	Route 3 south of King Georges Avenue across recreation ground and then via public footways to Red House Lane/ Linking to route 4 Safe link between all three sites and access to all routes.		0	0	2	0	0	1	3	Connectivity and Growth - This comment is focused on upgrading the existing PROW 16B and therefore will be scored in the Optimisation category. Modal Shift - No effect. Optimisation - The potential widening, resurfacing and upgrading of the existing footpath to support cycling will be a significant improvement to this route. Safety - No effect. Biodiversity - No effect. Leisure - This improvement will have a positive impact on access to Leisure facilities in Leiston and a score of one has been allocated to reflect the scale of this benefit.
Leiston Cum Sizewell	453	WATERLOO AVENUE (CHURCH ROAD) - PATH BEHIND MASTERLORD ESTATE - BUCKTON PLACE	Waterloo Avenue north on public footpath and then west across recreation ground to Harling Way. Safe route from west boundary into Town avoiding Waterloo Avenue and White Horse junction.		0	1	2	1	0	3	7	Connectivity and Growth - This comment looks to upgrade the existing footway to accommodate cycling and therefore will be scored under optimisation. Modal Shift - Waterloo Avenue received a PCT score of 76, therefore this improvement will have a modest impact on modal shift. Optimisation - Upgrading, widening and potentially resurfacing the existing footway to accommodate cycling effectively would be a significant improvement to this route. Safety - Although Waterloo Avenue has a 30mph speed limit, a score of 1 has been allocated to reflect the busy nature of the road. Biodiversity - No effect. Leisure - This improvement will improve access to the Town Centre where a number of key leisure facilities are located.
Leiston Cum Sizewell	454	WESTWARD HO (PEDESTRIAN RAILWAY CROSSING) - BUCKLESWOOD ROAD - ABBEY LANE	Route 9 where it turns west to recreation ground along public footpath to Buckleswood Road then on road west along Buckleswood Road to Harrow LaneLinks route 9 to route 1 from west of Town. Avoids single track, rat run route of Abbey Lane.		2	0	0	2	-3	1	2	Connectivity and Growth - This improvement will create a link between NW edge of Leiston and close to the Town Centre. Modal Shift - No effect. Optimisation - No effect. Safety - Buckleswood Road is a national speed limit road, therefore cars are likely to travelling at high speeds along this road. A score of 2 has been allocated to reflect the potential of the high speed vehicles as well as the low traffic nature of the road. Biodiversity - Both sides of Buckleswood Road have established mature hedges and trees. Improvements along this road would required the removal of these hedges which would be a significant loss to biodiversity. Leisure - This route would connect a small number of houses close to the Town Centre. A score of one has been given to reflect the fact that the route would not directly link to the Town Centre.
Leiston Cum Sizewell	455	PATH AROUND VICTORY ROAD RECREATION GROUND TO BE UPGRADED	PATH AROUND VICTORY ROAD RECREATION GROUND TO BE UPGRADED		0	0	1	0	0	1	2	Connectivity and Growth - No new connection is created. Modal Shift - No effect. Optimisation - Improvement to existing path around recreation area. Safety - No effect. Biodiversity - No effect. Leisure - The park is an important leisure facility in leiston, therefore improving the path will have a positive effect in regards to leisure.
Leiston Cum Sizewell	456	Abbey Lane	From B1122 Abbey Hill to Aldhust Farm Off road cycleway to avoid narrow section of Abbey Lane		1	0	0	0	-3	1	-1	Connectivity and Growth - New off road connection created to connect the existing holiday park to Abbey Road, North of Halesworth. Modal Shift - No effect. Optimisation - No existing infrastructure. Safety - Angel Lane is a small road with low levels of traffic and traffic is likely to be travelling slowly. Biodiversity - Potential removal of established trees, hedges and vegetation to accommodate off-road path. Leisure - Link to holiday park would provide benefits in regards to leisure.
Leiston Cum Sizewell	470	Route from Eastbridge Road to Leiston Footpath 20. -between TM454652, through Black Walks and Lower Abbey to TM458661	Much of this route is believed to be in the ownership of EDF . There are notices denying public access along it but it is believed to have been a freely available route for walkers in the past.	This route should be added to the Definitive Map by way of a Creation Agreement or Order as a safe alternative to the Eastbridge Road and between Footpath 20 and Bridleway 19 at the Round House.	0	0	0	0	0	3	3	Connectivity and Growth - This improvement will create a link between Eastbridge and the coast. This will be a new connection however, it is not of strategic importance and, therefore, a neutral score has been allocated to reflect this. Modal Shift - No effect. Optimisation - No physical improvements will be made to the route itself. Safety - No effect. Biodiversity - No effect. Leisure - This improvement will create a very attractive route for users to access the coast from Eastbridge.
Leiston Cum Sizewell	473	The British Energy permissive path between the small car park off Lovers Lan 6452.	This path forms part of the important recreational route known as The Sandlings Walk. Currently it is permissive only and as such can be withdrawn at any time.	It should be made into a permanent public right of way by means of a Creation Order or Agreement. The other adjoining permissive paths on British Energy's estate through Sizewell Belts should also be made permanent public rights of way.	0	0	0	0	0	1	1	Connectivity and Growth - No new connection is created. Modal Shift - No effect. Optimisation - No improvements are made to the route. Safety - No effect. Biodiversity - No effect. Leisure - The route provides leisure opportunities for residents and visitors.
Letheringham	620	Just north of Letheringham (the Street) on the way to the Hoo/Easton road.	There is a huge run off of wet mud from the field there and this creates an uneven, rippled and potentially hazardous surface for people on bikes.	Persuade the owner of the land/field to clear the mud on a regularly and frequently.	0	0	0	1	0	0	1	Community and Growth – Although this road currently has no cycling/pedestrian infrastructure, the proposal is not for new infrastructure, therefore the proposal cannot score under this category. Modal Shift – This proposal is unlikely to create a significant modal shift. Optimisation – The proposal does not improve existing infrastructure; therefore, it is not considered an optimisation. Safety – This section of the road has a national speed limit and the mud, or the obstruction, likely forces cyclists and pedestrians into the middle of the road. However, as the road is a minor road and as the proposal is not removing cyclists or pedestrians off the road, the safety benefits are limited. A score of 1 under 'safety' is considered therefore, reasonable. Biodiversity – No significant biodiversity benefits. Leisure – Unlikely to have significant leisure benefits.
Levington	199	Old Felixstowe Road (formerly A45) between Felixstowe Road/Seven Hills and Levington slip road off A14	Ideal stretch of road to introduce segregated cycle lanes &/or reduce speed limits &/or prohibit through vehicular movement other than if required for public transport or "Operation Stack" An alternative is needed to Cycle route 51 (via Stratton Hall, Levington Church and Nacton village, which although is a picturesque leisure ride, is considerably	This was once the main A45 (now A14), the speed limit is still 60mph or 70mph in the dual carriageway near Bridge Road. This 2-mile length of road could be provided with a separated cycle lane in both directions &/or have the speed limit reduced to 20 or 30mph as it runs completely parallel with the A14 dual carriageway. I appreciate the road has historically been used for	1	3	3	3	0	2	12	Connectivity and Growth: A cycle lane on the southside of Felixstowe Road 'south' between the junction with Felixstowe Road 'north' and the turning for Levington (Bridge Road) would be a useful addition, and may be deliverable given the two allocated sites in Levington. It may be useful for access to the SCLP12.20 Land at Felixstowe Road site too, depending on where the cycle/pedestrian or single access point to this site is planned for. However, it would not provide as high a degree of segregation as a cycle/pedestrian track, and therefore would likely have less appeal.

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			longer than the direct route, and is also quite hilly in several places	"Operation Stack", but Port of Felixstowe's Vehicle Booking System has largely removed the need for the road to be designated in this way 24/7/365.								This route is used by buses and HGVs as an alternative to the A14, particularly as there is an HGV rest stop/lay by south of the turning for Levington a track away from, but parallel to, the carriageway is therefore preferable. However a segregated cycle lane should provide sufficient safety gains to still score a 3 under safety. Modal Shift: PCT shows a high uplift potential along Felixstowe Road 'south'. Leisure: Commuting value aside, Levington is a popular leisure cycling destination due to its relative hilliness - a (bi-directional) segregated cycle lane here will add extra access (besides the Nacton Road route) to Levington/help to provide a safer circular route.
Levington	369	Levington and Stratton Hall	Public footpaths are enjoyed by many walkers but are increasingly being plagued by cyclists who endanger the use by walkers and erode narrow coastal paths, delicate in many places as previous breaches will testify. Once the strategy is adopted, the bridleways and cycle paths must be properly maintained to encourage their use. The poor state of the A14 cycle way is an example of poor maintenance.	Although the misuse of footpaths contravenes the tort law of trespass, it is highly unlikely to be enforced by any landowner. Any strategy needs to make clear that cycling on public footpaths is unacceptable and unlawful. Parishes like ours who welcome considerate walkers to the footpaths are becoming increasingly inundated by rubbish dumped. Although litter picks clear up their rubbish, it needs to be clear that rubbish dumping is an increasing nuisance and that measures should be introduced to eliminate it. The provision of cycle paths seems to be less than public footpaths and this needs to change to avoid clashes between those on foot and those on cycles.							N/A	This proposal has been scored 'N/A' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Levington	735	'Old' Felixstowe Rd between the Levington turn off / junction with the current Felixstowe Road	Cars travel at great speed along the 'old' Felixstowe Rd between the Levington turn off and the junction with the current Felixstowe Road.	There needs to be a dedicated cycle lane which continues through the layby area onto the dedicated cycle path on the 'current' Felixstowe Rd.	1	3	3	3	0	3	13	Connectivity and Growth: A cycle lane on the southside of Felixstowe Road 'east' between the junction with Felixstowe Road 'west' and the turning for Levington (Bridge Road) would be a useful addition, and may be deliverable given the two allocated sites in Levington. It may be useful for access to the SCLP12.20 Land at Felixstowe Road site too, depending on where the cycle/pedestrian or single access point to this site is planned for. However, it would not provide as high a degree of segregation as a cycle/pedestrian track, and therefore would likely have less appeal. This route is used by buses and HGVs as an alternative to the A14, particularly as there is an HGV rest stop/lay by south of the turning for Levington, and it is these vehicles that pose the highest casualty and fatality risks to cyclists and pedestrians, and often provide the worst environmental conditions through particulate pollution; a track away from, but parallel to, the carriageway is therefore preferable. However a segregated cycle lane should provide sufficient safety gains to still score a 3 under safety. Modal Shift: PCT shows high uplift potential uplift scenario along Felixstowe Road 'east'. Leisure: Commuting value aside, Levington is a popular leisure cycling destination due to its relative hilliness - a (bi-directional) segregated cycle lane here will add extra access (besides the Nacton Road route) to Levington/help to provide a safer circular route.
Little Bealings	328a	Playford Road - east of junction with The Street and Hall Road.	Playford Road and Martlesham Road has become much busier with through traffic between Ipswich and Woodbridge making it less unpleasant and much less safe to cycle on. The road was very popular during the lockdown when there was little or no traffic, as those new to cycling and those wanting to encourage their children to cycle found out.	This is an alternative suggestion made by an officer of East Suffolk Council is to upgrade, widen and surface (from Little Bealing's centre) Footpaths 7, 8, 12, 9 and 10 to Brook Lane/Top Street, and/or create a new connection to Seckford Hall Road via a new crossing over the A12 (which is recommended to be improved in the C&WS with a cycling and walking track on the east side) for access into Woodbridge centre.	3	1	0	1	-2	1	4	Connectivity and Growth - Little Bealings contains some services in the form of a school, village hall and church, but would benefit from connections to Martlesham/Woodbridge both of which offer significantly more services. Modal Shift - As a footpath PCT does not cover this route, however the current connection through Martlesham Road could be considered which showed a modest modal shift. Optimisation - This would involve significant new infrastructure so would not score under optimisation. Safety - Much of Martlesham Road appears to be 30mph and would likely be relatively quiet so a score of 1 was deemed reasonable. Biodiversity - Without a full assessment it is unclear how much biodiversity assets would be lost in widening and surfacing the path. A score of -2 was provided to reflect its attractive location, but this could change either up or down upon a full assessment. Leisure - The pathway would create an attractive visitor attraction in its own right as well as providing access for the residents of Little Bealings to leisure attractions in Woodbridge and Martlesham.
Little Bealings	550	Playford Road and Martlesham Road, Little Bealings	The Parish Council is aware that both these roads are used regularly by cyclists, including cycling clubs at weekends, and by walkers passing between footpaths. The route is a rat run to Ipswich for vehicles seeking to avoid the A1214 and there has long been concern over the volume and speed of traffic	Traffic calming, such as width restriction or a barrier across part of the road. There was hatching in Martlesham Road, but this faded and SCC did not replace it. There was also a surface change introduced in Playford Road at one time, but this has also gone due to resurfacing.	0	0	1	1	0	1	3	Connectivity and Growth: No connectivity and growth benefit as modal filters do not create new connections or increase permeability. Modal Shift: The MS impact of two modal filters in this location is likely to be negligible, though may have a large impact on rat running along this route, therefore improving the appeal of cycling; this is still more likely to be leisure cycling during quieter periods than having a significant uplift impact on peak time commutes. Optimisation: Optimisation score of 1 given as the reduction in rat running to bypass the A1214/Woobridge Road/Main Road will make cycling safer and more appealing in this location. Safety: Safety is increased for reasons outlined above. Biodiversity: No anticipated biodiversity impact. Leisure: Leisure score of one given for reasons stated above.
Little Bealings	328	Playford Road - east of junction with The Street and Hall Road.	Playford Road and Martlesham Road has become much busier with through traffic between Ipswich and Woodbridge making it less unpleasant and much less safe to cycle on. The road was very popular during the lockdown when there was little or no traffic, as those new to cycling and those wanting to encourage their children to cycle found out.	Close the road to the east of the junction along with closure further to the west so that cyclists have a safe and attractive route between Ipswich and Woodbridge, whilst allowing car drivers to reach Bealings from the A1214 if necessary.	0	0	1	1	0	1	3	Connectivity and Growth - No connectivity and growth benefit as modal filters do not create new connections or increase permeability. Modal Shift - The Modal Shift impact of two modal filters in this location is likely to be negligible, though may have a large impact on rat running along this route, therefore improving the appeal of cycling; this is still more likely to be leisure cycling during quieter periods than having a significant uplift impact on peak time commutes. Optimisation - Optimisation score of 1 given as the reduction in rat running to bypass the A1214/Woobridge Road/Main Road will make cycling safer and more appealing in this location. Safety - Safety is increased for reasons outlined above. Biodiversity - No anticipated biodiversity impact. Leisure - Leisure score of one given for reasons stated above.

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Lowestoft	22	B1532 (Marine Parade) in Lowestoft	This route is part of the Suffolk County Council Lowestoft Cycle route and designated a On-Road signed cycle route and approx 2km in length. Unfortunately due to lack of upgrading or maintenance around 80% of the white lines separating vehicles from cyclists have faded into the tarmac and now indistinguishable for motorists and cyclists. The only short parts of the cycle route which have been painted are those where the highways agency have completed recent road repairs see attached photo's.	Paint the white lines please along the length of Marine Parade which will link Pakefield in the South to Lowestoft town centre in the North.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Lowestoft	23	From Arbor Lane to Pakefield Rd along the current cliff top footpath	Link Pakefield (Arbor Lane) to Pakefield Road along the scenic cliff top and waterfront, with some will and a little modification to existing pedestrian infrastructure along a 1km section we could have a continuous 3km cycling route linking up to the traffic free sea-front and onto Lowestoft town centre, that is a winner for all.	Currently as you can see in the attached photographs this 1km section is narrow along parts of the route and even passing pedestrians have to step off the footpath which is also a popular route for cyclists especially school children cycling to local schools, yes I know cyclists are supposed to dismount and walk this 1km section but lets move on and grasp the nettle and make it a harmonious link for both pedestrians and cyclists from Pakefield and into Lowestoft, a win-win for all especially school children.	3	2	0	0	-1	3	7	Connectivity and Growth - This route is positioned on and forms a significant section of a key corridor within Lowestoft. Modal Shift - No PCT data exists as it is a footpath, however running parallel is London Road which shows significant modal shift potential. Clearly if this route is improved not every user will move from London Road so the potential modal shift has been split between the two routes. Furthermore the proposed infrastructure is assumed to the highest standard as an off-road route so a score of 2 has been given. Optimisation - As a footpath the creation a cycle route is considered 'new' as opposed to an optimisation of the existing. The pedestrian aspect is unlikely to be significantly improved. Safety - No significant safety benefit. Biodiversity - The widening of the path could result in the loss of grassed areas beside the path, for the most part these are managed grass areas, but it is over a significant area. Leisure - This is an important leisure route that runs alongside the coast.
Lowestoft	31	Roundabout A47 and Corton Long Lane - to Suffolk Border before Hopton!	Cycle path ends with no path from this roundabout to the Suffolk Border above Hopton. Where on the Norfolk side there is from Gt Yarmouth a cycle path from Gorleston to Hopton and this is where it ends.	A12 upgrade to A47 never improved the cycle ways infrastructure.	3	3	0	3	-2	0	7	Connectivity and Growth - The current route is indirect, but creating a more direct route provides connections between Lowestoft and Gorleston which are both sizeable towns meaning it receives the top score. Modal Shift - Using PCT it shows that upgrading the A47 or the current route will have significant modal shift. Considered together it gives the highest score. Optimisation - This does not optimise existing infrastructure. Safety - This will ensure that cyclists are either taken off the A47 (PCT suggests some, although not a significant number use this route) or off Coast Road. Getting people off the A47 by providing a more direct route provides a good opportunity for safety improvements. Biodiversity - The exact placement of the route is not clear, the comment suggests the route should be alongside the A47. Such a route would likely involve some vegetation removal whether cut verge which could score a minus 1 or trees which could score minus 3. A minus 2 is considered a reasonable score at this stage. Leisure - A connection between Hopton to Lowestoft would be considered a more commuter route than leisure, any leisure benefits would be relatively modest giving a neutral score.
Lowestoft	32	Lowestoft Town centre	No cycle path through precinct like there is marked out on sea front.	Designate a marked out path through Town Centre for cycles.	3	1	0	1	0	3	8	Connectivity and Growth - The town centre is the destination in itself with close access to the train station and Old High Street meaning a top score is considered reasonable here. Modal Shift - As the town centre is largely pedestrianised potential markings provide a better option than most on-road options and would be almost a shared surface. Some form of segregation would need to be applied to be current LTN1/20 compliant. PCT suggests that the roads flanking the town centre would achieve a modest modal shift if they are improved to a poor standard, so it is reasonable to assume creating this direct route would achieve something similar resulting in a score of 1. Optimisation - Not an existing cycle route so does not represent an optimisation. Safety - Would divert cyclists away from Battery Green Road which is a busy, albeit a 30mph road meaning it scores 1 point. Biodiversity - There are no significant biodiversity benefit. Leisure - There is a leisure benefit of connecting through the town centre this will allow direct connection to shops/cafes and other town centre uses.
Lowestoft	40	path linking Old Lane and Gunton Avenue Corton	is very narrow for shared use by cycle and pedestrian traffic has become rather overgrown making things worse, its difficult to get out of the way of cyclists and problem to social distance.	Keeping undergrowth cut back, while appreciate not possible to widen for whole distance some widening would make it safer for all	0	1	1	0	0	0	2	Connectivity and Growth - A shared pathway already exists and whilst on a key corridor some widening of the path (where possible) will not provide significant connectivity and/or growth. Modal Shift - The path is already a reasonable standard (off-road shared path) and PCT suggests limited modal shift potential. However, it is noted this doesn't factor in the Garden Neighbourhood to the north and this would be one of, if not the main, route into Lowestoft for cyclists so a score has been given to reflect this. Optimisation - This represents an improvement as opposed to an upgrade to a cycle/walking route type. It may not be possible to widen the whole route although allocation WLP2.20 may offer some aid here. Safety - As an existing off-road route it has not scored under safety. Biodiversity - Potential for small loss' of some verges to the south. Not deemed a significant loss to warrant a negative score. Leisure - This is considered more of a commuter and 'everyday' route and is not considered to create a significant leisure benefit.
Lowestoft	48	The end of Hamilton Road and the steps that connect it to the North Parade (Lat: 52.47643 Lon: 1.76064)	The steep steps from the end of Hamilton Road to the North Parade create a severe hazard and obstacle for cyclists and disabled who otherwise could have an uninterrupted route from the north end of Lowestoft down to Pakefield in the south. Replacing the steps with a ramp will allow tourists to travel from one end of the town to the other on a scenic route and one that follows the route of the coastal pathway.	a ramp	0	0	1	0	0	2	3	Connectivity and Growth - The additional access provided does not connect to any additional services instead it adds Leisure benefit meaning it does not score for this topic. Modal Shift - The access would only be to a small section of the coastal path and the numbers involved means it would not score significantly under modal shift. Optimisation - The improvements provides greater accessibility and inclusivity optimising an existing pathway scoring a point here. Safety - Whilst it is recognised that the stairs provided an impediment, this impediment means that access is blocked and the addition of the ramp won't provide a safety benefit as it is currently not possible to access. Biodiversity - No significant biodiversity benefit. Leisure - The seafront is a key strategic leisure location, whilst its noted access is available further north the importance of the location for leisure purposes and the inhibiting nature of the stairs means it scores a 2.

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Lowestoft	60	Gorleston Road (as an example)	The cycle lanes throughout Lowestoft all need repainting.	Paint plus workers							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Lowestoft	86	Cycle path... no cycle paths shown on the map so difficult to locate!	No dropped kerb on cycle path at this location	Install dropped kerb	0	0	1	1	0	0	2	Connectivity and Growth – A dropped kerb will provide some connectivity for some users, but connections are still available. Modal Shift – This improvement is not considered to provide a significant modal shift benefit. Optimisation – The cyclists or pedestrian (particularly if they have impaired mobility) will be forced to drop onto the road to move through Laxfield Way so for minor work a score of 1 is considered reasonable. Safety – The road lacks dropped kerbs generally meaning most cyclists will cross the raised kerb instead of taking the indirect approach of finding the nearest dropped kerb. This could represent a modest benefit warranting 1 point. Biodiversity – No significant biodiversity benefit. Leisure – There is no significant leisure benefit.
Lowestoft	124	The non car section of Raglan street, outside Jacobs Court, Lowestoft	This area is a designated cycle way but the bollards preventing cars from using the area for parking have not been replaced and cars park on here sometimes completely blocking the way for cyclists to negotiate through.	Replace the bollards so cars cannot be parked on the paved section. Maybe make signage more obvious.	0	1	1	0	0	0	2	Connectivity and Growth - The parked cars lessen the value to the traffic free section but their removal would not provide significant connectivity benefit. Modal Shift - Raglan Street shows significant growth potential under PCT however the traffic free section conversely is both underused and with low growth potential. This could be partially explained if the parked vehicles caused obstruction or required cyclists to dismount explaining why the greater use diverts around Cathcart and Jacobs Street. The PCT figures for Raglan Street suggests some benefit and a score of 1 is deemed appropriate despite the specific sections low growth potential according to PCT. Optimisation - Avoiding parked cars becoming an obstacle will provide an optimisation opportunity and a score of 1 is deemed appropriate. Safety - whilst parked cars do form an obstacle that may require cyclists to dismount it is not considered a significant safety issue and currently most cyclists appear to divert around this section. Biodiversity - No biodiversity impact. Leisure - This improvement appears to have limited leisure benefit.
Lowestoft	125	Dip Farm football pitches off Corton Road, Lowestoft	There is no where secure to lock a bicycle by the changing rooms car park area. With the popularity of the facility growing with the use by Waveney FC this has seen the car park heavily congested on busy match days and cars also create a hazard by parking along Corton Road often blocking the pavement. Putting a decent numbers of cycle racks here may encourage match goers to cycle instead of drive.	Install a generous number cycle racks	0	1	0	0	0	1	2	Connectivity and Growth – No significant connectivity and growth impacts. Modal Shift - Without full disposition of the parking it is a matter of judgement. Cycle Parking alone is unlikely to encourage large numbers of modal shift, but a certain level will be provided so a score of 1 is deemed appropriate. Optimisation – Whilst there is an element of on-road cycle infrastructure to the front cycle parking within the ground is unlikely to optimise the route significantly, particularly as most users are likely using the NCR as opposed to visiting the playingfield. Safety – No significant safety benefit. Biodiversity – No significant biodiversity benefit. Leisure – The playingfield provides leisure opportunities although likely only to a local significance giving this a score of 1.
Lowestoft	126	Corton Road, Lowestoft	The painted on cycle lanes along the length of Corton Road have been allowed to fade (like a lot of other cycles lanes on other roads in Lowestoft) and have not been repainted. The presence of these lanes and provide reassurance to cyclists using the road.	Repaint and maintain the cycle lanes.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Lowestoft	127	High Street between Camden Street and Mariners Street, Lowestoft	Cycles are permitted to ride south along this part and there is no contraflow cycle lane painted onto the road. If one was here it would give confidence to people cycling in that direction and also remind motorists this is permitted. The southern end of high street between Dukes head street and the Triangle market area, also needs resurfacing as its becoming very uncomfortable and bumpy when cycling over.	Paint a contraflow cycle lane and resurface the High street where it needs doing.	2	0	0	1	0	1	4	Connectivity and Growth - This section partially connects into the Old High Street and improves connection to the town centre. These key locations means it score a 3, however the infrastructure is likely to be lower quality and an alternative route exists along Jubilee Road so a score of 2 is appropriate. Modal Shift - PCT suggests there is limited cycling here, but Jubilee Road parallel does potentially have high use. This improvement may take some of these cyclists, however as a low quality improvement a score of 0 has been given. Optimisation - No optimisation benefit, the maintenance is not a matter for this project. Safety - A 30mph road, but an awkward junction so a score of 1 is deemed reasonable. Biodiversity - No biodiversity impact. Leisure - This route will help connect into the Old High Street, but lack of high quality and cohesive route limits overall impact.
Lowestoft	128	Gunton church lane near Yarmouth Road, Lowestoft	Accessing the cycle path can be difficult at busy times such as the school run as queues of traffic build up past Glebe close and sit too close to the kerb to be able to get past.	Make the pavement between Glebe Close and Yarmouth road shared use or paint a cycle lane on Gunton church lane to try and encourage motorists to leave a gap for cyclists.	0	1	0	1	0	0	2	Connectivity and Growth - The suggestion is for a small section of Gunton Church Lane so on its own doesn't offer significant connectivity. Modal Shift - PCT suggests improvements to a significant section of Gunton Church lane could yield significant modal shift, however the suggestion is for a smaller section so a score of 1 is deemed sufficient. Optimisation - This is new infrastructure so no optimisation benefit. Safety - The road is 30mph and would normally be relatively quiet, however school traffic can pose an obstacle so a score of 1 is deemed sufficient. Biodiversity - There are no significant biodiversity impact. Some managed grass could be lost if path is widened. Leisure - The Leisure benefit is limited although it is noted it connects to some attractive routes to the north.
Lowestoft	187	Lowestoft High Street, south of A47 near petrol garage and north of A47 near Artillery Way	The High Street has some interesting shops such as a zero waste shop, a bakers, Post Office, but the number of cycle racks there is extremely limited. It is a main route into Lowestoft from the wards of Gunton and St Margaret's and really should be better served with bike racks.		0	1	1	0	0	2	4	Connectivity and Growth – No significant connectivity and growth impacts. Modal Shift – Without full disposition of the parking it is a matter of judgement. Cycle Parking alone is unlikely to encourage large numbers of modal shift, but a certain level will be provided. Optimisation – The cycle parking adds to the existing infrastructure and this is a well used route with on-road markings so a single point has given provided. Safety – No significant safety benefit Biodiversity – No significant biodiversity benefit. Leisure – The High Street represents a strong leisure centre as it contains café/restaurant offers, heritage buildings and local attractions according the improvements will also have a strong impact giving 2 points.

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Lowestoft	189	Top of Lowestoft High Street at its junction with the A47 heading south and the junction with the north bound 2 lanes of the A47 there	If cycling north up Lowestoft High Street, when one comes to the A47 junction, there is no dedicated cycle route north. There is a cycle route south along the A47, but nothing the other way. Cyclists then have to traverse 2 lanes of the south bound A47 at a sharp bend by the petrol station, then cycle to the 2 lanes of the north bound A47 cross these and then get to head north. Crossing 4 lanes of a Highways England road, the main artery from Lowestoft to Yarmouth is a health and safety issue.	Provide a cycle route northwards from the High Street that does not involve crossing 4 lanes of A47 traffic.	3	2	0	2	0	2	9	Connectivity and Growth - This improvement is located on a key corridor with direct connections into the old High Street. Modal Shift - PCT suggests a high level of growth potential if quality infrastructure is provided. Optimisation - This is new infrastructure so has no optimisation benefit. Safety - The road is 30mph, but busy and with HGV traffic so a score of 2 is deemed reasonable if cyclists are taken wholly off the road. Biodiversity - There are not significant biodiversity impacts. Leisure - Connects through to the Old High Street which has leisure benefit. The improvement is comprehensive and connects to other routes giving it a good score.
Lowestoft	190	Lack of sufficient cycle racks in the pedestrianised London Road North	There are insufficient cycle racks in the main retail area of town. There should be significantly more to encourage people to cycle into town.		0	1	1	0	0	2	4	Connectivity and Growth – No significant connectivity and growth impacts. Modal Shift – Without full disposition of the parking it is a matter of judgement. Cycle Parking alone is unlikely to encourage large numbers of modal shift, but a certain level will be provided. Optimisation – The cycle parking adds to the existing infrastructure and this is a well used route with on-road markings so a single point has given provided. Safety – No significant safety benefit Biodiversity – No significant biodiversity benefit. Leisure – The High Street represents a strong leisure centre as it contains café/restaurant offers, heritage buildings and local attractions according to the improvements will also have a strong impact giving 2 points.
Lowestoft	219	The Road surface between The Falcon Public House and Mariners Street.	The road surface heading south as you leave the cycle lane and head passed the Falcon public house is unsuitable for road bikes. It has been patched hundreds of times over a period of many years and is now unfit for cycling without a mountain bike.	The road needs resurfacing.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Lowestoft	220	At the junction of Sussex road and Yarmouth road.	After some light rain the road here floods because of an ongoing problem with drainage. unfortunately there is a serious pothole next to a sunken drain cover which can end up submerged. If a cyclist was to ride through the flood and hit the pothole the accident would be serious.	This has been reported to Highways on a number of times with little effect. The flooding has been continuous for many years. You wouldn't think it would be too hard to drain an area like Yarmouth road which is on the top of a hill! (The Ravine). it needs a new drain and the pothole filling before someone gets hurt.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Lowestoft	221	Cycle Lane on Corton Road	There is a designated Cycle lane running the length of the Corton Rd, that no one can use because there are always cars parked in it. It feels dangerous as a cyclist to have to constantly overtake these parked vehicles without a designated Cycle Lane.	Move the cycle lane to the outside of the parked vehicles as they do in Holland, and similar to the High Street outside the Lighthouse.	0	0	1	1	0	1	3	Connectivity and Growth – The proposed alteration does not create additional connectivity. Modal Shift – The on-road cycle lane remains so no modal shift. Optimisation – The parked cars removes the viability of the cycle lane so whilst it is not suggesting an improvement to the type of cycle infrastructure it will optimise its use so is deemed a 1. Safety – Currently cyclists have to negate parked vehicles, whilst on-road markings do not offer significant safety benefits it will alert of drivers to their presence and stop the need to head into the road regularly meaning it is deemed a 1. Biodiversity – No significant biodiversity benefit. Leisure – There are a number of green spaces and a sports pitch to the north which this on-road cycle lane connects into meaning it has a modest leisure benefit.
Lowestoft	244	Slip roads gap beside A47,	Purposefully blocked footpath and cycle path that has been in use for a minimum of 40 years. The access between the slip road from Gunton Church Lane going north west has been blocked by Heras fencing, soil and twigs, even though there are 2 concrete bollards denoting where there is access. The blocking of this path, means that cyclists and pedestrians, including school children now have to go on to a very narrow path beside the A47. This does not fit with the active travel policy.	Unblocking of the gap to allow access and so maintain active travel away from the narrow A47 and narrow paths to the side of this road that are often overgrown with vegetation that narrows them further.	3	1	0	3	-1	1	7	This assessment is on connecting the two service roads together to avoid briefly re-entering the A47 and does not comment on any status of any footpath. Connectivity and Growth - The location is on a key arterial route through Lowestoft and a marked key corridor. Without this improvement cyclists would need to enter the A47 (albeit very briefly) which currently disrupts a cohesive path meaning a top score has been given. Modal Shift - PCT suggests this section of road if improved to a high standard would receive significant modal shift. However use of the service road would still entail sharing a surface with cars even though the road is very quiet. Accordingly a lower standard has been assessed and a score of 1 given. Optimisation - No judgement has been made on the status of the connection between the two service roads as this is a matter for SCC. Should this be deemed a footpath then additional weight would be added to this category. Safety - Whilst the A47 is 30mph, it is a busy arterial route with heavy HGV use. Using the A47 should be avoided. Entering and leaving the road in quick succession without suitable infrastructure only adds to the potential safety score. Biodiversity - At this stage the level of biodiversity assets that may need to be removed is unclear, any works should look to avoid any significant losses. This score could adjust with further information. Leisure - With eventual links through to the Old High Street a modest score has been given. Given its distance from the high street a higher score is not deemed suitable. The improvement also provides access to Foxburrow Wood more directly.
Lowestoft	245	At the end of the sea wall, at the end of Hamilton Road by the Onward.	Only steep step access, which is very steep, at the end of the sea wall, which is supposed to be part of the national coastal path. Bicycles using the sea wall to gain access to and from the town have to cycle through an industrial estate to Ness Point to get to the sea wall. It is near impossible to get a bike up these steps by yourself.	A ramp would be ideal for cyclists and pedestrians, including those who have mobility difficulties.	0	0	1	0	0	2	3	Connectivity and Growth – The additional access provided does not connect to any additional services instead it adds Leisure benefit meaning it does not score for this topic. Modal Shift – The access would only be to a small section of the coastal path and the numbers involved means it would not score significantly under modal shift. Optimisation – The improvements provides greater accessibility and inclusivity optimising an existing pathway scoring a point here. Safety – Whilst it is recognised that the stairs provided an impediment, this impediment means that access is blocked and the addition of the ramp won't provide a safety benefit as it is currently not possible to access. Biodiversity – No significant biodiversity benefit. Leisure – The seafront is a key strategic leisure location, whilst its noted access is available further north the importance of the location for leisure purposes and the inhibiting nature of the stairs means it scores a 2.

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Lowestoft	256	From Triangle market to top of High st. From Yarmouth Road to Yarmouth	There is no route that continues from the High St to the villages of Blundeston, Lound and Hopton. Lanes are faded and poorly maintained.	Enforce parking rules in the High St, repair the cycle path between Sussex Rd and Harris Avenue. Create a shared path through to Blundeston Roundabout, there are few pedestrians except when the schools comes out .and this is made worse by parents parking on the cycle path to collect their children. Create a purpose built cycle track either side of the Yarmouth Rd through to Yarmouth, Introduce a signal that allows cyclists to leave a traffic light before cars.	3	3	0	2	0	2	10	Connectivity and Growth - This improvement is located on a key corridor and will connect to the Old High Street. Modal Shift - PCT suggests a very high level of growth potential if quality infrastructure is provided. Optimisation - This is new infrastructure so has no optimisation benefit. Safety - The road is 30mph, but busy and with HGV traffic so a score of 2 is deemed reasonable if cyclists are taken wholly off the road. Biodiversity - There are not significant biodiversity impacts. Leisure - A comprehensive route that connects through to the Old High Street and town centre has leisure benefit. The improvement is comprehensive and connects to other routes giving it a good score.
Lowestoft	277	East side of A47 Yarmouth Road, Lowestoft between Gunton Church lane & Weston Road	Running parallel with the A47 are two slip roads that are closed for vehicular traffic as shown. Between the two slips was access for pedestrians & cycles, frequently used by children from Benjamin Britten High & Gunton Primary together with many pedestrians. Although this access is most likely privately owned access has been available for 40 years that I'm aware of. Access was blocked last March by a tree stump and barriers.	Application has been made to Highways to have the route classified as a footpath	3	1	0	3	-1	1	7	This assessment is on connecting the two service roads together to avoid briefly re-entering the A47 and does not comment on any status of any footpath. Connectivity and Growth - The location is on a key arterial route through Lowestoft and a marked key corridor. Without this improvement cyclists would need to enter the A47 (albeit very briefly) which currently disrupts a cohesive path meaning a top score has been given. Modal Shift - PCT suggests this section of road if improved to a high standard would receive significant modal shift. However use of the service road would still entail sharing a surface with cars even though the road is very quiet. Accordingly a lower standard has been assessed and a score of 1 given. Optimisation - No judgement has been made on the status of the connection between the two service roads as this is a matter for SCC. Should this be deemed a footpath then additional weight would be added to this category. Safety - Whilst the A47 is 30mph, it is a busy arterial route with heavy HGV use. Using the A47 should be avoided. Entering and leaving the road in quick succession without suitable infrastructure only adds to the potential safety score. Biodiversity - At this stage the level of biodiversity assets that may need to be removed is unclear, any works should look to avoid any significant losses. This score could adjust with further information. Leisure - With eventual links through to the Old High Street a modest score has been given. Given it distance from the high street a higher score is not deemed suitable. The improvement also provides access to Foxburrow Wood more directly.
Lowestoft	283	The link from Normanton Park to Harbour Road via the rail footbridge	Not only is the footbridge difficult to negotiate with a bicycle or a pushchair once you are on the south side you are dumped in to a sort of no man's land. there is an urgent need for this connection to Harbour road to be sorted out across the waste land rather than down to the foreshore, which of itself is unsatisfactory.	Get a decent, direct and surfaced path across the wasteland at the end of Harbour Road up to the railway bridge. Both East Suffolk and Suffolk CC have adequate powers to secure a route here. it must be possible to engineer a better solution to crossing the railway bridge that exists at present. Improving this route has been a long term aim of the council for years and yet nothing happens. Why not?							N/A	The application has been submitted and approved. Work is ongoing with this project.
Lowestoft	383	Denmark Road cycle path from station to Rotterdam Road	This must be the worst and most dangerous cycle path in the country. It is extremely uneven and shakes bones and bikes unbearably. There is also a concrete obstruction along with at least one place where the kerb has not been dropped.	Re-lay the path and drop the kerbs where required. Not sure what the obstruction is so unsure if it can be moved. Maybe designate the path on the opposite side as a shared footpath/cycle path as it is plenty wide enough along most of its length.	0	3	3	0	-1	3	8	Connectivity and Growth - An existing connection does exist in the form of an off-road shared path, whilst it is narrow and of poor quality this improvement provides improvements but not additional connectivity. Modal Shift - PCT shows that improving to the highest standard creates a high level of modal shift. Optimisation - This is a key route and improvements optimises an existing path. Safety - Cyclists are currently off-road albeit on a sub-par section of infrastructure so there is limited safety benefit. Leisure - This section provides a direct link into the town centre and train station so has a high leisure benefit.
Lowestoft	386	Cycle path outside Claremont Pier	Cyclists are asked to dismount for the short section passing the pier. I can see this may have been done for the safety of pedestrians, but think a warning to go slow and also for pedestrians to be aware of cyclist would be better.		1	0	0	0	0	2	3	Connectivity and Growth - The site is on a key corridor through Lowestoft, however this represents a very small section of the overall route so a score of 1 is deemed appropriate. Modal Shift - The small section of the overall route means there is unlikely to be a significant modal shift. Optimisation - A new piece of infrastructure, albeit a continuation of existing sections either side. Safety - No significant safety issues, however this category is concerned predominantly with conflict with vehicles and it is recognised that conflict with pedestrians could be an issue here. Biodiversity - No significant biodiversity issues. Leisure - This is a key leisure destination and facilitating improvements here will have a disproportionately high benefit to the visitor economy. As this represents a very small section, a score of 2 is deemed appropriate.
Lowestoft	387	Bridge	Cyclists and pedestrians share the path on both sides of the bridge. It's not very clear to pedestrians as they often give me abuse!	Better signs or separate lanes for bikes & pedestrians	2	3	2	0	0	2	9	Connectivity and Growth - A new cycle lane will provide additional connectivity into a key strategic location as the bridge does not allow cyclists requiring them to dismount so a new cycle lane provides a less disrupted path. However, it is not a significant hinderance so a full score is not considered reasonable. Modal Shift - PCT shows a high use of the bridge currently, but this significantly rise with good quality infrastructure which is possible (this doesn't factor in the third river crossing). A cycle lane would represent high quality infrastructure so scores a full 3. Optimisation - The cycle Lane represents an optimisation of the existing bridge by allowing improvements to the existing pedestrian pathway meaning it receives a score of 2. Safety - Whilst cyclists are forced to dismount currently they are adequately separated from vehicles so it is not considered a significant safety improvement. Biodiversity - There are no significant Biodiversity impacts Leisure - This is will improve connections between several important leisure locations i.e. train station, town centre and seafront. However, the bridge is not a significant hinderance so a full score should not be awarded.
Lowestoft	388	Peto Way heading towards Wickes	No cycle path on left of road so have to ride on the road. The cycle path on the other side is difficult to get to as you have to cross 2 lanes.	Cycle lane, or make it easier to get to lane on other side	0	1	0	1	-1	0	1	Connectivity and Growth - The improvement does reside on a key corridor through the town, however with good cycle infrastructure already on the road, connections do exist albeit with the need to cross the road. Modal Shift - PCT suggests some modest potential for modal shift growth. As the infrastructure is already at the highest standard it is unclear whether this would achieve the full modal shift,

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												however providing high quality infrastructure on both sides of the road is still an improvement. Optimisation - This represents new infrastructure so doesn't score for optimisation. Safety - The road is 30mph, but likely reasonably busy and additional traffic could be created by the third river crossing. The crossing at Normanston Park is light controlled, but between the commercial units it is not so a score of 1 is deemed appropriate. Biodiversity - A new cycle path to the appropriate standard would involve the removal of a managed grass verge, but could also eat into the denser foliage adjacent. A score of -1 has been given, but if trees need to be removed this could become a -2. Leisure - This improvement has limited leisure gain. Whilst connecting to Normanston Park offers a benefit, good connections already exist.
Lowestoft	413	Cycle path/pavement along Tom Crisp Way into Lowestoft	Separate pedestrian and cycle ways. Dog walkers, people with children and prams/pushchairs etc have very different requirements from cyclists. This is particularly bad over the Bascule bridge which is marked as combined cycle path/pavement but not really suitable	Clearly mark the pedestrian and cycle parts separately. Consider adding dedicated cycle lane on the road where pavement can't be widened	0	1	1	0	0	0	2	Connectivity and Growth - The route already provides good connections to the town centre and other locations so the improvement would not provide significant connectivity benefits. Modal Shift - PCT suggests a very high potential for growth here, however the route is already to a high standard. Datashine suggests limited pedestrian commuting in this area, but again the improvements are relatively modest. Moving to the highest standard by segregating cyclists and pedestrians is unlikely to result in the significant growth shown on PCT so a score of 1 is deemed reasonable. Optimisation - The improvement remains in the higher category in separating cyclists from the road. Safety - This category is primarily concerned with conflict with vehicles so there isn't significant safety benefit. Biodiversity - No significant biodiversity benefit. Leisure - The route is considered a largely commuter and service route.
Lowestoft	418	Ness Point	No cycle storage or racks whatsoever at Ness Point for people to lock up there bike! Britains most Easterly Point	More bike racks	0	0	1	0	0	2	3	Connectivity and Growth - The provision of cycle parking is unlikely to create significant connectivity and growth benefit. Modal Shift - To park at Ness Point would provide leisure benefit as opposed to the day-to-day benefit that would gain modal shift. Optimisation - The cycle parking would improve and optimise the wider route. As a relatively minor improvements a score of 1 is deemed appropriate. Safety - There are no significant safety benefits. Biodiversity - There are no significant biodiversity benefits. Leisure - This route has high leisure use with views of the sea, Ness Point and the new East Point park. Cycle parking would encourage greater use.
Lowestoft	490	On the cycle path running adjacent to Tom Crisp Way, South West of the main traffic light junction with Carlton Road and Long Road.	Steel post erected on the cycle path. This is a part of a sign (which consists of two posts) notifying road users of the distance to various destinations. One post is in the cycle lane, the other is in the grass verge. This post poses a heightened risk of a collision with it, especially in the dark where it can become near enough impossible to see it with the glare from oncoming vehicles when travelling North East on the cycle path.	To remove the post and if possible the sign. If it is still needed, have a smaller sign which would only need the use of one post.	0	0	1	0	0	0	1	Connectivity and Growth - There are no significant connectivity and growth benefits. Modal Shift - This is unlikely to create significant modal shift. Optimisation - This will improve the usability of the path to a modest degree. Safety - This category largely relates to interactions between cyclists/pedestrians and vehicles. Some safety improvement could occur here, but this would be weighed against the highway need. Overall a score of 0 is considered reasonable. Biodiversity - There are no significant biodiversity benefits. Leisure - This improvement is not considered to have a significant leisure benefit.
Lowestoft	492	On the cycle path running adjacent to Tom Crispway.	The use of multiple posts in the middle of the path to notify users of what he path is for.	These posts seem to offer little or no purpose. But what they do offer is an increased risk of a collision due to a cyclist crashing into a post which has no need to be there in the first place. In contrast, you wouldn't have a post in a road for no particular reason. One improvement would be to remove all the posts that have little or no reason for being there. I recognise the purpose of some of these to cause an obstruction to vehicles potentially using the paths, but ones like these are a danger.	0	0	1	0	0	0	1	Connectivity and Growth - There are no significant connectivity and growth benefit. Modal Shift - This is unlikely to create significant modal shift. Optimisation - This will improve the usability of the path to a modest degree. Safety - This category largely relates to interactions between cyclists/pedestrians and vehicles. Some safety improvement could occur here, but this would be weighed against the highway need. Overall a score of 0 is considered reasonable. Biodiversity - There are no significant biodiversity benefits. Leisure - This improvement is not considered to have a significant leisure benefit.
Lowestoft	493	The Bascule bridge in Lowestoft	This is a pinch point for cyclists & pedestrians crossing from south Lowestoft to North Lowestoft and vice versa. It is not easy to cycle or even push your cycle across this bridge at busy times. On the north-east side there is rather a lot of "street furniture" to contend with.	There are currently 3 lanes for motorised traffic crossing this bridge. It would be better if there were only 2 lanes for traffic and a half-lane on either side for cyclists.	2	3	2	0	0	2	9	Connectivity and Growth - A new cycle lane will provide additional connectivity into a key strategic location as the bridge does not allow cyclists requiring them to dismount so a new cycle lane provides a less disrupted path. However, it is not a significant hinderance so a full score is not considered reasonable. Modal Shift - PCT shows high use of the bridge currently, but this could significantly rise with good quality infrastructure which is possible (this doesn't factor in the third river crossing). A cycle lane would represent high quality infrastructure so scores a full 3. Optimisation - The cycle Lane represents an optimisation of the existing bridge by allowing improvements to the existing pedestrian pathway meaning it receives a score of 2. Safety - Whilst cyclists are forced to dismount currently, they are adequately separated from vehicles so it is not considered a significant safety improvement. Biodiversity - There are no significant Biodiversity impacts. Leisure - This will improve connections between several important leisure locations i.e. train station, town centre and seafront. However, the bridge is not a significant hinderance so a full score should not be awarded.
Lowestoft	522	Battery Green road in Lowestoft, as it approaches the bascule bridge crossing it is dual carriageway.	There is no cycle lane along Battery Green road which is an approach road to the bascule bridge, the only crossing point between North & South Lowestoft.	To help cycling could the nearside lane be restricted to buses, taxis and cyclists.	3	3	0	2	0	1	9	Connectivity and Growth - This road lies on a key corridor with connections to the powerpark and town centre. Modal Shift - PCT suggests significant growth if infrastructure is created to a good standard. Optimisation - This would not represent an optimisation. Safety - The road is 30mph and relatively wide, however as a main trunk road, busy and with HGV use this improvement would offer safety benefit. Biodiversity - There is no biodiversity impact unless accompanied by tree planting. Leisure - This scores 1 due to the close proximity and access afforded to the coastal path and town centre. However as an environment it is significant prohibitive even with cycle provision to not score higher for leisure.
Lowestoft	524	The A12 approach to the bascule bridge in Lowestoft	The bascule bridge is the only crossing point for cyclists between north and south Lowestoft. The cycle route from the bridge to Tom Crisp Way is not an easy route with many road crossings.	Could the inside lane of the dual carriageway be restricted to buses, taxis and cyclists only. This would make the route from the bridge to Tom Crisp Way a much easier and safer route for cyclists.	2	3	2	0	0	2	9	Connectivity and Growth - A new cycle lane will provide additional connectivity into a key strategic location as the bridge does not allow cyclists requiring them to dismount so a new cycle lane provides a less disrupted path. However it is not a significant hinderance so a full score is not considered reasonable. Modal Shift - PCT shows high use of the bridge currently with potential for significant rise with good quality infrastructure which is possible (this doesn't factor in the third river

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												crossing). A cycle lane would represent high quality infrastructure so scores a full 3. Optimisation – The cycle Lane represents an optimisation of the existing bridge by allowing improvements to the existing pedestrian pathway meaning it receives a score of 2. Safety – Whilst cyclists are forced to dismount currently they are adequately separated from vehicles so it is not considered a significant safety improvement. Biodiversity – There are no significant Biodiversity impacts Leisure – This is will improve connections between several important leisure locations i.e. train station, town centre and seafront. However the bridge is not a significant hinderance so a full score should not be awarded.
Lowestoft	525	Lowestoft to Hopton	The Suffolk Coastal Path starts/finishes at Royal Plain in Lowestoft. The Norfolk Coastal Path starts/finishes at Hopton.	Could some serious consideration be given to connecting the Norfolk Coastal Path at Hopton to the Suffolk Coastal Path at Lowestoft.	3	1	0	3	-1	3	9	Connectivity and Growth – Creating a route between Lowestoft and Hopton is part of the key corridor. Whilst remaining close to the coast may not provide the most direct route it would still have these benefits. Modal Shift – Using PCT it shows that upgrading the A47 and coast Road will have significant modal shift. Some of these numbers could utilise the coastal path instead, however it wouldn't be expected that the full modal shift will occur as many will take the alternative routes so a score of 1 is deemed reasonable. Optimisation – This does not optimise existing infrastructure Safety – This will ensure that some cyclists either are taken off the A47, but it is more likely that it will be cyclists using the Coast Road which will utilise this path or entirely new leisure cyclists. Coast Road is national speed limit and it is considered reasonable to score 3. Biodiversity – The pathway crosses what appears to be an unmanaged grass area, that borders agricultural land. The value of this land appears limited, however if it is part of a dune ecosystem its value may be greater. Leisure – As a costal path thats off-road this has high potential leisure value as a destination in its own right.
Lowestoft	526	East coast of Suffolk	The longest single signed cycle route in the world, approx. 6,000Km, is signed along the north Lowestoft sea wall and around the Gunton St Peter's estate. Each year many people travel from all around the world to cycle this route. Currently, the route goes from Norwich to Beccles and stays inland to Harwich missing out on the Suffolk coast.	In conjunction with Sustrans could some serious consideration be given to routing the North Sea Cycle Route from Beccles to Lowestoft and follow the Suffolk coast down to Harwich.							N/A	The comments raised have been considered in the formation of the strategy, however they are too broad to be scored under the MCAF system.
Lowestoft	527	Junction of the A47 Yarmouth Rd and Gunton St Peters Ave or anywhere cycle paths cross side roads.	Cyclists are required to stop at each side road.	This may be too radical for 2021 Lowestoft but it would be good to start thinking as the Dutch do - priority to cyclists. Instead of cycle paths stopping each time they cross a side road make the traffic stop and make the cycle path the priority. This would encourage cyclist to use cycle paths. As you probably know Cambridge are trialling a "Dutch" style roundabout giving priority to cyclists. One day we will catch up with the Dutch and cycling in the UK will be safe. Priorities will be cyclists, pedestrians, motorised traffic.	0	0	1	1	0	0	2	Connectivity and Growth - The crossing would not have significant Connectivity and Growth benefit. Modal Shift - The alteration of this crossing point would not garner significant modal shift. Optimisation - The crossing bisects 2 cycle routes either side so would serve to optimise this infrastructure. Safety - This would give more certainty to cyclists crossing the junction, however highways would need to consider whether the average driver is suitably aware of the crossing status. Biodiversity - There are no biodiversity benefit. Leisure - There is limited leisure benefit.
Lowestoft	537	Tonning Street/Bevan Street East Junction	When following the 517 (30) cycle route along Tonning Street there is no drop kerb at the traffic lights to go across to Bevan Street	Make a drop kerb near the traffic lights	0	0	1	1	0	0	2	Connectivity and Growth – A dropped kerb is not expected to provided significant connectivity and growth benefits. Modal Shift – No significant modal shift benefit. Optimisation – Whilst it doesn't contain cycle infrastructure, but does form part of NCR 517 and it is a requirement that the cyclist crosses the raised kerb meaning a score of 1 is reasonable here. Safety – The road lacks dropped kerbs generally meaning people are likely to be forced to use the raised kerb or find a less direct alternative dropped kerb. This could represent a safety benefit warranting 1 point. Biodiversity – No significant biodiversity benefit. Leisure – Whilst route 517 does eventually reach key leisure locations, there are other route options. There is no significant leisure benefit.
Lowestoft	538	Denmark Road, South side cycle track	Concrete bunker makes it difficult when passing, not to go into the road	Remove bunker	0	1	3	0	0	3	7	Connectivity and Growth - An existing connection does exist in the form of an off-road shared path, whilst it is narrow and of poor quality this improvement provides improvements but not additional connectivity. Modal Shift - PCT shows that improving to the highest standard creates a high level of modal shift. However the removal of the obstacle will not create significant modal shift on its own without further improvements. Optimisation - This is a key route and improvements optimises an existing path. Safety - Cyclists are currently off-road albeit on a poor section of infrastructure so there is limited safety benefit. Leisure - This section provides a direct link into the town centre and train station so has a high leisure benefit. Biodiversity - This comment solely relates to the removal of the obstruction to continue the pathway and as such has no significant biodiversity impact.
Lowestoft	539	Denmark Road, south side. near junction with Rotterdam Road	When reaching the end of the cycle track you have to go on to the road. You cannot cross to the cycle track on the other side as there is no drop kerb at this point on the north side.	This may all change with the construction of the new bridge. All the cycle tracks at this point should be reconsidered	0	3	3	0	-1	3	8	This has considered wider improvements along Denmark Road following the completion of the 3rd River Crossing. Connectivity and Growth - An existing connection does exist in the form of an off-road shared path, whilst it is narrow and of poor quality this improvement provides improvements but not additional connectivity. Modal Shift - PCT shows that improving to the highest standard creates a high level of modal shift. Optimisation - This is a key route and improvements optimises an existing path. Safety - Cyclists are currently off-road albeit on a poor section of infrastructure so there is limited safety benefit. Biodiversity - Should the path require widening some managed verge and scrub may require removal. Leisure - This section provides a direct link into the town centre and train station so has a high leisure benefit.

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Lowestoft	540	From the roundabout at the junction of Corton Lone Lane and A47	In addition to the lack of cycle lanes to the north of this junction on the A47 to Hopton. There are very few direct cycle lanes along the A47 to the centre of Lowestoft. There are good lanes along the new Millennium Way and also around the back roads into Lowestoft, but not a direct route down the A47	Please see if you can introduce lanes south, along the existing A47	3	3	0	3	-2	0	7	Connectivity and Growth – The current route is indirect, but creating a more direct route it provides connections between Lowestoft and Gorleston which are both sizeable towns meaning it receives the top score. Modal Shift – Using PCT it shows that upgrading the A47 or the current route will have significant modal shift. Considered together it gives the highest score. Optimisation – This does not optimise existing infrastructure Safety – This will ensure that cyclists either are taken off the A47 (PCT suggests some although not a significant number use this route) or off Coast Road. Getting people off the A47 by providing a more direct route gives this a top score. Biodiversity – The exact placement of the route is not clear, the comment suggests the route should be alongside the A47. Such a route would likely involve some vegetation removal whether cut verge which could score a minus 1 or trees which could score minus 3. A minus 2 is considered a reasonable score. Leisure – A connection between Hopton to Lowestoft would be considered a more commuter route than leisure, any leisure benefits would be relatively modest giving a neutral score.
Lowestoft	575	Between Corton Long Lane, Lowestoft, NR32 5, GBR going northwards to Hopton roundabout lack of cycle path/footpath	Cyclists/pedestrians/currently use the busy A47 or the bendy coast road B1385 which has no footpath. As a motorist I see the dangers of cyclists using this fast dual carriageway, even if they are entitled to, but people make bad choices. I have even seen a person in a mobility scooter using this road. Death wish. As a cyclist and pedestrian I use the coast road every time, but it is bendy, there is no footpath and it is a bus route.	It would be a great amenity and so much safer to have a cycle/footpath between Corton Long Lane, Lowestoft, NR32 5, GBR going northwards to link with the existing one at Hopton. It might help mobility scooter users too. Near the roundabout there would ideally have to be some kind of crossing point to link users into the existing track past St Margaret's Church, Hopton (grid ref: TG 5241 0004) on the old Lowestoft Road.	3	3	0	3	-2	0	7	Connectivity and Growth – The current route is indirect, but by creating a more direct route it provides connections between Lowestoft and Gorleston which are both sizeable towns meaning it receives the top score. Modal Shift – Using PCT it shows that upgrading the A47 or the current route will have significant modal shift. Considered together it gives the highest score. Optimisation – This does not optimise existing infrastructure Safety – This will ensure that cyclists either are taken off the A47 (PCT suggests some although not a significant number use this route) or off Coast Road. Getting people off the A47 by providing a more direct route gives this a top score. Biodiversity – The exact placement of the route is not clear, the comment suggests the route should be alongside the A47. Such a route would likely involve some vegetation removal whether cut verge which could score a minus 1 or trees which could score minus 3. A minus 2 is considered a reasonable score. Leisure – A connection between Hopton to Lowestoft would be considered a more commuter route than leisure, any leisure benefits would be relatively modest giving a neutral score.
Lowestoft	576	The clifftop cycle path/footpath at Pakefield going from The Jolly Sailors. Pakefield Street, NR33 0JS, to Arbor Lane	It's rather narrow for the amount of users it gets, especially at weekends and peak holiday times. The path is used by pedestrians, dog walkers, people in mobility scooters and cyclists and there has to be a lot of give and take between them. It can be snail pace for cyclists.	Widening of the route and having a dedicated cycle path would make life a lot easier for all concerned and allow cyclist to make progress.	3	2	0	0	-1	3	7	Connectivity and Growth - This route is positioned on and forms a significant section of a key corridor within Lowestoft. Modal Shift - No PCT data as it is a footpath, however running parallel is London Road which shows significant modal shift potential. Clearly if this route is improved not every user will move from London Road so the potential modal shift has been split between the two routes. Furthermore the growth is assumed to the highest standard as an off-road route so a score of 2 has been given. Optimisation - As a footpath the creation of a cycle route is considered 'new' as opposed to an optimisation of the existing. The pedestrian aspect is unlikely to be significantly improved. Safety - No significant safety benefit. Biodiversity - The widening of the path could result in the loss of grassed areas and more overgrown shrub areas beside the path, for the most part these are managed grass areas with low biodiversity value, but the shrubbed areas may have a greater value. Leisure - This is an important leisure route that runs alongside the coast.
Lowestoft	613	Lowestoft Promenade	I read there are several items on the agenda for safety, need and encouragement for even more cycle lanes to be improved, eg new lines to be re painted along the promenade. Surely this is such an easy task, low cost and needs no consolidation, as the cycle lane is already in use?	So, I ask this is to be given priority, after all there is no money issue, as I also researched the funding that central Government had given to you, I believe the sum of three million, this was to spend to fast track for cycle corridors, in the wake of the Covid 19.	0	0	1	0	0	3	4	Connectivity and Growth - The improvement involves reconsidering and repainting the cycle lanes along the promenade which won't create additional connectivity. Modal Shift - There is not considered to be significant modal shift. Optimisation - If the position of the lines are reconsidered and optimised it is considered 1 point is reasonable. Safety - There are no significant safety impact. This category generally relates to conflict with vehicles. Biodiversity - There are no significant biodiversity impact. Leisure - This area represents a key leisure destination and its attractive and efficient flow of pedestrians and cyclists is an important issue.
Lowestoft	614	Pakefield High School (opposite)	My last request, for the spending of the money given to Lowestoft, for the high demand and in identifying the NEED for a new cycle lane opposite Pakefield High School, NR337AQ. I travel on London Road frequently, either on my bike, walking or by my car. Last Thursday afternoon, when the student were finishing school, I witnessed a child stumble into the road, he was very lucky not to be injured.	I can see from your plans that Arbour Lane, MAY be improved? Look at taking this new cycle lane from Mc Donald's roundabout to Pakefield road and connects to the existing track along the promenade. There are over three hundred students at this school, the new safety improvements need to happen promptly. The safety of everyone in that area should not purely be down to luck.	3	2	0	2	-1	3	9	Connectivity and Growth - This route is positioned on and forms a significant section of a key corridor within Lowestoft. Modal Shift - No PCT data as it is a footpath, however running parallel is London Road which shows significant modal shift potential. Clearly if this route is improved not every user will move from London Road so the potential modal shift has been split between the two routes. Furthermore the growth is assumed to the highest standard as an off-road route so a score of 2 has been given. Optimisation - As a footpath the creation of a cycle route is considered 'new' as opposed to an optimisation of the existing. The pedestrian aspect is unlikely to be significantly improved. Safety - By extending the coastal path to the High School it will connect the school to the proposed coastal route within the key corridors section ensuring pupils can avoid on-road cycling along London Road Pakefield. The infrastructure would directly bypass the 20mph section, and the connection into the proposed coastal path means pupils can avoid the relatively busy London Road. Biodiversity - The widening of the path could result in the loss of grassed areas beside the path, for the most part these are managed grass areas. Leisure - This is an important leisure route that runs alongside the coast.
Lowestoft	616	The Promenade	May I please ask you to consider allowing cycling on the lower promenade during off peak times. For example, not during the peak holiday season or any Bank Holiday weekends. Additionally, when cycling in the designated cycle path on the top of the promenade, pedestrians		3	2	0	0	0	3	8	Connectivity and Growth - This route is positioned on and forms part of a potential key corridor within Lowestoft. On its own this improvement has limited connectivity benefit, but it should be considered alongside wider improvements along the coast. Modal Shift - No PCT data exists for this section, but it is assumed it would attract some cyclists who currently use London Road South. On its own the use of the lower

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			who wander aimlessly across the path also give cyclists a great amount of abuse.									promenade couldn't create this modal shift so it would need to be considered alongside improvements to the wider path. As an off-road route so a score of 2 has been given. Optimisation - This would represent a 'new' route as opposed to an optimisation of the existing. Safety - No significant safety benefit. Biodiversity - No significant biodiversity impact. Leisure - This is an important leisure route that runs alongside the coast.
Lowestoft	617	Sparrows Nest	cycling North up the High Street, but heading to The Sparrows Nest park, involves crossing lanes of traffic, around the central island where the garage is. As I want to get to Gunton Cliff and down Links Hill to cycle back to town along the Cycle path along North Beach, I find this section really dangerous.		3	2	0	2	0	2	9	Connectivity and Growth - This improvement is located on a key corridor with direct connections into the old High Street. Modal Shift - PCT suggests a high level of growth potential if quality infrastructure is provided. Optimisation - This is new infrastructure so has no optimisation benefit. Safety - The road is 30mph, but busy and with HGV traffic so a score of 2 is deemed reasonable if cyclists are taken completely off the road. Biodiversity - There are not significant biodiversity impacts. Leisure - Connects through to the Old High Street which has leisure benefit. The improvement is comprehensive and connects to other routes giving it a good score.
Lowestoft	618	Pakefield	From a leisure point of view, cycling Pakefield to Southwold would be excellent. Kessingland is a complete no-go, and beyond that, on the A12 would be nothing short of life threatening, yet there are many country footpaths that with a bit of care could be opened up to the cyclist.		2	3	0	3	-2	3	9	Connectivity and Growth - The suggested improvement connects 3 settlements together with a good range of services meaning a high score is reasonable, however it should be noted that the significant distances between the settlements means some cyclists will be dissuaded making a top score unviable so a score of 2 has been given. Modal Shift - PCT suggests a small number of cyclists use the A12 between Kessingland to Southwold so has the potential for a reasonable level of growth, but PCT suggests a very large modal shift between Kessingland to Lowestoft giving a top score. Optimisation - A new path so no optimisation. Safety - This proposal takes cyclists off the A12 which is a main road at national speed limit. There are limited alternatives currently between Benacre and Kessingland. Biodiversity - The definitive map shows a number of PROW routes, but these don't form a connected network. Along the coast there are desire lines even if not a PROW route shown on the definitive map. The full extent of biodiversity impact is not known at this stage, but given the length of the route it is likely some foliage will need removing and a score of -2 is deemed reasonable. Leisure - This route connects to important tourist locations and would form a highly attractive destination in its own right.
Lowestoft	637	going from High Street north on A47 (towards Corton)	I have no idea what I am supposed to do at the top of the High Street on a bicycle. There is a cycle lane coming south but I do not want to use it going into on-coming traffic. There is confusion about what pavement cycling as sometimes marked and then disappears. I don't want to cycle on the A47 as it is too fast but there is no alternative but more importantly NO SIGNAGE at all. The DENES HIGH SCHOOL is on the A47 and currently no cycle path from south to allow pupils to cycle safely.	Proper cycle ways that are NOT on the road and NOT on the pavement. Cyclists need to be protected from traffic on A roads. A cycle way along the whole of the A47	3	2	0	2	0	2	9	Connectivity and Growth - This improvement is located on a key corridor with direct connections into the old High Street. Modal Shift - PCT suggests a high level of growth potential if quality infrastructure is provided. Optimisation - This is new infrastructure so has no optimisation benefit. Safety - The road is 30mph, but busy and with HGV traffic so a score of 2 is deemed reasonable if cyclists are taken wholly off the road. Biodiversity - There are not significant biodiversity impacts. Leisure - Connects through to the Old High Street which has leisure benefit. The improvement is comprehensive and connects to other routes giving it a good score.
Lowestoft	652	It is a pointless exercise suggesting improvements to local infrastructure unless there is a coherent plan for cycling in Lowestoft.	- Lowestoft with its relatively flat terrain and low car ownership should be leading the way.- Instead there is a mish-mash of side streets and a few reasonable cycle routes. Few join up and almost all end in dangerous exit points at roundabouts and junctions. - Few routes are safe for children- No attempt to encourage cycle tourism, such as routes from the station to Oulton Broad or Carlton Marshes, or even signage to the beach!	First,come up with a proper co-ordinated strategy for cycling in Lowestoft not just minor cosmetic improvements (I would be happy to contribute).Secondly prioritise safe direct routes into town that you would be happy to let your children use.Thirdly, encourage cycle tourism by making Lowestoft a hub for routes to the Broads, and along the river Waveney.Fourthly get Sustrans and Lottery funds to make safe cycle tracks not dotted lines on the main road.Finally where there are shared routes with pedestrians, look at ways of separating the activities (eg different coloured surfaces) to increase pedestrian safety and acceptance of dual use routes.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Lowestoft	666	Lowestoft	The improvement that I feel needs making is that whilst it is reasonably possible to cycle within Lowestoft it is virtually impossible to cycle away from Lowestoft to any significant or interesting destination.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Lowestoft	777	Bascule Bridge, Lowestoft	The Bascule bridge is the biggest obstacle to more cycling due to being perceived as dangerous. It is ironically unfortunate because it is what links south and central Lowestoft, thus the town's main facilities. I am unconvinced the cycle/pedestrian bridge would be the best way to resolve it. I refer you to the enclosed copy of Cycling UK's (CUK) Hierarchy of Measures for Cycling Facilities. CUK's stance is that the priority should be to make the road environment comfortable for cycling.	The road over the Bascule bridge could be made so if there is the political will for radical interventions. Parts of the carriageway could be exclusively for cyclists by 'blocking off' with 'armadillos'/planters/bollards. It might require some realignments and widenings, however, it would be extremely disappointing if it was argued something to encourage active and sustainable travel cannot be afforded because of the amount spent on a facility for motor vehicles, which are unsustainable. Cyclists are currently allowed to share the footway over the Bascule bridge and then along Station Square. I think the Hierarchy of Measures in effect explains why CUK does not regard that as satisfactory. Indeed, at the point where the footway turns sharp left outside Lowestoft station toward Denmark Road, it surely goes completely against the point about sufficient sightlines.	2	3	2	0	0	2	9	Connectivity and Growth - A new cycle lane will provide additional connectivity into a key strategic location as the bridge does not allow cyclists requiring them to dismount so a new cycle lane provides a less disrupted path. However it is not a significant hinderance so a full score is not considered reasonable. Modal Shift - PCT shows a high number of cyclists potentially using bridge, but this could significant rise with good quality infrastructure which is possible (this doesn't factor in the third river crossing). A cycle lane would represent high quality infrastructure so scores a full 3. Optimisation - The cycle Lane represents an optimisation of the existing bridge by allowing improvements to the existing pedestrian pathway meaning it receives a score of 2. Safety - Whilst cyclists are forced to dismount currently they are adequately separated from vehicles so it is not considered a significant safety improvement. Biodiversity - There are no significant Biodiversity impacts Leisure - This is will improve connections between several important leisure locations i.e. train station, town centre and seafront. However the bridge is not a significant hinderance so a full score should not be awarded.

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Lowestoft	778	Pier Terrace, Lowestoft	Concerning Pier Terrace, it is more problematic. However, there is an off-road facility and, I think, as access is traffic lights controlled, many cyclists find the short on-road distance tolerable. That said, I do not think it should be a reason not to have brain storming discussions on possible improvements for cycling along Pier Terrace, particularly as the off-road facility is clumsy.	In conclusion, the most pertinent point is that a facility actually on the Bascule bridge and improvements to Station Square would enable cyclists to use the absolute direct route linking the main parts of Lowestoft, albeit with possible very minimal deviation due to realignments, it could make cycling quicker and less stressful than driving for short journeys	2	3	0	0	0	3	8	Improvements to Belvedere Road/Pier Terrace south of Bascule Bridge have been considered. Connectivity and Growth - This improvement lies on a key corridor providing access to the town centre, but doesn't get the highest score as Pier Terrace provides reasonable connections already. Modal Shift - PCT suggests the area of Belvedere Road outside Pier Terrace has significant modal shift growth potential, even if some of this is transferred to bypassing Pier Terrace a high level of modal shift could be expected. Optimisation - Creating a new route along the adjacent site would not represent an optimisation. Safety - An alternative route that bypasses Pier Terrace which appears to be a 30mph road has some safety merit, but it is a cul-de-sac and any traffic is unlikely to get to 30mph. Accordingly the proposal has limited safety benefits in bypassing this road. Biodiversity - No significant biodiversity impact. Leisure - Provides improved connections between the town centre, south beach and parts of south Lowestoft.
Lowestoft	779	Lowestoft	The third crossing will mean even less excuse for not having more 20 mph speed limits. There is plentiful evidence they create more cycling. I particularly argue Yarmouth Road would be a good candidate. Come the third crossing, I guess its classification could change. There is arguably a precedent in that in south Lowestoft stretches of Marine Parade/Wellington Esplanade/Kirkley Cliff Road, which are the A12 are 20 mph.	I realise that the A47 is the responsibility of Highways England. Frankly, the cycling provision is a shambles. For a lot of the way it is shared with pedestrians on PARTICULARLY narrow footways, passing bus stops, driveways and crossing roads without priority, i.e. it goes completely against CUK's guidance. There are points where the shared path stops so cyclists have to continuously temporarily rejoin the carriageway. That can increase danger as drivers do not expect it. Ironically, the one reasonable stretch of the cycle path, which is segregated from the footway and runs between Sussex Rd and Hollingsworth Rd, passing Ormiston Academy, gets parking on it at school run times. In my opinion, as the Northern Spine Road is part of a route to bypass Lowestoft centre to reduce congestion, there is no reason why Yarmouth Rd should not already be 20mph to the roundabout with the Northern Spine Road/Corton Long Lane/Blundeston Road. It could encourage compliance with using the bypass route.							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Lowestoft	783	Lowestoft	Concerning cycle lanes, i.e. white lines on roads, many of them in Lowestoft are not the stipulated minimum width of 1.5 metres. Local Transport Note (LTN) 2/08, paragraph 7.4.2 states: "Cycle lanes should be 2 metres wide on busy roads, or where traffic is travelling over 40 mph. A minimum width of 1.5 metres may be generally acceptable on roads with a 30 mph limit.	For cycle feeder lanes to advanced stop line arrangements, a minimum width of 1.2m may be acceptable. Cycle lanes less than 1.2 metres wide cannot easily accommodate tricycles or child carrying cycle trailers wholly within the lane." A pertinent point is that the Highway Code advises cyclists to ride 0.5 metres away from the kerb. Cycle lanes less than 1.5 metres can, ironically, increase cycling danger by misguiding drivers into thinking those are safe distances to overtake cyclists. LTN 2/08 was withdrawn on 20 July because it has been superseded by LTN 1/20. However, paragraph 6.4.2 indicates 1.5 metres is now only acceptable for one-way roads.							N/A	This point will be considered, but as it is not a specific location it is not possible to score.
Lowestoft	784	Bascule Bridge	The railway bridge is a close second to the bascule bridge in being the location in Lowestoft that most discourages cycling. As you know, there are "no cycling" signs on the footbridge but the vast majority of cyclists ride. I am uncomfortable about it.	It is inconsistent that on both sides of the bridge there is a shared cycle route and that cyclists have, strictly speaking, to dismount and walk, even though it is only a short distance. By that, I mean I accept the footbridge is narrow so a separate cycle bridge should be a priority.	2	3	1	0	0	1	7	This comment is unclear whether it relates to Bascule bridge or the railway bridge crossing. The assessment is for a new cycle/pedestrian bridge near to the Bascule Bridge. Connectivity and Growth - Whilst some connectivity exists already a new cycle bridge will provide additional connectivity into a key strategic location. The bridge does not allow cyclists to ride upon requiring them to dismount so a new cycle bridge provides a less disrupted path. Modal Shift - PCT shows high use of the bridge currently, but this could significant rise good quality infrastructure which is possible (this doesn't factor in the third river crossing). Clearly a new bridge would represent high quality infrastructure so scores a full 3. Optimisation - The bridge in itself does not represent an optimisation, but would allow improvements to the existing pedestrian pathway meaning it receives a score of 1. Safety - Whilst cyclists are forced to dismount currently they are adequately separated from vehicles so it is not considered a significant safety improvement. Biodiversity - There are no significant Biodiversity impacts. Leisure - This will improve connections between several important leisure locations i.e. train station, town centre and seafront, although connections exist (albeit of poor quality) it is still considered a 2 is a reasonable score.
Lowestoft	786	Horn Hill and Belvedere Road to/from Pier Terrace	I would like discussion on the cycle paths along Horn Hill and Belvedere Road to/from Pier Terrace. They were originally segregated but are now shared. The different coloured surfacing indicates they are segregated and although the signs indicate they are shared, it is confusing. I am not clear why they were changed. Possibly it relates to the fact they pass bus stops, which are supposed to be by-passed.	I am aware there was a cyclist/pedestrian collision at the Horn Hill bus stop in the easterly direction and I note cyclists now have to rejoin the carriageway for the short distance to the roundabout. I realise many cyclists cut through the Asda car park but that is not a good situation.	2	3	1	0	0	0	6	Connectivity and Growth - The suggested improvement lies on a key corridor, but doesn't score the full marks due to shared cycle path opposite and through ASDA meaning the extension of the shared path along the Belvedere frontage has limited impact. Modal Shift - PCT suggests improvements to the highest standard will gain significant modal shift growth. Optimisation - The addition of segregation on the shared path between cyclists and pedestrians would represent a modest optimisation. Safety - With options through ASDA or on the south of Belvedere Road which do not represent a significant diversion the safety benefit is considered limited. Biodiversity - There are no significant biodiversity benefit. Leisure - The improvement largely benefits day-to-day users as opposed to leisure cyclists.
Lowestoft	787	Ormiston Academy	I hope there will be discussion to resolve the issue of parents parking on the cycle path outside Ormiston Academy.		0	0	1	2	0	0	3	Connectivity and Growth - The proposed improvement will not significant impact connectivity. Modal Shift - The shared path is of reasonable quality, but it is not expected that temporary disruptions caused by parking will significantly improve the numbers using the path. Optimisation - Ensuring the path is fully utilised and

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												available throughout the day means this suggestion will represent an optimisation of the existing infrastructure. Safety - If the cycle path is disrupted this may force cyclists either onto the pedestrian side or onto the road. The road is 30mph, but busy and a main trunk road. Accordingly a good score will be provided here. Biodiversity - There are no significant biodiversity impacts. Leisure - The would likely impact the day-to-day users as opposed to leisure cyclists.
Lowestoft	793	Lowestoft	The Town Council is aware that more people in Lowestoft than the national average use the bicycle as a form of transport. Connectivity of routes through and around town should be reviewed and the East Suffolk Council should scrutinise and strongly lobby the County Council on lack of funding being allotted to Lowestoft as opposed to other Suffolk towns.	It is hoped the public will submit their individual comments to East Suffolk Council in response to this consultation, however, again, it is noted that a digital consultation is not inclusive to the whole community.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Marlesford	305	A12 from Marlesford Road to B1116 (NW side of A12)	To walk to Wickham Market from Marlesford requires several crossings of the A12. The path is often narrow and obstructed. A safe pedestrian and cycle way is required between Marlesford and Wickham Market. I'm sure many of the Council will have driven through Marlesford on the A12. Has anyone tried to walk from Bell Lane to the Framlingham Road (B1116)?	A combined cycle/pedestrian track is required from Marlesford Road to the B1116 roundabout. This should be away from the highway, on the NW side of the hedge.	3	1	0	3	0	0	7	The commenter proposes a footway with a segregated cycleway between Marlesford Road junction and the B1116 roundabout on the NW side of the A12 behind the hedgerow. Connectivity and Growth – With consideration to Sizewell C, the proposal will connect Wickham Market to the Southern Park and Ride. Wickham Market also has a number of key services, which are not available in Marlesford, therefore the infrastructure will likely have significant connectivity benefit. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, the proposal would result in a small modal shift hence a score of 1. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – This section of the A12 is a busy 'A' type road with a national speed limit and appears to be often utilised by HGVs. Despite this section of the A12 having an existing small footway adjoining the NW side, removing cyclists and pedestrians away from the road will have considerable safety benefit. A score of 3 is deemed reasonable. Biodiversity – The commenter proposes implementing the cycleway and footway behind the existing hedgerow; therefore, the proposal will only result in the removal of arable land, which is considered to have minimal to no biodiversity value. Leisure – No leisure impact.
Marlesford	459	A12 north of Wickham Market	As already commented regarding walking, there is not a safe way of cycling from the Wickham Market roundabout to Marlesford and beyond.	By providing a short section of Cycle/footpath this will allow cyclists direct access to Bell lane which in turn leads towards villages around Parham Airfield, which is a designated industrial development area. This would allow somebody to cycle to work from Wickham to Parham without using the B1116 which is a busy route to/from Framlingham. Children from the villages could also safely cycle to school or access shops in Wickham.	3	1	0	3	0	0	7	The commenter proposes a footway and cycleway between Bell Lane and the B1116 roundabout. Connectivity and Growth – With consideration to Sizewell C, the proposal will connect Wickham Market to the Southern Park and Ride. Wickham Market also has a number of key services, which are not available in Marlesford, therefore the infrastructure will likely have significant connectivity benefit. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, the proposal would result in a small modal shift hence a score of 1. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – This section of the A12 is a busy 'A' type road with a national speed limit and appears to be often utilised by HGVs. Despite this section of the A12 having an existing small footway adjoining the NW side, removing cyclists and pedestrians away from the road will have considerable safety benefit. A score of 3 is deemed reasonable. Biodiversity – The commenter proposes implementing the cycleway and footway behind the existing hedgerow; therefore, the proposal will only result in the removal of arable land, which is considered to have minimal to no biodiversity value. Leisure – No leisure impact.
Marlesford	650	lack of cycling facilities Framlingham - Parham - Hacheston - Wickham Market station	The B1116 is a very busy road, and parts have a national (60mph) speed limit. Some has 30/ 40mph but from Brick Lane to The Street in Parham there is no alternative. There is a back-lane route from Hacheston to Campsea via Marlesford but there is no safe crossing of the A12. A significant number of cyclists do use the A1116 but only fit and fast ones.	Re-create the Framlingham branch railway line for walking and cycling. For much of the way from Framlingham to Marlesford there are public footpaths paralleling the old railway alignment, or very near by. These could be diverted, through negotiation, and joined up to follow the track bed, and be reclassified as bridleway or cycle track. In the longer term the track bed could be acquired and the surface upgraded. As an extension - though more complex - path could be extended along the old freight railway line to Snape Maltings. There are very few truly traffic-free cycling facilities in this part of Suffolk (that are not muddy). This could develop into a fantastic and very well-used facility for leisure and other purposes.	3	0	0	3	-3	3	6	The commenter proposes cycleway/footway along the old Framlingham Branch Line where possible. Connectivity and Growth – The proposal will connect Marlesford, Hacheston, Parham, and Framlingham. Framlingham, a town, is likely considered a key service centre and connecting into a key service centre warrants a score of 3 under this category. Modal Shift – The B1116, which this route will provide an alternative for, does not have significant use according to PCT and it is unlikely that the infrastructure will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will provide an alternative to the B1116, which is a busy 'b' type road with a NSL. Removing cyclists and pedestrians off road warrants a score of 3 under this category. Biodiversity – The proposal will likely result in significant biodiversity loss. Leisure – The Framlingham Branch Line resided along the River Ore and connected into Framlingham, which is town centre that provides drinking establishments, eating establishments, and has historical/cultural attractions. Therefore, the route will likely have significant leisure value and scores a 3 under this category.
Martlesham	28	Cycle path between BT and towards Brightwell	Path is narrow, overgrown and dual carriageway is next to it and unprotected. Rationalisation of path required perhaps in conjunction with Brightwell lakes Development. Linkage of current national and local paths required in this area more generally.		0	2	3	0	-1	1	5	Connectivity and Growth – Existing shared path along this section of the A12, therefore the proposal is not considered a new connection. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, the proposal will result in a somewhat significant modal shift, therefore a score of 2 is considered reasonable. Optimisation – Upgrading a shared path to segregated cycle track usually warrants a score of 2, however the existing infrastructure is particularly narrow and is within

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												close proximity to the A12, which is a dual carriageway with a national speed limit. A score of 3 is considered reasonable. Safety – The cyclists are already separated from the route and whilst it is narrow, improving the pathway doesn't improve safety. Biodiversity – The proposal will result in the loss of a grassed area, which appears to be a mix of managed and unmanaged grass. Leisure – Brightwell Lakes provides some Leisure value, whilst the improvement would not have a significant leisure gain, a modest score is reasonable.
Martlesham	44	Re-route NCN1 to avoid retail park in Martlesham	The area around Gloster Road has become much busier since NCN1 was planned as has Felixstowe Road.	It would now be safer, shorter and more pleasant to route NCN1 straight on at the point shown on the map, along Main Road under the junction of A12/A1214 to rejoin the existing route at the junction of A1214 and Deben Avenue.	0	1	1	0	0	-2	0	Connectivity and Growth – As the route already exists, despite being indirect, the proposal does not warrant a score under this category. Modal Shift – PCT suggests that Main Road has significantly more use than existing NCN route, therefore it is likely that the proposal will result in a modest modal shift. Optimisation – Currently, some of the roads that the NCN resides along do not have existing cycle infrastructure. Despite being primarily on-road infrastructure, main road has existing infrastructure and is more direct, therefore a score of 1 is deemed reasonable. Safety – Although the Main Road has existing infrastructure, it is currently in a poor condition. Although the existing route contains minimal infrastructure in places, it does reside along quiet roads. A neutral score is deemed reasonable. Biodiversity – No biodiversity impact. Leisure – Currently the NCN route connects into Adastral Park, which has modest leisure benefit, and into PROWs, which are somewhat attractive. Changing the route will, therefore, detriment leisure. A negative score is considered reasonable.
Martlesham	46	Recreation Ground Martlesham	Fynn Valley Walk out of alignment. Walking East on the Fynn Valley walk at present means walking South from the junction of Post Office Lane and The Street, along School Lane before turning onto a footpath to Martlesham Creek.	If a permissive path could be negotiated with the land owner the route would be much improved by a link from the recreation ground at the point shown on the map to join the existing footpath round Martlesham Creek	0	0	0	0	0	3	3	Connectivity and Growth – The proposal will likely have more leisure value than connectivity and growth. Modal Shift – As a leisure route, it is unlikely that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will remove the need to walk along Main Road and School Lane, however pedestrian infrastructure already exists. The proposal receives a neutral score under this category. B – No significant biodiversity impact. Leisure – The proposal would remove the need to walk along Main Road and School Lane and results in a far more attractive route which extends alongside the River Fynn. As the proposal will connect into the PROW network that extends adjacent to the River Deben, it is considered that the improvement will have significant leisure benefit.
Martlesham	54	Retail areas, Martlesham Heath	All of the retail developments on Martlesham Heath have been created with large car parks and have each been created independently with no joined up approach to movement from one development to another. For example moving from B&M to Tesco is only a short distance but a lack of pavements and crossing points means that walking is a very hostile experience. People drive very short distances from car park to car park. MH has good foot cycle access until you actually get to the shops!	A new pedestrian / cycle crossing between Tesco and Pets at Home. New crossings between Poundland and Next. New crossings between M&S and B&M. The B&M development has no pavement access at all.	0	0	2	1	0	0	3	Connectivity and Growth – Connecting the car parks together provides modest connectivity benefits, whilst the current layout doesn't create significant diversion the improvement will help connect important employment sites, shops and services. The site does include some cycling and walking infrastructure, though connectivity is poor. This proposal is therefore better scored under optimisation, meaning its connectivity and growth score is 0. Modal Shift – There is insufficient evidence to suggest any significant modal shift, particularly as the current layout doesn't represent a large diversion. Optimisation – 2 Safety – The suggestion offers a small safety benefit as it reduces the continuous cycle movements onto and off of the connecting roads. The roads are not significantly hazardous so its provided a score of 1. Biodiversity – There are no significant Biodiversity impacts L – Whilst there is potentially a small leisure benefit this suggestion doesn't offer significant improvements in connecting the leisure routes.
Martlesham	57	The whole of Sandy Lane from old Martlesham to Woodbridge	There is currently no safe pedestrian access from Old Martlesham to Woodbridge. Would strongly recommend installing a footpath full length of Sandy Lane from Top Street Martlesham to Ipswich Rd Woodbridge.		3	1	0	3	-3	2	6	Connectivity and Growth – The proposal would create a new connection between Martlesham and Woodbridge, which are large and well-established settlements. As Sandy Lane resides within a key corridor, a score of 3 is considered reasonable. Modal Shift – Strava Metro suggests good usage of Sandy Lane. Datashine suggests that Sandy Lane has a low LQ and, as there are limited footways connecting Martlesham and Woodbridge, the proposal will likely result in a modest modal shift. A score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Sandy Lane is a particularly narrow road with at national speed limit. Removing pedestrians off the road will have safety benefits, hence a score of 3. Biodiversity – In order to implement a footway to adjoin Sandy Lane, the managed green verge and hedgerows will likely need to be removed, therefore a negative score is necessary. Leisure – The proposal would connect to the PROW routes which reside along Martlesham creek and the River Deben – as these are particularly attractive routes that extend through the AONB designation, a score of 2 is considered reasonable.
Martlesham	59	Main Rd Martlesham near junction of Hoflen Close	Main rd Martlesham is extremely busy with traffic and has become impossible to cross safely since Martlesham Retail Park has expanded. Crossing safely so that I can enjoy the countryside walks by the river Deben is almost impossible and creates much anxiety when taking your life into your hands with speeding traffic.	Please, please may we have a pedestrian crossing along Main Rd Martlesham so that all the local residents, leisure walkers, disabled users, school children, dog walkers can cross safely. We have such beautiful countryside here but we cannot get to enjoy it safely.	1	0	0	2	0	2	5	Assessment is based on the respondent's suggestion of a single pedestrian ('zebra' standard is assumed) over Main Road before the junction with Hoflen Close Connectivity and Growth – Main Road is 30mph but is often driven at higher speeds due to its excessive width. There are limited existing formal and informal crossing points, though none of them are signalised (some are 'islands' rather than crossing refuges, but can be used as crossing points for those without mobility limitations requiring a dropkerb/a formal crossing 'protected' by the legal requirement to stop and allow pedestrians to cross); at least one signalised crossing is highly needed on Main Road, though a more strategic approach (see alternatives) for maximising their locations for onward travel connections would earn a higher score. Even one crossing would provide better pedestrian access to surrounding local services (though multiple crossings would better achieve this). Modal Shift – Whilst the road itself is well used by cyclists the proposal is for a

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												crossing point which will not significantly unlock the modal shift potential. Optimisation – No existing crossing, so not an optimisation. Safety – See Connectivity and Growth; scored 2 as even one formal signalled crossing would provide significant pedestrian crossing benefits. Biodiversity – There are no significant biodiversity impacts Leisure – Even one crossing allows west-side pedestrians to more safely access onward pedestrian/cycle routes to the Deben Estuary/AONB and woodland (via Felixstowe Road, footpath 39A).
Martlesham	66	Broomfield to Eagle way,	The path is too narrow to safely support both cyclists and walkers due to a very tight bent. There have been collisions in the past at this point.	Cyclists should be re routed via Broomfield to Eagle Way	-2	-1	0	0	0	0	-3	Connectivity and Growth – This traffic-free shared pathway is the keystone connection within the Brightwell Lakes to Ipswich (via Long Strops Bridleway) strategic route. Removing this connection for cyclists (downgrading it to a footpath only) will negate the value of the Long Strops Bridleway route for cyclists, due to the consequent necessity for cyclists to either dismount and push their bikes through Broomfields (which is an unacceptable design response for a strategic route), or re-route up Portal Avenue. If re-routing up Portal Avenue, it would then make more sense for the cyclist to continue on to Ipswich via the A1214/Woodbridge Road, rather than channel back down to the Brightwell Lakes to Ipswich strategic route via Dobbs Lane, or, use a third option - Grange Lane/Grange Farm/Ropes Drive/Bell Lane. The Broomfields shared path connection also connects Martlesham Heath/Brightwell Lakes cyclists with Gorseland Primary School, which if removed, would likely only have a small impact (other primary schools in the area, and an all-through school is set to come forward at Brightwell Lakes), as cyclists would retain the freedom to dismount their bikes and push them along footpaths; however, as already stated, periods of dismount are inappropriate for inclusion within a strategic route. Broomfield therefore retains its key connection status for the Brightwell Lakes to Ipswich (via Long Strops Bridleway) strategic route. Widening the Broomfield connection would be far more effective than downgrading it and rerouting cyclists. However because an alternative is technically available for cyclists, it is precluded from scoring a full minus 3. Pedestrian options are less limited if the Broomfields connection was disrupted, due to the option of other footpaths (i.e. prohibit cycling). Modal Shift – As above, the presence of an alternative cycling route, the practical option for cyclists to dismount and push through Broomfields, and the limited impact on pedestrians mean significant harm to modal shift is unlikely. Many would continue to cycle the Broomfields connection notwithstanding the downgrade to a footpath if made, anyway. Optimisation – No optimisation of existing infrastructure. Safety - This category concentrates on conflict between vehicles and cyclists/pedestrians and the removal of part of the path would not alter this. Biodiversity – This is a paved shared route with no impact to biodiversity. Leisure - The routes appears to have limited leisure benefit.
Martlesham	68	Path alongside the A12	By mid summer the path becomes overgrown reducing it to single file.	If you cannot cut during bird nesting you should really cut back hard at the beginning of the summer or clear the vegetation altogether							N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Martlesham	72	Old felixstowe road, Martlesham	This road is supposed to be cycle friendly but the opposite is true as large quantities of traffic use it as a cut through to the industrial estate and are allowed in the cycle lanes. Very dangerous for cyclists and hence underused.	strict enforcement, separate cycle lanes with kerb.	0	1	3	3	-3	1	5	Connectivity and Growth – As the connection already exists, the proposal does not score under this category. Modal Shift – The road is reasonably well-used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift. Optimisation – Improving cycling infrastructure from on-road to segregated off-road warrants a score of 3 under this category. Safety – Whilst the road is a cycle priority route, it appears that many motorists do not treat the road as such. Whilst the road is 30mph, the improvement is considered to have benefits. Biodiversity – In order to implement a segregated cycleway, it is likely that the removal of established hedgerows, trees, and other foliage will be necessary. Therefore, a significant negative score is deemed reasonable. Leisure – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	73	Sandy lane, Martlesham	This lane is the connection between the cycle lanes of Martlesham/Kesgrave and Woodbridge. It is used as a short cut for traffic to and from Woodbridge and is national speed limit which creates dangerous conditions for all cyclists particularly those who don't know the road well and children.	20 or 30 MPH limit. Access only for motorised vehicles?	-3	0	0	3	0	-2	-2	The comment relating to speed falls outside the remit of the project and should be passed on to SCC. For the purpose of this assessment, allowing motorised vehicles access only shall be assessed. Connectivity and Growth – Sandy Lane resides along the Ipswich – Melton key corridor and Sandy Lane is a key connection between Martlesham and Woodbridge, therefore implementing motorised vehicle access only would have a detrimental impact on connectivity. Modal Shift – No significant modal shift impact. Optimisation – Not considered an optimisation. Safety – Restricting access to cyclists and pedestrians would remove potential conflict with motorised vehicles on a road at NSL. Taking pedestrians and cyclists is considered to have safety benefits. Biodiversity – No biodiversity impact. Leisure – PROW 11 and 10, which are attractive PROWs that extend through the AONB designation along the River Deben and Martlesham creek, are accessed on Sandy Lane, therefore restricting pedestrian and cyclist access will have a negative impact on leisure. A score of -2 is considered reasonable.
Martlesham	81	junction of felixstowe road with main road martlesham	whole of Felixstowe Road dangerous for cyclists due to excessive traffic and buses. road is narrow with narrow cycle lanes. Cyclists have to cycle down middle of the road alongside traffic queues as cars fill the cycle lanes. blind bends and heavy traffic mean many near misses. some collisions have happened with cars cutting in front of cyclists pushing them into the hedge. Turing	Make this one way for cars and buses and make half the width of the road into 2 way cycle lanes. This would mean solving the congestion from the retail park onto the A12 to force traffic to use the bypass instead of running through here to Woodbridge and the A12. But you would need to allow 50cc mopeds through as they	0	1	3	3	0	1	8	Connectivity and Growth – The connection here already exists so the suggestion does not score in this category. Modal Shift – The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift. Optimisation – This improvement would mean change of an on-road option to segregated cycle track which results in a score of 3. Safety – Whilst the road is a cycle priority route it appears that many motorists do not treat the road as

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			right at the junction is dangerous for cyclists as cars sometimes pass the cyclist on the RH side during the turn.	are restricted to 28 mph which is dangerous on a dual carriageway.								such. Reducing the road to one way for traffic, and segregated bi-directional lanes on the reclaimed other side would represent an uplift in cyclist safety. Biodiversity – There are no discernible biodiversity impacts Leisure – Brightwell Lakes provides some Leisure value, as do the leisure uses present within the Breadmore Park area (e.g. the Bowling alley, the leisure centre, etc.) which the improvement would provide safer access to - however it would make a modest difference to overall cyclist safety/leisure access in isolation of improvements to and from Felixstowe Road (Main Road, Beardmore Park generally, etc.).
Martlesham	82	alongside felixstowe road	when walking alongside this road on the footpath in or after rain pedestrians get soaked by cars spraying water from puddles. There is no where to get away from this and it can be significant. I carried shopping home along here one day and my shopping bag was drenched inside with puddle water and I had to throw away fresh bread and some fresh produce because of this.	make the road one way for cars and the other half of the road for cyclists and mopeds. The car lane could be furthestest away from the footpath.	0	1	3	2	0	1	7	Connectivity and Growth – The connection here already exists so the suggestion does not score in this category. Modal Shift – The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift. Optimisation – This improvement would mean change from an on-road option to a segregated cycle track which results in a score of 3. Safety – Whilst the road is a cycle priority route, it appears that many motorists do not treat the road as such. Whilst the road is 30mph the improvement is considered beneficial. Biodiversity – There are no discernible biodiversity impact. Leisure – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	83	main road martlesham	lack of safe crossing places for elderly and vulnerable persons. The road down hill from Crown Point is heavily used (88000+ cars per week) and uphill has a high % of speeding traffic. No pedestrian crossing anywhere along this road. There would also need to be pedestrian crossing across the junction of Felixstowe Rd by the fish shop to connect up a safe route to rural martlesham homes. blind man and his guide dog hit by car as he tried to cross main road downhill on 14 september 2020.	pedestrian crossing over Main Road near Black Tiles (upgrade the existing refuge ?) and another across junction with Felixstowe Road at Crown Point.	1	0	0	1	0	0	2	Connectivity and Growth – Main Road represents a modest barrier between those situated on either side, but as a 30mph it is crossable and there is a scattering of traffic islands. To the north of the proposed destination for the crossing point there are a limited number of services, but it does include a school. Therefore, a score of one is deemed reasonable. Modal Shift – Whilst the road itself is well used by cyclists; the proposal is for a crossing point which will not significantly unlock the modal shift potential. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Despite the road having a 30mph speed limit, it is relatively busy and as a school is located nearby, the crossing is awarded 1 point. Biodiversity – No biodiversity impact. Leisure – The suggestion provides limited leisure benefit and people either side of the road have good access to PROW leisure routes.
Martlesham	90	From Felixstowe Road junction with Mill Lane (track to the RSPCA) to just before Crown Point	Cars passing cyclists on 2 blind bends and having to cut back in across the path of the cyclist as a car comes the other way round the bend. I have personally had several 'near misses'. The area is a serious accident waiting to happen.	Increasingly busy as a 'rat run', the cars need to be slowed down. Suggest 2 speed humps: one by the Mill Lane/RSPCA junction and one further down near Crown Point to slow cars in both directions where the blind bends are.							N/A	The installation of speed bumps is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Martlesham	91	The Old Felixstowe Road is part of the national cycle network and is also a commuter route for cyclists between Woodbridge and the employment area at Martlesham	It is marked with cycle lanes on each side but they're far too narrow, especially at the north end where they're overgrown and there's a blind bend Some motorist assume that the lane markings means that it's safe to pass close to the lane marking, not so! It's 30 mph but there are no signs to remind users of this and although there are street lights - they're dim at night and scarcely visible during daytime. Spacing between some is too long to be legal indication of the 30mph limit.	1) clear out the over growth 2) remove the cycle lane markings and - they are more dangerous than having none 3) make the speed limit 20mph with proper signage to indicate this is a cycle route 4) improve to the lighting 5) ideally put chicanes in place to discourage motorist from using the route. See also my separate comment re the Sandy Lane speed limit which is part of the same Cycle network Route	0	1	3	2	0	1	7	The commenter proposes removing the cycle lanes and implementing chicanes along Felixstowe Road as the existing infrastructure is poor quality, however this will unlikely optimise the route. For the purpose of this assessment, widening the existing footway to include a segregated cycleway and making Felixstowe Road one way will be assessed. Connectivity and Growth – The connection here already exists so the suggestion does not score in this category. Modal Shift – The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift. Optimisation – This improvement would mean change from an on-road option to a segregated cycle track which results in a score of 3. Safety – Whilst the road is a cycle priority route, it appears that many motorists do not treat the road as such. Whilst the road is 30mph the improvement is considered beneficial. Biodiversity – There are no discernible biodiversity impact. Leisure – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	92	Anson Road in Martlesham at the small Tesco roundabout between Tesco and Pets at Home	This is the perfect place for a crossing. A lot of us that like to walk to the shops from Martlesham IP12 there is not a safe place to cross to get to the other side where all the other shops are. We have to put our lives at risk twice trying to cross this busy road and wait for a car to stop. Trying to park is sometimes a nightmare so walking is so much easier and this could be made a lot easier and safer for us all to do so and encourage more to do so by putting in a crossing at this roundabout.	A traffic light crossing with a button to physically stop the traffic when someone needs to cross. This would keep the traffic flowing and only be used as and when the public needed it. I have witnessed a few people now nearly get hit by cars not stopping for the people using the zebra crossing further up and so due to the high volume of traffic this is the only safe way to cross. We always have to wait for ages to cross or for one kind person to stop for us and wave us across when safe to do so. It's a matter of time before someone gets hit trying to cross this area between Tesco's and Pets at Home.	1	0	0	1	0	0	2	Connectivity and Growth – The proposal provides modest connectivity benefits, whilst there is a crossing point to the SE it will help connect important shops and services meaning it scores 1. Modal Shift – There is insufficient evidence to suggest any significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Despite being a 30mph road, Anson Road is particularly busy. As there is an existing zebra crossing to the east, a score of 1 is deemed acceptable. Biodiversity – No biodiversity impact. Leisure – No significant leisure benefits.
Martlesham	95	In and around Martlesham/Martlesham Heath and Woodbridge	Few, if any, footpaths are accessible for wheelchair users, which means that I cannot accompany my friends and family when they go for walks. Shared footpaths with cyclists are a problem because often I can't hear cyclists coming from behind me, and they ride too close.	Make more footpaths accessible for wheelchair users (and parents with prams/buggies) especially in local beauty spots Separate pedestrians from cyclists, or provide a barrier so that cyclists can't ride so close.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. The accessibility of the infrastructure and ensuring inclusivity is an important consideration in any proposal.
Martlesham	96	Sandy Lane between The Street and its junction with California north of the railway bridge	This is a derestricted section connecting two 30mph areas. It's part of the National Cycle Network serving commuters and businesses on Sandy Lane south of the railway. The Parish council has been asking for several years to have this made 30mph on safety grounds. Nothing has happened. To encourage sustainable transport this key part of the only viable cycle route	Make the section of Sandy Lane between The Street and California a 30mph area. The attached satellite view gives a good impression of the number of business along that road.							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.

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			between Woodbridge and Martlesham need be improved, as does the Old Felixstowe Road.									
Martlesham	109	Felixstowe Road, Martlesham leading to Main Road/TheStreet/Top Street Martlesham	Felixstowe Road is shown as a priority cycle route. It is not. It is a heavily used rat run which has made it nigh on impossible for cyclists to safely use it and the other roads listed above. The cycle lanes are dangerous and hardly used due to consistently heavy traffic and HGVs ignoring the weight limits. The speed limit of 30 is ignored (Police Speed Detection surveys prove this). Highways are aware and ignore complaints every time re concerns about ratrunning.	Either close Felixstowe Road to through traffic (buses don't need to use it..and Highways will put every objection possible to this as they see F Rd as a relief road for their failed traffic schemes for the retail park and A12, and have treated residents complaints and concerns with utter contempt) or make it one way. Then it will become a usable cycle and walking route instead of in name only. Put the traffic back onto the A12 instead of making cycling a dangerous and not very enjoyable pastime, and that may encourage the long suffering residents to get on their bikes. Because at the moment, nothing will encourage me to use the roads where I live other than by car.	0	1	3	2	0	1	7	Connectivity and Growth – The connection here already exists so the suggestion does not score in this category.Modal Shift – The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift.Optimisation – This improvement would mean change from an on-road option to a segregated cycle track which results in a score of 3.Safety – Whilst the road is a cycle priority route, it appears that many motorists do not treat the road as such. Whilst the road is 30mph the improvement is considered beneficial.Biodiversity – There are no discernible biodiversity impact.Leisure – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	131	Main Road, Martlesham - south end of road	There are no zebra crossings along the entire road. however there are a high number of elderly and disabled residents on the north side of the road. this restricts their ability to walk to the local shops such as Tesco. There are a small number of traffic islands, however six weeks ago a man with limited vision was knocked down by a car in this area. he believes this was partly due to a lack of safe spaces for him to cross and excess speeding.	a zebra crossing to be installed creating a link between both sides of the busy road.	1	0	0	1	0	0	2	Connectivity and Growth – This road represents a modest barrier between those situated on either side, but as a 30mph road it is crossable and there is a scattering of traffic islands. To the north of this road there are limited destinations, however it does include a school. Accordingly, it does provide connectivity benefit and scores 1 point. Modal Shift – A crossing point will unlikely unlock the modal shift potential on this road. Optimisation – The crossing point doesn't appear to improve the existing infrastructure. Safety – The road is 30mph, but relatively busy and as a school is nearby a crossing point has been awarded 1 point. Biodiversity – There are no biodiversity impacts. Leisure – The suggestion provides limited leisure benefit and people either side of the road have good access to PROW leisure routes.
Martlesham	145	Felixstowe Road, Martlesham - the entire length	Although the road is supposed to be a cyclists priority route it often feels less safe than a regular road with a single lane marker. I regularly cycle up and down the road to work and have witnessed many near misses, particularly as the road has become much busier in the last 15yrs with the development of the industrial estate. Not only cyclists but pedestrians are also at risk when using the road/footpaths.	Either make the road one way and provide much improved cycle lanes and footpaths or install traffic calming, either speed humps or island/priority sections to reduce the speed of traffic and increase its cycle friendliness.	0	1	3	2	0	1	7	Connectivity and Growth – The connection here already exists so the suggestion does not score in this category. Modal Shift – The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift. Optimisation – This improvement would mean change from an on-road option to a segregated cycle track which results in a score of 3. Safety – Whilst the road is a cycle priority route, it appears that many motorists do not treat the road as such. Whilst the road is 30mph the improvement is considered beneficial. Biodiversity – There are no discernible biodiversity impact. Leisure – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	151	Footpath across the A12 from Seckford (Bealings) to Woodbridge	Crossing the A12 on foot / bike is perilous here.	Consider upgrading to full traffic lighted crossing, underpass or bridge. It could be part of a longer useful & safe cycle/walking route to the Bealings, Grundisburgh and beyond...	1	0	0	2	0	0	3	Connectivity and Growth – The A12 represents a modest barrier between those situated on either side. Although there are limited services on the west side of the road, a score of 1 is deemed reasonable. Modal Shift – Insufficient evidence to suggest that a crossing point will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The suggestion offers safety benefit as this section of the A12 has an NSL. Whilst there are limited services to the west of the road, a score of 2 under this category is considered acceptable. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion provides limited leisure benefit.
Martlesham	152	A12 end of Seckford Hall Road (Woodbridge side of A12)	Wooden fence at end of Seckford Hall Road where path starts (out to A12) Difficult to negotiate for anyone on a bicycle, pushing a pram or a using mobility scooter	Redesign 'barrier' to allow easier access. This could be part of a bigger scheme to create a cycle / walking route from Woodbridge (south) to the out lying villages.	0	0	1	0	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – No significant modal shift benefit. Optimisation – No significant optimisation benefit. Safety – As a road with no sustainable travel infrastructure and with a national speed limit, a guidance sign may have a partial benefit, although whether any sign makes a significant difference in reality is unknown. Biodiversity – No significant biodiversity benefit. L – If cyclists are misusing the path this may effect enjoyment for walkers, however any existing rules should be adhered to anyway and signs on their own are unlikely to represent a significant leisure benefit. CandG – The barriers are passable albeit problematic so altering the design does not provide additional connectivity.
Martlesham	162	Felixstowe Road	It's not safe to cycle or walk along this road with the heavy traffic usage, blind bends and excess speeding. The overgrown plants, narrow path and cycle lanes, and lack of speed awareness ate not helping the situation. There's also nowhere safe to cross from the footpath into the community centre.	Speed signs, possibly even reduce it to 20mph, maintain/cut back roadside plants, provide crossings at crown point and community centre. Also widen the footpath and cycle lanes, making it a one way road would assist this and create a more pleasurable journey.	1	1	3	3	0	1	9	The comment in relation to speed falls outside the remit of the project and should be passed on to SCC. For the purpose of this assessment, making the road one way, adding crossing points, and widening the footpath and cycle lanes to create a segregated off-road cycle track will be assessed. Connectivity and Growth –The road represents a modest barrier between those situated on either side, but as a 30mph road it is crossable. The crossing would connect the cycling and walking infrastructure on the east to the community hall and fish and chip shop on the west. A score of 1 is considered reasonable. Modal Shift – The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard whilst making the road one-way would create modest shift. Optimisation – This improvement would mean change from an on-road to segregated

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												cycle track which results in a score of 3. Safety – Whilst the road is a cycle priority, it appears that many motorists do not treat the road as such. Whilst the road is 30mph, the improvement is considered beneficial. B – No biodiversity impact. L – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	163	The A12 underpass by PHQ	Underpasses are the only way to cross the A12 from Martlesham village, they're both dark, dirty and uninviting. When it's raining/snowing/icy it's difficult and unsafe to use them as they're so slippery, you could slide down but getting back up the other side can be akin to climbing a mountain. Those of us with disabilities want to get out and walk/cycle rather than travel short distances in cars but this is a massive obstacle.	Widen the underpasses to build steps as an alternative to the foot and cycle paths, lay an anti slip surface, hand rails, better lighting, discourage undesirables from hanging around	0	0	1	1	0	0	2	Connectivity and Growth – The connection already exists; therefore, the proposal does not score under this category. Modal Shift – The proposal only optimises a small section of the overall route and will unlikely result in a significant modal shift. Optimisation – The improvements provide greater accessibility and inclusivity optimising an existing pathway scoring a point here. Safety – Optimising the infrastructure will likely provide modest safety benefits to pedestrians utilising it. Biodiversity – No significant biodiversity impact. Leisure – No significant leisure benefit.
Martlesham	169	Bridge crossing A12 from Eagle way landing next to Martlesham Leisure	The Cycle / shared pedestrian path is totally unacceptable and has been from the day it was conceived, the landing from the bridge at Martlesham leisure is far too narrow as is the whole path . Cyclists come off the bridge at high speed with little regard to pedestrians .	The Path should be widened or the cyclists diverted onto the road leading to Gloster Road leaving the path for pedestrians. The landing area at the bottom of the bridge must be widened.	0	0	2	0	-1	1	2	Connectivity and Growth – The connection already exists; therefore, the proposal does not score under this category. Modal Shift – It is unlikely that improving the bridge, a small section of the existing infrastructure, will result in a significant modal shift. Optimisation – Upgrading existing infrastructure from a shared pathway to a segregated cycle track and footway warrants a score of 2 under this category. Safety – The cyclists are already separated from the road and whilst the comment suggests it is poor quality, improving the pathway doesn't improve safety. Biodiversity – In order to widen the infrastructure on either side of the bridge, the removal of the grassed areas will likely be necessary, it is not clear whether the planted landscaping will also be impacted. A modest negative score has been given, however if its only the grassed area this could be reduced. Leisure – This bridge provides a link into Martlesham Aastral park, which has some leisure value, therefore a modest score is considered acceptable.
Martlesham	246	Main Road Martlesham	No cycle lane toward Woodbridge. There appears to be a cycle lane on the uphill side of this road towards Martlesham but not on the downhill, Martlesham to Woodbridge Side	Provide a segregated lane to allow safe cycling in both directions.	2	1	0	1	0	0	4	Connectivity and Growth – The proposal would help in the connection of Woodbridge and Martlesham. Main Road resides along one of the key corridors which could warrant the highest score under this category, however the proposal is for on-road infrastructure hence a score of 2. Modal Shift – Implementing a cycle lane will likely result in a modest modal shift, hence one point under this category. Optimisation – The proposal is for new infrastructure and does not, therefore optimise the existing. Safety – Currently, if cyclists are travelling northbound, they are required to cycle along the road, which is a somewhat busy road with a 30mph speed limit. Whilst on-road cycle lanes do not offer significant safety benefits, it will alert drivers to their presence and stop the need for cyclists to utilise the road. A score of 1 is deemed reasonable. Biodiversity – No significant biodiversity impact. Leisure – No significant leisure benefit.
Martlesham	262	Road between Martlesham and Woodbridge	Currently there is no continuous footpath between Martlesham village and the outskirts of Woodbridge Town. This leads to many unnecessary vehicle journeys as use of a car is the only safe way to move between the two centres, a distance that many would be happy to walk if a safe pedestrian route existed.	Provision of a footpath along those sections of the road that currently do not have a footpath.	1	2	0	3	-2	0	4	The commenter proposes a continuous pedestrian path between Woodbridge and Martlesham. For the purpose of this assessment, the implementation of a footway adjoining Top Street north of the mini roundabout will be assessed. Connectivity and Growth – The infrastructure would connect Martlesham and Woodbridge, which are both large settlements with good levels of schools, employment, and shops, therefore there is unlikely going to be significant 'everyday' use hence a score of 1. Modal Shift – According to DataShine, Top Street currently has a low LQ, however the provision of infrastructure would likely encourage walking as it would make a direct connection into Woodbridge, which is a key service centre. Optimisation – The proposal is for new infrastructure and is not, therefore, considered an optimisation. Safety – Top Street has a NSL and notable bends whilst not having existing pedestrian infrastructure. It is considered, therefore, that the provision of pedestrian infrastructure will have safety benefits. Biodiversity – The proposal will result in the loss of foliage adjoining the road; therefore, a negative score is considered necessary. Leisure – No significant leisure benefits.
Martlesham	263	The entirety of the Martlesham retail development.	There is no pedestrian walkways between the myriad of large shops on the new retail development at Martlesham. Whilst the lack of footpaths was acceptable when this was a mainly commercial area, the explosion of retail outlets and consequential increase in footfall has meant both pedestrians, cyclists and motorists are now at considerable risk as they move about this area.	Provision of a complete footpath network linking all the parking and shopping areas such that by parking anywhere within the retail park area you can walk to any of the retail stores without having to walk along a roadway, with safe crossing places provided where any paths ways cross the road network.	1	0	0	1	0	1	3	Although there is existing infrastructure between the shops, there are some sections along the roads where this becomes to abrupt stop requiring pedestrians to cross the road. The commenter proposes a complete footpath network between shops. Connectivity and Growth – Connecting the shops provides modest connectivity benefits – whilst there is existing infrastructure, it may be slightly indirect, therefore a score of 1 is considered reasonable. Modal Shift – As there is existing infrastructure, it is unlikely that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and is not, therefore, considered an optimisation. Safety – The suggestion provides a small safety benefit as it reduces the need to continually cross the roads, however the roads are not significantly hazardous, so it's provided a score of 1. Biodiversity – In order to implement infrastructure, the removal of the highly managed grass areas adjoining the roads will likely need to be removed. Loss of grassed areas that are likely regularly cut. Leisure – Despite having some existing infrastructure, as some of the shops within

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												Adastral Park provide leisure benefit, it is considered that improving the infrastructure will likely have small leisure benefit.
Martlesham	264	General consideration of the motorist as a part of the cycling and walking strategy	The growing positive bias in Council policies and strategies towards walking and cycling seems at times to be bordering on a demonisation of all motorists. Any new initiatives should take into account Suffolk's rural environment and the need for many people - including the aged or disabled - to make journeys that are not viable on foot or by cycle. These people and their needs do not seem to be given due consideration in some of the rushed often ill-conceived initiatives that are proposed.	Ensure full and due consideration is given to all classes of road users when creating any schemes that seek to offer improvements to the built environment. Fulfilling the demands of any particular pressure group will undoubtedly lead to a less than optimum solution for the general populous who after all are the majority...In respects to all proposals there should be full consultation with all user groups prior to any initiative being taken forward, its especially important to reach out proactively to those who do not have the technical knowledge or access to the mainly internet focused mechanisms that currently form the backbone of the consultation process.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Martlesham	278	Brightwell lakes development Martlesham	Very little to indicate how this development will connect to the local cycling/walking infrastructure, especially on the west (Ipswich) side of the A12...how will a cyclist ride to Ipswich? How will a cyclist ride north to the retail park and beyond to Woodbridge. How will cyclist be protected whe cycling along 'Ipswich Road' Brightwell	1.) provide an independent cycle / pedestrain bridge over the A12 connecting with Lancaster Drive. 2.) provide some form of safe route to NCN 1 connection at the Gloster Road / Betts Avenue junction and upgrade (widen) the current pedestrian bridge across the A12. 3.) Ensure that all roads within the development have combined cycle / footpaths such as seen at Stowmarket Mortimer Road, such that a young child does not have to cycle on a road to get to school or the local park / shops.	2	2	0	3	0	2	9	Connectivity and Growth - The A12 is a significant barrier creating a wall between the residential areas to the west and the services and employment opportunities to the east. However, without teaming a second bridge that's located to the south with an onward route that cuts through the Martlesham Heath woodland up to north west Eagle Way, the onward travel gain of using the southern bridge rather than the existing A12 foot/pedal bridge is lost; the cyclist/pedestrian journey time (and energy) cost to reach the Broomfields shared paths for onward access to Longstrops Bridleway (which is set to be the 'keystone' of the strategic route between Brightwell Lakes and Ipswich) is higher than simply using the existing A12 foot/pedal bridge. The difference is not huge, though. With high levels of modal shift, a second bridge - regardless of the advantage lost - would be worth installing just to manage the pedestrian/cyclist flows, which would largely be coming from the south post-delivery. As suggested by the respondent, replacing the existing bridge with a higher-capacity (wider) and more accessible (less steep) bridge may achieve similar benefits, though. Modal Shift - Currently travellers must take a very indirect path, the modal shift figure on PCT for the alternative route suggests a reasonable modal shift would be obtained. The Brightwell Lakes development masterplan (at outline stage) indicates extensive use of shared paths is intended. Optimisation - Providing completely new infrastructure does not represent an optimisation. Safety - The A12 can be challenging to cross, though an existing bridge does exist and not at considerable distance given the need for onward travel via Broomfield, anyway, plus the new stretch of shared path now expected to come forward in the Martlesham Heath local centre area through a retirement living development (DC/20/1036/FUL) if it is permitted. Biodiversity - There are no significant biodiversity impacts, unless the new bridge is also teamed with a new route through Martlesham Heath, which is primarily birch woodland. Leisure - With leisure routes to the west and leisure uses within the employment park itself a reasonable score has been provided for the connections it provides.
Martlesham	329	Junction of Top Street Martlesham with Sandy Lane in conjunction with proposal further east.	This section of road is used as a rat run and alternative route for car drivers making it less pleasant and less safe for cyclists and walkers.	Close road to through traffic here as well as further east to provide cyclists with part of a safe and attractive route between Ipswich, Martlesham and Woodbridge.	3	1	0	3	0	2	9	Connectivity and Growth - the proposal would create a new connection between Martlesham and Woodbridge, which are large and well-established settlements. As Sandy Lane resides within a key corridor, a score of 3 is considered reasonable. Modal Shift - According to PCT, Sandy Lane is currently well used, and the improvement could score a 3 at the highest standard. However, the route is unlikely to be completely traffic free so the modal shift to the lower standard does not represent as a significant gain. A score of 1 is considered reasonable. Optimisation - Whilst the proposal provides benefits, it does not optimise the existing route. Safety - Sandy Lane is a narrow road with a national speed limit and is likely used as a rat run to bypass the main roads. As the road currently does not have either cycling or walking infrastructure, it is considered that a modal filter will provide safety benefits hence a score of 3. Biodiversity - There are no biodiversity impacts. Leisure - The proposal would connect to the PROW routes which reside along Martlesham creek and the River Deben - as these are particularly attractive routes that extend through the AONB designation, a score of 2 is considered reasonable.
Martlesham	332	Felixstowe Road	The road is used by motorists as a rat run making it very unattractive to cyclists. The road layout does not appear to give cyclists priority but causes confusion to cyclists and motorists.	Close road to north of the community centre to through traffic and provide cyclists and pedestrians with a safe and attractive route.	0	1	3	2	0	1	7	The commenter proposes closing half of Felixstowe Road to through traffic in order to upgrade existing infrastructure to an off-road option. Connectivity and Growth - The connection here already exists so the suggestion does not score in this category. Modal Shift - The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift. Optimisation - This improvement would mean change from an on-road option to a segregated cycle track which results in a score of 3. Safety - Whilst the road is a cycle priority route, it appears that many motorists do not treat the road as such. Whilst the road is 30mph the improvement is considered somewhat significant improvement. Biodiversity - There are no discernible biodiversity impact. Leisure - Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.

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Martlesham	344	Terrible bike path	The shared bike path pedestrian lane past Suffolk Constabulary has very poor surface with holes and rotten leaves	new surface regular clearing							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Martlesham	356	Cycle lanes anywhere in the east suffolk region	Can you make sure that any cycle lanes (road or pavement) that are installed are to the regulation width and not too narrow to use (some parts on Felixstowe Road Martlesham are about 60cm). If any of the plastic wands/bollards are used then the 2m width of the cycle lane should be used. I have a tricycle and cannot use the lanes in Ipswich which have wands installed without either hitting the kerb or wands as they are too narrow,	keep to the planning guidelines and standard for all cycle lane provision. That way motor vehicles can give some clearance to cyclists, even if driving right onto the white line or wand							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Martlesham	376	Footpath 10 coming from Old Martlesham to the roundabout to the Duke of York pub at Ipswich Road/Barrack Road junction.	There is a poorly maintained and overgrown footpath that goes right from Footpath 10 and crosses the B1438 close to the roundabout to continue along to the north side of the B1438 to the Duke of York.	With a more easterly crossing of the B1438 and upgrading this route would provide a safe footpath to Woodbridge from Old Martlesham and could be widened for cycle use as well.	1	2	0	2	-1	1	5	Connectivity and Growth – The proposal would help in the connection of Martlesham and Woodbridge, which are both large settlements with a good level of services, schools, and shops. As the proposal does not directly connect into the town centre, a score of 1 is considered reasonable. Modal Shift – If infrastructure can be delivered to the highest standard, PCT suggests that there would be a resultant modest modal shift. A score of 2 is deemed reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Despite the B1438 being a 30mph road, it is particularly busy, therefore a score of 2 is considered reasonable. Biodiversity – A small negative score has been given due to the likelihood of the loss of managed green verges and shrubbery that adjoins the existing infrastructure should it be expanded to a shared path. Leisure – Woodbridge is a key town centre and a footway/cycleway into the centre could warrant a high score under this category. However, the proposal is for a connection to the Duke of York public house which would be considered a small attraction, hence the 1 point.
Martlesham	377	Just east of the Seckford Hall roundabout ((A12/B1438)	Footpath 10 from Martlesham crosses the A12 here on a derestricted section of dual carriageway with no marking or warning for drivers of the crossing - extremely dangerous as unsighted for southbound drivers on the A12 until they accelerate west out of the roundabout. On the north side it also connects with a poorly maintained footway up to the B1079/A12 roundabout.	Install a pedestrian control traffic light crossing as per the current footpath crossing the A14 just west of the Dock spur roundabout outside Felixstowe. Upgrade the path up to the B1079/A12 roundabout to pedestrian and cycleway.	1	0	0	2	0	1	4	Connectivity and Growth – Pedestrian infrastructure along the south side of the A12 comes to an abrupt stop and a crossing point would connect the infrastructure along the northern side of the road. The A12 is a modest barrier to those situated on either side, therefore the proposal will likely have small connectivity benefit – a score of 1 is deemed reasonable. Modal Shift – Insufficient evidence to suggest that a crossing point will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – This section of the A12 is a dual carriageway with a national speed limit and a crossing point will, therefore, have a safety benefit. A score of 2 is considered acceptable. Biodiversity – No biodiversity impact. Leisure – PROW 10 crosses the A12 along this section, which connects into a wider attractive PROW network. It is likely, therefore, that the proposal will have small leisure benefit.
Martlesham	407	Footpath from Martlesham to Waldringfield along River Deben	For many years it has simply been accepted that part of the path was washed away by natural erosion, so the only way to walk to Waldringfield from Martlesham is along the road. This is shown by signposts at the access points to this section of footpath.	Re-instating this footpath (by mending the breach at TM279461 or providing a diversion following the high-water mark) would provide a continuous off-road footpath route along the entire west bank of the Deben estuary, with several suitable entry/exit points.	1	0	0	3	-1	3	6	Connectivity and Growth – The improvements will likely have more leisure benefit than connectivity, however the proposal would create a connection between Martlesham and Waldringfield. Martlesham provides services that Waldringfield does not have but there is unlikely going to be ‘everyday’ use as the connection is not direct. Modal Shift – Despite a new connection to Martlesham, it is indirect and will likely have more leisure value. It is not considered, therefore, that the proposal will result in a significant modal shift. Optimisation – The proposal is not considered an optimisation. Safety – The proposal will provide an alternative route to the use of Waldringfield Road which is narrow with a NSL. The proposal will have safety benefit, therefore a score of 3 is considered reasonable. Biodiversity – A modest minus point is deemed reasonable due to creating a footpath more inland will likely result in the loss of some foliage. Leisure – Re-instating the footpath will have significant leisure benefit as these paths represent high leisure links alongside the River Deben. This improvement warrants the highest score under this category.
Martlesham	435	Felixstowe road, especially between mill lane and main road	The road is not safe to cyclists or pedestrians, regardless of the time of day. I walk this road frequently for work and groceries and cars whizz past as dangerous speeds. The section between mill lane and main road is very overgrown which forces pedestrians closer to traffic, it is also poorly lit compared to further up the road and littered with debris which makes it difficult to see where the path ends and the road begins.	Deterring speeding, clearing the greenery and widening the foot path would be a good start however making the road one way would be the best option to make the road safe for cyclists as well. I avoid Felixstowe road altogether when cycling as the road is even less safe than the foot path.	0	1	3	2	0	1	7	Connectivity and Growth – The connection here already exists so the suggestion does not score in this category. Modal Shift – The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift. Optimisation – This improvement would mean change from an on-road option to a segregated cycle track which results in a score of 3. Safety – Whilst the road is a cycle priority route, it appears that many motorists do not treat the road as such. Whilst the road is 30mph the improvement is considered a somewhat beneficial. Biodiversity – There are no discernible biodiversity impact. Leisure – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	436	Felixstowe Road, Martlesham between Crown Point and junction with Anson Road	I've noticed a large increase in the volume of vehicles using Felixstowe Road in recent years. I regularly walk along this route but feel increasingly unsafe doing so. Traffic passes very close, if there are puddles at the road edge there is nowhere to move out of the way, as the path is narrow /overgrown in places. The street lighting is inadequate to see the path edge, I worry about slipping off the kerb into the road. I feel safer	Make the road one way for motor vehicles, with improved cycling lane. Widen the footpath, and introduce traffic calming measures. Additional street lighting.	0	1	3	2	0	1	7	Connectivity and Growth – The connection here already exists so the suggestion does not score in this category. Modal Shift – The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift. Optimisation – This improvement would mean change from an on-road option to a segregated cycle track which results in a score of 3. Safety – Whilst the road is a cycle priority route, it appears that many motorists do

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			walking down Mill Lane and around the field edge in the dark.									not treat the road as such. Whilst the road is 30mph, the improvement is considered a somewhat beneficial. Biodiversity – There are no discernible biodiversity impacts. Leisure – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	462	Riverside path leading from railway bridge on Sandy Lane to the river at Kyson Point.	Currently there is no provision for cyclists to cycle close to the river in the Woodbridge area. We are missing an opportunity to promote glorious cycling in our region.	Please could it be permitted for cyclists to use the river path with priority for pedestrians or permitted at certain times of day or weekdays only? We really need to have more shared usage tracks to encourage good manners and cooperation between cyclists and walkers rather than pitting them against each other always. Why can't we make East Suffolk lead the way in this country, - we are the gateway to the parts of Europe where cycling is king and we have so much to offer. At least make Sandy Lane a quiet Suffolk lane with priority for cyclists and pedestrians as when the tide is in the footpath at the bottom of the creek is impassable.	1	3	0	2	-1	3	8	Connectivity and Growth – The proposal would create a new connection between Martlesham, Woodbridge, and Melton, which are large and well-established settlements, however there is unlikely to be significant everyday use due to both settlements having good levels of schools, shops, employment opportunities. Due to the where the proposal is situated, it will likely have more leisure benefit, however a moderate score of 1 under this scoring category is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The River Deben path, being located parallel to this road, would be a viable alternative route between Martlesham, Woodbridge, and Melton. Using PCT, there would be a potentially significant uplift, this warrants the highest score under this category. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Removing cyclists off the majority of the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is a busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which create an obstacle. The proposal does, therefore, warrant a score of 2 under 'safety'. Biodiversity – The Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – Access alongside the River Deben will have significant leisure benefits, therefore a score of 3 is warranted.
Martlesham	515	Felixstowe Road	As you will know, it is marked as a "cyclist priority route" at both ends. But in practice it is not. The painted lines provide no latitude for a wobble. During the lockdown there was a reappearance of young families on bikes unthreatened by cars, but now the 4 x 4 are out in force again with their largely single occupants hell bent on going shopping. Their speeds are often estimated at 40/50mph. Coming out of Mill Lane one has about 2 seconds to exit.	I offer the following solution which has virtually no cost. Introduce vehicle free sundays, so that family cycles can explore and travel this short distance without the threat of extra danger.	0	1	2	2	0	1	6	The commenter proposes restricting vehicular access on Sundays. For the purpose of this assessment, restricting vehicular access will be assessed– this is similar to that seen along Cumberland Street, Woodbridge. Connectivity and Growth – The connection already exists so the suggestion does not score in this category. Modal Shift – PCT suggests modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. As the improvement, when the road is closed to vehicular traffic, could be considered high standard infrastructure, it is likely that the improvements would result in a modest modal shift. Optimisation – Again, the improvements could be, when the road is closed to vehicular traffic, infrastructure to the highest standard, therefore a score of 3 is normally warranted. As the road will only restrict vehicular access for certain days/times, a score of 2 is considered reasonable. Safety – Whilst the road is cycle priority route, it appears that many motorists do not treat the road as such. Whilst restricting vehicular access is considered a significant improvement for safety, this will only be for certain days/times, therefore a score of 2 is considered reasonable. Biodiversity – There are no discernible biodiversity impact. Leisure – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	519	Pathway from Martlesham Creek to Kyson Point and on to Woodbridge	Having made much use of the pathway from Martlesham Creek to Kyson point and on to Woodbridge over the last lockdown months we have often been forced to step aside into less than safe areas to let cyclists pass. They should not be on these narrow paths at all - signs are inadequate. There have been talks about making this route more accessible for cycling which would cause considerable work and disruption and cost a very large sum. We are against such a proposal.								N/A	Objection raised against other proposals. These do not need to be scored but will be considered against the proposal.
Martlesham	533	Gloster Road	The cycle lanes on this stretch are too narrow, a lot of cars drive exactly next to them and so leave far less space than the recommended 1.5m. It's especially worrying cycling next to big articulated lorries going to/from the shops/industrial estate.	Widen the cycle lanes	0	1	3	1	0	1	6	The commenter suggests that the cycle lanes are too narrow; therefore, for the purpose of this assessment, the implementation of an off-road segregated cycle track will be assessed. Connectivity and Growth – Connection already exists here, so does not score under this category. Modal Shift – PCT suggests that if infrastructure is delivered to the highest standard, that there will be a resultant modest modal shift. Optimisation – Optimising a route from an on-road cycle lane to an off-road segregated cycle track warrants the highest score under this category. Safety – Although Gloster Road has existing cycling infrastructure, it is poor quality. Removing cyclists off Gloster Road, scores a 1 under safety. Biodiversity – In order to implement the proposal, the removal of the well managed grass areas adjoining the road will be necessary. Leisure – Although there is existing infrastructure along this road, the improvement will likely have modest leisure benefit as it will provide improved access to the shops within Adastral Park. A score of 1 is considered reasonable.
Martlesham	534	Felixstowe Road	The road markings are completely bonkers. Cars sometimes drive in the middle very near to oncoming traffic as if they think it's one-way. Also, traffic moves too fast, often far quicker than 30mph which I guess is the limit. At rush hour, cars sit in the cycle lane in a long line queuing at the t-junction. The pavements are too	Impose a speed limit, sort out road markings, possibly chicanes (things that stop motorists using it as a rat run and really make it a cyclist priority route as intended).	0	0	1	1	0	1	3	The comment in relation to speed falls outside the remit of the project and should be passed onto SCC. For the purpose of the assessment, the widening of the road markings and the addition of chicanes will be assessed. Connectivity and Growth – Felixstowe has existing infrastructure; therefore, the proposed alteration will not create additional connectivity. Modal Shift – The on-road cycle lane remains so no modal shift.

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			narrow too. I cycle daily between martlesham and woodbridge and this is one of the bits which I think could be made much safer for cyclists and pedestrians.									Optimisation – Widening the cycle lanes and adding chicanes to prevent rat-running is considered a moderate optimisation, therefore a score of 1 is deemed acceptable. Safety – Felixstowe Road, although a 30mph road, is particularly busy and the proposal will likely have moderate safety benefits, therefore a score of 1 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – Brightwell lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	535	Right turn onto Sandy Lane	It is a hairy right-hand turn coming down the hill to turn right onto sandy lane.	Speed limit or separate waiting space would help	0	0	0	2	0	0	2	Reducing speed falls outside the remit of the project and should be passed onto SCC. The commenter requests road markings on the bend on Sandy Lane to allow cyclists to wait safely. Connectivity and Growth – The proposal does not create additional connectivity. Modal Shift – This does not create additional connectivity. Optimisation – This does not optimise existing infrastructure. Safety – This would be for highways to judge. The cyclist would remain on the road; however, the turn is sharp as well as narrow and the road has an NSL. Therefore, the proposal will likely have some safety benefit hence the score of 2 under this category. Biodiversity – No biodiversity impact. Leisure – No significant leisure benefit.
Martlesham	596	GR 254481	Dangerous to cross A12 to /from cycle way, especially just to west of roundabout and the B1458 into Woodbridge	A Toucan Crossing. Also resurface & remove foliage from cycle way	1	0	0	2	0	1	4	The commenter proposes a toucan crossing where PROW 10 crosses the A12. The commenter also proposes resurfacing and removing foliage from the footway, however this appears to be a maintenance issue and should be passed to SCC. Connectivity and Growth – Pedestrian infrastructure along the south side of the A12 comes to an abrupt stop and a crossing point would connect the infrastructure along the northern side of the road. The A12 is a modest barrier to those situated on either side, therefore the proposal will likely have small connectivity benefit – a score of 1 is deemed reasonable. Modal Shift – Insufficient evidence to suggest that a crossing point will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – This section of the A12 is a dual carriageway with a NSL, a crossing point will, therefore, have a safety benefit. A score of 2 is considered acceptable. Biodiversity – No biodiversity impact. L – PROW 10 crosses the A12 along this section, which connects into a wider attractive PROW network. It is likely, therefore, that the proposal will have small leisure benefit.
Martlesham	602	GR 260 451	At present, ATs aiming for the Martlesham Retail Park and to cross the A12 via the foot & cycle bridge or either of the tunnels in order to reach the Martlesham P&R, Kesgrave High School, Ipswich Hospital, Town, buses or rail station, and visitors coming the other way, tend to cycle along the tarmac strip as footpaths #23 & 43 are very rough.	When Brightwell Lakes are developed, good cycleways to the A12 crossings, must be provided	3	2	3	2	-1	1	10	The commenter proposes high quality cycleways to the existing A12 crossings. For the purpose of this assessment, improving the existing cycleway along the A12 towards Barrack Square, which should also have a segregated cycle track, will be assessed. Connectivity and Growth – Creating new cycle infrastructure along Barrack Square will likely have significant connectivity benefit. Barrack Square resides within a key corridor and connects into the Martlesham retail park. The highest score under this category is considered reasonable. Modal Shift – According to PCT, if infrastructure along the A12 is delivered to the highest standard, the proposal will result in a somewhat significant modal shift, therefore a score of 2 is warranted. Optimisation – In terms of improving the existing infrastructure along the A12, upgrading a shared path to segregated cycle track usually warrants a score of 2, however the existing infrastructure is particularly narrow and is within close proximity to the A12, which is a dual carriageway with a national speed limit. A score of 3 is considered reasonable. Safety – Although the cyclists are already separated from the road on the A12, providing infrastructure along Barrack Square will likely have some leisure benefit. Biodiversity – The proposal will result in the loss of a grassed area, which is likely regularly cut and of limited benefit. Leisure – Brightwell Lakes provides some Leisure value, whilst the improvement would not have a significant leisure gain, a modest score is reasonable.
Martlesham	604	GR 247 459 GR 248 454 & GR 193 453	Mainline buses at Tesco, Mrtlesham Heath & BT at southern end Gloster Road, are bus 'nodes' offering frequent services to & from Ipswich, Felixstowe & Woodbridge. They could complement cycling and walking to and from nearby rural settlements. But there are no hoops to which to secure bikes, and no urinals. Similarly there are no public toilets near the P&R bus stop at the roundabout north of the Hospital for ATs enroute to and from Ipswich, but I didn't flag it on your map.	Provide hoops to which to secure bikes, and toilets mainly for older ATs. Men only need urinals and now women likewise with advent of 'SheWees'! This may seem trivial to younger and middle aged persons but lack of them can be a serious deterrent to elderly Active Travelers.	0	1	0	0	0	2	3	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The provision of a WC will unlikely result in significant modal shift; however, cycle parking, although unlikely to encourage large numbers of modal shift on its own, will provide a certain level so a score of 1 is deemed acceptable. Optimisation – No optimisation benefit. Safety – No significant safety benefit. Biodiversity – No significant biodiversity impact. Leisure – As Martlesham Heath has a handful of small leisure attractions, the WC and cycle parking would help provide leisure benefits to visitors and would score a 2.
Martlesham	645	Footpath from Martlesham to Waldringfield along River Deben	The breach prevents walking between Woodbridge and Waldringfield without going on roads	Waldringfield Parish Council agrees that the footpath should be re-instated but disagrees that this should be done by mending the breach. We support the new inland footpath proposed by Natural England, and shown below:	0	0	1	1	-2	1	1	Unlikely to benefit many pedestrians due to the scale of the walk even to Martlesham Creek. Likely to have biodiversity impact - significance unknown but score of -2 given as a precaution - this is part of a European Site protecting ground nesting birds.
Martlesham	682	Martlesham Retail Park	The Martlesham Retail Park needs measures to allow safer circulation for pedestrians & cyclists. As with most retail parks, the emphasis is on the car, but many shoppers move between the different shopping areas on foot. In particular crossing Anson Road for pedestrians between Tesco & Pets At Home is difficult. There is a lack of dropped kerbs on Beardmore Park making it difficult for wheelchair users to move between the areas.		0	0	1	1	0	0	2	Connectivity and Growth: It is possible to connect to the subway under the A12 and onward travel up to Main Road without a new crossing over Anson Road (using existing crossing points) even if less convenient; dropped kerbs will make this easier for some users, which is scored under optimisation. Optimisation: Increases usability of the space by cyclists and pedestrians, and dropped kerbs particularly benefit wheelchair users and people pushing push chairs. Safety: Increases safety by providing a legitimate crossing (people probably run across Anson Road now, if they attempt to cross it at all)

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Martlesham	685	Manor Road crossing point of Eagle Way, near the Tesco roundabout	The Manor Road crossing point of Eagle Way, near the Tesco roundabout, is dangerous with traffic leaving the A12 at speed making it difficult for pedestrians and cyclists to cross.		1	0	1	2	0	0	4	Connectivity and Growth - This will mainly benefit active users living on the eastern side of Martlesham Heath (and possibly the western side of Brightwell Lakes, having accessed the eastern side of Martlesham Heath via the bridge over the A12) that are using the Martlesham Park & Ride bus service, having accessed it by bike. This connection is already available via an alley over to Portal Avenue, though the quality of the alley is limited and needs redevelopment - this is likely to come forward through the MRN bid or subsequent bids, as the option to turn this into a bus route with a parallel cycleway is being pursued. The extent of its strategic connectivity and growth importance is limited by the other options for accessing Main Road, Grange Farm Kesgrave or the anticipated Long Strops Bridleway route towards Ipswich, which is the direction Martlesham Heath and Brightwell Lakes cyclists/pedestrians are most likely to want to go. Cyclists travelling from the western side of Martlesham Heath are likely to access Main Road and the Park & Ride via the Broomfield alleys and Deben Avenue, the track behind the Police HQ site (and when delivered, PROW(s) through the site). The crossing would also aid people cycling/walking from the eastern-side of Martlesham Heath towards Woodbridge, though would have little benefit for those cycling/walking from the west as they would most likely already be positioned on the northern side of the road or able to cross at a safe point during their journey up Eagle Way. The crossing is therefore desirable rather than strategic, and though of benefit, is likely to benefit a small number of people (leading to BCR issues). Score of 1 is given on the basis that there is a connectivity benefit, but it is minor. Modal Shift - 0 - uplift in cycling to the Martlesham Park and Ride arising from a new crossing is most plausible, though not likely to be significant. Optimisation - 1 as it does improve the cycling/walking infrastructure, but minimally. Safety - A signalled crossing at this location is absent (and same for crossing Eagle Way to access the Broomfield alleys) even if such a crossing will likely to benefit relatively few it will have safety benefits. Biodiversity - 0 Leisure - 0
Martlesham	686	A12 underpasses at the Tesco and Park & Ride roundabouts	The two A12 underpasses at the Tesco and Park & Ride roundabouts are poorly lit, in particular the one between the Police HQ and old Martlesham. They are main cycle/pedestrian routes, but they are unattractive, appear to be infrequently cleaned and the vegetation can encroach. The slopes on the approaches, as well as on the footbridge between Martlesham Heath, make these dangerous routes for pedestrians and cyclists alike in icy weather.		0	0	1	2	1	0	4	Connectivity and Growth - No significant connectivity and growth benefit. Modal Shift - 0, unlikely to create statistically significant enough uplift even if the twisting shape and gradient of the descending/ascending sections are improved on safety grounds. Optimisation - Hard to argue it wouldn't be an optimisation at all, though does not constitute a recognisable jump through optimisation assessment table. 1 given as 0 unreasonable. Safety - 2 given for reduction in steepness (which creates quick acceleration) and/or improvement of visibility/reduction in 'twistiness' of the descending/ascending sections. 3 not given as cars not involved, so very unlikely for very serious incidents/fatalities to occur if the improvements are not carried out. Biodiversity - One as planting is suggested as part of improvements programme. Leisure - 0. No identified leisure benefit.
Martlesham	687	Safe crossing of the A12 for Brightwell Lakes	We have lobbied for a safe crossing of the A12 for Brightwell Lakes and suggested an upgrade of the existing bridle path to form safe links into Kesgrave/Rushmere/Ipswich and to the local national cycle networks.		0	0	3	-3	0	0	0	Connectivity and Growth - A bridge at this location, together with a partly re-aligned and upgraded bridleway 6 (to LTN 1/20 standards for at least bi-directional low cycle flow dimensions and surfacing standards) would open up direct active travel opportunities into Foxhall Heath, which may come forward for development in the future (currently outside Settlement Boundaries, which may be revised in future local plans), providing a safe crossing over Dobbs Lane was also provided. Some potential benefit recognised if Long Strops field comes forward, though this would need to be teamed with improvements to Dobbs Lane to allow a safe transition northwards. However, currently the suggested improvement would have limited benefit for future Brightwell Lakes residents, as it would 'dump' them at the bottom of the intersection of two well-used and high-speed roads (see 'safety' score). As this assessment can only reasonably be made at this stage in accordance with the current development plan, connectivity and growth is rated at 0. Modal Shift - PROW route improvements and new bridge cannot be picked up by PCT, so judgement call used. See above - unlikely to have modal shift impact as onward cycling at the end of bridleway 6/46 acts as no incentive. Optimisation - As totally traffic free 'greenfield' route, the highest quality infrastructure (total segregation, optimum dimensions and surfacing for cycling and walking) can be employed, therefore a score of 3 is given. This is an 'academic' score though, as the optimisation wouldn't currently represent a high enough BCR for delivery due to its disconnection for safe and direct onward travel. Safety - -3 Dobbs Lane is a long, straight and narrow road with no lighting. Similar case for Foxhall Road, which also contains undulation and bends (and no lighting until just before the roundabout). Biodiversity - This field is likely to be of minimal biodiversity value, unfortunately. Directing bridleway 6 in the way assumed it was intended to be suggested (as there was no direct suggestion) this would reduce SSSI impact to a minimum by only clipping the southern-most corner of the site. 0. Leisure - 0. Unless the suggestion was for a circular route in this field, the abrupt finish after a relatively short distance would prove little added value to the current PROW network. However, it may provide a safe space for children to play on bikes, skateboards, roller blades etc - though this could be better accommodated in the Brightwell Lakes development itself, and may be worth bringing up at reserved matters stage. 0.
Martlesham	688	Martlesham	The feedback by local parishioners shown on the ESC interactive map reinforces many of the issues raised by MPC over several years, in particular about the need to make improvements to encourage sustainable and safer travel between Martlesham and Woodbridge. This is all the more important given the climate emergency which SCC, ESC and MPC have declared. We refer you to the Martlesham NP which has a section on 'Getting Around' - see Cycling, walking and disabled access, p43, policies MAR13 & 14.		0	0	0	0	0	0	N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.

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Martlesham	757	Convoy riding on highway	Although riding in large groups is no doubt a pleasant experience, riding in convey without occasionally pulling in to allow build up of traffic to pass does put riders at risk of car drivers taking chances to pass. I have on more than one occasion been stuck behind such a convey from Martlesham through to Woodbridge with little opportunity to pass. One has to be patient but as said, some car drivers may try and overtake inappropriately risking themselves and cyclist to injury.								N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Martlesham	531B	Martlesham retail and business park, Old Felixstowe Road, Main Road Martlesham, Sandy Lane into Woodbridge	Volume of motorised traffic make this route unsafe for cyclists and pedestrians	1 Traffic management scheme within the retail/industrial area channeling traffic onto A12 2 Restoration of Old Felixstowe Road to a safe cycle priority route by limiting motorised through traffic to buses and emergency vehicles 3 Traffic calming chicanes in The Street, Martlesham 4 No through motorised traffic on Sandy Lane achieved by a physical barrier at the railway bridge	0	1	3	2	0	1	7	The commenter proposes closing Felixstowe Road to vehicular traffic except that of the emergency services. Connectivity and Growth – The connection here already exists so the suggestion does not score in this category. Modal Shift – The road is reasonably well used, PCT suggests a modest use contrary to its designation as a cycle priority path, but Strava suggests greater use. The improvement to a high standard would create a modest modal shift. Optimisation – This improvement would likely mean change from an on-road option to a segregated cycle track which results in a score of 3. Safety – Whilst the road is a cycle priority route, it appears that many motorists do not treat the road as such. Whilst the road is 30mph the improvement is considered a beneficial improvement. Biodiversity – There are no discernible biodiversity impact. Leisure – Brightwell Lakes provides some leisure value, whilst the improvement would not have significant leisure gain, a modest score is reasonable.
Martlesham	531C	Martlesham retail and business park, Old Felixstowe Road, Main Road Martlesham, Sandy Lane into Woodbridge	Volume of motorised traffic make this route unsafe for cyclists and pedestrians	1 Traffic management scheme within the retail/industrial area channeling traffic onto A12 2 Restoration of Old Felixstowe Road to a safe cycle priority route by limiting motorised through traffic to buses and emergency vehicles 3 Traffic calming chicanes in The Street, Martlesham 4 No through motorised traffic on Sandy Lane achieved by a physical barrier at the railway bridge	0	0	1	1	0	0	2	Connectivity and Growth – Connection already exists so the proposal does not score under this category. Modal Shift – There is insufficient evidence to suggest that the proposal will result in a significant modal shift. Optimisation – Although the improvement does not directly optimise the existing cycle lane along the Street, the implementation of chicanes will likely optimise its use, therefore a score of 1 is considered reasonable. Safety – The Street is a particularly busy road with a 30mph speed limit and has existing on-road cycling infrastructure. The implementation of chicanes will likely result in vehicular traffic to pass cyclists utilising the infrastructure at a safer speed. As the existing infrastructure will remain on-road, a score of 1 under safety is considered reasonable. Biodiversity – No biodiversity impacts. Leisure – No significant leisure benefits.
Martlesham	531	Martlesham retail and business park, Old Felixstowe Road, Main Road Martlesham, Sandy Lane into Woodbridge	Volume of motorised traffic make this route unsafe for cyclists and pedestrians	1 Traffic management scheme within the retail/industrial area channeling traffic onto A12 2 Restoration of Old Felixstowe Road to a safe cycle priority route by limiting motorised through traffic to buses and emergency vehicles 3 Traffic calming chicanes in The Street, Martlesham 4 No through motorised traffic on Sandy Lane achieved by a physical barrier at the railway bridge	3	1	0	3	0	2	9	Connectivity and Growth – The proposal would create a new connection between Martlesham and Woodbridge, which are large and well-established settlements. As Sandy Lane resides within a key corridor, a score of 3 is considered reasonable. Modal Shift – According to PCT, Sandy Lane is currently well used, and the improvement could score a 3 at the highest standard. However, the route is unlikely to be completely traffic free so the modal shift to the lower standard does not represent as a significant gain. A score of 1 is considered reasonable. Optimisation – Whilst the proposal provides benefits, it does not optimise the existing route. Safety – Sandy Lane is a narrow road with a national speed limit and is likely used as a rat run to bypass the main roads. As the road currently does not have either cycling or walking infrastructure, it is considered that a modal filter will provide safety benefits hence a score of 3. Biodiversity – There are no biodiversity impacts. Leisure – the proposal would connect to the PROW routes which reside along Martlesham creek and the River Deben - as these are particularly attractive routes that extend through the AONB designation, a score of 2 is considered reasonable.
Martlesham	681a	Felixstowe Road, Main Road and Sandy Lane, Martlesham	Felixstowe Road, Main Road, and to a lesser extent Sandy Lane, Martlesham, have become a rat run making them dangerous and unattractive routes for pedestrians and cyclists. Unless traffic is reduced on Main Road, it would benefit from safer crossing points for people of all abilities.	An MPC paper on Felixstowe Road, "Felixstowe Road traffic calming", is attached which was previously circulated to the principal authorities and the developer of Brightwell Lakes; the points raised remain pertinent. We have also been pushing for improvements to Sandy Lane via our County Councillors; an MPC paper, "Sandy Lane Speed Limit 2017 – briefing paper" is attached.	2	2	2	2	0	2	10	Assessment based on respondent's suggestions. Connectivity and Growth: C&W improvements and modal filtering of Felixstowe Road are critical to the success of the Felixstowe to Woodbridge (via Brightwell Lakes) key corridor - with it being of particular use to future residents of Brightwell Lakes for getting into Woodbridge, and Woodbridge residents in accessing the retail offer of Beardmore Park. However, a lightly modally filtered solution is not likely to significantly uplift usage from its already high (but would be higher) levels. Modal filtering of Sandy Lane and imposing a speed limit also very important, and its critical that they are done together in the interest of route continuity. Score of 2 given as need to address cycling route down Main Road and crossings not covered (see Officer's alternative below). Modal Shift: See above Optimisation: See above Safety: 2 Biodiversity: 0 Leisure: 2
Martlesham	681b	Felixstowe Road, Main Road and Sandy Lane, Martlesham			3	2	2	2	0	2	11	Connectivity and growth: 3 as improving the safety and usability of Sandy Lane, Felixstowe Road and Main Road are mission-critical to the establishment of adequate key corridor active infrastructure. Supporting the Portal Avenue MRN improvements indirectly supports the key corridors by providing more permeability and therefore usability of this area of the overall active movement network. Modal Shift: Modal shift only represents a modest uplift on Felixstowe Road and Sandy Lane when in 'near market' mode, which reflects the use of a modal filter on Felixstowe Road that include bus use and local resident use, rather than full segregation. Likewise, Sandy Lane would be closed to through traffic but still used by commercial vehicles for access to commercial properties at the southern end, and may still be used as a cut through when accessed via California (its not reasonable to modally filter them both as residents at the B1438 end would have to drive all the way around to the Street entry point to drive up and access their properties). However, Main Road has

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												significant potential for total segregation in places and the creation of LTN1/20 compliant shared paths (though usage along this corridor is likely to exceed the guidelines on the use of shared paths, which are only meant to be used for low cycling and/or low pedestrian flow routes). A high standard of improvement could have a potentially significant uplift. Averaged out a score of 2 was given. Optimisation: Optimisation only 2 and not 3 as total segregation not used consistently throughout. Safety: See above.
Martlesham	684 (category 1 - Point 1)	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.	1	0	1	1	0	1	4	Connectivity and Growth: The pedestrian crossing would inevitably also be used by cyclists, though it would not be designed appropriately for their use. Connectivity and growth benefits are likely to be low in impact, but significant enough to earn a score of 1 as per Minor Improvements matrix. Modal Shift: 0 Optimisation: 1 Safety: 1 - Felixstowe Road does not pose significant crossing risk except at peak times. Felixstowe Road may also become modal-filtered at a later stage as part of strategic plans for the Woodbridge to Brightwell Lakes/Felixstowe Key Corridor, which will reduce the need for a crossing even more. Biodiversity: 0 Leisure: 1 as it increases likelihood of use of footpath 40, particularly when upgrade to a bridleway, which is green and rural in nature. Also rates for leisure on the basis that it improved access to retail which is a leisure activity for some.
Martlesham	684 (category 1 - Point 10)	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.							N/A	Please see the assessment of comment 685
Martlesham	684 (category 1 - Point 2)	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.	0	0	0	0	0	0	0	No added benefits identified, rated zero across all MCAF categories.
Martlesham	684 (category 1 - Point 9)	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.	1	0	1	1	0	0	3	Connectivity and Growth: The whole of Beardmore Park is notoriously car-dominated despite the patchy provision of active infrastructure of varying levels of quality. A crossing over Anson Road is quite obviously missing, and is needed to give better north-south connectivity through the Park. Crossing onto a petrol station forecourt is not ideal however, so the placing of the crossing would need to be at least slightly diverted eastwards so pavement can be accessed on both sides. Modal Shift: Zero, though in reality generally reducing the domination of the car in this area has significant potential for increasing the number of cyclists from Kesgrave, Martlesham Heath and even the Deben Villages, especially after Brightwell Lakes infrastructure has been delivered to give them a safer cycle to Beardmore Park than Ipswich Road. Optimisation: 1 Safety: 1 Biodiversity: 0 Leisure: 0
Martlesham	684 (category 2)	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.	0	0	1	1	0	0	2	Connectivity and Growth - Alternations do not provide a significant connectivity and growth benefit. Modal Shift - The alterations are unlikely to provide a significant modal shift. Optimisation - The tactile paving and the removal of obsolete cycle markings would represent an optimisation to the infrastructure. Safety - The improvements would represent a modest safety improvement. Biodiversity - No significant biodiversity impact. Leisure - No significant Leisure impact.
Martlesham	684 (Category 3)	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.	0	0	1	1	0	0	2	Connectivity and Growth - Dropped kerbs are unlikely to create significant connectivity and growth benefit. Modal Shift - No significant modal shift expected. Optimisation - This improvement will offer a modest optimisation of the existing. Safety - A modest safety benefit is provided. Biodiversity - No significant biodiversity impact Leisure - No significant leisure benefit.
Melton	42	B1438 Melton Road Woodbridge	This road is a significant link between Woodbridge town centre and Melton. The road is very wide but has no cycle infrastructure or any reasonable alternative routes.	Provide good quality cycle infrastructure and Cycle advanced stop lines at traffic lights either end. May need to consider on street parking and the narrowing road at the Woodbridge end.	3	1	0	2	0	2	8	Connectivity and Growth - the proposed route will connect to Melton Primary School, a number of services along Melton Road, and to site allocation SCLP12.32. Modal Shift - Based on PCT data the proposal will have small potential modal shift, therefore scoring it a 1. Optimisation - The proposed improvements are new and do not optimise the existing. Safety - Despite Melton Road having a 30mph SL, it is a busy 'B' type road with many parked cars, which may be an obstacle for cyclists. Given the road and the parked cars, a pavement taking cyclists off the road provides a moderate improvement. Biodiversity - There are no biodiversity impacts. Leisure - the proposed infrastructure does connect to the river walks and to Melton Playing Fields giving the proposal a moderate leisure score.
Melton	45	Wilford Bridge Road, Melton	Popular route for recreational cycling without any cycle infrastructure. This road provides access to the railway station at Melton and is the only direct route between the populated areas of Ipswich / Woodbridge and the coast and forests that are so important for recreation. Very hostile road for cyclists with blind bends and double white line no overtaking restrictions. May be possible to open up the riverside path as alternative from Woodbridge?	Good quality cycle infrastructure replacing existing pavement between roundabout and the level crossing.	1	0	0	3	-2	2	4	Connectivity and Growth - the proposal provides a connection to a small handful of PROWs and to Melton railway station; however, it provides limited connections to other villages and services. Therefore, the proposal scores one under connectivity and growth. Modal Shift - As a leisure route without significant connectivity it is not considered that there will be significant modal shift. Optimisation - the proposed improvements are new and, therefore, do not optimise the existing hence a score of zero under 'Optimisation'. Safety - Wilford Bridge Road is a narrow 'A' type road; therefore, volume and speed of traffic is likely high. Further from this, a stretch of this road does have a NSL with a number of bends. With consideration to the road conditions, taking cyclists off this road provides benefits and receives the highest score under 'safety'.

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												Biodiversity – In order to develop the proposed infrastructure, the removal of vegetation that adjoins the footway would be necessary – vegetation will likely include a cut verge and unkept shrubs, therefore a score of minus 2 is considered reasonable. Leisure – the proposed route will connect the village of Melton to Melton Riverside, which contains walks along the River Deben, and a handful of other PROWs including both bridleways and footpaths; therefore, a score of 2 is considered reasonable.
Melton	77	Melton Rd Woodbridge.	Road surface is very bumpy/rutted for the length from Pythches Rd junction to near Dock Lane junction, causing cyclists to ride erratically. This is a main through route for cyclists to the Suffolk Coastal region from Ipswich and surrounding areas.	Resurfacing	3	3	0	2	-1	2	9	The responder proposes resurfacing the B1438 between Pytches Road and Dock Lane; however, this is a Suffolk County Council issue. Instead, a cyclist/pedestrian path along the B1438 could be provided as an off-road alternative. Connectivity and Growth – The proposal would connect Woodbridge and Melton, which are both large and well-established settlements, consequently there is unlikely to be significant ‘everyday use’ due to both settlements having good levels of schools, shops, and employment opportunities. However, the B1438 resides along the Ipswich-Melton key corridor and connects to site allocation SCLP12.32. A score of 3 under this category is, therefore, considered reasonable.Modal Shift – PCT suggests that the B1438 would experience significant modal growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. As a bidirectional cycle track and footway could be provided, using PCT, there would be a potentially significant uplift, this warrants the highest score under this category. Optimisation – This proposal is for new infrastructure and does not therefore, optimise the existing.Safety – Removing cyclists of the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is a busy ‘b’ type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which create obstacles for cyclists. The proposal does, therefore, warrant a score of 2 under safety.Biodiversity – The proposal will likely have a resultant loss of managed grass areas and the small hedgerow fronting Melton primary school, therefore a small negative score under this category is considered reasonable.Leisure – The B1438 connects to a small handful of PROWs, which then extends through the AONB along the Deben Estuary; the proposal would connect to Melton Playing fields which is a locally used green space; and the B1438 forms part of the route to Woodbridge town centre which is a leisure attraction due to comparative shopping, drinking/eating establishments, and historical/cultural attractions. With consideration to the previous, a score of 2 is considered reasonable under this category.
Melton	88	Woodbridge to villages (this issue also applies to every town in Suffolk)	There are no safe cycle routes between Woodbridge and villages within a 15 miles radius. Where they exist few drivers keep to the 30mph limits and there are far to many stretches with just the National Speed Limit. On relatively narrow roads this leaves cyclists and pedestrians very close to vehicles doing up to 70mph. Safety concerns are a major reason that more people do not cycle or walk.	Create dedicated cycle and pedestrian routes to link villages with Ipswich. Where possible these routes should exclude vehicles except for access or have enforced speed limits. The routes should also have the sort of cycling safety features that Holland has introduced							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Melton	154	A12 Footpath north of Melton Roundabout, no	The A12 is a busy (& dangerous) road for cyclists...there is no dedicated cycle route out to Bredfield and the outlying north western villages (particularly from the point of view of cyclists travelling from those villages into Woodbridge and having to negotiate the A12 dual carriageway)	Consider upgrading (widening) the existing footpath that runs along the west side of the A12 to a combined foot/cycle path.	3	0	0	3	-1	1	6	Connectivity and Growth – the proposal would create a cycle route to a small handful of villages, which include Bredfield and Ufford, to Woodbridge/Melton. Although Bredfield has a small food shop within the village, it is likely the villages would rely on Woodbridge and Melton for key services – including the primary schools and the high schools. Therefore, a score of 3 under ‘Connectivity and Growth’ is considered reasonable. Modal Shift – according to PCT a shared pavement would result in significant modal shift. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing infrastructure. Safety – the A12 has a national speed limit and as a straight ‘A’ type road, volume and speed of traffic is likely high. With consideration to the road conditions, infrastructure that removes cyclists off the road scores significantly under ‘Safety’. Biodiversity – the proposal would result in the loss of grassed areas that are likely regularly cut and of limited benefit, however the path extends over a significant distance meaning a score of 1 is deemed reasonable. Leisure – the proposal would connect a handful of PROWs warranting a small score; however, this route likely has more commuter benefit rather than leisure benefit.
Melton	160	B1438 Woodbridge to Wickham Market	This is a direct route between the two towns, avoiding the A12 Dual Carriageway. Local traffic uses this road in preference to the A12. With increased housing being seen in Wickham traffic levels will rise hence increasing the vulnerability of cyclists using this route, Including any young persons wishing to cycle to/from school in Woodbridge.	Create a dedicated cycle lane the whole route, improve cycling related signage and reduce speed limits. Make Melton traffic lights a cycle friendly road junction and extend the cycle route up Woods lane to the Melton A12 roundabout (connect with existing cycle route/path). Continue the cycle route into Woodbridge via Melton hill as per other suggestions. Maybe connect it with a riverside foot/cycle path at Wilford Bridge	3	0	0	3	-2	1	5	The commenter proposes a cycleway along the stretch of the B1438 between Wickham Market and the B1438/A1152 crossroad where the cycleway should then continue along Woods Lane connecting to the existing infrastructure on the A12. Connectivity and Growth – The proposal will connect Wickham Market, Pettistree, Ufford, Melton, and Woodbridge. As the proposed connection would connect to Woodbridge, a key service centre that offers significant services that are not necessarily available in some of the other settlements, then a score of 3 is considered reasonable under this category. Modal Shift –It is unlikely that infrastructure can be delivered to the highest standard; therefore it is unlikely that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore optimise the existing. Safety – This section of the B1438, which is a busy ‘b’ type road, consists of 30mph, 40mph, and national speed limits; therefore, as the proposal would remove cyclists and walkers off a significantly hazardous road, a score of 3 is considered reasonable. Biodiversity – The delivery of the proposed infrastructure will likely have a resultant loss of loss of grassed areas, which are regularly cut and of limited benefit, and small hedges/trees. Therefore, a score of -2 is considered reasonable. Leisure – The proposal will likely have more connectivity and growth benefit then

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												leisure benefit, however the proposal would connect to numerous PROWs, which may be small leisure attractions, and would also connect into Woodbridge which has a key town centre. As the proposal does not connect directly into the town centre and as the infrastructure is unlikely going to be delivered to the highest standard, a score of 1 is considered reasonable.
Melton	164	Between Woods Lane lights, Melton to Bromeswell Roundabout to Sutton Hoo	Road is extremely busy, narrow and has blind bends. It is the only way into Woodbridge (and beyond) for cyclists coming from villages on Bawdsey peninsula and yet there is no cycling infrastructure. The stretch between Melton level crossing and the junction on the Hollesley and Alderton roads near Sutton Hoo are particularly dangerous for cyclists with cars overtaking on blind bends and not giving space to cyclists.	Cycle lanes on all roads into Woodbridge from surrounding villages.	3	3	0	3	-2	1	8	Connectivity and Growth – the proposal would connect to Melton Primary school, multiple employment sites, and Melton Train Station. Also, this route forms part of the Ipswich – Melton key corridor and will, therefore, help towards the completion of said corridor. With this in mind, a score of 3 is considered reasonable. Modal Shift – It is likely that a segregated off-road cycle option is viable along the A1152 between Melton Road/Woods Lane junction and Melton train station. Using PCT, this section of the A1152 shows a potential significant uplift, therefore a score of 3 is considered reasonable. Optimisation – this proposal does not optimise the existing infrastructure. Safety – the majority of the A1152 is straight with a 30mph speed limit; however, when travelling west, the speed limit changes to a NSL and the road has a few sharp bends. Furthermore, the road is a busy ‘A’ type road so, with consideration to the road conditions, a score of 3 under ‘safety’ is considered reasonable. Biodiversity – the proposal would likely result in the loss of well-kept grass areas, some wild verges, and other small shrubbery. Therefore, the proposal scores -2. Leisure – the proposal will likely have small leisure benefit as it connects to the Wilford Bridge and a handful of PROWs (including those along the river). However, the proposal will likely have more connectivity and growth benefit than leisure, therefore a score of 1 is considered reasonable.
Melton	176	access to woodbridge from Melton for cyclists.	The towpath between Melton and Woodbridge is pedestrians only. The road between Melton and Woodbridge is getting increasingly busy with many more parked cars, hazards for cyclists. A cycle path next to the pedestrian footpath along the river, or one wide enough for both would make access to Woodbridge practical for cyclists, decreasing parking needs and increase shoppers. A proper cycle path on the road between Melton primary and the thoroughfare would be an improvement, if not ideal.	described above	1	3	0	2	-1	3	8	Connectivity and Growth – the use of the tow path for cyclists would create a new connection between Melton and Woodbridge, which are large and well-established settlements, however there is unlikely to be significant everyday use due to both settlements having good levels of schools, shops, and employment opportunities. Being a tow path, the proposal will likely provide more leisure benefit than connectivity benefits, however a moderate score of 1 under this category is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The tow path, being located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a potential significant uplift, this warrants the highest score under this category. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Removing cyclists off the B1438 has clear safety benefits. Despite the B1438 having a 30mph speed limit, it is busy ‘b’ type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which form an obstacle. The proposal does, therefore, warrant a score of 2 under ‘safety’. Biodiversity – the Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – the proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Melton	200	North of Melton Old Church	Road frequently flooded. This is especially dangerous for cyclists because there are often potholes that cannot be seen under the water. Also there is a thick layer of mud along the centre of the road. This is an important route for those wishing to cycle between Ufford and Melton/Woodbridge.	Flooding and mud has been reported numerous times but SCC Highways have failed to provide any drainage.							N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Melton	206	Waterhead Lane 'Bridleway' Melton	This is a useful 'off road' cycle route for avoiding the Melton traffic lights area, however in places it is not very cycle/wheeled user friendly, the surface is uneven, rutted and overgrown with trees and bushes	Consider upgrading it to a hard surface bridleway making it suitable for mobility scooter users, people with prams and inexperienced / young cyclists.	0	0	1	0	0	1	2	Connectivity and Growth – The proposal is in regard to a bridleway; therefore, a connection already exists, and the proposal cannot score under this category. Modal Shift – There is insufficient evidence suggesting that resurfacing the bridleway will result in a significant modal shift. Optimisation – Resurfacing an existing bridleway is considered a moderate optimisation, therefore a score of 1 is considered reasonable. Safety – As this is a bridleway, the cyclists are already separate from the road and whilst the comment suggests it is in a poor condition this is more of a maintenance issue, improving the pathway doesn't improve safety. Biodiversity – No significant impact to biodiversity. Leisure – This bridleway forms part of the network of PROWs that reside along the Deben estuary and providing an improved surface will likely provide leisure access for a wider range of people, therefore a point is warranted in this category.
Melton	213	River Wall path between Wilford Bridge and Woodbridge	This is currently a footpath, but could be changed to allow bikes.	Keeping the current surface would help to limit bike speed. Having a green cycle route between Melton & Woodbridge would provide relief from the poor road conditions.	1	3	0	2	-1	3	8	The proposal is in regard to the network of PROWs that form the tow path between Wilford Bridge and Woodbridge. The proposal is to change the footpaths to bridleways in order to allow access to cyclists. Connectivity and Growth – the use of the tow path for cyclists would create a new connection between Melton and Woodbridge, which are large and well-established settlements, however there is unlikely to be significant everyday use due to both settlements having good levels of schools, shops, and employment opportunities. Being a tow path, the proposal will likely provide more leisure benefit than connectivity benefits, however a moderate score of 1 under this category is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between

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												Woodbridge and Melton. The tow path, being located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a potential significant uplift, this warrants the highest score under this category. Optimisation – This would represent a new route for cyclists as opposed to an optimisation. Safety – Again, this proposal will likely result in removing cyclists off the B1438 and this has safety benefits. Despite the B1438 having a 30mph speed limit, it is busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which form an obstacle. The proposal does, therefore, warrant a score of 2 under 'safety'. Biodiversity – the Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – the proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Melton	214	Woodbridge Riverside path, Elmhurst park to Wilford Bridge Section	There is no dedicated cycle route from Woodbridge Town centre to the Wilford Bridge (linking to beyond eg. Rendlesham, Rock Barracks etc.) Cyclists have to travel along the busy Melton Road to the Melton Traffic lights and then turn right on to the even busier A1152 towards the Wilford bridge, there is no segregated cycling provision making the route unsuitable for young or inexperienced cyclists.	Consider upgrading the Riverside path to a combined cycle/footpath, especially the bit from Elmhurst park to the Wilford bridge, this would miss out the roads completely. There is a primary school at the Melton traffic Lights which could benefit from a dedicated cycle route nearby	1	3	0	2	-1	3	8	The proposal is in regard to the network of PROWs that form the tow path between Wilford Bridge and Woodbridge. The proposal is to change the footpaths to bridleways in order to allow access to cyclists. Connectivity and Growth – the use of the tow path for cyclists would create a new connection between Melton and Woodbridge, which are large and well-established settlements, however there is unlikely to be significant everyday use due to both settlements having good levels of schools, shops, and employment opportunities. Being a tow path, the proposal will likely provide more leisure benefit than connectivity benefits, however a moderate score of 1 under this category is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The tow path, being located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a potential significant uplift, this warrants the highest score under this category. Optimisation – This would represent a new route for cyclists as opposed to an optimisation. Safety – Again, this proposal will likely result in removing cyclists off the B1438 and this has safety benefits. Despite the B1438 having a 30mph speed limit, it is busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which form an obstacle. The proposal does, therefore, warrant a score of 2 under 'safety'. Biodiversity – the Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – the proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Melton	268	The pedestrian crossing island near Pytches Road does not give priority to cyclists who feel vulnerable as motorists try to narrowly overtake even if cyclists take up a central position to prevent this. Nicknamed "Cycle crushers"	Problem is cars overtaking cyclists too narrowly through the gap between the island. Either spend lots of money, like the Dutch, on engineering a proper cycle way or put a sign up giving cyclists priority over motorists. I have been the victim of a road rage incident here. The Police blamed me for hogging the road. I was preserving my life.	Highway code change imminent to support cyclists who take up central position? Sign to prioritise Cyclists. Better (eg more expensive) planning/cycle way engineering as in NLs.	3	3	0	2	-1	2	9	For the purpose of this assessment, the implementation of an off-road cycleway/footway along the B1438 will be explored. Connectivity and Growth – The proposal would connect Woodbridge and Melton, which are both large and well-established settlements, consequently there is unlikely to be significant 'everyday use' due to both settlements having good levels of schools, shops, and employment opportunities. However, the B1438 resides along the Ipswich-Melton key corridor and connects to site allocation SCLP12.32. A score of 3 under this category is, therefore, considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. As a bidirectional cycle track and footway could be provided, using PCT, there would be a potential significant uplift, this warrants the highest score under this category. Optimisation – This proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Removing cyclists of the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is a busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which create an obstacle. The proposal does, therefore, warrant a score of 2 under safety. Biodiversity – The proposal will likely have a resultant loss of managed grass areas and the small hedgerow fronting Melton primary school, therefore a small negative score under this category is considered reasonable. Leisure – The B1438 connects to a small handful of PROWs, which then extends through the AONB along the Deben Estuary; the proposal would connect to Melton Playing fields which is a locally used green space; and the B1438 forms part of the route to Woodbridge town centre which is a leisure attraction due to comparative shopping, drinking/eating establishments, and historical/cultural attractions. With consideration to the previous, a score of 2 is considered reasonable under this category.
Melton	326	New Housing development, Woods Lane Woodbridge	Example of where significant new housing has been allowed without provision for safe cycling to the local shops, centre of Woodbridge and the local primary school. The housing is disconnected from Woodbridge by the A12 & busy Woods lane, necessitating car ownership to access local services.	1). Upgrade the footpath along Bredfield Road into Woodbridge to cycle/footpath standard.2.) Create a cycle route down Woods lane to the Melton Traffic lights to connect with Melton Road	2	1	0	2	-2	1	4	CandG – The proposal would connect Woodbridge and Melton, which are both large and well-established settlement areas, however there is unlikely to be significant 'everyday use' due to both settlements having good levels of schools, shops, and employment opportunities. A score of 2 under this category is deemed appropriate as Woods Lane and Bredfield Road reside in the Ipswich – Melton key corridor and the proposal would connect to the existing cycling network along the A12. Modal Shift –

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												It is unlikely that infrastructure could be delivered to the highest standard on Woods Lane; consequently the proposal will unlikely lead to a significant modal shift. However, it may be viable to deliver a bidirectional cycle track and footway along Bredfield Road which, according to PCT, would lead to a moderate modal shift. Therefore, a score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Although both Woods Lane and Bredfield Road have 30mph speed limits, a score of 2 is likely more appropriate as Woods Lane is an ‘A’ type road and speed and volume of traffic is likely high, therefore removing cyclist off this road has safety benefits. Biodiversity – The proposal will likely result in the removal of managed grassed areas along both roads and the removal of small hedgerows along Woods Lane. Leisure – The proposal will likely have a small leisure benefit as it would connect to a small handful of PROWs along Woods Lane.
Melton	364	Road over Wilford Bridge	Road is narrow and busy and cars sometimes drive very close to cyclists.	Would be very useful to have a cycle path off-road to allow safer access to the coast / Bromeswell.	1	0	0	3	-2	2	4	Connectivity and Growth – The proposal provides a connection to a small handful of PROWs and to Melton railway station; however, it provides limited connections to other villages and services. Therefore, a score of 1 under this category is considered reasonable. Modal Shift – It is unlikely that infrastructure could be delivered to the highest standard; therefore, the proposal would not result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Wilford Bridge Road is a narrow ‘A’ type road; therefore, volume and speed of traffic is likely high. Further from this, a stretch of this road does have a national speed limit and some bends. With consideration to the road conditions, taking cyclists off this road receives the highest score under ‘safety’. Biodiversity – In order to develop the proposed infrastructure, the removal of vegetation that adjoins the footway would be necessary – vegetation will likely include a cut verge and wild shrubs/verges, therefore a score of -2 is considered reasonable. Leisure – The proposed route will connect the village of Melton to Melton Riverside, which contains walks along the River Deben, and a handful of other PROWs including both bridleways and footpaths; therefore, a score of 2 is considered reasonable.
Melton	392	New Street, Woodbridge	Introduce a 20mph speed limit throughout the centre of Woodbridge. Divert through traffic away from New Street. Introduce a chicane half way down New Street to slow the traffic.		2	0	0	1	0	2	5	The suggestion is to add modal filters to direct traffic away from New Street. This would make it more user friendly for cyclists and walkers who wish to walk into Woodbridge town centre. Connectivity and Growth – New Street is a direct route into Woodbridge town centre, which is a strategically important area, and contains a number of key services, but any modal filter to direct traffic away from this route will not remove traffic entirely so a score of 2 is considered reasonable. Modal Shift – The proposal will unlikely cause a significant modal shift. Optimisation – This does not optimise existing cycle infrastructure nor provides improvements to the pavement. Safety – The road has a 30mph speed limit and the proposal will likely provide a modest safety benefit to an already relatively safe road, hence a score of 1 under this category. Biodiversity – No significant biodiversity impact. Leisure – Again, the proposal would connect into Woodbridge town centre which is a leisure attraction, however any modal filter to direct traffic away from this route will not remove traffic entirely so a score of 2 is considered reasonable.
Melton	395	Melton and Woodbridge	Aside from cycling in the parks and A12 (cycle path) there are no family friendly or safe routes. No exclusive cycling options. I feel the narrow streets and way people drive is unsafe for children of primary age to cycle. Exclusive areas would improve children's and parents confidence and encourage families to get on bikes.	Research locations for family safe cycling routes and designate land where you could create this. Partner with land owners.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. Melton and Woodbridge form part of the key corridor so have been subject to a detailed assessment.
Melton	420	Station Road Melton	This is part of the main pedestrian route through the village. In places, the pavement is less than 1m wide. The road is used on a daily basis by HGVs and agricultural vehicles. This is not safe and is very polluting.	Work with other authorities e.g. Suffolk County Council to introduce weight/width restrictions. Work with satnav providers to direct heavy vehicles to more suitable routes.	0	0	0	1	0	0	1	Connectivity and Growth – As the proposal restricts HGVs, it does not make the route traffic-free and will unlikely, therefore, provide significant connectivity and growth benefits. Modal Shift – The proposal will unlikely result in a significant modal shift. Optimisation – The proposal is not improving existing infrastructure and does not, therefore, score under this category. Safety – The proposal will likely provide modest safety benefits. Station Road is 30mph and is narrow in places, however it is unlikely a significantly busy road. Therefore, a score of 1 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – No leisure benefit.
Melton	463	The roundabout top of Woods Lane / A12	1. impossible to see oncoming traffic coming from south on A12 when crossing A12 on the path from the north 2. Impossible to see oncoming traffic when crossing Woods Lane from North to South on the path	In both instances, the path could be closer to the roundabout	0	0	0	1	0	0	1	The commenter suggests that the segregation of the pathways from the road surrounding the A12/A1152 roundabout reduces visibility when crossing. The commenter proposes, therefore, that the pathway should be moved to be closer to the roundabout. Connectivity and Growth – The proposed alteration does not create additional connectivity. Modal Shift – The proposal would not result in a significant modal shift. Optimisation – Although minimal, the proposal will likely provide some benefit, however its impact on the wider route/network is minimal hence a score of 0. Safety – The proposal will likely provide small safety benefit to an already relatively safe route, therefore a score of 1 is deemed reasonable. Biodiversity – No significant biodiversity impact. Leisure – Again, connection already exists so will unlikely result in additional leisure benefit.

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Melton	464	river path woodbridge to Melton	little room for both pedestrian and cyclist although most cyclists dismount for pedestrians	where the path splits into 2 levels, make one for cyclists and one for pedestrians. Visiting cyclists to woodbridge cannot believe cyclists are not allowed along the whole of the river path	1	3	0	2	-1	3	8	The commenter proposes that the tow path, where it splits into two, should allow cyclist access; however, for the purpose of this assessment, segregated cyclist access for the entirety of the tow path (between Melton and Woodbridge) was assessed. Connectivity and Growth – The use of the tow path for cyclists would create a new connection between Melton and Woodbridge, which are large and well-established settlements, however there is unlikely to be significant everyday use due to both settlements having good levels of schools, shops, and employment opportunities. Being a tow path, the proposal will likely provide more leisure benefit than connectivity benefits, however a moderate score of 1 under this category is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The tow path, being located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a potentially significant uplift, this warrants the highest score under this category. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Again, the tow path is a viable alternative route to the B1438 (Melton Road). Removing cyclists off the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which create an obstacle. The proposal does, therefore, warrant a score of 2 under 'safety'. Biodiversity – The Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – The proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Melton	467	Footpath alongside Woods Lane heading down towards Melton traffic lights.	Observed Farlingaye School students going home to Melton village. Some were walking, others cycling. There were also other pedestrians. Those on bikes had chosen to ride on the pavement as the road is busy and often has large vehicles and is not wide. It is therefore safer on the pavement. However the pavement is not wide enough to accommodate everyone safely. The problem is aggravated by the steepness of the hill. I am a regular cyclist and don't use Woods Lane.	1. Find an alternative safe route for school children who live in Melton village and beyond. This might involve new permissive paths, resurfacing, etc. Basically Woods Lane is unsafe for cyclists. 2. Have a proper dedicated cycle lane. This would probably involve widening the road or the pavement.	2	0	0	2	-2	1	3	Connectivity and Growth – The proposal would connect Melton and Woodbridge, which are both large and well-established settlements, however there is unlikely going to be significant 'everyday use' due to both settlements having good levels of schools, shops, and employment opportunities. As the proposal would also connect into existing cycling and walking infrastructure along the A12 and as the western side of Woods Lane resides within the Ipswich-Melton key corridor, a score of 2 is considered reasonable. Modal Shift – It is unlikely that infrastructure can be delivered to the highest standard; therefore, the proposal will unlikely result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Despite Woods Lane being 30mph, it is a 'A' type road and is significantly busy, therefore the proposal has safety benefits. A score of 2 is deemed reasonable. Biodiversity – The proposal will likely result in the loss of wild verges and small trees along the southern side of the road, therefore a resultant score of -2 is reasonable. Leisure – The proposal would connect to a small handful of PROWs which connect into Woods Lane; therefore, the proposal has small leisure benefit and a score of 1 is considered reasonable.
Melton	479	Wilford Bridge Road leading onto Sutton Road onwards	In an ideal world separate coned cycle lanes would be in operation but due to roads being too narrow and in order for cyclists to feel reasonably safe, speed limits must be reduced for motorised traffic from 60 mph to 40 mph maximum on rural roads between 30 mph towns and villages to help avoid potentially fatal accidents involving cyclists and horse riders too. Ultimately we want more people on bicycles for commuting as well as leisure but safety is paramount if this is to happen.	As above. Will obviously also benefit pedestrians/those trying to cross increasingly busy roads.							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Melton	489	Riduna Park / East Suffolk Council Offices / Melton Train Station	There is no sign of any dedicated cycling infrastructure connecting East Suffolk Councils Offices & Riduna Park or Melton Train Station to central Woodbridge and other residential areas within the town. Anyone wishing to cycle to & from must do via a busy A road.	Widen the footpaths along Wilford Bridge Road and a cycle lane into woodbridge	3	3	0	2	0	2	10	Connectivity and Growth – The proposal will likely have significant connectivity and growth benefits as not only does Wilford Bridge Road form part of the Ipswich-Melton key corridor, but the proposal will connect to Melton train station, employment sites, and Melton Primary school. With consideration to the previous, a score of 3 is considered reasonable. Modal Shift – It is likely that infrastructure along this road could be delivered to the highest standard; therefore, using PCT the proposal will potentially result in a significant modal shift hence a score of 3 under this scoring category. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Although Wilford Bridge Road has a 30mph speed limit, it is an 'A' type road, therefore volume and speed of traffic is likely high. The proposal does have reasonable benefit as it removes cyclists off a road that is sufficiently hazardous. Biodiversity – The proposal will likely result in the loss of grassed areas that appear regularly cut and of limited benefit. Leisure – As the route connects into Melton Riverside, which likely has significant leisure value, the proposal has clear leisure benefits. It is likely that the proposal will

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												have more 'connectivity and growth' value than 'leisure', therefore a score of 2 is considered reasonable.
Melton	504	A1152 & Wilford Bridge	Lack of a cycle path, Melton traffic lights to Bromeswell Quiet lanes...	Having cycled along the footpaths on this route, there does seem to be enough room on the verge to widen the existing footpaths to create a cycle/footpath pretty much all the way along, past the station and across the bridge and round to the Bromeswell 'Quiet lane'	3	0	0	3	-3	2	5	Connectivity and Growth – The proposal would connect Bromeswell to Melton/Woodbridge, which is a key service centre, therefore there will likely be significant 'every-day' use. Modal Shift – It is unlikely that infrastructure can be delivered to the highest standard throughout the route; therefore, the proposal would not result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The A1152 has a national speed limit and as an 'A' type road, volume and speed of traffic is likely high. Getting both pedestrians and cyclists off the road will have a significant safety benefit. Biodiversity – The proposal will likely result in significant biodiversity losses including established hedgerows, small trees, and wild verges. Leisure – The proposal would connect Bromeswell and Melton to Melton Riverside, which contains walks along the River Deben, and a handful of other PROWs including both bridleways and footpaths. It is likely that the route will, however, have more connectivity and growth benefit than leisure. Therefore, a score of 2 is considered reasonable.
Melton	505	Riduna Park. Woodbridge	Example of new industrial development with...No obvious cycle parking facilities for...1) Members of the Public Visiting East Suffolk Council Offices2) Employees cycling to work at each unit2) Cyclists wishing to use units providing food and drink such as Honey & Harveys.	1) Encourage developers to give up one car parking space per unit as a dedicated cycle parking space with stands or provide secure storage as per the Councils own staff facility.2) Encourage developers to give up unit space to a dedicated indoor bike storage space including showers and lockers. This could be a shared facility for all on the park3) A few sheffield stands outside the front door of the Council Offices would be useful and look good to passers by. Include a dropped kerb at the roadside end of the main entrance path so that disabled users / buggies can easily access it from the Melton direction.	0	1	0	0	0	0	1	Connectivity and Growth – No significant connectivity and growth impacts.Modal Shift – Cycle parking alone is unlikely to encourage large numbers of modal shift, but a certain level will be provided so a score of 1 is deemed appropriate.Optimisation – The proposal does not optimise existing infrastructure.Safety – No significant safety benefit.Biodiversity – No significant biodiversity benefit.Leisure – No significant leisure benefit.
Melton	506	Melton	Well done to Melton Parish Council for converting this short length of footpath into a cycle/footpath. It might win the prize for the shortest cyclepath in East Suffolk but it is an example of where a small 'parish council' have been able to upgrade the designation of a footpath to a cyclepath.	East Suffolk DC to proactively support and encourage Parish Councils to upgrade footpaths to foot/cycle paths.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Melton	514	Road between Woodbridge Thoroughfare and Melton cross roads	The all day parking on both sides has reduced the width of this road by about half. Mostly shoppers or commuters are seeking to travel but the all day parkers are an obstruction and a danger to any under aware pedestrian. The other day I had an appointment in Common Lane, Melton and the traffic was gridlocked, from Woodbridge to Melton. I thought there must have been an accident but no. On the bike I was able to nimble past them it was a ridiculous situation.	Is it time for bikes only for trips under 10 miles? Some days a week. It is moving that way.	3	3	0	2	-1	2	9	The commenter proposes a 'bikes only' rule for trips under 10 miles, this falls outside the remit of the project. For the purpose of this assessment, the implementation of a segregated cycleway along Melton Road will be assessed. Connectivity and Growth – The proposal would connect Woodbridge and Melton, which are both large and well-established settlements, consequently there is unlikely to be significant 'everyday use' due to both settlements having good levels of schools, shops, and employment opportunities. However, the B1438 resides along the Ipswich-Melton key corridor and connects to site allocation SCLP12.32. A score of 3 under this category is, therefore, considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. As a bidirectional cycle track and footway could be provided, using PCT, there would be a potentially significant uplift, this warrants the highest score under this category. Optimisation – This proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Removing cyclists of the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is a busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which creates an obstacle. The proposal does, therefore, warrant a score of 2 under safety. Biodiversity – The proposal will likely have a resultant loss of managed grass areas and the small hedgerow fronting Melton primary school, therefore a small negative score under this category is considered reasonable. Leisure – The B1438 connects to a small handful of PROWs, which then extends through the AONB along the Deben Estuary; the proposal would connect to Melton Playing fields which is a locally used green space; and the B1438 forms part of the route to Woodbridge town centre, which is a leisure attraction due to comparative shopping, drinking/eating establishments, and historical/cultural attractions. With consideration to the previous, a score of 2 is considered reasonable under this category.
Melton	530	The junction with The Street/Wiford Bridge and Melton Hill Road	The crossing from The Street to the primary school is very narrow and there is considerable congestion during school hours. The traffic is also very heavy at these times, The Street should have light vehicles only using the road between Woodbridge and Ufford except for access to and from business in the area. As a walker I have nearly been struck several times by large vehicles passing along the road close to the pavement	Re landscape grass verges on the junctions with the lights and the crossings to Melton Primary School. Erect sign asking motorists to switch of engines when idling by lights. Prohibit large vehicles from using the road between Woodbridge, Melton and Ufford unless for delivery only to local business.	0	0	1	1	0	0	2	The commenter proposes restricting HGV access along The Street (B1438) and Melton Road (B1438) for the safety of pedestrians and cyclists utilising the route. Moreover, the widening of the crossing points was proposed as the existing crossing point is narrow. Connectivity and Growth – As the proposal restricts HGVs, it does not make the route traffic-free and will unlikely, therefore, provide significant connectivity and growth benefits. In terms of the crossing points, the proposal is considered an optimisation not a new connection. Modal Shift – The proposal will unlikely result in a significant modal shift. Optimisation – The proposal will result in the widening of the crossing points which is

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												considered a minor optimisation, therefore a small score of 1 is considered reasonable. Safety – The B1438, although a 30mph road, is a busy 'B' type road and it is likely that the proposal will have minor safety benefit, therefore a score of 1 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – No significant Leisure benefit.
Melton	532	Improve public footpath signs for walking between Melton and Woodbridge from Melton Fields	Lack of clear signs and way marks inviting people to walk away from road along footpath from Melton Fields to Woodbridge	Provide waymarks and show distance between Melton Fields and Woodbridge as part of exercise and well being campaign	0	0	1	0	0	0	1	Connectivity and Growth – No significant connectivity benefit. Modal Shift – The change is not considered to create significant modal shift. Optimisation – Although the route is not improved, the addition of signage represents a modest optimisation so scores 1 point. Safety – The proposal is not considered to have a safety benefit. Biodiversity – No significant impact on biodiversity. Leisure – Although the path has some leisure benefits, the signage is not deemed to have a significant score.
Melton	544	Melton Road / Melton Hill	Cycling into Woodbridge via Melton or the A12 is too unsafe or unpleasant. When cycling along past the Coach & Horses at Melton you have to pass numerous parked cars and twice now I have nearly been knocked off my bike by stationary motorists opening their doors. Also, as the incline steepens (near the old council offices) there are numerous cars parked on both sides of the road so, as a cyclist, you become something of an impediment to traffic because you tend to slow down as the hill steepens.	It is too far for me to walk (in terms of time) from Ufford to Woodbridge but I would frequently cycle IF there was a safer/pleasant route. The ideal solution, from my perspective, would be to create a cycle path along the riverbank but from the comments about this on Nextdoor.com it's easy to see that this is controversial topic! I do believe however that if the path was widened walkers and cyclists could amicably share the space. It would need the council to make clear that the route is legally open to walkers and cyclists. https://nextdoor.co.uk/news_feed/?post=17592194269906&comment=175922052359271 would really welcome a cycle path all the way along the riverside to Martlesham Creek - creating a sustainable transport option to the Martlesham retail sites.	1	3	0	2	-1	3	8	Connectivity and Growth – The proposal would create a new connection between Melton, Woodbridge, and Martlesham, which are large and well-established settlements, however there is unlikely going to be significant everyday use due to both settlements having good levels of schools, shops, and employment opportunities. Being a river path, the proposal will likely provide more leisure benefit that connectivity benefit, however a moderate score of 1 is considered reasonable.Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The tow path, being located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a potentially significant uplift, this warrants the highest score under this category.Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing.Safety – Again, the tow path is a viable alternative route to the B1438. Removing cyclists off the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which create an obstacle. The proposal does, therefore, warrant a score of 2 under 'safety'.Biodiversity – The Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity.Leisure– The proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Melton	563	Melton Rd, Woodbridge to Melton	Cycle use of this road is dangerous. Cars move too fast and the road has no cycle lanes.	20 mph speed limit would be helpful here. Purpose built cycle path ideally, until then marked cycle lanes on the road.	2	0	0	1	0	1	4	The commenter proposes a 20mph speed limit along Melton Road, however this is outside the remit of the project and should be passed through to SCC. However, the commenter also suggested cycle lanes along Melton Road. Connectivity and Growth – The proposal would connect Woodbridge and Melton, which are both large and well-established settlement areas, consequently there is unlikely to be significant 'everyday' use due to both settlements have good levels of schools, shops, and employment opportunities. However, the B1438 resides within the Ipswich-Melton key corridor and connects to site allocation SCLP12.32. As this proposal is for on-road infrastructure, a score of 2 is considered reasonable. Modal Shift – As on-road cycle lanes are not considered a high standard infrastructure, the proposal would not result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Despite the B1438 having a 30mph speed limit, it is a busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which create an obstacle. As the proposal will not remove cyclists off the road, a score of 1 under safety is considered reasonable. Biodiversity – No biodiversity impact. Leisure – The B1438 connects to a small handful of PROWs which extend through the AONB along the Deben Estuary; the proposal would connect to the Melton playing fields, which is a locally used green space; and the B1438 forms part of the route to Woodbridge town centre, which is a leisure attraction due to comparative shopping, drinking/eating establishments, and historical/cultural attractions. However, as the proposal is for on-road infrastructure, a score of 1 under this category is considered reasonable.
Melton	564	The road from Melton cross roads to Sutton Hoo has very poor cycle access.	This is a very busy route. Cyclists are an endangered species.	In the interest of increasing cycle access to Sutton Hoo there should be marked cycle lanes with signs, from the traffic lights at Melton crossroad all the way to Sutton Hoo.	2	0	0	2	0	2	6	Connectivity and Growth – The proposal will likely have significant connectivity and growth benefits as not only does Wilford Bridge Road form part of the Ipswich-Melton key corridor, but the proposal will connect to Melton train station, employment sites, Melton Primary school, and Sutton Hoo. However, as the suggested improvement is of a poor quality, a score of 2 is considered reasonable. Modal Shift – As the proposal is for cycle lanes, which PCT suggested that the proposal would not lead to a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Although the majority of Wilford Bridge Road is 30mph, heading eastbound it becomes NSL and this continues along the B1083 towards Sutton Hoo. As 'B' and 'A' type roads, speed and volume of traffic is likely high. Getting cyclists and walkers off road will have significant safety benefit, however cycle lanes will unlikely completely address the concern raised hence a score of 2.

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												Biodiversity – No biodiversity impact. Leisure – As the proposal will connect to Sutton Hoo, which is a leisure attraction, and to a handful of PROWs, a score of 2 is considered reasonable.
Melton	567	Melton Road between The Thoroughfare, Woodbridge, and Melton Traffic lights at junction of A1152	This is a popular route for cycles as it's the only way to get from Woodbridge to Melton and across to the Bawsey peninsular. The road is dangerous for cyclists because there is no space for them. It is heavily used by vehicular traffic and parked cars on the route are a real problem, since car doors can open suddenly as cycles attempt to pass.	A purpose built cycle path kept clear of parked cars. 20mph speed limit for motor vehicles.	3	3	0	2	-1	2	9	Connectivity and Growth – The proposal would connect Woodbridge and Melton, which are both large and well-established settlements, consequently there is unlikely to be significant 'everyday use' due to both settlements having good levels of schools, shops, and employment opportunities. However, the B1438 resides along the Ipswich-Melton key corridor and connects to site allocation SCLP12.32. A score of 3 under this category is, therefore, considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. As a bidirectional cycle track and footway could be provided, using PCT, there would be a potentially significant uplift, this warrants the highest score under this category. Optimisation – This proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Removing cyclists of the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is a busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars creating an obstacle. The proposal does, therefore, warrant a score of 2 under safety. Biodiversity – The proposal will likely have a resultant loss of managed grass areas and the small hedgerow fronting Melton primary school, therefore a small negative score under this category is considered reasonable. Leisure – The B1438 connects to a small handful of PROWs, which then extends through the AONB along the Deben Estuary; the proposal would connect to Melton Playing fields which is a locally used green space; and the B1438 forms part of the route to Woodbridge town centre which is a leisure attraction due to comparative shopping, drinking/eating establishments, and historical/cultural attractions. With consideration to the previous, a score of 2 is considered reasonable under this category.
Melton	584	Woods Lane	Children use this route for cycling from Melton to Farlingaye school. It is very busy with huge lorries coming to and from Rendlesham Bentwaters. Needs shared cycle/footway or cycle Lane to make safer for cyclists.		2	0	0	2	-2	1	3	Connectivity and Growth – The proposal would connect Melton and Woodbridge, which are both large and well-established settlements, however there is unlikely going to be significant 'everyday use' due to both settlements having good levels of schools, shops, and employment opportunities. As the proposal would also connect into existing cycling and walking infrastructure along the A12 and as the western side of Woods Lane resides within the Ipswich-Melton key corridor, a score of 2 is considered reasonable. Modal Shift – It is unlikely that infrastructure can be delivered to the highest standard; therefore, the proposal will unlikely result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Despite Woods Lane being 30mph, it is a 'A' type road and is significantly busy, therefore the proposal has safety benefits. A score of 2 is deemed reasonable. Biodiversity – The proposal will likely result in the loss of wild verges and small trees along the southern side of the road, therefore a resultant score of -2 is reasonable. Leisure – The proposal would connect to a small handful of PROWs which connect into Woods Lane; therefore, the proposal has small leisure benefit and a score of 1 is considered reasonable.
Melton	589	Wilford Bridge Road, between Melton Station and the roundabout	Wilford Bridge Road - in particular between Melton station and the roundabout, is becoming increasingly busy, with large amounts of lorry traffic. It is the only access route to the peninsula for cyclists and is extremely narrow and congested.	Cycle lane to be added	1	0	0	2	0	1	4	Connectivity and Growth – The proposal provides a connection to a small handful of PROWs and to Melton railway station; however, it provides limited connections to other villages and services. Therefore, a score of 1 under this category is considered reasonable. Modal Shift – The proposal is for cycle lanes; therefore, the proposal would not result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Wilford Bridge Road is a narrow 'A' type road; therefore, volume and speed of traffic is likely high. Further from this, a stretch of this road does have a NSL and there are a few bends, a score of 2 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – The proposed route will connect the village of Melton to Melton Riverside, which contains walks along the River Deben, and a handful of other PROWs including both bridleways and footpaths. However, as the proposal is of poor quality, a score of 1 is considered reasonable.
Melton	592	GR 267504 Immediately north of roundabout A12/ 52	Dangerous to cross A12 to reach cycle way beside the A12	A Toucan Crossing. Also resurface & remove foliage from cycle way	1	0	0	3	0	0	4	Connectivity and Growth – The A12 has NSL and is a modest barrier for those situated on either side and there does not appear to be an existing pedestrian crossing along this section of the A12. However, as there are a limited number of destinations either side, a score of 1 under this category is considered reasonable. Modal Shift – there is insufficient evidence that the proposal would lead to a modal shift. Optimisation – the proposal does not improve existing infrastructure. Safety – This stretch of the A12 has a NSL, straight, and is considerably busy. Therefore, as a toucan crossing would remove cyclists and walkers off road, a score of 3 under safety is considered reasonable. Biodiversity – the proposal will not have a significant biodiversity impact. Leisure – the proposal has limited leisure benefit. The commenter also proposes resurfacing of the cycleway; however, this is a maintenance issue and should be passed on to SCC.

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Melton	592	GR 267504 Immediately north of roundabout A12/ 52	Dangerous to cross A12 to reach cycle way beside the A12	The second part of the comment including resurfacing and removing foliage from the cycleway. Removing foliage is outside the remit of the project. It has been considered that the resurfacing means improved surface with marked segregation on the cycleway south of the roundabout.	0	1	1	0	0	0	2	Connectivity and Growth - Connectivity already exists so the impact will likely only be minimal. Modal Shift - Improving the path to the higher standard will have modest modal shift benefit. Optimisation - The pathway is already a shared path, but providing pedestrian/cycling segregation will provide modest optimisation. Safety - A shared pathway immediately south of the roundabout already exists so it is not a significant safety matter. Biodiversity - No significant biodiversity impact. If the path requires widening some grass may be lost. Leisure - No significant leisure benefit.
Melton	593	GR 282 504 to GR 294 496	Risky shared pedestrian & cycle way from Melton lights over rails, Wilford Bridge and up hill to access Bawdsey Peninsula.	Widen shared way that is beside busy highway and provide some safe crossing at Riduna and the A1152/B1083 roundabout.	2	0	0	3	-3	3	5	Connectivity and Growth – The proposal provides a connection to Melton railway station and to a small handful of villages that are situated adjacent to the B1083 (Sutton, Shottisham, Alderton, and Bawdsey), however the route to most of these villages exceeds the 8km cyclist average so there is unlikely to be ‘everyday’ use. As the proposal will likely have more leisure benefit than connectivity and growth benefit, a score of 2 is considered reasonable. Modal Shift – PCT suggests that the A1152 is currently moderately used by cyclists, however, as the proposal is not for infrastructure of the highest standard, it suggests that there would not be a significant modal shift. In terms of the B1083, PCT suggests that use is predominantly at a minimum and the proposal would not significantly change this. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Wilford Bridge Road is a narrow ‘A’ type road; therefore, volume and speed of traffic is likely high. Further from this, this stretch of the A1152 does have a NSL and there are a few bends, therefore the proposal of a crossing and a shared path will likely provide safety benefit. Although the B1083 is slightly wider, the road is predominantly similar to that of Wilford Bridge Road. With consideration to the previous a score of 3 is considered reasonable. Providing suitable crossing points will only add credence to this high score. Biodiversity – In order to develop the proposed infrastructure, the removal of vegetation that adjoins the footway along the A1052 would be necessary – vegetation will likely include a cut verge and wild shrubs/verges. Moreover, the removal of established hedgerows along the B1083 would also likely be necessary for the delivery of infrastructure. A score of -3 is considered reasonable. Leisure – The proposal would connect into Bawdsey peninsula which, being a beach, is a key strategic location; Sutton Hoo which will also be a significant leisure attraction; and a handful of PROWs including those that adjoin the River Deben. Therefore, a score of 3 under leisure is considered reasonable.
Melton	609	General	Encourage a cycle lock or loop fixed to walls outside certain shops, where appropriate. Invest in wider recreational cycle route creation to enhance the area for local cyclists, pedestrians and (staycation) tourism. (ie river wall route from Wilford Bridge to Felixstowe Ferry).	Further interconnection between towns and villages of the area, including tackling awkward areas where there is seemingly less space for cycle paths, such as from the outskirts of Woodbridge towards Martlesham where routes into Ipswich are found.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Melton	622	The Street, Melton	This is a historic route. The road is narrow and so are the pavements. Many of the buildings are hard against the pavement. At peak times, the vehicles are nose to tail. Pedestrians, including families on their way to school, have to run the gauntlet between the vehicles and the buildings, wreathed in exhaust fumes.	Measure the air pollution in real time to better understand the scale of the problem. Make The Street a no idling zone.							N/A	This is not within the remit of the project but will be brought to the attention of the relevant body.
Melton	633	Woods Lane, Junction with A12	With increasing traffic on Woods Lane trying to cross the road at this point is difficult / dangerous at times especially for the less abled. The footpath crosses the road at this point via gaps in the verge, it is not highlighted as a crossing point to drivers. The footpath is also designated for cycles on the Ipswich side of the road, but not the north bound side.	Provide a proper pedestrian and cycle crossing at this point, continue the cycle path up the A12 to where it then crosses it.	1	0	0	1	0	0	2	Connectivity and Growth – The road represents a modest barrier between those situated on either side, but as a 30mph road it is crossable. There are a limited number of destinations to the north, however a crossing would provide a safe connection to the existing pedestrian infrastructure, scoring it a 1 under connectivity and growth. Modal Shift – The proposal would not lead to a significant modal shift. Optimisation – The proposal does not optimise existing infrastructure. Safety – The A1152 is a 30mph road but is relatively busy, therefore the proposal has been awarded 1 point under safety. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion provides limited leisure benefit.
Melton	634	A12 Approach to Melton Junction	Crossing the A12 using the path at this point is difficult / dangerous and involves crossing three lanes of fast moving traffic. The lack of an adequate crossing point here and on the A1152 entry effectively cuts the paths in half and deters walkers & cyclists from using the A12 north bound path towards Bredfield and Debach. It would allow children to cycle from the villages to school at Farlingaye & in Woodbridge.	Provide a suitable crossing on the A12 at this point & A1152 Entry Connect the A12 north going path with the A12 south side cycle route to Farlingaye. Upgrade paths to Cycle / footpaths.	1	0	0	2	0	0	3	Connectivity and Growth – The A12 is busy road with a NSL and represents a modest barrier between those situated on either side. Despite having a limited number of destinations either side of the road, the proposal would provide a safe connection to the existing pedestrian infrastructure, scoring it a 1 under connectivity and growth. Modal Shift – The proposal would not lead to a significant modal shift. Optimisation – The proposal does not optimise existing infrastructure. Safety – This stretch of the A12 has a NSL, straight, and is considerably busy but a crossing point will not address the concern raised. Therefore, a score of 2 under safety is considered reasonable. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion provides limited leisure benefits.
Melton	642	River wall footpath from near Wilford Bridge to Martlesham	The path is narrow, in some parts hard to comply with social distancing. With steep slopes either side, often walked by children and elderly, sometimes even crowded, dogs on and off leads (either of which being potentially tricky for cyclists) - it is not safe for dual use at present. Not all pedestrians expect the presence of cyclists, cyclists need pedestrians to step aside, and to keep their dogs out of their way etc.	If the route is to be improved for cyclists, ideally the track should be separate from the pedestrian path. Meanwhile and as soon as possible: - make a decision about path etiquette, - Clarify with notices to users, sited at the path (as soon as possible and regardless of any future decision on improvement): whether or not cyclists are permitted to cycle on this route as it is. If they are already permitted, please make it clear that cyclists must dismount when passing	1	3	0	2	-1	3	8	Connectivity and Growth – The proposal would create a new connection between Melton and Woodbridge, and Martlesham, which are large and well-established settlements, however there is unlikely going to be significant everyday use due to both settlements having good levels of schools, shops, and employment opportunities. Being a river path, the proposal will likely have more leisure benefit than connectivity benefit, however a moderate score of 1 is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The tow path, being

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				pedestrians. In the interests of clarity and safety, this cannot be left to individual judgment.								located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a potentially significant uplift, this warrants the highest score under this category. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Again, the tow path is a viable alternative route to the B1438. Removing cyclists off the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which creates an obstacle. The proposal does, therefore, warrant a score of 2 under 'safety'. Biodiversity – The Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – The proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Melton	662	Woods Lane	Despite the 30mph zone, vehicles seldom adhere to it making this necessary pedestrian and cycling route very unpleasant and dangerous. In addition, for those wanting to turn into Woods Lane from side streets, the speed combined with the volume of traffic make this dangerous. There T-intersection with Leeks Hill is a public right of way frequented by walkers a school children and requiring them to cross.	Additional signage to ensure all drivers are aware of 30mph zone, and installation of a speed camera to ensure vehicle compliance. Potential traffic calming measures, including signage and a pedestrian crossing point. Alternatively, and better still, reducing the speed to a 20mph zone would vastly improve this stretch of road for other users while only adding 60 seconds to vehicle journeys and reducing local noise and pollution.	0	0	0	1	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – No significant modal shift benefit. Optimisation – No significant optimisation benefit. Safety – A sign may have a partial benefit, although whether any additional signage makes a significant difference is unknown. Biodiversity – No significant biodiversity benefit. Leisure – No significant leisure benefit.
Melton	353a	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Melton Road	0	0	0	2	0	0	2	The commenter proposes enforcement parking for multiple roads within Woodbridge and so, for the purpose of this assessment, each road has been assessed respectively. Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The proposal does not create new infrastructure and will unlikely result in a significant modal shift. Optimisation – The proposal does not improve existing infrastructure and does not, therefore, score under this category. Safety – The proposal has safety benefits. Melton Road is 30mph, but the parked vehicles result in cyclists having to move to the centre of the road, which is a busy 'b' type road. The improvements will provide modest safety benefit to a road, therefore a score of 2 is considered reasonable. Biodiversity – No significant biodiversity benefit. Leisure – The proposal is not for new infrastructure and will unlikely have leisure benefit.
Melton	353b	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Chapel Street	0	0	0	1	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The proposal does not create new infrastructure and will unlikely result in a significant modal shift. Optimisation – The proposal does not create new infrastructure and does not, therefore, score under this category. Safety – The proposal has moderate safety benefits. The road appears to have high levels of parking and, being an already narrow road, with vehicles parked along the side it does essentially become a single lane meaning cyclists have to mix with traffic. Therefore, a score of 1 is considered reasonable. Biodiversity – No significant biodiversity benefit. Leisure – The proposal is not for new infrastructure and will unlikely have leisure benefit.
Melton	353c	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Castle Street	0	0	0	1	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The proposal does not create new infrastructure and will unlikely result in a significant modal shift. Optimisation – The proposal does not create new infrastructure and does not, therefore, score under this category. Safety – The proposal has moderate safety benefits. The road does appear to be moderately narrow and the parked cars on the side of the road results in the road essentially becoming single lane meaning cyclists have to mix with traffic. Travelling northbound along this road, there is no footway which also results in pedestrians mixing with traffic. Biodiversity – No significant biodiversity benefit. Leisure – The proposal is not for new infrastructure and will unlikely have leisure benefit.
Melton	353d	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Bredfield Road	0	0	0	0	0	0	0	Despite the commenter proposing enforcing parking along Bredfield Road, there does not appear to be a significant issue.
Melton	353e	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Seckford Street and Theatre Street	0	0	0	1	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The proposal does not create new infrastructure and will unlikely result in a significant modal shift. Optimisation – The proposal does not create new infrastructure and does not, therefore, score under this category. Safety – The proposal has moderate safety benefits. The road appears to have high levels of parking and with vehicles parked along the side it does essentially become a single lane meaning cyclists have to mix with traffic. Therefore, a score of 1 is considered reasonable. Biodiversity – No significant biodiversity benefit. Leisure – The proposal is not for new infrastructure and will unlikely have leisure benefit.

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
Mettingham	101	Cycle route between Bungay and Beccles	Not currently a safe direct cycle route to Beccles from Bungay. The main road is very fast and cars often overtake on hills and blind corners, the smaller roads are equally fast with blind corners and generally poor road condition.	Cycle path along the B1062 road	3	1	0	3	-2	1	6	Connectivity and Growth - Beccles and Bungay currently are poorly connected for cyclists but represent large settlements with good services. In addition this is considered a key corridor so a top score is provided. Modal Shift - PCT suggests a modest modal shift arising from improvements here. Optimisation - No existing infrastructure so not considered an optimisation. Safety - A busy road over 50mph in places means the improvement has good potential benefits. Biodiversity - An initial assessment suggests a pathway could be installed in the wide, mostly unmanaged verges. However this score could become a -3 should mature trees or hedgerows require removal. Leisure - As 2 historic market towns there exist some leisure potential to travel between the destinations. However a cycle path alongside a busy road would deter many leisure cyclists so a score of 1 is deemed reasonable.
Middleton	368	Between Garden House Middleton and Middleton Moor	There used to be a permissive path from opposite Garden House towards Middleton Moor this is now closed. To get to Middleton Moor from the footpath that comes out next to Fordley Road you have to walk on the B1122 which although is supposed to be 30 mile per hour limit the visibility is not good and the lorries do not give way. The addition of a short piece of footpath would allow the footpaths and lanes towards Kelsale or Yoxford to link up with the paths and lanes out from Middleton.	Create a short piece of off road footpath beside the B1122 between Fordley Road and the Middleton Moor footpath	0	0	0	1	-1	0	0	Connectivity and Growth - Not a key connection. Modal Shift - No effect. Optimisation - No existing infrastructure. Safety - It is a 30mph road, however its on a bend and is potentially busy. Biodiversity - Loss of unmanaged grass verge. Leisure - Little to no effect on leisure.
Nacton	251	A1156 Nacton to Warren Heath Ipswich	Limited cycle path from Seven Hills / Nacton into Ipswich	Consider providing a full cycle/footpath all the way from Nacton (even Seven Hills Junction) towards Warren Heath (Past the Show Ground)	3	3	3	3	-2	2	12	The commenter proposes cycling infrastructure into Ipswich via Felixstowe Road, A1156. Felixstowe Road has some existing infrastructure along the route, which will need to be improved to a higher standard of infrastructure, and new infrastructure needs to be implemented along the sections which currently do not have cycling infrastructure. Connectivity and Growth – The proposal will have significant connectivity benefits as it will help towards the completion of a key corridor and creates a connection into Ipswich, which is a major service centre. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, there will be a resultant significant modal shift, therefore a score of 3 is warranted under this category. Optimisation – As the proposal will also optimise existing infrastructure from on-road infrastructure to cycle tracks, a score of 3 under this category is considered reasonable. Safety – Felixstowe Road, as a busy ‘A’ type Road with high-speed limits. Although Felixstowe Road does have cycling infrastructure along some sections of the road, it is poor quality, therefore the proposal will still likely be beneficial. A score of 3 is considered acceptable. Biodiversity – The proposal will likely result in the removal of managed grass areas, which have limited biodiversity benefit, and the loss of other shrubbery. A score of -2 is deemed reasonable. Leisure – The proposal will likely have more connectivity benefit than that of leisure, however the proposal will connect into Trinity Park Events centre, which likely has some leisure value, and connect into Ipswich, which is a key town centre and has significant leisure value. As the proposal will have more connectivity benefit and as the route is along Felixstowe Road, which is busy and unlikely considered an attractive leisure route, a score of 2 is considered reasonable.
Newbourne	603	GR 256 429	Newbourne #1/Brightwell #19 was un-signed and ploughed last time I tried to walk from Waldringfield to Bucklesham	Reinstate signs and ensure link to A12 (T) crossings at GR238431 and the tunnel at GR 241 433	0	0	1	0	0	1	2	The commenter suggests that PROWs 1 and 19 were ploughed and the lack of signage makes the paths hard to follow. The commenter proposes reinstating signage along these footpaths in order to create an effective link towards the A12. Connectivity and Growth – No significant connectivity benefit. Modal Shift – Unlikely that the proposal will provide modal shift benefit. Optimisation – Although the route is not improved, the addition of signage represents a modest optimisation so scores 1 point. Safety – No safety impact. Biodiversity – No biodiversity impact. Leisure – The footpaths are attractive and connect into a wider network of PROW routes, therefore it is likely that the optimisation will have modest leisure benefit.
North Cove	195	End of combined cycle-way/footpath from North Cove church to The Street	Cyclists exit the cycle way at speed without stopping to give way at the end sometimes going over the bonnets of cars travelling from the A146 towards Pinewood Gardens and Marsh Lane.	Just repainting the Give Way lines and triangle so that it shows up more to see if that helps resolve the problem.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Otley	93	The road between Otley and Crettingham	There are safe and pleasant routes for pleasure cycling around Monewden and Framsdon. The only way to access these routes from Otley is via Chapel Rd towards Crettingham. This road is narrow and has no speed limit. Vehicles drive very fast on this road. This road is a major reasons that families and children cannot cycle in safety around Otley	Add cycle lanes, reduce the speed limit, add warning signs	1	0	0	2	0	1	4	Connectivity and Growth – The proposal would create a cycle route into Crettingham and potentially Monewden; however, as these are small settlements with limited services, there is unlikely going to be ‘everyday use’ and the proposal is for low quality infrastructure. Therefore, a score of 1 is considered reasonable. Modal Shift – According to PCT, Chapel Road does not currently have high cycle activity and it is unlikely that the proposal would lead to a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Chapel Road has both a 30mph speed limit and an NSL. As the proposal is for on-road cycle lanes and as Chapel Road is a rural road, a score of 2 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – The proposal would connect to a few footpaths that form part of the PROW network in Otley, however, although attractive, these PROWs are not in designated areas. With consideration to the previous, a score of 1 is considered reasonable.

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Otley	143	Helmingham Rd from the centre of Otley to the White House pub and the houses at the edge of the village.	The road is fast and straight despite the 30mph limit. There is no foot or cycle path. This splits the village and makes it dangerous for cyclists and pedestrians to move to and from the village.	A shared cycle footpath would encourage both cycling and walking and reduce car use. This would be especially effective if it included traffic calming measures	2	0	0	1	-3	1	1	Connectivity and Growth – the proposal would connect the houses north of the Otley village centre, which are currently isolated from the village centre as there is no infrastructure connecting them, therefore the proposal scores moderately as this will provide a connection to the village shop, GP surgery, and the primary school. Modal Shift – the road is relatively quiet on PCT and there is insufficient evidence that the proposal would result in significant modal shift. Optimisation – the proposed infrastructure is new and does not therefore, optimise the existing. Safety – Helmingham Road (B1079) between the Otley village centre and the public house (The White Hart) has a 30mph speed limit and is relatively straight in nature, therefore the improvement will likely provide a modest safety benefit to an already relatively safe road, which is why a score of 1 is considered reasonable. Biodiversity – the proposal would likely result in the loss of kept grassed areas situated next to the existing footways, which stop at Ipswich Road junction. Furthermore, the proposal would also likely result in the loss of well established hedgerows which have high biodiversity value. Leisure – Not only would the proposal connect a handful of PROWs including both footpaths and bridleways, but it would also connect the village centre to the public house. However, it is likely that this route will have more 'everyday' value than leisure value.	
Otley	144	Footpath	Students walking through Otley bottom to Post office are a road hazard and often cannot be easily seen. Would also encourage locals that work at the college to walk to work.	To encourage locals to walk to work and to provide safety for students who always walk to the post office, provide a footpath. This will get them off the road, and reduce road hazards where traffic is fast through Otley bottom.	0	0	0	1	-1	0	0	Connectivity and Growth – the proposal would connect the school to the post office, however there is an existing footway situated opposite the primary school and post office (south side of the road) which can be used; therefore, the proposal does not warrant a score under 'Connectivity and Growth'. Modal Shift – PCT suggests that the road is not well used, therefore the proposal would not likely result in a significant modal shift. Optimisation – the proposed infrastructure is new and does not therefore, optimise the existing. Safety – Chapel Road (between the primary school and the post office) has a 30mph speed limit, it is likely that students will have to cross or walk along this road in order to get to the primary school, however the proposed infrastructure would prevent this. Therefore, the proposal has a small safety benefit warranting it a score of 1 under 'Safety'. Biodiversity – The proposal would likely result in the loss of kept grassed areas and small hedgerows, which front peoples houses, therefore there is a small negative biodiversity impact. Leisure – The road appears to have limited leisure potential.	
Otley	146	Gibraltar Road / Ipswich Road & Thomsons Lane	FYI - These three lanes have been proposed by Otley as potential 'Green Lanes' under SCC's latest initiative. They make an ideal cycle / walking /horse riding route between Otley, Ashbocking & Swilland avoiding the B1078 / B1077 & B1079 Road triangle.	Extend the 40mph Speed limit on the B1078 from Ashbocking towards Otley encompass the "Swilland" cross roads"....							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.	
Otley	147	Thomson's Lane, Otley.	FYI - Proposed by Otley as a potential Green Lane under the current SCC Initiative	Please support this proposal...								N/A	Quiet Lanes are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Otley	148	Ipswich Road, Otley	FYI - Proposed by Otley as a potential Green Lane under the current SCC Initiative	Please support this proposal...								N/A	Quiet Lanes are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Otley	149	Suffolk New Rural (Otley) Campus	There is no dedicated footpath from the College to Otley Village. Students are often see wandering across the fields.	As a minimum reinstate the permissive path that used to exist between the college and Otley Bottom. This has been fenced off by the Land owner / user. Consider a further permissive path option connecting the college with the path that runs along the 'gull' and on to the church / village	1	0	0	3	-2	1	3	Connectivity and Growth – Otley college is isolated from Otley village with no existing walking infrastructure along the roads, therefore the proposal does score moderately. However, there does appear to be a footpath east of the college (PROW 30) which forms part of a network of footpaths and bridleways to the village centre. Modal Shift – insufficient evidence that the proposal would cause significant modal shift. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing. Safety – the proposal would likely result in less pedestrians using the main road (B1079) in order to get to the village centre. The B1079 is a fast moving 'B' type road with a national speed limit with no existing infrastructure, therefore, with consideration to the road conditions, removing pedestrians off the road scores significantly under 'Safety'. Biodiversity – the creation of a footpath would likely result in some loss of some wild verges. Leisure – the proposal would likely have small leisure benefit as the footpath would connect to the existing PROW network along the 'Gull'.	
Otley	150	B1078, Otley College to Swilland	No footpath / wide verge making it unsafe to walk along	Consider making the 'permissive footpath' that runs along the northern edge of the large field permanent	2	0	0	3	0	0	5	Connectivity and Growth – The permissive path connects to Gibraltar and Otley College. Gibraltar has limited services and it is, therefore, unlikely that the path will be used on a daily basis. However, as it does connect to a school and there are no alternative routes, a moderate score under this category is considered reasonable. Modal Shift – Due to the limited connection to services, it is unlikely that the proposal would be used on a daily basis. PCT suggests that the proposal would not result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Currently, pedestrians likely utilise the B1078 which, in this particular section, has both a NSL and a 40mph speed limit. The proposal would provide an alternative safer route to that of the B1078, therefore a score of 3 is considered reasonable. Biodiversity – The proposal will not have a significant biodiversity impact. Leisure – The proposal would connect to PROW 33, however this will unlikely provide significant leisure benefit.	

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Otley	157	Chapel Road, Otley	Land allocated for significant housing development within the village. Increases in the number of houses within the village will inevitably increase the amount of motorised traffic within the village, which in turn will make the roads feel less safe for cyclists, parents of children and other road users (Mobility Scooters, Horse riders etc). This will have a detrimental effect on the plan to increase cycling and walking...	1. Install a 'Full sized' roundabout on Chapel Road at the point of this development (where the Primary School, Village Hall and Doctors Surgery are currently located). This would help significantly to reduce 'speeding' traffic along Chapel Road. 2. Reduce the Village 30mph speed limits to 20mph...	1	0	0	1	0	1	3	In terms of a roundabout along this section of Chapel Road, it is for highways to consider when the application for the site allocation comes in. Instead, a crossing could be considered. Connectivity and Growth – Chapel Road is not a significant barrier as it is a moderately quiet safe road, however there are key services situated on either side and a crossing point would connect these. Therefore, a score of 1 is considered reasonable. Modal Shift – A crossing is unlikely going to result in a significant modal shift. Optimisation – A crossing is considered new infrastructure and does not therefore, optimise the existing. Safety – Chapel Road has a 30mph speed limit and appears to be a moderately safe road, but it does not contain any crossing points and as a school is nearby a crossing point has been awarded 1 point. Biodiversity – There are no significant biodiversity impacts. Leisure – A crossing will likely result in moderate leisure benefit as it would connect a couple PROWs, hence a score of 1 under this category.
Otley	165	Chapel Road, Otley, and its continuation towards Crettingham	The fields around Otley have a good network of footpaths. Many are easily accessible for walkers with children and dogs, but those that lead off to the left and right of Chapel Road beyond the derestriction sign at the edge of Otley can only be reached by walking along the road itself or on a high, narrow verge. With cars passing at speed outside the 30 mph limit, this is not safe.	Continuation of the pavement from Otley village at least to the turn-off to Villa Farm; even better, continue the footpath to Shrubbery Farm.	0	0	0	3	-2	1	2	Connectivity and Growth – the new infrastructure offers limited connectivity benefit and will likely have more leisure value. Modal Shift – the proposal will unlikely result in significant modal shift. Optimisation – the proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – the road does have a national speed limit; therefore, removing pedestrians off the road warrants a score of 3. Biodiversity – the extension of the existing pavement along Chapel Road will likely result in the loss of well-kept grassed verges and potentially the loss of some small hedges/small shrubbery, hence a score of -2. Leisure – the proposal would have small leisure benefit as it connects a handful of PROWs, therefore a score of 1 is considered reasonable.
Otley	167	X-roads on B1078 with Gibraltar Rd. Otley and High Rd. Swilland.	V. dangerous junction because of speed of traffic and overtaking on B1078 .	Extend the speed limit of 40 mph at the Ashbocking x-roads so that it continues all the way to the 40 mph limit near Otley College.							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Otley	180	Footpath B1078, Swilland	Footpath comes out on side of B1078 without any protection for walkers, there is no option but to walk on the carriageway of this busy (fast) B road.	Provide some sort of roadside path to the next footpath or at least the swilland crossroads. This path is part of a local network of paths which are regularly used by dog walkers etc. Could form part of a footpath connection between Swilland and Suffolk rural College	1	0	0	3	-2	1	3	Connectivity and Growth – Whilst the proposal offers to connect footpaths that forms a route into Otley College so could score a 2, it is indirect and will likely be used for more leisure purposes. A score of 1 is considered reasonable. Modal Shift – It is unlikely that the proposal would lead to a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore optimise the existing. Safety – This section of the B1078 has a national speed limit and pedestrians currently have to walk along the road when exiting footpaths. As the proposal would remove walkers off a section of the road, it will have significant safety benefit. Biodiversity – It is likely that the proposal will have a resultant loss of managed grassed areas and small hedgerows, therefore a moderate negative score under this category is considered reasonable. Leisure – The proposal connects PROW routes which, although attractive, do not reside in designated areas. Therefore, a small score under this scoring category is considered reasonable.
Otley	182	Footpath East of Otley Bottom	Footpath that runs from driveway of Chalet Bungalow at Otleybottom up hill (NE direction) and across to unamed road from Church Road is often completely overgrown, muddy and lacking any form of maintenance including repair of broken styles and signage.	Maintain footpath to a higher standard....this path represents a viable walking route from Suffolk Rural College to Otley Village.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Otley	183	Permissive footpath Suffolk Rural to Otley Bottom	At some point in recent history the permissive footpath along the northside of the field has been withdrawn. This was a useful path connecting the end of public footpath at the College with the start of the one at Otley Bottom giving a safe walking route to Otley Village.	In this case reinstaing this path would give a viable walking route to Otley Village. Overall consider promoting the idea of 'Permissive Footpaths' again with our farming community	1	0	0	3	0	1	5	Connectivity and Growth – The proposal would connect PROW 30 to PROW 31 which, in turn, will provide a safe pedestrian route from Otley College to Otley village centre. Providing a connection to a somewhat isolated area can score a 2, however as the proposed route is indirect, a score of 1 is considered reasonable. Modal Shift – It is unlikely that the proposal would result in a significant modal shift. Optimisation – The proposal represents a new route for pedestrians as opposed to an optimisation. Safety – Currently pedestrians will need to walk along B1078, which has a 40mph speed limit, and the B1079, which has an NSL, to access PROW30 from PROW 31. Removing pedestrians off this section of the road has safety benefits and it is considered, therefore, that a score of 3 under this category is reasonable. Biodiversity – No significant biodiversity impact. Leisure – The proposal would connect two PROW routes which, although attractive, are in undesignated areas. Therefore, a score of 1 is considered reasonable.
Otley	185	Ipswich Road Otley	Initial Section of Footpath (Bridleway ?) known as Gipsy Lane is overgrown	Upgrade this path to bridleway status to provide a route from Otley towards Helmingham	1	0	0	2	-1	0	2	Connectivity and Growth – The alterations would allow cyclists north to access Helmingham whilst bypassing the B1077 and B1079 which are not suitable cyclist routes. Otley and Helmingham are both small settlements with limited services, however the connection will allow an element of service pooling. As the proposal does not connect directly into Otley and Helmingham, a score of 1 is considered reasonable. Modal Shift – Unlikely going to result in a significant modal shift. Optimisation – This is a new route and is not considered, therefore, an optimisation. Safety – Gipsy Lane will provide a safer alternative to the B1077 and B1079, which are busy 'B' type roads with NSLs and removing cyclists off these roads could receive full marks, however as it would not achieve a significant modal shift and as it does not directly connect into the centres of both settlements meaning other roads will still need to be used, a score of 2 is considered reasonable. Biodiversity – A modest minus point is deemed reasonable due to any widening of

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												the path will likely remove some foliage of a rural footpath. Leisure – No significant leisure benefits.
Otley	202	Connection to local footpath Network at Suffolk Rural College	Suffolk Rural (Otley) College does not have footpath access to Otley Village	A short section of 'permissive footpath' from the B1078, past the 'Motte' and down to the 'Gull' would connect up with the public footpath into Otley Village. This is an example where many people who live in Rural Suffolk but outside villages do not have direct and safe access to the local public footpath network. The 'B Road network' is becoming busier with increased levels of mixed traffic (ie. cars, lorries, farm vehicles) travelling at up to the national speed limit (60mph). There is a genuine feeling among local residents that walking and cycling on these roads 'is simply too dangerous' especially for children and less abled persons. This encourages more use of cars for local journeys eg the school run and popping to the local shop and hence the roads become busier.	1	0	0	2	-1	1	3	The commenter proposes a footpath through the fields north of Suffolk Rural (Otley) College to the PROWs that adjoin the 'Gull'. Connectivity and Growth – The route will provide a safe pedestrian route from Otley College to Otley village centre. Providing a connection to a somewhat isolated area can score a 2, however as the proposed route is indirect, a score of 1 is considered reasonable. Modal Shift – It is unlikely that the proposal would result in a significant modal shift. Optimisation – The proposal is for a new pedestrian route and does not, therefore, optimise the existing. Safety – The proposal will provide an alternative route to the B1079 where, as a 'B' type road with a NSL, volume and speed of traffic is likely high. Removing pedestrians off this road has safety benefits, however as the route would not expect to achieve a significant modal shift a score of 2 is considered reasonable. Biodiversity – A modest minus point is deemed reasonable due to the addition of the footpath will likely result in the removal of some foliage. Leisure – Although the PROW does not extend through a designated area, the route is particularly attractive as it extends along the 'Gull'. A score of 1 is deemed reasonable.
Otley	212	Thompson Lane Ashbocking/Otley	Road surface is falling apart making it difficult to cycle	Resurface and reduce crowning/camber to make cycling safer							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Otley	233	Chapel Road, Otley	The School, Village hall and Doctors surgeries are all co-located at this point on Chapel Road. These are magnets for cars particularly at drop off times, this creates an area of local congestion and conflict with pedestrians particularly those with children trying to cross the road or indeed cycle to the school. Through traffic travelling at speed compounds the safety risk as the village hall carpark (which is used as the school drop off area) exit/entrance is on a blind bend.	Given the potential of further significant housing development in this area it would make sense to create a roundabout at this point giving safer access to the Hall carpark and Doctors surgery and also serve to calm the through traffic on Chapel road, a carpark within the development would also ease the congestion and provide some public off street parking within the village.							N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Otley	372	B1078 junction with Charity Lane, Otley	B1078 Traffic turning right into Charity Lane often cuts across the junction ignoring the road markings which if you're a cyclist or car waiting to turn right out of it is quite disconcerting. The road markings have been rubbed away. This is typical of many junctions along this road where the mouth of a minor road is narrow. Vehicle drivers naturally cut the corner, rather than making the full 90 degree manoeuvre.	Improved markings on the B1078 & at the junction itself on Charity Lane.	0	0	0	2	0	0	2	Connectivity and Growth – The proposal does not create additional connectivity. Modal Shift – It is unlikely that the proposal would result in a significant modal shift. Optimisation – The proposal does not optimise the existing infrastructure. Safety – The junction is situated on the B1078 which has a 40mph speed limit and, as a 'b' type road, volume and speed of traffic is likely high. The cyclist would remain on the road, however improving the junction for cyclists does warrant 2 points under 'safety'. Biodiversity – There are no significant biodiversity benefits. Leisure – There are no leisure benefits.
Oulton	541	Gorleston Road, west side between Mobbs Way and Dunston Drive. Oulton	A build up of vegetation and leaves over the past 2 years has reduced the width of the footpath. This means that if a mobility scooter is coming on this path any other scooter, buggy or pedestrian has to walk into the road to get past.	Remove all debris from the tarmac footpath. The footpath extends to just behind the lamp posts and this will double the width of the footpath. My wife has rung up a number of times about this.							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Oulton Broad	49	Old High Street in the north and Kirkley in the south, business district	Lack of places to secure bikes whilst in shops, making people tie up bikes to lamp posts, benches and drain pipes. Even where there are some bike racks (in front of HSBC for instance) there are too few of them and often there is no place to properly secure a bike.	Where the paths are very narrow, narrow horse hitch style posts can be put next to buildings all (not the wider Sheffield bike racks). Old High Street	0	1	1	0	0	2	4	Connectivity and Growth – No significant connectivity and growth impacts. Modal Shift – Without full disposition of the parking it is a matter of judgement. Cycle Parking alone is unlikely to encourage large numbers of modal shift, but a certain level will be provided. Optimisation – The cycle parking adds to the existing infrastructure and this is a well used route with on-road markings so a single point has given provided. Safety – No significant safety benefit Biodiversity – No significant biodiversity benefit. Leisure – The High Street represents a strong leisure centre as it contains café/restaurant offers, heritage buildings and local attractions according the improvements will also have a strong impact giving 2 points.
Oulton Broad	191	Beccles Road to Suffolk Wildlife Trust's Carlton Marshes	There should be provision of cycle hire at Oulton Broad South railway station for visitors to the Carlton Marshes reserve who arrive by train, also a dedicated cycle route from the station to the nature reserve. This would assist ecotourism, visitor numbers to the reserve and assist locals cycling in the area as well.	Either a dedicated cycle route by the Angles Way route from the reserve to Oulton Broad or a dedicated cycle route along Beccles Road.	3	2	0	2	-1	3	9	Connectivity and Growth – A proposed route from Nicholas Everitt Park to Carlton Marshes and Burnt Hill Lane bypass Beccles Road which is both a key corridor and highlight uses as a route along the western edge of the town. Modal Shift – Improving Beccles Road to a high standard would create a high modal shift, however the potential improvements along the northern section of Beccles Road is low meaning a bypass would attract at least some of the modal shift. Optimisation – This would represent a new route for cyclists as opposed to an optimisation. Safety – Beccles Road is 30mph and to the north is relatively straight, but it is normally busy. The third river crossing may alter some traffic patterns but a score of 2 is deemed reasonable. Biodiversity – Paths appears a reasonable size currently so unlikely to need significant direct biodiversity removal, however there may be some removal in a sensitive area. Increased cyclists to important natural area would need to be considered. Leisure – The route could provide a leisure destination adjacent the river and adjoining the Carlton Marshes with its new visitor centre. The attractiveness of the route means it is considered a full score.
Oulton Broad	615	Carlton Marshes	creating a safe cross-country cycle route between Oulton Broad and Norwich making use of the re-established ferry crossing of the River Waveney at Burgh St Peter and the ferry crossing of the River Yare at Reedham. This continues to be a high priority objective of the BLAF.	Within Suffolk the route could commence at Nicholas Everitt Park in Oulton Broad and following either Footpaths 15 or 14 westwards to the newly established Suffolk Wildlife Trust Centre at Carlton Marshes. At Carlton Marshes these FPs link into Bridleway No 4 which goes northwestwards towards the River	3	2	0	2	-1	3	9	Connectivity and Growth – A proposed route from Nicholas Everitt Park to Carlton Marshes and Burnt Hill Lane bypass Beccles Road which is both a key corridor and highly used as a route along the western edge of the town. Modal Shift – Improving Beccles Road to a high standard would create a high modal shift, however the potential improvements along the northern section of Beccles Road is low meaning a bypass would attract at least some of the modal shift. Optimisation – This would

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				Waveney. Some 500 metres from the River Waveney the route to the ferry follows FP No 10 which sits on top of the Floodbank. The use of Footpaths for cycling may require upgrading the status of the highways to Bridleways although it is understood that there are other options available to allow cyclist to use Footpaths.								represent a new route for cyclists as opposed to an optimisation. Safety – Beccles Road is 30mph and to the north is relatively straight, but it is normally busy. The third river crossing may alter some traffic patterns but a score of 2 is deemed reasonable. Biodiversity – Paths appear to be a reasonable size currently so unlikely to need significant direct biodiversity removal, however increased cyclists to important natural area would need to be considered. Leisure – The route could provide a leisure destination adjacent the river and adjoining the Carlton Marshes with its new visitor centre. The attractiveness of the route means it is considered a full score.
Oulton Broad	644	At Oulton Broad South rail station adjacent to Bridge Road near Dell Road	A foot path / cycle path under the Bridge Road overpass connecting Oulton Broad South station to Dell Road.	The construction of a short foot path/ cycle path to go through an existing archway in the road bridge to connect Oulton Broad South rail station to Dell Road. The new route would open up the rail station to neighbourhoods north of Bridge Road for both cyclists and pedestrians who have no dedicated route to the station that is not step-free and segregated from road traffic entering/exiting via the station forecourt. The footpath would also create step-free and safe access to the Bridge Road foot-crossing via an existing archway in the bridge; presently two sets of steps must be navigated to make this journey. The scheme also negates the need for pedestrians and cyclists to use the busy junction at the station entrance.	2	2	0	0	0	0	4	Connectivity and Growth - The improvement provides access to the train station which, although geographically close, is difficult to reach due to Bridge Road. Modal Shift - The small section of Bridge Road between Dell Road and the train station shows a very high level of potential modal shift growth, however the suggested improvement only impacts those travelling from Dell Road and not travelling north-south so the full modal shift growth is not achievable. PCT still shows that Dell Road has reasonable potential and as it directly connects to a train station a score of 2 is deemed reasonable. Optimisation - This represents new infrastructure and not an optimisation. Safety - Bridge Road experiences a high level of traffic and the junction can be difficult to navigate. However a score of 0 has been given here as travellers from Dell Road can use a lighted crossing further along the road meaning a safe crossing is available. Biodiversity - There are no biodiversity benefits. Leisure - The improvements would have a greater day-day benefit over that of a leisure use as it doesn't create improvements to Oulton High street or Carlton Marshes and the Lowestoft train station offers better connections to the town centre.
Oulton Broad	653	Beccles Road, Carlton Colville between Ivy Lane and the roundabout linking A1145	The footpaths linking Oulton Broad (eastern Beccles Road) with the western end of Beccles Road are not safe. The Northern footpath has become excessively narrowed by the lack of maintenance to the hedgerows between Burnt Lane and Ivy Lane resulting in impossible for a parent to walk side by side with a young child. The southern footway does not link the whole way and is hidden from the road by trees providing for an unsafe environment.	Removal of overgrown vegetation and excessive amounts of soil on the verge for the length of footpath adjacent to the field used as paddocks. Cut back the trees immediately west of Burnt Hill Way to provide a clear view off the footway to passing traffic and making a safer environment. (note Martineau Lane, Norwich incident and action taken). Extend the footpath and create cycleway link past Chaulkers Crescent all the way to the roundabout with the A1145 and Anchor Way estate. Prevent unauthorised off-road / verge parking along this route.							N/A	Foliage that grows in private land are the responsibility of private landowners. Foliage that grows within the highway boundary is a Suffolk County Council (SCC) specific matter and have been shared with SCC for their consideration as the Highway Authority.
Oulton Broad	671	Bridge Road, Oulton Broad railway crossing	Can you tell me if there will be provision in your new, Cycling and walking strategy to modify the existing footpath over the railway bridge on Bridge Rd, Oulton Broad? Cycling over that bridge on the road is very off putting to many cyclists including myself. This is actively discouraging cycling in Oulton Broad.	Can the existing pedestrian footpath be modified/widened to accept cycles as well as pedestrians?	3	3	0	2	0	2	10	Connectivity and Growth - The bridge lies on a key corridor and represents a significant disruption in any cohesive route giving a high score. Modal Shift - PCT suggests that the section across the bridge has a high potential for modal shift growth. Optimisation - This would represent new cycling infrastructure. Safety - The section of the road is 30mph, but has scored higher due to its high level of traffic and narrow confines it has increased its score. Biodiversity - There are no biodiversity benefit. Leisure - This is a significant pinch point the restricts access through to Nicholas Everitt Park and Carlton Marshes.
Oulton Broad	781	Saltwater Way, Oulton Broad	Lowestoft's off-road facilities are a 'mixed bag.' A number of the more recent cycle-paths are quite good but some of the older ones are extremely bad and poorly thought through and, in some cases, not necessary. The legal position is that pedestrians can walk on cycle-paths but cyclists cannot ride on footways. However, it is reasonable to expect both to respect each others space.	Considering all the complaints about cyclists on footways, I feel peeved when I see far more pedestrians walking on cycle-paths alongside footways than vice-versa. That said, on a number of them, the pedestrian part is so narrow one could not reasonably expect them to not drift onto the cycle path. That is particularly the case for the cycle path/footway alongside Saltwater Way, Oulton Broad, continuing as the underpass. Indeed, at points, particularly close to the junction with Victoria Road, there is greenery that protrudes onto the footway section. The facility also changes from segregated to shared use and back to segregated, which is confusing. There is also the point the underpass is prone to flooding.	0	0	1	0	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – No significant modal shift benefit particularly as any drainage issue will be infrequent. Optimisation – It is likely to be a difficult issue to overcome, but it will optimise the Cycle path and walkway by keep it available throughout the year scoring it a 1. Safety – Whilst the site is flooded it is clearly signed that people should not cross. It is not considered a significant safety issue and would require a sudden flooding to form a hazard. Biodiversity – As an urban path there is no significant impact to biodiversity. Leisure – Whilst the path may provide additional access to some leisure uses, but other access options are available and the leisure benefit is not deemed significant.
Oulton Broad	782	Oulton Broad	There are good and bad things about the short stretch of cycle path running from the traffic lights just south of the Bridge Road/Saltwater Way/Victoria Road roundabout, past the fish and chip shop and former Spar store into Oulton Broad centre. The good point is that it gives cyclists a geographical advantage to/from the centre and links, via the toucan crossing, with the shared facility to/from the railway bridge.	Ironically, ideally it should be shorter, avoiding passing the fish and chip shop and former Spar. I cannot exaggerate how many more pedestrians walk on the cycle path instead of the footway, despite, in this case, being reasonably wide. Also, cars regularly park on it and when the Spar was open, it included lorries. The nature of the road means there would be no harm in cyclists having to ride it a little further, especially as a 20 mph speed limit would be easily enforceable.	0	0	0	0	0	0	0	Reducing the length of the path as pedestrians regularly use the cycle path and the member of public considers the road safe. 1 added to safety as the suggestion is remove pedestrian/cyclists conflict. Connectivity and growth – The removal of the cycle path adds no connectivity, however as the site is well situated and the proposal removes only a small section of the path it does not score a minus number either. Modal Shift – No significant Modal Shift Optimisation – No optimisation of existing infrastructure. Safety – This category concentrates on conflict between vehicles and cyclists/pedestrians and the removal of part of the path would not alter this. Biodiversity – This is an urban road with no impact to biodiversity. Leisure – The routes appears to have limited leisure benefit.
Oulton Broad	785	Nicholas Everitt Park	Considering the size of the Nicholas Everitt Park car park, I would think there is room for some quality covered cycle parking or, if not, in the park itself.		0	1	0	0	0	2	3	Connectivity and Growth - The addition of new cycle parking is not considered to create significant connectivity and growth benefit. Modal Shift - A modest modal shift could be expected as less people use cars to go to the park if cycle parking is available. Optimisation - This doesn't optimise existing cycle infrastructure. Safety - This has limited safety implications. Biodiversity - The location of the cycle parking could result in a minus score under biodiversity, but it is likely that the parking can be suitably located without significant biodiversity loss. Leisure - Nicholas Everitt Park represents a key leisure destination for Oulton and western Lowestoft so a reasonable score has been given here.

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Oulton Broad	49a	Old High Street in the north and Kirkley in the south, business district	Lack of places to secure bikes whilst in shops, making people tie up bikes to lamp posts, benches and drain pipes. Even where there are some bike racks (in front of HSBC for instance) there are too few of them and often there is no place to properly secure a bike.	Where the paths are very narrow, narrow horse hitch style posts can be put next to buildings all (not the wider Sheffield bike racks). Kirkley Business Park	0	1	1	0	0	0	2	Connectivity and Growth – no significant connectivity and growth impacts. Modal Shift – cycle parking alone is unlikely to encourage large numbers of modal shift, but a certain level will be provided so a score of 1 is deemed appropriate. Optimisation – The Kirkley Business Park is on a segment of cycle infrastructure and is also part of the Key corridor. While the parking may not provide significant optimisation it will provide improved capacity to existing/proposed improvements. Safety – No significant safety benefit. Biodiversity – No significant biodiversity benefit. Leisure – No significant leisure benefit.
Pettistree	79	River path Kyson to Wilford Bridge	Thank you for the no cycling signs on the Kyson part of this path. Some clear ones are needed on the Wilford Bridge section.	If you are going to allow cycling here then you need to keep cycle and pedestrian paths separate as very dangerous otherwise, as I have often found!	0	0	0	0	0	0	0	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – No significant modal shift benefit. Optimisation – No significant optimisation benefit. Safety – Whilst the proposed signs may reduce potential cyclist and pedestrian conflict the improvement to safety is limited. Biodiversity – No significant biodiversity benefit. Leisure – If cyclists are misusing the path this may effect enjoyment for walkers, however any existing rules should be adhered to anyway and signs on their own are unlikely to represent a significant leisure benefit.
Playford	135	C324 (The road between the B1079 and Butts Road Playford).	The part of the C324 between Boot Street and Tuddenham is part of the National Cycle route system Stowmarket to Woodbridge. During the week this road is a Rat-Run between Woodbridge and Ipswich and is very busy and at times highly dangerous for cyclists. Weekends see a great number of cyclists on this route, although still dangerous it is a lot more cycle friendly. Some signs along the route stating "Cyclists in Road" especially on bends would be very helpful.	Cyclists in Road signs on bends as part of the road is single lane.	0	0	0	1	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – No significant modal shift benefit. Optimisation – No significant optimisation benefit. Safety – As a road with no suitable cycling infrastructure and with a NSL, a guidance sign may have partial benefit. Biodiversity – No significant biodiversity benefit. Leisure – If cyclists are misusing the path effect enjoyment for walkers, however any existing rules should be adhered to anyway and signs on their own are unlikely to represent a significant leisure benefit.
Playford	140	Playford Road/Martlesham Road/Bealings Road	This is used as a rat run by drivers seeking to avoid congestion on the A1214 and the NSL applies over large parts of it, resulting in speeding vehicles and a hostile environment for cycling and walking. It is an obvious quiet route for cycling between Ipswich and Woodbridge.	Close the road to through motor traffic and provide a signalised cycle crossing at the western end to enable Ipswich-bound cyclists to continue on their way.	1	2	0	3	0	0	6	Connectivity and Growth – The proposal will likely help in connecting Ipswich to Woodbridge and Martlesham, however there are existing connections along the A1214. Due to both settlements having good levels of schools, shops, and employment opportunities, there will unlikely be 'everyday' use. A score of 1 is considered acceptable. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, Playford Road will have a relatively significant modal shift, therefore a score of 2 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Playford road has a NSL and is likely used as a rat-run to avoid the A1214 to Ipswich, therefore the proposal will likely have significant safety benefit. Biodiversity – No significant biodiversity impact. Leisure – The proposal will likely have more connectivity and growth benefit than leisure benefit and provides limited connections to attractive PROW routes.
Playford	217	'Bridleway end of Playford Lane to Playford & Little Bealings	The surface of this bridleway is poor, rutted and uneven in places making it difficult to cycle on or use a mobility scooter	Consider upgrading the surface for the full length of its course. This would provide a very viable and usable cycle path directly to Ipswich from the Playford / Bealings area.	0	0	1	0	0	1	2	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The alterations would not be expected to create significant modal shift although it will create better availability for some users. Optimisation – The improvements will help make the pathway more inclusive. Resurfacing warrants a score of 1 under this category. Safety – This issue is raised as a matter of access and usability over safety. Biodiversity – No significant biodiversity benefit. Leisure – The byway connects into allotments and the greater PROW network, therefore resurfacing and providing access to a wider range of people warrants a point in this category.
Playford	327	Playford Road - west of its junction with Butts Road.	Playford Road used by motorists wanting to avoid speed limit on A1214 making it unpleasant and less safe to cycle as many of them drive far to fast.	This route was really popular during the lockdown when there was much less traffic and cyclists felt safe. Closing the road here and at junction further east would provide an excellent cycle route to Woodbridge and yet allow motorists to travel between Playford and/or Bealings and the A1214.	1	2	0	2	0	0	5	Connectivity and Growth – The proposal will likely help in connecting Ipswich to Woodbridge and Martlesham, however there are existing connections along the A1214. Due to both settlements having good levels of schools, shops, and employment opportunities, there will unlikely be 'everyday' use. A score of 1 is considered acceptable. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, Playford Road will have a relatively significant modal shift, therefore a score of 2 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Playford road has a national speed limit and is likely used as a rat-run to avoid the A1214 to Ipswich, therefore the proposal will likely have safety benefit. However, as the proposal is not for a completely traffic free route, a score of 2 is considered reasonable. Biodiversity – No significant biodiversity impact. Leisure – The proposal will likely have more connectivity and growth benefit than leisure benefit and provides limited connections to attractive PROW routes.
Playford	363	Main A1214 from Martlesham to Ipswich (Kesgrave Town section)	Being frank the entire cycle path from Martlesham to Ipswich is a disgrace. The surface is worn due to car traffic crossing it to access the many houses along its length. The path is dangerous and cyclists are at more risk of collision with cars from the many side roads because the Stop lines are painted on A1214 not on the cycle lane and Give Way signs on the cycle path are worn away. It is therefore safer to cycle on the main road as the least dangerous option defeating the need for a path.	Maintain the cycle with a good surface, clearly mark give way signs. Improve visibility because you cant see cyclists when approaching the A1214 from the numerous side roads Mark "Give way" before the cycle path on all sideroad junctions rather than on the main road which is some 10 to 15m further away ; cars are still slowing down and not stopped so a 10 to 15 mph side on collision is very likely.	0	3	2	0	-1	0	4	For the purpose of this assessment, upgrading the existing cycle/pedestrian infrastructure, including resurfacing, widening, and implementation of cyclist priority over side road junctions, will be assessed. Connectivity and Growth – The proposal is regarding the existing cycling/pedestrian infrastructure along the A1214, or Woodbridge Road, and does not represent, therefore, a new connection. Modal Shift – According to PCT, the A1214 has high cycling traffic and the widening and resurfacing of the cycling infrastructure to the highest standard will likely increase this. The proposal will result in a significant modal shift, therefore a score of 3 under this category is considered reasonable. Optimisation – The proposal will upgrade the existing infrastructure from a shared path to a segregated cycle track. Also, currently cyclists are regularly forced to stop to give way to motorists so implementation of cyclist's priority will likely optimise the paths use. This optimisation warrants a score

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												of 2.Safety – Off-road cycling infrastructure already exists, therefore the proposal will not have significant safety benefit.Biodiversity – The proposal will likely result in the loss of adjoining managed grassed areas; therefore, a small negative score is deemed reasonable.Leisure – No leisure benefit.
Playford	521	tarmaced private drive to lux farm	If a footpath or access could be provided up this drive it would help connect Kesgrave to playford, grundisburgh and beyond via footpaths. There is a footpath from main road, all Saints Church passing heath cottages to Playford Road. It needs extending to Lux Farm. At the moment to get to Playford and beyond you have to take footpaths either via Rushmere St Andrew or via Little Bealings. This is a significant diversion out of your way by a couple of miles.	Provide a public right of way or negotiate public access up the drive to Luz farm so you can join footpath leading on the playford etc. It would encourage more peopel to walk to Playford and beyond.	2	2	0	3	0	0	7	The commenter proposes extending FP11 northwards to connect into FP10 into Lux Farm. Connectivity and Growth – The proposal would connect two PROWs, subsequently connecting Kesgrave to Little Bealings and Playford. Little Bealings and Playford have limited services and connecting them to Kesgrave, therefore, will likely have significant connectivity and growth benefits. A score of 2 is considered reasonable. Modal Shift – Providing a new and direct pedestrian route will likely create a modal shift. A score of 2 under modal shift is deemed reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – This network of footpaths could provide an alternative route into Little Bealings and Playford avoiding the NSL country roads, therefore the proposal will likely have safety benefits. A score of 3 is considered reasonable. Biodiversity – No significant biodiversity impact. Leisure – No significant leisure benefit.
Playford	632	Playford Road between junction with Bent Lane and Hall Road and along Martlesham Road	High traffic speeds. Feels very dangerous to cycle along Playford Road. Also drivers often play chicken - overtaking me on my bike when there is oncoming traffic and they cut in front of me. There have been far too many near misses... It must be terrifying for the oncoming cars too.	I am very impressed with the recently installed speed cushions further down Playford Road between Humber Doucy Lane and Bent Lane. A big thank you to whoever initiated/funded/implemented these. There is just enough space between the cushion and side of the road for cyclists to pass and the cushions are successful in slowing traffic speeds. Also, the new mini-roundabout by Bent Lane /The Street /Playford Rd seems to have helped slow traffic speeds too. Can speed cushions be installed all the way along Playford Road and Martlesham Rd please? It is a key cycling route, but too terrifying for many people to use. And lower speed limits would hopefully benefit pedestrians too?							0	The commenter proposes reducing speed limits along Playford Road, this is outside the remit of the project and should be passed to Suffolk County Council. Similarly, the proposal of speed bumps is also outside the remit of this project.
Playford	135a	C324 (The road between the B1079 and Butts Road Playford).	The part of the C324 between Boot Street and Tuddenham is part of the National Cycle route system Stowmarket to Woodbridge. During the week this road is a Rat-Run between Woodbridge and Ipswich and is very busy and at times highly dangerous for cyclists. Weekends see a great number of cyclists on this route, although still dangerous it is a lot more cycle friendly. Some signs along the route stating "Cyclists in Road" especially on bends would be very helpful.	This is an alternative suggestion made by an officer of East Suffolk Council is to upgrade footpath 5 south of the road through Playford.	1	1	0	3	-2	1	4	Connectivity and Growth - Whilst there are not significant differences in the level of services Tuddenham and Playford offer there are some services (particularly in Tuddenham) that has some benefit to Playford so a score of 1 is deemed reasonable. Modal Shift - A modest modal shift could be achieved according to PCT. Optimisation - Requires significant improvements. Safety - Completely removing cyclists off the road would yield significant benefit as it is in places fast flowing, winding and narrow. Biodiversity - Widening of the path would result some biodiversity loss. Requires a full assessment and this minus score could be increased. Leisure - Could form an attractive route in its own right, but would only have modest draw.
Purdis Farm	123	Purdis Heath SSSI - Purdis Farm Lane at the junction with Purdis Avenue	New fences with stiles have been erected in the past few weeks along with a large gate across the wide path. It looks like the plan is to be able to close the gate to prevent any vehicle/bike access but it's not clear whether there will be access for wheelchairs or buggies. We regularly use this path with a wheelchair buggy.	Stiles should not be being installed on any footpath without also providing a gate big enough for a large wheelchair or mobility scooter. This applies to all areas.	0	0	1	0	0	1	2	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – An improvement is not considered to create significant modal shift. Optimisation – Removing the barriers won't improve the overall infrastructure but would provide a modest optimisation benefit scoring 1 point. Safety – This appears to be an access issue rather than safety. Biodiversity – No biodiversity impact. Leisure – There may be modest leisure benefits to this route, which is an attractive PROW, but it is not clear that the removal of barriers will provide a significant benefit so a score of 1 is deemed reasonable.
Purdis Farm	318	Bike paths via Murrills Road park	The barriers at Murrills Road & Bucklesham Road are tight to get a cargo bike through. Cars are often parked at the Meadow Crescent entrance/exit.	Increase gap of barriers at Murrills Road & Bucklesham Road. Add 2m of double yellow line at Meadow Crescent.	0	0	1	0	0	0	1	Connectivity and Growth – The barriers are passable, albeit problematic, so altering the design does not provide additional connectivity. Modal Shift – The removal of the barrier is unlikely to create significant modal shift. Optimisation – Removing/improving the barriers and implementing enforcement parking will make it more user-friendly and accessible to a wider-range of people meaning it has been given a score. Safety – This does not appear to be a safety issue. Biodiversity – No significant biodiversity benefits. Leisure – Unlikely to provide significant leisure benefit.
Purdis Farm	319	Edge of A1156 adjacent to path through from Murrills Road	Lack of footpath to the pedestrian lights to cross the A1156, worn grass track (sometimes muddy), in danger of being overgrown by gorse bushes.	10m length of path to connect the North-South path from Murrills Road to the piece at the pedestrian controlled traffic lights.	0	0	2	0	0	0	2	The commenter proposes a new section of path along the A1156 just east of the A1189/A1156 roundabout, however, there appears to already be a path here, despite being in significantly poor condition. For the purpose of this assessment, widening and resurfacing this pavement will be assessed. Connectivity and Growth – Connection already exists so does not score under this category. Modal Shift – The alterations would not expect to create significant modal shift. Optimisation – Widening and resurfacing a pavement warrants a score of 2 under this category. Safety – Although poor quality, the pathway exists and improving the pathway is unlikely going to improve safety. Biodiversity – No significant impact. Leisure – No significant leisure impact.
Purdis Farm	433	Warren Heath where Ransomes Way joins Felixstowe Road close to the railway line	Over the last few years changes have been made on both Felixstowe Road and Ransomes Road to increase speed of traffic. This has made crossing Ransomes Road a difficult and dangerous manoeuvre. Each side of the road is shared use paths. To safely negotiate this crossing cyclists have to take the road. Pedestrians have no choice but to take a chance as the alternative crossings are very long detours.	Provision of a Puffin crossing as has been provided on the two approaches on Felixstowe Road.	0	0	1	2	0	0	3	Connectivity and Growth – A crossing already exists, albeit poor quality, therefore the proposal scores a 0 under this category. Modal Shift – Whilst the road itself is well used by cyclists; the proposal is for a high-quality crossing point which will not significantly unlock to the modal shift potential. Optimisation – Despite an existing pedestrian refuge, the road represents a modest barrier between those situated on either side. Improving the existing crossing by making it a high-quality crossing would provide improved cohesion between the cycleways/footways on either side of the road, therefore the optimisation scores a 1.

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												Safety – A crossing on Ransomes Way, which is a busy 40mph road, warrants a score of 2 under safety. Biodiversity – No biodiversity impact. Leisure – No leisure benefit.
Purdis Farm	737	Cycle way approaching Warren Heath Sainsburys roundabout	Also the cycle way approaching Warren Heath Sainsburys roundabout from Felixstowe is poorly maintained (often seriously overgrown) and this encourages cyclists to stay on the road which is not sensible with the road layout at the roundabout.								N/A	Foliage that grows in private land are the responsibility of private landowners. Foliage that grows within the highway boundary is a SCC specific matter and have been shared with SCC for their consideration as the Highway Authority.
Ramsholt	475	Ramsholt to Bawdsey – The stretch of river wall from Ramsholt to Bawdsey on the Deben	There is no public access along this stretch river wall	This should be made available to the public to connect with existing routes and become part of the England Coast Path. This section of river wall is not currently open to the public but could be made a public footpath with a minimum of alteration and expenditure with no inconvenience to the landowners. A Creation Order or Agreement is required. It will have a good deal of support from local residents as well as visitors.	1	0	0	0	0	3	4	Connectivity and Growth – The proposal will create a connection between Ramsholt and Bawdsey. There may be some 'everyday' movement as Ramsholt is within Bawdsey CEVC primary school's catchment area, however it is likely that the proposal will have more leisure value than connectivity and growth value. A score of 1 is deemed reasonable. Modal Shift – As a leisure route, it will unlikely result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – No safety benefit. Biodiversity – It is not clear to what extent work will be required in order to achieve a footpath along the river, however it is likely a neutral score. Leisure – The proposal will likely provide significant leisure benefit as it connects into Bawdsey which, having a beach, will have significant leisure benefit. Also, the route is situated along the River Deben creating a particularly attractive route. A score of 3 is deemed reasonable.
Rendlesham	142	A1152 Rendlesham	I note that there are planned developments for both housing and employment at Rendlesham and Bentwaters and yet there is little or no provision for cycling. There is plenty of space and a golden opportunity to make this area a "mini-holland" by providing Dutch-style cycling infrastructure.	Build grade-separated cycle paths along the main routes into and through both the village and the employment area. Convert the roundabout to a Dutch-style configuration, with proper provision for cyclists and pedestrians. Provide secure cycle parking at all the main facilities in the village (care centre, school, shopping area) and employment area.	2	1	0	3	-2	0	4	The commenter proposes multiple cycleways in and around Rendlesham. For the purpose of this assessment, cycleways along the A1152 connecting into the existing infrastructure along Acer Road and extending south into the Bentwaters entrance, whilst also implementing a dutch style roundabout, will be assessed. Connectivity and Growth – The proposal will likely have somewhat significant connectivity benefits as it will connect into the employment allocation at Bentwaters and into the existing infrastructure through the village centre. Modal Shift – PCT suggests that, if infrastructure is delivered to the highest standard, there would be a resultant small modal shift. Optimisation – The proposal is for new infrastructure and will not, therefore, optimise the existing. Safety – The proposal will likely have safety benefits. The A1152 is a busy 'A' type road with a NSL and removing cyclists off this road, which this proposal will successfully do, warrants the highest score under this category. Biodiversity – The proposal will likely result in the removal of foliage adjoining the road, hence a score of -2. Leisure – No significant leisure benefit.
Rendlesham	158	Rendlesham has no safe walking or cycling connectivity to anywhere else...	Rendlesham is accessible only from the A1152 - all entry/exits are along that road which has no foot/cycle path. There is no signage to indicate cyclists/walkers may be present. The speed limit of 40 stops before Rendlesham Mews - and is frequently exceeded by drivers who presume it's a safe-for-them straight stretch, they can see the upcoming increase of speed permission sign. Vehicles passing the Mews at 60 mph+ makes it unsafe for cyclists to turn into the Mews and lanes beyond.	Create a path along the A1152 to extend from the roundabout to the Mews. Extend the speed limit to 40 all the way to Eyke. This would remove the dangerous 60 stretch that includes turnings to the Mews and to the lanes that lead to Friday Street/the forest on one side and to Rendlesham St Gregory's Church/Campsey Ash/Wickham Market on the other. Put up signage on the A1152 that indicates to drivers that they are passing through a residential area where cyclists and walkers may be present.	2	0	0	3	-2	0	3	Connectivity and Growth – the proposal will provide moderate connectivity and growth benefit as it will connect the residential area of Rendlesham to the employment allocation 'SCLP12.40: Bentwaters Park, Rendlesham' and to the small handful of shops at Rendlesham Mews. Modal Shift – According to PCT, it is unlikely that the proposal will result in significant modal shift. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Despite this section of the A1152 having a 40mph speed limit, it is a straight 'A' type road so speed and volume of traffic is likely high, and it is often used by HGVs, therefore a score of 3 is considered reasonable. Biodiversity – a pedestrian/cycle path will primarily result in the loss of well-kept grass verges, but it will also likely result in the loss of wild verges, small hedges, and other shrubbery. A score of -2 is therefore, considered reasonable. Leisure – the proposal will likely have more connectivity value than leisure value.
Rendlesham	203	Rendlesham to Woodbridge A1152 Road	Provision of a dedicated cycle lane/path. With the intended major housing development at Rendlesham, it will only serve to increase the amount of motorised traffic travelling to and from Woodbridge via Wilford Bridge. This will actively discourage people from cycling.	There is a huge opportunity for a dedicated cycle/footpath lane to be established along this road to encourage people to cycle to/from Woodbridge rather than use their cars. (Similar maybe to the one already in existence between Leiston and Sizewell) There is plenty of room and it could easily connect with other cycle / walking infrastructure at Woodbridge. As well as use for local journeys such as cycling to school it would also be useful for leisure / tourist cycling connecting Woodbridge with the Rendlesham forest area and the coast	3	2	0	3	-3	2	7	Connectivity and Growth – The proposal would connect Rendlesham and Eyke to Melton. As Melton has a number of services that are not available in the other settlements, including a train station, therefore the proposal will likely have significant connectivity and growth benefit. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, it will likely result in a somewhat significant modal shift, hence a score of 2 under this category. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – As the A1152 is an 'a' type road with a NSL, volume and speed of traffic is likely high. Removing pedestrians and cyclists off the road will likely result in safety benefit. Biodiversity – The proposal will likely result in the removal of established hedgerows and trees that adjoin the A1152, therefore a significant negative score is deemed acceptable. Leisure – The proposal will likely have more connectivity and growth benefit than leisure, however connecting into Melton will, subsequently, connect into the PROW network around the River Deben.
Rendlesham	457	Proposed Bentwaters park development area.	Pedestrains walking / cycling across the A1152 from Rendlesham to Bentwaters.Its important that these two developments are 'connected' and not divided in two by the A1152. Crossing an A road on foot is always 'risky' and not safe for children walking to school or trying to access the local facilities within Rendlesham	1) Upgrade the paths at the roundabout to cycle paths or even create a 'dutch style' roundabout such as the one in Cambridge where vehicles are required to give way to Cyclists / Pedestrians.2) Provide a second Pedestrian/cycle crossing point at the end of the existing lane near to the Rendlesham Day Nursery.	2	0	1	2	0	0	5	Connectivity and Growth – Without suitable crossing points the A1152 forms a barrier to the Bentwaters employment area so scores a 2.Modal Shift – Currently, the A1152 has limited cycling and walking, however the roads in Rendlesham opposite do have some higher levels of cycling. However, to get significant modal shift the roads either side of the roundabout need improvement so no score has been given.Optimisation – Currently, there is limited cycling and walking infrastructure so provides limited optimisation without wider improvements, but does warrant a 1. Safety – The junction is busy with traffic and has a national speed limit. Whilst the improvement would only offer benefits to a small section of the road, it is a somewhat significant

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												safety improvement. Biodiversity – No significant biodiversity benefit. Leisure – There are some potential leisure uses in Bentwater, but the overall benefit to Leisure is not likely to be high.
Rendlesham	643	Rendlesham / Ivy Lodge Road	Distance and highway conditions from Rendlesham to Wickham Market station. Currently cycling between the two involves navigating the roundabout at the N end of the village and a 60mph stretch of the B1069, then the full length of Ivy Lodge Road.	Providing pedestrian and cycle access on the estate road within Rendlesham Park / old estate, which would remove the most dangerous part of the journey and also reduce the distance by 25%. Most of the route exists, though may need a new access point from Ivy Lodge Road.	2	1	0	3	-3	1	4	The commenter proposes cyclist and pedestrian access on the estate road within Rendlesham Park / Old Estate, however this will not connect directly into Campsea Ashe. For the purpose of this assessment, access through the estate and the addition of a cycleway along Ivy Lodge Road will be assessed. Connectivity and Growth – The proposal will connect Rendlesham to Campsea Ashe. Both settlements have limited services, however the connection will allow an element of service pooling and Campsea Ashe has a train station, therefore a score of 2 is warranted. Modal Shift – The proposal will provide an alternative to both the B1069 and Ivy Lodge Road. Although PCT suggests that Ivy Lodge Road is not currently well used and infrastructure will unlikely result in a significant modal shift, PCT also suggests that improving the infrastructure along the B1069 will result in a modest modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will provide an alternative route to that of the B1069 and Ivy Lodge Road, which are likely busy and have a NSL. Removing cyclists and pedestrians off road warrants a score of 3 under this category. Biodiversity – It is likely that the proposal will result in the removal of established hedgerows warranting a score of -3 in this category. Leisure – The proposal will connect into a handful of attractive PROWS; however, the proposal will likely have more connectivity and growth benefit. Therefore, a score of 1 is considered reasonable.
Reydon	34	Along the B1127, towards Potters Bridge.	The Suffolk Coastal Path comes onto this busy road and you have to walk along it in order to get to the next footpath past Potters Bridge. You actually have to walk along the road as there is no footpath at the side or anywhere else to walk. It is very scary as it is often very busy with cars and lorries, it is not a straight road either. It is impossible to do with children or dogs without putting them in danger.	I feel it should be possible to make this much safer for everyone to use by having a path alongside the road and not in the road, to join up the different footpaths.	0	1	0	3	-3	3	4	Connectivity and Growth - No significant connectivity and growth benefit Modal Shift - uplift of 41 according to PCT Optimisation - No existing infrastructure in which to optimise Safety - As the road speed is at national speed limit with no road markings or lighting at night a score of 3 is deemed reasonable. Biodiversity - Loss of established Hedge over a long distance is a significant biodiversity impact Leisure - Direct links to Southwold through Reydon which is a key leisure centre.
Reydon	37	Road from A12 Blythburgh to Southwold. and most Suffolk B roads.	Country roads not suitable for cyclists. Long hold ups behind cyclists who cannot be safely overtaken on narrow winding roads with or without opposing traffic. Put simply the increase in leisure cycling is a menace to other traffic on our local roads, causing traffic jams, prolonged journey times and inefficient use of fuel when stuck in low gears behind cyclists, and should not be encouraged. People living in the country need to get about by car. We do not need people 'playing' on our roads,	Separate cycle ways BUT not along existing footpaths. The Sustrans cycle path along Halesworth Millenium Meadow is a classic example of pedestrians and cyclists not mixing. Cyclists all too often approach walkers (often with dogs) from behind at great speed and give no warning as they hurtle past nearly injuring pedestrians and their pets. It became so bad at one stage that we stopped walking there.	0	0	0	3	-3	3	3	Connectivity and Growth - This improvement will create a new off road connection from Southwold to the North of Blythburgh. However a neutral score has been allocated due to the fact that the route will end at the A12 and does not completely connect users to Blythburgh. Modal Shift - no significant modal shift benefit. Optimisation - This improvement will look to create a new piece of infrastructure and therefore does not score under this category. Safety - The A1095 is an often busy road with areas of national speed limit. An off road cycle path would alleviate this risk completely. Biodiversity - The A1095 is lined with mature hedges and trees which would be impacted by the creation of this route. The loss of the hedge and trees would be significantly detrimental to the biodiversity of the surrounding area. Leisure - Southwold is considered to be a tourism and leisure hotspot and any new connection to Southwold will have a significant benefit to leisure.
Reydon	71	Jermyns road, entire length	Jermyns road is a road with Reydon primary school just off it, it is very dangerous with fast traffic. My son rides his bike to school but I am fearful of the traffic and would appreciate some traffic calming measures, as in most areas with a school on/near the road	Traffic calming, 20 mph limit							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Reydon	103	southwold and reydon main roads	Congestion in the tourist season makes it difficult for cyclists.	More cycle lanes.	0	0	0	3	-3	3	3	Connectivity and Growth - This improvement will create a new off road connection from Southwold to the North of Blythburgh. However a neutral score has been allocated due to the fact that the route will end at the A12 and does not completely connect users to Blythburgh. Modal Shift - no significant effect. Optimisation - This improvement will look to create a new piece of infrastructure and therefore does not score under this category. Safety - the A1095 is an often busy road with areas of national speed limit. An off road cycle path would provide safety benefit. Biodiversity - The A1095 is aligned with mature hedges and trees which would be impacted by the creation of this route. The loss of the hedge and trees would be significantly detrimental to the biodiversity of the surrounding area. Leisure - Southwold is considered to be a tourism and leisure hotspot and any new connection to Southwold will have a significant benefit to leisure.
Reydon	439	Wangford Road and Halesworth Road	1. There is no East West pedestrian access between Reydon and the A12 north of the estuary. Walking on either road is extremely dangerous as the roads are relatively narrow and traffic will only increase as more houses are built in Reydon (200 at Copperwheat with no possibility of improving the road infrastructure); double decker buses at speed; blind corners. 2. From the Hen Reed Beds to the A12 old footpaths have disappeared under the estuary. A solution needs to be found to reach Blythburgh.	1. Established hedges mean that road verges cannot be widened to create footpaths. The only solution is to incentivise the landowners to create footpaths inside the field hedges (c 1m wide?). This may be doable at national level as EU subsidies are replaced by a new UK system; but local initiatives need to be developed. 2. This requires negotiation with local landowners. To be born in mind when SCC has any dealings with landowners.	3	0	0	3	-1	3	8	Connectivity and Growth - This improvement will create a new off road connection from Southwold to Blythburgh which will be very beneficial for pedestrians. Modal Shift - No effect. Optimisation - This improvement will look to create a new piece of infrastructure and therefore, does not score under this category. Safety - The A1095 is an often busy road with areas of national speed limit. An off-road cycle path would provide safety benefit. Biodiversity - The A1095 is aligned with mature hedges and trees which would be impacted by the creation of this route. The loss of the hedge and trees would be significantly detrimental to the biodiversity of the surrounding area. However, this comment refers to utilising the existing agricultural field behind the hedge. The loss of the agricultural field space will have a small impact on biodiversity but it will be much less impactful than the removal of the existing hedge. Leisure - Southwold is considered to be a tourism and leisure hotspot and any new connection to Southwold will have a significant benefit to leisure.
Reydon	510	Wrentham Road entering Reydon	Footpath ends before the Reydon Business Centre, meaning there is no safe way to walk between the Business Centre and Reydon and Southwold.	Create a continuous length of pavement of pavement safely linking pedestrians to both the business centre and the bus stop on the east side of the road. Create a	2	1	0	1	-1	0	3	Connectivity and Growth - Extending the existing footway to link to the business park will provide a significant improvement to connectivity and growth. Modal Shift - PCT score of 51, connecting to the business park would have a benefit to commuters. Optimisation - The existing path will not be improved, only extended. Safety - The

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			There is no safe place to wait for the bus going into Southwold from the Reydon Business Centre.	bus waiting area on the verge by the bus stop on the east side of the road.								improvement will remove pedestrians off the road and the waiting area will increase the safety of people at the bus stop. There is a speed limit of 30mph on this stretch of road. Biodiversity - Extending the footpath will require the removal of grass verge and potentially cutting back of existing hedge. Leisure - This improvement will mainly have impact on commuting rather than leisure.
Reydon	675	Rear of St Felix School	A new cycle route from the rear of St. Felix School to Southwold is worthy of examination although there is no preferred route, per se.		1	0	0	2	-2	2	3	Connectivity and Growth - A new connection for cycling will be created between St Felix school and Southwold High Street (via golf course). There is already road side pavements along the A1095 that is suitable for walking but not for cycling. Modal Shift - No effect. Optimisation - No score as the suggestion is for a new piece of infrastructure. Safety - The A1095 is the main road into Southwold and therefore is expected to get very busy at peak times of the year. This suggestion provides an off-road route for cyclists which provides safety benefits. Biodiversity - Although not conclusive to tell without a site visit, a completely off-road route will require the removal of existing vegetation. Leisure - The created route would provide an attractive, off-road route to Southwold High Street for cyclist.
Rushmere St Andrew	43	Junction of Linksfield and Woodbridge Road to Ipswich border.	An adequate cycle route runs along the south side of the A1214 Woodbridge Road until Linksfield junction but cyclists riding to Ipswich must then join the busy caridgeway or illegally use the footway. This is a serious gap in the route network.	Widen footway onto common to allow space for shared use path with dividing line. Ideally allow bothway cycle use so that east bound riders from Glenavon Road do not have to cross Woodbridge road at Glenavon Road and again at Beach Road	3	3	0	2	0	0	8	Connectivity and Growth – The proposed connection resides along the Ipswich – Melton key corridor and will help in connecting multiple settlements, therefore a score of 3 is deemed acceptable. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, there will be a significant modal shift along this section of the A1214, hence a score of 3 under this category. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The end of the existing cycle infrastructure east of Rushmere Heath results in cyclists utilising the A1214 which, despite being a 30mph road, is relatively busy. Removing cyclists off the road scores a 2 under this category. Biodiversity – The proposal will likely result in the removal of the managed grass verges adjoining both sides of the A1214 along this section, however it is only a small section when considered in isolation. Leisure – The route will likely have more connectivity value than leisure.
Rushmere St Andrew	85	A1214 between Playford Road and Bent Lane	No cycle lane but one exists to the east and to the west	Widen footways to create dedicated cycle path	3	3	0	2	-1	0	7	Connectivity and Growth – The proposed connection resides along the Ipswich – Melton key corridor and will help in connecting multiple settlements, therefore a score of 3 is deemed acceptable. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, there will be a significant modal shift along this section of the A1214, hence a score of 3 under this category. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The ending of the existing cycle infrastructure east of Rushmere Heath results in cyclists utilising the A1214 which, despite being a 30mph road, is busy. Removing cyclists off the road scores a 2 under this category. Biodiversity – The proposal will likely result in the removal of the managed grass verges adjoining both sides of the A1214 along this section, however it is only a small section when considered in isolation. Leisure – The route will likely have more connectivity value than leisure.
Rushmere St Andrew	141	Rushmere Heath	Currently cycling along the footpath is not permitted here. There is a clear opportunity for a traffic-free route lining Kesgrave/Grange Farm with The Hospital and onward cycling route to the centre of Ipswich.	Install a surfaced cycle track alongside the footpath.	3	3	0	1	-2	2	7	Connectivity and Growth – The proposal would connect the existing bridleway to the east into Ipswich, subsequently creating a connection between Kesgrave and Ipswich. The connection also resides along the Ipswich to Melton key corridor. A score of 3 is considered reasonable. Modal Shift – The proposal will provide an alternative to the A1214 which, according to PCT, would result in a significant modal shift if infrastructure is delivered to the highest standard. Therefore, a score of 3 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal would provide an alternative to the A1214 which, although with existing infrastructure along some stretches of the road lacks infrastructure elsewhere. The A1214, despite having a 30mph speed limit, is a busy fast road. A score of 1 is deemed reasonable. Biodiversity – Widening of the footpath to create a bridleway will likely result in the removal of wild verges, therefore a score of -2 is deemed acceptable. Leisure – The proposal will create a particularly attractive route for leisure cycling, therefore a score of 2 is deemed reasonable.
Rushmere St Andrew	237	Bixley Drive / Gwendoline Road, Ipswich	No obvious signage to show that Gwendoline Drive & Chatsworth Drive is actually a cycle route to Ipswich & NCN 1	Some better cycle signage is all that is required.	0	0	1	0	0	0	1	Connectivity and Growth – No connectivity and growth benefits. Modal Shift – The change is not considered to create significant modal shift. Optimisation – Although the route is not improved, the addition of the signage represents a modest optimisation so scores 1 point. Safety – No significant safety benefits. Biodiversity – No biodiversity impact. Leisure – The route appears more utilitarian as opposed to an attractive destination and whilst it eventually reached Ipswich which has leisure benefits the overall leisure impact is considered minor unless part of a wider strategy.
Rushmere St Andrew	242	Cycle path and Footpath from Salehurst Road to Bucklesham Road	Cyclists have worn away much of the surface making it very hazardous for walking and almost impossible with a mobility scooter	From Salehurst Road the first section is either concrete or tarmac. After that it is basically compressed soil. This route is very popular and would benefit from a complete overhaul to establish a good quality walking route which can also be used safely by those with mobility issues.	0	0	1	0	0	0	1	Connectivity and Growth – No significant connectivity or growth benefit. Modal Shift – The alterations would not be expected to create a significant modal shift. Optimisation – The improvements will make the path more inclusive. This will provide an improvement to a path that is already off-road meaning it is considered one point. Safety – The issue is a matter of access and usability over safety. Biodiversity – No significant biodiversity impact. Leisure – This path does not have high leisure value, therefore there is limited leisure benefit.

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Rushmere St Andrew	432	East/west footpath across Rushmere Common.	For many cycling between Kesgrave and Ipswich is not seen as safe due to the section of route between Linksfield and where the ring road starts to the west.	Provide a section of shared use path east/west across the common.	3	2	1	-1	-1	1	5	Connectivity and Growth: Though the two routes across Rushmere Common are currently of footpath status, their upgrade and (re)surfacing has been recommended in the Strategy as part of the Ipswich to Melton Key Corridor; the improvement of the east to west route across the Common (to which this comment relates) has been given 'very high' priority status, and the north-west to south-east route has been given 'high' priority status. The delivery of at least one of these connections across the common is of high strategic importance for the Strategy, due to Rushmere Common's critical role in the delivery of the Long Strops Bridleway route between Rushmere and Martlesham Heath, which is arguably the 'key stone' to the delivery of the Ipswich to Melton Key Corridor recommendations. For this reason a full score of three is given. Modal Shift: Though PCT cannot be used on off-road routes, it is anticipated a high-quality route through the Common would be useful for commuters - particularly between east Ipswich and Martlesham, with key employers/institutions such as the Ipswich Hospital and the two high schools (Copleston and St Alban's) located within close range of this east-to-west route, other recommended infrastructure on the A1214, and existing infrastructure in this area. It is therefore anticipated that it will have high modal shift value. However, it is understood that Rushmere Common is already well cycled despite cycling currently being prohibited, and therefore the actual uplift to be anticipated should be accordingly adjusted for scoring under this category. A score of 2 was given. Optimisation: Optimisation score of 1 given as the routes are already segregated from cars, but the width and segregation is unsuitable for cycling/poor quality. Safety: A -1 score is given for safety as the Common is used as a golf course - encouraging more use of this route will require suitable management. Biodiversity: a -1 score for biodiversity is given due to the necessary loss of a small amount of heathland (and therefore plants such as gorse bushes) to facilitate wider and more appropriately surfaced cycle route(s) across the Common. Leisure: A modest leisure benefit is expected.
Rushmere St Andrew	516	Woodbridge Road across Rushmere Common	The whole of Woodbridge Road and Main Road Kesgrave is too narrow to accommodate both cars and cyclists safely. To improve the situation widening the footpath across Rushmere Common so it can take cyclists and pedestrians. Alternatively/additionally find another route across the common which can be linked to longstrops in Kesgrave which if upgraded (surfaced) would provide a route and not encroach on any common land.	Widening the footpath across Rushmere Common so it can take cyclists and pedestrians. Alternatively/additionally find another route across the common. There is a bridle way across the common which can be linked to longstrops in Kesgrave which if upgraded (surfaced) would provide a route and not encroach on any common land.	3	2	1	-1	-1	1	5	Connectivity and Growth: Though the two routes across Rushmere Common are currently of footpath status, their upgrade and (re)surfacing has been recommended in the Strategy as part of the Ipswich to Melton Key Corridor; the improvement of the east to west route across the Common (to which this comment relates) has been given 'very high' priority status, and the north-west to south-east route has been given 'high' priority status. The delivery of at least one of these connections across the common is of high strategic importance for the Strategy, due to Rushmere Common's critical role in the delivery of the Long Strops Bridleway route between Rushmere and Martlesham Heath, which is arguably the 'key stone' to the delivery of the Ipswich to Melton Key Corridor recommendations. For this reason a full score of three is given. Modal Shift: Though PCT cannot be used on off-road routes, it is anticipated a high-quality route through the Common would be useful for commuters - particularly between east Ipswich and Martlesham, with key employers/institutions such as the Ipswich Hospital and the two high schools (Copleston and St Alban's) located within close range of this east-to-west route, other recommended infrastructure on the A1214, and existing infrastructure in this area. It is therefore anticipated that it will have high modal shift value. However, it is understood that Rushmere Common is already well cycled despite cycling currently being prohibited, and therefore the actual uplift to be anticipated should be accordingly adjusted for scoring under this category. A score of 2 was given. Optimisation: Optimisation score of 1 given as the routes are already segregated from cars but the width and segregation is unsuitable for cycling/poor quality. Safety: A -1 score is given for safety as the Common is used as a golf course - encouraging more use of this route needs to be managed. Biodiversity: a -1 score for biodiversity is given due to the necessary loss of a small amount of heathland (and therefore plants such as gorse bushes) to facilitate wider and more appropriately surfaced cycle route(s) across the Common.
Rushmere St Andrew	577	A1214 cycle route through Kesgrave plus other locations	Like many of the cycle routes alongside roads in Suffolk cyclists need to give way at junctions. This requires looking over the right shoulder to look for cars turning left. This is dangerous and is also a major inconvenience having to slow down or stop at junctions. If cycling on the road the cyclist like vehicles has a right of way across the junction. Also pedestrians have a right of way at junctions according to the highway code.	I lived in Munich for 2 years and cycled there. Cycle routes had a right of way over side roads that they crossed. It worked well all vehicles gave way as needed.	0	0	1	0	0	0	1	Connectivity and Growth – The proposed alteration does not create additional connectivity. Modal Shift – The existing infrastructure remains so no modal shift. Optimisation – Currently cyclists are regularly forced to stop to give way to motorists so whilst it is not improving the type of existing infrastructure, it will optimise its use, therefore a score of 1 is deemed reasonable. Safety – No significant safety benefit. Biodiversity – No biodiversity impact. Leisure – No significant leisure benefit.
Rushmere St Andrew	631	A1214 across Rushmere Heath	Key section of route in the corridor between Ipswich - Kesgrave - Woodbridge. Cyclists have no alternative routes available which are safe and convenient e.g. the footpath across the Heath is a footpath - a sandy track across which there is no legal right to cycle and there is also a risk of being hit by golf balls. And the route via Rushmere village is a long detour. If we are to encourage more people to cycle then this key section of route needs some cycling provision. It's a missing link.	Widen the A1214 here to create dedicated cycle lanes on either side of the road, segregated from the pedestrian footway. Widen the footway on either side so it's suitable for mobility scooters, wheelchairs, buggies etc. Plant suitable trees along the edge of the footway and Heath - Birch, Oak etc? and a shrub layer - gorse? to create an attractive and sheltered route for pedestrians and an attractive feature in the landscape. I think the land either side of the A1214 here is Common Land - if so, then can the Council find an area of land, comparable in size and in quality in terms of wildlife/landscape quality and public amenity/access in East Suffolk to dedicate as Common Land to substitute/compensate for that taken? And as an enhancement, perhaps East Suffolk could discuss with the owners any appropriate support for wildlife e.g. a wildlife tunnel underneath the A1214 road if helpful for	3	3	2	2	-2	1	9	Connectivity and Growth: The Strategy recommends a cycling/walking track along the northern edge of the A1214 between the junction with Playford Road and (at least) Doctor Watson's Lane. Cycle Lanes would be a less efficient use of space, less segregated from vehicles, less flexible and may cause more loss of high biodiversity value heath/scrubland on the Common and on the land north of the Common than a track. However, both options would have high C&G value, due to the lack of infrastructure along the northern edge of Rushmere Common. Full score of three is given. Modal Shift: High potential for MS, so full score of three is given. Optimisation: As there is currently no infrastructure for cycling at this point, but segregated cycle lanes are not as effective as pedestrian/cycle tracks, and do not provide pedestrian infrastructure, a score of two is given. Safety: See O - score of 2 is given for this reason. Biodiversity: Score of -2 given for biodiversity due to the loss of potentially difficult to replace (and mitigate the effects of) Heathland, which is limited in this area; unknown if Rushmere Common has a supportive relationship with the Ipswich Heaths SSSI, which principally protects the silver studded blue butterfly.

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
				connectivity for amphibians /reptiles other creatures in lowland heath habitats or other support?								
Rushmere St Andrew	279a	Land allocated for Housing 'Humber Doucy Lane & Rushmere'	Land allocated for housing will increase the number of vehicles on the local roads particularly 'Tuddenham Road' & 'Humber Doucy Lane', this already a cut through road, but also popular with cyclists travelling out of Ipswich towards Tuddenham and the villages beyond. There is limited pavement and no cycle lane/protection along its route.	Humber Doucy lane could be widened to incorporate a dedicated footpath / cycle track connecting the development with Ipswichs cycle infrastructure.	1	1	3	3	-2	0	6	See allocation recommendations for full analysis, abridged version included below. Connectivity and Growth: Land north of Humber Doucy Lane is set to come forward between 2022-2036 for a total of 600 homes, and nearby the Ipswich Garden Suburb is planned for 3,500 homes, schools, shops and community infrastructure. Though Tuddenham Road and Humber Doucy Lane are currently largely leisure cycled, and minimally walked, their relevance as routes and connectors into north/central Ipswich and to the Ipswich to Melton Key Corridor for Martlesham and Woodbridge access will increase. In tandem with other cycling and walking infrastructure improvements in this area to LTN 1/20 standards, this will likely increase beyond current projections. However, a more comprehensive approach than the proposal provided here is required to realise this. Based on the provision of only a cycle/track to and then a shared path along Humber Doucy Lane's edge to the junction with Sidegate Lane, the connectivity and growth benefits are likely to be relatively small. A score of 1 is given. Modal Shift: PCT (based on 2011 Census commuter data) cannot be used in this instance as it cannot factor in the growth planned for. Officer judgement is, on its own, an uplift score of 1. Optimisation: Full score of 3 as there is scope for full segregation throughout this connection. Safety: Full score of 3 as there is scope for full segregation throughout this connection. Biodiversity: Score of -2 given for potential loss of native hedgerow (if delivered on east side) or trees (if delivered on west side). Leisure: 0
Rushmere St Andrew	279b	Land allocated for Housing 'Humber Doucy Lane & Rushmere'	Land allocated for housing will increase the number of vehicles on the local roads particularly 'Tuddenham Road' & 'Humber Doucy Lane', this already a cut through road, but also popular with cyclists travelling out of Ipswich towards Tuddenham and the villages beyond. There is limited pavement and no cycle lane/protection along its route.	There is an opportunity to upgrade the bridleway at the end of Tuddenham lane to provide a safe cycling and walking route to Tuddenham avoiding 'Tuddenham Main Road' which is a commuter route into Ipswich for cars.	1	0	0	3	-1	3	6	Connectivity and Growth: This route has the potential to create a connection (from Tuddenham St Martin) through and down to Colchester Road (for Ipswich) and Woodbridge Road (for the Ipswich to Melton Key Corridor to Martlesham/Woodbridge) in an area where there is currently no walking or cycling infrastructure, or where it does occur, does not meet minimum standards of accessibility. However, it would benefit a small number of people (the Tuddenham St Martin population), and is unlikely to pass the BCR test for delivery. Tuddenham does have a very small allocation of 25 dwellings in the Suffolk Coastal Local Plan, which could potentially feed CIL into a lower-cost off-road route (i.e. suitable for mountain bikes, without bound surfacing) if there was a lot of community support for it. Also, post delivery of the Ipswich Garden Suburb, and/or the further expansion of Tuddenham, a connection to Tuddenham may become increasingly relevant, and therefore able to achieve a BCR score in favour of delivery. A score of 1 is given due to the relevance in the absence of any infrastructure, but relatively low population to benefit from the scheme.
Rushmere St Andrew	41a	A1214 Rushmere / Kesgrave	Great historic cycle lane adjacent to this road that would be greatly improved by changed priorities on minor road junctions to prioritise cycles. The route reduces in width to an ordinary (shared) pavement at Rushmere Heath creating a significant gap in infrastructure.	Changed priorities on side roads and new, protected cycle lane at Rushmere Heath.	0	0	1	0	0	0	1	Connectivity and Growth – The proposed alteration does not create additional connectivity. Modal Shift – The existing infrastructure remains so no modal shift. Optimisation – Currently cyclists are regularly forced to stop to give way to motorists so whilst it is not improving the type of existing infrastructure, it will optimise its use, therefore a score of 1 is deemed reasonable. Safety – No significant safety benefit. Biodiversity – No biodiversity impact. Leisure – No significant leisure benefit.
Rushmere St Andrew	41b	A1214 Rushmere / Kesgrave	Great historic cycle lane adjacent to this road that would be greatly improved by changed priorities on minor road junctions to prioritise cycles. The route reduces in width to an ordinary (shared) pavement at Rushmere Heath creating a significant gap in infrastructure.	This is an alternative suggestion made by an officer of East Suffolk Council. Rather than changing priorities improve the infrastructure to a suitable width between Holly Road and Elma Road	0	3	2	0	0	0	5	Connectivity and Growth – The infrastructure already exists; therefore, no new connections are made. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, there would be a significant modal shift. A score of 3 is considered reasonable. Optimisation – The existing shared path is extremely narrow along this section of the A1214, therefore widening to an LTN 1/20 standard is considered a 2-point optimisation. Safety – The issue raised is a matter of access and usability over safety. Biodiversity – Widening the existing path would likely result in the removal of managed grassed areas adjoining it, however the loss is not deemed significant. Leisure – The connection already exists so it is unlikely that the improvement will have any leisure benefit.
Rushmere St Andrew	630a	A1214 junction with Bent Lane and Linkfield	1) Pedestrians find it difficult to cross the A1214 here - there are lots of people including dog walkers going to and from Rushmere Heath. There are also people trying to cross here to access the bus stops. 2) Cyclists find it difficult to turn right into Bent Lane (if travelling from the Woodbridge direction). Turning right into Bent Lane involves sitting in the middle of the road waiting for a gap in the oncoming traffic. Feels very unsafe.	Some redesign of the junction to slow traffic down and enable people to cross the road/enable cyclists to turn right into Bent Lane. Perhaps a toucan crossing? It's not enough to put in a right turn lane and traffic islands as experience at the A1214 / Cambridge Road junction and A1214 / Edmonton Road junction indicates that motorists rarely give way and you can wait in the middle of the road a very long time for a gap in the traffic. It feels unsafe. Some priority for cyclists and pedestrians would be welcome. They seem to always be at the bottom of the pile.	1	0	0	1	0	2	4	Connectivity and growth - A toucan crossing will provide modest connectivity and growth benefit. Modal Shift - On its own the crossing point is unlikely to have a significant benefit. Optimisation - This is not considered to significantly optimise the current infrastructure. Safety - The provision of the crossing point will have a modest benefit in crossing a potentially busy road. Biodiversity - No biodiversity impact. Leisure - Its position close to Rushmere Common and with connections into Ipswich is worthy of a reasonable score under leisure.
Saxmundham	33	Sailors' Path, Snape	Too many cyclists who are so quiet that you don't hear them approaching. They don't appear to have a bell, so they shout at you to get out of the way. Is this a designated cycle track, or simply for pedestrians. Cyclists have already taken the roads and pavements, now they want the FOOTPATHS.	Please clarify which paths are purely for pedestrians by marking on signs.	0	0	0	0	0	0	0	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – No significant modal shift benefit. Optimisation – No significant optimisation benefit. Safety – Whilst the proposed signs may reduce cyclist and pedestrian conflict the improvement to safety is limited. Biodiversity – No significant biodiversity benefit. Leisure – if cyclists are misusing the path this may affect the enjoyment for walkers, however any existing rules should be adhered to anyway and signs on their own are unlikely to represent a significant leisure benefit.

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Saxmundham	39	B1121 main road linking Benhall, Saxmundham, Kelsale	Lack of safe pedestrian/cycling route between Benhall, Saxmundham, Kelsale, Lack of cycling infrastructure (signs, secure parking, cycle lanes) East Suffolk Council, Suffolk County Council Highways Dept, Planning Dept do not seem to communicate with each other - a perfect example of this is the new train station in Saxmundham has no provision for secure bicycle parking.	The 3 Communities Link project report was completed in 2017 - it detailed a safe route between Benhall, Saxmundham, Kelsale for pedestrians and cyclists. It also linked to the local schools and Saxmundham railway station. The report is currently sitting with Suffolk County Council and has been included in their list of 100 cycling projects to be delivered in the next 5 years (see EADT article.) The report has been ratified and costed by SCC/Highways and is still awaiting funding. It is an "oven-ready" solution to the transport infrastructure issues in and around Saxmundham I am the author of the report file:///media/fuse/drives-234088169dc1f109c9a130868367d4ad/root/THE%203%20COMMUNITIES%20LINK%20Impact%20Audit%20&%20Report.pdf Our FB page: https://www.facebook.com/SaxTCCFocusGroup	2	1	0	3	-3	3	6	The commenter proposes implementing the cycle route improvements suggested within the 3 Communities Link Project report. Route improvements (Benhall – Saxmundham – Kelsale) include a cycle path from School Lane junction to Saxmundham entrance; unbound surfacing from Saxmundham entrance (south) utilising the existing path to Free School; and cycle track alongside Main Road between Brook Farm Road and Low Road. Connectivity and Growth – The proposal will likely have more leisure benefit than connectivity benefit, however the proposal does connect Kelsale and Benhall, which are reasonably small settlement areas and have limited services, to the market town Saxmundham. As the proposal will allow an element of service pooling, a score of 2 is deemed reasonable. Modal Shift – If the proposal can be implemented at the highest standard, the infrastructure will likely result in, according to PCT, a small modal shift. Therefore, a score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will give an alternative to cycling on the B1121 which has a NSL and is likely busy, therefore a score of 3 under this category is considered reasonable. Biodiversity – The route will result in the loss of grassed areas, established hedgerows, and foliage, therefore a negative score under this category is considered reasonable. Leisure – This route forms part of the leisure key corridor and will, therefore, have significant leisure benefit.
Saxmundham	192	B1121 between Benhall Saxmundham and Kelsale	Three villages cycle path	the three villages cycle path should be put in place ASAP	2	1	0	3	-3	3	6	The commenter proposes implementing the cycle route suggested within the 3 Communities Link Project report. Cycle route suggestion includes cycle path from School Lane junction to Saxmundham entrance; unbound surfacing from Saxmundham entrance (south) utilising the existing path to Free School; and cycle track alongside Main Road between Brook Farm Road and Low Road. Connectivity and Growth – The proposal will likely have more leisure benefit than connectivity benefit, however the proposal does connect Kelsale and Benhall, which are reasonably small settlement areas and have limited services, to the market town Saxmundham. As the proposal will allow an element of service pooling, a score of 2 is deemed reasonable. Modal Shift – If the proposal can be implemented at the highest standard, the infrastructure will likely result in, according to PCT, a small modal shift. Therefore, a score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will give an alternative to cycling on the B1121 which has a NSL and is likely busy, therefore a score of 3 under this category is considered reasonable. Biodiversity – The route will result in the loss of grassed areas, established hedgerows, and foliage, therefore a negative score under this category is considered reasonable. Leisure – This route forms part of the leisure key corridor and will, therefore, have significant leisure benefit.
Saxmundham	226	A12 / B1119 Junction Saxmundham	Crossing the A12 by bicycle or on foot at this junction is difficult /dangerous for any cyclist or pedestrian regardless of age and experience, there is no segregated provision. The B1119 Rendham to Sax road has effectively been cut in half by the A12.	Provide a dedicated pedestrian/cyclist crossing point with separate foot/cycle path linking the Rendham side of the A12 with the Saxmundham side. Enabling anyone from the Rendham direction to safely cycle/walk to Saxmundham.	3	0	0	3	0	1	7	Connectivity and Growth – the road represents a modest barrier between those situated on either side and there does not appear to be existing crossing points. Furthermore, the A12 will be a significant barrier between the mixed-use allocation SCLP12.29 to the east of the A12 and the employment allocation SCLP12.29 to the west of the A12, therefore the provision of a crossing for use by both cyclists and walkers would be beneficial. Modal Shift – currently low numbers along the A12 on PCT, therefore there is insufficient evidence that the proposal would lead to a modal shift. Optimisation – the crossing point does not appear to improve existing infrastructure. Safety – This section of the A12 is wide, straight, and has an NSL. The proposal of a crossing point, if delivered to the highest standard, will likely have safety benefits, therefore a score of 3 under 'Safety' is considered reasonable. Biodiversity – there are no significant biodiversity impacts. Leisure – The suggestion has a small leisure benefit as there are a couple PROWs on both sides of the road, therefore a crossing would connect them.
Saxmundham	411	There needs to be a safe cycle route between Benhall and Saxmundham, and preferably on to Kelsale	The B1121 between Benhall and Saxmundham is dangerous and absolutely unwelcoming for cyclists. A safe and properly constructed cycle path is needed	There is a public footpath on the inside of the hedge for much of the way. This should be made into a good quality cycle path as well as footpath. The 3C cycle route from Benhall to Sax to Kelsale was developed as concept several years ago and the Sax-Benhall part should be implemented as it forms part of site allocated for South Saxmundham Garden Neighbourhood and fits the policy for the site perfectly (including promoting cycling). Photo shows road looking south from South Entrance Saxmundham, with footpath parallel behind the hedge.	2	1	0	3	-3	3	6	The commenter proposes the implementation of the Benhall to Saxmundham route within the 3 Communities Link Project report. The cycle route includes a cycle path from School Lane junction to the Saxmundham entrance (south). Connectivity and Growth – The proposal will likely have more leisure benefit than connectivity benefit as it forms part of the leisure key corridor, however the proposal will connect Benhall, which is a small settlement with limited services, to Saxmundham. As Benhall does have services, although limited, there isn't going to be significant 'everyday use', but the proposal will allow an element of service pooling. Therefore, a score of 2 is considered reasonable. Modal Shift – According to PCT, if the cycling and walking infrastructure is delivered to the highest standard, the proposal will result in a small modal shift. Therefore, a score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal has safety benefits as the proposal provides an alternative to cycling on the B1121, which has a national speed limit and likely has high volumes of traffic, therefore a score of 3 under this category is considered reasonable. Biodiversity – The proposal will likely result in the removal of the established hedgerow adjoining the B1121 and does, therefore, score a significant negative score under this category. Leisure – This route forms part of the leisure key corridor and will, therefore, have significant leisure benefit.
Saxmundham	421	Many of the pavements in Saxmundham (particularly the high street and the	The pavements in Saxmundham are in many places very narrow and not fit for purpose. In many places they are too narrow for mobility scooters and pushchairs or	Making a section of the high street pedestrians/deliveries and disabled access only.	1	0	0	2	0	1	4	Connectivity and Growth – Whilst the town centre, or the high street, is the destination in itself, the modal filter would create cycle access to the shops situated within it. As the connectivity is limited to the town centre, however, only a small

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		roads off the cross roads at the traffic lights on town.	even for two pedestrians to pass safely. This is especially true on the high street.									score is deemed reasonable under this category. Modal Shift – The route is unlikely to be completely traffic free so the modal shift to the lower standard does not represent as a significant gain. A neutral score is considered reasonable. Optimisation – This doesn't optimise existing cycling infrastructure nor provide improvements to the pavements along this road; therefore, it does not score under this category. Safety – Despite the road having a 30mph speed limit, it is narrow in places and is likely busy as it is a 'B' type road, therefore a score of 2 is deemed reasonable. Biodiversity – There are no biodiversity impacts. Leisure – Again, although the modal filter would create cycle access to the cafes, and other small leisure attractions, it is limited to the town centre, therefore a score of 1 is considered reasonable.
Saxmundham	422	The B1121 between Kelsale, Saxmundham and Benhall	Lack of safe cycling route along this road which links two primary schools, two villages and the town centre and is used by motorists and lorries to access town/A12. It also has a very narrow pavement between Benhall and Saxmundham which forces pedestrians very close to the fast moving traffic.	Implementation of the Three Communities Link proposal. Providing an inclusive and safe cyclist and pedestrian route for vulnerable road users including those with children, pushchairs and mobility scooters. The plan already exists, just requires funding.	2	1	0	3	-3	3	6	The commenter proposes implementing the cycle route suggested within the 3 Communities Link Project report. Cycle route suggestion includes cycle path from School Lane junction to Saxmundham entrance; unbound surfacing from Saxmundham entrance (south) utilising the existing path to Free School; and cycle track alongside Main Road between Brook Farm Road and Low Road. Connectivity and Growth – The proposal will likely have more leisure benefit than connectivity benefit, however the proposal does connect Kelsale and Benhall, which are reasonably small settlement areas and have limited services, to the market town Saxmundham. As the proposal will allow an element of service pooling, a score of 2 is deemed reasonable. Modal Shift – If the proposal can be implemented at the highest standard, the infrastructure will likely result in, according to PCT, a small modal shift. Therefore, a score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal will give an alternative to cycling on the B1121 which has a national speed limit and is likely busy, therefore a score of 3 under this category is considered reasonable. Biodiversity – The route will result in the loss of grassed areas, established hedgerows, and foliage, therefore a negative score under this category is considered reasonable. Leisure – This route forms part of the leisure key corridor and will, therefore, have significant leisure benefit.
Saxmundham	483	9 points on Saxmundham bypass: TM380656 Kelsale FP 10; TM373646 Kelsale FP 38; TM376644 Kelsale FP 1; TM375639 Kelsale FP 3; TM375636 Sax FP 5; TM375632 Sax FP 11; TM376630 Sax FP 13; TM377621 Benhall FP 22; TM378616, Benhall BR 25	Paths severed by A12 bypass with no thought for walkers. Crossings lethal- single carriageway with 60 speed limit. No warnings to motorists- no central refuges- in two instances (TM 376 644 and TM 375 636) one must climb over Armco-type barriers on each side. TM 375 632 crossing is oblique requiring a considerable walk alongside the carriageway to cross it at a right angle. Traffic increased many fold by new housing on western edge of the town. Sizewell C traffic would exacerbate more.	These crossings must be made safer and easier through speed limits, warning signs to motorists, provision of gaps in the Armco barriers and the installation of central refuges and waiting areas.	2	0	0	2	0	1	5	Connectivity and Growth – The proposal of central refuges along this stretch of the A12, which is a significant barrier, provides modest connectivity benefits because it will provide cohesion between allocation SCLP12.29, an employment allocation, to the rest of Saxmundham. However, as the proposal is for low quality crossing points, a score of 2 is considered reasonable. Modal Shift – There is insufficient evidence that the proposal would lead to modal shift. Optimisation – Providing new infrastructure does not represent an optimisation. Safety – This section of the A12 is wide, straight, and has a national speed limit; therefore, the suggestion will likely have a modest safety benefit. However, a crossing point does not remove the cyclists/pedestrians off the road, therefore a score of 2 is considered reasonable. Biodiversity – There are no significant biodiversity impacts. Leisure – The PROW pathways are largely used for Leisure purposes and there are no crossing points along this stretch of the road, therefore having direct crossing points available for the PROWs will benefit its leisure purposes.
Shipmeadow	107	Between Low Road and Puddingmore / Ballygate	Busy road between Beccles and Bungay with no cycleway and only a broken bit of pavement could see a combined cycle/foot path added (as long as it doesn't destroy hedgerows / trees)	Low Road is an ideal and pleasant route into Bungay that avoids the hills and much of the main road from Beccles. However, to get to Low Road from Beccles there is no cycle path and only a patchy / unsuitable pedestrian path.	3	1	0	3	-2	2	7	Connectivity and Growth - Beccles and Bungay currently are poorly connected for cyclists but represent large settlements with good services. In addition this is considered a key corridor so a top score is provided. Modal Shift - PCT suggests a modest modal shift arising from improvements here. Optimisation - No existing infrastructure so not considered an optimisation. Safety - A busy road over 50mph in places giving a top score, this will create a completely off-road route Biodiversity - An initial assessment suggests that widening the footpaths to the north or installing new footpaths adjacent the road could create a limited amount of vegetation removal. The full extent needs to be assessed. Leisure - As 2 historic market towns there exist some leisure potential to travel between the destinations. As an off-road route through an attractive countryside and The Broads meaning a score of 2 is considered reasonable.
Sibton	484	Northern end of Footpath Sibton 1 near Wood Farm(TM 3644 7031)	The recorded footpath comes to a dead end and should continue further north or west.	1903 Ordnance Survey Map shows the path continuing west from TM 3644 7031 along the southern edge of Northgrange Farm to the Halesworth Road at TM 3597 7030. This path should be reinstated by way of a Creation Order or Agreement in order to restore the through-route.	0	0	0	0	-2	1	-1	Connectivity and Growth - Not a key connection. Modal Shift - No effect. Optimisation - No existing infrastructure. Safety - Completely off-road, therefore no safety benefit. Biodiversity - No access to google maps so cannot see what extent biodiversity loss will be. L - Little to no effect on leisure.
Snape	110	A1094 This is the only link between Woodbridge/Snape to Knodishall/Leiston.	The traffic is fast and frequent. The undulating road means people take risks when overtaking. Riding a bike feels unsafe and you have to cross both lanes of traffic.	Half a mile of cycleway beside the carriage way.	2	1	0	3	-3	3	6	Connectivity and Growth – The proposal would connect snape to Aldeburgh, which provides some key services, however the A1094 would also provide a connection to Knodishall and Friston. The route will, however, likely have more leisure value, therefore a score of 2 is considered reasonable.Modal Shift – According to PCT, the road is currently poorly used, however if segregated off-road infrastructure is deliverable PCT suggests there will be a small uplift, thus a score of 1 is considered reasonable.Optimisation – The proposed improvements are new and do not optimise the existing, hence a score of 0 under optimisation. Safety – The majority of the A1094 has a NSL, is unlit, and is an 'A' type road, which means volume and speed of traffic is likely high. With consideration to the road conditions, taking

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												cyclists/pedestrians off this road is beneficial. Biodiversity – The A1094 is lined with hedgerows, trees, and other shrubbery, therefore the proposal will likely result in significant biodiversity loss. Leisure – The proposal will have a significant Leisure benefit as not only will it provide cohesion of a number of PROWs but will also connect to Aldeburgh beach and the River Alde, which are leisure attractions.
Snape	207	Cycle route Snape to Aldeburgh avoiding A1094	Cycling along the A1094 can be perilous at times and not encouraging for inexperienced/young cyclists	Consider upgrading the Suffolk Coastal Route path from Snape to Aldeburgh to a 'gravel' cycle/footpath path from Snape, through marshes to the western fringe of Aldeburgh, continue 'cycle/footpath' into town centre.	1	1	0	3	-2	3	6	The commenter proposes a cycle/pedestrian route between Aldeburgh and Snape whilst avoiding the A1094. For the purpose of this assessment, upgrading FP17/1/19 to bridleways will be assessed. Connectivity and Growth – The proposal will likely have more leisure benefit than connectivity and growth benefit. A new connection is created between Snape and Aldeburgh, however there is unlikely to be significant 'everyday use' as it is somewhat indirect. A score of 1 is considered reasonable under this category. Modal Shift – The proposal will provide an alternative to the A1094 and, according to PCT, if infrastructure is delivered to the highest standard, there would be a small modal shift. A score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal would provide an alternative route to the A1094, which is a busy 'a' type road with a NSL, therefore providing an off-road route for cyclists and pedestrians will likely have safety benefits. Biodiversity – The proposal will require widening of the existing footpath which may require the removal of wild verges; therefore, a moderate negative score is deemed necessary. L – The proposal will have significant leisure benefit. Not only is the route particularly attractive and in close proximity of the River Alde, but it also connects into Aldeburgh which is a seaside town and is, therefore, a major leisure attraction.
Snape	424	Legitimise cycling between Snape and Aldeburgh.	To be able to cycle safely from Snape to Aldeburgh (and the other way of course) would be a major improvement and add to the economy by all the holidaymakers and second homers being able to cycle with children to Snape or vice versa and the route is almost there, along the river wall, down the sailors path and along the verge to Aldeburgh. Just a small spend to improve the river wall and the verge and you are there.. it would also be a fantastic addition for local folk to cycle it.	Maybe just a bit of edging along the river and verge to contain some road planings and a few signs to be respectful of pedestrians.	1	1	0	3	-2	3	6	Connectivity and Growth – The proposal will likely have more leisure benefit than connectivity and growth benefit. A new connection is created between Snape and Aldeburgh, however there is unlikely to be significant 'everyday use' as the route is somewhat indirect. A score of 1 is considered reasonable under this category. Modal Shift – According to PCT, if infrastructure along the A1094, which the proposal will become an alternative for, is delivered to a high standard, there will be a small modal shift. A score of 1 is, therefore, considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal would provide an alternative route to the A1094, which is a busy 'a' type road with a national speed limit, therefore providing an off-road route for cyclists and pedestrians will likely have safety benefits. Biodiversity – The proposal will likely result in the removal of wild verges adjoining the existing footpaths; therefore, a moderate negative score is deemed reasonable. Leisure – The proposal will have significant leisure benefit. Not only does the route reside within Sailors Path, which is particularly attractive, but it also connects into Aldeburgh which is a seaside town and is, therefore, a major leisure attraction.
South Cove	102	b1127	I agree that the B1127 is dangerous for cyclists and pedestrians. It would also be great to have a cycle route from Reydon to Kessingland, rather than crossing the A12	Make the Coastal path suitable for mountain bikes?	0	1	0	3	-3	3	4	Comment scored in relation to improvements to the B1127 Connectivity and Growth - Wrentham and Southwold have their own services and there is little development in between that would benefit the additional connectivity. Modal Shift - A modest uplift shown on PCT Optimisation - No existing infrastructure Safety - The road is at a national speed limit with no road markings or no lighting at night so there are safety benefits. Biodiversity - Loss of established Hedge over a long distance would represent a high minus score. Leisure - Direct links to Southwold through Reydon has some good leisure benefits.
South Cove	114	The B1127 between Wrentham and Reydon	It is extremely unfriendly for walkers and cyclists. In spite of it being a minor road with double bends and poor visibility cars come at speed making it very unsafe.	There should be speed restriction and a cycle lane	0	1	0	3	-3	3	4	Connectivity and Growth - Wrentham and Southwold have their own services and there is little development in between that would benefit the additional connectivity. Modal Shift - A modest uplift is deemed possible according to PCT. Optimisation - No existing infrastructure Safety - The road is at national speed limit with no road markings and no lighting at night meaning there is a safety benefit. Biodiversity - Loss of established Hedge over a long distance results in a large minus score. Leisure - Direct links to Southwold through Reydon has leisure benefit.
South Cove	668	Lowestoft to Southwold	Lowestoft to Southwold involves large detours to avoid the A12 from Kessingland but eventually arriving at a very dangerous crossing of the A12 at Wrentham followed by several miles of very dangerous travel along the B road to Reydon and Southwold. again there is no provision whatsoever for cyclists.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.	1	1	0	3	-3	3	5	Connectivity and Growth - Both Wrentham and Southwold have their own services and there is little development in between that would benefit. Modal Shift - Uplift of 41 according to PCT Optimisation - No existing infrastructure Safety - National speed limit, no road markings, no lighting at night Biodiversity - Loss of established Hedge over a long distance Leisure - Direct links to Southwold through Reydon
South Cove	674	B1127 Lowestoft Road	The B1127, Lowestoft Road is particularly dangerous for walkers and cyclists and safety measures to improve the lot of each would be welcome.		0	1	0	3	-3	3	4	Connectivity and Growth - Wrentham and Southwold have their own services and there is little development in between that would benefit. Modal Shift - Uplift of 41 according to PCT Optimisation - No existing infrastructure Safety - national speed limit, no road markings, no lighting at night Biodiversity - Loss of established Hedge over a long distance Leisure - Direct links to Southwold through Reydon
South Cove	114a	The B1127 between Wrentham and Reydon	It is extremely unfriendly for walkers and cyclists. In spite of it being a minor road with double bends and poor visibility cars come at speed making it very unsafe.	This is an alternative suggestion made by an officer of East Suffolk Council is to explore upgrading the multiple PROW routes between Wrentham to Reydon through Frostenden. Without a full exploration of these paths the assessment is broad only.	2	1	0	3	-2	3	7	Connectivity and Growth - Whilst this will not provide a connection to a key service centre Frostenden would benefit from connections to Southwold. Modal Shift - PCT along the main road suggests a small benefit. Optimisation - Would require significant new infrastructure. Safety - The road is at national speed limit with no road markings and no lighting at night along the B1127 means safety benefit. Biodiversity - A full assessment has not been undertaken, but it is likely that widening existing paths would have less biodiversity impact than a whole new path. However this would be subject to further assessment. Leisure - Provides connections to Southwold which has significant leisure appeal and the paths could be an attraction in its own right.

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
Southwold	30	Southwold; south End of main road, in Market Place	Lack of cycle parking, leading to passive-aggressive signs "not to park here" on various buildings	Provision of Sheffield racks (other designs of that sort are acceptable, designs holding only a wheel are not, whether bolted to the ground or to a wall)	0	0	1	0	0	2	3	Connectivity and Growth - Cycle parking does not increase connectivity. Modal Shift - Due to the nature of Southwold, it receives lots of visitors at key times of the year and cycle parking will have a slight impact on Modal Shift. Optimisation - No existing cycle infrastructure in the centre of Southwold. Safety - Reduces the risk of pedestrians tripping over poorly parked bicycles however this is not significant enough to score in this category. Biodiversity - No effect. Leisure - Although a small improvement, the nature of Southwold means it scores 2 in leisure.
Southwold	70	End of pier avenue (town end) Southwold	When walking to southwold from Reydon where I live with my young family it is very difficult and dangerous to cross the road at pier avenue. Southwold is very busy with traffic making it very difficult to cross over, with or without a buggy and a toddler on a bike.	A pedestrian crossing or similar, further up pier avenue for safety.	1	0	0	1	0	0	2	Connectivity and Growth - Crossing Pier Avenue is a modest barrier for people travelling in between Southwold and Reydon. The road is 30mph with pavements either side of the road and therefore the addition of a crossing would give a small benefit. Modal Shift - No significant modal shift. Optimisation - the crossing would not directly improve existing infrastructure. Safety - The crossing will provide a safe way to cross the Pier Avenue that currently does not exist. This will be a modest benefit due to the 30mph speed limit. Biodiversity - No effect. Leisure - Although Southwold is hotspot for leisure activities, this crossing will not add significant benefits in regards to leisure.
Southwold	84	Junction between Bulcamp Drift and the A1095 to Southwold	A fast, dangerous road for cycling and walking! No footpath from A12 to Wolsey Bridge, so no link up possible between footpaths from Southwold and to Halesworth. No appreciable verge and a very dangerous bend about 1/4 mile east of Bulcamp Drift - many accidents, several fatal. Living on the Bulcamp peninsula is like being on an island - we have to go everywhere by car. The bus stop at the end of the Drift has lost its designation and it's hard to persuade drivers to stop, though they should.	1: Extend the 40mph speed limit to Southwold. 2: Create a foot/cyclepath on the south side of the road on Henham Estate land between Wolsey Bridge and the A12. Put pressure on them? 3: Reinstate the bus-stop at the end of Bulcamp Drift, cutting the bushes back on the north side of the road to make it visible - there's a farm track/opening into the woods.	1	1	0	3	-3	3	5	Connectivity and Growth – The proposal would not only connect Reydon and Southwold, which are both large settlement areas, but would also connect to the isolated St Felix School. However, as both Reydon and Southwold are well-established settlements with their own key services, it is unlikely that the infrastructure will have daily use and it will likely have more leisure value than that of connectivity. Therefore, a score of 1 under this scoring category is considered reasonable. Modal Shift – The A1095 is relatively quiet on PCT but busy on Strava Metro suggesting that the route will likely have more leisure value; however, using PCT, the proposal would result in a small modal shift. A score of 1 under this scoring category is considered reasonable. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing. Safety – the majority of the A1095 has a national speed limit, but this is reduced to 30mph travelling eastbound into Southwold town centre. As a busy 'a' type road with no existing cycling infrastructure, the proposal warrants a score of 3 under 'safety' as it will have a safety benefit. Biodiversity – The majority of the A1095 is surrounded by established hedgerows, trees, and other shrubbery. It is likely therefore, that the addition of a segregated cycleway/footway would have a resultant significant loss. Leisure – the proposal would connect to Southwold which is a key strategic location with a beach and an array of shops, eating establishments, drinking establishments, and other attractions. Furthermore, it would connect to the isolated Southwold cycle hire, Southwold Maize Maze, and Old Hall Southwold Café. It is considered therefore, reasonable for the proposal to score 3 under this category.
Southwold	333	Southwold	At the present time the only cycle lane 'in' Southwold is the approach road from the Lowestoft Road junction to the North Road junction. This is completely useless as it is not a solid white line hence parking seems to be acceptable anywhere along it thus completely stopping cyclists from using it and further increasing the hazard of an accident as they swing out round parked cars. Southwold has a problem with speeding which is never picked up by the local town council.	I suggest the cycle lane be removed as it serves no purpose and a strictly enforced 20mph speed limit be put in place from St Felix School and also implemented in Reydon to make sure the whole, very popular cycling and walking area, is safer for cyclists and pedestrians alike.							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Southwold	441	No access to Easten Bavents beach	Suffolk Coastal path takes a huge inland diversion between Southwold and Covehithe. The latter is now spilling over with people trying to access the beach.	Safe steps over the breakwaters at the north end of Southwold Parade would meet a need, avoid people taking risks on the rocks and allow escape if stranded by rising tides.	0	0	0	1	0	1	2	Connectivity and Growth - No effect. Modal Shift - No effect Optimisation - No effect. Safety - Adding steps to this area will create a safer way for people to access the beach as opposed to the existing rocks. Biodiversity - No effect. Leisure - This improvement will have a slight improvement to access to the beach.
Southwold	509	Reydon-Southwold cycling/pedestrian links	1. The existing cycle lane Rt 31 goes over the bridge and stops before the most dangerous junction which is crossing into the Wrentham Rd. 2. No safe cycle crossing point onto the Wangford Rd. 3. No cycle route linking the proposed Copperfield Road development and the development proposed on land owned by the NHS around the surgery to Southwold.4. Cycle lane on the East and West sides of Mights Road has broken lines, creating risk to cyclists overtaking parked cars.	Improve cycle crossing points to Wangford Road and Wrentham Road by extending marked cycle land and showing the cross point with signage giving cyclists & pedestrians right of way. Create an unbroken foot path cum cycle path linking the new developments to Southwold via Keen Lane, the footpath from St Felix School to the Bund footpath on Botany Marsh through to the Blyth footpath leading to Station Rd and the Bailey Bridge. This would create a car-free linkage between the three parish/towns of the Southwold Ward. It would also benefit the proposed development on the St Felix playing fields. We would like to discuss this in more detail with you as this infrastructure improvement could have the greatest impact for cyclists and pedestrians. Distinguish cycle routes from car routes with unbroken lines to prevent parking. SCC should keep cycle lanes clear of debris and localised flooding from blocked drains.	2	0	0	0	0	0	2	Connectivity and Growth - Implementing a shared path along Keen Lane will provide a new connection onto the A1095 which has established walking infrastructure. Modal Shift - No effect. Optimisation - Signage on crossing points would have a small benefit on the effectiveness of the crossing. Safety - Signage on crossing points would have a small benefit on the effectiveness of the crossing. Biodiversity - No effect. Leisure - No effect.
Southwold	512	Southwold High Street	Due to the large number of pedestrians using the narrow pavements, and the large number of cars going through the High Street, pedestrians are forced to walk	A large sign/banner. Go slow, make way for pedestrians in the road. Or some such language. Widen pavements	0	0	0	1	0	0	1	Connectivity and Growth - Additional signage will not have an effect on connectivity and growth. Modal Shift - No effect. Optimisation - Signage will not improve the existing infrastructure. Safety - Alerting vehicles to pedestrians in the road will have a modest benefit to pedestrian safety. Cars are likely to be travelling at low speeds and,

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			in the roads, creating a safety hazard. Covid has accentuated an existing problem.	Or other traffic calming measures								therefore, a score of 1 is appropriate. Biodiversity - No effect. Leisure - Although Southwold is hotspot for leisure activities, this improvement will not add significant benefits in regards to leisure.
Southwold	621	From the bridge follow the line of the old railway up to Halesworth.	Although good footpaths and bridleways, the line of the old railway is not immediately apparent.	Join up the various footpaths and bridleways to create a cycle route between the River Blyth and Halesworth to follow the route of the railway.	3	0	2	2	-2	3	8	Connectivity and Growth - A complete connection between two market towns of Southwold and Halesworth would be created. The connection is currently broken and not continuous. Modal Shift - Likely to have no effect as the route will act as more of a leisure route than a commuting route. Optimisation - Upgrading and widening existing footpaths to accommodate cycling legally and safely will have a positive effect on the route and provide more opportunities for use. Safety - Score of 2 has been allocated as currently cyclists have to use the B1123 and A1095 to travelling between Halesworth and Southwold. This route will provide an off-road option for cyclists. Biodiversity - Score of -2 has been allocated due to the sensitive environment that the route is located in. Areas that need to be widened will require the removal of vegetation. Leisure - This route will provide a very attractive route that connects people to Southwold from Halesworth.
Southwold	673	Southwold Town Council	STC would like to support references that have been submitted already, namely: Refs: 333, 34 and 102 combined, and all references to the Coastal Path from north of the pier through Eastern Bavents.								N/A	The support for other comments has been noted.
Southwold	673a	Southwold Town Council			0	1	0	3	-3	3	4	Comment scored in relation to improvements to the B1127 Connectivity and Growth - Wrentham and Southwold have their own services and there is little development in between that would benefit. Modal Shift - uplift of 41 according to PCT Optimisation - No existing infrastructure Safety - national speed limit, no road markings, no lighting at night Biodiversity - Loss of established Hedge over a long distance Leisure - direct links to Southwold through Reydon
Southwold	673b	Southwold Town Council			0	1	0	3	-3	3	4	Connectivity and Growth - No effect Modal Shift - Uplift of 41 according to PCT Optimisation - No existing infrastructure Safety - national speed limit, no road markings, no lighting at night Biodiversity - Loss of established Hedge over a long distance Leisure - direct links to Southwold through Reydon
Southwold	673c	Southwold Town Council									N/A	The support for other comments has been noted.
Southwold	84a	Junction between Bulcamp Drift and the A1095 to Southwold	A fast, dangerous road for cycling and walking! No footpath from A12 to Wolsey Bridge, so no link up possible between footpaths from Southwold and to Halesworth. No appreciable verge and a very dangerous bend about 1/4 mile east of Bulcamp Drift - many accidents, several fatal. Living on the Bulcamp peninsular is like being on an island - we have to go everywhere by car. The bus stop at the end of the Drift has lost its designation and it's hard to persuade drivers to stop, though they should.	This is an alternative suggestion made by an officer of East Suffolk Council. An alternative is to explore whether footpath 5 can be upgraded. This would need to be explored fully so only a broad scoring is possible. Whilst it would avoid more of the A1095 it should be noted it adjoins the A1095 at a later point.	1	1	0	3	-2	3	6	Connectivity and Growth – The proposal would not only connect Reydon and Southwold, which are both large settlement areas, but would also connect to the isolated St Felix School. However, as both Reydon and Southwold are well-established settlements with their own key services, it is unlikely that the infrastructure will have daily use and it will likely have more leisure value than that of connectivity. Therefore, a score of 1 under this scoring category is considered reasonable. Furthermore the improvement of the entire length of the footpath would not result in any CandG improvement as it connects to the A1095 only. Modal Shift – The A1095 is relatively quiet on PCT but busy on Strava Metro suggesting that the route will likely have more leisure value; however, using PCT, the proposal would result in a small modal shift. A score of 1 under this scoring category is considered reasonable. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing. Safety – the majority of the A1095 has a national speed limit, but this is reduced to 30mph travelling eastbound into Southwold town centre. As a busy ‘a’ type road with blind corners and no existing cycling infrastructure, the proposal warrants a score of 3 under ‘safety’ as it will have a significant safety benefit. Whilst the improvement of the whole footpath would still result in use of the A1095 a high score in safety is deemed reasonable. Biodiversity – Whilst the upgrading and widening of the existing footpath will result in some biodiversity losses it would be less than a whole new path as the footpath itself has limited biodiversity value. Leisure – the proposal would connect to Southwold which is a key strategic location with a beach and an array of shops, eating establishments, drinking establishments, and other attractions. Furthermore, it would connect to the isolated Southwold cycle hire, Southwold Maize Maze, and Old Hall Southwold Café. It is considered therefore, reasonable for the proposal to score 3 under this category.
Sternfield	721	Between Snape and Saxmundham	I would like to see off-road cycle paths from Snape to Saxmundham.	This would link many local facilities and heritage attractions and also join up with local train stations for those wanting to come to the area with their bicycles by rail.	2	1	0	3	-3	3	6	The commenter proposes an off-road cycling route between Snape and Saxmundham. For the purpose of this assessment, implementing infrastructure along the B1069, the road opposite the B1069 travelling northbound, and the B1121 will be assessed. Connectivity and Growth – The proposal will connect Snape, Sternfield, and Saxmundham. The proposal will likely have more leisure benefit than that of connectivity, but Saxmundham does provide some key services that are not available in Snape and Sternfield. A score of 2 has been awarded. Modal Shift – PCT suggests that the proposal, for the majority of the route, will not provide a significant modal shift, however improving infrastructure along the B1121 to the highest standard may result in a modest modal shift. A point has, therefore, been awarded. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal would provide an alternative to cycling along the roads which, for the most part, have a national speed limit and are likely busy. Removing cyclists off road warrants a score of 3 under this category. Biodiversity – It is likely that the proposal would result in the removal of hedges that adjoin the roads. Removal of established hedgerows would normally result in a -3 under this category. Leisure – Connecting into Snape, which is situated by the River Alde and connects into Sailors Path towards Aldeburgh, will have significant leisure value, hence a score of 3.

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
Stratford St Andrew	210	Where the cycle route crosses the A12 just west of Farnham (Tinker Brook)	The 30mph limit stops just short of this crossing. If it was extended a 100 metres or so toward Glemham it would be safer to cross the A12 by bicycle.								N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Stratton Hall	47	Between Nacton and Trimley	Lack of safe walk routes between Nacton and Trimley	use 1/2 of the Felixstowe road as a cycle track and walkway	3	3	3	3	0	3	15	<p>Connectivity and Growth: This section forms a significant section of the Ipswich to Felixstowe Key Corridor. The use of Felixstowe Road 'east's redundant dual carriageway to create a cycle/pedestrian track will be of significant connectivity and growth value between Ipswich and Felixstowe, due to the current lack of LTN 1/20 quality infrastructure to facilitate safer cycling between them.</p> <p>Modal Shift: PCT shows high levels of potential uplift following the delivery of this route (11 to 125). StravaMetro shows significant current use.</p> <p>Optimisation: Though this creates a new scheme rather than improves an existing scheme, this scheme is unusual in it would put to use entirely redundant carriageway. It has therefore been provided with an optimisation score of three.</p> <p>Safety: The proposal has a high potential to provide safety benefits.</p> <p>Biodiversity: No anticipated biodiversity impact.</p> <p>Leisure: Scheme has high leisure value</p>
Stratton Hall	250	Levington, Felixstowe Road.	Crossing the A14 & travel between the villages on either side.	There is a private farm road and 'Tunnel' under the A14 at this point which could be upgraded to a PROW / bridleway between Felixstowe road and Brightwell Road to allow cyclists and pedestrians to cross the A14 safely and travel between the villages on either side of the A14	1	0	0	3	0	0	4	<p>Connectivity and Growth – The A14 is a significant barrier between those situated on either side and it may help in providing a more direct route into villages situated either side. A score of 1 is considered reasonable. Modal Shift – Insufficient evidence to suggest that the proposed infrastructure will result in a significant modal shift.</p> <p>Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – This section of the A14 is a dual carriageway with a NSL, therefore providing a high-level crossing to the other side has a safety benefit.</p> <p>Biodiversity – No significant biodiversity impact. Leisure – No significant leisure benefit</p>
Stratton Hall	623	Levington around the A14	There is no safe place for pedestrians / cyclists to cross the A14 in the vicinity of Levington, Bucklesham, Kirton et.c, except the underpass at Walk Farm opposite Stratton Hall Drift.	This lack of a crossing could be solved by making the track between the two minor roads either side of the A14 (including the underpass at Walk Farm) a public right of way.	1	0	1	2	0	1	5	<p>Connectivity and Growth: Although this may not be the optimum public crossing point, if delivered it would be the only A14 crossing point that is publicly accessible and safe between the Seven Hills Interchange and the A14 footbridge at Kirton Road. Although PROWs are mapped, Google Maps imagery (satellite and StreetView) suggest they are not being maintained - and even if they were, none of them include an underpass or bridge, so require crossing the A14, which is to be strictly avoided for safety reasons. It therefore has Connectivity and Growth value, if small. Bucklesham and Brightwell, and the forthcoming Brightwell Lakes urban extension will be accessible via the key corridor or via Kirton - and there is minimal development inbetween, lowering the need for high levels of permeability between them; this prevents a higher score. Modal Shift: No PCT assessment available as the crossing is on private land. Unlikely to have significant impact beyond an uplift in leisure cycling route options, as does not add a new residential/employment/retail link, only slightly shortens it. Strava Metro shows minimal use of it now, which is understandable given there is no legitimate PROW there. Bridging Levington Lane still seen as the better solution. Safety: Safety score of 2 given as access to the underpass would hopefully remove any temptation to cross the A14. However as the A14 foot bridge exists, it is expected that this would be used for crossing the A14 and accessing Kirton, Bucklesham, Brightwell etc, instead. Leisure: Leisure score of 1 as it gives another A14 crossing option for leisure cyclists, mostly (unlikely to benefit the commuter cyclist or pedestrians).</p>
Stratton Hall	761A	See attached documents - Stratton Hall	See attached documents		0	0	2	0	0	1	3	<p>Connectivity and Growth – No significant connectivity and growth benefit.</p> <p>Modal Shift – The alterations would not be expected to create significant modal shift although it will create better availability for some users.</p> <p>Optimisation – Flooding is likely to be a difficult issue to overcome, but it will optimise the footpath by keeping it available throughout the year. Re-surfacing and widening of pavement will also have somewhat significant optimisation benefits – a score of 2 is considered reasonable.</p> <p>Safety – The issue raised is a matter of access and usability over safety.</p> <p>Biodiversity – No significant biodiversity impact.</p> <p>Leisure – These paths represent high value leisure routes alongside the River Orwell. If improved, the routes will provide leisure access to a wider range of people and improved surfaces for all meaning it scores a point in this category.</p>
Stratton Hall	761B	See attached documents - Stratton Hall	See attached documents		0	0	2	0	0	1	3	<p>Community and Growth – No significant connectivity and growth benefit.</p> <p>Modal Shift – The alterations would not be expected to create significant modal shift although it will create better availability for some users.</p> <p>Optimisation – Resurfacing and widening a path warrants a score of 2 under this category.</p> <p>Safety – The issue raised is a matter of access and usability over safety.</p> <p>Biodiversity – No significant biodiversity impact.</p> <p>Leisure – These paths represent high value leisure routes alongside the River Orwell. If improved, the routes will provide leisure access to a wider range of people and improved surfaces for all meaning it scores a point in this category.</p>
Sudbourne	486	Bridleways Sudbourne 12 and 13 near the site of the old Marsh House. On Sudbourne Marshes linking Sudbourne village with the river wall.	Near where Bridleways 12 and 13 meet they cross dykes one of which is difficult and dangerous to cross even in the driest of weather. A bridge is required. Attempts were made in the 1990s to downgrade the path to a footpath so that a new footbridge would solve the problem at a much lower cost. This was objected to and never took place.	A bridleway bridge needs to be constructed to enable these paths to be linked.	0	0	1	0	0	1	2	<p>Connectivity and Growth – Any crossing would provide cohesion to PROW routes but offers limited connectivity opportunities to residential areas, services, or employment, hence a neutral score. Modal Shift – The numbers using these bridleways is unlikely to lead to a modal shift particularly as most users will likely be recreational users. Optimisation – Providing a bridge will optimise where the bridleways cross the water. A score of 1 is considered reasonable. Safety – No significant safety benefit. Biodiversity – There are no significant biodiversity impacts.</p> <p>Leisure – The PROW pathways are largely used for leisure purposes and likely have reasonable use. The construction of a small bridge will benefit the routes leisure purposes, therefore a score of 1 is considered reasonable.</p>

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Sudbourne	655	Cycling and Walking Improvements	Walking: 1) On the Snape Road to the north of the village (from the most northerly 30 mph sign in Sudbourne to the jumps at Tunstall Forest gate 23) which is particularly dangerous and regularly used by pedestrians. There is a combination of a narrow twisty road, shadow from over hanging trees and at times a low angle of light where a number of close incidents have been witnessed where pedestrians have been in danger of being hit.	1) A short foot path (approx. 300m) along this stretch could be introduced it would safely connect the pavement in Sudbourne, access to the footpath to Iken Boot (Sudbourne no 4) and access to the Tunstall Forest at gate 23. This would make a significant difference in both improving safety and would facilitate better use of footpath no4. 2) A short footpath (approx. 100m) along this stretch would connect the pavement to the two footpaths which being on a bend, un-sights motorists to the frequent local walkers and dog walkers. This would also create additional safe round walk options in the village. 3) The crossing of the B1084 needs to have improved visibility / or a different location as it is situated on both a bend and at a road junction. In addition, the footpath needs to be signposted at the Rustic cottage end of the path and for a new access and a clear route from the road crossing to the foot path network in the forest is required.	1	0	0	3	-2	2	4	Connectivity and Growth – The proposal will connect into PROW 4 which will improve connection into Iken. As the proposal will likely have more leisure value than that of connectivity and growth, a score of 1 is considered reasonable. Modal Shift – Insufficient evidence to suggest that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Currently, pedestrians are forced to walk along Snape Road, which appears narrow and has a NSL, in order to access PROW4. Removing pedestrians off road warrants a score of 3 under this category. Biodiversity – The proposal will likely result in the removal of foliage that adjoins Snape Road, hence the score of -2 under this category. Leisure – As PROW4 resides within the AONB, it likely has somewhat significant leisure value, therefore the proposal scores a 2 under leisure.
Sudbourne	656	Sudbourne	2) On the Snape Road immediately south of the village from the most southerly house to footpaths number 30 to the east and 42 to the west. 3) The safety of walkers crossing from the Rustic Drive footpath (linked to footpath 18) at Rustic Cottage to / from Tunstall Forest.	2) A short footpath (approx. 100m) along this stretch would connect the pavement to the two footpaths which being on a bend, un-sights motorists to the frequent local walkers and dog walkers. This would also create additional safe round walk options in the village. 3) The crossing of the B1084 needs to have improved visibility / or a different location as it is situated on both a bend and at a road junction. In addition, the footpath needs to be signposted at the Rustic cottage end of the path and for a new access and a clear route from the road crossing to the foot path network in the forest is required.	2	0	0	3	-2	2	5	Connectivity and Growth – Cohesion of PROW routes will provide an improved connection into Orford, therefore the proposal is awarded a score of 2 under this category. Modal Shift – Insufficient evidence to suggest that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Currently, pedestrians are forced to walk along Snape Road, which appears narrow and has a NSL, in order to access PROW30 and 29. Removing pedestrians off road warrants a score of 3 under this category. Biodiversity – The proposal will likely result in the removal of foliage that adjoins Snape Road, hence the score of -2 under this category. Leisure – As the PROW routes reside within the AONB, it likely has somewhat significant leisure value, therefore the proposal scores a 2 under leisure.
Sudbourne	657	Sudbourne	Cycling: 1. Time trial and organised events: The B1084 (Melton to Orford) is a popular route for time trial and organised events but has many narrow sections which can put cyclists and other traffic in conflict. In particular large agricultural vehicles with restricted speed, manoeuvrability and driver visibility can be hazardous for cyclists. This is a particular problem in mid / late summer with long daylight hours when they are on the same road at the same time.	1. Time trial and organised events: Organiser of these events should pre-warn affected Parish Councils of their intention to hold these organised events and routes in advance: to enable landowners / farmers in particular to ensure their vehicle movements are planned to ensure that there is minimal or reduced mixing of cycles and large agricultural vehicles. The onus has to be on the event organisers to ensure this is done in a timely manner. Information and advice for cyclists should be located at strategic locations such as Honey and Harveys in Melton a frequent meeting point for cycling groups. Event organisers should include their contact details on all roadside signage and once cycle events have been completed, they are responsible for its removal of all to reduce the amount of roadside litter created.							N/A	The proposal is in regards to promotion of cycling events. This falls outside the remit of the project but will be passed to the relevant team.
Sudbourne	659	Sudbourne	2. Condition of Suffolk Coastal Cycle Route 41 (Orford to Iken / Snape via Ferry Road through Sudbourne):This promoted rural route is quiet, picturesque and in many ways ideal for cyclists. However, the route suffers from multiple large areas of sand that have run-off from fields in particular near gate / road ways. This sand surface is especially dangerous for cyclists with smooth road tyres who have no grip on such surfaces.	2. Condition of National Cycle Route 41 (Orford to Iken via Ferry Road in Sudbourne):There are potentially three solutions that may be used individually or in combination. 1. Information should be added to publicity of the route that this is a hazard for cyclists to be aware of.2. Information on the actual route should highlight the hazard in advance for cyclists3. The land owners / Suffolk County Council should ensure the roads are clear of this washed off material.	0	0	0	1	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit.Modal Shift – No significant modal shift benefit.Optimisation – No significant optimisation benefit.Safety – a guidance sign may have partial safety benefit, although whether any sign makes a significant difference in reality is unknown.Biodiversity – no significant biodiversity benefit.Leisure – no significant leisure benefit.
Sudbourne	660	Sudbourne	3. Snape road and B1084 Snape to Orford. This road in particular is often very busy with frequent blind spots and drivers who drive too fast for the prevailing road conditions and don't anticipate individual and multiple cyclists. There are few safe passing places for cars and other vehicles on this road. An alternative for cyclist route should be investigated and implemented as a matter of urgency.	3. Snape road and B1084 Snape to Orford. In order to remove the hazards from the route from Orford to Snape an alternative route with a suitable surface should be built and clearly marked through Tunstall Forest. This would provide a safe cycling environment that would be enjoyed by a wide range of cyclists and reduce the hazard on the road. There are a variety of potential routes that can be explored in more detail which would enhance the risers experience and improve safety. 4. Information signs to bikers could be Tangham campsite , Snape Maltings , car park at Iken and Sandgalls	1	0	0	3	0	3	7	The commenter proposes safe cycleway through Tunstall Forest to create a route between Snape and Orford. For the purposes of this assessment, upgrading PROWs 3, 18, 16, 21, and 22 to bridleways will be assessed. This network of footpaths connects into bridleways and restricted byways into Snape. Connectivity and Growth – The proposal will connect Snape and Orford which would allow an element of service pooling; however, it would be indirect, exceed the 'everyday' cycling average distance of 8km, and it is likely that the proposal will have more leisure value than that of connectivity and growth. A score of 1 is considered reasonable. Modal Shift – PCT suggests that the proposal will unlikely result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal would provide an alternative to Snape Road and the B1078, which both have a NSL, therefore a score of 3 is considered reasonable. Biodiversity – No significant biodiversity impact.

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												Leisure – Both Orford and Snape reside adjacent to the River Alde and are, therefore, desirable destinations. The cycle route connecting the two will likely have significant leisure benefit.
Sutton	61	Bromeswell, cycling up Wilford Hollows	The hill is steep and many cyclists have to travel slowly. A separate cycle path would be a great idea	cut in to the bank	0	1	0	3	-2	2	4	Connectivity and Growth – This section provides limited connections to other villages and services. Modal Shift – Using PCT, the development of a cyclist and pedestrian shared pavement will have small modal shift, therefore scoring it a 1. Optimisation – The proposed improvements are new and do not optimise existing infrastructure. Safety – Providing a new pathway would provide improvements with good safety benefits. Biodiversity – The proposal will likely result in the removal of hedges and trees. The resultant loss means it has a somewhat high negative impact. Leisure – The proposal will connect the village of Bromeswell to Sutton Hoo and to multiple PROWs, therefore a score of 2 is considered reasonable.
Sutton	133	Between the end of the Walks and the entrance to Sutton Hoo	There is no footpath, which makes it unpleasant and dangerous when walking between Melton station and the peninsula.	Build a foot and cycle way to Melton station. Ideally this would not follow the road down the hill which is steep, bendy and a danger to cyclists; it would be great if a foot/cycle way could be created from Sutton Hoo to the river side and Wilford Bridge: this would make a lovely access route to Sutton Hoo and the peninsula from Melton train station, encouraging sustainable travel and tourism, and reducing congestion in Melton/Woodbridge.	0	1	0	3	-2	2	4	Connectivity and Growth – This section provides limited connections to other villages and services. Modal Shift – Using PCT, the development of a cyclist and pedestrian shared pavement will have small modal shift, therefore scoring it a 1. Optimisation – The proposed improvements are new and do not optimise existing infrastructure. Safety – The current footway is narrow and the road it adjoins has both sections of national speed limit and 40mph speed limit so removing cyclists off the road has high potential safety benefits. Biodiversity – The proposal will likely result in the removal of hedges and trees. The resultant loss means it has a somewhat high negative impact. Leisure – The proposal will connect Melton to Sutton Hoo and to multiple PROWs, therefore a score of 2 is considered reasonable.
Sutton	208	Sutton Heath	The tracks across the heath (especially north/south) are very sandy. This means that they are not practical for cycling. This is a shame as they offer direct routes between the villages and schools on the peninsula and would provide a suitable alternative to road use for cyclists.	Firm up the main paths across the heath with gravel or other hard infill, to facilitate cycling and make the roads safer. It wouldn't be necessary to tarmac them so that the beautiful landscape can be preserved.	0	0	1	0	0	1	2	Connectivity and Growth – The connection already exists and will not, therefore, create any additional connectivity. Modal Shift – According to PCT, it is unlikely that improving the PROWs to the highest standard would result in a modal shift. Optimisation – The improvements will help make the pathway more inclusive. This will provide an improvement to a path that is already off-road meaning it is considered 1 point. Safety – The issue raised is a matter of access and usability over safety. Biodiversity – No significant biodiversity impact. Leisure – The PROW route is particularly attractive and extends through the AONB designation. The improved surfaces will provide leisure access to a wider range of people meaning it scores a 1 in this category.
Sutton	276	Private road from East side of Melton Bridge through to Sutton Hoo .	Define the "Private" road from East side of Melton Bridge through to Sutton Hoo to be a Bridleway or similar. I believe there is already an outstanding request to confirm that this should be a Public Footpath. This will allow an easy access to Sutton Hoo from Woodbridge and the Melton Railway Station	Define the "Private" road from East side of Melton Bridge through to Sutton Hoo to be a Bridleway or similar. I believe there is already an outstanding request to confirm that this should be a Public Footpath. This will allow an easy access to Sutton Hoo from Woodbridge and the Melton Railway Station	0	2	0	3	0	2	7	Connectivity and Growth – No significant connectivity benefits. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, the proposal will lead to a modal shift. A score of 2 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal would provide an alternative route to the B1083 which has high speed limits and, as a 'B' type road, is busy. Removing cyclists and pedestrians off a the road and warrants a score of 3. Biodiversity – No significant biodiversity impact. Leisure – The proposal will connect to Sutton Hoo, which is a major leisure attraction, and to a network of attractive PROWs. Therefore, a score of 2 has been awarded.
Sutton	288	From Melton Station to the roundabout near Wilford Bridge and onwards up the hill towards Sutton Hoo.	This is a very busy piece of road. Many motorists seem impatient and overtake inappropriately. I have had several close calls along this road. I now find it too dangerous to cycle which means I can no longer cycle to Shingle Street except by a roundabout route or I go early Sunday morning. Ideally there should be a cycle lane separated from traffic but this is not a cheap solution.	Cycle lane.	0	1	0	3	-2	2	4	Connectivity and Growth – This section provides limited connections to other villages and services. Modal Shift – Using PCT, the development of a cyclist and pedestrian shared pavement will have small modal shift, therefore scoring it a 1. Optimisation – The proposed improvements are new and do not optimise existing infrastructure. Safety – The current footway is narrow and the road it adjoins has both a national speed limit and 40mph speed limit. Given the speed limit and a proposal that gets cyclists off the road, it does score highly for safety. Biodiversity – The proposal will likely result in the removal of hedges and trees. The resultant loss means it has a somewhat high negative impact. Leisure – The proposal will connect Melton to Sutton Hoo and to multiple PROWs, therefore a score of 2 is considered reasonable.
Sutton	507	Sutton Heath	Walking & Cycling along 'Heath Road'..As already noted this is a fast and straight road which makes it unsafe to walk or cycle along	Create a path parallel to the road but on the 'heath' side of the fence line where possible.The path could be a simple woodland style path suitable for walkers or those using mountain bikes. The verges are wide in places as well although it might mean some crossing of the road in places, but thats safer than walking down the road as I saw someone doing the other day.	1	1	0	3	-3	2	4	Connectivity and Growth – The proposal would connect Hollesley to Melton, however there is unlikely going to be 'everyday use' as Hollesley has good levels of key services. A score of 1 is considered reasonable. Modal Shift – If infrastructure is delivered to the highest standard, there would be a resultant moderate modal shift, hence a small score of 1.Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Heath Road has a national speed limit, therefore removing them off the road warrants a significant score. Biodiversity – The proposal will result in significant biodiversity losses including the loss of wild verges and established hedgerows. Leisure – The proposal connects to Sutton Hoo and highly attractive PROW routes, which include those that go through Sandlings Forest and Sutton and Hollesley Heaths. Therefore, a score of 2 is considered reasonable.
Sutton	568	B1083 between A1152 and Sutton Hoo entrance	Very hard to cycle up the hill to this beautiful site of national importance owned by the National Trust. Better access needed for cyclists.	Cycle lane, white paint with signs.	0	0	0	2	0	2	4	Connectivity and Growth – This section provides limited connections to other villages and services. Modal Shift – It is unlikely that on-road cycle lanes will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing infrastructure.

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												Safety – The B1083, which is a busy 'b' type road, has both a NSL and 40mph speed limit. As the proposal is for on-road infrastructure, they will have modest safety benefits and it is unlikely that they will completely address the concern raised. A score of 2 is deemed reasonable. Biodiversity – No biodiversity impact. Leisure – The proposal will connect Melton to Sutton Hoo and to multiple PROWs, therefore a score of 2 is considered reasonable.
Sutton	594	GR 282 504 along B1083 to 294 496	Risky cycling all along B1083 to & from Bawdsey Ferry	Provide a separate cycle way that could encourage AT & visitors	1	1	0	3	-3	3	5	Connectivity and Growth – The proposal will create a new connection between Melton, Sutton, Shottisham, Alderton, and Bawdsey. Many of the villages have limited services and the connection will allow an element of service pooling, however the proposal will likely have more leisure benefit than connectivity and the route will unlikely have significant 'everyday use' as it exceeds the 8km average cyclist distance. Modal Shift – Overall the B1083 has limited use, however, according to PCT, there are some sections of the B1083 that will have a small modal shift if infrastructure is delivered to the highest standard. A score of 1 is deemed reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The B1083, which is a busy 'b' type road and predominantly a NSL road. An improvement to remove cyclists and pedestrians off road warrants a score of 3. Biodiversity – The implementation of a segregated cycle track will likely have a resultant loss of established hedgerows adjoining the B1083, therefore a significant negative score is given under this category. Leisure – The proposal will have significant leisure benefit as it will connect into Bawdsey which, being a beach, is a key strategic location. A score of 3 is considered acceptable.
Swilland	94	Junction Gibraltar Rd and B1078	This is on route from Otley to Swilland and towards Ipswich. The B1078 is fast and straight with only NSL. Crossing on foot or bike from Otley is very dangerous. I do it by myself but would not risk it with a group especially if it included inexperienced cyclists or children	Better signage, speed limit, central reservation	0	0	0	2	0	0	2	Connectivity and Growth – a central reservation at this junction would offer limited connectivity opportunities to residential areas, services, or employment locations. Modal Shift – The numbers using this road is unlikely to lead to a significant modal shift. Optimisation – this suggestion does not optimise existing infrastructure. Safety – B1078 is a straight 'B' type road with a NSL and there currently are a limited number of other crossing points along this road, therefore the suggestion offers a moderate benefit. With consideration to the road conditions, a score of 2 under safety is regarded as reasonable. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion provides limited leisure benefit. There are two PROWs to the west of the Gibraltar Rd/B1078 junction, however it is unlikely that the central reservation would provide a leisure benefit for pedestrians utilising them.
Swilland	232	B1078 & Swilland Crossroads	Turning right off the B1078 for cyclists is perilous, particularly during the rushhour periods when the B1078 is busy with streams of vehicles travelling at the speed limit which at this point is 60mph. Its noticeable that there is a tendency amongst some motorists to overtake at speed along this stretch into the face of oncoming traffic which if you are a cyclist or walker is actually terrifying....Traffic does not 'naturally give way' to anyone attempting to walk along the road.	As a minimum the Ashbocking 40mph limit should be extended to the College 40mph to create one continuous 40mph limit							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Theberton	434	Old railway line between Aldeburgh and Leiston	Absence of safe cycling route for tourists and residents between Aldeburgh and Leiston. Roads are too dangerous and existing cycle route along coast path isn't accessible for most. We don't have an easily cycled tourist route like other parts of the country.	The old railway line between Aldeburgh and Leiston provides an ideal route. Starting from the caravan park, heading along the old line, across the road at Thorpeness holt, continuing along the line route until Crown Farm, this would join the existing cycle path along Lovers Lane, a new extension proposed by EDF (DCO) and Leiston's Cycle Strategy route into Town. A tarmac track (Suffolk's version of the 'cinder trail' - route 1 of National cycle network) would give access to many more residents who cannot currently cycle easily or safely between the two towns for work/recreation. It would be a boost for tourism as more people would access the route as a flat and easily cycled surface. E Bikes could be promoted to reduce car journeys.	3	2	2	0	0	3	10	Connectivity and Growth - Provides a new connection between Aldeburgh and Leiston and then on to potential tourist attractions such as Thorpeness. It is also part of the Tourism and Leisure key Corridor route. Modal Shift - No PCT data available, however it is reasonable to assume this will be a well used route. Optimisation - No existing infrastructure on the disused railway station. Biodiversity - No access to streetview so site visit is needed for an accurate assessment, however it is likely to have little to no effect on biodiversity. Leisure - This will be an attractive leisure route.
Thorington	487	Drive from Thorington Road at TM 4175 7421 to Walnut Tree Farm and beyond to meet Bramfield Footpath 7 at TM 4146 7329.	Bramfield Footpath 7 is recorded as coming to a dead end just short of Walnut Tree Farm. It should continue north to the Thorington Road along the existing farm road.	The missing link needs rectifying by means of a Creation Order or Agreement.	1	0	0	0	0	1	2	Connectivity and Growth - Connects Thorington to Bramfield. Bramfield has services that are not available in Thorington. Despite already being connected by one PROW, this would provide a more direct route, therefore a point has been awarded. Modal Shift - No effect. Optimisation - No existing infrastructure. Safety - Off-road so will not have significant safety benefits. Biodiversity - No access to Google maps and therefore cannot determine the impact. Leisure - Increases opportunity for leisure walking.
Trimley St Martin	117	Morston Hall Road. Trimley	This link road between the old A14, Felixstowe road and Trimley st. Martin. It is used by busses, local residents, cyclists and speeding motorists that would be better off using the actual A14. I suggest that it be used as a cycle and bus lane only with local residents access. It would give a safe route for the above to travel between Felixstowe and Ipswich. The cycle lane actually alongside the A14 is not fit for purpose. It's rough, bumpy and has heavy traffic thundering past making it unsafe.	Local residents of Morston hall road , bus and cycle lane only 20 mile an hour speed limit	3	3	2	2	0	3	13	Connectivity and Growth: Morston Hall Road forms a section of the Ipswich to Felixstowe Key Corridor, and therefore in aggregation with the rest of the scheme, provides a (mostly - bar this section, which is instead to be filtered so cycles share with busses) segregated connection between Ipswich and Felixstowe, which earns a full Connectivity and Growth score. Modal Shift: As above - as this is part of the Key Corridor, and PCT advises high potential uplift in cyclists, it is scored as 3. Optimisation: The installation of a modal filter between the two points on Morston Hall Road that still give access to the properties off Morston Hall Lane (Morston Hall Cottages etc.) is a workable option, and it has been assumed that it is these two points (at the junction with Felixstowe Road 'east' and the junction with Morston Hall Lane) that have been recommended by the respondent. Safety: A modal filter via bus gate is not a totally vehicle free solution, as buses will still use the carriageway so a full score cannot be given. It is however an improvement from sharing with cars, and bus movements between these points are relatively low (its not like a busy inner-city

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												bus route). Biodiversity: No significant biodiversity benefit Leisure: A full score of 3 is given, as in aggregate the Ipswich to Felixstowe Key Corridor improvements will allow for longer distance leisure cycling trips between Ipswich and Felixstowe.
Trimley St Martin	122	Cycle pathway alongside A14	It's over grown and VERY uneven	A significant tidy up, re tarmac pathway	0	3	2	0	-2	0	3	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – Improving the pathway here to the highest standard (segregated cycle lane) will provide a significant modal shift and would score 3 points. Optimisation – Moving from a shared path to a segregated cycle track is deemed to provide 2 points. Safety – The cyclists are already separated from the road and whilst the comment suggests it is in poor condition this is more of a maintenance issue. Improving the pathway doesn't significantly improve safety. Biodiversity – The proposal will likely result in the removal of the foliage adjoining the pathway, hence a score of -2. Leisure – The pathway does not provide significant leisure benefit.
Trimley St Martin	132	Howlett way to roundabout -over roundabout into kirton road and vice versa	This is the only route out of Felixstowe and the Trimley to the villages of Kirton Newbourne etc. Howlett road is a busy route and the roundabout is dangerous due to its size which allows traffic to negotiate at speed.	There is ample room to accommodate segregated cycle lane on the roundabout and on the wide verges leading to the roundabout along both sides of Howlett road.	3	3	0	3	-2	1	8	Connectivity and Growth: A route using a cycling and pedestrian track around the back of Trimley St Martin and down to the western arm of the Howlett Way/A14 roundabout would have high connectivity and growth value if combined with the track recommended to run along Howlett Way. Modal Shift: Though expensive, this route is anticipated to have high modal shift value. Optimisation: New route so score of 0 for optimisation. Safety: Providing it was designed and engineered well (Would be on a slope for some of the stretch), it would provide full segregation from traffic until Howlett Way was reached. Biodiversity: Potentially high biodiversity loss due to the presence of mature trees (green buffer to attenuate noise, screen and filter air pollution from the A14) which are of unknown biodiversity value - and its likely that in order to create enough physical segregation (distance and barrier/buffer strip) from the A14 that people would actually use the track, a significant amount of these trees would have to be cleared to accommodate it. A -2 score is given as biodiversity value unknown. Leisure: Low leisure value as this route would not be expected to be taken for access to the NFGN or coastline, where leisure cycling is more likely to be an enjoyable experience.
Trimley St Martin	139	Morston Hall Road between Levington and Trimley	This is mostly a single track road with passing places used by cyclists as a commuting and leisure route between Ipswich and Felixstowe. The width of the single lane sections does not leave a lot of room for vehicles to overtake or for oncoming vehicles to pass and a large proportion of drivers see no reason to slow down when passing, so it can often feel unsafe for cyclists.	There is a very wide verge along the whole length of Morston Hall Road which could be converted to a dedicated cycle path or shared use path.	3	3	3	2	-1	1	11	Connectivity and Growth: The existing shared path between Goslings Farm track and Morston Hall Lane is recommended for improvement in the Strategy, as an integral part to the continuous scheme between Ipswich and Felixstowe - two currently relatively disconnected settlements via bike or on foot; a full score of 3 is given to reflect its important role in the overall scheme delivery. Modal Shift: PCT indicates that the highest level of quality scheme for full segregation from vehicles, a significant uplift for commuting and school journeys would be expected on this route, earning this proposal a full score of 3. Optimisation: A high quality cycle/pedestrian track here, particularly if it was able to be shifted more towards Morston Hall Road and away from the 14 (its currently immediately adjacent to the A14) would be replacing a poor quality track, and therefore has a full score of 3. Safety: A track in this location would provide for full segregation, however, as Morston Hall Road is seldom used by vehicles other than buses and local traffic, the uplift from a high baseline level of safety means a score of 2 was deemed appropriate. B: A score of -1 was given because of the loss of vegetation on the green buffer between Morston Hall Road and the A14 which may have had biodiversity value. L: As well as being a commuter route, the Ipswich to Felixstowe Key Corridor will, and may predominantly be, a leisure cycling route. Providing track here will increase the leisure value of the cycle. However, it is still not a particularly attractive location to cycle in at this point, so a score of 1 was given.
Trimley St Martin	173	Beside the Westbound A14 from where the High Road joins it to where it meets Felixstowe Road.	The cycle/walking path alongside the A14 is not only very unpleasant but dangerous with no barriers between cyclists and pedestrians and very fast moving large container trucks and cars. I have personally experienced angry car drivers, who believe that the road belongs to them, when cycling along this "passing places" road. There is adequate land alongside this road on the south side.	As described above. The safer and more pleasant route (and that which most cyclists take) is along the Morton Hall Road where a separate path could be constructed alongside this road. If the path was moved to this location a lay-by could be constructed beside the A14 to allow for parked container trucks, etc.	3	3	3	2	-1	1	11	Connectivity and Growth: The existing shared path between Goslings Farm track and Morston Hall Lane is recommended for improvement in the Strategy, as an integral part to the continuous scheme between Ipswich and Felixstowe - two currently relatively disconnected settlements via bike or on foot; a full score of 3 is given to reflect its important role in the overall scheme delivery. Modal Shift: PCT indicates that the highest level of quality scheme for full segregation from vehicles, a significant uplift for commuting and school journeys would be expected on this route, earning this proposal a full score of 3. Optimisation: A high quality cycle/pedestrian track here, particularly if it was able to be shifted more towards Morston Hall Road and away from the 14 (its currently immediately adjacent to the A14) would be replacing a poor quality track, and therefore has a full score of 3. Safety: A track in this location would provide for full segregation, however, as Morston Hall Road is seldom used by vehicles other than buses and local traffic, the uplift from a high baseline level of safety means a score of 2 was deemed appropriate. Biodiversity: A score of -1 was given because of the loss of vegetation on the green buffer between Morston Hall Road and the A14 which may have had biodiversity value. Leisure: As well as being a commuter route, the Ipswich to Felixstowe Key Corridor will, and may predominantly be, a leisure cycling route. Providing track here will increase the leisure value of the cycle. However, it is still not a particularly attractive location to cycle in at this point, so a score of 1 was given.
Trimley St Martin	248	Road Bridge Kirton Road to Old Kirton Road	This bridge and its approaches are not cycle friendly or indeed for anyone using a mobility scooter or pushing a pram. It is on a useful back route from Kirton to Felixstowe.	Upgrade paths on both sides & bridge to a more cycle friendly standard	2	1	2	1	0	2	8	Connectivity and Growth: Improving the bridge's quality for cycling by removing the barriers, improving the surfacing to, over and from the bridge and any necessary amendments to the height of the parapets to make it cycle-safe (as it was originally designed as a pedestrian bridge) is critical for cycle connectivity between Felixstowe/the Trimleys and Kirton, Brightwell Lakes, Martlesham and Woodbridge

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												(etc.). Modal Shift: There's no PCT data on cycling over the bridge, presumably because it is not a recognised highway and PCT only demonstrates highway use. Strava Metro shows some use of the bridge, though a strong preference for the use of Kirton Road and the Howlett Way roundabout, though this space is highly unsuitable for bikes. It is likely a direct result of the cycle barriers over the bridge, as both routes essentially take the cyclist to the same point on the High Road. A score of one is given. Optimisation: A score of two for optimisation is given for upgrading the current bridge as ideally the bridge needs to be fully replaced because it is too narrow and steep to be accessible to non standard bikes or suitable for bi-directional use. Safety: A score of 1 is given for safety as upgrading the bridge would not increase the current level (full) of segregation from traffic, or make it safer for cyclists/pedestrians to pass or overtake each other on it, but improvement of the landing sides to make them less steep/smooth would reduce acceleration leaving/climb when entering the bridge. Biodiversity: No anticipated biodiversity effects. Leisure: Improving the bridge would likely have a significant uplifting effect on leisure cycling, particularly if the upgrades are effective in shifting cyclists away from Howlett Way roundabout, which, is highly car dominated and therefore unpleasant to experience on a leisure cycle.
Trimley St Martin	267	Capel Hall Lane/Brook Lane/Back Lane/Lower Road	Create a network of Quiet Lanes between Trimley St Martin (Capel Hall Lane) and Falkenham Church via Brook Lane/Back Lane/Lower Road/Falkenham Sink	As above - requires only designation and signage.							N/A	Quiet Lanes are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Trimley St Martin	331	Morston Hall Road	Cyclists and motorists and sometimes bus drivers come into conflict on this stretch of road which can be intimidating and off-putting.	Use physical measures to deter motorists from using the road e.g. traffic calming. Provide segregated cycle/pedestrian track to one side.	3	3	0	3	-2	1	8	Connectivity and Growth - Morston Hall Road forms a section of the Ipswich to Felixstowe Key Corridor, and therefore in aggregation with the rest of the scheme, provides a segregated connection between Ipswich and Felixstowe, which earns a full Connectivity and Growth score. Modal Shift - As above, high potential for modal shift along the Key Corridor anticipated. Optimisation - This proposal does not optimise existing cycling infrastructure. Safety - As above. Biodiversity - A score of -2 is given due to the likely need to reduce the existing vegetation on the strip between Morston Hall Road and the A14, or Morston Hall Road and the verge to the south. L: A leisure score of 2 is given for this section as in aggregate the Ipswich to Felixstowe Key Corridor's improvements will allow for safer longer distance leisure cycles between the two settlements.
Trimley St Martin	339	Cycle path alongside A14 dual carriageway near Morston Hall Road	Using this cycle path is unpleasant and very scary being so close to fast moving traffic on the A14 with NO crash barrier. I prefer to use Morston Hall Road but this is not wide enough for cars to pass cyclists.	Provide a cycle path adjacent to Morston Hall Road away from A14.	3	3	3	3	-2	1	11	Connectivity and Growth - Morston Hall Road forms a section of the Ipswich to Felixstowe Key Corridor, and therefore in aggregation with the rest of the scheme, provides a segregated connection between Ipswich and Felixstowe, which earns a full Connectivity and Growth score. Modal Shift - As above, high potential for modal shift along the Key Corridor anticipated. Optimisation - Full segregation throughout the route earns a full score of 3. Safety - As above. Biodiversity - A score of -2 is given due to the likely need to reduce the existing vegetation on the strip between Morston Hall Road and the A14, or Morston Hall Road and the verge to the south. Leisure - A leisure score of 2 is given for this section as in aggregate the Ipswich to Felixstowe Key Corridor's improvements will allow for safer longer distance leisure cycles between the two settlements.
Trimley St Martin	378	Howlett Way, Trimley St Martin, along its full length	This road carries traffic travelling to and from the A14 junction 59. The volume of traffic and the 40mph speed limit discourages cyclists. A new development of 340 houses is planned with vehicular access off Howlett Way with the result that Howlett Way will become very much more busy. Cyclists travelling from the new development to Trimley St Mary, Walton and Felixstowe, including pupils travelling to school, will have to negotiate a stretch of Howlett Way in order to reach the High Rd.	Install a separate, kerbed cycleway	2	3	3	3	0	1	12	Connectivity and Growth: This route is highly valuable for permeability to and from the site and east to west connections between the Trimleys (and beyond) and the NFGN via/to/from the two allocations (SCLP12.64 and SCLP12.65), and over to the west for the route down to The Port. Score of 2 given. Modal Shift: High modal shift anticipated associated with high quality infrastructure between the Land at Howlett Way site and the (relocated) Trimley St Martin Primary School by virtue of the high quality infrastructure to be continuously available between them. This route, the 'Dutch style' roundabout anticipated at Hogh Road and the shared paths through the Land Adjacent to Reeve Lodge site will together provide a safer transition over to the route down to The Port, which provides an opportunity for high levels of modal shift for new residents of both of these sites. Optimisation: Score of 3 given as this is a significant improvement on the current earth desire line. Safety: As above, plus priority crossings are expected over the arms of the two new roundabouts. Biodiversity: No anticipated effects. Leisure: Low anticipated leisure value, as Footpath 26 is anticipated to remain a footpath.
Trimley St Martin	379	The village of Trimley St Martin and its links to neighbouring villages	As a result of local plan allocations the number of dwellings in Trimley St Martin will increase by 630 which is over 50%. This is likely to result in traffic congestion and increased danger for those walking and cycling, but it also provides the opportunity to make significant improvements to encourage cycling.	The first step should be to conduct a full and detailed review of cycling within and around the village looking at the possibility of creating new off-road cycle routes as well as improving the provision for sections where on road routes are unavoidable.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. Trimley St Martin is part of a key corridor so has been considered in greater detail.
Trimley St Martin	380	Old Kirton Road, Trimley St Martin, Footbridge over A14	The existing pedestrian bridge across the A14 is not cycle-friendly	Widen the bridge and create a cycleway which would join both sides of the A14	3	3	3	0	0	3	12	Connectivity and Growth: This bridge is of high importance for direct connectivity to Trimley St Martin (Howlett Way roundabout is not advised for pedestrians or cyclists, and no improvements that would facilitate its use by pedestrians/cyclists it have been included in the Strategy), though the alternative of a bi-directional track to the east of Kirton Road from opposite Roselea Nursery down the North Felixstowe Garden Neighbourhood has been included, and can be used as an alternative access via Thurmans Lane. This is obviously a substantial diversion if a cyclists/pedestrian is looking to access Trimley St Martin, and therefore the relevance of the existence/location of the bridge is high. Modal Shift: No PCT data, but considered to have high overall modal shift value due to location between Kirton and Felixstowe, and location within Woodbridge to Felixstowe via Brightwell Lakes route. Optimisation: Full score for optimisation if the bridge had to be replaced. Safety: Full score for safety as a bridge segregated from vehicles is beneficial. Biodiversity: Bridge replacement considered unlikely to have any biodiversity affect. Leisure: A

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												replacement bridge would have high leisure value for cycling between Felixstowe and the (west of the) Deben estuary villages, as the current bridge restricts cycling.
Trimley St Martin	442	Cycle path alongside A14 between Goslings Farm & Levington turn-off	cycle path surface quality is VERY poor throughout this stretch - very bumpy, strewn with debris, high risk of punctures. This increases temptation to ride along the parallel bus route (Morston Hall Road) which is fast and smooth, but not intended for cyclists and probably slightly dangerous and may cause delays for buses etc.	properly resurface (not just patch up) this fairly short stretch of cycle path, with a slight camber to keep surface clear of debris and standing water. Also trim back adjacent hedges. At same time consider widening the cycle path to allow two cycles to pass in opposite directions - there appears to be sufficient space for this, along most of the stretch at least.	1	3	3	3	-1	1	10	<p>C&G: The existing shared path between Goslings Farm track and Morston Hall Lane is recommended for improvement in the Strategy. The Strategy also recommends a cycle/pedestrian track along the eastern side of Felixstowe Road 'east' (Old Felixstowe Road), which would serve the turn off to Levington. As an integral part to the continuous scheme between Ipswich and Felixstowe - two currently relatively disconnected settlements via bike or on foot; a score has been given to reflect its important, but as an existing cycle path and existing connection a full score has not been given.</p> <p>M: PCT indicates that, with the highest level of quality scheme for full segregation from vehicles, a potentially significant uplift for commuting and school journeys would be expected on this route, earning this proposal a full score of 3.</p> <p>O: A high quality cycle/pedestrian track here, particularly if it was able to be shifted more towards Morston Hall Road and away from the 14 (its currently immediately adjacent of the A14), and redundant carriageway in the dualled section of Felixstowe Road 'east' was used, this would be (a) be replacing a poor quality track, and (b) be making use of redundant carriageway space, and therefore has been given a full score of 3.</p> <p>S: A track in this location would provide for full segregation, which is most useful along the Felixstowe Road 'east' section, which can be heavily trafficked.</p> <p>B: A score of -1 was given because of the loss of vegetation on the green buffer between Morston Hall Road and the A14 which may have had biodiversity value.</p> <p>L: As well as being a commuter route, the Ipswich to Felixstowe Key Corridor will, and may predominantly be, a leisure cycling route. However, it is still not a particularly attractive location to cycle in at this point (adjacent to A14), so a score of 1 was given.</p>
Trimley St Martin	495	Cycle path adjacent to Trimley to Levington link road	This path is in a very poor state with many uneven bumps and potholes, and is also dangerously close to a fast section of the A14. As a result, many cyclists choose the link road, slowing vehicular traffic and causing drivers to be impatient.	Ideally, the path should be re-sited to run alongside the link road, far safer. In short term, it should be resurfaced and a sturdy barrier placed to shield it from the A14	3	3	3	2	-1	1	11	<p>Connectivity and Growth: The existing shared path between Goslings Farm track and Morston Hall Lane is recommended for improvement in the Strategy, as an integral part to the continuous scheme between Ipswich and Felixstowe - two currently relatively disconnected settlements via bike or on foot; a full score of 3 is given to reflect its important role in the overall scheme delivery.</p> <p>Modal Shift: PCT indicates that, with the highest quality scheme for full segregation from vehicles, a potentially significant uplift for commuting and school journeys would be expected on this route, earning this proposal a full score of 3.</p> <p>Optimisation: A high quality cycle/pedestrian track here, particularly if it was able to be shifted more towards Morston Hall Road and away from the 14 (its currently immediately adjacent of the A14) would be replacing a poor quality track, and therefore has a full score of 3.</p> <p>Safety: A track in this location would provide for full segregation, however, as Morston Hall Road is seldom used by vehicles other than buses and local traffic, the uplift from a high baseline level of safety means a score of 2 was deemed appropriate.</p> <p>Biodiversity: A score of -1 was given because of the loss of vegetation on the green buffer between Morston Hall Road and the A14 which may have had biodiversity value.</p> <p>Leisure: As well as being a commuter route, the Ipswich to Felixstowe Key Corridor will, and may predominantly be, a leisure cycling route. Providing track here will increase the leisure value of the cycle. However, it is still not a particularly attractive location to cycle in at this point, so a score of 1 was given.</p>
Trimley St Martin	528	There is no safe way for pedestrians to cross between Howlett Way and Kirton Road.	When crossing from Trimley St Martin on Howlett Way, the verge on the right hand side is totally overgrown with brambles, forcing the user onto the road which is very unsafe. Having crossed the slip road of the A14 from Felixstowe, crossing the sliproad to Felixstowe is difficult because of poor visibility of vehicles coming round the roundabout and onto this sliproad fast.	A pedestrian crossing controlled by traffic lights would be the only safe way. The brambles would also need to be cut back too regularly.	1	0	3	3	0	1	8	<p>Connectivity and Growth: This is highly relevant given the intensification of this area arising from the build out of allocated sites SCLP12.65 and SCLP12.64, which will lead to more pedestrian and cycle movements - particularly as both are set to have high quality cycling and walking infrastructure incorporated into them. A signalised crossing is therefore important for connecting journeys from this settlement into the Trimleys, however, opportunities to cross at the western end where the roundabout with the High Road is, provides an acceptable alternative. Score of 1 is given. Modal Shift: A crossing is unlikely to create significant modal shift on its own. Score of 0 given. Optimisation: A signalised crossing for both cyclists and pedestrians would earn a top score due to the uplift on the current crossing point. Safety: As above. Biodiversity: No foreseen biodiversity impact. Leisure: Low leisure uplift from being able to extend walks more safely between the Trimleys, the site, and over to the North Felixstowe Garden Neighbourhood/countryside to the east.</p>
Trimley St Martin	545	Kirton Road, parallel to A14, Trimley St Martin. Unlit country road.	Trees growing to road edge, leaving no walking space, also forces cyclists out further out into traffic. The verges have been mown, but under the trees	Cut back trees as far as ensibly possible							N/A	This proposal has been scored '0' in each of the MCAF categories because it relates to an issue more appropriately dealt with directly by the Highways Authority (e.g. highway maintenance, speed reductions), rather than through the Strategy.
Trimley St Martin	570	Trimley St. Mary to Kirton via Howlett Way (and return)	There is a known history of accidents involving motor vehicles and cyclists on this route, sadly including the recent death of a cyclist as a result of a collision with a	Provision of a clearly marked cycleway along the roads connecting the Trimleys to Kirton via Howlett Way, including the roundabouts, to give better protection to	1	1	0	3	0	0	5	Connectivity and Growth: Assessment based on a fully segregated bi-directional track provided using absorbed excess carriageway space and highway verges along this route from the western end of Howlett Way up to Kirton via Kirton Road (or at least

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			motor vehicle entering the roundabout via the A14 eastbound off-slip junction. The roads connecting the Trimleys to Kirton via Howlett Way, including this roundabout, could conceivably accommodate a safe and clearly marked cycleway offering better protection to cyclists and improving drivers' awareness of other road users.	cyclists and improving drivers' awareness of other road users.								to opposite Roselea Nursery). A score of 1 is given as alternatives are available. Modal Shift: PCT suggests quite low levels of MS would be achieved. Score of 1 given. Optimisation: New route so cannot be scored under this category. Safety: Full segregation and therefore full score. Still may not be considered a pleasant route due to the proximity to the A14, even despite a means of segregation. Biodiversity: No anticipated negative effects. Leisure: No particular leisure benefit anticipated.
Trimley St Martin	573	SCLP12.65 New Primary School	A new safe cycleway (preferably segregated) will be required to get Kirton and Falkenham Children to and from the new Primary School at SCLP12.65 . Many children from Kirton go to the existing Trimley St Martin Primary school and as it is being moved provision needs to be made for a safe access cycle path from the new site to Kirton and Falkenham.	The footpath over the A14 is the obvious route. There is adequate land from Kirton Green on the western side of Trimley Road(in the same ownership as land that the school is being built on) to accommodate a segregated path through to Roselea Nursery and thence to the footbridge. A new safe route would then be needed to access the school. This could form the basis of an interconnected route which would benefit East Suffolk's Climate change and Greener Future Agenda	3	2	0	3	-1	3	10	Connectivity and Growth: This route forms part of the Woodbridge to Felixstowe via Brightwell Lakes route, which is of high C&G value as the (rural) on-carriageway route cyclists/pedestrians would have to take now is quite unsuitable for cycling due to reduced visibility and speeding on rural roads, and unsuitable for pedestrians as there is a lack of pavement/surfaced footpaths, making it less accessible to walks. It takes a different route to the roads as these were considered difficult to create parallel segregation on. The stretch from Kirton to the A14 bridge to Trimley St Martin does have a pavement but it is narrow and therefore not up to accessibility standards for minimum 1.5 wide pavements. This route will principally be for leisure overall, but this section will be useful for school runs between Kirton (and possibly surrounding villages) to Felixstowe Schools - as noted by the respondent the Trimley St Martin Primary School and also likely Felixstowe Academy, too. Modal Shift: This section has a score of 2 as it is part of a larger (mostly off-road and therefore not assessable using the PCT) route between Felixstowe and Woodbridge via Brightwell Lakes, that is considered likely to create some modal shift. Optimisation: A score of 0 is given under this category as a new cycling/walking track from Kirton to the A14 bridge to run parallel to (but separate from) Kirton Road is an entirely new stretch of cycling/walking infrastructure. Safety: As above Biodiversity: Some loss of farmland along Kirton Road, which is likely to be of low biodiversity value. Leisure: As noted above, this route will principally be for leisure.
Trimley St Martin	582	Cars parked near the shop	Highly dangerous to cycle past the shop area (in particular in the east direction) due to slowing / stopping cars that are parking for the shop, also cars pulling out after using the shop. Frequent near misses due to poor awareness of cycling traffic. Cycle lane is constantly parked on. The road is also very narrow at this point.	20 mph zone? mandatory cycle lane? Dedicated parking bay surrounded by double yellow lines? parking enforcement?							N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Trimley St Martin	635	Between the footbridge over the A14 in Trimley Saint Martin and Capel Hall Lane.	There is no safe way for pedestrians of reaching Capel Hall Lane from the footbridge. There is a footpath marked on the ordnance survey map 197 which is part of the Stour and Orwell walk. There is no safe way to reach this footpath from the footbridge.	Create a safe route from the footbridge to the footpath. Only a short section is required. This could be done by having steps put in between the bridge approach and the path. Alternatively, cutting back the brambles along Kirton Road so it is safe to walk on the verge.	1	1	0	0	0	3	5	Connectivity and Growth: A score of 1 is given, as: (a) this connection would be a useful addition to the safety of the Candle Track to Kirton Road transition, which may be an important walking and cycling leisure route for residents of the western side of the NFGN, and may benefit businesses on Kirton Road, which has a continuous pavement up to Kirton from the landing area of the footbridge onwards. And; (b) Kirton and Trimley St Martin are not otherwise reasonably accessible by foot with segregation - this would require a long walk down Candle Track to Thurmans Lane, which most people wouldn't do to access Kirton - they would risk the quick connection up the southern-most stretch of Kirton Road; this stretch is potentially made more difficult by speed gain from people exiting from the roundabout having been on the A14. It also appears to have poor visibility around its curves, particularly in the summer months when vegetation is dense. According to StravaMetro, the route between High Road, Howlett Way roundabout and Kirton Road is actually used much more by cyclists than the A14 footbridge (which is still well used). This may be due to the barriers on the bridge, and therefore need to dismount, acting as a deterrent. The complete reverse is true for pedestrians, that almost exclusively use the bridge - likely due to the lack of pavement down the southern-most side of Kirton Road. Falkenham is also accessed a significant amount by both active user types via Capel Hall Lane. Activity picks up again along Candle Track - which demonstrates an obvious infrastructure gap - and demand for its closure - between Kirton Road and Candle Track. Modal Shift: It would not be likely to have a significant uplifting impact on commuting numbers, due to its location. However, surprisingly, PCT suggests an uplift of around 80 commuter cyclists with e-bike standards applied to this stretch. Score of 1 therefore given. Leisure: Full score given due to ability to close a 'safety gap' caused by the lack of active infrastructure around the Kirton Road stretch immediate to the Howlett Way roundabout, which has none currently. This would provide safe passage over to Candle Track, which is of (and increasingly of) high leisure value.
Trimley St Martin	640	Enable footbridge to take cyclists so they do not use A14 Roundabout	The A14 Roundabout is perilous for cyclists - enable the footbridge to take cyclist and pedestrians safely	Widen foot bridge and encourage cyclists to use bridge rather than attempting the A14 Roundabout from Kirton to Trimley St Martin, cars need to slow down. This is where a number of accidents have taken place with cyclists.	2	0	1	1	0	1	5	Connectivity and Growth: The bridge is usable by most cyclists currently providing they dismount to navigate the barriers. However, there is growth potential from removing the barriers to open up the accessibility of the bridge to more cyclists, and upgrading its legal status (so its current use by cyclists can be legitimised). There is further growth potential if the bridge is fully replaced with a newer wider bridge, as this opens up its accessibility further and increases its attractiveness. However, this growth potential is limited unless the bridge is actually coming to the end of its working life soon, anyway. The bridge is critical for the Woodbridge to Felixstowe (via Brightwell Lakes) corridor, however the current bridge is acceptable in dimensions for use by most cyclists, particularly once the barriers are removed; it is therefore not considered a necessity to replace it for the key corridor. Upgrading its legal PROW status to bridleway and foot/cycle bridge is critical. Score of 2 is given on the strength of increasing its accessibility and making its use legal for the key corridor. Modal Shift: The number of people likely to benefit from its upgrade is relatively small due to small populations in the west-of-the-Deben villages, and the long distance between Woodbridge and Felixstowe (though more accessible on an e-bike).

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												Reaching Felixstowe from the west-of-the-Deben also necessitates some hill climbing, which will be a deterrent from commuting by bike for many unless they have a reliable e-bike. Therefore the uplift in cycling trips is likely to be small from a BCR perspective. Presumably because the bridge currently has footpath status, there's no Propensity to Cycle Tool data to inform modal shift. Score of 1 is given on the strength of 0 is given, with accessibility benefits captured under optimisation below. Optimisation: The bridge is already well used by cyclists, even if not legally. Removing the barriers and widening the bridge would make it more accessible, and would therefore constitute an infrastructure upgrade. Replacing the existing bridge with a newer, wider and more attractive bridge, and removing the barriers will increase the accessibility of the bridge to a wider range of cyclists (e.g. cargo bikes). However, in BCR terms this is unlikely to massively uplift commuting and sustainable trade (through cargo bikes) without being part of a more comprehensive scheme (which is intended for the Felixstowe to Woodbridge via Brightwell Lakes key corridor, but not part of the respondent's proposal), but will on its own at least improve leisure cycling experiences at least somewhat. Score of 1 given.
Trimley St Martin	736	Cycle way along A14 from Goslings	The cycle way along A14 from Goslings onwards is poorly maintained.		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because it relates to an issue more appropriately dealt with directly by the Highways Authority (e.g. highway maintenance, speed reductions), rather than through the Strategy.
Trimley St Martin	762	See attached documents	See attached documents		1	0	2	0	-1	2	4	Connectivity and Growth: A score of 1 was given due to the enhanced connectivity between the North Felixstowe Garden Neighbourhood (NFGN) and Falkenham, and therefore alternative access to Kirton and onward travel towards Brightwell Lakes/Woodbridge. Modal Shift: No significant Modal Shift anticipated. Optimisation: Uplift of 2 due to the opening up of cycling between the NFGN and Falkenham and improved drainage (SUDS would be expected to be co-delivered with the infrastructure scheme). S: No uplift in safety because baseline of safety from vehicles is very high in this location. B: -1 given due to potential loss of field-edge vegetation on this route, which is of unknown biodiversity value. L: Score of 2 is given due to anticipated principal use of the route as a rural off-road leisure route.
Trimley St Martin	120a	A14 cycle path Felixstowe to Levington	The cycle path is in a terrible state of disrepair, overgrown and strewn with debris. It is not maintained and the surface is dangerously uneven. It's also frighteningly close to A14 traffic. Because of these issues it's considered by most cyclists to be unusable, and certainly not safe for families with children.	The cycle path was installed prior to the single track link road which now runs beside it. It would be great if the cycle path could be relocated to nearer the quieter link road and away from the A14.	3	3	3	2	-1	1	11	Connectivity and Growth: The existing shared path between Goslings Farm track and Morston Hall Lane is recommended for improvement in the Strategy, as an integral part to the continuous scheme between Ipswich and Felixstowe - two currently relatively disconnected settlements via bike or on foot; a full score of 3 is given to reflect its important role in the overall scheme delivery. Modal Shift: PCT indicates that, with the highest level of quality scheme for full segregation from vehicles, a potentially significant uplift for commuting and school journeys would be expected on this route, earning this proposal a full score of 3. Optimisation: A high quality cycle/pedestrian track here, particularly if it was able to be shifted more towards Morston Hall Road and away from the A14 (its currently immediately adjacent to the A14) would be replacing a poor quality track, and therefore has a full score of 3. Safety: A track in this location would provide for full segregation, however, as Morston Hall Road is seldom used by vehicles other than buses and local traffic, the uplift from a high baseline level of safety means a score of 2 was deemed appropriate. Biodiversity: A score of -1 was given because of the loss of vegetation on the green buffer between Morston Hall Road and the A14 which may have had biodiversity value. Leisure: As well as being a commuter route, the Ipswich to Felixstowe Key Corridor will, and may predominantly be, a leisure cycling route. Providing track here will increase the leisure value of the cycle. However, it is still not a particularly attractive location to cycle in at this point, so a score of 1 was given.
Trimley St Martin	120b	A14 cycle path Felixstowe to Levington	The cycle path is in a terrible state of disrepair, overgrown and strewn with debris. It is not maintained and the surface is dangerously uneven. It's also frighteningly close to A14 traffic. Because of these issues it's considered by most cyclists to be unusable, and certainly not safe for families with children.	Or maybe an alternative route could be considered following the river Orwell to give traffic free access right into Ipswich?	2	0	0	-1	-3	3	1	Connectivity and Growth: Though not likely to be considered deliverable due to the designations restraints and floodplain restraints, a route along the River Orwell would have moderate connectivity and growth value. However, the Ipswich to Felixstowe Key Corridor is intended to serve this purpose. Modal Shift: No PCT or StravaMetro data to support the route; Ipswich to Felixstowe Key Corridor is intended to serve this purpose. Probably also, due to it being further out than the Key Corridor route, it would be unlikely for this route to be used for commuting (more as a longer distance leisure route). Optimisation: N/A would be a new route. Safety: Cautious -1 score given as route is on a floodplain unless well engineered *likely at high cost) this would likely effect the useability and surfacing quality of the route. Biodiversity: Potential high environmental impact. Leisure: High leisure value
Trimley St Mary	25	High Road , Trimley.	Cars parked on cycle lane, necessitating cyclists moving out and in from main road repeatedly. Cycle lane disjointed with many short sections.	Ban parking in cycle lane. Have one continuous cycle lane. Similar problem exists in many other areas in Felixstowe with disjointed cycle lanes.	0	1	1	1	0	0	3	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The proposal doesn't create new infrastructure, however significant parking over the on-road cycle lane does reduce its effectiveness. As an on-road cycle path is the lowest standard, it was assessed against the PCT lowest standard and resulted in 1 point. Optimisation – Removing the parked vehicles doesn't create new infrastructure, but optimises the existing giving a point. Safety – The road is 30mph (i.e not a fast road), but the parked vehicles result in cyclists having to continually move to the centre of the road or cycle continuously in the prime position; the advisory stretch is also quite long, meaning cyclist's safety may be compromised for a

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												significant amount of time when parking in the cycle lanes is extensive (as may be expected at school pick up/drop off times); one points for safety is therefore considered reasonable. Biodiversity – No significant biodiversity benefit. Leisure – There appears to be no significant leisure benefit.
Trimley St Mary	35	Trimley St Martin	6 pathways leading to open countryside have been closed across the railway line. This hardly promotes improved walking and cycling access.	Reinstate those crossings where there is still only one track to cross so not making the pathways any less safe than before.	0	-1	0	0	0	1	0	Connectivity and Growth – Any crossing would provide cohesion to PROW routes but offers limited connectivity opportunities to residential areas, services or employment. There is potential to use this route to connect through to the Port but several crossing points are available and the Port would be a significant distance meaning this opportunity is limited. Modal Shift – The existence of level crossing points on railway lines (rather than bridges over them) may limit the maximum speed a line can operate at, which in turn reduces the attractiveness of modal shift via train as it extends the journey duration due to the lower speed. The numbers using this path is unlikely to lead to a modal shift particularly as most users will likely be recreational users. Optimisation - There is potential that the proposed crossing points will provide a limited improvements to the existing routes, however other crossing points are available. Safety – The alternative routes that any pedestrian or cyclist is forced to take does not appear to represent a hazard. Biodiversity – There are no significant biodiversity impacts Leisure – The PROW pathways are largely used for leisure purposes and Strava suggests they have reasonable use. Whilst there are other crossing points available more direct crossing points will provide the most leisure benefit.
Trimley St Mary	115	Trinket high road	Cycle lane markings are virtually invisible and need re painting.	Re mark cycle lanes							N/A	This issue is a more highway specific matter and have been shared with SCC for their consideration as the Highways Authority.
Trimley St Mary	265	Blofield Track (from Cordys Lane, Trimley St Mary to Nicholas Road, Port of Felixstowe Campus	Upgrade to decent surface for the whole extent - this is a bridleway (BW12) much used by cyclists from Trimley to the Port which avoids busy main roads, but the surface is very poor.	The first "section", as far as the north-west entrance to the new Gun Club site (near the junction with FP30), is very rough, with large stones exposed. The second "section", from the north-west entrance to the Gun Club to the railway overbridge is basically a mud track, which is often flooded at, and near, the railway bridge - making it barely passable. The third "section" from the railway bridge to Pentalver's Yard on Nicholas Road is generally in better condition, although it does puddle in places. Th entire extent needs surfacing with an all weather surface.	2	2	2	3	0	0	9	Connectivity and Growth: Improvements to this section are included in the Strategy as part of the route between the Port and Trimley St Martin (running along the western edge of the Trimley villages and Felixstowe); this route is the main route that would be taken from the Trimley villages to the Port as a route via the High Road, even after retrofitting with intermittent cycle lanes, would still relatively hostile in comparison. It therefore has very high C&G value; scored at 2. Modal Shift: It is expected that a significant number of Port employees will live in the Trimley villages, which suggests high potential for modal shift with improvements to this relatively direct route (assuming Bridleway 12 is accessed via Cordy's Lane or the recommended PROW improvements to/from the bridleway bridge west of Gun Lane). The development of the two allocated sites (SCLP12.64 & SCLP12.65) will likely add further demand for the Port route and potential for modal shift from the car to cycling/walking to The Port. Optimisation: Score of 2 given as baseline is already segregated. Safety: Fully segregated so full score given. Biodiversity: No adverse effects anticipated. Leisure: No leisure value anticipated, purely intended for commuting.
Trimley St Mary	266	Upgrade Bridleway 12 (Trimley St Mary) - from Cordys Lane, Trimley St Mary to Nicholas Road on the Port of Felixstowe campus -to all weather surface	Very poor surface on this bridleway, much used by cyclists and walkers avoiding the busy Trimley High Road/High Street/High Road West/Garrison Lane/Langer Road/Walton Avenue route from the Trimley villages to the Port of Felixstowe	The first "section", as far as the north-west entrance to the new Gun Club site (near the junction with FP30), is very rough, with large stones exposed. The second "section", from the north-west entrance to the Gun Club to the railway overbridge is basically a mud track, which is often flooded at, and near, the railway bridge - making it barely passable. The third "section" from the railway bridge to Pentalver's Yard on Nicholas Road is generally in better condition, although it does puddle in places. The entire extent needs surfacing with an all weather surface.							N/A	Response not scored as is a duplicate of 265.
Trimley St Mary	270	Trimley St Mary Bridleway 14: Clickett Hill Road to Nicholas Road	The area immediately to the west of Clickett Hill Road becomes very damp and muddy over the autumn-winter-spring period and needs to be surfaced - as part of Suffolk Cycle Route 5	As above							N/A	Response not scored as is a duplicate of 265.
Trimley St Mary	316	Level crossing from Fagbury Road	On occasions the gates governing access across the level crossing are electronically locked for no apparent reason. It is not seem possible to predict when this may occur. This results in a significant detour to the nearest available level crossing which is a considerable distance away. The risk is that frustration will lead to persons crossing the railway when unsafe to do so.	If there is a need for the gates to be temporarily locked for safety reasons, there needs to be a way for a pedestrian or cyclist to find out how long the delay will be and/or to contact someone in control of the locking mechanism to request access.	0	0	0	-3	0	0	-3	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: No optimisation benefit anticipated from the proposal. Safety: The Strategy does not recommend improvements to this route, which is a bridleway crossing the Port's railway into a field of leisure cycling value. The context of the crossing is a highly industrial and utilitarian environment that would be difficult to improve for safe pedestrian/cyclist use without compromising its function or incurring significant expense. The gates are likely to be being locked in accordance with the operation of trains, and therefore adjusting this system would pose a hazard to safety. Combining the safety risk with the existence of reasonable alternative routes available for entry into this field, its improvement is unlikely to come forward. The Strategy instead suggests the improvement of Parker Avenue, Nicholas Road, Blofield Track (BW12 & BW14), FP30 (upgraded to bridleway) and FP32B for onward travel. From the description it sounds like it would be difficult to secure a safe means of pedestrians/cyclists being able to reliable cross the crossing via the mechanised gate, which is unlikely to be manned and instead connected to a timed system. Therefore, if a new crossing were to be introduced here, it would need to be via a new bridge over the railway lines - this would be much more reliable and much safer than waiting for the gate and crossing the railway tracks. Assessment based on leaving the gates unlocked. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
Trimley St Mary	320	High Road Trimley nr Faulkeners Way	Cars parked in cycle lane and even on cycle path approaching mini roundabout.	Solid white lines and no parking in bike lanes with enforcement.	0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because unprotected cycle lanes (advisory and mandatory lanes, created using painted lines) in this location are not considered to be adequate to meet LTN 1/20 Cycle Infrastructure Design standards in this location.
Trimley St Mary	340	End of Thurmans Lane, Trimley & Bridleway to Gulpher Road	The bridleway provides a safe link from Thurmans Lane to Gulpher Lane towards north Felixstowe and the ferry. The condition of the path is poor and rutted in places and becomes muddy.	Improve surface of the Bridleway	1	2	1	1	-1	3	7	Connectivity and Growth - This route forms a key section of the connection between Trimley ST Martin, Kirton and the west of the Deben villages and the NFGN, which in turn facilitates onward travel to Felixstowe Ferry and the north-eastern section of Felixstowe's coastline. However, as an existing bridleway, it scores lower in this section. Modal Shift - Full modal shift potential cannot be calculated through the PCT due to it being off-road, however it is anticipated it will be a highly valuable commuter connection post-infrastructure delivery between the NFGN and Trimley St Martin/Kirton (and beyond). A score of 2 (rather than 3) is given as employment opportunities and access to education (i.e. necessitating school runs) in Trimley St Martin and Kirton are limited. Optimisation - Uplift from earth track to cycle/pedestrian track is scored at 2 because, though most of it is already segregated from traffic (and therefore a significant uplift in quality from the baseline would not be delivered as this is already high, particularly for mountain bikes and pedestrians as they are most able to access it), the eastern most section still includes vehicular access to a small number of properties west of Gulpher Road/on Candlet Track, making segregated infrastructure her more valuable. Safety - 1 as above, small uplift in what is already a fairly safe cycling route in terms of segregation from vehicles. Biodiversity - A score of -1 is given for biodiversity due to the necessary loss/absorption of green space for providing an LTN 1/20 compliant scheme. Leisure - Likely to be a highly valued leisure cycling route for local cycling, particularly cycling with children due to the high degree of segregation from vehicles intended for this route in the future.
Trimley St Mary	543	Gaymer's Lane	A safer way to cycle to Trimley was via a path on to Gaymer's lane (then the new Bridle way) from St Stennetts Close, (come up the Avenue) but someone has now blocked this.	removal of barrier	0	0	1	0	0	0	1	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: Removal of the barrier will improve internal permeability within this area of Trimley St Mary. Safety: Removal of the barrier will improve internal permeability within this area of Trimley St Mary. Biodiversity: No anticipated biodiversity benefits. Leisure: No anticipated uplift in leisure.
Trimley St Mary	587	The track beyond Cordy's lane that goes as far as the nature reserve.	The surface has improved recently but is still not suitable for running/cycling due to the inconsistent surface and large stones. This is a huge missed opportunity for recreation for this part of Trimley St Mary.	Durable resurface suitable for light foot traffic.	0	0	2	0	0	3	5	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: This proposal aligns with the Strategy's recommendation for the creation of a small circular leisure route that goes around the inside perimeter of this field, created using Cordy's Lane and bridleways 24, 25, 26, 16, 37 and 12). This is mostly intended for dog walking, running and leisure walking, though it is recommended that the surfacing be appropriate for leisure cycling, too. The Strategy also suggests, as an extra measure, that works to create a circular leisure track includes tree planting. This is to increase drainage of the field, and provide health, wellbeing and biodiversity benefits. Assessment based on this surfacing also being suitable for cycling. Given a score of 2 because the existing route is already segregated from traffic and is useable (if less accessible than if it were surfaced). Safety: No significant anticipated safety benefit. Biodiversity: No anticipated significant biodiversity impact. Leisure: See optimisation - high leisure value anticipated.
Trimley St Mary	658	Clickett Hill Road at junction thereof with entrance to new Unilever development and existing footpath / Cycle Route 51 to east & north	This point should be seen as the Core Hub for a range of improved (short term) or new (long-term) routes around North Felixstowe and Trimley, and to Ipswich and to Martlesham / Woodbridge. It has good but not always well maintained access to Western Felixstowe, although of uncertain public access status. The access towards Trimley is generally useable, but of varying quality, as well as status.	Options which should be explored:a) A new safe crossing of the now busy Clickett Hill Road as HGV access to the current Unilever development b) A new route adjacent to the western perimeter of the Unilever development to link with Footpath 30 railway crossing to the north and then onwards to the Deben valley, including linking with the forthcoming Felixstowe Garden Village development.b) Provision of a new Pedestrian / Cyclist route adjacent to Clickett Hill Road to the south to improve safety of access to the port employment areac) A consistent standard of surface and access rights on the existing Route 51 to Trimley High Roadd) A major new strategic initiative to provide a much more cycle friendly route to Ipswich than the current Route 51. Specifically the lanes through Levington and Nacton are not seen as cycle friendly due to the combination of their twisting nature and traffic levels / speeds. However the challenges of this are recognised to be significant.	2	2	2	1	0	0	7	Connectivity and Growth: Upgrading FP30 to a bridleway, teamed with the necessary improvements between Blofield Track/BW12/BW14/Clickett Hill Road helps to provide a traffic free transition between the High Road and the Port - and corroborates the routing of the Orange/Port route already proposed (which instead heads down Nicholas Road/Parker Avenue to avoid Trinity Avenue). Modal Shift: There is clear but moderate demand for both Clickett Hill Road and Nicholas Road, though Nicholas Road is slightly higher - likely due to its better connectivity for onward travel. There is clear demand for improvements to Bridleways 12 and 14 and Footpaths 32B and 30. As the majority of this proposal overlaps with the Strategy's recommended route between the Trimley villages and Port, a high modal shift score is given. Optimisation: These routes are already traffic free, so the uplift in quality to LTN 1/20 standards is moderate. However, the surfacing is poor, rocky and understood to be prone to flooding, and therefore resurfacing (teamed with better drainage) in this location is likely to create a substantial uplift in quality from the current baseline. Score of 2 given. Safety: As above - score of 1 given. Biodiversity: No anticipated adverse effects. Leisure: 0 as not intended to be a leisure route.
Trimley St Mary	672		There are a couple of areas on this road that are pinch points and of particular danger to cyclists, not least outside the school entrance on the High road and near McColls shop. If there are no plans to re-paint or enhance the cycle lane provision in this area, are there any other plans to address road safety issues in these areas?	The Parish council are also keen to find out if there would be any funding available to introduce a mini roundabout at the High road / Station road junction. This would reduce speeding in the immediate area as well as improve the road junction. Extend the temporary 'mandatory' cycle lane through Walton and then through Trimley St Mary / Trimley St Martin	0	1	1	1	0	0	3	Connectivity and Growth: No new connections made. Modal Shift: PCT data not applicable for the mini roundabout. However, it is reasonable to presume in this instance that improving this junction for cyclists and drivers may support modal shift to train travel (from Trimley train station). It is however not joined up and comprehensive in nature without it also being teamed with significant tracks of segregated cycle lanes to and from this junction; its positive impact is therefore limited, and a 1 is given. Optimisation: 1 is given for the upgrade to the east-bound cycle lane around the roundabout, which though not suggested by the respondent, is reasonable given as a co-delivery with the roundabout as pavement would need to be absorbed, anyway. However, again, it is not a comprehensive improvement and therefore its positive impact is limited and a 1 is given.

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												Safety: Main benefit safety-wise would be a painted box before the roundabout and an advisory transition lane to bring east-bound cyclists turning right down Station Road into the primary position in an 'anticipated' way, which is an improvement on the current design which does not include a turning box for either road user, or an advisory transition lane for cyclists turning right. Biodiversity: 0 Leisure: 0
Trimley St Mary	690	Fagbury Rd level crossing (Walking)	Both the route to the crossing and the crossing itself have minimal signage, approach is "hostile" – appears to be private haulage yard. Safety issues of the actual railway crossing need investigation and explanatory signage. Are there not Security issues regarding access to the Port railway system? It is also likely to be a critical link on the National Coast Path, underlining the need for safety and signage for non-local users.		0	0	0	-3	0	0	-3	Connectivity and Growth: No foreseen connectivity and growth benefits. Modal Shift: No anticipated modal shift benefit. Optimisation: No optimisation benefit anticipated from the proposal. Safety: The Strategy has not incorporated improvements for this crossing into its recommendations, but instead recommends the improvement of Parker Avenue, Nicholas Avenue and bridleways 12 and 14, and footpath 30 to increase permeability through to the field to the north-west (where a circular leisure route for walking, cycling, running and dog walking is recommended to be established). This is considered to be safer and more reliable, and less expensive than constructing an accessible pedestrian and cycle bridge (if practically possible) over these railway lines to the field. Assessment based on signage to alert pedestrians and cyclists of the existing crossing. Biodiversity: No anticipated significant biodiversity impact. Leisure: No anticipated uplift in leisure value.
Trimley St Mary	727	The Candlet Track	The Candlet Track needs to be upgraded to enable cyclists to leave North Felixstowe and reach Trimley St Martin and Kirton on a traffic free route.		1	2	3	1	-1	3	9	Connectivity and Growth: This route forms a key section of the connection between Trimley St Martin, Kirton and the west of the Deben villages and the NFGN, which in turn facilitates onward travel to Felixstowe Ferry and the north-eastern section of Felixstowe's coastline. However as an existing bridleway it scores lower in this section. Modal Shift: Full modal shift potential cannot be calculated through the PCT due to it being off-road, however it is anticipated it will be a highly valuable commuter connection post-infrastructure delivery between the NFGN and Trimley St Martin/Kirton (and beyond). A score of 2 (rather than 3) is given as employment opportunities and access to education (i.e. necessitating school runs) in Trimley St Martin and Kirton are limited. Optimisation: Uplift from earth track to cycle/pedestrian track is scored at 2 because, though most of it is already segregated from traffic (and therefore a significant uplift in quality from the baseline would not be delivered as this is already high, particularly for mountain bikes and pedestrians as they are most able to access it), the eastern most section still includes vehicular access to a small number of properties west of Gulpher Road/on Candlet Track, making segregated infrastructure here more valuable. Safety: 1 as above, small uplift in what is already a fairly safe cycling route in terms of segregation from vehicles. Biodiversity: A score of -1 is given for biodiversity due to the necessary loss/absorption of green space for providing an LTN 1/20 compliant scheme. Leisure: Likely to be a highly valued leisure cycling route for local cycling, particularly cycling with children due to the high degree of segregation from vehicles intended for this route in the future.
Trimley St Mary	760	Cycle lanes along highway	A white line separating cyclist from vehicles is not a safe option. Cars parked in cycle lanes requires cyclists to move around cars in the hope no one opens a car door as rider passes. I appreciate the solution is not an easy one but one has to be found if we are to encourage more cyclists to use network of roads. I personally have ceased cycling into Felixstowe from Trimley.		0	0	0	0	0	0	0	This proposal has been scored '0' in each of the MCAF categories because no proposal for new or improved cycling and/or walking infrastructure has been included in the response.
Tuddenham St Martin	89	westerfield lane and high street tuddenham st martin	this lane is used as a rat run throughout the day and quite often speeding motorists, HGVs petrol tankers brewery lorries. This is a single track lane and during lockdown it was very pleasant to cycle, walk down this lane as then you didnt have to dive for cover when an annoyed motorist would want you to jump out their way asap. Which is quite dangerous at times....little lane has pull ins and these are being made bigger by the heavy traffic that tries and push forward, so ruining the verges	make this lane a QUIET LANE and NO access to HGV's only for local traffic its even worse when orwell bridge is shut as its like the M25 !!!! with alot of near misses							N/A	Quiet Lanes are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Tuddenham St Martin	225	Bridleway connecting 'Green Lane; with 'Tuddenham Lane'	This bridleway is cyclable by someone with a mountain bike, however the surface is not good enough for use by 'normal cyclists' being rutted, muddy and stoney in places	Upgrade the surface to allow the bridleway to be used by young and inexperienced cyclists, it provides a route from Tuddenham to NE Ipswich avoiding the ever increasing traffic on the C road into Ipswich. It could be particularly useful for children accessing Northgate High School and Rushmere Primary Schools by bike							N/A	Maintenance of highways are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Tunstall	351	main road between Rendlesham and Tunstall	It is too dangerous for children even with adult supervision to cycle to Rendlesham school from Tunstall and Blaxhall. Road is very busy and has narrow 2 lanes with limited visibility due to the bends.	Off road cycle path would be best solution this could also be extended to Tunstall Forest where the Viking cycle trail is located allowing the public to cycle there instead of having to take their bikes on vehicles.	2	1	0	3	-3	2	5	Connectivity and Growth – The proposal would connect Tunstall and Rendlesham. Although Rendlesham will provide a number of services not available within Tunstall, it is likely that trips to supermarkets would still need to be taken to other settlements, however the proposal will allow an element of service pooling. A score of 2 is considered reasonable. Modal Shift – According to PCT, if infrastructure is delivered to the highest standard on the B1069, there would be a resultant modest modal shift. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – The B1069 has a national speed limit and, as a 'b' type road, is likely busy, therefore providing an off-road cycleway will likely have safety benefits. A score of 3 is considered reasonable. Biodiversity – The proposal will likely have a resultant loss in established hedgerows which warrants a significant negative score under this category.

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												Leisure – As the proposal will connect into the Viking MTB Trail, which is likely considered a major leisure attraction alongside the attractive PROWs it connects into, the proposal will likely have some leisure benefit. A score of 2 has been awarded.
Tunstall	352	Part of the Sandling walk, from Blaxhall to Snape on the busy Snape road (B1069)	Part of the Sandling walk that goes from Blaxhall to Snape is signposted down the busy Snape road with no footpath option. Very unsafe to walk or cycle to Snape Maltings down this stretch	There is a wide overgrown banking on one side of the road which could maybe be removed to make a footpath/cycle lane. If possible, a path from Blaxhall Common through the woods joining up with this would also be advantageous instead of walking the road into Blaxhall too.	0	0	0	3	-1	3	5	Connectivity and Growth – The proposal will have more leisure gain and is not considered to provide significant connectivity benefits. Modal Shift – No significant modal shift. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – This section of the B1069 contains sharp bends, is busy, and has a national speed limit. As the proposal will remove cyclists and pedestrians off road, a score of 3 is warranted. Biodiversity – The proposal will likely have a resultant loss of a small managed grass verge which warrants a small negative score. Leisure – The proposal will likely have significant leisure benefit as it will form part of the Sandlings walk, which extends along the coast, therefore a significant score is considered reasonable.
Tunstall	414	Access to Wickham Market Train Station in Campsea Ashe from Tunstall	Dangerous road for cyclists and walkers, pot holes are uneven surface on edge of road on Ashe Road, very sharp blind corners and road is regularly used by lorries. This means poor access for both cyclist and walkers to the train station. Public transport in this area is poor so access to the train station is vital for allowing people greener methods of transport.	The best solution would be cycle lanes and footpaths that allow direct access between Tunstall and Campsea Ashe or alternatively follow the road. Alternative solution would be improving Ashe Lane and adding protected cycle lanes.	2	0	0	3	-3	1	3	Connectivity and Growth – The proposal would connect Tunstall and Campsea Ashe, which are both small settlements with limited services. As the connection will allow an element of service pooling and as Campsea Ashe has a train station, a score of 2 is considered reasonable. Modal Shift – PCT suggests that the proposal will not result in a significant modal shift. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – This section of the B1078, which has no existing pedestrian/cycling infrastructure, has a national speed limit and is somewhat narrow. With consideration to the previous, it is likely that the proposal will have significant safety benefits. Biodiversity – This section of the B1078 is adjoined by well-established hedgerows and trees, therefore the implementation of a cycleway will likely have detrimental biodiversity impact. A score of -3 is considered reasonable. L – The proposal would connect into some attractive, but not within designated areas, PROW routes, therefore the proposal will likely have modest leisure benefit.
Tunstall	415	Cycle access between Tunstall and Woodbridge	There is poor cycle access between Tunstall and Woodbridge the next proper sized town. The main road is busy, poorly lit and fast moving and not particularly safe for cyclists or walkers for that matter. Many people in villages have to rely on cars when proper cycle access may encourage people to be greener. There is also a lack of access to local schools in neighbouring villages and the high school in Woodbridge.	Dedicated cycle paths linking Woodbridge and Tunstall would be valuable as it would allow village residents access to the facilities of the town centre while reducing traffic in Woodbridge. It would also allow those in Woodbridge dedicated cycle lanes linking them to Tunstall forest. This would give more people in Woodbridge the chance to enjoy the countryside and forest. It could also provide safe access for children to go to school by cycling rather than car or bus.	3	2	0	3	-3	2	7	Connectivity and Growth – The proposal will connect Tunstall, Rendlesham, Eyke, and Melton/Woodbridge. Although the connection from Tunstall to Woodbridge exceeds the 'everyday' cycling distance of 8km, the proposal will still be successful in connecting Woodbridge into other smaller settlements. As Woodbridge is a town centre with numerous key services, a score of 3 is considered reasonable. Modal Shift – If infrastructure is delivered to the highest standard, PCT suggests that improving infrastructure along the B1069 and A1152 will likely result in a somewhat significant modal shift. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – Both the B1069 and A1152 contains national speed limits and are busy. Removing cyclists and pedestrians off the road warrants a score of 3 under this category. Biodiversity – The proposal will likely result in significant biodiversity loss as the implementation of the infrastructure will likely require the removal of established hedgerows and other foliage. Leisure – The proposal will likely have more connectivity benefit than leisure, however the proposal will connect into multiple PROWs, which are attractive and will have some leisure benefit, and into Woodbridge/Melton, which has comparative shopping, drinking establishments, and eating establishments. A score of 2 is considered reasonable.
Tunstall	416	Snape Road, Tunstall Footpaths	There is a big gap between the footpaths on Snape Road meaning walkers have to walk on a blind bend to get to the next footpath. There is currently a footpath to the forest between Walk Farm Road and Snape Road. The next footpath on Snape road is much further down the road and you have to walk round a blind bend. This is one of the quickest access points to the forest from the village for walkers.	The current right of way could be made into a t shape rather than an l shape allowing for 2 points of access in the field. Alternatively a path could be installed on Snape road.	0	0	0	3	-3	3	3	Connectivity and Growth – The proposal has more leisure benefit than that of connectivity, hence the neutral score. Modal Shift – Insufficient evidence to suggest the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Currently pedestrians will have to walk along the B1069 in order to utilise other PROWs, however the B1069 is likely busy 'b' type road with a national speed limit. Removing pedestrians off this road warrants a score of 3 under this category. Biodiversity – The proposal will likely detrimentally impact biodiversity. In order to implement the infrastructure, the removal of established hedgerows will likely be necessary. Leisure – If pedestrian infrastructure is delivered along Snape Road, it will likely have significant leisure benefit as it will connect the PROWs along Snape Road into Snape and, therefore, Sailors Path.
Tunstall	423	Exampler - Snape Maltings but applies to towns, villages and popular visitor locations.	Provide or assist businesses in providing sufficient good quality and secure cycle parking. These need to be in high footfall areas with CCTV and good lighting to discourage theft. Cycle lockers at station and other transport hubs would be ideal. Unless cyclist feel confident that there are good cycle parking facilities that are safe they just won't visit these places.	As above.	0	1	0	0	0	2	3	Connectivity and Growth – No significant connectivity and growth impacts. Modal Shift – Cycle parking alone is unlikely to encourage large numbers of modal shift, but a certain level will be provided so a score of 1 is deemed appropriate. Optimisation – The proposal does not optimise existing infrastructure. Safety – No significant safety benefit. Biodiversity – No significant biodiversity benefit. Leisure – Snape Maltings contains café/restaurant offers and is situated near the Alde-Ore Estuary, which offers leisable walks, therefore cycle parking will likely have a strong impact awarding the proposal 2 points.
Tunstall	719	Orford to Aldeburgh via Snape	I would like to see off-road cycle paths from Orford to Aldeburgh via Snape (sections of this exist already, for instance the Sailor's Path);		2	1	0	3	-3	3	6	The commenter proposes a cycle route between Orford and Aldeburgh via Sailors Path, Snape. Cycling infrastructure along Sudbourne Road and Snape Road into the B1069 at Snape should be created, whilst also widening the Sailors Path into bridleway. Connectivity and Growth – The proposal will likely have more leisure value than

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												connectivity, however Sudbourne has limited services and the proposal will connect into three other settlements allowing an element of service pooling. Connecting into Aldeburgh, a key town, would normally warrant a score of 3 but commuting into Aldeburgh from Sudbourne and Orford exceeds the 'everyday' cycling average of 8km and the route is slightly indirect from Snape and will, therefore, have more leisure value. A score of 2 is deemed reasonable. Modal Shift – Although improving the route between Orford and Snape will not result in a significant modal shift, PCT suggests that the A1094, which Sailors Path provides an alternative to, will have a resultant modest modal shift if infrastructure is delivered to a high standard. A score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – The proposal would provide an alternative to utilising the roads with national speed limits, including the B1069 and A1094. Removing cyclists off roads warrants a score of 3. Biodiversity – The proposal will likely have a significant biodiversity impact. In order to implement high quality infrastructure, the removal of established hedgerows, trees, and other foliage will be necessary. Leisure – The proposal will have significant leisure value as it will connect into Sailors Path, which adjoins the River Alde, and connect into Aldeburgh, which is a town situated along the coast. A score of 3 is warranted.
Tunstall	734	Woodbridge, Campsea Ashe, Snape, Iken and Bawdsey	The area between Woodbridge, Campsea Ashe, Snape, Iken and Bawdsey could become a 'Cycling paradise area' for visitors and residents with the correct restrictions on the roads, ie 'quiet lanes'.								N/A	This response provides general points from their experience for our consideration and not a specific issue to be scored. Some of the areas highlighted form part of the proposed key corridors.
Ufford	159	Ufford Junction with A12 at Woodbridge	Cycling from Ufford to Bredfield and vice versa involves negotiating a big and fast road junction. The old section of road can be used but is not ideal, it is only a basic path on the side of the A12 southbound and on the other side of the A12 at the Ufford Road junction	Create a dedicated cycle/footpath path along the old section of roadway and then extend it down the A12 verge to a proper crossing point opposite the house just north of Ufford Road (meeting the footpath on the west side of the A12 at that junction)	2	0	2	3	0	0	7	Connectivity and Growth – As the roadway is an existing bridleway, the connection already exists and the proposal does not represent a new connection. However, the A12 does represent a modest barrier between those situated on either side and there does not appear to be an existing pedestrian crossing along this stretch of the A12, therefore a moderate score of 2 is considered reasonable. Modal Shift – PCT suggests that the proposal will not cause a significant modal shift. Optimisation – the old roadway is an existing bridleway and, if a dedicated segregated cycleway and footway can be developed, the proposal is considered a moderate optimisation. Therefore, a score of 2 is considered reasonable. Safety – Both the B1438 and the A12 contain NSLs and, as a 'b' and 'a' type road, volume and speed of traffic is likely high. It is considered therefore, reasonable to score the proposal 3 under this category. Biodiversity – The proposal will not have a significant biodiversity impact. Leisure – The proposal will not have a significant leisure impact.
Ufford	393	A12 at Grove Farm Ufford where cycle routes from Ufford need to continue to the WEST side of the A12 to access cycle path South to Woodbridge or villages West of A12.	cycling with my children from Ufford, west along the old A12 cyclepath in Ufford towards Bredfield or to access the cyclepath south along the A12 to Woodbridge, involves a dangerous crossing of the A12 at Grove Farm Ufford. We have to dash across a busy dual carriageway which is terrifying. There desperately needs to be a way for cyclists and pedestrians to cross the A12 at this point - or there is no safe cycle path access out of the village of Ufford towards the South or West.	A pedestrian crossing of the A12 at Grove Farm Ufford where the dual carriageway starts.	2	0	0	2	0	0	4	Connectivity and Growth – The A12 represents a modest barrier between those situated on either side and there does not appear to be a pedestrian crossing along this stretch of the A12, therefore a moderate score of 2 is considered reasonable. Modal Shift – There is insufficient evidence that a crossing point will result in significant modal shift. Optimisation – This does not improve existing infrastructure. Safety – This stretch of the A12 has a national speed limit, straight, and is considerably busy but a crossing point will not completely address the concern raised. Therefore, a score of 2 under 'safety' is considered reasonable. Biodiversity – The proposal will not have a significant biodiversity impact. Leisure – The proposal provides limited leisure benefit.
Ufford	394	The footpath in Ufford, going North towards Pettistree and Wickham Market.	The footpath from Ufford towards Wickham Market is overgrown and too narrow for pushchairs and children's bikes, with numerous potholes and stinging nettles.	Clear, widen and resurface the footpath from Ufford towards Pettistree. It is too narrow, overgrown with stinging nettles in the summer and full of potholes. It is too narrow for a pushchair, and children's bikes - their legs also get stung and scratched. The path has been resurfaced from Pettistree to Wickham market, but the Ufford stretch has not been. There is no shop or services in Ufford, so pedestrian and cycle access Wickham Market is essential.	0	1	2	0	-1	0	2	Connectivity and Growth – As the proposal is for an existing footway, it does not score under this category. Modal Shift – As the proposal is providing moderate improvements to existing pedestrian infrastructure, it is likely to see small modal shift, therefore a score of 1 is considered reasonable. Optimisation – If the pavement is widened to a good width and resurfaced, a score of 2 is considered reasonable under this scoring category. Safety – No significant safety benefit. Biodiversity – The proposal will result in the loss of managed grass areas over a reasonable length. Leisure – The pathway exists already and whilst it connects into a handful of PROWs it appears to be utilised more for commuter purposes into Wickham Market, so no score is given.
Ufford	396	Footpath along B1438	The footpath for almost the whole way from Melton up to the top of Yarmouth Road is too narrow. In places this appears to just be overgrown where the vegetation has been allowed to reclaim the footpath - especially at the upper end around Ufford Park entrance. This leaves pedestrians walking perilously close to the road.	Cut back the vegetation and hedges, widen the path properly. Then keep the path cleared regularly to avoid this in future.							N/A	Foliage that grows in private land are the responsibility of private landowners. Foliage that grows within the highway boundary is a SCC specific matter and have been shared with SCC for their consideration as the Highway Authority.
Ufford	397	Footpath between Ufford and Wickham Market alongside B1438	This footpath is very narrow and in poor condition. The path surface has fractured and it is overgrown with weeds. In places the path is non-existent or is heavily rutted. Pedestrians and particularly those with children are in danger from passing traffic and from trip and slip hazards. The path is quite well used but could see much greater footfall if improvements were made.	Widen and resurface this footpath and make sure that the missing sections are filled in. Cut back overhanging bushes to avoid pedestrians having to step into the road	0	1	2	0	-1	0	2	Connectivity and Growth – As the proposal is for an existing footway, it does not score under this category. Modal Shift – As the proposal is providing moderate improvements to existing pedestrian infrastructure, it is likely to see small modal shift, therefore a score of 1 is considered reasonable. Optimisation – If the pavement is widened to a good width and resurfaced, a score of 2 is considered reasonable under this scoring category. Safety – No significant safety benefit. Biodiversity – The proposal will result in the loss of managed grass areas over a reasonable length. Leisure – The pathway exists already and whilst it connects into a handful of PROWs it

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												appears to be utilised more for commuter purposes into Wickham Market, so no score is given.
Ufford	399	between The Avenue and Loudham lane Ufford. the hedge on the west side need cutting back. there are branches and brambles that stick out which cars coming down loudham lane push you into.ut	the hedge on the west side need cutting back. there are branches and brambles that stick out which cars coming down loudham lane push you into.	cutting hedge							N/A	Foliage that grows in private land are the responsibility of private landowners. Foliage that grows within the highway boundary is a SCC specific matter and have been shared with SCC for their consideration as the Highway Authority.
Ufford	400	Ufford	There are many footpaths in and around Ufford that are widely used by residents. While many are across fields and through woodland, walkers are obliged to use the lanes in Ufford to access them. There are very few pavements in the village, obliging walkers to compete with vehicle traffic on single track lanes.	Installing pavements is impractical in most instances due to cost and planning issues. However, there is a simple, cost effect improvement available. The vehicle speed limit within the village is 30 mph. Decreasing this to 20 mph on single lane roadways would dramatically increase safety for both walkers and cyclists, with little effect on traffic flow. Ufford lane road traffic is largely local, with little through traffic.							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Ufford	402	The whole of Byng Hall Road but particularly where it passes the houses up to the underpass of the A12	Concerns around visibility here particularly around the Public Rights of Way path that has its entrance/exit on the inside of the bend outside "Wayside", and the visibility along Byng Hall Road for both vehicles travelling in opposing directions and the pedestrians/cyclists/equestrian users. 2 speed roundels (outside Wayside & Woodcott) that have been consumed by the vegetation. The encroachment of the verge onto the carriageway on the eastern side of Byng Hall Road.	To complete the work highlighted from the site visit and then either introduce 20 mph speed limits or designate as a Quiet Lane	0	0	1	1	0	1	3	Connectivity and Growth – This improvement does not provide significant connectivity benefits. Modal Shift – These changes are unlikely to create significant modal shift. Optimisation – This would provide an improvement to an existing PROW so has scored a point here. Safety – The PROW exits onto a narrow road, which has 30mph speed limit, therefore the improvement has been awarded 1 point here. Biodiversity – No significant biodiversity benefit. Leisure – Whilst it does improve a leisure route, it is unlikely to have a significant leisure benefit, a score of 1 has been given.
Ufford	403	Spring Lane from the High Street to Lower Ufford	Single track road often used by pedestrians, cyclists and equestrians that is very tight with some blind bends. Danger of accidents with some of the aforementioned parties with vehicles. Often overgrown and often not able to drive down in a car without the vegetation coming in to contact with the vehicle	Vegetation control (cutting) and Categorise as a Quiet Lane							N/A	Foliage that grows in private land are the responsibility of private landowners. Foliage that grows within the highway boundary is a SCC specific matter and have been shared with SCC for their consideration as the Highway Authority.
Ufford	404	Lower road Ufford - the entire length.	Single track road often used by pedestrians, cyclists and equestrians that is often flooded and muddy.	Look at improving the drainage and because of the frequent use by pedestrians, cyclists and equestrians designate as a Quiet Lane.	1	0	0	2	0	1	4	The commenter proposes improving drainage along Lower Road, however any drainage improvement on a public highway and not a dedicated cycle path or footpath does not need to be scored but passed to SCC. As the commenter also proposes a quiet lane, the comment will be assessed in regard to this. Connectivity and Growth – The proposed quiet lane will help connect Melton to Ufford for cyclists and walkers. Ufford has limited to no services, which can be provided by Melton, therefore there is likely going to be 'everyday' use. However, as the proposal is slightly indirect and as the quiet lane would not connect all the way through to Melton, a score of 1 is considered reasonable. Modal Shift – The road appears to have no cycle traffic on PCT, but reasonably busy on Strava. Even if improvements are provided to the best standard, it is unlikely going to provide significant modal shift so has scored 0. Optimisation – Whilst it provides benefits, it does not optimise an existing route hence a score of 0. Safety – Although relatively quiet, this road has a national speed limit and is narrow. It could on the basis of speed and layout score a 3, however as a limited number of traffic would still use the road after a quiet lane designation, a score of 2 is considered reasonable. Biodiversity – No significant biodiversity impact. Leisure – The proposal would connect a couple of PROW routes including PROW 6 (footpath) which feeds into the walks along the river. However, records suggest it is a footpath as opposed to a bridleway meaning cycle improvements won't provide significant connection to these. A score of 1 is considered reasonable.
Ufford	406	Yarmouth Road footpath adjacent to Ufford Park Hotel.	Due to the encroachment of soil and grass and other plants over the concrete footpath, the footpath is now extremely narrow. This has resulted in pedestrians having to walk very close to the road side. The footpath is only wide enough for pedestrians to walk in single file thereby making it impossible to safely hold a young child's hand or to push a toddler's buggy. It is extremely uncomfortable and dangerous to walk this part of the footpath as being so close to the road is dangerous.	The soil/grass/plants need to be dug or scraped back so that the full width of the concrete footpath is available.							N/A	Foliage that grows in private land are the responsibility of private landowners. Foliage that grows within the highway boundary is a SCC specific matter and have been shared with SCC for their consideration as the Highway Authority.
Ufford	465	Footpath on the A12 slip road between High Street and the A12	This footpath is overgrown and the tarmac surface is cracked. the path and verges have not been cut so it means that pedestrians and cyclist have to use the busy road. The path links Ufford with a footpath across to Bredfield and to the site of the Sogenhoe Chapel.	Cut the overgrown grass verges and recondition the overgrown and worn pathway. Make the path wider to allow cyclists to use it.	0	0	0	3	-2	0	1	Connectivity and Growth – The proposal offers limited connectivity benefits. This section joins PROW 31 (bridleway), but provides limited connections to other villages or services and would not provide significant connectivity to Westleton. Modal Shift – PCT suggests that the route is not currently well used and any improvements are unlikely to cause a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – As a 'B' type road with a national speed limit, volume and speed of traffic is likely high; therefore, as the proposal will remove cyclists of this road, a score of 3 is considered reasonable. Biodiversity – The proposal will likely have a resultant loss of wild green verges, therefore a modest negative score is considered reasonable.

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												Leisure – There are limited leisure routes nor does it connect to leisure attractions so it scores a 0.
Ufford	466	Hawkeswade Bridge on road from Ufford to Eyke	This bridge is on a narrow lane with a blind corner, making visibility poor for both vehicles and pedestrians. The footpath and area nearby is used by walkers and cyclists so is often hazardous. Although there is 30 mph sign just before the bridge, there is no road narrows sign and traffic often speeds or has to back up. The road is used by traffic cutting through to the A12 as well as by lorries and tractors from nearby farms.	Improve signage at this dangerous point and also near Melton hamlet where this snoter blind corner for pedestrians. Consider adopting a 20 mph limit on this difficult section.	0	0	0	1	0	0	1	The commenter proposes the addition of 'road narrows' signage as guidance for both vehicular traffic and pedestrians/cyclists. Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – No significant modal shift benefit. Optimisation – No significant optimisation benefit. Safety – As Lower Street, a relatively narrow road, has both a national speed limit and a 30mph speed limit, a guidance sign may have partial benefit, although whether any sign makes a significant difference in reality is unknown. Biodiversity – No significant biodiversity benefit. Leisure – No significant leisure benefit.
Ufford	523	Ufford	Ufford residents are currently rather trapped in the village by busy roads and unable to safely leave the village for trips to school or the shops. I'm part Danish and long to be able to use my bike instead of the car for school, shopping, etc, as my family do in Denmark. However with small children there's no way I'd venture onto the roads to Woodbirge or Wickham Market. I hate how much I have to use the car.	I would love a cycle friendly route between Wickham Market and Woodbridge. It would enable so many children to get to school safely.	3	0	0	3	-2	1	5	The commenter proposes a cycle friendly route between Wickham Market and Woodbridge for commuting purposes. The most direct route would be along the B1438 so, for the purpose of this assessment, an off-road cycleway adjoining the B1438 will be assessed. Connectivity and Growth – Not only does the proposal connect Wickham Market, Pettistree, Ufford, and Melton, but it would also help towards a connection to Woodbridge, which is a key service centre. Melton has high levels of services which are not available in the connecting villages, therefore there will likely be 'everyday' use. With consideration to the previous, a score of 3 is considered reasonable. Modal Shift – It is unlikely that infrastructure can be delivered to the highest standard; therefore, it is unlikely that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore optimise the existing. Safety – This section of the B1438, which is a busy 'b' type road, consists of 30mph, 40mph, and national speed limits; therefore, as the proposal would remove cyclists and walkers off road, a score of 3 is considered reasonable. Biodiversity – The delivery of the proposed infrastructure will likely have a resultant loss of loss of grassed areas, which are regularly cut and of limited benefit, and small hedges/trees. Therefore, a score of -2 is considered reasonable. Leisure – The proposal will likely have more connectivity and growth benefit then leisure benefit, however the proposal would connect to numerous PROWS, which may be small leisure attractions, and would also help connect into Woodbridge which has a key town centre. As the proposal does not connect directly into the town centre and as the infrastructure is unlikely going to be delivered to the highest standard, a score of 1 is considered reasonable.
Ufford	590	Ufford Road junction with A12 single carriage way has a bridle way Xing	Dangerous to cross A12 as cars very fast to & after dual carriage way	A Toucan Crossing. Also resurface & remove foliage from cycle way	2	0	0	2	0	0	4	Connectivity and Growth – the A12 represents a modest barrier between those situated on either side and there does not appear to be a pedestrian crossing along this stretch of the A12, therefore a small score of 1 is considered reasonable.Modal Shift – there is insufficient evidence that a crossing point will result in signification modal shift.Optimisation – this does not improve existing infrastructure. Safety – This stretch of the A12 has a NSL, straight, and is considerably busy but a crossing point will not remove pedestrians/cyclists off the road. Therefore, a score of 2 under safety is considered reasonable.Biodiversity – The proposal will not have a significant biodiversity impact.Leisure – The proposal provides limited leisure benefit.
Waldringfield	186	Footpaths in and around Waldringfield, and elsewhere throughout East Suffolk	With the rising popularity of cycling we seem to have lost respect for the differences between footpaths and bridleways. Cyclists seem to no longer acknowledge that footpaths are not for cycling along, making it potentially dangerous for walkers and causing damage to footpaths.In the same way that cyclists wish to see improvements to the road infrastructure to feel safe from vehicles we need to acknowledge that there are similar issues on footpaths...which are NOT rights of way for cyclists.	A campaign of education about the differences between footpaths and bridleways coupled with improved signage and potentially sanctions for non compliance							N/A	The comments raised have been considered in the formation of the strategy, however the education of cyclists is beyond the scope of the strategy and cannot be scored under the MCAF system.
Waldringfield	409	Waldringfield	No WC accessible to the public walking or cycling in the area. It would get more people out walking and/or cycling if they could feel sure that they would be able to find WCs en route. Waldringfield is a classic example of a place in a prime location for walkers, but no toilets. This applies to most villages these days so Waldringfield is just one example.	Public WCs should be brought back in villages. Funding could perhaps be eased by charging, and since there is little call to carry coins these days, perhaps this could be arranged via a mobile phone app similar to car-parking. Pubs and cafes (in Waldringfield the Maybush is perfectly located) should be encouraged, or even compelled, to allow passers-by to use their toilets for a small charge (which they might even refund if the user then decides to buy something) - rather than walkers "go" in the bushes.	0	0	0	0	0	2	2	Connectivity and Growth – No significant connectivity benefit. Modal Shift – The provision of these services may create some additional leisure cyclists, but unlikely to result in significant modal shift. Optimisation – Whilst this may represent a popular place to visit and Strava provides support for this, the WC would be sufficiently separate from cycling and walking infrastructure to say it is optimising the existing infrastructure so should be a neutral score. Safety – No significant safety benefit. Biodiversity – No significant Biodiversity benefit. Leisure – As a popular destination the WC would help provide leisure benefits to visitors and would score a 2.
Waldringfield	601	GR 265 450	When Brightwell Lakes are developed, ATs will want to enter the AONB to reach the R. Deben & Maybush Inn. The permissive footpath from GR 264452 to the Quiet Lane at 273454 is not a PROW	If Waldringfield Heath Golf Course new owners do NOT provide a footpath in due course, then ESC & WPC might negotiate with Howes Farm owner of that permissive path, for it to become a PROW.	2	1	0	3	0	3	9	Connectivity and Growth – The proposal will likely have more leisure value than that of connectivity, however the route will connect help in connecting Waldringfield to Martlesham Heath Aadastral Park. Waldringfield does have a school but will likely use Martlesham for food shops. A score of 2 is considered reasonable. Modal Shift – Creating a direct new connection into a service centre from a somewhat isolated village will likely create a modal shift. A score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The proposal would provide an alternative to Ipswich Road, which has a NSL and no existing pedestrian infrastructure, therefore a score of 3 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – The

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												proposal will likely have high leisure value as it creates an east to west route helping connect the PROW network along the River Deben to the PROW network that extends through Martlesham Heath. The attractiveness of the route, which extends through the designated AONB, means it is considered a full score.
Waldringfield	646	Footpaths in and around Waldringfield, and elsewhere throughout East Suffolk (Ref186)	Waldringfield Parish Council agrees with this. WPC has put up No Cycling signs on several footpath following complaints by residents, and most of these have been destroyed, presumably by cyclists.		0	0	1	0	0	0	1	Connectivity and Growth: No benefit. Modal Shift: No benefit. Optimisation: Increased or improved signage creates certainty about Rights of Way for both pedestrians and cyclists, and legibility if opportunity is taken to team it with helpful wayfinding/route identification information, as is often the case. Safety: No safety benefit. Biodiversity: There may under some circumstances be a biodiversity benefit to a footpath not being ridden by cyclists, due to increased pressure on a fragile structure that has habitat value (e.g. river walls) though this would be easy to overstate in most instances (overall activity levels versus carrying capacity more indicative than user types). Score of zero is given. Leisure: No leisure benefit.
Waldringfield	647	River Wall north of Waldringfield (Footpath 11)	There is a serious problem on the river wall footpath north of Waldringfield (FP11), where cycling damages the structure of the river wall and could eventually result in a breach. No cycling signs are regularly ignored by cyclists.	Barriers would be effective but are problematic because they make access for mobility vehicles difficult. Better signage might help.	0	0	0	0	0	0	0	Connectivity and Growth - No significant benefit. Modal Shift - Insufficient evidence to suggest that enforcement signs will result in a significant modal shift. Optimisation - No significant optimisation benefit. Safety - Whilst the improvement may reduce cyclist and pedestrian conflict, the improvement to safety is limited. Biodiversity - No significant biodiversity impact. Leisure - If cyclists are misusing the path this may affect enjoyment for walkers, however any existing rules should be adhered to anyway and signs on their own are unlikely to represent a significant leisure benefit.
Waldringfield	648	Waldringfield (Ref 409)	Waldringfield Parish council agrees with this, except that we do not support compelling pubs such as the Maybush to provide toilets – encouragement is far better. The absence of public toilets leaves walkers with little choice if they are ‘caught out’, resulting in health hazards as well as being offensive and off-putting.	A public toilet in the Maybush car park. There should also be far more litter bins at the start and end of public footpaths.	0	0	0	0	0	2	2	Connectivity and Growth – No significant connectivity benefit. Modal Shift – The provision of these services may create some additional leisure cyclists, but unlikely to result in significant modal shift. Optimisation – Whilst this may represent a popular place to visit, and Strava provides support for this, the WC would be sufficiently separate from Walking and Cycling infrastructure to say it is optimising the existing infrastructure so should be a neutral score. Safety – No significant safety benefit. Biodiversity – No significant biodiversity benefit. Leisure – As a popular destination the WC would help provide leisure benefits to visitors and would score a 2.
Waldringfield	649	Bridleway from Waldringfield to the Waldringfield Heath crossroads (FPs 24 & 29)	When the Brightwell Lakes development is completed there will be far more people using this route to/from Waldringfield. When the new school is operational, pupils are likely to cycle from Waldringfield to it every day. The bridleway seems to end at the crossroads, where there is no choice but to use the road. In fact FP35 is a bridleway, but isn’t signposted as such, and is very narrow, and where it crosses the Ipswich Rd (turning into FPs 27, 8 & 34) is dangerous and also poorly signposted.	Widen FP35, improve the signposting, and provide proper road crossing facilities for cyclists so that the route FPs 24-29-35-27-34/8 can be cycled with minimal interaction with road traffic. (There is currently no signpost at the crossroads end of FP29)	0	0	1	3	-2	1	3	Connectivity and Growth – The comment is relating to optimising existing infrastructure; therefore, the proposal does not warrant a score under connectivity and growth. Modal Shift – Insufficient evidence to suggest that the proposed infrastructure will result in a significant modal shift. Optimisation – Widening existing infrastructure warrants a score of 1 under optimisation. Safety – Ipswich Road has a NSL. If the crossing points are delivered to the highest standard, a score of 3 is deemed acceptable. Biodiversity – Widening PROW35 could potentially result in the removal of immature hedgerows, hence the negative score. Leisure – The PROW routes, which extend through Martlesham Heath, are largely used for leisure purposes and Strava suggests they have reasonable use. The addition of crossing points and optimising the bridleway will provide modest leisure benefits. A score of 1 is considered reasonable.
Waldringfield	692	footpath from Martlesham to Waldringfield along River Deben	For context we have included the comments taken from the map, WPC’s responses are labelled as ‘our response’. For many years it has simply been accepted that part of the path was washed away by natural erosion, so the only way to walk to Waldringfield from Martlesham is along the road. This is shown by signposts at the access points to this section of footpath.	Re-instating this footpath (by mending the breach at TM279461 or providing a diversion following the high-water mark) would provide a continuous off-road footpath route along the entire west bank of the Deben estuary, with several suitable entry/exit points. We agree that the footpath should be re-instated but disagree that this should be done by mending the breach. We support the new inland footpath proposed by Natural England, and see attached.	1	0	0	3	-1	3	6	Connectivity and Growth – The improvements will likely have more leisure benefit than connectivity, however the proposal would create a connection between Martlesham and Waldringfield. Martlesham provides services that Waldringfield does not have but there is unlikely going to be ‘everyday’ use as the connection is not direct. Modal Shift – Despite a new connection to Martlesham, it is indirect and will likely have more leisure value. It is not considered, therefore, that the proposal will result in a significant modal shift. Optimisation – The proposal is not considered an optimisation. Safety – The proposal will provide an alternative route to the use of Waldringfield Road which is narrow with a national speed limit. The proposal will have safety benefit, therefore a score of 3 is considered reasonable. Biodiversity – A modest minus point is deemed reasonable due to creating a footpath more inland will likely result in the loss of some foliage. Leisure – Re-instating the footpath will have significant leisure benefit as these paths represent high leisure links alongside the River Deben. This improvement warrants the highest score under this category.
Waldringfield	694	Footpaths in and around Waldringfield, and elsewhere throughout East Suffolk	With the rising popularity of cycling we seem to have lost respect for the differences between footpaths and bridleways. Cyclists seem to no longer acknowledge that footpaths are not for cycling along, making it potentially dangerous for walkers and causing damage to footpaths. In the same way that cyclists wish to see improvements to the road infrastructure to feel safe from vehicles we need to acknowledge that there are similar issues on footpaths...which are NOT rights of way for cyclists.	For context we have included the comments taken from the map, WPC’s responses are labelled as ‘our response’. A campaign of education about the differences between footpaths and bridleways coupled with improved signage and potentially sanctions for non compliance Our response: We agree with this. WPC has put up No Cycling signs on several footpath following complaints by residents, and most of these have been destroyed, presumably by cyclists. We also have a more serious problem on the river wall footpath north of Waldringfield (FP11), where cycling							N/A	Issues relating to the enforcement of PROW routes are a SCC specific matter have been shared with SCC for their consideration as the Highways Authority.

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				damages the structure of the river wall and could eventually result in a breach.								
Waldringfield	695	Waldringfield	For context we have included the comments taken from the map, WPC's responses are labelled as 'our response'. No WC accessible to the public walking or cycling in the area. It would get more people out walking and/or cycling if they could feel sure that they would be able to find WCs en route. Waldringfield is a classic example of a place in a prime location for walkers, but no toilets. This applies to most villages these days so Waldringfield is just one example.	For context we have included the comments taken from the map, WPC's responses are labelled as 'our response'. Our response: We agree with this, except that we do not support compelling pubs such as the Maybush to provide toilets – encouragement is far better. The absence of public toilets leaves walkers with little choice if they are 'caught out', resulting in health hazards as well as being offensive and off-putting. There should also be far more litter bins at the start and end of public footpaths.	0	0	0	0	0	2	2	Connectivity and Growth – No significant connectivity benefit. Modal Shift – The provision of these services may create some additional leisure cyclists, but unlikely to result in significant modal shift. Optimisation – Whilst this may represent a popular place to visit, and Strava provides support for this, the WC would be sufficiently separate from Walking and Cycling infrastructure to say it is optimising the existing infrastructure so should be a neutral score. Safety – No significant safety benefit. Biodiversity – No significant biodiversity benefit. Leisure – As a popular destination the WC would help provide leisure benefits to visitors and would score a 2.
Waldringfield	698	Bridleway from Waldringfield to the Waldringfield Heath crossroads (FPs 24 & 29)	When the Brightwell Lakes development is completed there will be more people using this route to/from Waldringfield. When the new school is operational, pupils are likely to cycle from Waldringfield to the new school. The bridleway seems to end at the crossroads, where there is no choice but to use the road. In fact FP35 is a bridleway, but isn't signposted as such, and is very narrow, and where it crosses the Ipswich Rd (turning into FPs 27, 8 & 34) is dangerous and also poorly signposted.	Widen FP35, improve the signposting, and provide proper road crossing facilities for cyclists so that the route FPs 24-29-35-27-34/8 can be cycled with minimal interaction with road traffic. (There is currently no signpost at the crossroads end of FP29)	0	0	1	3	-2	1	3	Connectivity and Growth – The comment is relating to optimising existing infrastructure; therefore, the proposal does not warrant a score under connectivity and growth. Modal Shift – Insufficient evidence to suggest that the proposed infrastructure will result in a significant modal shift. Optimisation – Widening existing infrastructure warrants a score of 1 under optimisation. Safety – Ipswich Road has a NSL a. If the crossing points are delivered to the highest standard, a score of 3 is deemed acceptable. Biodiversity – Widening PROW35 could potentially result in the removal of immature hedgerows, hence the negative score. Leisure – The PROW routes, which extend through Martlesham Heath, are largely used for leisure purposes and Strava suggests they have reasonable use. The addition of crossing points and optimising the bridleway will provide modest leisure benefits. A score of 1 is considered reasonable.
Walpole	310	Heart of Suffolk - Cycle loop passing through Halesworth, Framlingham, Debenham, Eye, Hoxne and Bungay requires improved signage and route granting	This beautiful previously published loop ("The Heart of Suffolk") passes through unspoilt countryside on minor roads and passing churches and other historic points of interest, linking several old market towns. The brown waymarked signs has fallen into real disrepute over the last 5 years or so, and should be granted a formal county route number plus get better signage. The loop can boost local tourism and cafe/craft visits along its whole length.	Review the whole loop and grant a formal route 'number' for the county. Replace existing deteriorated and erroneous direction signs, and republish the loop on an appropriate map and/or website to include GPS files which can be downloaded by other cyclists. Promote links to nearest rail and bus services enroute, to ease the way for shorter distance or less able cyclists. The originator of this request has cycle navigation files which could be used as a basis for publicising online via relevant cycling internet sites.	0	0	1	0	0	1	2	Connectivity and Growth - Not a key connection.Modal Shift - No effect.Optimisation - Small impact on existing infrastructure.Safety - No effect.Biodiversity - No impact.Leisure - It is a leisure route, therefore repairing signs will have small benefit.
Walpole Robbie	24	Forge Cottage, Walpole, IP19 9AZ	Walking from one village to another is extremely dangerous especially where there are bends and hills with high banks and no escape for pedestrians. Some drivers exceed the 30 mph speed limit and others drive into the winter sun unable to see the road at all, Other rural roads that are NSL are narrow and should be 20 or 30 mph. Walking and cycling should be encouraged. We have no bus service to our nearest shops which are over 2 miles away, as are schools, pubs and active churches.	Walking and cycling, especially between towns and villages should be made safer. Narrow roads should be 20 or 30 mph. Attention should be given to improving the visibility of cyclists and pedestrians especially on hills and bends and where there are high banks. New footpaths at such points through adjacent fields would reduce the risks. Banks could be cut back at key points.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Wantisden	247	Future Rendlesham / Bentwaters Development	Lack of Public Right of Way's connecting 'Rendlesham' to 'Rendlesham Forest', Wantisden, Butley and the coast.	1. Consider running a new cycle/footpath across Bentwaters Airfield to connect Rendlesham Housing estates with Wantisden Corner road. Provides an off road walking route and removes the need for cyclists to use the local 'B roads'. 2. Consider upgrading the 'path' that runs across the eastern end of the runway towards Friday Street.	2	1	0	3	0	2	8	Connectivity and Growth – The proposal would connect Wantisden into Rendlesham through the employment allocation. Although these connections would allow an element of service pooling, many trips would likely still need to be taken to other settlements, therefore a score of 2 is considered acceptable. Modal Shift – PCT suggests that if off-road infrastructure were to be delivered as an alternative to the 'B' type roads surrounding the Bentwaters allocation, there would be a resultant small modal shift. A score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not, therefore optimise the existing. Safety – Currently all routes into Rendlesham from Wantisden has a NSL. Removing cyclists and pedestrians off road warrants the highest score under this category. Biodiversity – The proposal will unlikely result in a significant biodiversity impact. Leisure – The proposal would connect into a handful of PROWs including a particularly attractive bridleway which extends through Rendlesham Forest, which is situated within the AONB. A score of 2 is considered reasonable.
Wantisden	720	Between Orford and Woodbridge	I would like to see off-road cycle paths from Orford to Woodbridge (and Sutton Hoo).	This would link many local facilities and heritage attractions and also join up with local train stations for those wanting to come to the area with their bicycles by rail.	2	1	0	3	-3	3	6	The commenter proposes an off-road cycleway from Orford to Woodbridge. The most direct route would be along the B1084 and the A1152. Connectivity and Growth – The proposal creates a connection between Orford, Chillesford, Butley, and Melton. It is unlikely that there would be 'everyday' cycling to Melton, however, as the route exceeds to average of 8km. These connections will allow an element of service pooling which warrants a score of 2. Modal Shift – PCT suggests that if high standard infrastructure is delivered on the B1084, there would be a modest modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – The B1084 is a busy road, used by HGVs, and has a national speed limit. Removing cyclists off road warrants a score of 3 under this category. Biodiversity – Implementing such infrastructure would likely result in a significant biodiversity loss. The B1084 appears to have high biodiversity adjoining the roads in some places.

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												Leisure – The proposal will likely have significant leisure benefit as the proposal connects into Orford which, with the Orford Ness National Nature Reserve, is likely a leisure destination. Moreover, the proposal connects into numerous attractive PROW routes which connect into the B1084.
Wenhaston	806	Bramfield Road (A144)	Link residential areas to the main town destinations and the NCR1.	Create a route down Bramfield Road (A144), to the Mells/Walpole Grange Road crossroads, making use of Durban Close if required. This would connect directly to the NCR1 route going south towards Walpole and into the Blyth Road industrial estate and on into the Millennium Green.	0	0	3	3	-1	3	8	Connectivity and Growth - Existing connection in place Modal Shift - No effect Optimisation - Re-surfacing and widening of existing pathway to create an off-road shared pathway. Safety - Partially national speed limit along A144 Biodiversity - Loss of cut grass verge however potential to have more impact if existing hedge is affected. Leisure - Provides a route into Halesworth
Westerfield	138	Lower Road, Westerfield	Lower Road and Church Lane are used as a rat run by large numbers of motorists seeking a short cut to main routes West of Ipswich. This is made worse when there are closures of the Orwell Bridge. There is no footpath along much of this route, forcing pedestrians to mix with often speeding traffic. As a resident of the village, I know that a number of other residents are afraid to walk there, particularly the more elderly. This results in both unnecessary car journeys and social isolation.	My suggestion would be to make both Lower Road and Church Lane one-way for motor traffic, as there are viable alternative routes into and out of the village. Proper footways could then be installed and a contraflow cycle lane, preferably with grade separation, or, at minimum, flexible wands or similar.	2	1	0	1	0	0	4	Connectivity and Growth - The improvement could result in cycle lanes or footpaths being created if one of the suggested roads are made 1 way. This will then allow large sections of the village to be connected to the village centre with its associated services. Modal Shift - PCT suggests that an improvement to a low standard would not create significant modal shift growth for cycling. However there may be greater benefit for pedestrians if a footpath could be added. Optimisation - This would not represent an optimisation. Safety - Both Lower Road and Church Lane are 30mph, although can be busy during peak times. Creating a one-way road wouldn't remove cyclists away from traffic, but some modest safety benefit can be achieved. Biodiversity - There are no significant biodiversity benefit. Leisure - This improvement is to avoid significant traffic through Westerfield, but the impact for leisure purposes is not deemed significant.
Westerfield	218	Westerfield Business Centre / Station	Possible site for an Ipswich northern 'Park & Cycle' car park. There is nowhere to park when using Westerfield Station.	Given the emerging development north of Ipswich this would make a good spot for a park,ride and cycle carpark similar to those seen around the fringes of Cambridge. This would enable those of us travelling into Ipswich from the North (aka East Suffolk District) to park up and then either use the train to go northward towards Lowestoft or cycle(or walk) or bus the short distance into the middle of Ipswich.	0	1	0	0	0	1	2	Connectivity and Growth - No significant connectivity and growth benefit. Modal Shift - Providing space to park at the train station allowing for commuting and every-day travel elsewhere will provide modest modal shift growth. Optimisation - This does not optimise the existing cycle infrastructure. Safety - This does not significantly relate to safety. Biodiversity - There are no significant biodiversity impacts. Leisure - Whilst there are some leisure benefits due to connects elsewhere the overall leisure impact is considered modest.
Westerfield	334	Westerfield Business Park/Westerfield Station	With reference to the comment of having a cycle park for using the railway , the last time I wanted to use it to take my cycle to Woodbridge I found that the majority of Lowestoft trains do not stop at Westerfield.Could there be liaison with the railway companies to make Westerfield Station at least a request Halt for cyclists to use all trains.	Request to make Westerfield Station at least a request Halt Station for all users.							N/A	The train stops are outside the remit of the project.
Westerfield	337	Westerfield Railway Station	Liaise with rail operating company to have all trains stop at least on a request Halt basis for use by cyclists. As far as I am aware very few Lowestoft trains stop at Westerfield whereas they used to.	Provide parking facility for cyclists and request all passenger trains at least be available to pedestrians or cyclists.	0	1	0	0	0	1	2	The train stops are outside the remit of the project, but cycle parking has been assessed. Connectivity and Growth - Cycle parking does not represent additional connectivity. Modal Shift - Providing space to park at the train station allowing for commuting and every-day travel elsewhere will provide modest modal shift growth. Optimisation - This does not optimise the existing cycle infrastructure. Safety - This is not significantly relate to safety. Biodiversity - There are no significant biodiversity impacts. Leisure - Whilst there are some leisure benefits due to connects elsewhere the overall leisure impact is considered modest.
Westerfield	478	Moss Lane Westerfield	This road is single track and used by a large range of vehicles as a short cut. It is unsuitable as a rat run and should be closed to through traffic thus protecting cyclist and pedestrians.		0	0	0	2	0	0	2	Connectivity and Growth – The proposed quiet lane will help connect Tuddenham and Westerfield for cyclists and walkers. These are 2 rural settlements, neither with significant services it would normally result in a connectivity and growth score, however the quiet lane would not connect all the way through to Westerfield itself limiting the benefit. Modal Shift – According to PCT, even if infrastructure was delivered to the highest standard, it will unlikely result in a significant modal shift. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – The road is to national speed limit, it is narrow, although relatively quiet. It could on the basis of speed and layout score 3, however as a limited number of traffic would still use the road even after a quiet lane designation the score has been given a 2. Biodiversity – No biodiversity impact. Leisure – The road itself would be improved for leisure users and it is unlikely to become a commuter route, however any leisure improvement is not significant, and it doesn't feed into wider PROW routes. (FP 6 and 8 cross the 2 villages currently albeit across the north).
Westerfield	764	Main Road B1077	The Main Road B1077 connects Ipswich with Debenham and villages to the North of the County and for most of its length in there is frontage development and a 30mph Speed limit. A suitable width footway exists between the Railway Level Crossing and The Swan PH but northwards this footway is of inadequate width.		0	0	2	1	-1	0	2	Connectivity and Growth - A pavement does exist, although it is recognised that the width can be prohibitive. It is not considered that significant connectivity and growth benefit is created. Modal Shift - The modal shift benefit is likely to be limited due to the low number of properties to benefit. Optimisation - Creating a full standard path from a sub standard path provides good optimisation and allows for greater use by a range of users. Safety - The width of the path may mean that some users of the path are forced onto the road meaning a modest safety score is deemed reasonable. To the south of this improvement there appears limited capacity to widen the path to the north nearer the field edge there is a greater potential. The loss of a largely managed field edge could have a small biodiversity impact. Leisure - Whilst it is recognised it would better connect the public house the overall leisure impact is deemed limited.

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Westerfield	765	Church Lane and Lower Road	An East/West route, Church lane (unclassified) and Lower Road (C Class), is used by many vehicles as an alternative to busy roads across the North of Ipswich. This route in many places is only 5 metres wide and has no footpaths and no walkable verges while the peak hour flow of traffic has been measured at over 500 vehicles per hour.		2	1	0	1	0	0	4	Connectivity and Growth - The improvement could result in cycle lanes or footpaths being created if traffic filters and safety measures are applied to Church Lane and Lower Road. This will then allow large section of the village to connected to the village centre with its associated services. Modal Shift - PCT suggests that an improvement to a low standard would not create significant modal shift growth for cycling. However there may be greater benefit for pedestrians if a footpath could be added. Optimisation - This would not represent an optimisation. Safety - Church Lane/Lower Road is 30mph, although can be busy during peak times. However some safety benefit can be achieved. Biodiversity - There are no significant biodiversity benefit. Leisure - The impact for leisure purposes is not deemed significant.
Westerfield	766	Westerfield footpaths	The Parish Council have sought to apply for definitive status for a number of footpaths that were known to be used by residents but in all cases access to these routes for a circular walk includes use walking along dangerous local roads.								N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. Defining PROW routes is a matter for SCC.
Westerfield	767	Westerfield	The only recognition of cycling in the village is that a section of the East/West route from Lower Road and Church Lane and then Moss Lane to Tuddenham is part of a Long-Distance Cycle Route.								N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
Westerfield	768	Section of track leaving the B1077 going west between Mill Farm and High Acre	Section of track leaving the B1077 going west between Mill Farm and High Acre, not on the definitive map but currently used as a footpath to be adopted as a public right of way to link with Footpath 18 (Fonnereau Way) as part of the Ipswich Garden Suburb and hence enable access to the proposed footbridge over the Railway line and the footpath towards Ipswich.		1	0	0	0	0	0	1	Connectivity and Growth – The proposal would create a more direct connection from Westerfield Road into the PROW network, which extends into Ipswich, however the PROW network can already be accessed on Lower Road. A score of 1 is considered reasonable. Modal Shift – It is unlikely that the proposal will have a resultant significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – No significant safety benefit. Biodiversity – No significant biodiversity impact. Leisure – No significant leisure value.
Westerfield	769	Section of track leaving the B1077 going east and then passing under the two railway bridges	Section of track leaving the B1077 going east and then passing under the two railway bridges to be adopted as a public right of way to join with the network of routes passing Red House Farm within the Ipswich Garden suburb and giving access to Tuddenham Road. This would enable residents of Westerfield to gain access to Northgate High School and Northgate Sports Centre without having to use heavily trafficked roads.		1	1	0	1	0	1	4	Connectivity and Growth - For pedestrians connections do already exist along Westerfield Road and footpaths. For cyclists these connections are poor, but the proposal will not be accessible to all cyclists with significant improvement. A score of 1 is deemed reasonable. Modal Shift - Pedestrians are already reasonably well connected from Westerfield to north Ipswich. Using the alternative to the suggested improvement is Westerfield Road which PCT shows has a modest potential for cycling modal shift, but the adoption of the pathway may not achieve this growth, but a score of 1 is deemed reasonable. Optimisation - This would represent a new route as opposed to an optimisation. Safety - There is the potential to take a small amount of cyclists of Westerfield Road, however the numbers are unlikely to be significantly high. Biodiversity - There is unlikely to be significant biodiversity impact. Leisure - Creating an attractive rural route is considered to have some leisure benefit.
Westerfield	770	Lower Road, Westerfield	This road is unsuitable for cyclists and pedestrians due to the amount and the speed of traffic. This narrow road does not have footways or walkable verges and where the minimum width is 5 metres a drainage ditch is immediately adjacent only protected by reflective marker posts.	Although Speed indicators are present it is obvious that physical measures are needed to improve reduce traffic speeds and enable cyclists and pedestrians to use this road in safety. Consideration should be given to traffic management measures such as restricting vehicles to single lane working alongside pedestrian/cycling facilities and/or any other provision to decrease the number and speed of vehicles.	2	1	0	1	0	0	4	Connectivity and Growth - The improvement could result in cycle lanes or footpaths being created if traffic filters and safety measures are applied to Church Lane. This will then allow large section of the village to connected to the village centre with its associated services. Modal Shift - PCT suggests that an improvement to a low standard would not create significant modal shift growth for cycling. However there may be greater benefit for pedestrians if a footpath could be added. Optimisation - This would not represent an optimisation. Safety - Lower Road is 30mph, although can be busy during peak times. However some safety benefit can be achieved. Biodiversity - There are no significant biodiversity benefit. Leisure - The impact for leisure purposes is not deemed significant.
Westerfield	771	Church Lane, Westerfield	This road is unsuitable for cyclists and pedestrians due to the amount and the speed of traffic. This narrow road does not have footways or walkable verges and limited visibility is an additional hazard.	Although Speed indicators are present it is obvious that physical measures are needed to improve reduce traffic speeds and enable cyclists and pedestrians to use this road in safety. Consideration should be given to traffic management measures such as restricting vehicles to single lane working alongside pedestrian/cycling facilities and/or any other provision to decrease the number and speed of vehicles.	2	1	0	1	0	0	4	Connectivity and Growth - The improvement could result in cycle lanes or footpaths being created if traffic filters and safety measures are applied to Church Lane. This will then allow large section of the village to connected to the village centre with its associated services. Modal Shift - PCT suggests that an improvement to a low standard would not create significant modal shift growth for cycling. However there may be greater benefit for pedestrians if a footpath could be added. Optimisation - This would not represent an optimisation. Safety - Church Lane is 30mph, although can be busy during peak times. However some safety benefit can be achieved. Biodiversity - There are no significant biodiversity benefit. Leisure - The impact for leisure purposes is not deemed significant.
Westerfield	772	Moss Lane	This road is single vehicle width and used by a large range of vehicles as a short cut. It is unsuitable as a rat run and should be closed to through traffic thus protecting cyclist and pedestrians. The SCC ROW Improvement Plan referred to possible classification as a Green Lane (Similar Comment to that already registered No478)		0	0	0	2	0	0	2	Connectivity and Growth – The proposed quiet lane will help connect Tuddenham and Westerfield for cyclists and walkers. These are 2 rural settlements, neither with significant services it would normally result in a connectivity and growth score, however the quiet lane would not connect all the way through to Westerfield itself limiting the benefit. Modal Shift – According to PCT, even if infrastructure was delivered to the highest standard, it will unlikely result in a significant modal shift. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – The road is to national speed limit, it is narrow, although relatively quiet. It could on the basis of speed and layout score 3, however as a limited number of traffic would still use the road even after a quiet lane designation the score has been given a

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												2. Biodiversity – No biodiversity impact. Leisure – The road itself would be improved for leisure users and it is unlikely to become a commuter route, however any leisure improvement is not significant, and it doesn't feed into wider PROW routes. (FP 6 and 8 cross the 2 villages currently albeit across the north).
Westerfield	774	Westerfield Railway Station and Greater Anglia	In order to make better use of rail services and reduce dependence of local residents on car travel there's a need for East Suffolk Line services to stop at Westerfield. In the past it has been possible to use this service to or from Woodbridge as part of a cycle ride or a ramble, in fact it's listed as an East Suffolk Line walk. Stopping trains on the East Suffolk line would therefore help to encourage walking and cycling while also eliminating car journeys and contributing to "Green" policies.								N/A	The train stops are outside the remit of the project.
Westerfield	775	Sandy Lane and Route of Bridleway (Westerfield ROW No 1) from Lower Road, Westerfield to Henley	It is suggested that this route could be upgraded to be suitable for all classes of cyclist. This would enable social/recreational links between the two villages to be enjoyed while not having to mix with fast moving traffic on roads with no footpaths or verges.		0	1	1	0	0	1	3	Connectivity and Growth - The re-surfacing of Sandy Lane will provide a more accessible route to different types of cyclists, but won't create a significant connectivity and growth benefit. Modal Shift - The alternative route using Henley Road shows that high quality improvements would have a modest modal shift growth. The suggested improvement would not be expected to achieve the same level of growth as many cyclists would already be conformable with the surface and some cyclists would continue to use Henley Road. However a score of 1 is deemed reasonable. Optimisation - The PROW is already of a reasonable standard as an off-road bridleway. However it is recognised that that re-surfacing would optimise the route further by allowing greater accessibility so a score has been provided. Safety - The suggestion will not improve the interactions between cyclists and vehicles to a significant degree. Biodiversity - There are not significant biodiversity impact. Leisure - Creating greater accessibility to a relatively attractive rural route creates a score here.
Westhall	50	The issue concerns the full length of a bridleway which passes through the parishes of Holton (BR2), Sotherton(BR4), and Westhall (BR16). The point pinned on the map is the (new) section that would need the most work to make it suitable for cycles.	This long public bridleway (aka 'Scalesbrook Lane') leading from Holton to Westhall could be improved to make it more suitable for cyclists – remembering public bridleways carry cycle rights as well as equestrian rights over them. If Network Rail (as it appears they will, eventually) ever close the Millpost Crossing further to the west (which many cyclists use), then this would be the only direct route from Halesworth/Holton to Westhall, and beyond, that avoids use of the A144 'Bungay Straight'.	The central section of the route was diverted, following WW2, along the perimeter of the former airfield, and so is fairly even. As is the first section adjacent to the turkey factory. However, there is a short section at its north end, through a copse, that has recently been (re)added to the Definitive Map; which because of its being newly clear as a through-route would not be suitable for cyclists, even though it is passable by those on foot and probably by those on horseback as well. Therefore, if this section could be made up in some way that would make it more usable by cyclists, then I'm sure it would be used more readily by them. Especially, (and more especially with any future closure of the Millpost Crossing), as this could end up being the ONLY safe route for cyclists to use between Halesworth/Holton and Westhall making the latter parish feel even more isolated than it already is. It then being the only option that avoids two busy and dangerous roads, the A144 and the B1244.	0	1	3	0	-2	3	5	Connectivity and Growth - Existing connection in place so a significant uplift is not achieved. Modal Shift - PCT suggests an uplift of 52 that could potentially use this route Optimisation - Resurfacing and widening of existing bridleway to accommodate cyclists Safety - Track already off road B - Potential removal of wild grassland when widening or resurfacing route L - route links to Halesworth and through attractive woodland.
Westleton	27	On the Reckford Road between Westleton and Middleton	It would be extremely useful and much safer for pedestrians if there was a footpath from the Southern end of Black Slough to the junction of Reckford Road and Back Road (Middleton). This is a popular walk and would link up with several other footpaths in the area.		0	0	0	3	-3	1	1	Connectivity and Growth – This section joins the bridleway to Middleton, but provides limited connections to other villages or services and would not provide significant connectivity to Westleton. Modal Shift – As a leisure route without significant connectivity it is not considered that there will be significant modal shift. Optimisation – The proposed improvements are new and do not optimise the existing. Safety – The road is relatively narrow with a NSL, walkers have to use the narrow cut grass verge if they want to go to Middleton or enter other PROW. Given the road and speed limit and a pavement would get them off the road it does score highly for safety. Biodiversity – The proposal will result in potential significant loss of wild growth and hedges which have a high biodiversity value meaning a significant minus score is likely. Leisure – This proposal will connect a pair of country walks to the village of Middleton meaning it has a modest leisure benefit.
Westleton	97	Westleton. Between Reckford Bridge (TM436677) and the start of Black Slough (TM438679)	Walkers wishing to link between Footpath Westleton 25 (Reckford Bridge) and Bridleway Westleton 26 (Black Slough) have to walk along a dangerous stretch of the B1125 where there is no space for pedestrians around a tight bend.	A public footpath of 0.12 mile between Reckford Bridge (TM436677) and the start of Black Slough (TM438679) must be created inside the hedges of the farm land to provide a safe alternative to walking along the busy carriageway of the B1125 between Public Footpath Westleton 25 and Bridleway Westleton 26 and enable valuable circular walks around Middleton, Eastbridge, Minsmere and Westleton to be walked safely. The danger here will be worsened even more if the B1125 is to carry construction traffic for Sizewell C.	0	0	0	3	-3	2	2	Connectivity and Growth - This is not considered to create significant connectivity. Modal Shift - no significant modal shift Optimisation - no existing infrastructure Safety - A narrow road at national speed limit with visibility constraints means the suggestion is considered to offer safety benefit. Biodiversity - Potential impact on existing hedge results in a negative score Leisure - Will join existing leisure routes so is considered worthy of a good score.
Weston	100	Roundabout A145	Poorly thought out cycle path for cyclist. Safest way to get onto the cycle path is heading south along the B1062. If heading north onto the roadabout from the a145, you have two choices head straight onto the B1062 then stop in the middle of the road to cross onto	If heading heading west along the cycle path to join traffic you have to cross over a busy road with limited visibility from the left. Dropped kurb to join cycle path on the a145. A middle	0	0	1	0	0	0	1	Connectivity and Growth – No significant connectivity and growth benefit Modal Shift – No significant modal shift benefit. Optimisation – The cycle and walking infrastructure is new and to a very good standard ensuring the best access onto it provides an optimisation and deemed to score 1.

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			the cycle path. Or turn right onto the a145 heading east then get stuck on the road or hop up the kirk at the safest opportunity.	island on the b1062 to wait and cross in to rejoin to head north.								Safety – It is unclear whether there is a safety issue particularly are there is an entrance onto the cycle path to the north. A neutral score is considered acceptable. Biodiversity – No significant biodiversity benefit. Leisure – There is not considered to be a significant leisure benefit.
Wickham Market	229	Wickham Market, new housing developments	Example of where significant housing development has been, and will be allowed without adequate local cycling infrastructure ie a cycle path to enable young people and their parents to cycle to the local primary school safely or indeed the village centre. Parents will always take the easy option when it comes to the daily school run and without safe infrastructure it will be to drive to school or pop down the local shops.	Create a safe cycle route either alongside the B1438 or along Chapel Lane, with a 20mph limit in the middle of Wickham, make the local streets limited to 20mph to encourage more of a sense of a nice neighbourhood where children can roam the streets free and safely.	2	0	0	3	-1	0	4	The commenter proposes reducing speed limits to 20mph through Wickham Market, however this is outside the remit of the project and should be passed onto highways. For the purpose of this assessment, introducing a cycleway and footway along the B1438 into Wickham Market village centre and to Pettistree will be assessed. Connectivity and Growth – The proposal would connect Pettistree and Wickham Market. As Wickham Market has a number of services not available within Pettistree, the proposal will likely have a somewhat significant connectivity benefit, therefore a score of 2 is considered reasonable. Modal Shift – As it is unlikely that infrastructure can be delivered to the highest standard within the Wickham Market village centre, PCT suggests that the proposal will not result in a significant modal shift. Optimisation – The proposal is for new infrastructure and is not considered, therefore, an optimisation. Safety – This section of the B1438 does contain a NSL and, as a 'b' type road, is likely busy, therefore the proposal will likely have safety benefits. A score of 3 is warranted under this category. Biodiversity – The proposal will likely result in the loss of managed grass areas, but over a significant length hence the small negative score. Leisure – There are limited leisure routes nor does it connect to leisure attractions so it scores a 0.
Wickham Market	374	A section of permissive footpath on our circular walks route, south side of B1078 The Gallows Route developed with SCC (Discover Suffolk)	A section of permissive footpath on our circular walks route, blue The Gallows Route developed with SCC (Discover Suffolk) has been closed by the landowner forcing people to walk along the dangerous B1078.	Liaise with landowner and SCC Highways to arrange re-opening please. Raised several times this year with SCC and a Cllr.	0	0	0	3	0	1	4	Connectivity and Growth – The proposal will unlikely have significant connectivity benefit. Modal Shift – Insufficient evidence to suggest that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Currently, pedestrians have to walk along the B1083, which is a busy road with a national speed limit, providing a footpath will safely connect PROWs and remove pedestrians off road. Biodiversity – No significant biodiversity impact. Leisure – The proposal would connect a number of PROWs, which are particularly attractive, but are in undesignated areas – this warrants a score of 1.
Wickham Market	619	Between Potsford Brook and the footpath that goes to the Gallows on the B1078 west of Wickham Market.	There is already an improved suggestion but if the landowner declines to allow walking along the field edge on the north side of the 1078, then consider opening up a part of the woodland on the south side as a right of way or permissive path.		0	0	0	3	-3	1	1	Connectivity and Growth – The proposal will unlikely have significant connectivity benefit. Modal Shift – Insufficient evidence to suggest that the proposal will result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Currently, pedestrians have to walk along the B1083, which is a busy road with a NSL, providing a footpath will safely connect PROWs and remove pedestrians off road. Biodiversity – A significant negative score is deemed reasonable due to the likely resultant loss of the established hedgerow and trees adjoining the south side of the road. Leisure – The proposal would connect a number of PROWs which are particularly attractive but are in undesignated areas – this warrants a score of 1.
Wickham Market	661	There are pinch points on the Hill at Wickham Market, at the Post Office and at The Teapot Tea Rooms. The hill coming up from Bordercot Lane on to The Hill	Cyclists to feel safe these areas to encourage them to cycle in and around the village	The introduction of 20mph speed limits and 'shared space' for cyclists, pedestrians and vehicles.	-1	0	0	-1	0	0	-2	The commenter proposes reducing the speed limit to 20mph, but this falls outside the remit of the project and should be passed to SCC. In terms of this assessment, removing the footways and creating a shared space for vehicles, pedestrians, and cyclists will be assessed. Connectivity and Growth – Removing the existing footway reduces connectivity and warrants a small negative score. Modal Shift – Insufficient evidence to suggest the proposal will provide a modal shift. Optimisation – The proposal is not considered an optimisation. Safety – Although the implementation of a shared space may make drivers more aware of pedestrians and cyclists, this section of the High Street is a 'b' type road is likely busy, therefore the removal of existing infrastructure in order to implement this warrants a score of -1. Biodiversity – No biodiversity impact. Leisure – No significant leisure benefit.
Wissett	280	A separate cycle/pathway along the south side of Halesworth Road from Wissett to Halesworth.	A separate cycle/walkway alongside the Halesworth Road from Wissett to Halesworth would make walking and cycling a lot safer for non-vehicle users along this narrow twisty country road which has a high bank and big hedges along its northern side. Many potential users do not use this route due to its obvious dangers for walkers and cyclists.		2	1	0	3	-3	2	5	Connectivity and Growth - connects Wissett to Halesworth which is a Market Town with important services and facilities. Modal Shift - A modest potential modal shift potential. Safety - national speed limit, narrow road, sharp bends. Biodiversity - Large stretch of road with some mature trees. Leisure - creates a connection to Halesworth which has lots of leisure opportunities.
Wissett	284	Halesworth Road from Wissett to Halesworth is very dangerous for cyclist and pedestrians	This Halesworth Road is narrow, twisting and bounded by a high bank on the north side. There is space on the south side of this road for a dedicated cycle/pathway which would encourage more people to cycle or walk the short distance into Halesworth. Currently it is too dangerous, except for the brave and the foolhardy to risk it. The number of bends means that drivers are often suddenly confronted with a walker or cyclist in a road that is only just wide enough for two cars		2	1	0	3	-3	2	5	Connectivity and Growth - Connects Wissett to Halesworth which is a Market Town with important services and facilities. Modal Shift - A modest potential uplift potential according to PCT. Safety- national speed limit, narrow road, sharp blind bends. Biodiversity - Large stretch of road with some mature trees. Leisure - Creates a connection to Halesworth which has lots of leisure opportunities.
Wissett	738	West and north of Halesworth	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and	Formalise newly devised circular walks to the West and North East of the town, that use existing public rights of way through SCC map creation. (working with the SCC PROW team to commission new maps).	0	0	0	0	0	0	0	The issue and recommendation provided has been considered in the creation of the strategy, however it is too broad in scope to be realistically and effectively scored against the methodology

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			wellbeing of residents, and supporting the town as a tourist destination.									
Woodbridge	74	Ipswich Road, Woodbridge	Very dangerous for cyclists on the route into Woodbridge	Dedicated cycle lane, possibly two way alongside/incorporating the wide footpath, as far as the Cherry tree road junction. Provide some quality bike parking in Woodbridge.	2	1	0	2	0	2	7	Connectivity and Growth – The proposed infrastructure will create a cycle route connecting Martlesham to Woodbridge. Connecting the two settlement areas will likely have significant connectivity benefits (despite Martlesham already being a well-established settlement area) with Woodbridge being a market town containing key services. The proposal would also connect to Woodbridge train station. Modal Shift – Using PCT, a shared cyclist/pedestrian path will provide a small uplift, therefore a score of 1 is considered reasonable. Optimisation – This does not optimise existing infrastructure. Safety – Despite Ipswich Road having a 30mph speed limit, it is ‘B’ type road, therefore speed and volume of traffic is expected to be high. With consideration to the road conditions, having a pavement that takes cyclists off the road receives a score of 2. Biodiversity – There are no biodiversity impacts. Leisure – the improvement will create a route to Woodbridge town centre, which has numerous restaurants/public houses and cafes. Moreover, Ipswich Road is a key route in order to get to the walks along the River Deben and to Kingston Avenue Recreation Ground.
Woodbridge	80	Melton to Martlesham road	Not a problem for me but many others say they won't cycle on the main road from Melton to Woodbridge as there is no designated space for them.	Provide a designated cycling space on main road from Melton, though Woodbridge, meeting up with the cycling section in Martlesham, which then goes to Ipswich.	3	2	0	2	0	2	9	Connectivity and Growth – Woodbridge, Melton, and Martlesham are well-established settlement areas with their own schools, shops, and employment opportunities. However, the proposal would connect these three settlement areas via the B1438, which resides along the Woodbridge key corridor. Furthermore, the proposed infrastructure would connect to the existing cycle infrastructure in Martlesham which forms part of the cycle route to Ipswich, therefore the proposal scores significantly under 'connectivity and growth'. Modal Shift – Using PCT the proposed infrastructure would provide a moderate modal shift uplift (mostly within Melton), therefore a score of 2 is considered reasonable. Optimisation – the proposal is for new infrastructure and does not optimise the existing. Safety – the B1438 between Melton and Martlesham has a 30mph speed limit, however it is a busy 'B' type road which contains a couple sharp corners along Lime Kiln Quay Road and numerous parked cars along Melton Hill and Melton Road. With consideration to the road conditions, infrastructure that removes cyclists off the road scores moderately. Biodiversity – there are no significant biodiversity impacts. Leisure – The proposed infrastructure will also likely provide moderate leisure benefit as it connects other settlement areas to Woodbridge which represents a strong leisure centre as it contains café/restaurant offers, heritage buildings, and local attractions.
Woodbridge	98	Ipswich Road Woodbridge	Pedestrians have to cross the road 3 or 4 times walking in or out of Woodbridge (. from the duke of York) The road is very busy and it's dangerous	Make new footpath so that there is a footpath on both sides of the road. Provide a safe crossing place at the Framfield house surgery	1	0	0	2	-1	0	2	Connectivity and Growth – the proposal provides modest connectivity benefit as it would connect existing infrastructure which, subsequently, would create a more direct route into Woodbridge town centre as it reduces the need to cross the road numerous times in order to walk on a footway. Modal Shift – It is unlikely that the proposal would result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – As the current infrastructure along the road is not connected it requires a pedestrian to cross the B1438, which is a busy 'b' type road with a 30mph speed limit and speed and volume of traffic is likely high, therefore the proposal would likely provide moderate safety benefits. The proposal warrants a score of 2 under this category. Biodiversity – The proposal would likely result in the loss in tracts of grassed verges. Leisure – It is unlikely that the proposal will provide significant leisure benefits.
Woodbridge	155	Footpath / cycleway from Farlingaye Coach park to Woods lane	In places the path is not wide enough for cyclists and pedestrians to pass safely.	Consider widening the path to minimum national standards for combined cycle/footpath, in places there appears to be significant grass verge to allow this to be done. Ensure rigorous pruning of path side vegetation.	0	0	1	0	-1	0	0	Connectivity and Growth – the proposal will have no significant connectivity benefits as it is already a shared pavement. Modal Shift – No impact. Optimisation – Widening the shared pavement makes the route more user friendly, therefore a score of 1 in this category is considered reasonable. Safety – no significant safety benefit. Biodiversity – the proposal would result in the loss of grass verges segregating the A12 from the shared pavement, a small negative score under 'Biodiversity' is considered reasonable due to the length of improvements required. Leisure – the proposal will have limited leisure benefit as it is already an existing pavement.
Woodbridge	156	Footpath west of A12 bypass, between Seckford Hall Lane & Dobbies (Wyevale) Roundabout	Path can be overgrown at times and is not wide enough to cycle along. Cyclist will come from Grundisburgh via B1079 to Wyevale roundabout and then want to travel south towards 'Melton End' of Woodbridge. This would be a more direct route connecting with the Footpath Crossing just south of Seckford Hall lane	Widen path to cycle / footpath standard	2	0	0	3	-1	0	4	Connectivity and Growth – Although the southern side of Woodbridge is primarily a residential area, the proposal would likely have moderate connectivity benefits as it will connect the residential area to the existing cycle and walking infrastructure just north of the B1079/A12 roundabout, which is a key commuter route to Farlingaye. Also, the proposal would provide connection to Kyson Primary School. It is worth noting, that this part of the A12 forms part of the Woodbridge key corridor, however the proposals are for the east side of the road rather than the west side. Modal Shift – according to PCT a shared pavement is unlikely to create a significant modal shift. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing. Safety – This stretch of the A12 has a national speed limit and as a straight 'A' type road, volume and speed of traffic is likely going to be high. With consideration to the

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												road conditions, a score of 3 under this category is considered reasonable. Biodiversity – The proposal will result in the loss of well-kept grassed areas; the proposal scores a small negative score under 'Biodiversity' due to the length of improvements required. Leisure – No significant leisure benefit.
Woodbridge	170	Cumberland Street	Drivers consistently ignore the time restrictions and use this route as a rat-run.	Turning the road into fully 1-way from North-East to South-West would reduce it's desirability as a rat-run - but continue to allow 2-way bicycle traffic	0	0	1	1	0	0	2	Connectivity and Growth – Cumberland Street allows one-way entry and restricts access on Monday-Saturday between 10am-3pm for vehicles, therefore the connection already exists so the proposal does not score in this category. Modal Shift – it is unlikely that the proposal would result in significant modal shift. Optimisation – the proposal does provide moderate improvements to this existing connection as it will prevent two-way traffic subsequently allowing more space for cyclists and pedestrians, therefore a score of 1 is considered reasonable. Safety – Although this road may be used to bypass a small section of Station Road, it is a minor road with a 30mph speed limit and has restricted access between 10am-3pm on Monday-Saturday. It is narrow however, and it is likely that two-way traffic would cause conflict between cyclists and vehicles. With this in mind, a score of 1 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – Woodbridge town centre, which Cumberland Street directly connects to, is a key strategic location and includes an array of shopping, eating, and drinking establishments, however as an existing connection the proposed improvement will not have a significant impact on leisure.
Woodbridge	171	The Thoroughfare	Cars using the road as a rat-run	Reversing the one-way direction would remove the routes desirability as a rat-run.	0	0	1	1	0	0	2	Connectivity and Growth – Cumberland Street allows one-way entry and restricts access on Monday-Saturday between 10am-3pm for vehicles, therefore the connection already exists so the proposal does not score in this category. Modal Shift – it is unlikely that the proposal would result in significant modal shift. Optimisation – the proposal does provide moderate improvements to an existing connection, as it would reduce the number of vehicles using the road in order to bypass Station Road; furthermore, a one-way system throughout the road would allow more room for vehicles to safely overtake cyclists using the road. With consideration to the previous, a score of 1 is considered reasonable. Safety – the reversing of the one-way system may reduce the number of vehicles using this road, therefore making it moderately safer for cyclists and pedestrians to use. Biodiversity – no biodiversity impact. Leisure – Cumberland Street directly connects to Woodbridge town centre, which is key strategic location and includes an array of shopping, eating, and drinking establishments, however as an existing connection the proposed improvements will not have a significant impact on leisure.
Woodbridge	179	Riverside path from Broomfield to Woodbridge	This is a single track path suitable only for walkers, and believe cyclists are not permitted. However over the past year more and more cyclists are using it and it is plainly not suitable for mixed use.	Widen the path to permit a cycle lane to be built or prevent cyclists from using it with physical barriers.	1	3	0	2	-1	3	8	Connectivity and Growth – the proposal would create a new connection between Melton and Woodbridge, which are large and well-established settlements, however there is unlikely to be significant everyday use due to both settlements having good levels of schools, shops, employment opportunities. Due to where the proposal is situated, it will likely have more leisure benefit than connectivity benefit, however a moderate score of 1 under this scoring category is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The River Deben path, being located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a significant uplift, this warrants the highest score under this category. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Removing cyclists off the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which form an obstacle. The proposal does, therefore, warrant a score of 2 under 'safety'. Biodiversity – the Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – the proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Woodbridge	204	The Thoroughfare, Woodbridge	This is a narrow ancient street where cars pedestrians and cyclists are not segregated. Despite the no access to vehicles at certain times restriction cars and delivery vehicles are still ignoring this, creating a conflict particularly between pedestrians, mobility scooters and vehicles.	Install 'pop up' barriers/bollards at the Melton End (& retain existing one way system) as per the centre of Cambridge to remove all non essential motorised traffic from this street completely. This would make the whole Thoroughfare a more pleasant place to 'be in' both for local residents, shoppers, and visitors to woodbridge. Deliveries to shops could be made overnight, emergency services could have transponders...it works in Cambridge why not Woodbridge or indeed other East Suffolk towns which have a 'thoroughfare' style main street.	0	0	1	1	0	0	2	Connectivity and Growth – The Woodbridge Thoroughfare is a pedestrian zone and restricts vehicular access between 10am-4pm on Mon-Sat, therefore the connection already exists so the suggestion does not score in this category. Modal Shift – the road is relatively quiet on PCT, but busy on Strava Metro. Even if improvements are provided, it is unlikely to provide significant modal shift, hence a score of 0. Optimisation – the proposal does provide moderate improvements to a cyclist/pedestrian priority route as it will restrict some vehicular traffic, therefore a score of 1 is considered reasonable. Safety – the Thoroughfare is a narrow road with a 30mph speed limit, and the proposal would restrict further vehicular access, therefore a moderate score of 1 is considered reasonable. Biodiversity – no biodiversity impact. Leisure – the Thoroughfare is a key strategic location and includes an array of shopping, eating, and drinking establishments, however as an existing pedestrian zone the proposed improvement will not have a significant impact.

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Woodbridge	234	Sandy Lane, Woodbridge	Sunday 8th November I found Sandy Lane closed to vehicles and barriered off just north of the nursery entrance due to a burst water main...It was wonderful..there were a number of people walking and cycling along it in complete safety not a car in sight. I was following NCN 1 from Charsfield to Ipswich Waterfront on my bike.	This shows that by making it a dead end with some bollards at this location a well known rat run can be turned into a pleasant place for people to cycle and walk along in complete safety. Access to the businesses along it would not be affected.	3	1	0	3	0	2	9	Connectivity and Growth – the proposal would create a new connection between Martlesham and Woodbridge, which are large and well-established settlements. As Sandy Lane resides within a key corridor, a score of 3 is considered reasonable. Modal Shift – According to PCT, Sandy Lane is currently well used, and the improvement could score a 3 at the highest standard. However, the route is unlikely to be completely traffic free so the modal shift to the lower standard does not represent as a significant gain. A score of 1 is considered reasonable. Optimisation – Whilst the proposal provides benefits, it does not optimise the existing route. Safety – Sandy Lane is a narrow road with a national speed limit and is likely used as a rat run to bypass the main roads. As the road currently does not have either cycling or walking infrastructure, it is considered that a modal filter will provide safety benefits hence a score of 3. Biodiversity – There are no biodiversity impacts. Leisure – the proposal would connect to the PROW routes which reside along Martlesham creek and the River Deben - as these are particularly attractive routes that extend through the AONB designation, a score of 2 is considered reasonable.
Woodbridge	235	NCN 1 Junction of Old Barrack Road with the B1438	When approaching this junction from Old Barrack Road cyclists have to use the road junction itself to cross into California. This can be problematical if the B1438 is busy and not suitable for the young and inexperienced rider.	There is a central refuge for the footpath adjacent to the pub. This footpath could be widened into a combined cycle/footpath separate from the actual junction itself, so that there is an obvious route across the road for cyclists/pedestrians into 'California'. Particularly as this junction forms part of NCN 1 and the cycle route to Martlesham	1	0	0	2	0	0	3	Connectivity and Growth – This section of the B1438 provides limited connections, however it does reside within the Melton-Ipswich key corridor and it is likely the proposal would help in the completion of a small section of the key corridor. Therefore, a score of one under 'connectivity and growth' is considered reasonable. Modal Shift – The proposal will unlikely lead to a significant modal shift. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing. Safety – The B1438 has a 30mph speed limit, however it is a busy 'b' type road so volume and speed of traffic is likely to be high. Despite the proposal covering a small section of the road, it is considered to provide a moderate safety benefit. Biodiversity – The development of a shared pavement will likely result in part loss of the well-kept green verge adjacent the public house, however it likely has limited biodiversity value hence a score of 0 under this category. Leisure – the proposal provides limited leisure benefit.
Woodbridge	238	The junction of Warren Hill Road with Ipswich Road.	When cycling up the hill from the Cherry Tree Road mini roundabout it is extremely difficult and dangerous to move across in order to turn right into Warren Hill Road. When waiting at the junction in the middle of the road for a gap in the traffic in order to turn right is very hazardous.	Road markings need to mark out a right turn lane and a illuminated bollard would provide some protection/safety when waiting to turn.	0	0	0	2	0	0	2	Connectivity and Growth – The proposed alteration does not create additional connectivity Modal Shift – This does not create a modal shift Optimisation – This does not optimise existing cycle/walking infrastructure. Safety – This would be for highways to judge. The cyclist would remain on the road, however improving the junction is considered to warrant 2 points . Biodiversity – There is no significant biodiversity benefit. Leisure – There appears to be limited leisure benefits.
Woodbridge	239	The traffic lights at the junction of The Thoroughfare and Melton Road.	When cycling into Woodbridge you may need to turn right at these traffic lights to either go straight over into the Thoroughfare or right into St.Johns Street. There is nothing marked on the road to show where cyclists should wait and nothing to protect you from oncoming traffic. The filter system of the lights often mean that you are waiting in the middle whilst traffic squeezes by on your inside and is also passing you on the other side.	A space for cyclists to wait, a bollard to protect and make traffic keep their distance.A mini roundabout may help.	0	0	0	2	0	0	2	Connectivity and Growth – The proposed alteration to the junction does not create additional connectivity.Modal Shift – This does not create a modal shift.Optimisation – this does not optimise existing cycling or walking infrastructure.Safety – the cyclist would remain on the road, however improving the junction is considered to warrant 2 points.Biodiversity – There are no significant biodiversity benefits.Leisure – there appears to be limited leisure benefit.
Woodbridge	254	Sandy Lane, Woodbridge	This is a National Cycle Route and could be improved by closing the road to through traffic by bollarding off underneath the railway bridge.	Bollarding off the carriageway can be achieved as there are adjacent turning areas.We achieved this on another site in the West Midlands. I have submitted a report to you covering Woodbridge and Melton on walking and cycling and am happy to give suggestions free of charge.	3	1	0	3	0	2	9	Connectivity and Growth – the proposal would create a new connection between Martlesham and Woodbridge, which are large and well-established settlements. As Sandy Lane resides within a key corridor, a score of 3 is considered reasonable. Modal Shift – According to PCT, Sandy Lane is currently well used, and the improvement could score a 3 at the highest standard. However, the route is unlikely to be completely traffic free so the modal shift to the lower standard does not represent as a significant gain. A score of 1 is considered reasonable. Optimisation – Whilst the proposal provides benefits, it does not optimise the existing route. Safety – Sandy Lane is a narrow road with a national speed limit and is likely used as a rat run to bypass the main roads. As the road currently does not have either cycling or walking infrastructure, it is considered that a modal filter will provide safety benefits hence a score of 3. Biodiversity – There are no biodiversity impacts. Leisure – the proposal would connect to the PROW routes which reside along Martlesham creek and the River Deben - as these are particularly attractive routes that extend through the AONB designation, a score of 2 is considered reasonable.
Woodbridge	257	Grundisburgh road B1079 and Grove Road roundabout , close to garden centre	It is very difficult to cross the roundabout on foot or by bike to get from Woodbridge town to the garden centre and/or beyond. The pedestrian lights further up the A12 are not a direct route for pedestrians .Traffic does not always stop at these lights as it tends to speed up after the roundabout.	A better crossing for bikes and pedestrians , closer to the roundabout. Or reduced speed restrictions on this stretch of road between the roundabout and existing traffic lights	1	0	0	1	0	0	2	Connectivity and Growth – the A12 represents a modest barrier between those situated on either side, but there is – although not as direct – a pedestrian crossing with traffic lights north of the roundabout which can be used. Therefore, a score of 1 is considered reasonable. Modal Shift – There is insufficient evidence to suggest any significant modal shift. Optimisation – This does not improve the existing infrastructure. Safety – The suggestion offers a small safety benefit as the A12 (Grove Road) is 40mph busy dual carriageway, but there is already a safe crossing point north of the roundabout which can be used. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion provides limited leisure benefit.
Woodbridge	261	Deben riverside path from Wilford Bridge to Martlesham	There is no cycling permitted along this route along the Deben. It would be the obvious choice for cycling due to the flat nature of the terrain and the hilly nature of Woodbridge. This would encourage children and parents to cycle to the Melton primary school.It would	A shared track with pedestrians would be an improvement. In the short term allowing cycling as it is but with signs informing cyclists that pedestrians have the right of way. If this is done it would help ES to monitor the situation to asses the pros and cons.	1	3	0	2	-1	3	8	Connectivity and Growth – the proposal would create a new connection between Melton and Woodbridge, which are large and well-established settlements, however there is unlikely to be significant everyday use due to both settlements having good levels of schools, shops, employment opportunities. Due to where the proposal is situated, it will likely have more leisure benefit than connectivity benefit, however a

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			possibly help alleviate the pollution at the junctions in Woodbridge and Melton. Cycling to the stations from areas of Melton and Woodbridge would be much easier and would relieve pressure on traffic and station parking.									moderate score of 1 under this scoring category is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The River Deben path, being located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a potential significant uplift, this warrants the highest score under this category. Optimisation – the proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Removing cyclists off the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which form an obstacle. The proposal does, therefore, warrant a score of 2 under 'safety'. Biodiversity – the Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – the proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Woodbridge	269	The length of the Woodbridge Thoroughfare.	Frequency and speed of traffic is unacceptable and totally unreasonable.	Vehicles & cycles need to be banned and the Thoroughfare made pedestrian only. Residents would need to be given access at certain hours. The car park could increase disabled parking to assist but at present the speed and frequency of traffic is unacceptable and totally unreasonable. There are plenty of examples of where this has been successfully implemented.	-3	0	0	0	0	-3	-6	Connectivity and Growth – The Thoroughfare resides within the Ipswich-Melton key corridor, therefore restricting access to cyclists would disrupt this route. Also, as there are a number of key services along the Thoroughfare, due to the Thoroughfare forming part of the town centre, the proposal has a significant negative impact on connectivity and growth. Modal Shift – No significant modal shift. Optimisation – No optimisation of existing infrastructure. Safety – The Thoroughfare has a 30mph speed limit; however, as there are existing vehicular restrictions during particular times of the day, it is unlikely that the proposal would have a significant safety benefit. Furthermore, this category concentrates on conflict between vehicles and cyclists/pedestrians, resulting in no safety benefit with restricting cyclists. With consideration to the previous, the proposal would not have a significant safety benefit resulting in a score of 0. Biodiversity – No impact on biodiversity. Leisure – As the Thoroughfare is one of the roads that forms Woodbridge town centre, restricting access to cyclists would also restrict access to leisure attractions such as drinking and eating establishments. The proposal has a negative impact on Leisure, therefore a score of -3 is considered reasonable.
Woodbridge	271	Willford Bridget to Martlesham creek. Waldringfield along the river front to Woodbridge	We walk these areas and are passed by cycles on these footpath routes, it is a bone of contention for walkers and cyclist. In Scotland I believe that footpaths can be used by cyclist as well as walkers, why can we not just adopt this policy. The paths can be used by both as long as cyclist pass with caution and slow down. I like to cycle also but in Woodbridge we are restricted to the roads as the only safe cycle route is by the bypass, and you have to cycle the roads to get there.	solution make the footpaths for cycles as well, with the emphasis that the walker has the right of way with the cyclist either dismounting or passing with care.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. The PROW system has been a strong consideration in the formation of the strategy and where specific paths would benefit from upgrades to bridleways these have been proposed.
Woodbridge	273	Woodbridge Maidensgrave area	No dedicated cycle route from the thoroughfare to this part of Woodbridge for local cyclists. The B1438 is not a cycle friendly road, especially when turning right into Warren Hill Raod.	NCN 1 runs along Old Barrack Road from the Thoroughfare...consider making this a local cycle route with 20mph limit, proper segregation and signage to encourage local cyclists, rather than just those following the NCN, to use it as a safe route to and from the centre of Woodbridge (encompassing Kyson Primary School).	3	0	0	1	-1	3	6	Connectivity and Growth – The proposal would likely have significant connectivity and growth benefits. The NCN1 connects to key services and provides a direct connection into Woodbridge town centre, which is a strategically important area, and also forms part of the Ipswich to Melton key corridor. With consideration to the previous, the proposal scores a 3 under this category. Modal Shift – It is unlikely that infrastructure to the highest standard could be delivered on these roads; therefore the proposal would not result in a significant modal shift hence a score of 0 under this category. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – The roads that form part of the NCN1 along Old Barrack through to the Thoroughfare have 30mph speed limits, therefore taking cyclists and pedestrians off-road will provide moderate safety benefit to an already relatively safe road. Biodiversity – The proposal would likely result in the loss of some managed grassed areas, which have small biodiversity value, therefore a small negative score under this category is justified. Leisure – As the proposal would connect directly into Woodbridge town centre, it will provide significant leisure benefit due to the comparative shopping, eating/drinking establishments, and historic/cultural attractions.
Woodbridge	274	Woodbridge Station	Lack of secure undercover cycle storage...useful for anyone commuting to work or making longer journeys the facility to leave your bike fro extended periods of time in a safe undercover facility like the one at Ipswich Station. Rather than just locking it to a 'Sheffield Stand' out in the open, not covered by CCTV and hoping for the best.	Provide a storage facility similar to that at Ipswich Platform 1	0	1	1	0	0	2	4	Connectivity and Growth – No significant connectivity and growth impacts. Modal Shift – Cycle parking alone is unlikely to encourage large numbers of modal shift, but a certain level may be provided so a score of 1 is deemed appropriate. Optimisation – The security and cover add to the existing infrastructure, so a single point has been awarded. Safety – No significant safety benefit. Biodiversity – No significant biodiversity benefit. Leisure – Woodbridge station is sandwiched between the town centre, which represents a strong leisure centre as it contains café/restaurant offers and local attractions, and the Deben Estuary, therefore the improvements will likely have a strong impact awarding the proposal 2 points.

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Woodbridge	330	Sandy Lane, south of junction with Broomheath.	Sandy Lane is used as a rat run or alternative route for car drivers which makes cycling and walking a less safe and less attractive option.	Close road here to through traffic to provide part of a safe cycle route between Woodbridge, Martlesham and Ipswich.	3	1	0	3	0	2	9	Connectivity and Growth – The proposal would create a new connection between Martlesham and Woodbridge, which are large and well-established settlements. As Sandy Lane resides within a key corridor, a score of 3 is considered reasonable. Modal Shift – According to PCT, Sandy Lane is currently well used, and the improvement could score a 3 at the highest standard. However, the route is unlikely to be completely traffic free so the modal shift to the lower standard does not represent as a significant gain. A score of 1 is considered reasonable. Optimisation – Whilst the proposal provides benefits, it does not optimise the existing route. Safety – Sandy Lane is a narrow road with a national speed limit and is likely used as a rat run to bypass the main roads. As the road currently does not have either cycling or walking infrastructure, it is considered that a modal filter will provide safety benefits hence a score of 3. Biodiversity – There are no biodiversity impacts. Leisure – The proposal would connect to the PROW routes which reside along Martlesham creek and the River Deben - as these are particularly attractive routes that extend through the AONB designation, a score of 2 is considered reasonable.
Woodbridge	355	The whole of the river path from Martlesham to Melton is unsuitable for dual use (pedestrians and cyclists). Cyclists are currently prohibited, but very few take notice of the fact and push past	The path is only just wide enough for pedestrians to pass in a lot of places. To widen it to the necessary regulation width for dual use would likely not be possible and would also spoil the area. Enforcement is necessary before someone is seriously injured.	enforcement action against cyclists using the path							N/A	Issues relating to the enforcement of PROW routes are a SCC specific matter have been shared with SCC for their consideration as the Highways Authority.
Woodbridge	384	Junction of the top (i.e. west end) of Market Hill and west-bound Seckford Street	Firstly, visibility from the top of Market Hill into west-bound Seckford Street is non-existent. One has to pull out across the junction to see if there is anything coming, and if there is, then there is little space for the oncoming vehicle. Secondly, vehicles coming up the south side of Market Hill and turning across the top of Market Hill cut the corner, right into the path of any cyclist waiting to turn right into Seckford Street.	Make the Market Hill a one-way street all the way round, clockwise. This will clear the problem completely.	0	0	0	2	0	1	3	Connectivity and Growth – Although the proposal is located within Woodbridge town centre, which is a strategically important area, it does not connect to any key services. The proposal would connect to a small handful of leisure attractions, such as public houses and cafés, therefore the proposal would likely have more leisure benefit than connectivity benefit. It is considered therefore, reasonable for the proposal to not score under this category. Modal Shift – The road is relatively quiet on PCT and improvements are unlikely to provide significant modal shift. Optimisation – Whilst it provides benefits, it does not optimise an existing route. Safety – Market Hill has a 30mph speed limit and the B1079/Market Hill junction has limited visibility. The proposal will prevent two-way traffic, subsequently allowing more space for cyclists. Furthermore, the proposal will likely allow cyclists to approach the B1079/Market Hill junction at a wider stance, which will increase visibility. Therefore, a score of 2 under 'safety' is considered reasonable. Biodiversity – No biodiversity impact. Leisure – The proposal connects to small leisure attractions which includes a public house, cafés, and other small shops. With consideration to the previous, the proposal warrants a score of 1 under leisure.
Woodbridge	385	Junction of the top (west end) of Market Hill and the east side	Cyclists going north along the top of Market Hill and wanting to turn east down the side of the Shire Hall have no visibility of oncoming traffic coming down Theatre Street, and so have to pull out to look, into the path of any oncoming vehicle. As vehicle exiting from the top of Angel Lane tend to cause vehicles travelling down Theatre Street to pull out, this means these vehicles are already on the wrong side of the road when they meet the Market Hill junction, thus compounding the problem.	Make the Market Hill a one-way street all the way round, clockwise. This will allow cyclists to get into the right hand lane at the top of Market Hill and have greater visibility up Theatre Street. This will clear the problem completely.	0	0	0	2	0	1	3	Connectivity and Growth – Although the proposal is located within Woodbridge town centre, which is a strategically important area, it does not connect to any key services. The proposal would connect to a small handful of leisure attractions, such as public houses and cafés, therefore the proposal would likely have more leisure benefit than connectivity benefit. It is considered therefore, reasonable for the proposal to not score under this category. Modal Shift – The road is relatively quiet on PCT and improvements are unlikely to provide significant modal shift. Optimisation – Whilst it provides benefits, it does not optimise an existing route. Safety – Market Hill has a 30mph speed limit and the B1079/Market Hill junction has limited visibility. The proposal will prevent two-way traffic, subsequently allowing more space for cyclists. Furthermore, the proposal will likely allow cyclists to approach the B1079/Market Hill junction at a wider stance, which will increase visibility. Therefore, a score of 2 under 'safety' is considered reasonable. Biodiversity – No biodiversity impact. Leisure – the proposal connects to small leisure attractions which includes a public house, cafés, and other small shops. With consideration to the previous, the proposal warrants a score of 1 under leisure.
Woodbridge	460	The entire Riverside of Woodbridge and Melton from Kyson Point to Wilford Bridge	Tourism is vital to Woodbridge's economy and the river is a major tourist attraction. I know that cycle tour companies have expressed amazement that it is not possible to cycle through Woodbridge along the river bank. It is scandalous that we do not make the most of our beautiful river and actively discourage cyclists. There is no safe provision anywhere in the town for them.	From Kyson Point to The Avenue there is a rough narrow grass track below and to the left of the raised river path that could be made into a cycle path. From just beyond Deben Road to Wilford Bridge in many places there are already two clear paths and it should be possible to convert and extend one of these into a cycle path. In the few places where this would not be possible could there not be signs saying 'cycling permitted but priority must always be given to pedestrians'. In my experience if you are a polite careful cyclist, pedestrians have no objection to cyclists along the part of the river. Between The Avenue and Deben Road there should be signs diverting cyclists along the road. A 20 mph limit should be established on the Avenue, Cherry Tree Road, Kingston Farm Road, Kingston Road and Station Road, so that where there is not a dedicated route along the river cyclists can be diverted to a cycle friendly route.	1	3	0	2	-1	3	8	Connectivity and Growth – The proposal would create a new connection between Melton and Woodbridge, which are large and well-established settlements, however there is unlikely to be significant everyday use due to both settlements having good levels of schools, shops, employment opportunities. Due to where the proposal is situated, it will likely have more leisure benefit than connectivity benefit, however a moderate score of 1 under this scoring category is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The River Deben path, being located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a potentially significant uplift, this warrants the highest score under this category. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Removing cyclists off the majority of the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is a busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which create obstacles. The proposal does, therefore, warrant a score of 2 under 'safety'. Biodiversity – The Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – The proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in

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												itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Woodbridge	461	the junction of the Thoroughfare and Lime Kiln Quay Road, Woodbridge (traffic lights)	dangerous junction for cyclists	provision of a cyclists' box marked out in front of the car traffic - particularly necessary if travelling from Melton Hill and going right or straight on at the lights and if travelling up Lime Kiln Quay Road going right.	0	0	0	2	0	0	2	Connectivity and Growth – The proposed alteration to the junction does not create additional connectivity. Modal Shift – This does not create a modal shift. Optimisation – This does not optimise existing cycling or walking infrastructure. Safety – The cyclist would remain on the road, however improving the junction is considered to warrant 2 points. Biodiversity – There are no significant biodiversity benefits. Leisure – There appears to be limited leisure benefit.
Woodbridge	513	Sandy Lane, Martlesham as far as Ipswich Road, Woodbridge	Many motorists tend to drive too fast and show their reluctance to slow down for less powerful craft such as a bicycle. The railway bridge often results in a last second lurch for many. For a cyclist to exit the bottom of the hill from Broomheath on the way to Woodbridge, it has become quite difficult to exit onto Ipswich Road going to Woodbridge.	Possible solution might be to widen the pavement thus curbing the motorists and allow cyclist to share with the few pedestrians. I would be interested to hear your views.	3	1	0	3	-3	2	6	Although the comment refers to the entirety of Sandy Lane, the proposal is to widen the existing pavement to a shared cyclist/pedestrian path, however the existing path is only situated at the north of the road. Therefore, for the purpose of this assessment, the implementation of a new shared pathway throughout the route will be scored. Connectivity and Growth – The proposal would create a new connection between Martlesham and Woodbridge, which are large and well-established settlements. As Sandy Lane resides within a key corridor, a score of 3 is considered reasonable. Modal Shift – According to PCT, Sandy Lane is currently well used, and the improvement could score a 3 at the highest standard. However, the road is narrow, and it is unlikely that infrastructure can be delivered to the highest standard; therefore, the infrastructure will likely result in a small uplift hence a score of 1. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Sandy Lane is a narrow road with a national speed limit and is likely used as a rat run to bypass the main roads. Therefore, getting cyclists off road will have safety benefit so a score of 3 is considered reasonable. Biodiversity – The road is narrow so, in order to implement the proposed infrastructure, the removal of the established hedgerows and wild verges located along both sides of Sandy Lane is likely needed. Leisure – The proposal would connect to the PROW routes which reside along Martlesham creek and the River Deben – as these are particularly attractive routes that extend through the AONB designation, a score of 2 is considered reasonable.
Woodbridge	536	Ipswich Road	There isn't any provision for cyclists here and the traffic moves very impatiently. There's a lot of unsafe overtaking, especially when there are two cyclists going in different directions and motorists on each side trying to overtake.	Cycles lanes and wider pavements would be great on this stretch. If it felt safe walking or cycling between woodbridge and martlesham I'm sure many more people would do it.	2	1	1	1	0	2	7	Connectivity and Growth – The proposal will connect Woodbridge and Martlesham which are both large well-established settlements, therefore connectivity benefits are unlikely going to be significant. However, as Ipswich Road forms part of the Ipswich to Melton key corridor, a score of 2 is considered reasonable. Modal Shift – The proposal would likely result in a moderate modal shift hence a score of 1. Optimisation – The widening of the pavements is considered an optimisation, however it is unlikely that they could be widened to a width of 2m alongside the proposed cycle lanes. A score of 1 is considered reasonable. Safety – Despite Ipswich Road having a 30mph speed limit, it is a 'b' type road, therefore speed and volume of traffic is likely high. As the proposal would not take cyclists off-road, a score of 1 under safety is considered reasonable. Biodiversity – No biodiversity impacts. Leisure – Ipswich Road forms part of the route to Woodbridge town centre, which is a leisure attraction due to comparative shopping, eating and drinking establishments, and historic/cultural attractions. As Ipswich Road does not directly connect into the town centre, a score of 2 in this category is considered reasonable.
Woodbridge	552	JUNCTION between Warren Hill Road and Ipswich Road.	This is a very dangerous junction for cyclists turning right into Warren Hill Road. Motorists coming down the hill are going faster, also they often fail to see cyclists waiting in the centre of Ipswich Road to turn right; the driver side A pillar of their vehicle obscures the waiting cyclist. Also, vehicles bearing right round the bend tend to move to the centre of the road. This is so dangerous I will no longer make this turn by bike.	There needs to be a safe space for cyclists in the middle of the road. This requires an illuminated island at the junction and line markings on the road indicating cyclist space. NOT just white lines, these could cause more problems by giving the appearance of safe space. There have already been accidents involving cyclists at this junction.	1	0	0	2	0	0	3	Connectivity and Growth – Without a suitable junction, Ipswich road is a modest barrier for cyclists accessing north of the B1438 (Ipswich Road). Despite the junction providing limited opportunities to key services or employment land, it will likely improve the connection to the existing residential area north of the road, therefore a score of 1 under connectivity and growth is considered reasonable. Modal Shift – the proposal is unlikely to cause a significant modal shift. Optimisation – no significant optimisation benefit. Safety – Ipswich Road has a 30mph speed limit, however it is a busy 'B' type road, therefore the proposal of a safer junction for cyclists is awarded 2 points. Biodiversity – No significant biodiversity impact. Leisure – No significant leisure benefit.
Woodbridge	553	Junction of Ipswich Road with Warren Hill Road	When cycling up the hill along the Ipswich Road it is very dangerous turning right into Warren Hill Road. The oncoming traffic is fast, often breaking the 30mph speed limit, because the road is wide and the traffic is going downhill. Visibility for both traffic and cyclist is poor because it is on a blind bend. The cyclist is forced to wait in the middle of the road, between lines of traffic.	A safe space for cyclists in the centre of the road. Painted white lines as these are not visible enough to traffic, and could even make the problem worse by creating an illusion of safety for cyclists. Cyclists need to feel safe. An island is the only solution.	1	0	0	2	0	0	3	Connectivity and Growth – Without a suitable junction, Ipswich road is a modest barrier for cyclists accessing north of the B1438 (Ipswich Road). Despite the junction providing limited opportunities to key services or employment land, it will likely improve the connection to the existing residential area north of the road, therefore a score of 1 under connectivity and growth is considered reasonable. Modal Shift – The proposal is unlikely to cause a significant modal shift. Optimisation – No significant optimisation benefit. Safety – Ipswich Road has a 30mph speed limit, however it is a busy 'B' type road, therefore the proposal of a safer junction for cyclists is awarded 2 points. Biodiversity – No significant biodiversity impact. Leisure – No significant leisure benefit.
Woodbridge	554	Cumberland St off the B1438	Pavements are way too narrow here. Since resurfacing, cars go far too fast, often on the school run. Pedestrians have to walk on the road to maintain social distancing and are constantly at risk from vehicles.	This lovely medieval street should be shared use; space for vehicles should be reduced to one way with passing places and pedestrian space should be made wider by use of bollards and planters; an inexpensive solution. Ideally, resurface at one level.	0	0	1	1	0	0	2	Connectivity and Growth – Cumberland Street allows one-way entry and restricts access on Monday-Saturday between 10am-3pm for vehicles, therefore the connection already exists so the proposal does not score in this category. Modal Shift – It is unlikely that the proposal would result in significant modal shift. Optimisation – The proposal does provide moderate improvements to this existing connection as it will prevent two-way traffic, subsequently allowing more space for cyclists and pedestrians, therefore a score of 1 is considered reasonable. Safety – Although this road may be used to bypass a small section of Station Road, it

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												is a minor road with a 30mph speed limit and has restricted access between 10am-3pm on Monday-Saturday. It is narrow however, and it is likely that two-way traffic would cause conflict between cyclists and vehicles. With this in mind, a score of 1 is considered reasonable. Biodiversity – No biodiversity impact. Leisure – Woodbridge town centre, which Cumberland Street directly connects to, is a key strategic location and includes an array of shopping, eating, and drinking establishments, however as an existing connection the proposed improvement will not have a significant impact on leisure.
Woodbridge	555	The Avenue, north east side of Kingston Field	There is no pedestrian pavement on The Avenue, it is poorly lit. Vehicles now use the new car park adjacent to this road and it is consequently much busier than before. On dark nights pedestrians are all but invisible.	Safe access for pedestrians is required. Pavement?	0	0	0	1	0	1	2	Connectivity and Growth – No significant connectivity and growth benefits. Modal Shift – The proposal would not result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – The Avenue, also known as Jetty Lane, has a 30mph speed limit and it is relatively narrow, therefore taking pedestrians off-road will have a small safety benefit. Biodiversity – The proposal would likely result in the loss of a small managed grass verge, however due to its size it unlikely has significant biodiversity value. Leisure – The proposed new pathway would connect to Kingston Fields playground and does therefore, have small leisure benefit.
Woodbridge	556	Cumberland Street off B1438	Cumberland St is a beautiful medieval street which should be a pleasure to walk along. Instead it is an intimidating place because the pavements are very narrow. Cars go very close by at 30mph (and sometimes more) as there is nothing to slow them down, since the road is very smooth and the double yellow lines keep the road generally free of parked cars. People frequently need to walk in the road, if they need to pass each other, or walk two abreast for example.	Shared space for vehicles and pedestrians. Traffic could be slowed easily by putting planters alongside the pavement at intervals, narrowing the access for traffic and making it slow down. Drivers should be made aware that they need to share this space with other road users.	0	0	1	1	0	0	2	Connectivity and Growth – Cumberland Street allows one-way entry and restricts access on Monday-Saturday between 10am-3pm for vehicles, therefore the connection already exists so the proposal does not score in this category. Modal Shift – it is unlikely that the proposal would result in significant modal shift. Optimisation – the proposal does provide moderate improvements to this existing connection as it will allow more space for cyclists and pedestrians, therefore a score of 1 is considered reasonable. Safety – Although this road may be used to bypass a small section of Station Road, it is a minor road with a 30mph speed limit and has restricted access between 10am-3pm on Monday-Saturday. It is narrow however, and it is likely that traffic pass pedestrians and cyclists closely. With this in mind, a score of 1 is considered reasonable. Biodiversity – no biodiversity impact. Leisure – Woodbridge town centre, which Cumberland Street directly connects to, is a key strategic location and includes an array of shopping, eating, and drinking establishments, however as an existing connection the proposed improvement will not have a significant impact on leisure.
Woodbridge	557	Kingston Field	Kingston Field is entirely surrounded by kerbed areas; there is, surprisingly, no disabled access to this field.	Put in flat driveway type access in at least two places. Not too expensive and VERY disabled friendly.	0	0	0	0	0	1	1	Connectivity and Growth – No significant connectivity and growth benefit. Modal Shift – The dropped kerb would access onto the grass and other accesses are present nearby. The improvement is unlikely to have a significant impact to Modal Shift. Optimisation – This doesn't optimise existing network and it appears there are dropped kerbs available at different locations. Safety – The kerb onto grass would not appear a currently well used as an access onto the field when others appear available so this doesn't resolve a safety issue. Biodiversity – No significant biodiversity benefit. Leisure – The field provides leisure benefits albeit to a local catchment meaning 1 point has been scored here.
Woodbridge	558	The Avenue off Kingston Farm Road, Woodbridge	There is no pavement along here despite traffic increasing as a result of the new car park at the bottom of this Street. People walking along it have to dodge parked cars as well as traffic and have no space to stand or walk and feel safe.	Create a pavement	0	0	0	1	0	1	2	Connectivity and Growth – No significant connectivity and growth benefits. Modal Shift – The proposal would not result in a significant modal shift. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – The Avenue, also known as Jetty Lane, has a 30mph speed limit and it is relatively narrow, therefore taking pedestrians off-road will have a small safety benefit. Biodiversity – The proposal would likely result in the loss of a small managed grass verge, however due to its size it unlikely has significant biodiversity value. Leisure – The proposed new pathway would connect to Kingston Fields playground and does therefore, have small leisure benefit.
Woodbridge	559	Kingston Field, Woodbridge	No disabled access on to this important and intensively used council owned leisure space.	There should be two points of access, I suggest one at the bottom of Cherry Tree Road and another near the car park entrance on The Avenue.	0	0	0	0	0	1	1	Connectivity and Growth - No significant connectivity and growth benefit. Modal Shift – The dropped kerb would access onto the grass and other accesses are present nearby. The improvement is unlikely to have a significant impact to Modal Shift. Optimisation – This doesn't optimise existing network and it appears there are dropped kerbs available at different locations. Safety – The kerb onto grass would not appear a currently well used as an access onto the field when others appear available so this doesn't resolve a safety issue. Biodiversity - No significant biodiversity benefit. Leisure – The field provides leisure benefits albeit to a local catchment meaning 1 point has been scored here.
Woodbridge	560	The Turban Centre, Woodbridge.	There is nowhere to securely leave a bike in the Turban Centre.	Very simple. I appreciate that large cycle hoops will decrease pedestrian access; perhaps some rings in the wall of the Boots store to allow short term parking for, say, three bikes to lie alongside the wall there.	0	1	0	0	0	2	3	Connectivity and Growth – no significant connectivity and growth impacts. Modal Shift – cycle parking alone is unlikely to encourage large numbers of modal shift, but a certain level will be provided so a score of 1 is deemed appropriate. Optimisation – the proposal does not optimise existing infrastructure. Safety – no significant safety benefit. Biodiversity – no significant biodiversity benefit. Leisure – The turban centre is situated in Woodbridge town centre, which represents a strong leisure centre as it contains café/restaurant offers and local attractions, therefore the proposal will have a strong impact awarding the proposal 2 points.

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Woodbridge	561	Turban Centre, Woodbridge	Nowhere to leave cycles in the Turban Centre. Nearest cycle stores are too far away (next to Nero's in Thoroughfare or outside car park WCs)	Hoops in the wall of Boots, as standard cycle stores would take up too much pedestrian space.	0	1	0	0	0	2	3	Connectivity and Growth – No significant connectivity and growth impacts. Modal Shift – Cycle parking alone is unlikely to encourage large numbers of modal shift, but a certain level will be provided so a score of 1 is deemed appropriate. Optimisation – The proposal does not optimise existing infrastructure. Safety – No significant safety benefit. Biodiversity – No significant biodiversity benefit. Leisure – The Turban centre is situated in Woodbridge town centre, which represents a strong leisure centre as it contains café/restaurant offers and local attractions, therefore the proposal will have a strong impact awarding the proposal 2 points.
Woodbridge	562	Quay St, Church St, New St..	All these streets have inadequate space for pedestrians. Pavements are too narrow, vehicles go too fast.	Widen the pavements; if need be with temporary bollards, helping to maintain social distancing. Slow down the cars with obstructions. Better still, shut the cars out.	3	0	0	1	0	3	7	Connectivity and Growth – These roads connect into the Thoroughfare, which is currently an existing connection as a cyclist/pedestrian priority route, and the proposal will, therefore, create a direct connection into a strategically important area. It is considered reasonable therefore, for the proposal to score a 3 under this category. Modal Shift – According to PCT, these roads are currently moderately used, and the improvement could score a 2 at the highest standard. However, it is unlikely that the roads can be made completely traffic free and that the infrastructure can be delivered to the highest standard; therefore, the infrastructure will not likely result in significant modal shift. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Although these roads have a 30mph speed limit and are one-way, they are 'b' type roads and volume of traffic could be high; therefore, a proposal that could limit vehicular traffic, or remove cyclists off the road, will likely have moderate safety benefit. Biodiversity – The proposal will not have a significant impact on biodiversity. Leisure – Again, the proposal will create a direct connection into Woodbridge town centre which has significant leisure benefit due to the comparative shopping, eating/drinking establishments, and historical/cultural attractions.
Woodbridge	565	The whole of Quay Street, Church Street and New Street, Woodbridge	Here we have beautiful medieval town centre streets which are impossible to walk along feeling safe because the pavements are so narrow. Priority is given to the traffic using these streets, with pedestrians having to get out of the way. This traffic goes close by at 30mph (or more if it's breaking the current speed limit). As well as being dangerous is is polluting and noisy, especially HGVs. People must be allowed to feel safe, and be able too social distance from other pedestrians.	The traffic must be slowed down, and much more emphasis must be placed on traffic giving way to pedestrians. Pavements could be widened and the roads narrowed until the traffic can be shut out completely. Even Quay street could be treated in this way. The other two roads are one way so could easily be narrowed. Chicanes along New Street (one on South side of B1079, one near Mariners Pub) would slow traffic coming down the hill here. Another solution is to take away all distinctions between pavements and road, levelling the whole space in order to make the dominant hierarchy of road usage by cars less clear, forcing traffic to slow down for pedestrians,	3	0	0	1	0	3	7	Connectivity and Growth – These roads connect into the Thoroughfare, which is currently an existing connection as a cyclist/pedestrian priority route, and the proposal will, therefore, create a direct connection into a strategically important area. It is considered reasonable therefore, for the proposal to score a 3 under this category. Modal Shift – According to PCT, these roads are currently moderately used, and the improvement could score a 2 at the highest standard. However, it is unlikely that the roads can be made completely traffic free; therefore, the infrastructure will not result in significant modal shift. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Although these roads have a 30mph speed limit and are one-way, they are 'b' type roads and volume of traffic could be high; therefore, a proposal that could limit vehicular traffic will likely have moderate safety benefit. Biodiversity – The proposal will not have a significant impact on biodiversity. Leisure – Again, the proposal will create a direct connection into Woodbridge town centre which has significant leisure benefit due to the comparative shopping, eating/drinking establishments, and historical/cultural attractions.
Woodbridge	566	A12, south of the B1079 roundabout	There is only one pedestrian crossing of the A12 on the entire Woodbridge bypass, opposite Russell Close, this is insufficient. Residents would walk / cycle to the retail and associated areas if they could cross the road safely.	Pedestrians have to cross the A12 at the one crossing opposite Russell Close. There needs to be another crossing south of the B1079 roundabout, this will allow pedestrian and cycle access to the retail area and beyond without making an unnecessary detour. This will decrease car use and increase local shopping.	1	0	0	2	0	0	3	Connectivity and Growth – the A12 represents a modest barrier between those situated on either side, but there is – although not as direct – a pedestrian island just north of the roundabout and a pedestrian crossing with traffic lights north of that. Therefore, a score of 1 is considered reasonable. Modal Shift – There is insufficient evidence to suggest any significant modal shift. Optimisation – This does not improve the existing infrastructure. Safety – The suggestion offers a small safety benefit as the A12 (Grove Road) is 40mph busy dual carriageway, but there is already a safe crossing point north of the roundabout which can be used. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion provides limited leisure benefit.
Woodbridge	569	A12 to South side of B1079	There is currently only one pedestrian/cycle crossing across A12 from Woodbridge. A12 is an extremely busy road and impossible for cyclists and pedestrians to otherwise cross. They need to be able to get from Woodbridge, with its sizeable population, to the other side, to access the Garden centre and other shops in the development. Only car users can currently access. The only crossing at present is too far away to be of practical use.	A pedestrian and cycle crossing	1	0	0	2	0	0	3	Connectivity and Growth – the A12 represents a modest barrier between those situated on either side, but there is – although not as direct – a pedestrian island just north of the roundabout and a pedestrian crossing with traffic lights north of that. Therefore, a score of 1 is considered reasonable. Modal Shift – There is insufficient evidence to suggest any significant modal shift. Optimisation – This does not improve the existing infrastructure. Safety – The suggestion offers a small safety benefit as the A12 (Grove Road) is 40mph busy dual carriageway, but there is already a safe crossing point north of the roundabout which can be used. Biodiversity – There are no significant biodiversity impacts. Leisure – The suggestion provides limited leisure benefit.
Woodbridge	579	The path along the river defence	The path is too narrow, people walk either side of the path leaving an often muddy stretch on each side of the path.	Widen the surfaced path. This would improve the experience of walkers. If the path was wider it would become possible for the path to be shared with cyclists	1	3	0	2	-1	3	8	Connectivity and Growth – the proposal would create a new connection between Melton, Woodbridge, and potentially Martlesham, which are large and well-established settlements, however there is unlikely to be significant everyday use due to both settlements having good levels of schools, shops, employment opportunities. Due to where the proposal is situated, it will likely have more leisure benefit than connectivity benefit, however a moderate score of 1 under this scoring category is considered reasonable. Modal Shift – PCT suggests that the B1438 would experience significant modal shift growth should it be improved to the highest standard. It appears to be a strong commuter route between Woodbridge and Melton. The River Deben path, being located parallel to this road, would be a viable alternative route between Melton and Woodbridge. Using PCT, there would be a potentially significant uplift, this warrants

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												the highest score under this category. Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing. Safety – Again, the River Path is an alternative route to using the 'b' type roads and removing cyclists off the B1438 has safety benefits. Despite the B1438 having a 30mph speed limit, it is busy 'b' type road, thus volume and speed of traffic is likely high. Also, Melton Road (B1438) has numerous parked cars which create an obstacle. The proposal does, therefore, warrant a score of 2 under 'safety'. Biodiversity – the Deben River path does not generally have high biodiversity around the path; however, the proposal would likely result in the loss of some grassed areas that grow adjacent the path, hence a score of -1 under biodiversity. Leisure – the proposal has clear significant leisure benefits. Not only does the path connect into Woodbridge town centre, which is a leisure attraction due to the array of shopping, eating establishments, and drinking establishments, but the route in itself will likely be a significant route attraction due to its location along the Deben estuary. A score of 3 is considered therefore, reasonable under this scoring category.
Woodbridge	583	Burkitt Road	It feels unsafe walking on the pavement here between st mary's primary and market hill with little ones. The pavement is narrow in places and the traffic moves very quickly and very close to the kerb. Sometimes cars pull on to the kerb because the road is narrow for 2 cars to pass each other. Apparently there is a 20 mph limit outside the school but it doesn't seem to be marked properly.	Traffic calming measures, clearer marking of / enforcement of 20mph limit	2	0	0	2	0	2	6	The suggestion is to introduce traffic calming measures as to make the road feel safer to utilise as pedestrians. This proposal would also make the route, which has no existing cycling infrastructure, more user-friendly for cyclists. Connectivity and Growth – The proposal would not only connect to a primary school, but Burkitt Road is also a route into Woodbridge town centre. However, as it is unlikely that the road could be made completely traffic free, a score of 2 is considered reasonable. Modal Shift – The proposal will not result in a significant modal shift as it is unlikely that infrastructure can be delivered to the highest standard. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Although Burkitt Road has a 30mph speed limit, it also has numerous parked cars on the southern side of the road which likely forces cyclists into the middle of the road. Traffic calming measures will likely have some safety benefit, therefore a score of 2 is considered reasonable. Biodiversity – No impact on biodiversity. Leisure – Again, Burkitt Road is a main route into Woodbridge town centre, which has significant leisure benefit; however, as it is unlikely infrastructure can be delivered to the highest standard, a score of 2 is considered reasonable.
Woodbridge	585	Junction of Line Kiln Quay Road, St John's Street and Thoroughfare	I use this junction several times a week as I'm cycling home from work. I have to go straight over onto the main bit of the Thoroughfare, so I have to wait on the right-hand side of my lane, which is absolutely terrifying. There is no space for cyclists and the traffic turning from Lime Kiln Quay passes so close to me – it's particularly scary if it's a bus!		0	0	0	2	0	0	2	Connectivity and Growth – the proposed alteration to the junction does not create additional connectivity. Modal Shift – the proposal is unlikely to create a significant modal shift. Optimisation – this does not optimise the existing cycling or walking infrastructure; therefore, the proposal does not score under this category. Safety – the cyclist would remain on the road, however improving the junction for cyclists does warrant for 2 points under 'safety'. Biodiversity – There are no significant biodiversity benefits. Leisure – There appears to be limited leisure benefit.
Woodbridge	586	Theatre Street and Burkitt Road	I either cycle or walk my son to playgroup at St Mary's Primary School before I head off to work. Walking or cycling are both a bit hairy as the traffic often zooms by on this stretch – there's no indication of what the speed limit is so people take that as licence to go as fast as they please – often speeds in excess of 30mph. This is a busy stretch filled with children on the way to Farlingaye and St Mary's – please put up a 20mph sign!	Please put up a 20mph sign!							N/A	Issues relating to speed are a SCC specific matter and have been shared with SCC for their consideration as the Highways Authority.
Woodbridge	595	GR 260 492 just South of A12/Grundidburg roundabout	Dangerous to cross A12 to /from cycle way on west side of A12	A Toucan Crossing. Also resurface & remove foliage from cycle way	1	0	0	2	0	0	3	Connectivity and Growth – The A12 represents a modest barrier between those situated on either side, but there is, although not as direct, a pedestrian island just north of the roundabout and a pedestrian crossing with traffic lights north of that. Therefore, a score of 1 is considered reasonable.Modal Shift – There is insufficient evidence to suggest any significant modal shift.Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing.Safety – The suggestion offers a small safety benefit as the A12 (Grove Road) is 40mph busy dual carriageway, but there is already a safe crossing point north of the roundabout which can be used.Biodiversity – There are no significant biodiversity impacts.Leisure – The suggestion provides limited leisure benefit.
Woodbridge	607	General	The issue is that most if not all the few existing cycle paths are marked poorly. There is no right of way marked for pedestrians or cyclists on the existing paths (ie A12 path or Martlesham to Ipswich). Most byways and other footpaths positively discriminate AGAINST cyclists, with for example, much protest about mostly harmless cycling on the river wall and bars to prevent cycles passing at most town footpath entrances and exits.	Campaigns to promote a cycle 'economy' around new cycle routes, recognising that every cyclist reduces congestion for road users, reduces pollution, increases the mental and physical health of the cyclists themselves, which in turn saves more money for NHS and authorities. Promotion of positive recognition of cyclists who deliberately commute to better their health and lower local pollution, (combatting climate emergency) vs the negative/destructive effect of driving short distances to school and work. School promotion of cycling within a certain distance instead of driving, especially where onward commute to work is not a consideration. Enforce existing traffic legislation designed to promote the safety of cyclists. (ie speed limits, distances for passing cyclists, parking on cycle paths).							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.

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Woodbridge	627	Sandy Lane	This is a key desire line for cyclists between Ipswich - Kesgrave - Woodbridge. There really isn't any other choice for on-road cycling. The A12 is even more dangerous for cycling and so is the "hairpin bend" route going through old Martlesham. But Sandy Lane is unsafe and unattractive to use due to motorists - including van drivers - trying to squeeze past at speed. A particular area of concern is under the railway bridge where the road is narrow and turns sharply and cyclists get squeezed.	Please can Sandy Lane be closed off to motor vehicles part way along this route as motorists have an alternative through route they could use instead of using Sandy Lane. Please can it also be made a 20mph zone which would make it safer for pedestrians/walkers (e.g. it's a leisure route for those walking along the riverside area, walking a circuit). A reduced speed limit would also help those of us who struggle to get back up the hill at the Woodbridge end and of Sandy Lane and sometimes walk with our bikes!	3	1	0	3	0	2	9	Connectivity and Growth – the proposal would create a new connection between Martlesham and Woodbridge, which are large and well-established settlements. As Sandy Lane resides within a key corridor, a score of 3 is considered reasonable. Modal Shift – According to PCT, Sandy Lane is currently well used, and the improvement could score a 3 at the highest standard. However, the route is unlikely to be completely traffic free so the modal shift to the lower standard does not represent as a significant gain. A score of 1 is considered reasonable. Optimisation – Whilst the proposal provides benefits, it does not optimise the existing route. Safety – Sandy Lane is a narrow road with a national speed limit and is likely used as a rat run to bypass the main roads. As the road currently does not have either cycling or walking infrastructure, it is considered that a modal filter will provide safety benefits hence a score of 3. Biodiversity – There are no biodiversity impacts. Leisure – the proposal would connect to the PROW routes which reside along Martlesham creek and the River Deben - as these are particularly attractive routes that extend through the AONB designation, a score of 2 is considered reasonable.
Woodbridge	179a	Riverside path from Broomfield to Woodbridge	This is a single track path suitable only for walkers, and I believe cyclists are not permitted. However over the past year more and more cyclists are using it and it is plainly not suitable for mixed use.	Erect barriers to prevent cyclists	0	0	1	0	0	0	1	Connectivity and Growth - Normally reducing cycling ability will score negatively, however if the path is pedestrian only then it is not reducing the connectivity, but nor is it increasing it. Modal Shift - No significant modal shift growth. Optimisation - If the path is deemed suitable only to cyclists then this would represent a modest optimisation. Safety - This category largely relates to interactions between vehicles and cyclists/walkers and this suggestion does not impact this. Biodiversity - No significant biodiversity benefit. Leisure - Whilst a limited leisure benefit for cyclists is possible overall it is considered generally neutral.
Woodbridge	205a	Hasketon Road/Ransome Road, Woodbridge	Recognising that Farlingaye does not have very good access there is often a conflict between cars & cars and cars & bicycles in this part of Woodbridge, particularly during the morning rush hour / School drop off hour. School hours generally coincide with the morning rush hour creating increased numbers of cars and cycles (young cyclist) in this area of woodbridge including the B1079.	1) Consider some form of dedicated 'cycle' route to/from this area. Allowing children to cycle to school (Woodbridge & Farlingaye) on a car free route.	2	0	0	1	0	0	3	Connectivity and Growth – Farlingaye High School is currently isolated in terms of cycle infrastructure connections; however, the proposal would connect to the school and create a new cycle route within Woodbridge. It is considered therefore, that a score of 2 is reasonable. Modal Shift – According to PCT, the road is poorly used currently, and improvements are unlikely to cause significant modal shift. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Ransom Road does have a 30mph speed limit, however the road is narrow and there are multiple parked cars which means cyclists need to go into the middle of the road. Providing infrastructure for cyclists will, therefore, likely have moderate safety benefit. Biodiversity – the proposal will not have a significant impact on biodiversity. Leisure – No significant impact on leisure.
Woodbridge	205b	Hasketon Road/Ransome Road, Woodbridge	Recognising that Farlingaye does not have very good access there is often a conflict between cars & cars and cars & bicycles in this part of Woodbridge, particularly during the morning rush hour / School drop off hour. School hours generally coincide with the morning rush hour creating increased numbers of cars and cycles (young cyclist) in this area of woodbridge including the B1079.	2) Look at the 'on street parking' around this area, maybe some(or less) more yellow lines.	0	0	0	1	0	0	1	Connectivity and Growth – no significant connectivity and growth benefit. Modal Shift – the removal of the cars off the road does not create new infrastructure and is not considered to create a significant modal shift to warrant a score here. Optimisation – there is no existing cycling or walking infrastructure which this proposal optimises. Safety – the road is narrow along the main access road, or Ransom Road, and the parked cars forces cyclists into the middle of the road creating a modest safety benefit. Biodiversity – No significant biodiversity benefit. Leisure – This road appears to have limited leisure potential.
Woodbridge	205c	Hasketon Road/Ransome Road, Woodbridge	Recognising that Farlingaye does not have very good access there is often a conflict between cars & cars and cars & bicycles in this part of Woodbridge, particularly during the morning rush hour / School drop off hour. School hours generally coincide with the morning rush hour creating increased numbers of cars and cycles (young cyclist) in this area of woodbridge including the B1079.	3) Consider making Hasketon Road and the B1079 roads oneway utilising the A12 roundabouts and a roundabout at the Hasketon/B1079 junction.	2	0	0	1	0	0	3	Connectivity and Growth – Hasketon Road connects to the cycle infrastructure along the A12, Farlingaye High School access road, and to the co-op food store. It is considered, therefore, likely that the proposal would have moderate connectivity benefits. Modal Shift – The road is reasonably quiet, PCT suggests that the proposal would not cause a significant modal shift, therefore it does not score under this category. Optimisation – Whilst it provides benefits, it does not optimise an existing route. Safety – This road has a 30mph speed limit, therefore making it into a one-way road would likely have moderate safety benefits. Biodiversity – There are no biodiversity impacts. Leisure – This road appears to have limited leisure potential.
Worlingham	26	Road between Ceder Drive and Relief Road	Lack of safe walking path between the end of Ceder drive and the roundabout at the end of the relief road. Pedestrians are forced to walk down the neighbouring field to walk safely. This is a major route between a large number of housing (Ceder Drive and Ellough Road and surroundings) and the industrial areas at Ellough. Alternative routes are a significant distance on foot. Waiting for potential works for new housing is not practical as it is likely 10's of years until this happens.	Surfaced path from Ceder drive to roundabout to provide safe walking route along side road. Would complement planned extension of cycleway from relief road to next roundabout	2	1	0	3	-2	0	4	Connectivity and Growth – Connects through to isolated employment uses and would benefit the proposed Garden Neighbourhood. The employment land isn't a key service so 2 points have been given. Also benefits from connecting 2 identified key corridors. Modal Shift – There other routes south onto the new infrastructure and the allocated Garden village may also provide additional connectivity, however Datashine shows no walking to work in this area, as an employment area, albeit isolated some gain could be made here. Overall a score of 1 is deemed reasonable. Optimisation – The proposed improvements are new and do not optimise the existing. Safety – The road is narrow and connects to employment areas so HGV's could be expected. Removing walkers off the road would represent a high potential for safety benefit. Biodiversity – The grass verges would have to be removed and they are currently largely wild meaning in the short term at least there would be a negative biodiversity

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												impact so minus 2 has been given. Leisure – There are limited leisure routes nor does it connect leisure attractions so it scores 0. It should be noted that if an alternative connection is provided through the proposed Garden Neighbourhood this could lower the connectivity and growth and modal shift scores.
Worlingham	196	At the end of The Lowestoft old road which runs from North Cove Church to Marsh Lane Worlingham, the crossing of the A146 is from a sloping blind path onto the exit of the roundabout	The cycle crossing across the A146 slopes down and is blind just as cars come off the roundabout. from the other side of the A146 it is difficult. and vegetation can make it blind. It needs to be made safer for children cycling to the schools in Worlingham and Beccles. it is difficult for walkers to cross as well.	Light controlled crossing.	2	1	1	3	0	0	7	Connectivity and Growth - The crossing point is on a key corridor, however a score is deemed appropriate as it results in an improvement of only a small section of the wider route. Modal Shift - PCT assesses the crossing specifically and it shows reasonable modal shift growth giving a score of 1. Optimisation - The crossing is considered to offer a modest optimisation to the existing routes. Safety - As a relatively busy and fast flowing road the crossing point may provide safety benefit. However it is unclear whether a lighted crossing would be suitable in this location. Biodiversity - No significant biodiversity impact. Leisure - No significant leisure benefit although better access into the countryside is provided.
Worlingham	228	Section of Ellough Road south of Cedar Drive.	No footpath/cycle path.	Provide a footpath/cycle path.	2	1	0	3	-2	0	4	Connectivity and Growth – Connects through to isolated employment uses and would benefit the proposed Garden Neighbourhood. The employment land isn't a key service so 2 points have been given. Also benefits from connecting 2 identified key corridors. Modal Shift – PCT suggests the road is poorly used currently, there other routes south onto the new infrastructure and the allocated Garden village may also provide additional connectivity, however Datashine shows no walking to work in this area, as an employment area, albeit isolated some gain could be made here. Overall a score of 1 is deemed reasonable. Optimisation – The proposed improvements are new and do not optimise the existing. Safety – The road is narrow and NSL, removing cyclists/walkers off this road would have safety benefits scoring it a 3. Biodiversity – The grass verges would have to be removed and they are currently largely wild meaning in the short term at least there would be a negative biodiversity impact so minus 2 has been given. Leisure – There are limited leisure routes nor does it connect leisure attractions so it scores 0. It should be noted that if an alternative connection is provided through the proposed Garden Neighbourhood this could lower the connectivity and growth and modal shift scores.
Worlingham	428	Ellough road from college lane to the Industrial park	No public footpath/ cycle path to link College lane to the Industrial Park. Many pedestrians use the grass verge to walk to work and this is clearly hazardous. There is a partial cycle path linking college lane to the A146roundabout but this needs to extend to the industrial park and also back towards Beccles as far as Cedar Drive	Convert the grass verge to a foot/ cycle path	3	1	0	3	0	0	7	Connectivity and Growth - This cycle/walking path extension lies on a key corridor and provides a full connection from Beccles into the employment zone. Modal Shift - PCT suggests limited growth, however it is based on census data and may not factor the new infrastructure alongside the southern bypass nor the garden village so a score has been provided here. Datashine suggests low pedestrian commuting levels currently. Optimisation - This represents new infrastructure and not an optimisation. Safety - This is a national speed limit road, busy and with a likely high level of HGV traffic, getting cyclists and walkers off the road has a high safety benefit. Biodiversity - This will result in a modest section of well managed grass verge only. Leisure - The connections to employment areas suggests a day-to-day use over a leisure use.
Worlingham	542	Ellough Road between Cedar Drive, Worlingham and Ellough Industrial Estate	There is no direct walking/cycle route between Beccles/Worlingham and the major employment area of the Ellough Industrial Estate. The road is a 60MPH limit with bends and the brow of a hill which obscure vision. It's a significant diversion to avoid this section of road. Pedestrians currently use the verge and adjacent fields which is obviously exceptionally dangerous, especially in the dark. Cyclists suffer close passes as motorists frequently overtake only to encounter oncoming cars.	A shared use cycle and pedestrian path from Cedar Drive to the Industrial estate. Ideally this would continue through the estate, connecting residential areas with individual places of work in this major centre of employment. This path would also link up with the eastern end of the cycle/pedestrian path adjacent to the relief road, creating a traffic free circular recreation route for families/exercise etc.	2	1	0	3	-2	0	4	Connectivity and Growth – Connects through to isolated employment uses and would benefit the proposed Garden Neighbourhood. The employment land isn't a key service so 2 points have been given. Also benefits from connecting 2 identified key corridors. Modal Shift – PCT shows the road is poorly used currently, there other routes south onto the new infrastructure and the allocated Garden village may also provide additional connectivity, however Datashine shows limited walking to work in this area, as an employment area, albeit isolated some gain could be made here. Overall a score of 1 is deemed reasonable. Optimisation – The proposed improvements are new and do not optimise the existing. Safety – The road is narrow and NSL, removing cyclists/walkers off this road would have safety benefits scoring it a 3. Biodiversity – The grass verges would have to be removed and they are currently largely wild meaning in the short term at least there would be a negative biodiversity impact so minus 2 has been given. Leisure – There are limited leisure routes nor does it connect leisure attractions so it scores 0. It should be noted that if an alternative connection is provided through the proposed Garden Neighbourhood this could lower the connectivity and growth and modal shift scores.
Worlingham	665	Ellough Road to Cedar Drive	With respect to the proposed routes, it was considered that urgent consideration be given to new cycles paths from the new bypass along Ellough Road to Cedar Drive.		2	1	0	3	-2	0	4	Connectivity and Growth – Connects through to isolated employment uses and would benefit the proposed Garden Neighbourhood. The employment land isn't a key service so 2 points have been given. Also benefits from connecting 2 identified key corridors. Modal Shift – PCT shows the road is poorly used currently, there other routes south onto the new infrastructure and the allocated Garden village may also provide additional connectivity, however Datashine shows no walking to work in this area, as an employment area, albeit isolated some gain could be made here. Overall a score of 1 is deemed reasonable. Optimisation – The proposed improvements are

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												new and do not optimise the existing. Safety – The road is narrow and NSL, removing cyclists/walkers off this road would have safety benefits scoring it a 3. Biodiversity – The grass verges would have to be removed and they are currently largely wild meaning in the short term at least there would be a negative biodiversity impact so minus 2 has been given. Leisure – There are limited leisure routes nor does it connect leisure attractions so it scores 0. It should be noted that if an alternative connection is provided through the proposed Garden Neighbourhood this could lower the connectivity and growth and modal shift scores.
N/A	53	The old river crossing ,north gate, Beccles	May not be East Suffolk, but there is a disused railway line goes from the old railway river crossing in Beccles, to Gillingham, Geldeston, Ellingham, Bungay. I tried to cycle a small section recently, impossible, very overgrown... But as in Derbyshire, a reclaimed railway line are brilliant for traffic free walking and cycling	Talk to the land owner / set up a charity work party							N/A	This is predominantly in the NCC council area. This has been discussed with NCC
N/A	58	many places	on narrow FOOTPATHS cycles and buggy (go carts) creep up on walkers or ride at speed towards and fail to give warning before speeding up from behind. cyclists along the sea front seem to prefer to ride on the footpath rather than the designated cycle path never dismount at the pier - ride like hooligans on the bascular bridge regardless of pedestrians social distancing is more important now than ever	keep bikes and walkers separate in well defined areas in the last 10 years I have walked 77million steps mainly in the Lowestoft outton broad area footpaths need to be safe for us walkers							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. Providing infrastructure that segregates cyclists and pedestrians is an aim in this strategy.
N/A	75	County wide	The issue for cyclists is a lack of dedicated infrastructure along with having to cycle on fast, dangerous small roads alongside drivers who assume entitlement.	We have a vast network of ancient lanes and byways, many of which are not heavily used by motorised vehicles but do not necessarily join up to go anywhere safely. Some of these lanes could be connected with new sections built to join settlements as needed. Possible rules along these routes: 1. No through traffic 2. A new speed limit of 25mph for all other traffic requiring access. 3. A change in insurance liability similar to the Dutch article 185 of road law along these routes, thus deterring traffic further and encouraging family use. As most of the roads already exist, it could be a cost effective solution with major impact. Such routes, if well planned, may well serve to encourage family cycling holidays, such as are seen in other countries, and if a few campsites or cheap lodgings were encouraged along the way, would likely boost tourism substantially.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. Better use of the PROW system and other existing routes has been considered in the formation of the strategy.
N/A	87	Ipswich to villages (this issue also applies to every town in Suffolk)	There are no safe cycle routes between Ipswich and villages within a 15 miles radius. Where they exist few drivers keep to the 30mph limits and there are far too many stretches with just the National Speed Limit. On relatively narrow roads this leaves cyclists and pedestrians very close to vehicles doing up to 70mph. Safety concerns are a major reason that more people do not cycle or walk.	Create dedicated cycle and pedestrian routes to link villages with Ipswich. Where possible these routes should exclude vehicles except for access or have enforced speed limits. The routes should also have the sort of cycling safety features that Holland has introduced							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	136	New cycle lane barriers	The barriers are an improvement of sorts except that they seem to give drivers the impression that they can drive as close to them as they like! If you have a bike with 2 full panniers, it is difficult to join and exit through the barriers.	Make the cycle lanes wider and improve entrance and exit areas especially near roundabouts. General comment for ALL cycle lanes - STOP any vehicles parking in them!							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	181	Bridleways & Footpaths missing from mapping software	The mapping system does not appear to show 'bridleways' and 'footpaths'. Suffolk has many bridleways which make good offroad routes for walkers and cyclists both for leisure and for local use as connections to local services. The marker is tagging the end of bridleway that connects Gosbeck with Pettaugh as an example, this route is often overgrown and rutted by tractors.	Ensure that all bridleways (RUPP's, BOATs' et al) are maintained to a minimum standard of width and firm surface to enable cyclists and less abled walkers to use them safely.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. The PROW system has been a strong consideration in the formation of the strategy.
N/A	240	Along A1071 between hadleigh road and A1214	No cycle route provided along this way for cyclists coming from south of Ipswich and needing to get to hadleigh road.	With new estate being built a route through could be planned there is an existing foot path across fields that could be upgraded or an extra lane on either side of the existing A1071							N/A	Not within the East Suffolk area and has been given to the appropriate council.
N/A	241	Underpass under the A14	Lack of cycling access through to Sproughton meaning cyclists either have to go to central Ipswich or the very busy Sproughton high street if attempting to get to the Sproughton road/Morrisons areas of Ipswich	The current underpass be redesignated as having cycling access, and the steps on the hadleigh road side replaced with a ramp which will help cyclists, pedestrians with pushchairs/trolleys and those with walking difficulties							N/A	Not within the East Suffolk area and has been given to the appropriate council.
N/A	292	All Schools.	If we want to increase safe cycle usage it should start with young people so that it becomes absolutely normal to cycle, and especially to school.	I suggest that all schools have a cycling policy produced by stake holders eg teachers, parents, students, police, local council, etc. The policy would include among other things:							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. The location of schools and improving their accessibility has been a consideration in the formation of the strategy.

Parish	Reference	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	Connectivity and Growth	Modal Shift	Optimisation	Safety	Biodiversity	Leisure	Total	Scoring Comments
				Suggested safe routes to school from all the main centres of population that feed into the school. And perhaps roads that should be avoided as unsafe for cyclists to use. The council should consider providing suitable signage for cyclists and other users along the routes. Safe dry cycle storage within the school. Safe storage of helmets, hi-vis clothing. Cycling competency certification schemes.								
N/A	325	Widen and improve the current footpath to make it a shared pedestrian and cycleway.	Cyclists are currently sharing a dual carriageway with fast moving traffic.	If the path was widened to make a shared footpath/cycleway, it would to separate cycles from traffic using the dual carriageway. This would be especially effective where slow moving cyclists are riding up the hill from Ipswich to Copdock.							N/A	Not within the East Suffolk Area and has been given to the appropriate council.
N/A	335	Cycle paths in Ipswich	There is a lack of clarity in Ipswich as to where cycle paths begin and end and which footpaths are shared space.	Paint all cycle tracks to increase visibility for pedestrians and cyclists							N/A	This issue is more of a maintenance matter and will be passed the relevant authority. Ipswich falls outside the boundary of East Suffolk.
N/A	336	The junction between the Market Place and Bridges Street and the contraflow cycle lane.	The junction going uphill is rather dangerous because cyclists must give way to unpredictable traffic. The turn from the market place makes larger cars/vans/lorries swing into the cycle lane round a blind corner. The 20 mph speed limit in Bridge Street is frequently ignored. Cars and vans park in the cycle lane, pushing cyclists into the path of oncoming traffic. Bridge Street is a rat run for traffic going to Norwich. The noise levels and vibration are unacceptable.	"No Entry (except cycles)" at the Market Place/Bridge Street junction, preferably with a planter partially blocking the access for vehicles. "Access to Bridge Street via Nethergate Street", enabling deliveries and residents access while quietening the road. Widening the pavements, initially with paint and identified loading bays to enable street life to take place safely.	0	0	1	2	0	0	3	This would need further exploration with the Highway Department to ensure access to Bridge Street via Nethergate Street represented an improvement for cyclists and Walker safety. Connectivity and Growth - The connections already exist and these improvements do not represent a significant improvement to connectivity. Modal Shift - Whilst PCT suggests some modal shift is possible along Bridge Road, the south western junction only represents a small part of the overall road so it doesn't score here. Optimisation - As a contraflow cycle lane already exists this suggestion will present a modest optimisation. Safety - As a relatively slow moving area for traffic the safety would normally mean that this suggestion scores no higher than 1, but recognising that larger vehicles turning in could represent additional pressure a score of 2 has been given. Biodiversity - There are no biodiversity impacts. Leisure - Whilst it represents a modest improvement for access into the town centre with its associated leisure benefits, it is not deemed a significant improvement given the contraflow cycle lane already exists.
N/A	348	Ribbons Park Development, Ipswich	Exemplar & Award winning example of a new housing development with a Modeshift STARS "Residential Travel Plan" https://www.modeshiftstars.org/first-residential-development-achieves-national-stars-accreditation/	This requirement should be included with all new housing developments within Suffolk.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	354	Riverside Beccles	The path becomes very muddy in autumn and winter. It would be excellent if path could be maintained ie adding grit or building a broadwalk. This would encourage many more people to use the path.	Add grit or build broadwalk							N/A	Not within the East Suffolk Area and has been given to the appropriate council.
N/A	357	All over Suffolk	Your footpath signs are rubbish, they keep falling over and have to be reported and a worker brought out to stand them up again. Change to metal? Sit them inside some kind of flange plate with soil on top. Label with the footpath number. Could even have suggestions where they lead to! Look at Kent system.	As above							0	The comments raised have been considered in the formation of the strategy, however they are broad or generalised so cannot be scored under the MCAF system.
N/A	358	All over Suffolk	Stiles	Get rid of them and have metal kissing gates that the less able and dogs can use.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	427	Felixstowe Road, Martlesham.	Ref 145 already reported	Totally agree with comments. Priority for Cyclists route is now dangerous due to volume of traffic. Needs to be one way with cycle lanes each side. Part of National Cycle Route 1 so should be a high priority.							N/A	Considered under previous response.
N/A	494	This is a general comment, Sport England, as a non-statutory consultee, supports the development of this strategy, which will improve opportunities for physical activity, in line with Sport England's Active Design principles.	n/a	n/a							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	548	Grange Farm Avenue, close to junction with Brackley Close	There is a traffic-calming measure here which requires east bound traffic to give way to westbound traffic. However some motorists often do not respect oncoming cyclists when the cyclist has right of way and this has clear potential to cause a head-on collision. (There is a sign that reads "think bike" however it faces traffic that DOES have right of way so I am not sure what its purpose is). There is a similar issue with Mill Lane at the point where the bridge crosses the railway line.	Either - reverse the sign that reads "think bike" so that it faces traffic that is required to give way; Or, preferably, remove the aforementioned sign and replace with a sign that more specifically reminds traffic that they need to give way to cyclists when the cyclist has right of way.	0	0	0	1	0	0	1	The comment has been assessed for new signage Connectivity and Growth - The additional of signage has limited connectivity benefit. Modal Shift - The addition of signage is unlikely to create significant modal shift. Optimisation - The signage is to benefit on-road cycling and has limited optimisation benefit to existing cycle infrastructure. Safety - The improved/additional signage has modest safety benefit. Biodiversity - There are no biodiversity impact. Leisure - There are no significant leisure benefit.
N/A	574	Southwold to Felixstowe via Woodbridge	Following a good deal of British success at the elite level and a general desire to improve mental and physical health, cycling has become an increasingly	There is enough open space to build a cycle path from Lowestoft to Felixstowe via Woodbridge. This could be done quickly and at modest expense.							N/A	Improved and cohesive connections across the district is a key ambition, but the comment so too broad to score under the MCAF system.

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			popular activity, whether it be commuting or for leisure. However, poorly lit roads and busy traffic prevent it becoming more commonplace with people still opting for four wheels rather than two.									
N/A	580	General comment about public footpaths	Officially public footpaths are not for use by cyclists. A lot could probably be opened up to cyclists and would provide safe off-road routes.	Open suitable public footpaths to cyclists							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. The PROW system has been a strong consideration in the formation of the strategy and where specific paths would benefit from upgrades to bridleways these have been proposed.
N/A	581	Speed of cars on country lanes endangers cyclists and pedestrians	cars travel too fast on country lanes and endanger cyclists and pedestrians	For many country lanes (especially single track lanes) a realistic speed limit would be 30mph. The speed limit on country lanes should be reduced to 30mph. It would probably have a minimal effect on journey times along the country lanes for cars. It would also improve villages if the speed limit within the settlement boundary is 20mph. This would also reduce CO2 emissions etc.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. If there are specific concerns regarding vehicular speed this would need to be raised with SCC.
N/A	588	Sandy Lane, Woodbridge, Ipswich Rd junction to railway bridge	Sandy Lane is a dangerous place to walk because there is no escape from speeding traffic! The stretch from Broomheath Rd to the Railway Bridge (part of Circular River Walk) is especially dangerous. The narrowness and blind bends make it unsafe.	Sandy Lane needs a footpath! And a 20mph speed limit.	3	1	0	3	-3	2	6	Connectivity and Growth – The proposal would create a new connection between Martlesham and Woodbridge, which are large and well-established settlements. As Sandy Lane resides within a key corridor, a score of 3 is considered reasonable. Modal Shift – As the proposal connects Martlesham and Woodbridge, which currently does not have a safe and direct pedestrian connection, the implementation of such will likely result in a modest modal shift. A score of 1 is considered reasonable. Optimisation – The proposal is for new infrastructure and does not therefore, optimise the existing. Safety – Sandy Lane is a narrow road with a national speed limit and is likely used as a rat run to bypass the main roads. Therefore, getting pedestrians off road will have safety benefit so a score of 3 is considered reasonable. Biodiversity – The road is narrow so, in order to implement the proposed infrastructure, the removal of the established hedgerows and wild verges located along both sides of Sandy Lane is likely needed. Leisure – The proposal would connect to the PROW routes which reside along Martlesham creek and the River Deben – as these are particularly attractive routes that extend through the AONB designation, a score of 2 is considered reasonable.
N/A	608	General	Nearly all cycle paths stop abruptly at some point with direction onto a busy road with poor direction and often no further option but to stay on the road. Even the poorly marked cycle paths on main roads are usually blocked at some point by parked cars. Hurried commuters often have little time for slower cyclists who are often viewed as a non-road-fee-paying nuisance. Walkers are well catered for in most areas but can view the bike as an unwelcome nuisance also.	Support this campaign by creating and investing in a considered and continuous infrastructure of cycle paths and facilities, such as marking paths with cycle and pedestrian areas, widening existing paths, traffic reduction schemes citing the reason for promoting cycling. (such as the welcome sign for the Thoroughfare which says 'except cycles'). These paths should connect outlying villages as well as provide cross-town routes, cycling off road wherever possible. (ie routes from Bromeswell to Woodbridge using part of the river wall, which is wide enough to accommodate cycles and pedestrians. Rendlesham to Woodbridge, Bredfield to Woodbridge, Hasketon to Woodbridge, and so on).							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	639	Whole of Lowestoft	Cycle routes are good in the town of Lowestoft (compared to other UK cities/towns but NOT when compared with most of Europe). However there is NO WAY of getting OUT of LOWESTOFT to other places: Yarmouth unsafe, Southwold unsafe/non-existent; Beccles not great; Hadiscoe very unsafe and non-existent. How do people work in other places and commute by bike (or even public transport)?	You need to work with Highways and Norfolk. There is just no investment to go from place to another. Why not work with Sustrans? (who have pulled out of Suffolk because not enough funding). More strategic thinking about cycling as a mode of transport NOT just a Sunday jolly.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. The strategy has created key corridors highlighting important connections to allow for better commuting.
N/A	641	Cycle paths and footpaths throughout East Suffolk	Concerns about the surface and width of footpaths and cycle paths.	Cycle paths and foot paths should be at least 2 metres wide to allow for two wheelchairs to pass. The surface should be tarmac so that all people can walk / use wheelchairs easily. They should be reasonably level, with no hills or steps, or gates. They should be regularly maintained.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	663	N/A	Beccles Town Council, noting that as Suffolk County Council also have a cycling and walking strategy, the ESC cycling and walking strategy should not duplicate this and that the two strategies should link together, particularly as Suffolk County Council are responsible for the highways and transportation infrastructure. The linking of both strategies is also important to ensure that all comments received by the separate strategies, are duly considered when the overall strategy is reviewed.								N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	670	East Suffolk	We are of the opinion that in a period of scarce resources we consider that the three priorities for walking should be as follows: 1. Improve existing PROWs by maintaining gates, stiles, finger posts and signage and clarify who can and cannot access PROWs. 2. Ensure land owners co-operate with this	We note that there are already a number of cycling routes supported by ESC and SCC and these should also be given greater publicity. Greater use of cycling is a much more complex issue that requires a high level strategic approach across all relevant councils. As a small parish we can support and publicise these routes as required.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.

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			maintenance and engage with SCC highways on how to improve condition of PROWs on their land. 3. Ensure Town and Parish councils appoint PROW officer and make sure public are aware who to contact.									
N/A	676	East Suffolk	In seeking to improve the cycling and walking experience in East Suffolk the safety of each is paramount - from separating them from motorised transport to ensuring that mountain and trial bikes do not despoil the environment by increasing erosion.	The most essential aspect for me, from a cyclists' viewpoint, has a to be ensuring that no parking is allowed in any cycle lane; it's crazy and euphemistic! Any mitigation, by way of educating and persuading car users to reduce their dependence upon the motor car, would be welcome, even to the extent of escalating car parking charges, perhaps?							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	679	N/A	See attached.	See attached.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	680	East Suffolk	See attached.	See attached.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	718	East Suffolk	See attached.	See attached.							N/A	This response provides general points from their experience for our consideration and not a specific issue to be scored. Their interest in an Orford to Woodbridge route was submitted under comment 720 so its score can be found there. The Aldeburgh/Snape/Saxmundham area is also part of a key corridor.
N/A	763	Cycling - general comments	See attached document. Points 2 to 5 are plotted on the map in the relevant area which relates to the matter.								N/A	The individual points have been plotted to be considered separately.
N/A	773	B1077 near Westerfield Railway Station	Parked vehicles near the level crossing are a hazard to cyclists and pedestrians.	A solution is to work with Greater Anglia (re current usage) and Ipswich Borough Council as part of the Ipswich Garden suburb to provide facilities for off road parking.							N/A	Providing new parking areas are outside the remit of the project.
N/A	776	East Suffolk	More and more cyclists are riding on footpaths and some are very arrogant and dangerous with it. Can we PLEASE have signs saying that these are FOOT PATHS and therefore cycling is forbidden.								N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	780	East Suffolk	CUK's position is that priority should be to consider whether the road environment can be made comfortable for cycling and that sharing with pedestrians should be the last resort. The latest guidance from the Department for Transport is in agreement stating improved facilities for pedestrians and cyclists should be separated and road-narrowing to enable correct width cycle lanes should be considered which is in effect saying making roads comfortable for cycling should be the first consideration.	Considering the nature of many of Lowestoft's busier roads, I understand why on-road facilities would be difficult. I hope there will be proper consultation (CUK would probably accept off-road facilities are more appropriate anyway). Many cyclists will say they want more cycle paths and they don't mind sharing with pedestrians as anything is better than being on road. It is impossible for there to be off-road facilities everywhere. The more cyclists on the roads the safer on-road cycling is, especially if there are 20mph limits. Routes need to be as direct as possible, perhaps even giving cycling time-saving, advantages over driving. Many off-road routes involve time-consuming waits at toucan crossings etc. There are pedestrians who dislike sharing with cyclists, so even considerate riders on shared facilities experience hostility. Having to slow for pedestrians, and possibly dismount and walk, works against cycling being quicker than driving for short journeys.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	788	East Suffolk	20 mph speed limits just outside schools do not encourage more cycling of the school run.	Where there are not off-road facilities on popular school routes, often along residential roads, there need to be 20 mph limits. They have been proved to work.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	789	See attached.	See attached.	See attached.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	790	See attached.	See attached.	See attached.							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. This response provides general points in regards to equestrian users of roads and PROW for our consideration and not a specific issue to be scored.
N/A	791	East Suffolk	See below.	I would like to ask that when compiling your cycling and walking strategy, you also take into account the wishes and needs of horse riders, for the following reasons: <ul style="list-style-type: none"> • Horse riding is also a healthy form of outdoor exercise • Horse riders share rights of way (bridlepaths and byways) with cyclists, and their needs may be different. For example putting down a hard surface to make a right of way better for cyclists would be detrimental if not dangerous for horse riders • Horse riding contributes significantly to the local economy, such as riding schools, livery yards, farriers, vets, feed merchants, tack shops etc • Horses have to be kept all year round, we don't just put them in a shed for the winter and get them out again when the weather improves! 							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.

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				Local horse riding organisations, and the British Horse Society, should be consulted for their views on any proposed changes to bridlepaths and byways.									
N/A	792	East Suffolk	Good Issues- Cheap- Good lights & clothing- Short journeys don't take longer than car- Healthy- Fun in good weather- Reduced oil consumption- Panniers & back packs assist shoppingBad Issues- Punctures- Hills and inclines (e-bikes help!)- Bad weather- Aggressive driving- Most local roads have 60mph limit- Hard verges and kerbs reduce vehicle options when being overtaken or vehicle approaching from opposite direction (cars rarely wait for cyclists, agricultural vehicles NEVER do)	I suggest that we start by looking at short journeys of 5 miles or fewer. This could include travelling to work or school and daily and intra weekly shopping trips for most people in East Suffolk. To provide encouragement, the following notes may help: - Direct cyclists to cycle-friendly routes? - Can we provide shopping discounts for people who arrive by cycle or walk? - Encourage more frequent shopping trips for lighter, smaller loads - Shop close to home – you're saving on fuel to compensate for any higher prices - Have we got enough cycle racks? - Are they fit for purpose and in a suitable place? Cycle routes don't require lots of infrastructure, but the following help greatly: - Appropriate signage at each end of the route - Preferably NOT along 60 mph roads - Quiet lanes are perfect - Soft level verges (assuming single carriageway roads) - Well-maintained road surfaces - No hedge cutting using flails!!!							N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.	
N/A	794	East Suffolk	As a result of the number of consultations we are currently receiving, we regret that we are unable to comment specifically at this time.									N/A	No comments have been submitted in which to score under the MCAF system.
N/A	796	East Suffolk	Natural England has no comments to make at this time. However, we will be happy to comment on future forward planning consultations which come forward.									N/A	No comments have been submitted in which to score under the MCAF system.
N/A	798	East Suffolk	The County Council supports the underlying sustainable aims and objectives of the emerging Strategy and would suggest that engagement is made with neighbouring authorities in Norfolk (i.e. Great Yarmouth Borough Council and Norfolk County Council) to ensure that the maximum benefits can be made through cross-boundary working in respect of cycling and walking routes to Norfolk Settlements.									N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	799	East Suffolk	1. despite recent resurfacing work there are many pot holes on back lanes 2. again on many lanes there is a build up of sand, gravel or tree debris 3. in autumn when farmers cut hedges the tractor powered methods strew the road with sharp fragments which create a very significant risk of punctures not only to bikes but also cars 4. signposts, so valuable to anyone not relying on sat-nav, are increasingly corroded through and lying in the verge	1. Cyclists are aware of road condition and promoting the existing SCC online reporting tool amongst them would reduce the need for staff to carry out road surveys. 2. Reduce verge cutting, which is prejudicial to wildlife, spend it on sweeping roads free of sand and flints which are a particular problem with the local geology. 3. Anyone strewing a road with tacks would soon be subject to enforcement action so it seems strange that there are no moves to deal with the hacking of hedges with no regard to the state the road is left in. 4. Signposts are in a poor state. If there is insufficient money to replace, an imaginative solution needs to be found. Perhaps a plastic insert to reconnect the tubular uprights on an interim basis? 5. A new, imaginative look at our roads needs to be promoted rather than just doing, or not doing, what always has been. While much of the direct responsibility for remedial work lies with higher tiers, is E.S.C. supportive of the objectives?								N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system.
N/A	800	Walberswick	The Council strongly supports developing a cycling and walking strategy. We support putting in additional cycling and walking routes and increasing the level of maintenance that ESC and SCC spend on maintaining routes. Walberswick Parish Council has already objected to Sizewell C including that its construction period will make it impossible to cycle on the roads in and around the area as huge increases in traffic, HGVs and rat running will make roads busy and dangerous for cyclists and walkers.	Should Sizewell C go ahead, ESC should address this particular issue in the Cycling and Walking Strategy along with the ongoing work in the rest of the District.								N/A	The comments raised have been considered in the formation of the strategy, however as they relate to more broad or generalised concerns they have not been scored under the MCAF system. The area around sizewell has been considered as part of the key corridors.

Appendix 2 – Cycling and Walking Strategy Infrastructure Delivery Framework

The implementation of the Strategy’s recommendations is key to ensuring that local communities have access to high quality cycle and walking infrastructure. There are many different ways to deliver and fund the Strategy’s recommendations, and this Infrastructure Delivery Framework (IDF) outlines which delivery and funding methods could be most appropriate for specific recommendations.

This IDF is heavily related to and must be read in conjunction with the Suffolk Coastal Local Plan IDF , Waveney Local Plan IDF , and the East Suffolk Infrastructure Funding Statement (IFS) . The IDFs appended to the two Local Plans identify and prioritise infrastructure needed to support the planned development set out in the Local Plans. This C&WS IDF builds on the infrastructure needs set out in the Local Plans’ IDFs in respect of cycling and walking infrastructure. The IFS reports on the developer contributions (Community Infrastructure Levy and s106) that ESC has received, spent and plans to spend on infrastructure in support of planned development . This C&WS IDF provides up to date estimated costs for cycling and walking infrastructure as set out in the Strategy’s recommendations, which can then be used as evidence for the annual review of the IFS. Taken together the IDFs and IFS provide a transparent suite of documents that help local communities and developers understand the infrastructure required to support planned development, how developer contributions have been spent and what future funds will be spent on.

The priorities identified for each infrastructure requirement set out in the Local Plans’ IDFs are defined as follows

- Critical – Infrastructure needed to unlock development sites (i.e. development cannot take place until this project is delivered)
- Essential – Infrastructure necessary to support development and mitigate impacts. Without this the developments’ sustainability would be undermined
- Desirable – Infrastructure that could support development and make it more sustainable, but development would be sustainable without it

As the C&WS identifies cycling and walking infrastructure recommendations across East Suffolk and is not confined to the identification of infrastructure that would support planned development, a new priority category (beneficial) has been identified and is defined as follows

- Beneficial – Infrastructure that would not support planned development but would improve the sustainability of existing and future communities.

It should be noted that simply because a recommendation is identified as ‘beneficial’, and therefore would not be specifically needed to support or facilitate planned development across East Suffolk, it does not mean the infrastructure improvement is unimportant.

In addition to the priority categories identified above, other forms of prioritisation have already been included within the Strategy such as the Key Corridors that have been attributed priority categories that provide a separate yet no less valuable function. The Key Corridors operate as networks of cycling and walking infrastructure and while each recommendation has its own benefits, there will inevitably be situations where a number of recommendations provide similar benefits to the overarching cycling and walking network. In order to differentiate between key corridor recommendations and highlight their relative importance, each recommendation has been ranked as either medium, high or very high priority. In determining the priority of a particular recommendation the following principles were considered

- The importance of the recommendation within the context of the wider Key Corridor,
- Whether the recommendation is an alternative to a more important recommendation, and
- The likely potential for delivery.

This IDF incorporates all of the Strategy’s recommendations except the Leisure Routes. The large scale and ambitious Leisure Routes identified in the Strategy could be brought forward through a number of means and designed in a number of ways, which makes it challenging to identify the specific infrastructure needed for each Leisure Route and therefore the costs of such infrastructure are highly uncertain. However, the implementation of Leisure Routes, as well as all other Strategy recommendations, will be monitored and further prioritised through the ongoing Prioritisation Methodology, about which more information can be found here.

The following table provides an explanation for each of the columns that form the Strategy’s IDF.

C&WS Recommendation	C&WS Priority	Approximate Cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
Refers to the specific C&WS recommendation.	Outlines whether the recommendation forms part of a key corridor and if so, the priority category (medium, high, or very high) attributed to the key corridor recommendation.	<p>Gives a broad cost estimate. The cost is an estimate only and should not be viewed as a complete or detailed costing. A standard cost was applied per linear metre. Please note the costs only factor the construction costs.</p> <p>Where the current standard cost does not apply the estimated cost is given as 'Unknown'</p>	<p>Outlines the organisations which may provide funding opportunities. It is important to note where a public sector organisation is referenced it relates to that organisation's ability to potentially access a funding pot and does not refer to the organisation's capital budgets.</p> <p>Abbreviations:- ESC - East Suffolk Council SCC - Suffolk County Council DFT - Department for Transport CIL - Community Infrastructure Levy NH - National Highways TC/PC - Town Council/Parish Council</p>	<p>Outlines specific funding mechanism/s that could be used should they be known.</p> <p>Abbreviations:- SCC - Suffolk County Council CIL - Community Infrastructure Levy DFT - Department for Transport S106 - Section 106 S278 - Section 278 NH - National Highways TC/PC - Town Council/Parish Council</p>	Shows the relationship between recommendations identified in the Strategy and projects detailed in the Suffolk Coastal and Waveney Local Plans' Infrastructure Delivery Frameworks, if a correlation between the two frameworks exists.	Shows the priority (critical, essential, desirable) of projects as identified in the Suffolk Coastal and Waveney Local Plans' Infrastructure Delivery Frameworks as well as the new priority category (beneficial) identified for recommendations that do not relate to planned development.

Key Corridors

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
IM1	PROW59/66	Very High	£250,000-£300,000	SCC	SCC	N/A	Beneficial
IM2	Woodbridge Road	High	£650,000-£700,000	SCC	SCC	N/A	Beneficial
IM3	PROW57	High	£150,000-£200,000	SCC	SCC	N/A	Beneficial
IM4	Long Strops Bridleway	Very High	£750,000-£800,000	SCC	SCC	N/A	Beneficial
IM5	Main Road	High	£1,900,000-£2,000,000	SCC, CIL	SCC, CIL	N/A	Essential
IM7	Grange Farm cycleway	High	£2,000,000-£2,100,000	SCC, DFT, CIL	SCC, DFT, CIL	N/A	Essential
IM8	Main Road	High	£900,000-£950,000	SCC, CIL	SCC, CIL	N/A	Essential
IM9	Suffolk Police HQ	High	£150,000-£200,000	Developer, SCC, CIL	S106, S278, CIL, SCC	Access, junction, cycle and footway improvements at Suffolk Police HQ	Essential/Critical
IM10	Dobbs Lane / Felixstowe Road	Very High	£850,000-£900,000	SCC, DFT, CIL	SCC, DFT, CIL	N/A	Essential
IM11	Eagle Way / Betts Avenue	Very High	£700,000-£750,000	SCC, DFT	SCC, DFT	N/A	Beneficial
IM12	Martlesham Woods / Brightwell Lakes	Medium	£1,400,000-£1,500,000	SCC, DFT, CIL	SCC, DFT, CIL	N/A	Essential
IM13	Barrack Square/Gloster Road	Very High	£300,000-£350,000	SCC, DFT	SCC, DFT	N/A	Beneficial
IM14	Footpaths/Felixstowe Road	Very High	£150,000-£200,000	SCC	SCC	N/A	Beneficial
IM15	Main Road/Woodbridge Town Football Club site	Very High	£300,000-£350,000	SCC	SCC	N/A	Beneficial
IM16	Sandy Lane	Very High	Unknown	SCC	SCC	N/A	Beneficial
IM17	Outer Woodbridge route	High	£3,500,000-£4,000,000	SCC, DFT, Developer, CIL	S106, S278, CIL, SCC, DFT	Access and junction improvements at Land at Woodbridge Town Football Club	Essential/Critical
IM18	Ipswich Road - Woodbridge Train Station	Very High	£750,000-£800,000	SCC, DFT	SCC, DFT	N/A	Beneficial
IM20	Thoroughfare/Melton Road	High	Unknown	SCC, Developer, CIL	S106, S278, CIL, SCC	N/A	Desirable/Essential
IM22	Bredfield Road/Melton Road	High	£1,000,000-£1,100,000	SCC	SCC	N/A	Beneficial
IM23	Melton Train Station/Melton Park	High	£350,000-£400,000	SCC, CIL	SCC, CIL	N/A	Essential
IM25	A12/Bridleway 31/18/32/51	Medium	£1,800,000-£1,900,000	SCC, DFT	SCC, DFT	N/A	Beneficial
IM26	Brightwell/Bucklesham	High	£1,000,000-£1,100,000	SCC	SCC	N/A	Beneficial
IM27	Portal Avenue - Eagle Way	Very High	£350,000-£400,000	Developer, SCC, CIL	S106, S278, CIL, SCC	Access, junction, cycle and footway improvements at Suffolk Police HQ	Essential/Critical
IM28	Felixstowe Road	Very High	Unknown	SCC, DFT, CIL	SCC, DFT, CIL	N/A	Essential
IM29	Peterhouse Crescent-Ipswich Road	High	£500,000-£550,000	SCC	SCC	N/A	Beneficial
IM30	Sutton Hoo	High	Unknown	SCC, NE	SCC, NE	N/A	Beneficial
IM31	Brightwell Lakes	Very High	Unknown	Developer, SCC, CIL	S106, S278, CIL, SCC, DFT	N/A	Essential
IM32	Long Strops - Brightwell Lakes	Very High	Unknown	Developer, SCC, CIL	S106, S278, CIL, SCC, DFT	N/A	Essential
IF1	Felixstowe Road 'west' (A1156) existing shared path, Warren Heath	High	£300,000-£350,000	SCC, NH, Developer, CIL	SCC, NH, CIL	Significant access improvements and improvements to the wider Land at Felixstowe Road (Policy SCLP12.20)	Critical
IF2	Ransomes Way (A1189)/Felixstowe Road 'west' (A1156) roundabout	Very High	£350,000-£400,000	SCC, NH, Developer, CIL	SCC, NH, CIL	Significant access improvements and improvements to the wider Land at Felixstowe Road (Policy SCLP12.20)	Critical

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
IF3	The section of Felixstowe Road (A1156) 'west' between Ransomes Way (A1189)/Felixstowe Road 'west' roundabout and the Trinity Park roundabout	Very High	£800,000-£850,000	SCC, NH, Developer, CIL	SCC, NH, CIL	Significant access improvements and improvements to the wider Land at Felixstowe Road (Policy SCLP12.20)	Critical
IF4	Trinity Park roundabout (southern side)	High	£50,000-£100,000	SCC, NH,	SCC, NH, CIL	Significant access	Critical
IF5	Murrills Road	Medium	£700,000-£750,000	SCC, CIL	SCC, CIL	Significant access	Desirable/Essential
IF36	Trinity Park roundabout (northern arms)	Very High	£50,000-£100,000	SCC	SCC	N/A	Beneficial
IF7	Felixstowe Road 'west' - Trinity Park to A14 bridge	Very High	£2,000,000-£2,500,000	Developer, CIL	S106, S278, CIL, SCC, DfT	Footway improvements at Ransomes, Nacton Heath (Policy SCLP12.21)	Essential
IF10	Straight Road	Medium	£200,000-£250,000	SCC, NH,	SCC, NH, CIL	Significant access	Critical
IF11	Felixstowe Road 'west' A14 bridge to Felixstowe Road 'east'	Very High	£400,000-£450,000	SCC, NH, Developer, CIL	SCC, NH, CIL	Significant access improvements and improvements to the wider Land at Felixstowe Road (Policy SCLP12.20) and Footway improvements at Ransomes, Nacton Heath (Policy SCLP12.21)	Critical
IF13	Felixstowe Road 'east'	Very High	£1,800,000-£1,900,000	Developer, SCC, NH, CIL	S106, S278, CIL, SCC, DfT	Significant access improvements and improvements to the wider land at Felixstowe Road (Policy SCLP12.21)	Critical
IF14	Levington Lane to Main Road, Bucklesham	Medium	Unknown	SCC, CIL	SCC, DFT, CIL	Significant access improvements and improvements to the wider Land at Felixstowe Road (Policy SCLP12.20)	Desirable/Essential
IF18	Morston Hall Road	Very High	£1,000,000-£1,100,000	SCC	SCC, DFT	N/A	Beneficial
IF20	High Road, Trimley St Martin	Very High	£200,000-£250,000	SCC	SCC	N/A	Beneficial
IF21	High Road, Trimley St Martin	Very High	£100,000-£150,000	SCC	SCC, DFT	N/A	Beneficial
IF22	Field edge, north of Trimley St Martin	Very High	£550,000-£600,000	SCC	SCC	N/A	Beneficial
F1	High Road (Section 1)	Very High	£1,300,000-£1,400,000	SCC, NH, Developer, CIL	SCC, NH, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F2	High Road via Trimley St Mary Primary School	High	£200,000-£250,000	SCC	SCC	N/A	Beneficial
F4	Walton High Street/High Road West	Very High	£250,000-£300,000	SCC, CIL	SCC, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F5	High Road West	Very High	Unknown	SCC, Developers, CIL, TC	S106, S278, CIL, SCC, DfT	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F6	High Road East	Very High	£500,000-£550,000	SCC, CIL	SCC, CIL	Pedestrian and cycle Enhancements at Land at Brackenbury Sports Centre and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F7	Cliff Road	Very High	£600,000-£650,000	SCC, CIL	SCC, CIL	Pedestrian and cycle Enhancements at Land at Brackenbury Sports Centre and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F9	Trimley Marshes Nature Reserve circular route	Medium	£1,700,000-£1,800,000	SCC, AONB, DFT	SCC, AONB, DFT	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F10	Grimston Lane to bridleway bridge via Footpath 32	Very High	£200,000-£250,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F11	Land between PROW 32 and the Land Adjacent to Reeve Lodge site's western boundary/Footpath 31	Very High	£100,000-£150,000	SCC, CIL	SCC, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F13	Land adjacent to Reeve Lodge, High Road, Trimley St Martin - Primary route	Medium	£100,000-£150,000	Developer, SCC, CIL	S106, S278, CIL, SCC, DfT	N/A	Essential
F14	Gun Lane to bridleway bridge	Very High	Unknown	Developer, SCC, CIL	S106, S278, CIL, SCC, DfT	N/A	Essential
F137	SCLP12.65 Land adjacent to Reeve Lodge, High Road, Trimley St Martin - Connection to Gun Lane	Very High	Unknown	Developer, SCC, CIL	S106, S278, CIL, SCC, DfT	N/A	Essential
F15	South-western most point of Grimston Lane	Very High	Unknown	SCC	SCC	N/A	Beneficial
F16	The Howlett Way/High Road/Land Adjacent to Reeve Lodge access road roundabout	Very High	Unknown	SCC, CIL	SCC, CIL	Access, footway and cycle connectivity improvements at Land off Howlett Way	Essential/Critical
F17	Howlett Way	Very High	£250,000-£300,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, footway and cycle connectivity improvements at Land off Howlett Way	Essential/Critical
F18	Footpath 4	Medium	£50,000-£100,000	Developer, CIL	S106, S278, CIL	N/A	Essential/Critical
F22	Bridleway 5 (east to west section)	Very High	£100,000-£150,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, footway and cycle connectivity improvements at Land off Howlett Way	Essential/Critical
F24	PROW Footpath 26 (southern half)	High	£50,000-£100,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, footway and cycle connectivity improvements at Land off Howlett Way	Essential/Critical

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F25	Bridleway 5 (north to south section)	High	£50,000-£100,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, footway and cycle connectivity improvements at Land off Howlett Way	Essential/Critical
F26	Thurmans Lane	Very High	Unknown	SCC, NH	SCC, NH	N/A	Beneficial
F27	Thurmans Lane green space	High	£50,000-£100,000	SCC	SCC	N/A	Beneficial
F28	Faulkeners Way	High	£50,000-£100,000	SCC	SCC	N/A	Beneficial
F29	Faulkeners Way	Medium	£750,000-£800,000	SCC	SCC	N/A	Beneficial
F30	PROW 9	Very High	Unknown	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F31	Abbey Walk	High	£50,000-£100,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Access and connectivity improvements at Land north of Conway Close and Swallow Close	Essential/Critical
F32	Gosling's Farm track down to Grimston Lane	Very High	£100,000-£150,000	SCC	SCC	N/A	Beneficial
F33	Land between Cliff Road and Roman Way	Medium	£50,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F34	Land adjacent to Cliff Road	Medium	£50,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F35	Bridleway bridge	Very High	£50,000-£100,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
F36	Grimston Hall to Cordy's Lane via Keeper's Lane	Very High	£350,000-£400,000	SCC	SCC	N/A	Beneficial
F37	PROW Bridleways 12 and 14	Very High	£200,000-£250,000	SCC, CIL, Developer	S106, S278, CIL, SCC	N/A	Essential
F38	PROW Footpath 30	Very High	£200,000-£250,000	SCC, CIL	SCC, CIL	N/A	Essential
F39	Nicholas Road and Parker Avenue	Very High	£600,000-£650,000	SCC, Developer, CIL	S106, S278, CIL, SCC	N/A	Essential
F40	Fagbury Road from junction with Parker Avenue to Dock Gate 2 roundabout	Very High	£100,000-£150,000	SCC, Developer, CIL	S106, S278, CIL, SCC	N/A	Essential
F41	Dock Gate 2 roundabout	Very High	Unknown	SCC, Developer, CIL	S106, S278, CIL, SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F42	Ferry Lane (Option 1) OR Trinity Avenue/Blofield Road (Option 2)	Very High	£350,000-£400,000 (option 1)	SCC, Developer, CIL	S106, S278, CIL, SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F43	PROW Footpath 32B to Rendlesham Road and Hintlesham Drive	Very High	£150,000-£200,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F44	Kirton Road to Gulpher Road via new track and Candlet Track	Very High	£750,000-£800,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Critical
F46	Land at SCLP12.3 North Felixstowe Garden Neighbourhood / Footpath 28	Very High	£50,000-£100,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Critical
F47	Land North of Walton High Street	Very High	£500,000-£550,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Critical
F48	Walton Hall Drive / Footpath 31 / Railway bridge / Runnacles Way / Footpath 43 / Maidstone Road	Very High	£250,000-£300,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F49	Land at SCLP12.3 North Felixstowe Garden Neighbourhood	Medium	Unknown	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F50	Land between Western Avenue and Cliff Road	High	£100,000-£150,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F51	Land at SCLP12.3 North Felixstowe Garden Neighbourhood	Very High	£1,000,000-£1,100,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Critical
F52	Gulpher Road to Back Lane (Falkenham)	Medium	£500,000-£550,000	SCC	SCC	N/A	Beneficial
F54	Colneis Road	Very High	£800,000-£850,000	SCC, CIL	SCC, CIL	N/A	Essential
F55	Hyem's Lane, Land at SCLP12.3 North Felixstowe Garden Neighbourhood	Very High	Unknown	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Critical
F56	PROW 12 and PROW 13	Very High	£150,000-£200,000	SCC, CIL	SCC, CIL	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F57	Ferry Road and Church Road	Very High	£600,000-£650,000	SCC, CIL	SCC, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F58	Land at SCLP12.3 North Felixstowe Garden Neighbourhood	Very High	Unknown	SCC, Developer, CIL	S106, S278, CIL, SCC	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Critical
F59	Runnacles Way, Grange Farm Avenue/Wesel Avenue to Ferry Lane and Grange Road	Very High	£550,000-£600,000	SCC, CIL	SCC, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F60	Railway bridge between Runnacles Way and Hawkes Lane	Very High	Unknown	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F61	Grange Farm Avenue	High	£200,000-£250,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F62	Maidstone Road/Grange Road	High	£650,000-£700,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F63	Mill Lane	Very High	£350,000-£400,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F65	Cavendish Park	Medium	Unknown	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F68	Coronation Drive to Garrison Lane 'south'	High	£500,000-£550,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F69	Garrison Lane 'north' and the Garrison Lane / Candlet Road / Grove Road / Cowpasture Allotments access roundabout	Very High	£450,000-£500,000	SCC, CIL	SCC, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F70	Garrison Lane 'north' connection into Fairfield Avenue	High	£50,000-£100,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F71	Garrison Lane/High Road cross roads	Medium	Unknown	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F72	Land to the south of High Road West, between Garrison Lane and Railway Approach (for Felixstowe railway station)	High	£200,000-£250,000	SCC, CIL	SCC, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe and Access improvements to rail stations	Essential
F75	Mill Lane/Garrison Lane crossroads	Very High	Unknown	SCC, CIL	SCC, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe and Access improvements to rail stations	Essential
F76	Chaucer Road	Very High	Unknown	SCC, CIL	SCC, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe and Access improvements to rail stations	Essential
F77	Undercliff Road West	Medium	£50,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F78	Langer Road	Very High	£50,000-£100,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F79	Langer Road (East side, Port bound)	Very High	£300,000-£350,000 (option C)	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F80	Langer Road (West side, centre bound)	Very High	£650,000-£700,000 (option A)	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F81	Langer Park	Medium	Unknown	ESC	ESC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F82	Langer Road Junction	Very High	Unknown	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F83	Beach Station Road ('west')	Very High	£600,000-£650,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Sustainable pedestrian and cycle connectivity at Land at Haven Exchange and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F84	Beach Station Road ('west')/Walton Avenue	Very High	£300,000-£350,000	SCC, Developer	S106, S278, CIL, SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F86	Walton Avenue	Very High	£500,000-£550,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Sustainable pedestrian and cycle connectivity at Land at Haven Exchange and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F87	Undercliff Road West (Spa Pavilion Theatre/Promenade) (cycle parking)	Medium	Unknown	ESC	ESC	Sustainable pedestrian and cycle connectivity at Land at Haven Exchange and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F138	The Promenade (Cycle parking)	Medium	Unknown	SCC, ESC	SCC, ESC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F89	Sea Road (cycle parking)	Medium	Unknown	SCC, CIL	SCC, CIL	Sustainable pedestrian and cycle connectivity at Land at Haven Exchange and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F91	Beatrice Avenue	High	Unknown	SCC, CIL	SCC, CIL	Sustainable pedestrian and cycle connectivity at Land at Haven Exchange and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F92	High Road West / High Road East / Hamilton Road / Beatrice Avenue roundabout	Medium	Unknown	SCC, CIL	SCC, CIL	Sustainable pedestrian and cycle connectivity at Land at Haven Exchange and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F93	Railway Approach/High Road West (for Felixstowe railway station)	Very High	Unknown	SCC, CIL	SCC, CIL	Sustainable pedestrian and cycle connectivity at Land at Haven Exchange and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F139	Great Eastern Square	Medium	Unknown	SCC, ESC	SCC, ESC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F98	The Triangle	Medium	Unknown	SCC, CIL	SCC, CIL	Sustainable pedestrian and cycle connectivity at Land at Haven Exchange and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F100	Crescent Road	Very High	£550,000-£600,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F102	Rosemary Avenue	Medium	£250,000-£300,000	SCC, CIL	SCC, CIL	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F105	Elmcroft Lane / Footpath 8 (West)	High	£50,000-£100,000	SCC, CIL	SCC, CIL	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F106	Westmorland Road	Medium	£300,000-£350,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F109	Golf Road (cycle parking)	Medium	Unknown	ESC	ESC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F111	Cliff Road (cycle parking)	Medium	Unknown	ESC	ESC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F112	Undercliff Road East (cycle parking)	Medium	Unknown	ESC	ESC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F113	Circular leisure route - bridleways 24, 25, 26, 18, 16, 37	Medium	£750,000-£800,000	SCC, DFT	SCC, DFT	N/A	Beneficial
F115	The Dip to Felixstowe Ferry via PROW Footpath 62	High	£450,000-£500,000	SCC	SCC	N/A	Beneficial
F116	Gap in the sea wall (east of Martello Lane)	High	Unknown	ESC	ESC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F117	Manor Terrace and Promenade	High	£50,000-£100,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F118	Manor Terrace	Medium	£250,000-£300,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F119	Landguard Nature Reserve / Landguard Point	High	£400,000-£450,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F121	Maidstone Road	Medium	Unknown	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F140	Longcroft to Maidstone Road	Medium	Unknown	SCC, ESC	SCC, ESC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F123	Maidstone Road roundabout	Medium	Unknown	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F124	Seaton Road	High	£300,000-£350,000	SCC, CIL	SCC, CIL	Access improvements at Bridge Road and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F126	Land between Recreation Lane and Plymouth Road	Medium	£50,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F128	PROW32	Medium	£50,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F131	Howlett Way to Church Lane/Bridleway 5	Very High	£150,000-£200,000	Developer	S106, S278	Access, footway and cycle connectivity improvements at Land off Howlett Way and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential/Critical
F134	Mill Lane (Railway bridge)	Very High	£50,000-£100,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F135	Mill Lane (East)	Very High	£100,000-£150,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
F136	Brackenbury Sports Centre	High	£100,000-£150,000	Developer	S106, S278	Pedestrian and cycle enhancements at Land at Brackenbury Sports Centre and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
F141	Trimley St Martin Play Area, off Goslings Way	Medium	Unknown			N/A	Essential
F142	Gunn Lane to High Road	High	Unknown	Developer, SCC, CIL	S106, S278, CIL, SCC	N/A	Essential
L1	Millennium Way / Peto Way	Very High	Unknown	SCC, CIL	SCC, CIL	N/A	Essential
L2	Corton Long Lane	High	£7500,000-£800,000	Developer, SCC, CIL	S106, S278, CIL, SCC	N/A	Essential
L3	Old Lane/Gunton Park (Lowestoft and Yarmouth Rugby Club site)	Very High	Unknown	Developer, SCC, CIL	S106, S278, CIL, SCC	N/A	Critical
L4	Bentley Drive	High	Unknown	SCC	SCC	N/A	Beneficial
L5	A47 (to Gunton Church Lane)	Very High	£200,000-£250,000	SCC, NH, CIL	SCC, NH, CIL	Potential safety improvements to A47 to accommodate to the North Lowestoft Garden Village (Policy WLP2.12)?	Potentially Critical
L6	A47 Outside Ormiston Denes Academy	Very High	£1,200,000-£1,300,000	SCC, NH	SCC, NH, DFT	N/A	Beneficial
L7	Former Railway Line	Very High	Unknown	SCC	SCC	N/A	Beneficial
L8	Oulton Road	High	£200,000-£250,000	SCC	SCC	N/A	Beneficial
L9	Oulton Road-Church Lane to St Margaret's Academy	High	£550,000-£600,000	SCC	SCC	N/A	Beneficial
L10	Denmark Road	Very High	£450,000-£500,000	SCC, CIL	SCC, CIL	N/A	Essential
L11	Station Square/Bascule Bridge	Very High	Unknown	SCC, DFT, NH, CIL	SCC, DFT, NH, CIL	Removal of pinch points across Lowestoft	Desirable/Essential
L12	Pier Terrace/Belvedere Road	Very High	Unknown	Developer, SCC, CIL	S106, S278, CIL, SCC	Removal of pinch points across Lowestoft	Desirable/Essential
L13	Kirkley Waterfront and Sustainable Urban Neighbourhood (WLP2.4)	Very High	Unknown	Developer, SCC, ESC	S106, S278, CIL, SCC	Brooke Peninsula Pedestrian and Cycle Bridge and Improvements to local infrastructure to assist in access and use of Enterprise Zones	Essential
L14	Third River Crossing ('Gull Wing' crossing)	Very High	Being Built	SCC	SCC	Third Crossing over Lake Lothing	Essential
L15	Kirkley Rise	High	Unknown	SCC	SCC	N/A	Beneficial
L16	Tom Crisp Way to path behind Kimberley Road	Very High	£10,000-£50,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
L17	Victoria Road/Dell Road	Very High	Unknown	SCC	SCC	N/A	Essential
L19	Bridge Road/Saltwater Way	Very High	Unknown	SCC, CIL	SCC, CIL	Removal of pinch points across Lowestoft	Desirable/Essential
L57	Cotmer Road/Elm Tree Road	High	£1,000,000-£1,100,000	SCC	SCC	N/A	Beneficial
L20	Bridge Road/Commodore Road/Harbour Road	Very High	Unknown	SCC, ESC, PC, CIL	SCC, ESC, PC, CIL	Normanston Park Pedestrian and Cycle Bridge	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
L21	Normanston Drive	High	£700,000-£750,000	ESC, SCC	ESC, SCC	N/A	Essential
L22	Normanston Park	Very High	£300,000-£350,000	ESC, TC	ESC, TC, CIL	N/A	Beneficial
L23	Peto Way	Very High	£550,000-£600,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
L24	Tom Crisp Way	Very High	Unknown	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
L25	Long Road (west of Elm Tree Road)	Very High	£250,000-£300,000	SCC	SCC	N/A	Beneficial
L26	Castleton Avenue	High	£250,000-£300,000	SCC	SCC	N/A	Beneficial
L27	Castleton Avenue	Very High	£250,000-£300,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
L28	Chapel Road/Church Lane	Medium	Unknown	Developer, SCC, CIL	S106, S278, CIL, SCC	N/A	Essential
L29	Dorley Dale, Gratton Dale and Thixendale	Medium	Unknown	Developer, SCC, CIL	S106, S278, CIL, SCC	N/A	Essential
L30	Land South of the Street (WLP12.16)	Medium	Unknown	Developer	S106, S278	N/A	Essential
L31	Corton Road/Links Road	High	£1,900,000-£2,000,000	ESC, DFT	ESC, DFT	N/A	Beneficial
L32	Gunton Cliff	Medium	£650,000-£700,000	SCC	SCC	N/A	Beneficial
L33	Coastal Path	Very High	£1,000,000-£1,100,000	SCC, ESC	SCC, ESC	N/A	Desirable/Essential
L34	Yarmouth Road	Very High	£200,000-£250,000	SCC, NH	SCC, NH	N/A	Beneficial
L35	Jubilee Way	Very High	Unknown	SCC, CIL, Developer	S106, S278, CIL, SCC	N/A	Essential
L36	High Street	High	Unknown	SCC	SCC	N/A	Beneficial
L37	London Road North	Very High	Unknown	SCC, ESC, TC, CIL	SCC, CIL	N/A	Essential
L38	PowerPark	High	Unknown	SCC, ESC, Developer, CIL	S106, S278, CIL, SCC, ESC	Improvements to local infrastructure to assist in access and use of Enterprise Zones	Essential
L39	Coastal Path (continuation)	Medium	Unknown	SCC, ESC, Developer, CIL	S106, S278, CIL, SCC, ESC	N/A	Essential
L40	Wilde Street – Rant Score	Medium	£300,000-£350,000	SCC, CIL	SCC, CIL	Improvements to local infrastructure to assist in access and use of Enterprise Zones	Essential
L41	Battery Green Road	High	Unknown	SCC, CIL, Developer	S106, S278, CIL, SCC	N/A	Essential
L42	South Beach	High	Unknown	SCC, ESC	SCC, ESC	N/A	Beneficial
L43	The Cliffs	High	£1,200,000-£1,300,000	SCC, ESC	SCC, ESC	N/A	Beneficial
L44	Bloodmoor Road	Very High	Unknown	SCC, CIL, Developer	S106, S278, CIL, SCC, ESC	Improvements to Bloodmoor Roundabout, Lowestoft	Essential
L45	London Road	Very High	£450,000-£500,000	SCC, DFT, CIL	SCC, DFT, CIL	Improvements to local infrastructure to assist in access and use of Enterprise Zones	Essential
L46	Beccles Road	High	£350,000-£400,000	SCC, Developer, CIL	S106, S278, CIL, SCC	N/A	Essential
L47	Behind Beccles Road	High	£550,000-£600,000	SCC	SCC	N/A	Beneficial
L48	Holly Road	High	Unknown	SCC	SCC	N/A	Beneficial
L49	Bonds Meadow	High	£100,000-£150,000	SCC, ESC	SCC, ESC	N/A	Beneficial

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
L50	Gorleston Road	High	£950,000-£1,000,000	SCC, CIL	SCC, CIL	N/A	Essential
L51	Sands Lane	High	£550,000-£600,000	SCC, CIL	SCC, CIL	N/A	Essential
L52	Lime Avenue	High	£200,000-£250,000	SCC, Developer, CIL	S106, S278, CIL, SCC	N/A	Essential
L53	Mendip Road	Medium	£150,000-£200,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Improvements to local infrastructure to assist in access and use of Enterprise Zones	Essential
L54	Woods Loke West	High	Unknown	SCC	SCC	N/A	Beneficial
L55	Woods Meadow Development/Hall Lane	High	£550,000-£600,000	SCC, Developer, CIL	S106, S278, CIL, SCC	Extensions to footpaths along Hall Lane and Union Lane, Oulton	Essential
L56	WLP2.14 Land North of Union Lane, Oulton and and WLP2.15 Land Between Hall Lane and Union Lane, Oulton	High	£400,000-£450,000	SCC, Developer	S106, S278, SCC	N/A	Essential
LH1	WLP2.13 North of Lowestoft Garden Village	Very High	Unknown	Developer	S278, S106	Cycle link between Lowestoft and Hopton	Essential
LH2	A47 (adjacent)	High	£1,000,000-£1,100,000	NH, SCC, Developer, CIL	S278, S106, CIL, SCC, NH, DFT	Cycle link between Lowestoft and Hopton and Potential safety improvements to A47 to accommodate the North Lowestoft Garden Village (Policy WLP2.12)	Potentially Critical
LH3	Sewage Treatment Works	High	£200,000-£250,000	SCC, Developer, CIL	S278, S106, CIL, SCC	Cycle link between Lowestoft and Hopton	Essential
LH4	WLP2.13 North of Lowestoft Garden Village	High	£150,000-£200,000	SCC, Developer, CIL	S278, S106, CIL, SCC	Cycle link between Lowestoft and Hopton	Essential
LH5	Off-road route (line indicative)	Very High	£700,000-£750,000	SCC, CIL	CIL, SCC, DFT	Cycle link between Lowestoft and Hopton and potential safety improvements to A47 to accommodate the North Lowestoft Garden Village (Policy WLP2.12)	Essential
LH6	Stirrups Lane	High	Unknown	SCC, CIL	CIL, SCC, DFT	Cycle link between Lowestoft and Hopton	Essential
LH7	Coast Road	High	Unknown	SCC, CIL	S278, S106, CIL, SCC, DFT	Cycle link between Lowestoft and Hopton	Essential
LH8	A47 (north of Stirrups Lane)	Medium	£600,000-£650,000	SCC, ESC, NH, CIL	S278, S106, CIL, SCC, NH, DFT	Cycle link between Lowestoft and Hopton and potential safety improvements to A47 to accommodate the North Lowestoft Garden Village (Policy WLP2.12)	Essential

Reference (copied from Word 2022-08-04)	Location (copied from Word 2022-08-04)	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
LK1	A12 (between Tower Road and London Road)	Very High	£550,000-£600,000	SCC, CIL	SCC, CIL	N/A	Essential
LK2	London Road	Very High	£600,000-£650,000	SCC, DFT, Developer, CIL	S278, S106, CIL, SCC, DFT	N/A	Essential
LK3	Kessingland Neighbourhood Plan Allocations (SA1 Former Ashely Nurseries site, SA2 Land at Laurel Farm West and South and SA3 Land at Laurel Farm East)	Very High	£300,000-£350,000	Developer	S278, S106	N/A	Essential
LK4	Clare Road	High	£100,000-£150,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
LK5	London Road – High Street	Medium	£250,000-£300,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
LB1	Beccles Road (A146)	Very High	£1,800,000-£1,900,000 (cycle infrastructure only not highway)	SCC, DFT, CIL	SCC, CIL, DFT	N/A	Essential
LB2	Swan Lane	Very High	Unknown	SCC, CIL	SCC, CIL	N/A	Essential
LB3	Mutford Wood	Very High	Unknown	SCC, Developer, CIL	S106, S278, CIL, SCC	N/A	Essential
LB4	Mutford Wood Lane	High	Unknown	SCC, Developer, CIL	S106, S278, CIL, SCC	N/A	Essential
LB5	Bridleway 12, 6, 4, 8 and 24	Medium	£600,000-£650,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
LB6	New Road	Very High	£200,000-£250,000	SCC, CIL	SCC, CIL	N/A	Essential
LB7	Church Road/Hulver Road	Very High	£250,000-£300,000	SCC	SCC	N/A	Beneficial
LB8	North Cove	Very High	Unknown	SCC, CIL	SCC, CIL	N/A	Essential
LB9	Lowestoft Road	Very High	Unknown	SCC, CIL	SCC, CIL	N/A	Essential
LB10	Benacre Road	Very High	£1,500,000-£1,600,000	SCC, DFT, Developer, CIL	S278, S106, CIL, SCC, DFT	Improvements to local infrastructure to assist in access and use of Enterprise Zones	Essential
LB39	Sandpit Lane	Medium	Unknown	SCC	SCC	N/A	Beneficial
LB11	Copland Way	High	£850,000-£900,000	SCC, CIL	SCC, CIL	Improvements to local infrastructure to assist in access and use of Enterprise Zones	Essential
LB12	Lowestoft Road	High	£1,200,000-£1,300,000	SCC, DFT	SCC, DFT	N/A	Beneficial
LB40	Ellough Road	Medium	£450,000-£500,000	SCC	SCC	N/A	Beneficial
LB13	Hillside Avenue	Medium	£200,000-£250,000	SCC	SCC	N/A	Beneficial
LB14	NCR (various) Part 1	High	£50,000-£100,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
LB15	Cedar Drive/Rowan Way	High	£300,000-£350,000	SCC, CIL	SCC, CIL	N/A	Essential
LB16	Adjacent Ellough Road	Very High	£400,000-£450,000	Developer	S278, S106	N/A	Essential
LB17	Beccles Southern Bypass	Very High	Unknown	SCC, Developer, CIL	S278, S106, CIL, SCC, NH	Improvements to local infrastructure to assist in access and use of Enterprise Zones	Essential
LB18	NCR (various) Part 2	High	£850,000-£900,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
LB19	Rigbourne Hill	Very High	£650,000-£700,000	SCC, Developer, CIL	S278, S106, CIL, SCC	N/A	Essential

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LB20	WLP3.3 Beccles and Worlingham Garden Neighbourhood	Very High	Unknown	SCC, Developer	SCC, S278, S106	N/A	Essential
LB21	NCR (various) Part 3	Very High	£300,000-£350,000	Developer, SCC, CIL	S278, S106, SCC, CIL	N/A	Essential
LB22	Wash Lane	Very High	Unknown	Developer, SCC, CIL	S278, S106, SCC, CIL	N/A	Essential
LB23	Ballygate or Puddingmoor	High	Unknown	SCC	SCC	N/A	Beneficial
LB24	Common Lane	High	Unknown	SCC, TC	SCC, TC	N/A	Beneficial
LB25	Beccles to Shipmeadow	Very High	£1,000,000-£1,100,000	SCC, DFT	SCC, DFT	N/A	Beneficial
LB26	Ringsfield Road north	Very High	£450,000-£500,000	SCC	SCC	N/A	Beneficial
LB27	Ringsfield Road (south)	High	£50,000-£100,000	SCC	SCC	N/A	Beneficial
LB28	Bridleways between Ringsfield Road and Church Road	High	£350,000-£400,000	SCC	SCC	N/A	Beneficial
LB29	Ringsfield	High	£100,000-£150,000	SCC, CIL	SCC, CIL	Extensions to footpaths along School Road, Ringsfield	Essential
LB30	Bridleway 8	Medium	£300,000-£350,000	SCC	SCC	N/A	Beneficial
LB31	Bridleways 6 to 16	High	£600,000-£650,000	SCC	SCC	N/A	Beneficial
LB32	Ilketshall St Andrew	High	Unknown	SCC	SCC	N/A	Beneficial
LB33	Hall Lane, Footpaths 11 and 17	Medium	£350,000-£400,000	SCC	SCC	N/A	Beneficial
LB34	Clarke's Lane, Byways 5, 16 and 17	Medium	£1,000,000-£1,100,000	SCC	SCC	N/A	Beneficial
LB35	Low Road	Very High	Unknown	SCC	SCC	N/A	Beneficial
LB36	Castle Road/Annis Hill	High	£1,500,000-£1,600,000	SCC	SCC	N/A	Beneficial
LB37	Bridleway 1 and 8	Medium	£400,000-£450,000	SCC	SCC	N/A	Beneficial
LB38	Hillside Road East	High	£450,000-£500,000	SCC, CIL	SCC, CIL	N/A	Desirable/Essential
MF1	A14 Pedestrian Bridge	Very High	Unknown	SCC	SCC	N/A	Beneficial
MF2	Kirton Road/Trimley Road/Bucklesham Road	Very High	Unknown	SCC, DFT	SCC, DFT	N/A	Beneficial
MF3	Bucklesham to Brightwell Lakes	High	Unknown	SCC, Developer,	S278, S106, CIL, SCC	N/A	Desirable/Essential

Community Recommendations

Ref	Parish	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
22	Lowestoft	B1532 (Marine Parade) in Lowestoft	This route is part of the Suffolk County Council Lowestoft Cycle route and designated a On-Road signed cycle route and approx 2km in length. Unfortunately due to lack of upgrading or maintenance around 80% of the white lines separating vehicles from cyclists have faded into the tarmac and now indistinguishable for motorists and cyclists. The only short parts of the cycle route which have been painted are those where the highways agency have completed recent road repairs see attached photo's.	Paint the white lines please along the length of Marine Parade which will link Pakefield in the South to Lowestoft town centre in the North.	Issues relating to maintenance have not been assessed for the IDF.					
23	Lowestoft	From Arbor Lane to Pakefield Rd along the current cliff top footpath	Link Pakefield (Arbor Lane) to Pakefield Road along the scenic cliff top and waterfront, with some will and a little modification to existing pedestrian infrastructure along a 1km section we could have a continuous 3km cycling route linking up to the traffic free sea-front and onto Lowestoft town centre, that is a winner for all.	Currently as you can see in the attached photographs this 1km section is narrow along parts of the route and even passing pedestrians have to step off the footpath which is also a popular route for cyclists especially school children cycling to local schools, yes I know cyclists are supposed to dismount and walk this 1km section but lets move on and grasp the nettle and make it a harmonious link for both pedestrians and cyclists from Pakefield and into Lowestoft, a win-win for all especially school children.	High	£1,200,000-£1,300,000	SCC, ESC	SCC, ESC	SCC, ESC	Beneficial
24	Walpole Robbie	Forge Cottage, Walpole, IP19 9AZ	Walking from one village to another is extremely dangerous especially where there are bends and hills with high banks and no escape for pedestrians. Some drivers exceed the 30 mph speed limit and others drive into the winter sun unable to see the road at all, Other rural roads that are NSL are narrow and should be 20 or 30 mph. Walking and cycling should be encouraged. We have no 'bus service to our nearest shops which are over 2 miles away, as are schools, pubs and active churches.	Walking and cycling, especially between towns and villages should be made safer. Narrow roads should be 20 or 30 mph. Attention should be given to improving the visibility of cyclists and pedestrians especially on hills and bends and where there are high banks. New footpaths at such points through adjacent fields would reduce the risks. Banks could be cut back at key points.	Speed reductions have not been assessed for the IDF.					
25	Trimley St Mary	High Road , Trimley.	Cars parked on cycle lane, necessitating cyclists moving out and in from main road repeatedly. Cycle lane disjointed with many short sections.	Ban parking in cycle lane. Have one continuous cycle lane. Similar problem exists in many other areas in Felixstowe with disjointed cycle lanes.	Very High	£1,300,000-£1,400,000	SCC, NH, Developer, CIL	SCC, NH, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
29	Kesgrave	Main road kesgrave	Cycle track not fit for purpose, especially around Windrush Road where potholes on road are dangerous. Very uneven and old cycle track surface, many cyclists forced to use Road.	Resurface section from police station to Kesgrave fisheries.	Medium	£1,800,000-£1,900,000	SCC, DFT	SCC, DFT	N/A	Beneficial
31	Lowestoft	Roundabout A47 and Corton Long Lane - to Suffolk Border before Hopton!	Cycle path ends with no path from this roundabout to the Suffolk Border above Hopton. Where on the Norfolk side there is from Gt Yarmouth a cycle path from Gorleston to Hopton and this is where it ends.	A12 upgrade to A47 never improved the cycle ways infrastructure.	High	£1,000,000-£1,100,000	NH, SCC, Developer, CIL	S278, S106, CIL, SCC, NH, DFT	Cycle link between Lowestoft and Hopton and potential safety improvements to A47 to accommodate the North Lowestoft Garden Village (Policy WLP2.12)	Essential

Ref	Parish	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
35	Trimley St Mary	Trimley St Martin	6 pathways leading to open countryside have been closed across the railway line. This hardly promotes improved walking and cycling access.	Reinstate those crossings where there is still only one track to cross so not making the pathways any less safe than before.	Not assessed for IDF.					
37	Reydon	Road from A12 Blythburgh to Southwold. and most Suffolk B roads.	Country roads not suitable for cyclists. Long hold ups behind cyclists who cannot be safely overtaken on narrow winding roads with or without opposing traffic. Put simply the increase in leisure cycling is a menace to other traffic on our local roads, causing traffic jams , prolonged journey times and inefficient use of fuel when stuck in low gears behind cyclists ,and should not be encouraged. People living in the country need to get about by car. We do not need people 'playing' on our roads,	Separate cycle ways BUT not along existing footpaths. The Sustrans cycle path along Halesworth Millenium Meadow is a classic example of pedestrians and cyclists not mixing. . Cyclists all too often approach walkers(often with dogs) from behind at great speed and give no warning as they hurtle past nearly injuring pedestrians and their pets. It became so bad at one stage that we stopped walking there.	No Cycling and Walking infrastructure improvement suggested.					
39	Saxmundham	B1121 main road linking Benhall, Saxmundham, Kelsale	Lack of safe pedestrian/cycling route between Benhall, Saxmundham, Kelsale, Lack of cycling infrastructure (signs, secure parking.cycle lanes) East Suffolk Council, Suffolk County Council Highways Dept, Planning Dept do not seem to communicate with each other - a perfect example of this is the new train station in Saxmundham has no provision for secure bicycle parking.	The 3 Communities Link project report was completed in 2017 - it detailed a safe route between Benhall, Saxmundham, Kelsale for pedestrians and cyclists. It also linked to the local schools and Saxmundham railway station. The report is currently sitting with Suffolk County Council and has been included in their list of 100 cycling projects to be delivered in the next 5 years (see EADT article.) The report has been ratified and costed by SCC/Highways and is still awaiting funding. It is an "oven-ready" solution to the transport infrastructure issues in and around Saxmundham I am the author of the report file:///media/fuse/drivefs-234088169dc1f109c9a130868367d4ad/root/THE%203%20COMMUNITIES%20LINK%20Impact%20Audit%20&%20Report.pdf Our FB page: https://www.facebook.com/SaxTCCFocusGroup	N/A	£1,300,000-£1,400,000	SCC, DFT, Developer, CIL	S278, S106, CIL, SCC, NH, DFT		
44	Martlesham	Re-route NCN1 to avoid retail park in Martlesham	The area around Gloster Road has become much busier since NCN1 was planned as has Felixstowe Road.	It would now be safer, shorter and more pleasant to route NCN1 straight on at the point shown on the map, along Main Road under the junction of A12/A1214 to rejoin the existing route at the junction of A1214 and Deben Avenue.	No Cycling and Walking infrastructure improvement suggested.					
47	Stratton Hall	Between Nacton and Trimley	Lack of safe walk routes between Nacton and Trimley	use 1/2 of the Felixstowe road as a cycle track and walkway	Very High	£1,800,000-£1,900,000	Developer, SCC, NH, CIL	S106, S278, CIL, SCC, DFT	Significant access improvements and improvements to the wider land at Felixstowe Road (Policy SCLP12.21)	Critical

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51	Aldringham Cum Thorpe	the entire A1094 from Friday street to Aldeburgh but especially the stretch between Frisyon and Alfeburgh.	fast road with cars doing 60mph, having to brake heavily when coming upon bikes. road is often busy both ways and insulates meaning it becomes difficult to pass the cyclists safely.with the increase in hgvs traffic expected for the wind farm installation something needs to be done to protect the cyclists	I have no solution but as a motorist I'm petrified of slow moving cyclists going up.hill and meeting them before I've been able to brake sufficiently.	No Cycling and Walking infrastructure improvement suggested.					
52	Felixstowe	Old Felixstowe, walk to Felixstowe Ferry	The pathway by the sea down to Felixstowe Ferry is hard core or gravel, which makes walking difficult and renders it almost impossible for wheelchair users or buggies to complete the walk to the ferry and the cafes at Felixstowe Ferry.	To replace the rough walking surface with a smooth surface to encourage walkers to reach Felixstowe Ferry.	High	£450,000-£500,000	SCC	SCC	N/A	Beneficial
53	<Null>	The old river crossing ,north gate, Beccles	May not be East Suffolk, but there is a disused railway line goes from the old railway river crossing in Beccles, to Gillingham, geldeston, ellingham, bungay . I tried to cycle a small section recently, impossible, very overgrown... But as in Derbyshire, a reclaimed railway line are brilliant for traffic free walking and cycling	Talk to the land owner / set up a charity work party	Largely outside East Suffolk so has not been assessed for IDF.					
57	Martlesham	The whole of Sandy Lane from old Martlesham to Woodbridge	There is currently no safe pedestrian access from Old Martlesham to Woodbridge. Would strongly recommend installing a footpath full length of Sandy Lane from Top Street Martlesham to Ipswich Rd Woodbridge.	There is currently no safe pedestrian access from Old Martlesham to Woodbridge. Would strongly recommend installing a footpath full length of Sandy Lane from Top Street Martlesham to Ipswich Rd Woodbridge.	Very High	£850,000-£900,000	SCC	SCC	N/A	Desirable
58	<Null>	many places	on narrow FOOTPATHS cycles and buggy(go carts) creep up on walkers or ride at speed towards and fail to give warning before speeding up from behind. cyclists along the sea front seem to prefer to ride on the footpath rather than the designated cycle path never dismount at the pier - ride like hooligans on the bascular bridge regardless of pedestrians social distancing is more important now than ever	keep bikes and walkers separate in well defined areas in the last 10 years I have walked 77million steps mainly in the Lowestoft oulton broad area footpaths need to be safe for us walkers	Comment too generalised to assess in IDF.					
60	Lowestoft	Gorleston Road (as an example)	The cycle lanes throughout Lowestoft all need repainting.	Paint plus workers	Issues relating to maintenance have not been assessed for the IDF.					

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62	Aldeburgh	Thorpe Rd Aldeburgh, the full length of this road between Aldeburgh and Thorpeness.	Many cyclists use this road as it is difficult to cycle all the way to Thorpeness along the beach/foreshore, both because of the terrain and the number of people using the footpath. This road has a 60mph speed limit and because it is straight many people drive fast. It is therefore a dangerous road for cyclists and families to use. It should also be noted that this road runs along side a nature reserve and the risk to wildlife is significant. Deer are also a danger to drivers.	Get the speed limit reduced to 30mph so that it becomes safer and links the 30mph limits in Aldeburgh and Thorpeness together.	Speed reductions have not been assessed for the IDF.					
63	Kesgrave	Main road Kesgrave from Martlesham to Ipswich hospital	You talk about cycling strategies to improve access- I have reported this many times over the years about the poor state of the cycle path and poor condition potholed surface on Kesgrave to Ipswich main road cycle path. It's simple- improve cycling numbers by providing Dutch style standard surfaces to cycle on. No more cycle repairs due to rubbish poorly maintained cycle paths like this one!!!!	I've mentioned this as above	High	£1,900,000-£2,000,000	SCC, CIL	SCC, CIL	N/A	Essential
64	Felixstowe	Footpath leading to steps to the beach at the end of Martello Lane, Felixstowe. Known as Jacobs Ladder I believe	The footpath is overgrown. You need to weave your way along avoiding weeds, plants, dead foliage etc along with overhanging branches from neighbouring houses	The footpath is overgrown. You need to weave your way along avoiding weeds, plants, dead foliage etc along with overhanging branches from neighbouring houses	Issues relating to maintenance have not been assessed for the IDF.					
65	Barnby	New Road	A general issue that reports of road problems which affect cyclists are not taken seriously by the highways department. At this location there is a big dip in the road where the telegraph line crosses the road. It is a downhill stretch and if you do not know about it then it could lead to a cyclist being dismounted or coming off the road (this has happened).	The highways department to take cycling issues seriously and fix accordingly.	Highway matter not assessed by IDF.					
66	Martlesham	Broomfield to Eagle way,	The path is too narrow to safely support both cyclists and walkers due to a very tight bent. . There have been collisions in the past at this point.	Cyclists should be re routed via Broomfield to Eagle Way	Not assessed for IDF.					
67	Kesgrave	Grange Farm Cycle way	Very poorly maintained and by end of summer is badly overgrown. Additionally people enter the combined Cycle / walkway from hidden junctions.	Need a better maintenance and clearance so its possible to see people entering the cycle track.	Issues relating to maintenance have not been assessed for the IDF.					
68	Martlesham	Path alongside the A12	By mid summer the path becomes overgrown reducing it to single file.	If you cannot cut during bird nesting you should really cut back hard at the beginning of the summer or clear the vegetation altogether	Issues relating to maintenance have not been assessed for the IDF.					

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69	Hollesley	road from hollesley village (rectory road) , moors farm corner to shingle street.	The road to Shingle Street from Moors farm, which is a minor road, has 5 very dangerous blind corners, yet it is sign posted at national speed limit. This road has become very busy with walkers and cyclists (including many children), horse riders and dog walkers, tourists including campervans, 'boy racers' and large heavy vehicles. It also includes a national cycle way and is used as a Duke of Edinburgh Award walk. Further information on request as I have lived on this road for 35 years.	Reduce speed limit to 30 or less and please look at the corners before their is fatalities	Speed reductions have not been assessed for the IDF.					
71	Reydon	Jermyns road, entire length	Jermyns road is a road with Reydon primary school just off it, it is very dangerous with fast traffic. My son rides his bike to school but I am fearful of the traffic and would appreciate some traffic calming measures, as in most areas with a school on/near the road	Traffic calming, 20 mph limit	Speed reductions have not been assessed for the IDF.					
73	Martlesham	Sandy lane, Martlesham	This lane is the connection between the cycle lanes of Martlesham/Kesgrave and Woodbridge. It is used as a short cut for traffic to and from Woodbridge and is national speed limit which creates dangerous conditions for all cyclists particularly those who don't know the road well and children.	20 or 30 MPH limit. Access only for motorised vehicles?	Speed reductions have not been assessed for the IDF.					
74	Woodbridge	Ipswich Road, Woodbridge	Very dangerous for cyclists on the route into Woodbridge	Dedicated cycle lane, possibly two way alongside/incorporating the wide footpath, as far as the Cherry tree road junction. Provide some quality bike parking in Woodbridge.	N/A	£850,000-£900,000	SCC, CIL	S106, S278, CIL, SCC, DFT	Access and junction improvements at Land at Woodbridge Town Football Club	Essential/Critical

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75 <Null>		County wide	The issue for cyclists is a lack of dedicated infrastructure along with having to cycle on fast, dangerous small roads alongside drivers who assume entitlement.	<p>We have a vast network of ancient lanes and byways, many of which are not heavily used by motorised vehicles but do not necessarily join up to go anywhere safely. Some of these lanes could be connected with new sections built to join settlements as needed.</p> <p>Possible rules along these routes:</p> <ol style="list-style-type: none"> 1. No through traffic 2. A new speed limit of 25mph for all other traffic requiring access. 3. A change in insurance liability similar to the Dutch article 185 of road law along these routes, thus deterring traffic further and encouraging family use. <p>As most of the roads already exist, it could be a cost effective solution with major impact.</p> <p>Such routes, if well planned, may well serve to encourage family cycling holidays, such as are seen in other countries, and if a few campsites or cheap lodgings were encouraged along the way, would likely boost tourism substantially.</p>	Comment too generalised to assess in IDF.					
87 <Null>		Ipswich to villages (this issue also applies to every town in Suffolk)	There are no safe cycle routes between Ipswich and and villages within a 15 miles radius. Where they exist few drivers keep to the 30mph limits and there are far to many stretches with just the National Speed Limit. On relatively narrow roads this leaves cyclists and pedestrians very close to vehicles doing up to 70mph. Safety concerns are a major reason that more people do not cycle or walk.	Create dedicated cycle and pedestrian routes to link villages with Ipswich. Where possible these routes should exclude vehicles except for access or have enforced speed limits. The routes should also have the sort of cycling safety features that Holland has introduced	Comment too generalised to assess in IDF.					
88 Melton		Woodbridge to villages (this issue also applies to every town in Suffolk)	There are no safe cycle routes between Woodbridge and and villages within a 15 miles radius. Where they exist few drivers keep to the 30mph limits and there are far to many stretches with just the National Speed Limit. On relatively narrow roads this leaves cyclists and pedestrians very close to vehicles doing up to 70mph. Safety concerns are a major reason that more people do not cycle or walk.	Create dedicated cycle and pedestrian routes to link villages with Ipswich. Where possible these routes should exclude vehicles except for access or have enforced speed limits. The routes should also have the sort of cycling safety features that Holland has introduced	Comment too generalised to assess in IDF.					

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89	Tuddenham St Martin	westerfield lane and high street tuddenham st martin	this lane is used as a rat run throughout the day and quite often speeding motorists, HGVs petrol tankers brewery lorries. This is a single track lane and during lockdown it was very pleasant to cycle, walk down this lane as then you didnt have to dive for cover when an annoyed motorist would want you to jump out their way asap. Which is quite dangerous at times....little lane has pull ins and these are being made bigger by the heavy traffic that tries and push forward, so ruining the verges	make this lane a QUIET LANE and NO access to HGV's only for local traffic its even worse when orwell bridge is shut as its like the M25 !!!! with alot of near misses	Quiet Lanes assessed by Quiet Lane project team.					
90	Martlesham	From Felixstowe Road junction with Mill Lane (track to the RSPCA) to just before Crown Point	Cars passing cyclists on 2 blind bends and having to cut back in across the path of the cyclist as a car comes the other way round the bend. I have personally had several 'near misses'. The area is a serious accident waiting to happen.	Increasingly busy as a 'rat run', the cars need to be slowed down. Suggest 2 speed humps: one by the Mill Lane/RSPCA junction and one further down near Crown Point to slow cars in both directions where the blind bends are.	Traffic management not assessed by IDF.					
93	Otley	The road between Otley and Crettingham	There are safe and pleasant routes for pleasure cycling around Monewden and Framdsen. The only way to access these routes from Otley is via Chapel Rd towards Crettingham. This road is narrow and has no speed limit. Vehicles drive very fast on this road. This road is a major reasons that families and children cannot cycle in safety around Otley	Add cycle lanes, reduce the speed limit, add warning signs	N/A	£2,500,000-£3,000,000	SCC, DFT	SCC, DFT	N/A	Beneficial
94	Swilland	Junction Gibraltar Rd and B1078	This is on route from Otley to Swilland and towards Ipswich. The B1078 is fast and straight with only NSL. Crossing on foot or bike from Otley is very dangerous. I do it by myself but would not risk it with a group especially if it included inexperienced cyclists or children	Better signage, speed limit, central reservation	Speed reductions have not been assessed for the IDF.					
95	Martlesham	In and around Martlesham/Martlesham Heath and Woodbridge	Few, if any, footpaths are accessible for wheelchair users, which means that I cannot accompany my friends and family when they go for walks. Shared footpaths with cyclists are a problem because often I can't hear cyclists coming from behind me, and they ride too close.	Make more footpaths accessible for wheelchair users (and parents with prams/buggies) especially in local beauty spots Separate pedestrians from cyclists, or provide a barrier so that cyclists can't ride so close.	Comment too generalised to assess in IDF.					

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96	Martlesham	Sandy Lane between The Street and its junction with California north of the railway bridge	This is a derestricted section connecting two 30mph areas. It's part of the National Cycle Network serving commuters and businesses on Sandy Lane south of the railway. The Parish council has been asking for several years to have this made 30mph on safety grounds. Nothing has happened. To encourage sustainable transport this key part of the only viable cycle route between Woodbridge and Martlesham need be improved, as does the Old Felixstowe Road.	Make the section of Sandy Lane between The Street and California a 30mph area. The attached satellite view gives a good impression of the number of business along that road.	Speed reductions have not been assessed for the IDF.					
101	Mettingham	Cycle route between Bungay and Beccles	Not currently a safe direct cycle route to Beccles from Bungay. The main road is very fast and cars often overtake on hills and blind corners, the smaller roads are equally fast with blind corners and generally poor road condition.	Cycle path along the B1062 road	N/A	£4,000,000-£4,500,000	SCC, DFT	SCC, DFT	N/A	Beneficial
102	South Cove	b1127	I agree that the B1127 is dangerous for cyclists and pedestrians. It would also be great to have a cycle route from Reydon to Kessingland, rather than crossing the A12	Make the Coastal path suitable for mountain bikes?	Not assessed for IDF.					
103	Reydon	southwold and reydon main roads	Congestion in the tourist season makes it difficult for cyclists.	More cycle lanes.	Comment too generalised to assess in IDF.					
105	Leiston Cum Sizewell	On the shared use cycle path along Lovers Lane towards Sizewell.	The cycle path is great but in a few places there are bollards on the pavement which encroach on the space and make it impossible for a cyclist to pass a pedestrian or other cycle on the path. This shared use path is well used by walkers and cyclists but we repeatedly have to join the road here as it is not possible to pass others. It is particularly awkward as this is really well used by families and children.	The bollards just need removing! I am not sure why they are there. Also, perhaps a guide line on the path for pedestrians/cyclists half of the path?	Not assessed for IDF.					
106	Beccles	Between Suffolk town centre of Beccles and Suffolk town centre of Bungay (in partnership with Norfolk).	Having no direct route between the Suffolk towns and having the old railway route unused.	Between Suffolk town of Beccles and Suffolk town of Bungay (in partnership with Norfolk). Reconnect the town's by making use of the old railway route as a new cycle path. This would be away from roads, existing infrastructure (bridges, embankments and cuttings), minimal / no gradients, countryside views, direct route between town centres and for the majority of their route likely to be unused and already furnished with trees, hedges and the odd bit of history along the way.	Largely outside East Suffolk so has not been assessed for IDF.					
107	Shipmeadow	Between Low Road and Puddingmore / Ballygate	Busy road between Beccles and Bungay with no cycleway and only a broken bit of pavement could see a combined cycle/foot path added (as long as it doesn't destroy hedgerows / trees)	Low Road is an ideal and pleasant route into Bungay that avoids the hills and much of the main road from Beccles. However, to get to Low Road from Beccles there is no cycle path and only a patchy / unsuitable pedestrian path.	Very High	£1,600,000-£1,700,000	SCC, DFT	SCC, DFT	N/A	Beneficial

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108	Darsham	A 12 cycle path from Kelsale to Hinton is not maintained and is largely therefore unsafe to use.	Both the surface and surrounding hedgerows etc are not maintained and the cycle path in many places isn't usable, so you have to cycle on the A12, which is often quite unpleasant on a bike among fast, heavy traffic	Maintain the cycle paths	Issues relating to maintenance have not been assessed for the IDF.					
110	Snape	A1094 This is the only link between Woodbridge/Snape to Knodishall/Leiston.	The traffic is fast and frequent. The undulating road means people take risks when overtaking. Riding a bike feels unsafe and you have to cross both lanes of traffic.	Half a mile of cycleway beside the carriage way.	N/A	£1,300,000-£1,400,000	SCC, DFT	SCC, DFT	N/A	Beneficial
111	Hollesley	Sutton Hoo to Hollesley Village (Melton Road/Heath Road)	Road is unsafe for cyclists due to large volume of fast traffic. As the road is straight it gives the impression that you can drive fast. It is undulating and very narrow. Alternative routes to Hollesley or Hollesley Common are a long way round.	A separate lane for cyclists. Maybe through the forest or making use of bridleways across Sutton Common (with surface for normal bikes).	N/A	£1,600,000-£1,700,000	SCC, DFT	SCC, DFT	N/A	Beneficial
113	Friston	Snape to Aldeburgh	The A1094 is too busy and there is no other way of cycling to Aldeburgh.	Use of the coastal path for cyclists as well as walkers. Surfacing in some places, fencing of livestock and extending from Hazlewood Common into Aldeburgh.	N/A	£1,300,000-£1,400,000	SCC, DFT	SCC, DFT	Access and junction improvements at Land rear of Rose Hill	Essential/Critical
114	South Cove	The B1127 between Wrentham and Reydon	It is extremely unfriendly for walkers and cyclists. In spite of it being a minor road with double bends and poor visibility cars come at speed making it very unsafe.	There should be speed restriction and a cycle lane	N/A	£3,000,000-£3,500,000	SCC, DFT	SCC, DFT	N/A	Beneficial
115	Trimley St Mary	Trinket high road	Cycle lane markings are virtually invisible and need re painting.	Re mark cycle lanes	Issues relating to maintenance have not been assessed for the IDF.					
116	Felixstowe	High Road East, Felixstowe	Very poor road surface in cycle lane	Road needs resurfacing, not just another top dressing, which makes matters worse for cyclists	Issues relating to maintenance have not been assessed for the IDF.					
118	Felixstowe	No entry in to th ASL from 2 directions	The Garrison lane traffic lights has no entry lane into the box either from the south bound direction or the west bound	Your the engineers work it out. Last time I commented on the west bound and you removed the north bound. The whole system needs a rethink. Painted advisory cycle lanes are continually parked on rendering them useless, they are often not wide enough especially when they contain drains	Highway matter not assessed by IDF.					
119	Felixstowe	School traffic	At school start time there is a lot of contention when parents park on the double yellow lines across the cycleway or crisscrossing the cycle way to drop off kids.	Why can't they use the drop off circle that was designed for this within the school freeing up the high road . And the school should reopen the Maidstone entrance for cyclist	No Cycling and Walking infrastructure improvement suggested.					
122	Trimley St Martin	Cycle pathway alongside A14	It's over grown and VERY uneven	A significant tidy up, re tarmac pathway	Issues relating to maintenance have not been assessed for the IDF.					

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123	Purdis Farm	Purdis Heath SSSI - Purdis Farm Lane at the junction with Purdis Avenue	New fences with stiles have been erected in the past few weeks along with a large gate across the wide path. It looks like the plan is to be able to close the gate to prevent any vehicle/bike access but it's not clear whether there will be access for wheelchairs or buggies. We regularly use this path with a wheelchair buggy.	Stiles should not be being installed on any footpath without also providing a gate big enough for a large wheelchair or mobility scooter. This applies to all areas.	Not assessed for IDF.					
126	Lowestoft	Corton Road, Lowestoft	The painted on cycle lanes along the length of Corton Road have been allowed to fade (like a lot of other cycles lanes on other roads in Lowestoft) and have not been repainted. The presence of these lanes and provide reassurance to cyclists using the road.	Repaint and maintain the cycle lanes.	Issues relating to maintenance have not been assessed for the IDF.					
127	Lowestoft	High Street between Camden Street and Mariners Street, Lowestoft	Cycles are permitted to ride south along this part and there is no contraflow cycle lane painted onto the road. If one was here it would give confidence to people cycling in that direction and also remind motorists this is permitted. The southern end of high street between Dukes head street and the Triangle market area, also needs resurfacing as its becoming very uncomfortable and bumpy when cycling over.	Paint a contraflow cycle lane and resurface the High street where it needs doing.	Issues relating to maintenance have not been assessed for the IDF, on-road white lines have not been assessed by the IDF.					
130	Hollesley	Street between Duck Corner and Woodbridge Walk, Hollesley	main road between two parts of the village, but no cycle or footpath. Both parts of the village are within a cycling distance but the 60mph speed limit and no pathways make it too dangerous. Has been spoken about for at least twenty years but no positive outcome.	Some cycle or footpath to allow people to safely walk from one part of the village to another.	N/A	£450,000-£500,000	SCC	SCC	N/A	Beneficial
134	Frostenden	Frostenden Hall	Cyclists using footpaths putting walkers, employees and contractors in danger. It is illegal for a cyclist to cycle along a public footpath without the land owner's permission. Very few cyclists are aware of this.	Educate cyclists . Identification numbers on cycles will help deter persistent offenders. Inform navigation apps that some of their information could be incorrect	No Cycling and Walking infrastructure improvement suggested.					
136	<Null>	New cycle lane barriers	The barriers are an improvement of sorts except that they seem to give drivers the impression at they can drive as close to them as they like! If you have a bike with 2 full panniers, it is difficult to join and exit through the barriers.	Make the cycle lanes wider and improve entrance and exit areas especially near roundabouts. General comment for ALL cycle lanes - STOP any vehicles parking in them!	Comment too generalised to assess in IDF.					
137	Felixstowe	Felixstowe, Undercliffe Rd at the Leisure Centre car park	Section of road (part of national cycle route 51) extremely dangerous for cyclists due to uncontrolled parking along the road on the Leisure Centre car park side.	Double yellow lines along this section of road on the car park side. Could provide some 30 minute free parking spaces in the nearby leisure centre and Convalescent Hill car parks to mitigate any impact on the businesses facing the leisure centre car park.	Double yellow lines have not been assessed in IDF.					

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138	Westerfield	Lower Road, Westerfield	Lower Road and Church Lane are used as a rat run by large numbers of motorists seeking a short cut to main routes West of Ipswich. This is made worse when there are closures of the Orwell Bridge. There is no footpath along much of this route, forcing pedestrians to mix with often speeding traffic. As a resident of the village, I know that a number of other residents are afraid to walk there, particularly the more elderly. This results in both unnecessary car journeys and social isolation.	My suggestion would be to make both Lower Road and Church Lane one-way for motor traffic, as there are viable alternative routes into and out of the village. Proper footways could then be installed and a contraflow cycle lane, preferably with grade separation, or, at minimum, flexible wands or similar.	Modal Filter					
139	Trimley St Martin	Morston Hall Road between Levington and Trimley	This is mostly a single track road with passing places used by cyclists as a commuting and leisure route between Ipswich and Felixstowe. The width of the single lane sections does not leave a lot of room for vehicles to overtake or for oncoming vehicles to pass and a large proportion of drivers see no reason to slow down when passing, so it can often feel unsafe for cyclists.	There is a very wide verge along the whole length of Morston Hall Road which could be converted to a dedicated cycle path or shared use path.	N/A	£1,000,000-£1,100,000	SCC, DFT	SCC, DFT	N/A	Beneficial
146	Otley	Gibraltar Road / Ipswich Road & Thomsons Lane	FYI - These three lanes have been proposed by Otley as potential 'Green Lanes' under SCC's latest initiative. They make an ideal cycle / walking /horse riding route between Otley, Ashbocking & Swilland avoiding the B1078 / B1077 & B1079 Road triangle.	Extend the 40mph Speed limit on the B1078 from Ashbocking towards Otley encompass the "Swilland" cross roads"....	Speed reductions have not been assessed for the IDF.					
147	Otley	Thomson's Lane, Otley.	FYI - Proposed by Otley as a potential Green Lane under the current SCC Initiative	Please support this proposal...	Quiet Lanes assessed by Quiet Lane project team.					
148	Otley	Ipswich Road, Otley	FYI - Proposed by Otley as a potential Green Lane under the current SCC Initiative	Please support this proposal...	Quiet Lanes assessed by Quiet Lane project team.					
153	Great Bealings	Seckford Hall Road (West of A12 Woodbridge)	Consider incorporating this lane into a designated cycle route from woodbridge to the Bealings and out lying villages.	Some sort of protected status such as Green Lane, no HGV' route, reduced speed limit, currently national speed limit status	Quiet Lanes assessed by Quiet Lane project team.					
157	Otley	Chapel Road, Otley	Land allocated for significant housing development within the village. Increases in the number of houses within the village will inevitably increase the amount of motorised traffic within the village, which in turn will make the roads feel less safe for cyclists, parents of children and other road users (Mobilty Scooters, Horse riders etc). This will have a detrimental effect on the plan to increase cycling and walking..	1. Install a 'Full sized' roundabout on Chapel Road at the point of this development (where the Primary School, Village Hall and Doctors Surgery are currently located). This would help significantly to reduce 'speeding' traffic along Chapel Road. 2. Reduce the Village 30mph speed limits to 20mph...	Highway matter not assessed by IDF.					

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158	Rendlesham	Rendlesham has no safe walking or cycling connectivity to anywhere else...	Rendlesham is accessible only from the A1152 - all entry/exits are along that road which has no foot/cycle path. There is no signage to indicate cyclists/walkers may be present. The speed limit of 40 stops before Rendlesham Mews - and is frequently exceeded by drivers who presume it's a safe-for-them straight stretch, they can see the upcoming increase of speed permission sign. Vehicles passing the Mews at 60 mph+ makes it unsafe for cyclists to turn into the Mews and lanes beyond.	Create a path along the A1152 to extend from the roundabout to the Mews. Extend the speed limit to 40 all the way to Eyke. This would remove the dangerous 60 stretch that includes turnings to the Mews and to the lanes that lead to Friday Street/the forest on one side and to Rendlesham St Gregory's Church/Campsey Ash/Wickham Market on the other. Put up signage on the A1152 that indicates to drivers that they are passing through a residential area where cyclists and walkers may be present.	N/A	£850,000-£900,000	Developer, SCC, CIL	S106, S278, CIL, SCC	N/A	Beneficial
160	Melton	B1438 Woodbridge to Wickham Market	This is a direct route between the two towns, avoiding the A12 Dual Carriageway. Local traffic uses this road in preference to the A12. With increased housing being seen in Wickham traffic levels will rise hence increasing the vulnerability of cyclists using this route, including any young persons wishing to cycle to/from school in Woodbridge.	Create a dedicated cycle lane the whole route, improve cycling related signage and reduce speed limits. Make Melton traffic lights a cycle friendly road junction and extend the cycle route up Woods lane to the Melton A12 roundabout (connect with existing cycle route/path). Continue the cycle route into Woodbridge via Melton hill as per other suggestions. Maybe connect it with a riverside foot/cycle path at Wilford Bridge	N/A	£1,300,000-£1,400,00	SCC	SCC	N/A	Beneficial
164	Melton	Between Woods Lane lights, Melton to Bromeswell Roundabout to Sutton Hoo	Road is extremely busy, narrow and has blind bends. It is the only way into Woodbridge (and beyond) for cyclists coming from villages on Bawdsey peninsula and yet there is no cycling infrastructure. The stretch between Melton level crossing and the junction on the Hollesley and Alderton roads near Sutton Hoo are particularly dangerous for cyclists with cars overtaking on blind bends and not giving space to cyclists.	Cycle lanes on all roads into Woodbridge from surrounding villages.	High	£3,500,000-£4,000,000	SCC, DFT, Developer	S106, S278, CIL, SCC, DFT	Access and junction improvements at Land at Woodbridge Town Football Club	Essential/Critical
166	Bromeswell	Road between Sutton Hoo and Rock Barracks	No pavement or cycle lane - vehicles travel extremely fast on this road (60mph) and yet there is no cycle lane or pedestrian route from the barracks into Woodbridge. Many people walk this route (especially from the Travellers Site) and it is very dangerous - especially in the dark. There should be a safe cycle route from all the villages into Woodbridge to enable people to commute by bicycle instead of driving, especially as the bus services are so infrequent and do not connect with trains.	Cycle lane from villages into Woodbridge plus pavement/pedestrian footpath between Barracks and Melton.	N/A	£1,800,000-£1,900,000	SCC, DFT	SCC, DFT	N/A	Beneficial
167	Otley	X-roads on B1078 with Gibraltar Rd. Otley and High Rd. Swilland.	V. dangerous junction because of speed of traffic and overtaking on B1078 .	Extend the speed limit of 40 mph at the Ashbocking x-roads so that it continues all the way to the 40 mph limit near Otley College.	Speed reductions have not been assessed for the IDF.					

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168	Hoo	Chimer Lane/Hall Lane/Honeypot Lane junction near Charsfield	This whole area not just this confluence of c -roads is an exceptionally rich completely rural area which offers outstanding cycling. The nature of the roads is that of restricted width and with many blind bends. Unfortunately motorists seem to think it is a racetrack and often are moving at unsafe speeds for cyclists. At least once in last month I have been almost brushed by a passing car at speed, unsafe for him/her and me	The diversity of nature is outstanding in this area. Just today cycling that route I encountered a young stag with approximately 8 points on his antlers, several buzzards, hunting; various other birds and rabbits. An upper speed limit of 40mph on such roads whilst not making them safe would reduce some of the risk. Could we have a countryside limit please in Suffolk or lobby for such nationally on roads of a diminished width?	Speed reductions have not been assessed for the IDF.					
170	Woodbridge	Cumberland Street	Drivers consistently ignore the time restrictions and use this route as a rat-run.	Turning the road into fully 1-way from North-East to South-West would reduce it's desirability as a rat-run - but continue to allow 2-way bicycle traffic	Modal Filter comments from the community comment section have not been assessed in IDF.					
171	Woodbridge	The Thoroughfare	Cars using the road as a rat-run	Reversing the one-way direction would remove the routes desirability as a rat-run.	Modal Filter comments from the community comment section have not been assessed in IDF.					
172	Aldeburgh	Aldeburgh...et al	Like many of our towns Aldeburgh high street is often full of cars...especially during holiday seasons..making life difficult for pedestrians, cyclists and mobility scooter users.	Promote the idea of regular car free days across the district....where cars are banned from the centre of towns such as Aldeburgh, Woodbridge, Southwold, Framlingham, Halesworth, Beccles, Bungay etc...Maybe one Sunday per month..in support of World Car free day..it works in London why not in Suffolk	No Cycling and Walking infrastructure improvement suggested.					
177	Clopton	B1078 junction with Manor Road at Clopton IP13 6QN	Traffic coming up the hill in Easterly direction is often speeding and also often overtakes on the brow of the hill where the driver can have no view of road ahead. At the top of the hill is a road junction, a blind corner, a village hall, a childrens' play area and a bus stop. Cycling and walking along this stretch of road is made suicidal by speeding traffic, and HGVs. It is necessary to cross this road to access local footpaths, the childrens play area and the village hall.	A speed limit through the village of 30mph would be a good idea to start with. At the very least, double white lines (no overtaking) up the hill to prevent blind overtaking would be a step forward.	Speed reductions have not been assessed for the IDF.					
178	Clopton	B1079 between Grundisburgh and Otley	Twisty narrow road with considerable lorry traffic is not safe for cyclists or walkers.	Newly developed cycling routes should avoid this road.	No Cycling and Walking infrastructure improvement suggested.					

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181	<Null>	Bridleways & Footpaths missing from mapping software	The mapping system does not appear to show 'bridleways' and 'footpaths'. Suffolk has many bridleways which make good offroad routes for walkers and cyclists both for leisure and for local use as connections to local services. The marker is tagging the end of bridleway that connects Gosbeck with Pettaugh as an example, this route is often overgrown and rutted by tractors.	Ensure that all bridleways (RUPP's, BOATs' et al) are maintained to a minimum standard of width and firm surface to enable cyclists and less abled walkers to use them safely.	Comment too generalised to assess in IDF.					
182	Otley	Footpath East of Otley Bottom	Footpath that runs from driveway of Chalet Bungalow at Otleybottom up hill (NE direction) and across to unamed road from Church Road is often completely overgrown, muddy and lacking any form of maintenance including repair of broken styles and signage.	Maintain footpath to a higher standard....this path represents a viable walking route from Suffolk Rural College to Otley Village.	Issues relating to maintenance have not been assessed for the IDF.					
184	Burgh	B1079, Grundisburgh to Otley	This particular section of the B1079 is a narrow, windy and undulating road and poses a real safety challenge to anyone wishing to walk, mobility Scoot, cycle or ride a horse along it. Its common to see organised 'charity' rides using it as part of their route planning to/from Woodbridge, which further puts cyclists at risk as well as making overtaking difficult for following vehicles.	1. Create one continuous 30mph speed limit along its length, Otley to Woodbridge. 2. Develop an alternative 'cycle' route via the parallel smaller lanes. 3. Encourage organised rides not to use this part of the B1079.	Speed reductions have not been assessed for the IDF.					
186	Waldringfield	Footpaths in and around Waldringfield, and elsewhere throughout East Suffolk	With the rising popularity of cycling we seem to have lost respect for the differences between footpaths and bridleways. Cyclists seem to no longer acknowledge that footpaths are not for cycling along, making it potentially dangerous for walkers and causing damage to footpaths. In the same way that cyclists wish to see improvements to the road infrastructure to feel safe from vehicles we need to acknowledge that there are similar issues on footpaths...which are NOT rights of way for cyclists.	A campaign of education about the differences between footpaths and bridleways coupled with improved signage and potentially sanctions for non compliance	No Cycling and Walking infrastructure improvement suggested.					
188	Corton	Hopton to North Lowestoft lack of a cycle route either along the A47, the coast road from Corton to Hopton or on bits of the old railway line.	There is no dedicated cycle route from north Lowestoft to Gorleston or Yarmouth. There is a dedicated cycle path alongside the A47 in Norfolk, from Gorleston to Hopton, after that there is nothing. Cyclists either have to go along the busy A47 or the coast road, which has high hedges, sharp bends and adds distance to the journey. This road is used by tourists staying at facilities in Corton and Hopton, who are not used to tight bends and cyclists. It is a real health and safety issue.	The options are either a continuation of the cycle path alongside the A47 from Hopton to the Corton Long Lane roundabout and possibly a spur off to Oulton Broad or a dedicated cycle route alongside the coast road.	Medium	£1,000,000-£1,100,000	SCC, CIL, NH	S278, S106, CIL, SCC, NH, DFT	Potential safety improvements to A47 to accommodate the North Lowestoft Garden Village (Policy WLP2.12) Cycle link between Lowestoft and Hopton	Essential

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191	Oulton Broad	Beccles Road to Suffolk Wildlife Trust's Carlton Marshes	There should be provision of cycle hire at Oulton Broad South railway station for visitors to the Carlton Marshes reserve who arrive by train, also a dedicated cycle route from the station to the nature reserve. This would assist ecotourism, visitor numbers to the reserve and assist locals cycling in the area as well.	Either a dedicated cycle route by the Angles Way route from the reserve to Oulton Broad or a dedicated cycle route along Beccles Road.	High	£950,000-£1,000,000	SCC	SCC		Beneficial
192	Saxmundham	B1121 between Benhal Saxmundham and Kelsale	Three villages cycle path	the three villages cycle path should be put in place ASAP	N/A	£1,300,000-£1,400,000	SCC, DFT, Developer, CIL	S278, S106, CIL, SCC, NH, DFT		
194	Framlingham	Framlingham - New Road to B1120 Brabling Green	Road is crying out to be a Quiet Lane. Heavily used by both cyclists and walkers pretty much the whole length. Also, the 60 mph speed limit should be reduced and appropriate signage installed at each end plus repeaters at appropriate intervals.	Road is crying out to be a Quiet Lane. Heavily used by both cyclists and walkers pretty much the whole length. Also, the 60 mph speed limit should be reduced and appropriate signage installed at each end plus repeaters at appropriate intervals.	Quiet Lanes assessed by Quiet Lane project team. Speed reductions have not been assessed for the IDF.					
195	North Cove	End of combined cycle-way/footpath from North Cove church to The Street	Cyclists exit the cycle way at speed without stopping to give way at the end sometimes going over the bonnets of cars travelling from the A146 towards Pinewood Gardens and Marsh Lane.	Just repainting the Give Way lines and triangle so that it shows up more to see if that helps resolve the problem.	Issues relating to maintenance have not been assessed for the IDF.					
197	Barnby	Barnby bends	The road is far too narrow and winding and it needs a cycle path/lane that follows the same route but takes cycles off the main road as it is dangerous and causes huge tailbacks. The only cycle route takes cyclists so far off this route that they just don't use it! I would not dare cycle to work because it is just dangerous and any other route is far too far round (via Mutford)	Totally bypass the Barnby bends and include a cycle path - this has been needed for decades! At least widen the road to include a proper cycle path on each side of the road	Very High	£1,800,000-£1,900,000 (cycle infrastructure only not highway)	SCC, DFT, CIL	SCC, ESC, DFT		Essential
199	Levington	Old Felixstowe Road (formerly A45) between Felixstowe Road/Seven Hills and Levington slip road off A14	Ideal stretch of road to introduce segregated cycle lanes &/or reduce speed limits &/or prohibit through vehicular movement other than if required for public transport or "Operation Stack" An alternative is needed to Cycle route 51 (via Stratton Hall, Levington Church and Nacton village, which although is a picturesque leisure ride, is considerably longer than the direct route, and is also quite hilly in several places	This was once the main A45 (now A14), the speed limit is still 60mph or 70mph in the dual carriageway near Bridge Road. This 2-mile length of road could be provided with a separated cycle lane in both directions &/or have the speed limit reduced to 20 or 30mph as it runs completely parallel with the A14 dual carriageway. I appreciate the road has historically been used for "Operation Stack", but Port of Felixstowe's Vehicle Booking System has largely removed the need for the road to be designated in this way 24/7/365.	Very High	£1,800,000-£1,900,000	Developer, SCC, NH, CIL	S106, S278, CIL, SCC, DFT	Significant access improvements and improvements to the wider land at Felixstowe Road (Policy SCLP12.21)	Critical
200	Melton	North of Melton Old Church	Road frequently flooded. This is especially dangerous for cyclists because there are often potholes that cannot be seen under the water. Also there is a thick layer of mud along the centre of the road. This is an important route for those wishing to cycle between Ufford and Melton/Woodbridge.	Flooding and mud has been reported numerous times but SCC Highways have failed to provide any drainage.	Highway matter not assessed by IDF.					

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201	Bredfield	Junction of A12 and New Road between Melton and Bredfield	At busy times it is very difficult and hazardous for cyclists to cross the A12 when travelling between Melton and Bredfield. The A12 carriageway is very wide at this junction	Provide central reservation for cyclists and pedestrians. This could also make the junction safer for motorists.	Not assessed for IDF.					
203	Rendlesham	Rendlesham to Woodbridge A1152 Road	Provision of a dedicated cycle lane/path. With the intended major housing development at Rendlesham, it will only serve to increase the amount of motorised traffic travelling to and from Woodbridge via Wilford Bridge. This will actively discourage people from cycling.	There is a huge opportunity for a dedicated cycle/footpath lane to be established along this road to encourage people to cycle to/from Woodbridge rather than use their cars. (Similar maybe to the one already in existence between Leiston and Sizewell) There is plenty of room and it could easily connect with other cycle / walking infrastructure at Woodbridge. As well as use for local journeys such as cycling to school it would also be useful for leisure / tourist cycling connecting Woodbridge with the Rendlesham forest area and the coast	N/A	£3,600,000-£3,700,000	SCC, DFT	SCC, DFT		Essential
204	Woodbridge	The Thoroughfare, Woodbridge	This is a narrow ancient street where cars, pedestrians and cyclists are not segregated. Despite the no access to vehicles at certain times, restriction cars and delivery vehicles are still ignoring this, creating a conflict particularly between pedestrians, mobility scooters and vehicles.	Install 'pop up' barriers/bollards at the Melton End (& retain existing one way system) as per the centre of Cambridge to remove all non essential motorised traffic from this street completely. This would make the whole Thoroughfare a more pleasant place to 'be in' both for local residents, shoppers, and visitors to Woodbridge. Deliveries to shops could be made overnight, emergency services could have transponders...it works in Cambridge why not Woodbridge or indeed other East Suffolk towns which have a 'thoroughfare' style main street.	Modal Filter comments from the community comment section have not been assessed in IDF.					
207	Snape	Cycle route Snape to Aldeburgh avoiding A1094	Cycling along the A1094 can be perilous at times and not encouraging for inexperienced/young cyclists	Consider upgrading the Suffolk Coastal Route path from Snape to Aldeburgh to a 'gravel' cycle/footpath path from Snape, through marshes to the western fringe of Aldeburgh, continue 'cycle/footpath' into town centre.	N/A	£1,300,000-£1,400,000	SCC, DFT	SCC, DFT	Access and junction improvements at Land rear of Rose Hill	Essential/Critical
209	Hollesley	The road to Shingle Street	The road is very congested and during the summer a huge number of cars park on the verges, ruining the unique beauty of the beach and marshes. It is difficult and dangerous for walkers and cyclists to navigate the traffic.	The road should be used by vehicles only for access to the homes at Shingle Street. Visitors should be required to park at the Shepherd & Dog pub or the Suffolk Punch Trust and walk or cycle to the beach. Bikes and trailers could be offered for hire to raise funds for the community, and the Trust, village shop and pub would also benefit from increased footfall in the village.	Modal Filter comments from the community comment section have not been assessed in IDF.					
210	Stratford St Andrew	Where the cycle route crosses the A12 just west of Farnham (Tinker Brook)	The 30mph limit stops just short of this crossing. If it was extended a 100 metres or so toward Glemham it would be safer to cross the A12 by bicycle.	The 30mph limit stops just short of this crossing. If it was extended a 100 metres or so toward Glemham it would be safer to cross the A12 by bicycle.	Speed reductions have not been assessed for the IDF.					
211	Cransford	Bannocks Lane Cransford	This is on a marked cycle route. When the road was resurfaced pot holes were not filled prior to coverage with chippings. This makes the pot holes more dangerous as it is much more difficult to see them. This applies in many other areas of the region and is potentially very dangerous both to cycles and cyclists.	All pot holes should be repaired prior to any surface dressing being applied. Contractors work needs to be thoroughly checked by council officials.	Issues relating to maintenance have not been assessed for the IDF.					

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212	Otley	Thompson Lane Ashbocking/Otley	Road surface is falling apart making it difficult to cycle	Resurface and reduce crowning/camber to make cycling safer	Issues relating to maintenance have not been assessed for the IDF					
213	Melton	River Wall path between Wilford Bridge and Woodbridge	This is currently a footpath, but could be changed to allow bikes.	Keeping the current surface would help to limit bike speed. Having a green cycle route between Melton& Woodbridge would provide relief from the poor road conditions.	Not assessed for IDF.					
218	Westerfield	Westerfield Business Centre / Station	Possible site for an Ipswich northern 'Park & Cycle' car park. There is nowhere to park when using Westerfield Station.	Given the emerging development north of Ipswich this would make a good spot for a park,ride and cycle carpark similiar to those seen around the fringes of Cambridge. This would enable those of us travelling into Ipswich from the North (aka East Suffolk District) to park up and then either use the train to go northward towards lowestoft or cycle(or walk) or bus the short distance into the middle of Ipswich.	Not assessed for IDF.					
219	Lowestoft	The Road surface between The Falcon Public House and Mariners Street.	The road surface heading south as you leave the cycle lane and head passed the Falcon public house is unsuitable for road bikes. It has been patched hundreds of times over a period of many years and is now unfit for cycling without a mountain bike.	The road needs resurfacing.	Issues relating to maintenance have not been assessed for the IDF					
220	Lowestoft	At the junction of Sussex road and Yarmouth road.	After some light rain the road here floods because of an ongoing problem with drainage. unfortunately there is a serious pothole next to a sunken drain cover which can end up submerged. If a cyclist was to ride through the flood and hit the pothole the accident would be serious.	This has been reported to Highways on a number of times with little effect. The flooding has been continuous for many years. You wouldn't think it would be too hard to drain an area like Yarmouth road which is on the top of a hill! (The Ravine). it needs a new drain and the pothole filling before someone gets hurt.	Highway matter not assessed by IDF.					
221	Lowestoft	Cycle Lane on Corton Road	There is a designated Cycle lane running the length of the Corton Rd, that no one can use because there are always cars parked in it. It feels dangerous as a cyclist to have to constantly overtake these parked vehicles without a designated Cycle Lane.	Move the cycle lane to the outside of the parked vehicles as they do in Holland, and similar to the High Street outside the Lighthouse.	Not assessed for IDF.					
222	Blundeston	Lowestoft road coming into Blundeston Village	The walking/cycling links into and out of the village are awful, especially for kids who frequently use this road to access the skate park in the summer and vice versa with those venturing out. A pathway along the entire road would vastly improve access out of the village for those of all ages. There is a large development of houses about to be built near that road, meaning this worse is even more essential.	Investigate the safety of pedestrians in Blundeston entering and existing the village, especially children. Think about how it could improve social isolation. Also factor in this matter when giving permission to large housing developments.	N/A	£850,000-£900,000	SCC	SCC		Essential

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232	Swilland	B1078 & Swilland Crossroads	Turning right off the B1078 for cyclists is perilous, particularly during the rushhour periods when the B1078 is busy with streams of vehicles travelling at the speed limit which at this point is 60mph. Its noticeable that there is a tendency amongst some motorists to overtake at speed along this stretch into the face of oncoming traffic which if you are a cyclist or walker is actually terrifying....Traffic does not 'naturally give way' to anyone attempting to walk along the road.	As a minimum the Ashbocking 40mph limit should be extended to the College 40mph to create one continuous 40mph limit	Speed reductions have not been assessed for the IDF.					
233	Otley	Chapel Road, Otley	The School, Village hall and Doctors surgeries are all co-located at this point on Chapel Road. These are magnets for cars particularly at drop off times, this creates an area of local congestion and conflict with pedestrians particularly those with children trying to cross the road or indeed cycle to the school. Through traffic travelling at speed compounds the safety risk as the village hall carpark (which is used as the school drop off area) exit/entrance is on a blind bend.	Given the potential of further significant housing development in this area it would make sense to create a roundabout at this point giving safer access to the Hall carpark and Doctors surgery and also serve to calm the through traffic on Chapel road, a carpark within the development would also ease the congestion and provide some public off street parking within the village.	Highway matter not assessed by IDF.					
238	Woodbridge	The junction of Warren Hill Road with Ipswich Road.	When cycling up the hill from the Cherry Tree Road mini roundabout it is extremely difficult and dangerous to move across in order to turn right into Warren Hill Road. When waiting at the junction in the middle of the road for a gap in the traffic in order to turn right is very hazardous.	Road markings need to mark out a right turn lane and a illuminated bollard would provide some protection/safety when waiting to turn.	Traffic management not assessed by IDF.					
239	Woodbridge	The traffic lights at the junction of The Thoroughfare and Melton Road.	When cycling into Woodbridge you may need to turn right at these traffic lights to either go straight over into the Thoroughfare or right into St.Johns Street. There is nothing marked on the road to show where cyclists should wait and nothing to protect you from oncoming traffic. The filter system of the lights often mean that you are waiting in the middle whilst traffic squeezes by on your inside and is also passing you on the other side.	A space for cyclists to wait, a bollard to protect and make traffic keep their distance. A mini roundabout may help.	Traffic management not assessed by IDF.					
240	<Null>	Along A1071between hadleigh road and A1214	No cycle route provided along this way for cyclists coming from south of ipswich and needing to get to hadleigh road.	With new estate being built a route through could be planned there is an existing foot path across fields that could be upgraded or an extra lane on either side of the existing A1071	Outside East Suffolk so not scored for IDF.					
241	<Null>	Underpass under the A14	Lack of cycling access through to sproughton meaning cyclists either have to go to central ipswich or the very busy Sproughton high street if attempting to get to the Sproughton road/Morrisons areas of ipswich	The current underpass be redesignated as having cycling access, and the steps on the hadleigh road side replaced with a ramp which will help cyclists, pedestrians with pushchairs/trolleys and those with walking difficulties	Largely outside East Suffolk so has not been assessed for IDF.					

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243	Aldringham Cum Thorpe	This whole redundant railway line should be surfaced and rebuilt as a cycleway between Leiston and Aldeburgh	Could be a dedicated cycleway with funding from the windfarms perhaps? You know - like a proper dedicated route like they have in other parts of the country.	Could be a dedicated cycleway with funding from the windfarms perhaps? You know - like a proper dedicated route like they have in other parts of the country.	N/A	£1,200,000-£1,300,000	SCC, DFT	SCC, DFT	N/A	Beneficial
247	Wantisden	Future Rendlesham / Bentwaters Development	Lack of Public Right of Way's connecting 'Rendlesham' to 'Rendlesham Forest', Wantisden, Butley and the coast.	1. Consider running a new cycle/footpath across Bentwaters Airfield to connect Rendlesham Housing estates with Wantisden Corner road. Provides an off road walking route and removes the need for cyclists to use the local 'B roads'. 2. Consider upgrading the 'path' that runs across the eastern end of the runway towards Friday Street.	N/A	£1,000,000-£1,100,000	Developer, SCC, CIL	S106, S278, CIL, SCC, DFT	N/A	Desirable
249	Bucklesham	Levington Lane & crossing the A14 at this point	There is a public right of way that crosses the A14 (Levington Lane) at this point via a gap in the central reservation. It is possible to get across without being killed but you have to be quick.... The A14 verges are often overgrown....	Tidy verges so that there is better visibility of the crossing.	Issues relating to maintenance have not been assessed for the IDF.					
251	Nacton	A1156 Nacton to Warren Heath Ipswich	Limited cycle path from Seven Hills / Nacton into Ipswich	Consider providing a full cycle/footpath all the way from Nacton (even Seven Hills Junction) towards Warren Heath (Past the Show Ground)	N/A	£1,600,000-£1,700,000	Developer, SCC, CIL	S106, S278, CIL, SCC, DFT	N/A	Essential
253	Kettleburgh	Easton to Kettleburgh Road, big dip in road about 0.75m from verge going up the hill into Kettleburgh, catches cyclists and motorbiked out.	Raise grate and level road	Raise grate and level road	Highway matter not assessed by IDF.					
255	Bromeswell	Wilford Bridge Melton	This is a dangerous road to cross for pedestrians using the footpaths either side of the river and also bad for cyclists too.	Slowing traffic down so pedestrians get a chance to cross the road ,or narrow the road to slow traffic down and widen the pavements which could then accommodate a bike lane.	Speed reductions have not been assessed for the IDF.					
263	Martlesham	The entirety of the Martlesham retail development.	There is no pedestrian walkways between the myriad of large shops on the new retail development at Martlesham. Whilst the lack of footpaths was acceptable when this was a mainly commercial area, the explosion of retail outlets and consequential increase in footfall has meant both pedestrians, cyclists and motorists are now at considerable risk as they move about this area.	Provision of a complete footpath network linking all the parking and shopping areas such that by parking anywhere within the retail park area you can walk to any of the retail stores without having to walk along a roadway, with safe crossing places provided where any paths ways cross the road network.	Comment too generalised to assess in IDF.					
264	Martlesham	General consideration of the motorist as a part of the cycling and walking strategy	The growing positive bias in Council policies and strategies towards walking and cycling seems at times to be bordering on a demonisation of all motorists. Any new initiatives should take into account Suffolk's rural environment and the need for many people - including the aged or disabled - to make journeys that are not viable on foot or by cycle. These people and their needs do not seem to be given due consideration in some of the rushed often ill-conceived initiatives that are proposed.	Ensure full and due consideration is given to all classes of road users when creating any schemes that seek to offer improvements to the built environment. Fulfilling the demands of any particular pressure group will undoubtedly lead to a less than optimum solution for the general populous who after all are the majority... In respects to all proposals there should be full consultation with all user groups prior to any initiative being taken forward, its especially important to reach out proactively to those who do not have the technical knowledge or access to the mainly internet focused mechanisms that currently form the backbone of the consultation process.	Comment too generalised to assess in IDF.					

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267	Trimley St Martin	Capel Hall Lane/Brook Lane/Back Lane/Lower Road	Create a network of Quiet Lanes between Trimley St Martin (Capel Hall Lane) and Falkenham Church via Brook Lane/Back Lane/Lower Road/Falkenham Sink	As above - requires only designation and signage.	Quiet Lanes assessed by Quiet Lane project team.					
269	Woodbridge	The length of the Woodbridge Thoroughfare.	Frequency and speed of traffic is unacceptable and totally unreasonable.	Vehicles & cycles need to be banned and the Thoroughfare made pedestrian only. Residents would need to be given access at certain hours. The car park could increase disabled parking to assist but at present the speed and frequency of traffic is unacceptable and totally unreasonable. There are plenty of examples of where this has been successfully implemented.	Modal Filter comments from the community comment section have not been assessed in IDF.					
270	Trimley St Mary	Trimley St Mary Bridleway 14: Clickett Hill Road to Nicholas Road	The area immediately to the west of Clickett Hill Road becomes very damp and muddy over the autumn-winter-spring period and needs to be surfaced - as part of Suffolk Cycle Route 5	As above	Highway matter not assessed by IDF.					
271	Woodbridge	Willford Bridget to Martlesham creek. Waldringfield along the river front to Woodbridge	We walk these areas and are passed by cycles on these footpath routes, it is a bone of contention for walkers and cyclist. In Scotland I believe that footpaths can be used by cyclist as well as walkers, why can we not just adopt this policy, The paths can be used by both as long as cyclist pass with caution and slow down. I like to cycle also but in Woodbridge we are restricted to the roads as the only safe cycle route is by the bypass, and you have to cycle the roads to get there.	solution make the footpaths for cycles as well, with the emphasis that the walker has the right of way with the cyclist either dismounting or passing with care.	Not assessed for IDF.					
275	Bredfield	Pavement through Bredfield	Much of the "pavement" is now too broken or overgrown for safe walking, particularly for anyone with a buggy, a wheeled walker. or a wheelchair People are forced to walk in the road.	The "pavement" needs to be resurfaced and parts of it need to be remade.	Issues relating to maintenance have not been assessed for the IDF.					
280	Wissett	A separate cycle/pathway along the south side of Halesworth Road from Wissett to Halesworth.	A separate cycle/walkway alongside the Halesworth Road from Wissett to Halesworth would make walking and cycling a lot safer for non-vehicle users along this narrow twisty country road which has a high bank and big hedges along its northern side. Many potential users do not use this route due to its obvious dangers for walkers and cyclists.	A separate cycle/walkway alongside the Halesworth Road from Wissett to Halesworth would make walking and cycling a lot safer for non-vehicle users along this narrow twisty country road which has a high bank and big hedges along its northern side. Many potential users do not use this route due to its obvious dangers for walkers and cyclists.	N/A	£850,000-£900,000	SCC	SCC	N/A	Beneficial

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284	Wissett	Halesworth Road from Wissett to Halesworth is very dangerous for cyclist and pedestrians	This Halesworth Road is narrow, twisting and bounded by a high bank on the north side. There is space on the south side of this road for a dedicated cycle/pathway which would encourage more people to cycle or walk the short distance into Halesworth. Currently it is too dangerous, except for the brave and the foolhardy to risk it. The number of bends means that drivers are often suddenly confronted with a walker or cyclist in a road that is only just wide enough for two cars	This Halesworth Road is narrow, twisting and bounded by a high bank on the north side. There is space on the south side of this road for a dedicated cycle/pathway which would encourage more people to cycle or walk the short distance into Halesworth. Currently it is too dangerous, except for the brave and the foolhardy to risk it. The number of bends means that drivers are often suddenly confronted with a walker or cyclist in a road that is only just wide enough for two cars	N/A	£850,000-£900,000	SCC	SCC	N/A	Beneficial
290	Kesgrave	The service road/cycle lane that runs the southern length of Main Road A1214 along the settlement boundary of Kesgrave.	The cycle path was created from a service road with pedestrian access to shared cycle use. Due to neglect it is unfit for purpose and is dangerous and therefore unused. The surface is poor and the many side roads are hazardous. Cars frequently drive straight out over the cycle path exiting shops/garages. Give Way signs have worn away or are non-existent. Cars park on it (esp near shops and school) again making the case for cyclists to choose the main road.	This is a golden opportunity to do something to put cycling and walking at the centre of transport policy for the future while not actually preventing other road users having access. The land is there to be properly utilised and turned into a modern cycling freeway on a major through route into Ipswich. It needs real imagination and investment.	High	£1,900,000-£2,000,000	SCC, CIL	SCC, CIL	N/A	Essential
292	<Null>	All Schools.	If we want to increase safe cycle usage it should start with young people so that it becomes absolutely normal to cycle, and especially to school.	I suggest that all schools have a cycling policy produced by stake holders eg teachers, parents, students, police, local council, etc. The policy would include among other things: Suggested safe routes to school from all the main centres of population that feed into the school. And perhaps roads that should be avoided as unsafe for cyclists to use. The council should consider providing suitable signage for cyclists and other users along the routes. Safe dry cycle storage within the school. Safe storage of helmets, hi-vis clothing. Cycling competency certification schemes.	Comment too generalised to assess in IDF.					
293	Halesworth	A144 roundabout joining Quay Street and Saxons Way (Hooker House), up to the Triple Plea Roundabout where Sparrowhawk Road joins the A144 Norwich Road	The current main south-north cycle and pedestrian route up Norwich Road to businesses to the north of the town, and importantly to the Edgar Sewter Primary School, is dangerous, too complex (multiple road crossings with varying priorities) and does not serve the primary school for sustainable transport	From the Norwich Road/Quay Street roundabout (A144), move the existing cycle route from the east side of the A144 across to the west. Create a 'Copenhagen' or similar vastly improved crossing at Wissett Road junction, widen what would become the shared pedestrian/cycle path on the west side, remove all existing parking where necessary on the west side (especially near Wissett Road junction, and up A144 past the police station), and replace with single yellow lines with waiting limits of 1 hour (to support school visits and drop-offs). This route must link from the Quay Street Hooker House roundabout up as far as the Sparrowhawk Road roundabout near the Triple Please Road and pub. Suggest NCR1 route is also amended to utilise this new safer less complex route, once established, and once connected to other proposals entered onto the interactive map. Agreed with the Halesworth NPSG Cycle Advisory Team	N/A	£850,000-£900,000	Developer, SCC, CIL	S106, S278, CIL, SCC	N/A	Essential

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304	Halesworth	Halesworth - provide new 20mp speed limit through town to calm traffic and promote safer cycling and low speed vehicle use	Unlike many Suffolk and National towns and villages, Halesworth has not reduced speed limits to 20mph even outside the Edgar Sewter Primary School. This is creating direct danger to cyclists and pedestrians alike, particularly being combined with very poor parking practices in London Road, Norwich Road, Holton Road and Quay Street.	Halesworth requires traffic calming/slowing measures, and the popular and effective way like other nearby market towns would be to provide 20mph speed limiting as follows: 1. The main A144 north-south route from Bramfield Road/London Road junction (Kerridges garage) all the way along London Road, Saxons Way, and Norwich Road as far north as "The Avenue". 2. Eastwards from the Norwich Road Hooker House roundabout along Quay Street and Holton Road, as far as "Castle House" at the top of Holton Road hill. 3. Westwards from the Angel Link roundabout and London Road (Coop roundabout) to the junction of Roman Way and Chediston Road. 4. Roman Way from its junction at Chediston Road, to the junction at London Road near the Rifle Hall.	Speed reductions have not been assessed for the IDF.					
306	Halesworth	Halesworth - Remove parking and apply waiting limits to Norwich Road between its junctions with "Wissett Road" and "The Avenue"	Current unrestricted parking is posing an immediate safety hazard to other road users - cyclists and people/children crossing Norwich Road. It is believed the current prolonged parking may be businesses and Police Station employees. Current parking risks doors being opened into other road users' paths, and pedestrian/children crossing between parked cars onto the main through-route including HGVs, is very dangerous. This is a site of previous cyclists being knocked off cycles by cars.	Provide double yellow lines between Wissett Road junction and opposite Hammonds Ford Garage, and from there northwards to the junction with "The Avenue" provide single yellow line restricted parking for 1 hour to enable school drop-off and school visit parking.	Double yellow lines have not been assessed in IDF.					
308	Alderton	Alderton Road/Hollesley Road between the two villages (60mph section).	This is, not unreasonably, a 60mph stretch of road, so has fast cars upon it. It is, however, too narrow in all places to allow vehicles to pass at speed, let alone for cyclists to be/feel safe.	There appears to be significant potential on farmland on the east side to both expand the road and to add a cycle/footpath adjacent to the road.	N/A	£1,200,000-£1,300,000	SCC	SCC	N/A	Beneficial
310	Walpole	Heart of Suffolk - Cycle loop passing through Halesworth, Framlingham, Debenham, Eye, Hoxne and Bungay requires improved signage and route granting	This beautiful previously published loop ("The Heart of Suffolk") passes through unspoilt countryside on minor roads and passing churches and other historic points of interest, linking several old market towns. The brown waymarked signs has fallen into real disrepute over the last 5 years or so, and should be granted a formal county route number plus get better signage. The loop can boost local tourism and cafe/craft visits along its whole length.	Review the whole loop and grant a formal route 'number' for the county. Replace existing deteriorated and erroneous direction signs, and republish the loop on an appropriate map and/or website to include GPS files which can be downloaded by other cyclists. Promote links to nearest rail and bus services enroute, to ease the way for shorter distance or less able cyclists. The originator of this request has cycle navigation files which could be used as a basis for publicising online via relevant cycling internet sites.	No Cycling and Walking infrastructure improvement suggested.					

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311	Holton	Halesworth - suggested new waymarked county cycle loop (Halesworth, Beccles and Bungay)	This suggested loop follows all back/minor roads and links three prominent market towns, plus would join the route from Beccles to Southwold at Stoven/Sotterley. It would enable joining the loop by train links at either Halesworth, Brampton or Beccles	The originator has navigation files that could be used to illustrate and publicise this route which is a family-safe and beautifully scenic route that can be done in parts or as a whole (total 35-40 miles). Heads north from Halesworth through Holton, Brampton, Stoven, Sotterley, Ellough, Beccles, Ringsfield, Ilketshall St Andrews, Mettingham, Bungay, St Peters, St Margarets, Rumburgh and back to Halesworth. Granting of a formal route number and signage would be required - navigation files are available for this very safe route that also piggy-backs a part of NCR1.	Not assessed for IDF.					
312	Felixstowe	Traffic light controlled cross roads of Langer Road and Beach Station Road, Felixstowe.	The traffic lights are activated by sensors in the road. However, they are not activated by cyclists. If a cyclist approaches the junction during quiet times, they face the choice of either waiting for a car to come along and activate the sensor, or jumping red lights. It is incredibly frustrating watching the lights on the intersecting road change through multiple cycles of green orange and red whilst the lights controlling your own progress remain fixed on red.	The sensors need either to be adjusted to ensure that a lone cyclist will be detected and will activate the traffic lights, or the whole system needs to be changed to a simple timer with the requirement for a vehicle to activate a sensor being dispensed with completely.	Highway matter not assessed by IDF.					
313	Felixstowe	Cross roads controlled by traffic lights, at High Road West and Garrison Lane, Felixstowe	The traffic lights are activated by sensors in the road. However, they are not activated by cyclists. If a lone cyclist approaches the junction during quiet times, they face the choice of either waiting for a car to come along and activate the sensor, or jumping red lights. It is incredibly frustrating watching the lights on the intersecting road change through multiple cycles of green orange and red whilst the lights controlling your own progress remain fixed on red.	The sensors need either to be adjusted to guarantee that a lone cyclist will be detected and will activate the traffic lights, or the whole system needs to be changed to a timer with the requirement for a vehicle to activate a sensor being dispensed with completely.	Highway matter not assessed by IDF.					
315	Felixstowe	The bridleway which passes Hill House Cottages and Candlet Farm between Gulpher Road and Thurmans Lane	Someone else has suggested diverting cyclists from the High Road to this bridleway. This would be a significant and grossly unreasonably lengthy diversion for cyclists needing to transit between eastern Felixstowe and Trimley. That said, the improvement of the bridleway is a good idea to benefit cyclists who already use it, but it should not be on condition that cyclists who would otherwise use the High Road being expected to divert, as the likely net result would be a reduction in cycling.	Someone else has suggested diverting cyclists from the High Road to this bridleway. This would be a significant and grossly unreasonably lengthy diversion for cyclists needing to transit between eastern Felixstowe and Trimley. That said, the improvement of the bridleway is a good idea to benefit cyclists who already use it, but it should not be on condition that cyclists who would otherwise use the High Road being expected to divert, as the likely net result would be a reduction in cycling.	No Cycling and Walking infrastructure improvement suggested.					

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316	Trimley St Mary	Level crossing from Fagbury Road	On occasions the gates governing access across the level crossing are electronically locked for no apparent reason. It is not seem possible to predict when this may occur. This results in a significant detour to the nearest available level crossing which is a considerable distance away. The risk is that frustration will lead to persons crossing the railway when unsafe to do so.	If there is a need for the gates to be temporarily locked for safety reasons, there needs to be a way for a pedestrian or cyclist to find out how long the delay will be and/or to contact someone in control of the locking mechanism to request access.	No Cycling and Walking infrastructure improvement suggested.					
318	Purdis Farm	Bike paths via Murrills Road park	The barriers at Murrills Road & Bucklesham Road are tight to get a cargo bike through. Cars are often parked at the Meadow Crescent entrance/exit.	Increase gap of barriers at Murrills Road & Bucklesham Road. Add 2m of double yellow line at Meadow Crescent.	Not assessed for IDF.					
320	Trimley St Mary	High Road Trimley nr Faulkeners Way	Cars parked in cycle lane and even on cycle path approaching mini roundabout.	Solid white lines and no parking in bike lanes with enforcement.	Not assessed for IDF.					
322	Felixstowe	High Road East, Felixstowe, & out through Trimleys	Cars regularly parked in cycle lanes	Change from dotted to continuous white line and enforce no parking in bike lanes.	Not assessed for IDF.					
325	<Null>	Widen and improve the current footpath to make it a shared pedestrian and cycleway.	Cyclists are currently sharing a dual carriageway with fast moving traffic.	If the path was widened to make a shared footpath/cycleway, it would to separate cycles from traffic using the dual carriageway. This would be especially effective where slow moving cyclists are riding up the hill from Ipswich to Copdock.	Outside East Suffolk so not scored for IDF.					
326	Melton	New Housing development, Woods Lane Woodbridge	Example of where significant new housing has been allowed without provision for safe cycling to the local shops, centre of Woodbridge and the local primary school. The housing is disconnected from Woodbridge by the A12 & busy Woods lane, necessitating car ownership to access local services.	1). Upgrade the footpath along Bredfield Road into Woodbridge to cycle/footpath standard. 2.) Create a cycle route down Woods lane to the Melton Traffic lights to connect with Melton Road	Very High	£800,000-£850,000	SCC	SCC	N/A	Beneficial
327	Playford	Playford Road - west of its junction with Butts Road.	Playford Road used by motorists wanting to avoid speed limit on A1214 making it unpleasant and less safe to cycle as many of them drive far to fast.	This route was really popular during the lockdown when there was much less traffic and cyclists felt safe. Closing the road here and at junction further east would provide an excellent cycle route to Woodbridge and yet allow motorists to travel between Playford and/or Bealings and the A1214.	Modal Filter comments from the community comment section have not been assessed in IDF.					
333	Southwold	Southwold	At the present time the only cycle lane 'in' Southwold is the approach road from the Lowestoft Road junction to the North Road junction. This is completely useless as it is not a solid white line hence parking seems to be acceptable anywhere along it thus completely stopping cyclists from using it and further increasing the hazard of an accident as they swing out round parked cars. Southwold has a problem with speeding which is never picked up by the local town council.	I suggest the cycle lane be removed as it serves no purpose and a strictly enforced 20mph speed limit be put in place from St Felix School and also implemented in Reydon to make sure the whole, very popular cycling and walking area, is safer for cyclists and pedestrians alike.	Speed reductions have not been assessed for the IDF.					

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334	Westerfield	Westerfield Business Park/Westerfield Station	With reference to the comment of having a cycle park for using the railway , the last time I wanted to use it to take my cycle to Woodbridge I found that the majority of Lowestoft trains do not stop at Westerfield. Could there be liaison with the railway companies to make Westerfield Station at least a request Halt for cyclists to use all trains.	Request to make Westerfield Station at least a request Halt Station for all users.	A matter outside the Strategy and not assessed in the IDF.					
335	<Null>	Cycle paths in Ipswich	There is a lack of clarity in Ipswich as to where cycle paths begin and end and which footpaths are shared space.	Paint all cycle tracks to increase visibility for pedestrians and cyclists	Largely outside East Suffolk so has not been assessed for IDF.					
336	<Null>	The junction between the Market Place and Bridges Street and the contraflow cycle lane.	The junction going uphill is rather dangerous because cyclists must give way to unpredictable traffic. The turn from the market place makes larger cars/vans/lorries swing into the cycle lane round a blind corner. The 20 mph speed limit in Bridge Street is frequently ignored. Cars and vans park in the cycle lane, pushing cyclists into the path of oncoming traffic. Bridge Street is a rat run for traffic going to Norwich. The noise levels and vibration are unacceptable.	"No Entry (except cycles)" at the Market Place/Bridge Street junction, preferably with a planter partially blocking the access for vehicles. "Access to Bridge Street via Nethergate Street" , enabling deliveries and residents access while quietening the road. Widening the pavements, initially with paint and identified loading bays to enable street life to take place safely.	Modal Filter comments from the community comment section have not been assessed in IDF.					
339	Trimley St Martin	Cycle path alongside A14 dual carriageway near Morston Hall Road	Using this cycle path is unpleasant and very scary being so close to fast moving traffic on the A14 with NO crash barrier. I prefer to use Morston Hall Road but this is not wide enough for cars to pass cyclists.	Provide a cycle path adjacent to Morston Hall Road away from A14.	N/A	£1,000,000-£1,100,000	SCC, DFT	SCC, DFT	N/A	Beneficial
343	Kesgrave	Cycle lane along Woodbridge road east	The cycle path/lane on the pavement along woodbridge road is a joke: it is old, raid surface is terrible, too narrow and occupied by pedestrians, blocked by driveways making it very dangerous and cars d not stop	The cycle path/lane on the pavement along woodbridge road is a joke: it is old, raid surface is terrible, too narrow and occupied by pedestrians, blocked by driveways making it very dangerous and cars d not stop	Issues relating to maintenance have not been assessed for the IDF.					
346	Aldeburgh	Between Aldeburgh and Thorpeness	As in a previous comment, the road is unsuitable for riding a bike comfortably, safely and pleasantly. The path is really a footpath not a cycle path. Shared use paths are against LTN 1/20 so the best thing to do is build a new cycle only path. This will be welcomed by people who walk and cycle there.	So that the new cycle path has greater currency, there is a need to link with cycle routes at either end. If there aren't any, then either build them or designate a new route using existing infrastructure.	N/A	£1,300,000-£1,400,000	SCC, DFT	SCC, DFT	N/A	Desirable
348	<Null>	Ribbons Park Development, Ipswich	Exemplar & Award winning example of a new housing development with a Modeshift STARS "Residential Travel Plan" https://www.modeshiftstars.org/first-residential-development-achieves-national-stars-accreditation/	This requirement should be included with all new housing developments within Suffolk.	Comment too generalised to assess in IDF.					

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351	Tunstall	main road between Rendlesham and Tunstall	It is too dangerous for children even with adult supervision to cycle to Rendlesham school from Tunstall and Blaxhall. Road is very busy and has narrow 2 lanes with limited visibility due to the bends.	Off road cycle path would be best solution this could also be extended to Tunstall Forest where the Viking cycle trail is located allowing the public to cycle there instead of having to take their bikes on vehicles.	N/A	£1,100,000-£1,200,000	SCC, DFT	SCC, DFT	N/A	Desirable
354	<Null>	Riverside Beccles	The path becomes very muddy in autumn and winter. It would be excellent if path could be maintained ie adding grit or building a broadwalk. This would encourage many more people to use the path.	Add grit or build broadwalk	Not assessed for IDF.					
355	Woodbridge	The whole of the river path from Martlesham to Melton is unsuitable for dual use (pedestrians and cyclists). Cyclists are currently prohibited, but very few take notice of the fact and push past	The path is only just wide enough for pedestrians to pass in a lot of places. To widen it to the necessary regulation width for dual use would likely not be possible and would also spoil the area. Enforcement is necessary before someone is seriously injured.	enforcement action against cyclists using the path	A matter outside the Strategy and not assessed in the IDF.					
356	Martlesham	Cycle lanes anywhere in the east suffolk region	Can you make sure that any cycle lanes (road or pavement) that are installed are to the regulation width and not too narrow to use (some parts on Felixstowe Road Martlesham are about 60cm). If any of the plastic wands/bollards are used then the 2m width of the cycle lane should be used. I have a tricycle and cannot use the lanes in Ipswich which have wands installed without either hitting the kerb or wands as they are too narrow,	keep to the planning guidelines and standard for all cycle lane provision. That way motor vehicles can give some clearance to cyclists, even if driving right onto the white line or wand	Comment too generalised to assess in IDF.					
357	<Null>	All over Suffolk	Your footpath signs are rubbish, they keep falling over and have to be reported and a worker brought out to stand them up again. Change to metal? Sit them inside some kind of flange plate with soil on top. Label with the footpath number. Could even have suggestions where they lead to! Look at Kent system.	As above	Comment too generalised to assess in IDF.					
358	<Null>	All over Suffolk	Stiles	Get rid of them and have metal kissing gates that the less able and dogs can use.	Comment too generalised to assess in IDF.					
360	Halesworth	Round Halesworth	A Councillor has suggested a list of cycle route round the town. I support all of the councillors ideas and am not going to write all out again on this cumbersome system.	Do, what the Councillor suggests.	No Cycling and Walking infrastructure improvement suggested.					
362	Kelsale Cum Carlton	Yoxford to Saxmundham	Cycleway alongside A12 from Yoxford to the B1121 turnoff to Saxmundham is poorly maintained or non-existent. This could provide a direct route to access important local services in Saxmundham such as the medical centre, shops and pharmacy for cyclists from Parishes to the north	Cycleway alongside A12 from Yoxford to the B1121 turnoff to Saxmundham is poorly maintained or non-existent. This could provide a direct route to access important local services in Saxmundham such as the medical centre, shops and pharmacy for cyclists from Parishes to the north	N/A	£3,000,000-£3,500,000	SCC, DFT	SCC, DFT	N/A	Beneficial

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363	Playford	Main A1214 from Martlesham to Ipswich (Kesgrave Town section)	Being frank the entire cycle path from Martlesham to Ipswich is a disgrace. The surface is worn due to car traffic crossing it to access the many houses along its length. The path is dangerous and cyclists are at more risk of collision with cars from the many side roads because the Stop lines are painted on A1214 not on the cycle lane and Give Way signs on the cycle path are worn away. It is therefore safer to cycle on the main road as the least dangerous option defeating the need for a path.	Maintain the cycle with a good surface, clearly mark give way signs. Improve visibility because you cant see cyclists when approaching the A1214 from the numerous side roads Mark "Give way" before the Cycle path on all sideroad junctions rather than on the main road which is some 10 to 15m further away ; cars are still slowing down and not stopped so a 10 to 15 mph side on collision is very likely.	Issues relating to maintenance have not been assessed for the IDF.					
369	Levington	Levington and Stratton Hall	Public footpaths are enjoyed by many walkers but are increasingly being plagued by cyclists who endanger the use by walkers and erode narrow coastal paths, delicate in many places as previous breaches will testify. Once the strategy is adopted, the bridleways and cycle paths must be properly maintained to encourage their use. The poor state of the A14 cycle way is an example of poor maintenance.	Although the misuse of footpaths contravenes the tort law of trespass, it is highly unlikely to be enforced by any landowner. Any strategy needs to make clear that cycling on public footpaths is unacceptable and unlawful. Parishes like ours who welcome considerate walkers to the footpaths are becoming increasingly inundated by rubbish dumped. Although litter picks clear up their rubbish, it needs to be clear that rubbish dumping is a increasing nuisance and that measures should be introduced to eliminate it. The provision of cycle paths seems to be less than public footpaths and this needs to change to avoid clashes between those on foot and those on cycles.	Comment too generalised to assess in IDF.					
371	Kesgrave	Bus stop opposite Penzance Road in Bell Lane Kesgrave	there is a sign here stating pedestrians and cyclists allowed. Cyclists assume they are able to cycle from here to Foxhall Road on the pavement as they have been allowed so to do from the Woodbridge Road end of Bell Lane. Pedestrians are of a different opinion, and there is contention	If cyclists are allowed to cycle all the way to Foxhall Road from the last sign at the junction of PenzanceRd/Bell Ln then more signs are needed. If they are not then a sign saying cycling ceases/stops/not permitted is needed to stop confusion and a likely future accident	Not assessed for IDF.					
372	Otley	B1078 junction with Charity Lane, Otley	B1078 Traffic turning right into Charity Lane often cuts across the junction ignoring the road markings which if you're a cyclist or car waiting to turn right out of it is quite disconcerting. The road markings have been rubbed away. This is typical of many junctions along this road where the mouth of a minor road is narrow. Vehicle drivers naturally cut the corner, rather than making the full 90 degree manoeuvre.	Improved markings on the B1078 & at the junction itself on Charity Lane.	Highway matter not assessed by IDF.					

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373	Felixstowe	Junction of Chaucer Road and Garrison Lane	Cyclists travelling northward along Garrison Lane wishing to then head towards Western Felixstowe are compelled to continue along the busy Garrison Lane all the way to the crossroads with Mill Lane to turn left onto Mill Lane. There is a junction however with Chaucer Road which is exit only to all traffic including cyclists.	Alter the junction between Chaucer Road and Garrison Lane to permit cyclists bound for Western Felixstowe to turn left from Garrison Lane onto Chaucer Road so that they can avoid the busy part of Garrison Lane approaching the crossroads. Chaucer Road is much quieter and suitable for cycling as well as slightly shortening the distance travelled. The junction would require physical work to safely permit cyclists, but not motorists, to enter from Garrison Lane. It should also permit cyclist travelling south along Chaucer Road to turn right onto Garrison Lane or straight over onto Orwell Road.	Not assessed for IDF.					
374	Wickham Market	A section of permissive footpath on our circular walks route, south side of B1078 The Gallows Route developed with SCC (Discover Suffolk)	A section of permissive footpath on our circular walks route, blue The Gallows Route developed with SCC (Discover Suffolk) has been closed by the landowner forcing people to walk along the dangerous B1078.	Liaise with landowner and SCC Highways to arrange re-opening please. Raised several times this year with SCC and a Cllr.	Not assessed for IDF.					
375	Bredfield	the thoroughfare woodbridge.	walking/shopping on this street at times when motorised vehicles have unrestricted access can be a very unpleasant experience, it becomes a noisy, dangerous and polluted area, and pavement parking further limits the safe public space, forcing vulnerable pedestrians/ shoppers onto the space remaining to compete with powerful industrial machines. this is in complete contrast to the safer, relaxed, more sociable atmosphere that prevails when motorised vehicle movement is restricted.	consider making this street safe for shoppers/ walkers / cyclists / vulnerable people like children, elderly and disabled at all times, not just for a few hours each day. if you need to know how its done look at other towns and cities, much bigger and more complex than Woodbridge, that confronted and resolved this conflict years ago. this has to be considered low hanging fruit for any council developing a cycling and walking strategy.	Modal Filter comments from the community comment section have not been assessed in IDF.					
379	Trimley St Martin	The village of Trimley St Martin and its links to neighbouring villages	As a result of local plan allocations the number of dwellings in Trimley St Martin will increase by 630 which is over 50%. This is likely to result in traffic congestion and increased danger for those walking and cycling, but it also provides the opportunity to make significant improvements to encourage cycling.	The first step should be to conduct a full and detailed review of cycling within and around the village looking at the possibility of creating new off-road cycle routes as well as improving the provision for sections where on road routes are unavoidable.	Comment too generalised to assess in IDF.					
382	Barnby	There need to be a safe cycle track from Carlton Colville to Beccles on the A146	Several people cycle the A146 and it is very dangerous especially by the Barnby Bends. The back rounds are hazardous in the dark morning and evening so there is no safe route. If there was a cycle track I'm sure more people would cycle rather than use cars.	Decent cycle track to link towns and villages	Very High	£1,800,000-£1,900,000 (cycle infrastructure only not highway)	SCC, DFT, CIL	SCC, CIL, DFT		Essential
383	Lowestoft	Denmark Road cycle path from station to Rotterdam Road	This must be the worst and most dangerous cycle path in the country. It is extremely uneven and shakes bones and bikes unbearably. There is also a concrete obstruction along with at least one place where the kerb has not been dropped.	Re-lay the path and drop the kerbs where required. Not sure what the obstruction is so unsure if it can be moved. Maybe designate the path on the opposite side as a shared footpath/cycle path as it is plenty wide enough along most of its length.	Very High	£450,000-£500,000	SCC	SCC		Essential

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384	Woodbridge	Junction of the top (i.e. west end) of Market Hill and west-bound Seckford Street	Firstly, visibility from the top of Market Hill into west-bound Seckford Street is non-existent. One has to pull out across the junction to see if there is anything coming, and if there is, then there is little space for the oncoming vehicle. Secondly, vehicles coming up the south side of Market Hill and turning across the top of Market Hill cut the corner, right into the path of any cyclist waiting to turn right into Seckford Street.	Make the Market Hill a one-way street all the way round, clockwise. This will clear the problem completely.	Modal Filter comments from the community comment section have not been assessed in IDF.					
385	Woodbridge	Junction of the top (west end) of Market Hill and the east side	Cyclists going north along the top of Market Hill and wanting to turn east down the side of the Shire Hall have no visibility of oncoming traffic coming down Theatre Street, and so have to pull out to look, into the path of any oncoming vehicle. As vehicle exiting from the top of Angel Lane tend to cause vehicles travelling down Theatre Street to pull out, this means these vehicles are already on the wrong side of the road when they meet the Market Hill junction, thus compounding the problem.	Make the Market Hill a one-way street all the way round, clockwise. This will allow cyclists to get into the right hand lane at the top of Market Hill and have greater visibility up Theatre Street. This will clear the problem completely.	Modal Filter comments from the community comment section have not been assessed in IDF.					
386	Lowestoft	Cycle path outside Claremont Pier	Cyclists are asked to dismount for the short section passing the pier. I can see this may have been done for the safety of pedestrians, but think a warning to go slow and also for pedestrians to be aware of cyclist would be better.	Cyclists are asked to dismount for the short section passing the pier. I can see this may have been done for the safety of pedestrians, but think a warning to go slow and also for pedestrians to be aware of cyclist would be better.	Not assessed for IDF.					
390	Kesgrave	Main Road Kesgrave	the cycling path which runs along Main Road is an asset to Kesgrave. The High School, which is located along the Main Road has one of the highest amount of pupils who cycle to school in the County. This cycle path is in great need of repair. the markings, signage and surfacing all need updating, re instating and re tarmacking. If ESC wish to encourage cycling and walking in East Suffolk then these issues need to be addressed ASAP.	As above.	Issues relating to maintenance have not been assessed for the IDF.					
392	Melton	New Street, Woodbridge	Introduce a 20mph speed limit throughout the centre of Woodbridge. Divert through traffic away from New Street. Introduce a chicane half way down New Street to slow the traffic.	Introduce a 20mph speed limit throughout the centre of Woodbridge. Divert through traffic away from New Street. Introduce a chicane half way down New Street to slow the traffic.	Speed reductions have not been assessed for the IDF.					

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395	Melton	Melton and Woodbridge	Aside from cycling in the parks and A12 (cycle path) there are no family friendly or safe routes. No exclusive cycling options. I feel the narrow streets and way people drive is unsafe for children of primary age to cycle. Exclusive areas would improve children's and parents confidence and encourage families to get on bikes.	Research locations for family safe cycling routes and designate land where you could create this. Partner with land owners.	Comment too generalised to assess in IDF.					
396	Ufford	Footpath along B1438	The footpath for almost the whole way from Melton up to the top of Yarmouth Road is too narrow. In places this appears to just be overgrown where the vegetation has been allowed to reclaim the footpath - especially at the upper end around Ufford Park entrance. This leaves pedestrians walking perilously close to the road.	Cut back the vegetation and hedges, widen the path properly. Then keep the path cleared regularly to avoid this in future.	N/A	£1,300,000-£1,400,00	SCC	SCC	N/A	Beneficial
397	Ufford	Footpath between Ufford and Wickham Market alongside B1438	This footpath is very narrow and in poor condition. The path surface has fractured and it is overgrown with weeds. In places the path is non-existent or is heavily rutted. Pedestrians and particularly those with children are in danger from passing traffic and from trip and slip hazards. The path is quite well used but could see much greater footfall if improvements were made.	Widen and resurface this footpath and make sure that the missing sections are filled in. Cut back overhanging bushes to avoid pedestrians having to step into the road	N/A	£1,300,000-£1,400,00	SCC	SCC	N/A	Beneficial
398	Hollesley	The level of traffic on the small lane to Shingle Street	It is dangerous to walk down this lane to Shingle Street in the summer months because of the number of visitor cars to the area. It is a popular route for walkers, local families, rambler groups, D of E groups to visit Shingle Street. The large volume of cars using the lane makes it very dangerous for non-vehicle users because it is narrow, with unmarked 90 degree bends and there is nowhere to escape if a car is travelling too fast or misjudges the space available to safely pass	Register the lane under the Quiet Lane Scheme. Mark out on the road surface a lane for walkers/cyclists to reduce the speed of the cars by highlighting the lack of space for the cars to pass other users Ban cars parking from the bridge down to Shingle Street, except resident vehicles during the summer months.	Quiet Lanes assessed by Quiet Lane project team.					
399	Ufford	between The Avenue and Loudham lane Ufford. the hedge on the west side need cutting back. there are branches and brambles that stick out which cars coming down loudham lane push you into.	the hedge on the west side need cutting back. there are branches and brambles that stick out which cars coming down loudham lane push you into.	cutting hedge	Issues relating to maintenance have not been assessed for the IDF.					
400	Ufford	Ufford	There are many footpaths in and around Ufford that are widely used by residents. While many are across fields and through woodland, walkers are obliged to use the lanes in Ufford to access them. There are very few pavements in the village, obliging walkers to compete with vehicle traffic on single track lanes.	Installing pavements is impractical in most instances due to cost and planning issues. However, there is a simple, cost effective improvement available. The vehicle speed limit within the village is 30 mph. Decreasing this to 20 mph on single lane roadways would dramatically increase safety for both walkers and cyclists, with little effect on traffic flow. Ufford lane road traffic is largely local, with little through traffic.	Speed reductions have not been assessed for the IDF.					

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401	Campsea Ashe	Mill Lane Campsea Ashe	Narrow road, high hedges, no footpaths, heavy traffic from agriculture	Mark as unsuitable for cyclists/walkers	No Cycling and Walking infrastructure improvement suggested.					
402	Ufford	The whole of Byng Hall Road but particularly where it passes the houses up to the underpass of the A12	Concerns around visibility here particularly around the Public Rights of Way path that has its entrance/exit on the inside of the bend outside "Wayside", and the visibility along Byng Hall Road for both vehicles travelling in opposing directions and the pedestrians/cyclists/equestrian users. 2 speed roundels (outside Wayside & Woodcott) that have been consumed by the vegetation. The encroachment of the verge onto the carriageway on the eastern side of Byng Hall Road.	To complete the work highlighted from the site visit and then either introduce 20 mph speed limits or designate as a Quiet Lane	Quiet Lanes assessed by Quiet Lane project team. Speed reductions have not been assessed for the IDF.					
403	Ufford	Spring Lane from the High Street to Lower Ufford	Single track road often used by pedestrians, cyclists and equestrians that is very tight with some blind bends. Danger of accidents with some of the aforementioned parties with vehicles. Often overgrown and often not able to drive down in a car without the vegetation coming in to contact with the vehicle	Vegetation control (cutting) and Categorise as a Quiet Lane	Quiet Lanes assessed by Quiet Lane project team. Issues relating to maintenance have not been assessed for the IDF.					
404	Ufford	Lower road Ufford - the entire length.	Single track road often used by pedestrians, cyclists and equestrians that is often flooded and muddy.	Look at improving the drainage and because of the frequent use by pedestrians, cyclists and equestrians designate as a Quiet Lane.	Highway matter not assessed by IDF.					
406	Ufford	Yarmouth Road footpath adjacent to Ufford Park Hotel.	Due to the encroachment of soil and grass and other plants over the concrete footpath, the footpath is now extremely narrow. This has resulted in pedestrians having to walk very close to the road side. The footpath is only wide enough for pedestrians to walk in single file thereby making it impossible to safely hold a young child's hand or to push a toddler's buggy. It is extremely uncomfortable and dangerous to walk this part of the footpath as being so close to the road is dangerous.	The soil/grass/plants need to be dug or scraped back so that the full width of the concrete footpath is available.	Issues relating to maintenance have not been assessed for the IDF.					
409	Waldringfield	Waldringfield	No WC accessible to the public walking or cycling in the area. It would get more people out walking and/or cycling if they could feel sure that they would be able to find WCs en route. Waldringfield is a classic example of a place in a prime location for walkers, but no toilets. This applies to most villages these days so Waldringfield is just one example.	Public WCs should be brought back in villages. Funding could perhaps be eased by charging, and since there is little call to carry coins these days, perhaps this could be arranged via a mobile phone app similar to car-parking. Pubs and cafes (in Waldringfield the Maybush is perfectly located) should be encouraged, or even compelled, to allow passers-by to use their toilets for a small charge (which they might even refund if the user then decides to buy something) - rather than walkers "go" in the bushes.	Not assessed for IDF.					

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414	Tunstall	Access to Wickham Market Train Station in Campsea Ashe from Tunstall	Dangerous road for cyclists and walkers, pot holes are uneven surface on edge of road on Ashe Road, very sharp blind corners and road is regularly used by lorries. This means poor access for both cyclist and walkers to the train station. Public transport in this area is poor so access to the train station is vital for allowing people greener methods of transport.	The best solution would be cycle lanes and footpaths that allow direct access between Tunstall and Campsea Ashe or alternatively follow the road. Alternative solution would be improving Ashe Lane and adding protected cycle lanes.	N/A	£2,300,000-£2,400,000	SCC, DFT	SCC, DFT	N/A	Beneficial
417	Framlingham	Castle Street btw Double Street and Fore Street	Castle Street is one-way eastbound which reduces access to the town centre and church from estates on the east side of the town	Suggested contraflow cycle lane. There wouldn't be any loss of parking as the only parking currently is the widest section - there are two exit / queuing lanes and you only need one. West of Double Street may well be too narrow but not a problem as cycles can turn down Double Street which is 2-way	Not assessed for IDF.					
419	Kesgrave	Cycle path A1214 Kesgrave Road	A typical example of a 'stop start' cycle path where motor vehicles are given priority at each minor road junction and property driveway entrance, hence impeding the steady progress of cyclists and pedestrians	Consider giving cyclists & pedestrians the right of way at minor junctions by removing the 'giveaway' from the cyclepath and moving the road 'giveaway' lines back from the junction to before where the cycle path crosses it. Also where a cyclepath crosses the front of a property entrance put the giveaway lines across the entrance to ensure that anyone leaving the property gives way to the cyclist, rather than relying on the cyclist having to dodge vehicles sticking their nose out onto the cycle path. This is common practice in countries where cyclists are given priority over vehicles, rather than in the uk where vehicles are given priority over cyclists (and pedestrians, mobility scooter users etc).	Not assessed for IDF.					
420	Melton	Station Road Melton	This is part of the main pedestrian route through the village. In places, the pavement is less than 1m wide. The road is used on a daily basis by HGVs and agricultural vehicles. This is not safe and is very polluting.	Work with other authorities e.g. Suffolk County Council to introduce weight/width restrictions. Work with satnav providers to direct heavy vehicles to more suitable routes.	No Cycling and Walking infrastructure improvement suggested.					
421	Saxmundham	Many of the pavements in Saxmundham (particularly the high street and the roads off the cross roads at the traffic lights on town.	The pavements in Saxmundham are in many places very narrow and not fit for purpose. In many places they are too narrow for mobility scooters and pushchairs or even for two pedestrians to pass safely. This is especially true on the high street.	Making a section of the high street pedestrians/deliveries and disabled access only.	Modal Filter comments from the community comment section have not been assessed in IDF.					
422	Saxmundham	The B1121 between Kelsale, Saxmundham and Benhall	Lack of safe cycling route along this road which links two primary schools, two villages and the town centre and is used by motorists and lorries to access town/A12. It also has a very narrow pavement between Benhall and Saxmundham which forces pedestrians very close to the fast moving traffic.	Implementation of the Three Communities Link proposal. Providing an inclusive and safe cyclist and pedestrian route for vulnerable road users including those with children, pushchairs and mobility scooters. The plan already exists, just requires funding.	N/A	£1,300,000-£1,400,000	SCC, DFT, Developer, CIL	S278, S106, CIL, SCC, NH, DFT		Essential

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423	Tunstall	Example - Snape Maltings but applies to towns, villages and popular visitor locations.	Provide or assist businesses in providing sufficient good quality and secure cycle parking. These need to be in high footfall areas with CCTV and good lighting to discourage theft. Cycle lockers at station and other transport hubs would be ideal. Unless cyclist feel confident that there are good cycle parking facilities that are safe they just won't visit these places.	As above.	Comment too generalised to assess in IDF.					
424	Snape	Legitimise cycling between Snape and Aldeburgh.	To be able to cycle safely from Snape to Aldeburgh (and the other way of course) would be a major improvement and add to the economy by all the holidaymakers and second homers being able to cycle with children to Snape or vice versa and the route is almost there, along the river wall, down the sailors path and along the verge to Aldeburgh. Just a small spend to improve the river wall and the verge and you are there.. it would also be a fantastic addition for local folk to cycle it.	Maybe just a bit of edging along the river and verge to contain some road planings and a few signs to be respectful of pedestrians.	N/A	£1,000,000-£1,100,000	SCC	SCC		Desirable
434	Theberton	Old railway line between Aldeburgh and Leiston	Absence of safe cycling route for tourists and residents between Aldeburgh and Leiston. Roads are too dangerous and existing cycle route along coast path isn't accessible for most. We don't have an easily cycled tourist route like other parts of the country.	The old railway line between Aldeburgh and Leiston provides an ideal route. Starting from the caravan park, heading along the old line, across the road at Thorpeness holt, continuing along the line route until Crown Farm, this would join the existing cycle path along Lovers Lane, a new extension proposed by EDF (DCO) and Leiston's Cycle Strategy route into Town. A tarmac track (Suffolk's version of the 'cinder trail' - route 1 of National cycle network) would give access to many more residents who cannot currently cycle easily or safely between the two towns for work/recreation. It would be a boost for tourism as more people would access the route as a flat and easily cycled surface. E Bikes could be promoted to reduce car journeys.	N/A	£1,000,000-£1,100,000	SCC	SCC		Essential
437	Felixstowe	Area bounded by Candlet Rd, Gulpher Rd, The Grove	This area is the subject of a major planning application for 560 houses, ref DC/20/1002/ARM, containing significant walking & cycling proposals. Although the formal comment period for that is closed, those interested in this area may wish to look at that for information, and possibly also add a comment there.	All Walking and cycling matters in this area and those to West and East planned for development in the East Suffolk Local Plan should be considered in the context of the entire area.	Comment too generalised to assess in IDF.					
438	Felixstowe	Area bounded by Candlet Rd, Gulpher Rd and approximately the track to Candlet Farm	This area is the subject of major proposals for development of housing and a sports centre in the East Suffolk Local Plan containing significant walking & cycling proposals. Those interested in this area may wish to look at that for information. The relevant policy is at pages 215-221.	All Walking and cycling matters in this area and those to the East planned for development in the East Suffolk Local Plan should be considered in the future context of the entire area.	Comment too generalised to assess in IDF.					

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440	Felixstowe	Area bounded by Links Avenue, Upperfield Drive, Ferry Rd, Gulpher Rd to The Grove	This area is the subject of major proposals for development of housing in the East Suffolk Local Plan containing significant walking & cycling proposals. Those interested in this area may wish to look at that for information. The relevant policy is at pages 215-221.	All walking and cycling matters in this area and those to the East planned for development in the East Suffolk Local Plan should be considered in the future context of the entire area	Comment too generalised to assess in IDF.					
441	Southwold	No access to Easten Bavents beach	Suffolk Coastal path takes a huge inland diversion between Southwold and Covehithe. The latter is now spilling over with people trying to access the beach.	Safe steps over the breakwaters at the north end of Southwold Parade would meet a need, avoid people taking risks on the rocks and allow escape if stranded by rising tides.	Not assessed for IDF.					
458	Kesgrave	Brendan Drive	NCN 1 & the cycle route into Ipswich is via an estate road at this point and sections are cluttered with parked cars, and a couple of short hilly sections where less abled and older riders have to get off and push.	It would make sense to upgrade the footpath that runs across Rushmere Common to Heath Road to a Cycle/footpath there by giving cyclists a section of the route that is traffic free and relatively flat. It would also connect in the other direction with the bridle way that runs east towards Bell lane and beyond... giving a continuous traffic free cycle route from the Hospital to almost the Brightwell Development Area.	Very High	£450,000-£500,000	SCC	SCC	N/A	Beneficial
460	Woodbridge	The entire Riverside of Woodbridge and Melton from Kyson Point to Wilford Bridge	Tourism is vital to Woodbridge's economy and the river is a major tourist attraction. I know that cycle tour companies have expressed amazement that it is not possible to cycle through Woodbridge along the river bank. It is scandalous that we do not make the most of our beautiful river and actively discourage cyclists. There is no safe provision anywhere in the town for them.	From Kyson Point to The Avenue there is a rough narrow grass track below and to the left of the raised river path that could be made into a cycle path. From just beyond Deben Road to Wilford Bridge in many places there are already two clear paths and it should be possible to convert and extend one of these into a cycle path. In the few places where this would not be possible could there not be signs saying 'cycling permitted but priority must always be given to pedestrians'. In my experience if you are a polite careful cyclist, pedestrians have no objection to cyclists along the part of the river. Between The Avenue and Deben Road there should be signs diverting cyclists along the road. A 20 mph limit should be established on the Avenue, Cherry Tree Road, Kingston Farm Road, Kingston Road and Station Road, so that where there is not a dedicated route along the river cyclists can be diverted to a cycle friendly route.	N/A	£1,000,000-£1,100,000	SCC, DFT	SCC, DFT	N/A	Beneficial
461	Woodbridge	the junction of the Thoroughfare and Lime Kiln Quay Road, Woodbridge (traffic lights)	dangerous junction for cyclists	provision of a cyclists' box marked out in front of the car traffic - particularly necessary if travelling from Melton Hill and going right or straight on at the lights and if travelling up Lime Kiln Quay Road going right.	Not assessed for IDF.					
463	Melton	The roundabout top of Woods Lane / A12	1.impossible to see oncoming traffic coming from south on A12 when crossing A12 on the path from the north 2. Impossible to see oncoming traffic when crossing Woods Lane from North to South on the path	In both instances, the path could be closer to the roundabout	Not assessed for IDF.					

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466	Ufford	Hawkeswade Bridge on road from Ufford to Eyke	This bridge is on a narrow lane with a blind corner, making visibility poor for both vehicles and pedestrians. The footpath and area nearby is used by walkers and cyclists so is often hazardous. Although there is 30 mph sign just before the bridge, there is no road narrows sign and traffic often speeds or has to back up. The road is used by traffic cutting through to the A12 as well as by lorries and tractors from nearby farms.	Improve signage at this dangerous point and also near Melton hamlet where this snoter blind corner for pedestrians. Consider adopting a 20 mph limit on this difficult section.	Not assessed for IDF.					
467	Melton	Footpath alongside Woods Lane heading down towards Melton traffic lights.	Observed Farlingaye School students going home to Melton village. Some were walking, others cycling. There were also other pedestrians. Those on bikes had chosen to ride on the pavement as the road is busy and often has large vehicles and is not wide. It is therefore safer on the pavement. However the pavement is not wide enough to accommodate everyone safely. The problem is aggravated by the steepness of the hill. I am a regular cyclist and don't use Woods Lane.	1. Find an alternative safe route for school children who live in Melton village and beyond. This might involve new permissive paths, resurfacing, etc. Basically Woods Lane is unsafe for cyclists. 2. Have a proper dedicated cycle lane. This would probably involve widening the road or the pavement.	Very High	£800,000-£850,000	SCC	SCC	N/A	Beneficial
468	Gedgrave	River Wall - eastern side of Butley River. The path along the river wall between the points TM 393 505 and TM 396 485	This section of river wall is blocked off to the public by fencing. Its omission from the Definitive map could simply be an anomaly as the route recorded on the Definitive Map as Chillesford Footpath 18 stops abruptly at the Chillesford/Gedgrave parish boundary which is absurd.	This route must be added to the Definitive map by way of a Creation Order or Agreement. The proper recording of this route would enable a fine circular walk linking Chillesford and the Butley Ferry.	Not assessed for IDF.					
471	Gedgrave	River Wall – Butley River, The Gull, River Ore. Butley Ferry to Tide Guage (TM393481 to TM415484).	This is another section where there is no apparent reason for the route not to be recorded on the Definitive Map. It is freely used (possibly on a permissive basis) but is another instance where a Creation Order or Agreement should be funded.	Path should be added to the Definitive Map by way of a Creation Order or Agreement.	Not assessed for IDF.					
472	Iken	Alde River wall east of Iken Church (TM412567 - TM443556)	This is another section of river wall that should be opened to the public as a public footpath to link Iken Church with Public Footpath Iken 7. We are recommending to Natural England that it becomes part of the England Coast Path.	A Creation Order or Agreement is needed.	Not assessed for IDF.					
473	Leiston Cum Sizewell	The British Energy permissive path between the small car park off Lovers Lan 6452.	This path forms part of the important recreational route known as The Sandlings Walk. Currently it is permissive only and as such can be withdrawn at any time.	It should be made into a permanent public right of way by means of a Creation Order or Agreement. The other adjoining permissive paths on British Energy's estate through Sizewell Belts should also be made permanent public rights of way.	Not assessed for IDF.					

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474	Aldeburgh	The old railway track bed between TM 4601 5745 and TM 4622 5945.	This forms part of much walked circular routes taking in Aldeburgh, Thorpeness, the Aldringham Fen and Aldringham Walks. It also presents for walkers and cyclists a safe alternative to the B1122 which is a fast and extremely dangerous road and the only other direct link between Aldeburgh and Leiston Much of the track bed appears to be in private ownership but is open, presumably as a permissive path. Permissive paths are unsatisfactory because the permission can be withdrawn at any time.	Creation Agreements or Orders should be funded to secure the route as a permanent public right of way. An ideal solution would be for a bridleway to be created over the track bed as this would provide a multi-user facility for walkers, horseriders and cyclists.	N/A	£1,200,000-£1,300,000	SCC, DFT	SCC, DFT	N/A	Beneficial
475	Ramsholt	Ramsholt to Bawdsey – The stretch of river wall from Ramsholt to Bawdsey on the Deben	There is no public access along this stretch river wall	This should be made available to the public to connect with existing routes and become part of the England Coast Path. This section of river wall is not currently open to the public but could be made a public footpath with a minimum of alteration and expenditure with no inconvenience to the landowners. A Creation Order or Agreement is required. It will have a good deal of support from local residents as well as visitors.	Not assessed for IDF.					
476	Aldeburgh	Verge of the A1094 near Aldeburgh Golf Course forming part of “the Sailors’ Path”,	Until recently there was no safe link at the Aldeburgh end between the small car park at TM443581 and the footway at TM448577. Walkers were expected to walk in the carriageway of a fast and dangerous road after it leaves the 30mph limit. Verges are narrow, sloping and uneven with drainage channels - totally inadequate. SCC has secured a licensed path but this is understood to be a ten-year agreement only.	A permanent right of way is required over this licensed path. The verges on the southern side of the road fronting the gardens between the Golf Club and the small car park also need dedicating.	N/A	£450,000-£500,000	Developer, SCC, CIL	S106, S278, CIL, SCC	Access and junction improvements at Land rear of Rose Hill, Saxmundham	Essential/Critical
478	Westerfield	Moss Lane Westerfield	This road is single track and used by a large range of vehicles as a short cut. It is unsuitable as a rat run and should be closed to through traffic thus protecting cyclist and pedestrians.	This road is single track and used by a large range of vehicles as a short cut. It is unsuitable as a rat run and should be closed to through traffic thus protecting cyclist and pedestrians.	Modal Filter					
479	Melton	Wilford Bridge Road leading onto Sutton Road onwards	In an ideal world separate coned cycle lanes would be in operation but due to roads being too narrow and in order for cyclists to feel reasonably safe, speed limits must be reduced for motorised traffic from 60 mph to 40 mph maximum on rural roads between 30 mph towns and villages to help avoid potentially fatal accidents involving cyclists and horse riders too. Ultimately we want more people on bicycles for commuting as well as leisure but safety is paramount if this is to happen.	As above. Will obviously also benefit pedestrians/those trying to cross increasingly busy roads.	Speed reductions have not been assessed for the IDF.					

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480	Halesworth	The thoroughfare, Halesworth. Between Halesworth Library and the Thoroughfare/London Road junction	Cycling to be allowed in both directions, thus allowing both local and visiting cyclists to travel through The Thoroughfare and use its facilities	Cycling to be allowed in both directions, thus allowing both local and visiting cyclists to travel through The Thoroughfare and use its facilities	Not assessed for IDF.					
481	Ilkeshall St Lawrence	The high street and the A143 junction	We live between bungay and spexhall, we have no pathways at all, it would be fantastic to have a walkway or cycle path put in between bungay where we do our shopping and spexhall where our local public house is situated that we use for social events, I cycle but feel very unsafe riding on the main road as it is very dangerous, my partner has a mobility scooter that she could never use between these two points on the map, so we have to always use the car but would much rather use our cycle and scooter	Pathway or cycle lane from bungay to spexhall along the A143	N/A	£5,500,000-£6,000,0000	SCC, DFT	SCC, DFT	N/A	Beneficial
483	Saxmundham	9 points on Saxmundham bypass: TM380656 Kelsale FP 10; TM373646 Kelsale FP 38; TM376644 Kelsale FP 1; TM375639 Kelsale FP 3; TM375636 Sax FP 5; TM375632 Sax FP 11; TM376630 Sax FP 13; TM377621 Benhall FP 22; TM378616, Benhall BR 25	Paths severed by A12 bypass with no thought for walkers. Crossings lethal- single carriageway with 60 speed limit. No warnings to motorists- no central refuges- in two instances (TM 376 644 and TM 375 636) one must climb over Armco-type barriers on each side. TM 375 632 crossing is oblique requiring a considerable walk alongside the carriageway to cross it at a right angle. Traffic increased many fold by new housing on western edge of the town. Sizewell C traffic would exacerbate more.	These crossings must be made safer and easier through speed limits, warning signs to motorists, provision of gaps in the Armco barriers and the installation of central refuges and waiting areas.	Speed reductions have not been assessed for the IDF.					
490	Lowestoft	On the cycle path running adjacent to Tom Crisp Way, South West of the main traffic light junction with Carlton Road and Long Road.	Steel post erected on the cycle path. This is a part of a sign (which consists of two posts) notifying road users of the distance to various destinations. One post is in the cycle lane, the other is in the grass verge. This post poses a heightened risk of a collision with it, especially in the dark where it can become near enough impossible to see it with the glare from oncoming vehicles when travelling North East on the cycle path.	To remove the post and if possible the sign. If it is still needed, have a smaller sign which would only need the use of one post.	Not assessed for IDF.					
491	Grundisburgh	Proposed 80 house development in Grundisburgh	A proposed large housing development accessed only via two minor roads with no direct access to the 'B' road network. Increased motorised traffic during construction and when inhabited will increase the risk factor for cyclists, pedestrians and other vulnerable road users trying to negotiate Park, Chapel, Lower & Ipswich Roads all of which have limited if any pavements. This will actively discourage walking and cycling in the area, particularly with regard to those less able...	Motorised traffic on these local roads need to be forcefully restricted to allow more vulnerable road users to safely walk, cycle, scoot or trot along them to/from local amenities. The developer should be instructed to provide suitable cycle/footpaths along the roadside boundaries of the development and off site connecting with the School and local amenities. There is considerable local opposition to this development as per the comments on the current planning application.	Modal Filter comments from the community comment section have not been assessed in IDF.					

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492	Lowestoft	On the cycle path running adjacent to Tom Crispway.	The use of multiple posts in the middle of the path to notify users of what he path is for.	These posts seem to offer little or no purpose. But what they do offer is an increased risk of a collision due to a cyclist crashing into a post which has no need to be there in the first place. In contrast, you wouldn't have a post in a road for no particular reason. One improvement would be to remove all the posts that have little or no reason for being there. I recognise the purpose of some of these to cause an obstruction to vehicles potentially using the paths, but ones like these are a danger.	Not assessed for IDF.					
494	<Null>	This is a general comment, Sport England, as a non-statutory consultee, supports the development of this strategy, which will improve opportunities for physical activity, in line with Sport England's Active Design principles.	n/a	n/a	Comment too generalised to assess in IDF.					
495	Trimley St Martin	Cycle path adjacent to Trimley to Levington link road	This path is in a very poor state with many uneven bumps and potholes, and is also dangerously close to a fast section of the A14. As a result, many cyclists choose the link road, slowing vehicular traffic and causing drivers to be impatient.	Ideally, the path should be re-sited to run alongside the link road, far safer. In short term, it should be resurfaced and a sturdy barrier placde to shield it from the A14	N/A	£1,000,000-£1,100,000	SCC, DFT	SCC, DFT	N/A	Beneficial
496	Campsea Ashe	Marlesford Lane dips beneath railway line at Bucks Head bridge.	Road often floods after rain in winter and from irrigation run-off in summer. Existing drain usually blocked. Water depth often sufficient to prevent access by walkers and cyclists - sometimes deep enough to cause abandonment of motor vehicles.	New drainage works.	Highway matter not assessed by IDF.					
497	Hacheston	Lane leaving B1078 adjacent to where southbound A12 slip road joins B1078	Register as a quiet cycling route to Campsea Ashe, avoiding the B1078 which can be busy with motor traffic and which, in places, is narrow with high banks. This also gives access at Well Cottage to a lane which crosses the railway line via the Blackstock level crossing to give a quite cycling route via Station Road to Blaxhall and on to Snape.	Register as a quiet cycling route to Campsea Ashe, avoiding the B1078 which can be busy with motor traffic and which, in places, is narrow with high banks. This also gives access at Well Cottage to a lane which crosses the railway line via the Blackstock level crossing to give a quite cycling route via Station Road to Blaxhall and on to Snape.	Quiet Lanes assessed by Quiet Lane project team.					
499	Campsea Ashe	Ashe Road between Campsea Ashe and Eyke / Rendlesham	Register as a quiet walking and cycling route between Campsea Ashe station and Eyke or Rendlesham. Give priority to walkers and cyclists.	Register as a quiet walking and cycling route between Campsea Ashe station and Eyke or Rendlesham. Give priority to walkers and cyclists.	Quiet Lanes assessed by Quiet Lane project team.					
500	Campsea Ashe	Ivy Lodge Road between Campsea Ashe and Rendlesham / Bentwaters	Register as a quiet cycling route.	Frequently used as a short cut by lorries accessing Bentwaters from the A12. Road not suitable for HGVs and potentially dangerous for walkers and cyclists Prohibit HGVs from using this route (with exception of agricultural vehicles).	Quiet Lanes assessed by Quiet Lane project team.					
501	Bredfield	A12 between Ufford Road junction to Bredfield and Woods Lane roundabout	There is only a pedestrian path alongside the main road, not authorised for cyclists.	1. Authorise making this a shared user (pedestrians & cyclists) and thus legitimise current practise. 2. Widen the path	Very High	£1,400,000-£1,500,000	SCC, DFT	SCC, DFT	N/A	Desirable/Essential
504	Melton	A1152 & Wilford Bridge	Lack of a cycle path, Melton traffic lights to Bromeswell Quiet lanes...	Having cycled along the footpaths on this route, there does seem to be enough room on the verge to widen the existing footpaths to create a cycle/footpath pretty much all the way along, past the station and across the bridge and round to the Bromeswell 'Quiet lane'	High	£850,000-£900,000	SCC	SCC	N/A	Beneficial

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505	Melton	Riduna Park. Woodbridge	Example of new industrial development with... No obvious cycle parking facilities for... 1) Members of the Public Visiting East Suffolk Council Offices 2) Employees cycling to work at each unit 2) Cyclists wishing to use units providing food and drink such as Honey & Harveys.	1) Encourage developers to give up one car parking space per unit as a dedicated cycle parking space with stands or provide secure storage as per the Councils own staff facility. 2) Encourage developers to give up unit space to a dedicated indoor bike storage space including showers and lockers. This could be a shared facility for all on the park 3) A few sheffield stands outside the front door of the Council Offices would be useful and look good to passers by. Include a dropped kerb at the roadside end of the main entrance path so that disabled users / buggies can easily access it from the Melton direction.	Not assessed for IDF.					
506	Melton	Melton	Well done to Melton Parish Council for converting this short length of footpath into a cycle/footpath. It might win the prize for the shortest cyclepath in East Suffolk but it is an example of where a small 'parish council' have been able to upgrade the designation of a footpath to a cyclepath.	East Suffolk DC to proactively support and encourage Parish Councils to upgrade footpaths to foot/cycle paths.	Comment too generalised to assess in IDF.					
507	Sutton	Sutton Heath	Walking & Cycling along 'Heath Road'..As already noted this is a fast and straight road which makes it unsafe to walk or cycle along	Create a path parallel to the road but on the 'heath' side of the fence line where possible. The path could be a simple woodland style path suitable for walkers or those using mountain bikes. The verges are wide in places as well although it might mean some crossing of the road in places, but thats safer than walking down the road as I saw someone doing the other day.	N/A	£1,200,000-£1,300,000	SCC	SCC	N/A	Beneficial
513	Woodbridge	Sandy Lane, Martlesham as far as Ipswich Road, Woodbridge	Many motorists tend to drive too fast and show their reluctance to slow down for less powerful craft such as a bicycle. The railway bridge often results in a last second lurch for many. For a cyclist to exit the bottom of the hill from Broomheath on the way to Woodbridge, it has become quite difficult to exit onto Ipswich Road going to Woodbridge.	Possible solution might be to widen the pavement thus curbing the motorists and allow cyclist to share with the few pedestrians. I would be interested to hear your views.	Very High	£850,000-£900,000	SCC	SCC	N/A	Desirable
514	Melton	Road between Woodbridge Thoroughfare and Melton cross roads	The all day parking on both sides has reduced the width of this road by about half. Mostly shoppers or commuters are seeking to travel but the all day parkers are an obstruction and a danger to any under aware pedestrian. The other day I had an appointment in Common Lane, Melton and the traffic was gridlocked, from Woodbridge to Melton. I thought there must have been an accident but no. On the bike I was able to nimble past them it was a ridiculous situation.	Is it time for bikes only for trips under 10 miles? Some days a week. It is moving that way.	No Cycling and Walking infrastructure improvement suggested.					

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519	Martlesham	Pathway from Martlesham Creek to Kyson Point and on to Woodbridge	Having made much use of the pathway from Martlesham Creek to Kyson point and on to Woodbridge over the last lockdown months we have often been forced to step aside into less than safe areas to let cyclists pass. They should not be on these narrow paths at all - signs are inadequate. There have been talks about making this route more accessible for cycling which would cause considerable work and disruption and cost a very large sum. We are against such a proposal.	Having made much use of the pathway from Martlesham Creek to Kyson point and on to Woodbridge over the last lockdown months we have often been forced to step aside into less than safe areas to let cyclists pass. They should not be on these narrow paths at all - signs are inadequate. There have been talks about making this route more accessible for cycling which would cause considerable work and disruption and cost a very large sum. We are against such a proposal.	Not assessed for IDF.					
526	Lowestoft	East coast of Suffolk	The longest single signed cycle route in the world, approx. 6,000Km, is signed along the north Lowestoft sea wall and around the Gunton St Peter's estate. Each year many people travel from all around the world to cycle this route. Currently, the route goes from Norwich to Beccles and stays inland to Harwich missing out on the Suffolk coast.	In conjunction with Sustrans could some serious consideration be given to routing the North Sea Cycle Route from Beccles to Lowestoft and follow the Suffolk coast down to Harwich.	Not assessed for IDF.					
530	Melton	The junction with The Street/Wiford Bridge and Melton Hill Road	The crossing from The Street to the primary school is very narrow and there is considerable congestion during school hours. The traffic is also very heavy at these times, The Street should have light vehicles only using the road between Woodbridge and Ufford except for access to and from business in the area. As a walker I have nearly been struck several times by large vehicles passing along the road close to the pavement	Re landscape grass verges on the junctions with the lights and the crossings to Melton Primary School. Erect sign asking motorists to switch of engines when idling by lights. Prohibit large vehicles from using the road between Woodbridge, Melton and Ufford unless for delivery only to local business.	Modal Filter comments from the community comment section have not been assessed in IDF.					
534	Martlesham	Felixstowe Road	The road markings are completely bonkers. Cars sometimes drive in the middle very near to oncoming traffic as if they think it's one-way. Also, traffic moves too fast, often far quicker than 30mph which I guess is the limit. At rush hour, cars sit in the cycle lane in a long line queuing at the t-junction. The pavements are too narrow too. I cycle daily between martlesham and woodbridge and this is one of the bits which I think could be made much safer for cyclists and pedestrians.	Impose a speed limit, sort out road markings, possibly chicanes (things that stop motorists using it as a rat run and really make it a cyclist priority route as intended).	Speed reductions have not been assessed for the IDF.					
535	Martlesham	Right turn onto Sandy Lane	It is a hairy right-hand turn coming down the hill to turn right onto sandy lane.	Speed limit or separate waiting space would help	Speed reductions have not been assessed for the IDF.					

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536	Woodbridge	Ipswich Road	There isn't any provision for cyclists here and the traffic moves very impatiently. There's a lot of unsafe overtaking, especially when there are two cyclists going in different directions and motorists on each side trying to overtake.	Cycles lanes and wider pavements would be great on this stretch. If it felt safe walking or cycling between woodbridge and martlesham I'm sure many more people would do it.	N/A	£850,000-£900,000	SCC	SCC	Access and junction improvements at Land at Woodbridge Town Football Club	Essential/Critical
539	Lowestoft	Denmark Road, south side. near junction with Rotterdam Road	When reaching the end of the cycle track you have to go on to the road. You cannot cross to the cycle track on the other side as there is no drop kerb at this point on the north side.	This may all change with the construction of the new bridge. All the cycle tracks at this point should be reconsidered	Very High	£450,000-£500,000	SCC, CIL	SCC, CIL	N/A	Essential
540	Lowestoft	From the roundabout at the junction of Corton Lone Lane and A47	In addition to the lack of cycle lanes to the north of this junction on the A47 to Hopton. There are very few direct cycle lanes along the A47 to the centre of Lowestoft. There are good lanes along the new Millennium Way and also around the back roads into Lowestoft, but not a direct route down the A47	Please see if you can introduce lanes south, along the existing A47	Very High	£2,200,000-£2,300,000	Developer, SCC, CIL, NH, DFT	S106, S278, CIL, SCC, DFT, NH	Cycle link between Lowestoft and Hopton	Essential
541	Oulton	Gorleston Road, west side between Mobbs Way and Dunston Drive. Oulton	A build up of vegetation and leaves over the past 2 years has reduced the width of the footpath. This means that if a mobility scooter is coming on this path any other scooter, buggy or pedestrian has to walk into the road to get past.	Remove all debris from the tarmac footpath. The footpath extends to just behind the lamp posts and this will double the width of the footpath. My wife has rung up a number of times about this.	Issues relating to maintenance have not been assessed for the IDF.					
543	Trimley St Mary	Gaymer's Lane	A safer way to cycle to Trimley was via a path on to Gaymer's lane (then the new Bridle way) from St Stennetts Close, (come up the Avenue) but someone has now blocked this.	removal of barrier	Not assessed for IDF.					
545	Trimley St Martin	Kirton Road, parallel to A14, Trimley St Martin. Unlit country road.	Trees growing to road edge, leaving no walking space, also forces cyclists out further out into traffic The verges have been mown, but under the trees	Cut back trees as far as ensibly possible	Issues relating to maintenance have not been assessed for the IDF.					
548	<Null>	Grange Farm Avenue, close to junction with Brackley Close	There is a traffic-calming measure here which requires east bound traffic to give way to westbound traffic. However some motorists often do not respect oncoming cyclists when the cyclist has right of way and this has clear potential to cause a head-on collision. (There is a sign that reads "think bike" however it faces traffic that DOES have right of way so I am not sure what its purpose is). There is a similar issue with Mill Lane at the point where the bridge crosses the railway line.	Either - reverse the sign that reads "think bike" so that it faces traffic that is required to give way; Or, preferably, remove the aforementioned sign and replace with a sign that more specifically reminds traffic that they need to give way to cyclists when the cyclist has right of way.	Not assessed for IDF.					

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549	Felixstowe	South Hill, Felixstowe	Due to parking of cars on both sides the width of carriageway available on South Hill is limited and it is not possible for a car to pass a cyclist safely, and many motorists especially those descending refuse to slow down or wait for cyclists and pass dangerously, there is the risk that a speeding motorist coming down the hill will have a head on collision with a cyclist climbing the hill.	Make South Hill one way for motor vehicles, I suggest this should be uphill only (and retain two-way passage for cyclists) reflecting the solution arrived at for Bent Hill several years ago as a response to a serious accident. Convalescent Hill is the only one of the three roads ascending the cliff in this area between Sea Road and the Spa Pavilion that is suitable for through motorised traffic.	Modal Filter comments from the community comment section have not been assessed in IDF.					
550	Little Bealings	Playford Road and Martlesham Road, Little Bealings	The Parish Council is aware that both these roads are used regularly by cyclists, including cycling clubs at weekends, and by walkers passing between footpaths. The route is a rat run to Ipswich for vehicles seeking to avoid the A1214 and there has long been concern over the volume and speed of traffic	Traffic calming, such as width restriction or a barrier across part of the road. There was hatching in Martlesham Road, but this faded and SCC did not replace it. There was also a surface change introduced in Playford Road at one time, but this has also gone due to resurfacing.	Modal Filter comments from the community comment section have not been assessed in IDF.					
551	Aldringham Cum Thorpe	Old rail line running between aldeburgh and crown farm, lovers Lane, leiston, sizewell	Restore old rail line route from Aldeburgh to leiston (crown farm junction) a hard surfaced cycle route for tourists. This could then be extended through to Southwold	Suffolk's own cinder track for cyclists. Smooth hard surface available to all and not just hardcore 'off roaders'	N/A	£1,200,000-£1,300,000	SCC, DFT	SCC, DFT	N/A	Beneficial
552	Woodbridge	JUNCTION between Warren Hill Road and Ipswich Road.	This is a very dangerous junction for cyclists turning right into Warren Hill Road. Motorists coming down the hill are going faster, also they often fail to see cyclists waiting in the centre of Ipswich Road to turn right; the driver side A pillar of their vehicle obscures the waiting cyclist. Also, vehicles bearing right round the bend tend to move to the centre of the road. This is so dangerous I will no longer make this turn by bike.	There needs to be a safe space for cyclists in the middle of the road. This requires an illuminated island at the junction and line markings on the road indicating cyclist space. NOT just white lines, these could cause more problems by giving the appearance of safe space. There have already been accidents involving cyclists at this junction.	Not assessed for IDF.					
553	Woodbridge	Junction of Ipswich Road with Warren Hill Road	When cycling up the hill along the Ipswich Road it is very dangerous turning right into Warren Hill Road. The oncoming traffic is fast, often breaking the 30mph speed limit, because the road is wide and the traffic is going downhill. Visibility for both traffic and cyclist is poor because it is on a blind bend. The cyclist is forced to wait in the middle of the road, between lines of traffic.	A safe space for cyclists in the centre of the road. Painted white lines as these are not visible enough to traffic, and could even make the problem worse by creating an illusion of safety for cyclists. Cyclists need to feel safe. An island is the only solution.	Not assessed for IDF.					
554	Woodbridge	Cumberland St off the B1438	Pavements are way too narrow here. Since resurfacing, cars go far too fast, often on the school run. Pedestrians have to walk on the road to maintain social distancing and are constantly at risk from vehicles.	This lovely medieval street should be shared use; space for vehicles should be reduced to one way with passing places and pedestrian space should be made wider by use of bollards and planters; an inexpensive solution. Ideally, resurface at one level.	Modal Filter comments from the community comment section have not been assessed in IDF.					

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556	Woodbridge	Cumberland Street off B1438	Cumberland St is a beautiful medieval street which should be a pleasure to walk along. Instead it is an intimidating place because the pavements are very narrow. Cars go very close by at 30mph (and sometimes more) as there is nothing to slow them down, since the road is very smooth and the double yellow lines keep the road generally free of parked cars. People frequently need to walk in the road, if they need to pass each other, or walk two abreast for example.	Shared space for vehicles and pedestrians. Traffic could be slowed easily by putting planters alongside the pavement at intervals, narrowing the access for traffic and making it slow down. Drivers should be made aware that they need to share this space with other road users.	Modal Filter comments from the community comment section have not been assessed in IDF.					
557	Woodbridge	Kingston Field	Kingston Field is entirely surrounded by kerbed areas; there is, surprisingly, no disabled access to this field.	Put in flat driveway type access in at least two places. Not too expensive and VERY disabled friendly.	Not assessed for IDF.					
559	Woodbridge	Kingston Field, Woodbridge	No disabled access on to this important and intensively used council owned leisure space.	There should be two points of access, I suggest one at the bottom of Cherry Tree Road and another near the car park entrance on The Avenue.	Not assessed for IDF.					
562	Woodbridge	Quay St, Church St, New St..	All these streets have inadequate space for pedestrians. Pavements are too narrow, vehicles go too fast.	Widen the pavements; if need be with temporary bollards, helping to maintain social distancing. Slow down the cars with obstructions. Better still, shut the cars out.	N/A	£450,000-£500,000	SCC	SCC	N/A	Beneficial
563	Melton	Melton Rd, Woodbridge to Melton	Cycle use of this road is dangerous. Cars move too fast and the road has no cycle lanes.	20 mph speed limit would be helpful here. Purpose built cycle path ideally, until then marked cycle lanes on the road.	Speed reductions have not been assessed for the IDF.					
565	Woodbridge	The whole of Quay Street, Church Street and New Street, Woodbridge	Here we have beautiful medieval town centre streets which are impossible to walk along feeling safe because the pavements are so narrow. Priority is given to the traffic using these streets, with pedestrians having to get out of the way. This traffic goes close by at 30mph (or more if it s breaking the current speed limit). As well as being dangerous is is polluting and noisy, especially HGVs. People must be allowed to feel safe, and be able too social distance from other pedestrians.	The traffic must be slowed down, and much more emphasis must be placed on traffic giving way to pedestrians. Pavements could be widened and the roads narrowed until the traffic can be shut out completely. Even Quay street could be treated in this way. The other two roads are one way so could easily be narrowed. Chicanes along New Street (one on South side of B1079, one near Mariners Pub) would slow traffic coming down the hill here. Another solution is to take away all distinctions between pavements and road, levelling the whole space in order to make the dominant hierarchy of road usage by cars less clear, forcing traffic to slow down for pedestrians,	N/A	£450,000-£500,000	SCC	SCC	N/A	Beneficial
571	Foxhall	Junction from Felixstowe Road (A1156) and Straight Road	Cyclists seek westbound on the popular A1156 Felixstowe Road seeking to turn north up Straight Road have a limited opportunity to safely merge to the centre of the road with fast moving traffic behind them.	Provision of a cycleway along the A1156 and any additional safety features to enable cyclists to be able to turn right in to Straight Road (and potentially right from Straight Road on to the A1156).	Very High	£2,400,000-£2,500,000	Developer, SCC, CIL, DFT	S106, S278, CIL, SCC, DFT, NH	Significant access improvements and improvements to the wider Land at Felixstowe Road and Footway improvements at Ransomes,	Critical

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574 <Null>		Southwold to Felixstowe via Woodbridge	Following a good deal of British success at the elite level and a general desire to improve mental and physical health, cycling has become an increasingly popular activity, whether it be commuting or for leisure. However, poorly lit roads and busy traffic prevent it becoming more commonplace with people still opting for four wheels rather than two.	There is enough open space to build a cycle path from Lowestoft to Felixstowe via Woodbridge. This could be done quickly and at modest expense.	Comment too generalised to assess in IDF.					
575	Lowestoft	Between Corton Long Lane, Lowestoft, NR32 5, GBR going northwards to Hopton roundabout lack of cycle path/footpath	Cyclists/pedestrians/currently use the busy A47 or the bendy coast road B1385 which has no footpath. As a motorist I see the dangers of cyclists using this fast dual carriageway, even if they are entitled to, but people make bad choices. I have even seen a person in a mobility scooter using this road. Death wish. As a cyclist and pedestrian I use the coast road every time, but it is bendy, there is no footpath and it is a bus route.	It would be a great amenity and so much safer to have a cycle/footpath between Corton Long Lane, Lowestoft, NR32 5, GBR going northwards to link with the existing one at Hopton. It might help mobility scooter users too. Near the roundabout there would ideally have to be some kind of crossing point to link users into the existing track past St Margaret's Church, Hopton (grid ref: TG 5241 0004) on the old Lowestoft Road.	High	£1,000,000-£1,100,000	NH, SCC, Developer, CIL	S278, S106, CIL, SCC, NH, DFT	Cycle link between Lowestoft and Hopton and potential safety improvements to A47 to accommodate the North Lowestoft Garden Village (Policy WLP2.12)	Essential
576	Lowestoft	The clifftop cycle path/footpath at Pakefield going from The Jolly Sailors. Pakefield Street, NR33 0JS, to Arbor Lane	It's rather narrow for the amount of users it gets, especially at weekends and peak holiday times. The path is used by pedestrians, dog walkers, people in mobility scooters and cyclists and there has to be a lot of give and take between them. It can be snail pace for cyclists.	Widening of the route and having a dedicated cycle path would make life a lot easier for all concerned and allow cyclist to make progress.	High	£1,200,000-£1,300,000	SCC, ESC	SCC, ESC	N/A	Beneficial
577	Rushmere St Andrew	A1214 cycle route through Kesgrave plus other locations	Like many of the cycle routes alongside roads in Suffolk cyclists need to give way at junctions. This requires looking over the right shoulder to look for cars turning left. This is dangerous and is also a major inconvenience having to slow down or stop at junctions. If cycling on the road the cyclist like vehicles has a right of way across the junction. Also pedestrians have a right of way at junctions according to the highway code.	I lived in Munich for 2 years and cycled there. Cycle routes had a right of way over side roads that they crossed. It worked well all vehicles gave way as needed.	Comment too generalised to assess in IDF.					
580 <Null>		General comment about public footpaths	Officially public footpaths are not for use by cyclists. A lot could probably be opened up to cyclists and would provide safe off-road routes.	Open suitable public footpaths to cyclists	Comment too generalised to assess in IDF.					
581 <Null>		Speed of cars on country lanes endangers cyclists and pedestrians	cars travel too fast on country lanes and endanger cyclists and pedestrians	For many country lanes (especially single track lanes) a realistic speed limit would be 30mph. The speed limit on country lanes should be reduced to 30mph. It would probably have a minimal effect on journey times along the country lanes for cars. It would also improve villages if the speed limit within the settlement boundary is 20mph. This would also reduce CO2 emissions etc.	Speed reductions have not been assessed for the IDF.					

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582	Trimley St Martin	Cars parked near the shop	Highly dangerous to cycle past the shop area (in particular in the east direction) due to slowing / stopping cars that are parking for the shop, also cars pulling out after using the shop. Frequent near misses due to poor awareness of cycling traffic. Cycle lane is constantly parked on. The road is also very narrow at this point.	20 mph zone? mandatory cycle lane? Dedicated parking bay surrounded by double yellow lines? parking enforcement?	Speed reductions have not been assessed for the IDF.					
584	Melton	Woods Lane	Children use this route for cycling from Melton to Farlingaye school. It is very busy with huge lorries coming to and from Rendlesham Bentwaters. Needs shared cycle/footway or cycle Lane to make safer for cyclists.	Children use this route for cycling from Melton to Farlingaye school. It is very busy with huge lorries coming to and from Rendlesham Bentwaters. Needs shared cycle/footway or cycle Lane to make safer for cyclists.	Very High	£800,000-£850,000	SCC	SCC	N/A	Beneficial
586	Woodbridge	Theatre Street and Burkitt Road	I either cycle or walk my son to playgroup at St Mary's Primary School before I head off to work. Walking or cycling are both a bit hairy as the traffic often zooms by on this stretch – there's no indication of what the speed limit is so people take that as licence to go as fast as they please – often speeds in excess of 30mph. This is a busy stretch filled with children on the way to Farlingaye and St Mary's – please put up a 20mph sign!	Please put up a 20mph sign!	Speed reductions have not been assessed for the IDF.					
588	<Null>	Sandy Lane, Woodbridge, Ipswich Rd junction to railway bridge	Sandy Lane is a dangerous place to walk because there is no escape from speeding traffic! The stretch from Broomheath Rd to the Railway Bridge (part of Circular River Walk) is especially dangerous. The narrowness and blind bends make it unsafe.	Sandy Lane needs a footpath! And a 20mph speed limit.	Very High	£850,000-£900,000	SCC	SCC	N/A	Desirable
602	Martlesham	GR 260 451	At present, ATs aiming for the Martlesham Retail Park and to cross the A12 via the foot & cycle bridge or either of the tunnels in order to reach the Martlesham P&R, Kesgrave High School, Ipswich Hospital, Town, buses or rail station, and visitors coming the other way, tend to cycle along the tarmac strip as footpaths #23 & 43 are very rough.	When Brightwell Lakes are developed, good cycleways to the A12 crossings, must be provided	Very High	£1,100,000-£1,200,000	SCC, DFT	SCC, DFT	N/A	Beneficial

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607	Woodbridge	General	The issue is that most if not all the few existing cycle paths are marked poorly. There is no right of way marked for pedestrians or cyclists on the existing paths (ie A12 path or Martlesham to Ipswich). Most byways and other footpaths positively discriminate AGAINST cyclists, with for example, much protest about mostly harmless cycling on the river wall and bars to prevent cycles passing at most town footpath entrances and exits.	<p>Campaigns to promote a cycle 'economy' around new cycle routes, recognising that every cyclist reduces congestion for road users, reduces pollution, increases the mental and physical health of the cyclists themselves, which in turn saves more money for NHS and authorities.</p> <p>Promotion of positive recognition of cyclists who deliberately commute to better their health and lower local pollution, (combatting climate emergency) vs the negative/destructive effect of driving short distances to school and work. School promotion of cycling within a certain distance instead of driving, especially where onward commute to work is not a consideration.</p> <p>Enforce existing traffic legislation designed to promote the safety of cyclists. (ie speed limits, distances for passing cyclists, parking on cycle paths).</p>	Comment too generalised to assess in IDF.					
608 <Null>		General	Nearly all cycle paths stop abruptly at some point with direction onto a busy road with poor direction and often no further option but to stay on the road. Even the poorly marked cycle paths on main roads are usually blocked at some point by parked cars. Hurried commuters often have little time for slower cyclists who are often viewed as a non-road-fee-paying nuisance. Walkers are well catered for in most areas but can view the bike as an unwelcome nuisance also.	Support this campaign by creating and investing in a considered and continuous infrastructure of cycle paths and facilities, such as marking paths with cycle and pedestrian areas, widening existing paths, traffic reduction schemes citing the reason for promoting cycling. (such as the welcome sign for the Thoroughfare which says 'except cycles'). These paths should connect outlying villages as well as provide cross-town routes, cycling off road wherever possible. (ie routes from Bromeswell to Woodbridge using part of the river wall, which is wide enough to accommodate cycles and pedestrians. Rendlesham to Woodbridge, Bredfield to Woodbridge, Hasketon to Woodbridge, and so on).	Comment too generalised to assess in IDF.					
609	Melton	General	Encourage a cycle lock or loop fixed to walls outside certain shops, where appropriate. Invest in wider recreational cycle route creation to enhance the area for local cyclists, pedestrians and (staycation) tourism. (ie river wall route from Wilford Bridge to Felixstowe Ferry).	Further interconnection between towns and villages of the area, including tackling awkward areas where there is seemingly less space for cycle paths, such as from the outskirts of Woodbridge towards Martlesham where routes into Ipswich are found.	Comment too generalised to assess in IDF.					
610	Barnby	Barnby Bends	Large dip on westbound although road surface not broken. Possibility of dismounting cyclist since it is downhill and cyclists could be travelling at reasonable speed. Almost dismounted cyclist in front of me yesterday - I am aware of dip so can avoid	Large dip on westbound although road surface not broken. Possibility of dismounting cyclist since it is downhill and cyclists could be travelling at reasonable speed. Almost dismounted cyclist in front of me yesterday - I am aware of dip so can avoid	Highway matter not assessed by IDF.					

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613	Lowestoft	Lowestoft Promenade	I read there are several items on the agenda for safety, need and encouragement for even more cycle lanes to be improved, eg new lines to be re painted along the promenade. Surely this is such an easy task, low cost and needs no consolidation, as the cycle lane is already in use?	So, I ask this is to be given priority, after all there is no money issue, as I also researched the funding that central Government had given to you, I believe the sum of three million, this was to spend to fast track for cycle corridors, in the wake of the Covid 19.	Issues relating to maintenance have not been assessed for the IDF.					
614	Lowestoft	Pakefield High School (opposite)	My last request, for the spending of the money given to Lowestoft, for the high demand and in identifying the NEED for a new cycle lane opposite Pakefield High School, NR337AQ. I travel on London Road frequently, either on my bike, walking or by my car. Last Thursday afternoon, when the student were finishing school, I witnessed a child stumble into the road, he was very lucky not to be injured.	I can see from your plans that Arbour Lane, MAY be improved? Look at taking this new cycle lane from Mc Donald's roundabout to Pakefield road and connects to the existing track along the promenade. There are over three hundred students at this school, the new safety improvements need to happen promptly. The safety of everyone in that area should not purely be down to luck.	High	£1,200,000-£1,300,000	SCC, ESC	SCC, ESC	N/A	Beneficial
616	Lowestoft	The Promenade	May I please ask you to consider allowing cycling on the lower promenade during off peak times. For example, not during the peak holiday season or any Bank Holiday weekends. Additionally, when cycling in the designated cycle path on the top of the promenade, pedestrians who wander aimlessly across the path also give cyclists a great amount of abuse.	May I please ask you to consider allowing cycling on the lower promenade during off peak times. For example, not during the peak holiday season or any Bank Holiday weekends. Additionally, when cycling in the designated cycle path on the top of the promenade, pedestrians who wander aimlessly across the path also give cyclists a great amount of abuse.	High	£1,200,000-£1,300,000	SCC, ESC	SCC, ESC	N/A	Beneficial
617	Lowestoft	Sparrows Nest	cycling North up the High Street, but heading to The Sparrows Nest park, involves crossing lanes of traffic, around the central island where the garage is. As I want to get to Gunton Cliff and down Links Hill to cycle back to town along the Cycle path along North Beach, I find this section really dangerous.	cycling North up the High Street, but heading to The Sparrows Nest park, involves crossing lanes of traffic, around the central island where the garage is. As I want to get to Gunton Cliff and down Links Hill to cycle back to town along the Cycle path along North Beach, I find this section really dangerous.	Very High	£2,000,000 - £2,100,000	SCC, NH, DFT, Developer	SCC, NH, DFT, S106, S278, CIL	Potential safety Improvements to A47 to accommodate the North Lowestoft Garden Village (Policy WLP2.12)	Potentially critical
619	Wickham Market	Between Potsford Brook and the footpath that goes to the Gallows on the B1078 west of Wickham Market.	There is already an improved suggestion but if the landowner declines to allow walking along the field edge on the north side of the 1078, then consider opening up a part of the woodland on the south side as a right of way or permissive path.	There is already an improved suggestion but if the landowner declines to allow walking along the field edge on the north side of the 1078, then consider opening up a part of the woodland on the south side as a right of way or permissive path.	Not assessed for IDF.					

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620	Letheringham	Just north of Letheringham (the Street) on the way to the Hoo/Easton road.	There is a huge run off of wet mud from the field there and this creates an uneven, rippled and potentially hazardous surface for people on bikes.	Persuade the owner of the land/field to clear the mud on a regularly and frequently.	Issues relating to maintenance have not been assessed for the IDF.					
621	Southwold	From the bridge follow the line of the old railway up to Halesworth.	Although good footpaths and bridleways, the line of the old railway is not immediately apparent.	Join up the various footpaths and bridleways to create a cycle route between the River Blyth and Halesworth to follow the route of the railway.	N/A	£1,600,000-£1,700,000	SCC, DFT, Developer	SCC, DFT, S278, S106, CIL	Access, sustainable transport, cycle and footway improvements for South Saxmundham Garden Neighbourhood (Policy SCLP12.29)	Critical
622	Melton	The Street, Melton	This is a historic route. The road is narrow and so are the pavements. Many of the buildings are hard against the pavement. At peak times, the vehicles are nose to tail. Pedestrians, including families on their way to school, have to run the gauntlet between the vehicles and the buildings, wreathed in exhaust fumes.	Measure the air pollution in real time to better understand the scale of the problem. Make The Street a no idling zone.	No Cycling and Walking infrastructure improvement suggested.					
628	Kesgrave	The A1214 between Ipswich and the A12 junction and the cycle footways alongside the A1214 that's used for Kesgrave High School access	1) The A1214 between Ipswich and the A12 junction is a key route for everyday transport cycling but is congested/polluted and on-road improvements are needed. 2) The design of the cycle/footways by Kesgrave Fisheries and Kesgrave High School are not fit for purpose and also need repair/resurfacing 3) Damage to the cycle/footways is exacerbated by vehicles driving and parking on them and vehicles also cause obstructions 4) The side road cycle priority crossings have also deteriorated.	1) Make the whole of the A1214 between Ipswich and the A12 junction a 20mph zone with priority for cyclists. It runs past a school and residential housing and lower speeds would make it safer /more attractive for cyclists/pedestrians 2) Widen the road across Rushmere Heath to create dedicated cycle lanes on either side, separated from the footway. And plant Oak/Birch etc trees along the Heath edge 3) Turn the sections of shared cycle footway by Kesgrave Fisheries, Kesgrave High School etc into wide attractive pedestrian-only routes - they are too narrow /dangerous for shared use by cycles/pedestrians/mobility scooters/wheelchairs/buggies 4) Where space allows e.g. by KHS the new pedestrian-only route could be designed and built as a wide and pleasant tree-lined boulevard to accommodate the very high level of foot traffic at school times including buggies, dogs etc. Trees would also help soak up some of the traffic pollution and help improve health, the environment and visual amenity.	Speed reductions have not been assessed for the IDF.					

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629	Kesgrave	A1214 Kesgrave especially its junction with Bell Lane and the section up to All Saints Church and Ropes Drive West roundabout and in the other direction going to Heath Road roundabout	1) The cycle/footway is too narrow on south side of A1214 and at Bell Lane junction and is heavily used for walking and cycling to/from Kesgrave High School 2) There is no pedestrian crossing of the A1214 and this is needed to enable people to cross the road from All Saints Church to access the Cemetery, Carpet Cuts and the bus stop 3) High level of air pollution by The Bell caused by traffic congestion and queing here which creates health risks for everyone- especially car drivers and occupants	Redesign A1214 corridor as safe and attractive for people to walk, cycle and use a bus. Helps address the climate emergency and public health crisis (reduces NHS burden if people can choose active travel). Make the A1214 a priority route for cyclists, buses and disabled users who need to use their cars. It's a key bus route and First Bus have previously asked for improvements to A1214. In return, ask them - with support from local councils/central government funding - to offer free bus use for a month (+ ongoing offers) to persuade people out of cars e.g. The Park and Ride bus service is excellent but few people have tried it. More bus use = less single occupancy car use +less congestion and pollution. Turn A1214 into a 20mph road to encourage cycling, offer free cycle training and bike repairs locally. Redesign the cycle/footway on the south side of A1214 as a pedestrian-only route with pedestrian crossing of A1214 and ped/cycle/bus friendly redesign of the Bell Lane/a1214 junction.	Modal Filter					comments from the community comment section have not been assessed in IDF.
631	Rushmere St Andrew	A1214 across Rushmere Heath	Key section of route in the corridor between Ipswich - Kesgrave - Woodbridge. Cyclists have no alternative routes available which are safe and convenient e.g. the footpath across the Heath is a footpath - a sandy track across which there is no legal right to cycle and there is also a risk of being hit by golf balls. And the route via Rushmere village is a long detour. If we are to encourage more people to cycle then this key section of route needs some cycling provision. It's a missing link.	Widen the A1214 here to create dedicated cycle lanes on either side of the road, segregated from the pedestrian footway. Widen the footway on either side so it's suitable for mobility scooters, wheelchairs, buggies etc. Plant suitable trees along the edge of the footway and Heath - Birch, Oak etc? and a shrub layer - gorse? to create an attractive and sheltered route for pedestrians and an attractive feature in the landscape. I think the land either side of the A1214 here is Common Land - if so, then can the Council find an area of land, comparable in size and in quality in terms of wildlife/landscape quality and public amenity/access in East Suffolk to dedicate as Common Land to substitute/compensate for that taken? And as an enhancement, perhaps East Suffolk could discuss with the owners any appropriate support for wildlife e.g. a wildlife tunnel underneath the A1214 road if helpful for connectivity for amphibians /reptiles other creatures in lowland heath habitats or other support?	High	£1,200,00 - £1,300,000	SCC, DFT	SCC, DFT	N/A	Beneficial
632	Playford	Playford Road between junction with Bent Lane and Hall Road and along Martlesham Road	High traffic speeds. Feels very dangerous to cycle along Playford Road. Also drivers often play chicken - overtaking me on my bike when there is oncoming traffic and they cut in front of me. There have been far too many near misses... It must be terrifying for the oncoming cars too.	I am very impressed with the recently installed speed cushions further down Playford Road between Humber Doucy Lane and Bent Lane. A big thank you to whoever initiated/funded/implemented these. There is just enough space between the cushion and side of the road for cyclists to pass and the cushions are successful in slowing traffic speeds. Also, the new mini-roundabout by Bent Lane /The Street /Playford Rd seems to have helped slow traffic speeds too. Can speed cushions be installed all the way along Playford Road and Martlesham Rd please? It is a key cycling route, but too terrifying for many people to use. And lower speed limits would hopefully benefit pedestrians too?	Speed reductions					have not been assessed for the IDF.

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637	Lowestoft	going from High Street north on A47 (towards Corton)	I have no idea what I am supposed to do at the top of the High Street on a bicycle. There is a cycle lane coming south but I do not want to use it going into on-coming traffic. There is confusion about what pavement cycling as sometimes marked and then disappears. I don't want to cycle on the A47 as it is too fast but there is no alternative but more importantly NO SIGNAGE at all. The DENES HIGH SCHOOL is on the A47 and currently no cycle path from south to allow pupils to cycle safely.	Proper cycle ways that are NOT on the road and NOT on the pavement. Cyclists need to be protected from traffic on A roads. A cycle way along the whole of the A47	Very High	£2,000,000 - £2,100,000	SCC, NH, DFT, Developer	SCC, NH, DFT, S106, S278, CIL	Potential safety Improvements to A47 to accommodate the North Lowestoft Garden Village (Policy WLP2.12)	Potentially critical
638	Kessingland	Kessingland + A12 going south	There is no cycle route at all. There is no way for cycles to travel safely along the A12. How do we even get to Benacre from Lowestoft? Why no cycle way along the A12?	Cycle way along the A12. At present no way of getting to Lowestoft until Kessingland is reached (and then it's not very good)	Very High	£4,000,000-£4,100,000	SCC, NH, DFT	SCC, NH, DFT	N/A	Essential
639	<Null>	Whole of Lowestoft	Cycle routes are good in the town of Lowestoft (compared to other UK cities/towns but NOT when compared with most of Europe). However there is NO WAY of getting OUT of LOWESTOFT to other places: Yarmouth unsafe, Southwold unsafe/non-existent; Beccles not great; Hadiscoe very unsafe and non-existent. How do people work in other places and commute by bike (or even public transport)?	You need to work with Highways and Norfolk. There is just no investment to go from place to another. Why not work with Sustrans? (who have pulled out of Suffolk because not enough funding). More strategic thinking about cycling as a mode of transport NOT just a Sunday jolly.	Comment too generalised to assess in IDF.					
641	<Null>	Cycle paths and footpaths throughout East Suffolk	Concerns about the surface and width of footpaths and cycle paths.	Cycle paths and foot paths should be at least 2 metres wide to allow for two wheelchairs to pass. The surface should be tarmac so that all people can walk / use wheelchairs easily. They should be reasonably level, with no hills or steps, or gates. They should be regularly maintained.	Comment too generalised to assess in IDF.					
646	Waldringfield	Footpaths in and around Waldringfield, and elsewhere throughout East Suffolk (Ref186)	Waldringfield Parish Council agrees with this. WPC has put up No Cycling signs on several footpath following complaints by residents, and most of these have been destroyed, presumably by cyclists.	Waldringfield Parish Council agrees with this. WPC has put up No Cycling signs on several footpath following complaints by residents, and most of these have been destroyed, presumably by cyclists.	Comment too generalised to assess in IDF.					
647	Waldringfield	River Wall north of Waldringfield (Footpath 11)	There is a serious problem on the river wall footpath north of Waldringfield (FP11), where cycling damages the structure of the river wall and could eventually result in a breach. No cycling signs are regularly ignored by cyclists.	Barriers would be effective but are problematic because they make access for mobility vehicles difficult. Better signage might help.	Not assessed for IDF.					

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648	Waldringfield	Waldringfield (Ref 409)	Waldringfield Parish council agrees with this, except that we do not support compelling pubs such as the Maybush to provide toilets – encouragement is far better. The absence of public toilets leaves walkers with little choice if they are ‘caught out’, resulting in health hazards as well as being offensive and off-putting.	A public toilet in the Maybush car park. There should also be far more litter bins at the start and end of public footpaths.	Not assessed for IDF.					
650	Marlesford	lack of cycling facilities Framlingham - Parham - Hacheston - Wickham Market station	The B1116 is a very busy road, and parts have a national (60mph) speed limit. Some has 30/ 40mph but from Brick Lane to The Street in Parham there is no alternative. There is a back-lane route from Hacheston to Campsea via Marlesford but there is no safe crossing of the A12. A significant number of cyclists do use the A1116 but only fit and fast ones.	Re-create the Framingham branch railway line for walking and cycling. For much of the way from Framingham to Marlesford there are public footpaths paralleling the old railway alignment, or very near by. These could be diverted, through negotiation, and joined up to follow the track bed, and be reclassified as bridleway or cycle track. In the longer term the track bed could be acquired and the surface upgraded. As an extension - though more complex - path could be extended along the old freight railway line to Snape Maltings. There are very few truly traffic-free cycling facilities in this part of Suffolk (that are not muddy). This could develop into a fantastic and very well-used facility for leisure and other purposes.	N/A	£2,100,000 - £2,200,000	SCC, DFT	SCC, DFT	N/A	Beneficial
651	Felixstowe	from the Dip northwards to Felixstowe Ferry along sea wall/ promenade	to be consistent with Prom south of Cobbolds Point, allow cycling access as shared use with pedestrians along prom/sea wall north off Dip. This will mean children / families won't have to use fast section of Ferry Rd through golf course if they wish to get to Fx Ferry - a popular spot for families. Also, Fx Ferry as a dead end, has a traffic and parking congestion problem, which improved cycle access to the hamlet would help mitigate.	Give permission for considerate cycling, while maintaining pedestrian priority. Narrow stretch near Cliff car park may need widening or signs for cyclists to dismount for this short stretch.	High	£450,000-£500,000	SCC	SCC	N/A	Beneficial
652	Lowestoft	It is a pointless exercise suggesting improvements to local infrastructure unless there is a coherent plan for cycling in Lowestoft.	<ul style="list-style-type: none"> - Lowestoft with its relatively flat terrain and low car ownership should be leading the way. - Instead there is a mish-mash of side streets and a few reasonable cycle routes. Few join up and almost all end in dangerous exit points at roundabouts and junctions. - Few routes are safe for children - No attempt to encourage cycle tourism, such as routes from the station to Oulton Broad or Carlton Marshes, or even signage to the beach! 	<p>First, come up with a proper co-ordinated strategy for cycling in Lowestoft not just minor cosmetic improvements (I would be happy to contribute).</p> <p>Secondly prioritise safe direct routes into town that you would be happy to let your children use.</p> <p>Thirdly, encourage cycle tourism by making Lowestoft a hub for routes to the Broads, and along the river Waveney.</p> <p>Fourthly get Sustrans and Lottery funds to make safe cycle tracks not dotted lines on the main road.</p> <p>Finally where there are shared routes with pedestrians, look at ways of separating the activities (eg different coloured surfaces) to increase pedestrian safety and acceptance of dual use routes.</p>	Comment too generalised to assess in IDF.					

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654	Aldringham Cum Thorpe	B1353 running from Aldringham to Thorpeness	This road is heavily used by families to cycle to and from Thorpeness. The speed of traffic combined with the ever reducing width of the road makes this activity very dangerous.	A new cycle path/footpath linking these two villages would reduce the ever increasing risk to cyclists and pedestrians.	N/A	£1,300,000-£1,400,000	SCC, DFT	SCC, DFT	N/A	Desirable
657	Sudbourne	Sudbourne	Cycling: 1. Time trial and organised events: The B1084 (Melton to Orford) is a popular route for time trial and organised events but has many narrow sections which can put cyclists and other traffic in conflict. In particular large agricultural vehicles with restricted speed, manoeuvrability and driver visibility can be hazardous for cyclists. This is a particular problem in mid / late summer with long daylight hours when they are on the same road at the same time.	1. Time trial and organised events: Organiser of these events should pre-warn affected Parish Councils of their intention to hold these organised events and routes in advance: to enable landowners / farmers in particular to ensure their vehicle movements are planned to ensure that there is minimal or reduced mixing of cycles and large agricultural vehicles. The onus has to be on the event organisers to ensure this is done in a timely manner. Information and advice for cyclists should be located at strategic locations such as Honey and Harveys in Melton a frequent meeting point for cycling groups. Event organisers should include their contact details on all roadside signage and once cycle events have been completed, they are responsible for its removal of all to reduce the amount of roadside litter created.	No Cycling and Walking infrastructure improvement suggested.					
659	Sudbourne	Sudbourne	2. Condition of Suffolk Coastal Cycle Route 41 (Orford to Iken / Snape via Ferry Road through Sudbourne): This promoted rural route is quiet, picturesque and in many ways ideal for cyclists. However, the route suffers from multiple large areas of sand that have run-off from fields in particular near gate / road ways. This sand surface is especially dangerous for cyclists with smooth road tyres who have no grip on such surfaces.	2. Condition of National Cycle Route 41 (Orford to Iken via Ferry Road in Sudbourne): There are potentially three solutions that may be used individually or in combination. 1. Information should be added to publicity of the route that this is a hazard for cyclists to be aware of. 2. Information on the actual route should highlight the hazard in advance for cyclists 3. The land owners / Suffolk County Council should ensure the roads are clear of this washed off material.	No Cycling and Walking infrastructure improvement suggested.					
660	Sudbourne	Sudbourne	3. Snape road and B1084 Snape to Orford. This road in particular is often very busy with frequent blind spots and drivers who drive too fast for the prevailing road conditions and don't anticipate individual and multiple cyclists. There are few safe passing places for cars and other vehicles on this road. An alternative for cyclist route should be investigated and implemented as a matter of urgency.	3. Snape road and B1084 Snape to Orford. In order to remove the hazards from the route from Orford to Snape an alternative route with a suitable surface should be built and clearly marked through Tunstall Forest. This would provide a safe cycling environment that would be enjoyed by a wide range of cyclists and reduce the hazard on the road. There are a variety of potential routes that can be explored in more detail which would enhance the riders experience and improve safety. 4. Information signs to bikers could be Tangham campsite , Snape Maltings , car park at Iken and Sandgalls	N/A	£850,000-£900,000	SCC	SCC	N/A	Beneficial
661	Wickham Market	There are pinch points on the Hill at Wickham Market, at the Post Office and at The Teapot Tea Rooms. The hill coming up from Bordercot Lane on to The Hill	Cyclists to feel safe these areas to encourage them to cycle in and around the village	The introduction of 20mph speed limits and 'shared space' for cyclists, pedestrians and vehicles.	Speed reductions have not been assessed for the IDF.					

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662	Melton	Woods Lane	Despite the 30mph zone, vehicles seldom adhere to it making this necessary pedestrian and cycling route very unpleasant and dangerous. In addition, for those wanting to turn into Woods Lane from side streets, the speed combined with the volume of traffic make this dangerous. There T-intersection with Leeks Hill is a public right of way frequented by walkers a school children and requiring them to cross.	Additional signage to ensure all drivers are aware of 30mph zone, and installation of a speed camera to ensure vehicle compliance. Potential taffic calming measures, including siganage and a pedestrian crossing point. Alternatively, and better still, reducing the speed to a 20mph zone would vastly improve this stretch of road for other users while only adding 60 seconds to vehicle journeys and reducing local noise and pollution.	Not assessed for IDF.					
663	<Null>	N/A	Beccles Town Council, noting that as Suffolk County Council also have a cycling and walking strategy, the ESC cycling and walking strategy should not duplicate this and that the two strategies should link together, particularly as Suffolk County Council are responsible for the highways and transportation infrastructure. The linking of both strategies is also important to ensure that all comments received by the separate strategies, are duly considered when the overall strategy is reviewed.	Beccles Town Council, noting that as Suffolk County Council also have a cycling and walking strategy, the ESC cycling and walking strategy should not duplicate this and that the two strategies should link together, particularly as Suffolk County Council are responsible for the highways and transportation infrastructure. The linking of both strategies is also important to ensure that all comments received by the separate strategies, are duly considered when the overall strategy is reviewed.	No Cycling and Walking infrastructure improvement suggested.					
666	Lowestoft	Lowestoft	The improvement that I feel needs making is that whilst it is reasonably possible to cycle within Lowestoft it is virtually impossible to cycle away from Lowestoft to any significant or interesting destination.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.	Comment too generalised to assess in IDF.					
667	Ashby, Herringfleet And Somerleyton	Between Haddiscoe and Reedham via Somerleyton	The marshes between Haddiscoe and Reedham via Somerleyton involves cycling along 'car fast' narrow lanes which have no provision whatsoever for cyclists, and any attempt to avoid fast roads involves miles of detours with in real terms no real gain in safety.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.	Comment too generalised to assess in IDF.					
668	South Cove	Lowestoft to Southwold	Lowestoft to Southwold involves large detours to avoid the A12 from Kessingland but eventually arriving at a very dangerous crossing of the A12 at Wrentham followed by several miles of very dangerous travel along the B road to Reydon and Southwold. again there is no provision whatsoever for cyclists.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.	Comment too generalised to assess in IDF.					
669	Ellough	Lowestoft to Ellough	Cycling to the Ellough farmers market from Lowestoft. The majority of this route is currently satisfactory despite no obvious provision for cyclists once out of Lowestoft, but at the end cyclists are deposited onto a very fast busy B road with no provision for cyclists.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.	Comment too generalised to assess in IDF.					

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670 <Null>		East Suffolk	We are of the opinion that in a period of scarce resources we consider that the three priorities for walking should be as follows: 1. Improve existing PROWs by maintaining gates, stiles, finger posts and signage and clarify who can and cannot access PROWs. 2. Ensure land owners co-operate with this maintenance and engage with SCC highways on how to improve condition of PROWs on their land. 3. Ensure Town and Parish councils appoint PROW officer and make sure public are aware who to contact.	We note that there are already a number of cycling routes supported by ESC and SCC and these should also be given greater publicity. Greater use of cycling is a much more complex issue that requires a high level strategic approach across all relevant councils. As a small parish we can support and publicise these routes as required.	Comment too generalised to assess in IDF.					
672	Trimley St Mary	<Null>	There are a couple of areas on this road that are pinch points and of particular danger to cyclists, not least outside the school entrance on the High road and near McColls shop. If there are no plans to re-paint or enhance the cycle lane provision in this area, are there any other plans to address road safety issues in these areas?	The Parish council are also keen to find out if there would be any funding available to introduce a mini roundabout at the High road / Station road junction. This would reduce speeding in the immediate area as well as improve the road junction. Extend the temporary 'mandatory' cycle lane through Walton and then through Trimley St Mary / Trimley St Martin	Not assessed for IDF.					
673	Southwold	Southwold Town Council	STC would like to support references that have been submitted already, namely: Refs: 333, 34 and 102 combined, and all references to the Coastal Path from north of the pier through Eastern Bavenets.	STC would like to support references that have been submitted already, namely: Refs: 333, 34 and 102 combined, and all references to the Coastal Path from north of the pier through Eastern Bavenets.	No Cycling and Walking infrastructure improvement suggested.					
674	South Cove	B1127 Lowestoft Road	The B1127, Lowestoft Road is particularly dangerous for walkers and cyclists and safety measures to improve the lot of each would be welcome.	The B1127, Lowestoft Road is particularly dangerous for walkers and cyclists and safety measures to improve the lot of each would be welcome.	N/A	£3,000,000-£3,500,000	SCC, DFT	SCC, DFT	N/A	Beneficial
676 <Null>		East Suffolk	In seeking to improve the cycling and walking experience in East Suffolk the safety of each is paramount - from separating them from motorised transport to ensuring that mountain and trial bikes do not despoil the environment by increasing erosion.	The most essential aspect for me, from a cyclists' viewpoint, has a to be ensuring that no parking is allowed in any cycle lane; it's crazy and euphemistic! Any mitigation, by way of educating and persuading car users to reduce their dependence upon the motor car, would be welcome, even to the extent of escalating car parking charges, perhaps?	Comment too generalised to assess in IDF.					
678	Campsea Ashe	B1078 between Campsea Ashe and Five Ways / Lower Hacheston	very dangerous conditions for pedestrians and cyclists trying to access the key bus stops at Five Ways from Campsea Ashe	Pavement / footway-cycleway; some can be done as pavement adjacent to kerb (e.g., in front of houses and Lower Hacheston) some as segregated track parallel to the road, behind hedgerows	N/A	£850,000-£900,000	SCC	SCC	N/A	Beneficial
679 <Null>		N/A	See attached.	See attached.	Comment too generalised to assess in IDF.					
680 <Null>		East Suffolk	See attached.	See attached.	Comment too generalised to assess in IDF.					

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688	Martlesham	Martlesham	<p>The feedback by local parishioners shown on the ESC interactive map reinforces many of the issues raised by MPC over several years, in particular about the need to make improvements to encourage sustainable and safer travel between Martlesham and Woodbridge. This is all the more important given the climate emergency which SCC, ESC and MPC have declared.</p> <p>We refer you to the Martlesham NP which has a section on 'Getting Around' – see Cycling, walking and disabled access, p43, policies MAR13 & 14.</p>	<p>The feedback by local parishioners shown on the ESC interactive map reinforces many of the issues raised by MPC over several years, in particular about the need to make improvements to encourage sustainable and safer travel between Martlesham and Woodbridge. This is all the more important given the climate emergency which SCC, ESC and MPC have declared.</p> <p>We refer you to the Martlesham NP which has a section on 'Getting Around' – see Cycling, walking and disabled access, p43, policies MAR13 & 14.</p>	Not assessed for IDF.					
689	Felixstowe	Felixstowe	See attached.	See attached.	Assessed Separately					
691	Felixstowe	Foxgrove Lane / High Rd (Walking)	Poorly signposted, heavily overgrown, poor surface	Poorly signposted, heavily overgrown, poor surface	Issues relating to maintenance have not been assessed for the IDF.					
693	Felixstowe	Brook Lane / Park Avenue (Walking)	Signposting, maintenance	Signposting, maintenance	Issues relating to maintenance have not been assessed for the IDF.					
694	Waldringfield	Footpaths in and around Waldringfield, and elsewhere throughout East Suffolk	<p>With the rising popularity of cycling we seem to have lost respect for the differences between footpaths and bridleways. Cyclists seem to no longer acknowledge that footpaths are not for cycling along, making it potentially dangerous for walkers and causing damage to footpaths. In the same way that cyclists wish to see improvements to the road infrastructure to feel safe from vehicles we need to acknowledge that there are similar issues on footpaths...which are NOT rights of way for cyclists.</p>	<p>For context we have included the comments taken from the map, WPC's responses are labelled as 'our response'.</p> <p>A campaign of education about the differences between footpaths and bridleways coupled with improved signage and potentially sanctions for non compliance</p> <p>Our response: We agree with this. WPC has put up No Cycling signs on several footpath following complaints by residents, and most of these have been destroyed, presumably by cyclists.</p> <p>We also have a more serious problem on the river wall footpath north of Waldringfield (FP11), where cycling damages the structure of the river wall and could eventually result in a breach.</p>	Not assessed for IDF.					

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695	Waldringfield	Waldringfield	For context we have included the comments taken from the map, WPC's responses are labelled as 'our response'. No WC accessible to the public walking or cycling in the area. It would get more people out walking and/or cycling if they could feel sure that they would be able to find WCs en route. Waldringfield is a classic example of a place in a prime location for walkers, but no toilets. This applies to most villages these days so Waldringfield is just one example.	For context we have included the comments taken from the map, WPC's responses are labelled as 'our response'. Our response: We agree with this, except that we do not support compelling pubs such as the Maybush to provide toilets – encouragement is far better. The absence of public toilets leaves walkers with little choice if they are 'caught out', resulting in health hazards as well as being offensive and off-putting. There should also be far more litter bins at the start and end of public footpaths.	Not assessed for IDF.					
697	Felixstowe	Martello Lane / beach (Walking)	Signposting, maintenance	Signposting, maintenance	Not assessed for IDF.					
701	Felixstowe	York Rd / rear St. Felix Church (Walking)	Signposting, maintenance	Signposting, maintenance	Not assessed for IDF.					
707	Felixstowe	Footpath xx Philip Avenue to Coronation Drive	Was closed due to slippage. What is current status?	Was closed due to slippage. What is current status?	Not assessed for IDF.					
710	Felixstowe	High Row Field / High Road (Walking)	Status? Created as part of High Row Field development. Signposting, maintenance. NB reference effects of potential redevelopment of Brackenbury Sports Centre site.	Status? Created as part of High Row Field development. Signposting, maintenance. NB reference effects of potential redevelopment of Brackenbury Sports Centre site.	Not assessed for IDF.					
711	Felixstowe	College Green / Maybush Lane (Walking)	Status? Created as part of College development. Signposting, maintenance. Ownership & rights complex. Reference correspondence about Planning Application DC/20/4188/FUL	Status? Created as part of College development. Signposting, maintenance. Ownership & rights complex. Reference correspondence about Planning Application DC/20/4188/FUL	Not assessed for IDF.					
714	Felixstowe	<Null>	Open connection beneath Leisure Centre walkway to promenade between Pier Bight Car Park existing route and the Events Area (Cycling)	Although not obvious, careful informal survey appears to indicate this is feasible. Would need negotiation with Leisure Centre operator. Previously identified by SCC 2015. Also a good principle to establish ahead of potential future development of Leisure Centre site.	Not assessed for IDF.					
715	Felixstowe	Exit Martello Park to Manor Terrace - See also map Cycle Route 51B & Insets	Track ends at boundary of Martello Park development. Cycle Route 51 continues onto Manor Terrace to Landguard via the Car Park. The large area of unmade ground is without known ownership.	This needs to be researched again (ESC did some work c. 1999 as part of South Sea Front project) and ESC should seek to claim it, as was done recently nearby on corner of Manor Road & Terrace. Could then serve as Cycling and Walking Route, and also possibly additional residents parking for Manor Terrace properties, frequently requested. But it is also a critical access route for both ESC and EA for plant access to 2 vehicular flood gates for flood defence maintenance. Protection is believed to be formalised for EA by flood defence regulations. Layout must recognise that. NB the land cannot be built on, for that reason.	Not assessed for IDF.					
719	Tunstall	Orford to Aldeburgh via Snape	I would like to see off-road cycle paths from Orford to Aldeburgh via Snape (sections of this exist already, for instance the Sailor's Path);	I would like to see off-road cycle paths from Orford to Aldeburgh via Snape (sections of this exist already, for instance the Sailor's Path);	N/A	£6,100,000-£6,200,000	SCC, DFT	SCC, DFT	N/A	Beneficial

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720	Wantisden	Between Orford and Woodbridge	I would like to see off-road cycle paths from Orford to Woodbridge (and Sutton Hoo).	This would link many local facilities and heritage attractions and also join up with local train stations for those wanting to come to the area with their bicycles by rail.	N/A	£9,300,000-£9,400,000	SCC, DFT	SCC, DFT	N/A	Beneficial
721	Sternfield	Between Snape and Saxmundham	I would like to see off-road cycle paths from Snape to Saxmundham.	This would link many local facilities and heritage attractions and also join up with local train stations for those wanting to come to the area with their bicycles by rail.	N/A	£2,700,000-£2,800,000	SCC, DFT, Developer	SCC, DFT, S106, S278, CIL	Access, sustainable transport, cycle and footway improvements for South Saxmundham Garden Neighbourhood (Policy SCLP12.29)	Critical
723	Felixstowe	The Grove and Abbey Grove	Access to The Grove and Abbey Grove needs to have kissing gates to prevent cycling. Mountain bikes would soon ruin the pathways for walking.	Access to The Grove and Abbey Grove needs to have kissing gates to prevent cycling. Mountain bikes would soon ruin the pathways for walking.	Not assessed for IDF.					
726	Felixstowe	Beatrice Ave/Colnies roundabout to Taunton Rd	A high standard cycle path on the verge from Beatrice Ave/Colnies roundabout to Taunton Rd and into Ataka and then Gulper would work very well.	A high standard cycle path on the verge from Beatrice Ave/Colnies roundabout to Taunton Rd and into Ataka and then Gulper would work very well.	Very High	£950,000-£1,000,000	SCC, Developer	S106, S278, CIL, SCC	Access, cycle and footway improvements for North Felixstowe Garden Neighbourhood and Sustainable transport, traffic management and cycle route improvements at Felixstowe	Critical
728	Felixstowe	Cycle ways in Felixstowe	Many of the so called cycle ways in Fx are too narrow given the road camber and gutter to make for safe and comfortable cycling. Too many allow cars to park in them. Cars expect cyclists to be in the lanes when they are unsuitable.	Maybe the pavement on one side of the road should be a cycle way. Again Kesgrave is very good in this respect. These lanes need to be kept clear of grit and debris that cars push into them. Better signage needed for cyclists and cars. Thought needs to be given at junctions.	Comment too generalised to assess in IDF.					
729	Felixstowe	Garrison Lane traffic lights	It is dangerous for a cyclist at Garrison Lane traffic lights if a vehicle behind at the lights turns left in front of the cyclists.	There needs to be a period during the light change that is for cyclists only. I realise this would make the lights even slower but if we want more cyclists on the road it is needed.	Highway matter not assessed by IDF.					
731	Felixstowe	Links Avenue and Upperfield Drive	Links Avenue and Upperfield Drive should become cycle ways.	Cars could be confined to Colneis Rd unless for access. This would aid pupils reaching Colneis and Kingsfleet Schools	Modal Filter comments from the community comment section have not been assessed in IDF.					
732	Felixstowe	Quiet lanes	Quiet lanes should have enforceable restrictions placed on them. Motorists do not seem to take any notice in Gulpher Rd.	It needs a mandatory scheme. Many more warning cyclists signs would help, the flashing speedo signs are good. Maybe the tarmac could be a different colour. Could the roads be access only for vehicles to stop the joy riders.	Comment too generalised to assess in IDF.					
733	Hemley	Newbourne, Hemley and Waldringfield	The lanes out towards and through Newbourne, Hemley and Waldringfield need to be 'quiet lanes'. Maybe they could be for access only by cars.	The lanes out towards and through Newbourne, Hemley and Waldringfield need to be 'quiet lanes'. Maybe they could be for access only by cars.	Quiet Lanes assessed by Quiet Lane project team.					

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734	Tunstall	Woodbridge, Campsea Ashe, Snape, Iken and Bawdsey	The area between Woodbridge, Campsea Ashe, Snape, Iken and Bawdsey could become a 'Cycling paradise area' for visitors and residents with the correct restrictions on the roads, ie 'quiet lanes'.	The area between Woodbridge, Campsea Ashe, Snape, Iken and Bawdsey could become a 'Cycling paradise area' for visitors and residents with the correct restrictions on the roads, ie 'quiet lanes'.	Quiet Lanes assessed by Quiet Lane project team.					
735	Levington	'Old' Felixstowe Rd between the Levington turn off / junction with the current Felixstowe Road	Cars travel at great speed along the 'old' Felixstowe Rd between the Levington turn off and the junction with the current Felixstowe Road.	There needs to be a dedicated cycle lane which continues through the layby area onto the dedicated cycle path on the 'current' Felixstowe Rd.	Very High	£1,800,000-£1,900,000	Developer, SCC, NH, CIL	S106, S278, CIL, SCC, DfT	Significant access improvements and improvements to the wider land at Felixstowe Road (Policy SCLP12.21)	Critical
736	Trimley St Martin	Cycle way along A14 from Goslings	The cycle way along A14 from Goslings onwards is poorly maintained.	The cycle way along A14 from Goslings onwards is poorly maintained.	Issues relating to maintenance have not been assessed for the IDF.					
737	Purdis Farm	Cycle way approaching Warren Heath Sainsburys roundabout	Also the cycle way approaching Warren Heath Sainsburys roundabout from Felixstowe is poorly maintained (often seriously overgrown) and this encourages cyclists to stay on the road which is not sensible with the road layout at the roundabout.	Also the cycle way approaching Warren Heath Sainsburys roundabout from Felixstowe is poorly maintained (often seriously overgrown) and this encourages cyclists to stay on the road which is not sensible with the road layout at the roundabout.	Issues relating to maintenance have not been assessed for the IDF.					
738	Wissett	West and north of Halesworth	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Formalise newly devised circular walks to the West and North East of the town, that use existing public rights of way through SCC map creation. (working with the SCC PROW team to commission new maps).	No Cycling and Walking infrastructure improvement suggested.					
739	Halesworth	Halesworth	I have been looking at the plans for the Cycling and Walking Strategy for Halesworth and I think these are all good ideas.	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).	Not assessed for IDF.					

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740	Halesworth	Town Centre to Millennium Green	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Support the improvement to the routes and connectivity from the Town Centre to the Millennium Green (see Objective 7) so encouraging more use.	No Cycling and Walking infrastructure improvement suggested.					
743	Halesworth	Halesworth	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Rationalise the walking maps available so they can form a suite of information online and in leaflet form and that reference each other. Some are signposted. Some need updating. Some have a specific historical focus. NB Subsequent agreement to work with Green Access team at SCC to produce a leaflet of circular walks for the Discover Suffolk website and to digitise the other leaflets so they can be accessed on the same website. Erect well designed and coherent signage once the maps and routes are finalised. (not a planning matter but a potential use of CIL money).	No Cycling and Walking infrastructure improvement suggested.					
752	Halesworth	Norwich Road	Make walking, cycling and scootering to Edgar Sewter primary school a safe and healthy option for children and parents. Based on consultation with years 5 and 6 children and with parents in the walking expert group the following are the suggestions as to how to achieve this policy. The planned extension to the school gives an opportunity to make changes. The proposed rerouting of the cycle track up the West side of Norwich Road could support these changes.	Make a 20mph zone along the Norwich Road in front of the main school entrance preferably from the Quay Street roundabout to The Avenue or beyond.	Speed reductions have not been assessed for the IDF.					
753	Halesworth	Thoroughfare	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Pedestrianisation of the Thoroughfare, (Objective 7 and 6).	Modal Filter comments from the community comment section have not been assessed in IDF.					
756	Halesworth	Halesworth	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Styles, gates and seats – better designs required to make walking in the countryside easier for the less mobile and more seats around town to encourage more walking to shops etc. (advice needed on what a NP can do on this)	Comment too generalised to assess in IDF.					
757	Martlesham	Convoy riding on highway	Although riding in large groups is no doubt a pleasant experience, riding in convey without occasionally pulling in to allow build up of traffic to pass does put riders at risk of car drivers taking chances to pass. I have on more than one occasion been stuck behind such a convey from Martlesham through to Woodbridge with little opportunity to pass. One has to be patient but as said, some car drivers may try and overtake inappropriately risking themselves and cyclist to injury.	Although riding in large groups is no doubt a pleasant experience, riding in convey without occasionally pulling in to allow build up of traffic to pass does put riders at risk of car drivers taking chances to pass. I have on more than one occasion been stuck behind such a convey from Martlesham through to Woodbridge with little opportunity to pass. One has to be patient but as said, some car drivers may try and overtake inappropriately risking themselves and cyclist to injury.	Comment too generalised to assess in IDF.					

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758	Felixstowe	Bent Hill, Felixstowe	Cyclist riding at speed down the middle of Bent Hill thus risking themselves, walkers and car drivers to injury. An accident waiting to happen (but should it wait?) Incidentally the same goes for skateboarders.	Cyclist riding at speed down the middle of Bent Hill thus risking themselves, walkers and car drivers to injury. An accident waiting to happen (but should it wait?) Incidentally the same goes for skateboarders.	Speed reductions have not been assessed for the IDF.					
759	Felixstowe	Hamilton Road shared space	Cycling one way, same as traffic, would help with safety of walkers especially the deaf and poor sighted. Cyclists/skateboarders play in this area.	Cycling one way, same as traffic, would help with safety of walkers especially the deaf and poor sighted. Cyclists/skateboarders play in this area.	Modal Filter comments from the community comment section have not been assessed in IDF.					
760	Trimley St Mary	Cycle lanes along highway	A white line separating cyclist from vehicles is not a safe option. Cars parked in cycle lanes requires cyclists to move around cars in the hope no one opens a car door as rider passes. I appreciate the solution is not an easy one but one has to be found if we are to encourage more cyclists to use network of roads. I personally have ceased cycling into Felixstowe from Trimley.	A white line separating cyclist from vehicles is not a safe option. Cars parked in cycle lanes requires cyclists to move around cars in the hope no one opens a car door as rider passes. I appreciate the solution is not an easy one but one has to be found if we are to encourage more cyclists to use network of roads. I personally have ceased cycling into Felixstowe from Trimley.	Comment too generalised to assess in IDF.					
762	Trimley St Martin	See attached documents	See attached documents	See attached documents	Not assessed for IDF.					
763	<Null>	Cycling - general comments	See attached document. Points 2 to 5 are plotted on the map in the relevant area which relates to the matter.	See attached document. Points 2 to 5 are plotted on the map in the relevant area which relates to the matter.	Assessed Separately					
766	Westerfield	Westerfield footpaths	The Parish Council have sought to apply for definitive status for a number of footpaths that were known to be used by residents but in all cases access to these routes for a circular walk includes use walking along dangerous local roads.	The Parish Council have sought to apply for definitive status for a number of footpaths that were known to be used by residents but in all cases access to these routes for a circular walk includes use walking along dangerous local roads.	Comment too generalised to assess in IDF.					
767	Westerfield	Westerfield	The only recognition of cycling in the village is that a section of the East/West route from Lower Road and Church Lane and then Moss Lane to Tuddenham is part of a Long-Distance Cycle Route.	The only recognition of cycling in the village is that a section of the East/West route from Lower Road and Church Lane and then Moss Lane to Tuddenham is part of a Long-Distance Cycle Route.	No Cycling and Walking infrastructure improvement suggested.					
770	Westerfield	Lower Road, Westerfield	This road is unsuitable for cyclists and pedestrians due to the amount and the speed of traffic. This narrow road does not have footways or walkable verges and where the minimum width is 5 metres a drainage ditch is immediately adjacent only protected by reflective marker posts.	Although Speed indicators are present it is obvious that physical measures are needed to improve reduce traffic speeds and enable cyclists and pedestrians to use this road in safety. Consideration should be given to traffic management measures such as restricting vehicles to single lane working alongside pedestrian/cycling facilities and/or any other provision to decrease the number and speed of vehicles.	Highway matter not assessed by IDF.					

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771	Westerfield	Church Lane, Westerfield	This road is unsuitable for cyclists and pedestrians due to the amount and the speed of traffic. This narrow road does not have footways or walkable verges and limited visibility is an additional hazard.	Although Speed indicators are present it is obvious that physical measures are needed to improve reduce traffic speeds and enable cyclists and pedestrians to use this road in safety. Consideration should be given to traffic management measures such as restricting vehicles to single lane working alongside pedestrian/cycling facilities and/or any other provision to decrease the number and speed of vehicles.	Highway matter not assessed by IDF.					
772	Westerfield	Moss Lane	This road is single vehicle width and used by a large range of vehicles as a short cut. It is unsuitable as a rat run and should be closed to through traffic thus protecting cyclist and pedestrians. The SCC ROW Improvement Plan referred to possible classification as a Green Lane (Similar Comment to that already registered No478)	This road is single vehicle width and used by a large range of vehicles as a short cut. It is unsuitable as a rat run and should be closed to through traffic thus protecting cyclist and pedestrians. The SCC ROW Improvement Plan referred to possible classification as a Green Lane (Similar Comment to that already registered No478)	Modal Filter comments from the community comment section have not been assessed in IDF.					
774	Westerfield	Westerfield Railway Station and Greater Anglia	In order to make better use of rail services and reduce dependence of local residents on car travel there's a need for East Suffolk Line services to stop at Westerfield. In the past it has been possible to use this service to or from Woodbridge as part of a cycle ride or a ramble, in fact it's listed as an East Suffolk Line walk. Stopping trains on the East Suffolk line would therefore help to encourage walking and cycling while also eliminating car journeys and contributing to "Green" policies.	In order to make better use of rail services and reduce dependence of local residents on car travel there's a need for East Suffolk Line services to stop at Westerfield. In the past it has been possible to use this service to or from Woodbridge as part of a cycle ride or a ramble, in fact it's listed as an East Suffolk Line walk. Stopping trains on the East Suffolk line would therefore help to encourage walking and cycling while also eliminating car journeys and contributing to "Green" policies.	No Cycling and Walking infrastructure improvement suggested.					
776	<Null>	East Suffolk	More and more cyclists are riding on footpaths and some are very arrogant and dangerous with it. Can we PLEASE have signs saying that these are FOOT PATHS and therefore cycling is forbidden.	More and more cyclists are riding on footpaths and some are very arrogant and dangerous with it. Can we PLEASE have signs saying that these are FOOT PATHS and therefore cycling is forbidden.	Comment too generalised to assess in IDF.					
779	Lowestoft	Lowestoft	The third crossing will mean even less excuse for not having more 20 mph speed limits. There is plentiful evidence they create more cycling. I particularly argue Yarmouth Road would be a good candidate. Come the third crossing, I guess its classification could change. There is arguably a precedent in that in south Lowestoft stretches of Marine Parade/Wellington Esplanade/Kirkley Cliff Road, which are the A12 are 20 mph.	I realise that the A47 is the responsibility of Highways England. Frankly, the cycling provision is a shambles. For a lot of the way it is shared with pedestrians on PARTICULARLY narrow footways, passing bus stops, driveways and crossing roads without priority, i.e. it goes completely against CUK's guidance. There are points where the shared path stops so cyclists have to continuously temporarily rejoin the carriageway. That can increase danger as drivers do not expect it. Ironically, the one reasonable stretch of the cycle path, which is segregated from the footway and runs between Sussex Rd and Hollingsworth Rd, passing Ormiston Academy, gets parking on it at school run times. In my opinion, as the Northern Spine Road is part of a route to bypass Lowestoft centre to reduce congestion, there is no reason why Yarmouth Rd should not already be 20mph to the roundabout with the Northern Spine Road/Corton Long Lane/Blundeston Road. It could encourage compliance with using the bypass route.	Speed reductions have not been assessed for the IDF.					

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780 <Null>		East Suffolk	CUK's position is that priority should be to consider whether the road environment can be made comfortable for cycling and that sharing with pedestrians should be the last resort. The latest guidance from the Department for Transport is in agreement stating improved facilities for pedestrians and cyclists should be separated and road-narrowing to enable correct width cycle lanes should be considered which is in effect saying making roads comfortable for cycling should be the first consideration.	Considering the nature of many of Lowestoft's busier roads, I understand why on-road facilities would be difficult. I hope there will be proper consultation (CUK would probably accept off-road facilities are more appropriate anyway). Many cyclists will say they want more cycle paths and they don't mind sharing with pedestrians as anything is better than being on road. It is impossible for there to be off-road facilities everywhere. The more cyclists on the roads the safer on-road cycling is, especially if there are 20mph limits. Routes need to be as direct as possible, perhaps even giving cycling time-saving, advantages over driving. Many off-road routes involve time-consuming waits at toucan crossings etc. There are pedestrians who dislike sharing with cyclists, so even considerate riders on shared facilities experience hostility. Having to slow for pedestrians, and possibly dismount and walk, works against cycling being quicker than driving for short journeys.	Comment too generalised to assess in IDF.					
782	Oulton Broad	Oulton Broad	There are good and bad things about the short stretch of cycle path running from the traffic lights just south of the Bridge Road/Saltwater Way/Victoria Road roundabout, past the fish and chip shop and former Spar store into Oulton Broad centre. The good point is that it gives cyclists a geographical advantage to/from the centre and links, via the toucan crossing, with the shared facility to/from the railway bridge.	Ironically, ideally it should be shorter, avoiding passing the fish and chip shop and former Spar. I cannot exaggerate how many more pedestrians walk on the cycle path instead of the footway, despite, in this case, being reasonably wide. Also, cars regularly park on it and when the Spar was open, it included lorries. The nature of the road means there would be no harm in cyclists having to ride it a little further, especially as a 20 mph speed limit would be easily enforceable.	Speed reductions have not been assessed for the IDF.					
783	Lowestoft	Lowestoft	Concerning cycle lanes, i.e. white lines on roads, many of them in Lowestoft are not the stipulated minimum width of 1.5 metres. Local Transport Note (LTN) 2/08, paragraph 7.4.2 states: "Cycle lanes should be 2 metres wide on busy roads, or where traffic is travelling over 40 mph. A minimum width of 1.5 metres may be generally acceptable on roads with a 30 mph limit.	For cycle feeder lanes to advanced stop line arrangements, a minimum width of 1.2m may be acceptable. Cycle lanes less than 1.2 metres wide cannot easily accommodate tricycles or child carrying cycle trailers wholly within the lane." A pertinent point is that the Highway Code advises cyclists to ride 0.5 metres away from the kerb. Cycle lanes less than 1.5 metres can, ironically, increase cycling danger by misleading drivers into thinking those are safe distances to overtake cyclists. LTN 2/08 was withdrawn on 20 July because it has been superseded by LTN 1/20. However, paragraph 6.4.2 indicates 1.5 metres is now only acceptable for one-way roads.	Comment too generalised to assess in IDF.					
787	Lowestoft	Ormiston Academy	I hope there will be discussion to resolve the issue of parents parking on the cycle path outside Ormiston Academy.	I hope there will be discussion to resolve the issue of parents parking on the cycle path outside Ormiston Academy.	No Cycling and Walking infrastructure improvement suggested.					
788 <Null>		East Suffolk	20 mph speed limits just outside schools do not encourage more cycling of the school run.	Where there are not off-road facilities on popular school routes, often along residential roads, there need to be 20 mph limits. They have been proved to work.	Speed reductions have not been assessed for the IDF.					
789 <Null>		See attached.	See attached.	See attached.	Comment too generalised to assess in IDF.					

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790 <Null>		See attached.	See attached.	See attached.	Comment too generalised to assess in IDF.					
791 <Null>	East Suffolk		See below.	<p>I would like to ask that when compiling your cycling and walking strategy, you also take into account the wishes and needs of horse riders, for the following reasons:</p> <ul style="list-style-type: none"> • Horse riding is also a healthy form of outdoor exercise • Horse riders share rights of way (bridlepaths and byways) with cyclists, and their needs may be different. For example putting down a hard surface to make a right of way better for cyclists would be detrimental if not dangerous for horse riders • Horse riding contributes significantly to the local economy, such as riding schools, livery yards, farriers, vets, feed merchants, tack shops etc • Horses have to be kept all year round, we don't just put them in a shed for the winter and get them out again when the weather improves! <p>Local horse riding organisations, and the British Horse Society, should be consulted for their views on any proposed changes to bridlepaths and byways.</p>	Comment too generalised to assess in IDF.					
792 <Null>	East Suffolk		<p>Good Issues</p> <ul style="list-style-type: none"> - Cheap - Good lights & clothing - Short journeys don't take longer than car - Healthy - Fun in good weather - Reduced oil consumption - Panniers & back packs assist shopping <p>Bad Issues</p> <ul style="list-style-type: none"> - Punctures - Hills and inclines (e-bikes help!) - Bad weather - Aggressive driving - Most local roads have 60mph limit - Hard verges and kerbs reduce vehicle options when being over-taken or vehicle approaching from opposite direction (cars rarely wait for cyclists, agricultural vehicles NEVER do) 	<p>I suggest that we start by looking at short journeys of 5 miles or fewer. This could include travelling to work or school and daily and intra weekly shopping trips for most people in East Suffolk. To provide encouragement, the following notes may help:</p> <ul style="list-style-type: none"> - Direct cyclists to cycle-friendly routes? - Can we provide shopping discounts for people who arrive by cycle or walk? - Encourage more frequent shopping trips for lighter, smaller loads - Shop close to home – you're saving on fuel to compensate for any higher prices - Have we got enough cycle racks? - Are they fit for purpose and in a suitable place? <p>Cycle routes don't require lots of infrastructure, but the following help greatly:</p> <ul style="list-style-type: none"> - Appropriate signage at each end of the route - Preferably NOT along 60 mph roads - Quiet lanes are perfect - Soft level verges (assuming single carriageway roads) - Well-maintained road surfaces - No hedge cutting using flails!!! 	Comment too generalised to assess in IDF.					

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793	Lowestoft	Lowestoft	The Town Council is aware that more people in Lowestoft than the national average use the bicycle as a form of transport. Connectivity of routes through and around town should be reviewed and the East Suffolk Council should scrutinise and strongly lobby the County Council on lack of funding being allotted to Lowestoft as opposed to other Suffolk towns.	It is hoped the public will submit their individual comments to East Suffolk Council in response to this consultation, however, again, it is noted that a digital consultation is not inclusive to the whole community.	No Cycling and Walking infrastructure improvement suggested.					
794 <Null>		East Suffolk	As a result of the number of consultations we are currently receiving, we regret that we are unable to comment specifically at this time.	As a result of the number of consultations we are currently receiving, we regret that we are unable to comment specifically at this time.	No Cycling and Walking infrastructure improvement suggested.					
795	Butley	Butley	What is not shown are the number of footpaths in existence. Surely if you want to get people to get out walking and use the footpaths you need to identify them! In the EADT last week it commented that 1904 miles of footpaths had been lost in SUFFOLK alone. They could not have just disappeared! There has been an erosion of the rights of walkers by farmers ploughing up the ways. Establish where these paths are and get them re-established.	As chairman of Butley PC I have raised the issue of farmers ploughing up paths and never even received any answer from Suffolk CC. So lets have some joined up thinking and action. Otherwise this is all a waste of time and money.	Comment too generalised to assess in IDF.					
796 <Null>		East Suffolk	Natural England has no comments to make at this time. However, we will be happy to comment on future forward planning consultations which come forward.	Natural England has no comments to make at this time. However, we will be happy to comment on future forward planning consultations which come forward.	No Cycling and Walking infrastructure improvement suggested.					
798 <Null>		East Suffolk	The County Council supports the underlying sustainable aims and objectives of the emerging Strategy and would suggest that engagement is made with neighbouring authorities in Norfolk (i.e. Great Yarmouth Borough Council and Norfolk County Council) to ensure that the maximum benefits can be made through cross-boundary working in respect of cycling and walking routes to Norfolk Settlements.	The County Council supports the underlying sustainable aims and objectives of the emerging Strategy and would suggest that engagement is made with neighbouring authorities in Norfolk (i.e. Great Yarmouth Borough Council and Norfolk County Council) to ensure that the maximum benefits can be made through cross-boundary working in respect of cycling and walking routes to Norfolk Settlements.	No Cycling and Walking infrastructure improvement suggested.					

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799 <Null>		East Suffolk	<p>1. despite recent resurfacing work there are many pot holes on back lanes</p> <p>2. again on many lanes there is a build up of sand, gravel or tree debris</p> <p>3. in autumn when farmers cut hedges the tractor powered methods strew the road with sharp fragments which create a very significant risk of punctures not only to bikes but also cars</p> <p>4. signposts, so valuable to anyone not relying on sat-nav, are increasingly corroded through and lying in the verge</p>	<p>1. Cyclists are aware of road condition and promoting the existing SCC online reporting tool amongst them would reduce the need for staff to carry out road surveys.</p> <p>2. Reduce verge cutting, which is prejudicial to wildlife, spend it on sweeping roads free of sand and flints which are a particular problem with the local geology.</p> <p>3. Anyone strewing a road with tacks would soon be subject to enforcement action so it seems strange that there are no moves to deal with the hacking of hedges with no regard to the state the road is left in.</p> <p>4. Signposts are in a poor state. If there is insufficient money to replace, an imaginative solution needs to be found. Perhaps a plastic insert to reconnect the tubular uprights on an interim basis?</p> <p>5. A new, imaginative look at our roads needs to be promoted rather than just doing, or not doing, what always has been. While much of the direct responsibility for remedial work lies with higher tiers, is E.S.C. supportive of the objectives?</p>	Comment too generalised to assess in IDF.					
800 <Null>		Walberswick	<p>The Council strongly supports developing a cycling and walking strategy. We support putting in additional cycling and walking routes and increasing the level of maintenance that ESC and SCC spend on maintaining routes. Walberswick Parish Council has already objected to Sizewell C including that its construction period will make it impossible to cycle on the roads in and around the area as huge increases in traffic, HGVs and rat running will make roads busy and dangerous for cyclists and walkers.</p>	<p>Should Sizewell C go ahead, ESC should address this particular issue in the Cycling and Walking Strategy along with the ongoing work in the rest of the District.</p>	Comment too generalised to assess in IDF.					
805 Halesworth		new development at Chediston Street/Roman Way	<p>Link residential areas to the main town destinations and the NCR1</p>	<p>The proposed new development at Chediston Street/Roman Way includes suggestion for a cycle route up Chediston Street into the town centre. This is considered dangerous and an alternative route should be planned. From the estate a route should be created into Allington Road. This makes best use of the contours of the land and connects into Dukes Drive near to the bus stop. It would then cross Roman Way to connect to the existing cycle route in Holmere Drive and into Church Farm Lane. This creates a relatively safe cycling route into the Market Place and town centre via the quiet northern end of London Road around the St Mary's Church yard.</p>	No Cycling and Walking infrastructure improvement suggested.					

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811	Halesworth	Roundabout at Quay Street up the Norwich Road to Sparrowhawk Road	<p>Create a direct and safe 'key movement' cycle route from the Sparrowhawk Road roundabout in the north to the Bramfield Road/London Road intersection in the South.</p> <p>This would reduce the 'inconsistent and confused approach for cyclists and pedestrians' and thereby reduce conflict for all users' as they navigate the Town Centre (Waveney Local Plan). Rerouting of NCR1 would be needed.</p>	<p>Cycle route from the roundabout at Quay Street up the Norwich Road should be on the west side of the road. The partial and inadequate cycle route that goes up to Harrisons Lane on the east should be decommissioned as dangerous.</p> <p>The west side of the road would solve some of the issues for children cycling to school. At present they cannot cross safely from the present cycle route to the school.</p> <p>Poor parking on the west side of Norwich Road (from Edgar Sewter Primary School to 'The Avenue'), caused by overspill from the Police Station, businesses in town, and by parents dropping children off at school, would need to be resolved.</p> <p>This route would become a re-routed NCR1 doing away with the confusing route down Harrisons Lane into Holton and then up to Sparrowhawk Road. At Sparrowhawk Roundabout the NCR1 route could go up the road in front of the Triple Plea pub and join the present NCR1 route at Butts Road in a more direct and straightforward route towards the railway Mill Post Crossing.</p>	N/A	£1,500,000-£1,600,000	SCC, Developer, DFT	SCC, S106, S278, CIL, DFT	N/A	Essential
812	Halesworth	Saxons Way	<p>Create a direct and safe 'key movement' cycle route from the Sparrowhawk Road roundabout in the north to the Bramfield Road/London Road intersection in the South.</p>	<p>The pavements along Saxons Way, from Quay Street roundabout to the Coop/London Road roundabout should become safe, shared cycle and pedestrian paths. (the east side of Saxons Way may be the best option as it links with the proposed east side route on London Road and would not impinge on the entrance to the new development on the west side or the entrance to the car park).</p> <p>The Saxons Way route would remove the confusing one way cycling in the Thoroughfare and the dismount instruction at the southern end of the Thoroughfare.</p> <p>The route should then continue along the eastern side of London Road to the turning with Bramfield Road (the main route into Halesworth from the A12)</p>	N/A	£1,500,000-£1,600,000	SCC, Developer, DFT	SCC, S106, S278, CIL, DFT	N/A	Essential
813	Halesworth	Thoroughfare / Bridge Street	<p>Reroute the NCR1 away from the Thoroughfare / Bridge Street.</p> <p>The rerouting of NCR1 away from the Thoroughfare/Bridge Street between the Quay Street and the entrance to the car park removes a confusing and badly signposted national route from a semi pedestrianised shopping street and allows for the Thoroughfare to become safer and more pedestrianised route.</p>	<p>Reroute the NCR1 away from the Thoroughfare / Bridge Street.</p> <p>The rerouting of NCR1 away from the Thoroughfare/Bridge Street between the Quay Street and the entrance to the car park removes a confusing and badly signposted national route from a semi pedestrianised shopping street and allows for the Thoroughfare to become safer and more pedestrianised route.</p>	No Cycling and Walking infrastructure improvement suggested.					
112b	Benacre	Kessingland to Southwold	<p>To make this journey by bike you have to go inland through Henstead to avoid the A12. A long way out of your way.</p>	<p>This is an alternative suggestion made by an officer of East Suffolk Council in exploring whether there is potential along a more coastal path.</p>	N/A	£2,100,000-£2,200,000	SCC, DFT	SCC, DFT	N/A	Beneficial

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120a	Trimley St Martin	A14 cycle path Felixstowe to Levington	The cycle path is in a terrible state of disrepair, overgrown and strewn with debris. It is not maintained and the surface is dangerously uneven. It's also frighteningly close to A14 traffic. Because of these issues it's considered by most cyclists to be unusable, and certainly not safe for families with children.	The cycle path was installed prior to the single track link road which now runs beside it. It would be great if the cycle path could be relocated to nearer the quieter link road and away from the A14.	Very High	£1,900,000-£2,000,000	SCC, DFT	SCC, DFT	N/A	Beneficial
179a	Woodbridge	Riverside path from Broomfield to Woodbridge	This is a single track path suitable only for walkers, and I believe cyclists are not permitted. However over the past year more and more cyclists are using it and it is plainly not suitable for mixed use.	Erect barriers to prevent cyclists	Not assessed for IDF.					
205b	Woodbridge	Hasketon Road/Ransome Road, Woodbridge	Recognising that Farlingaye does not have very good access there is often a conflict between cars & cars and cars & bicycles in this part of Woodbridge, particularly during the morning rush hour / School drop off hour. School hours generally coincide with the morning rush hour creating increased numbers of cars and cycles (young cyclist) in this area of woodbridge including the B1079.	2) Look at the 'on street parking' around this area, maybe some(or less) more yellow lines.	No Cycling and Walking infrastructure improvement suggested.					
205c	Woodbridge	Hasketon Road/Ransome Road, Woodbridge	Recognising that Farlingaye does not have very good access there is often a conflict between cars & cars and cars & bicycles in this part of Woodbridge, particularly during the morning rush hour / School drop off hour. School hours generally coincide with the morning rush hour creating increased numbers of cars and cycles (young cyclist) in this area of woodbridge including the B1079.	3) Consider making Hasketon Road and the B1079 roads oneway utilising the A12 roundabouts and a roundabout at the Hasketon/B1079 junction.	Modal Filter comments from the community comment section have not been assessed in IDF.					
279a	Rushmere St Andrew	Land allocated for Housing 'Humber Doucy Lane & Rushmere'	Land allocated for housing will increase the number of vehicles on the local roads particularly 'Tuddenham Road' & 'Humber Doucy Lane', this already a cut through road, but also popular with cyclists travelling out of Ipswich towards Tuddenham and the villages beyond. There is limited pavement and no cycle lane/protection along its route.	Humber Doucy lane could be widened to incorporate a dedicated footpath / cycle track connecting the development with Ipswich cycle infrastructure.	Largely outside East Suffolk so has not been assessed for IDF.					
279b	Rushmere St Andrew	Land allocated for Housing 'Humber Doucy Lane & Rushmere'	Land allocated for housing will increase the number of vehicles on the local roads particularly 'Tuddenham Road' & 'Humber Doucy Lane', this already a cut through road, but also popular with cyclists travelling out of Ipswich towards Tuddenham and the villages beyond. There is limited pavement and no cycle lane/protection along its route.	There is an opportunity to upgrade the bridleway at the end of Tuddenham lane to provide a safe cycling and walking route to Tuddenham avoiding 'Tuddenham Main Road' which is a commuter route into Ipswich for cars.	Partially outside East Suffolk so has not been assessed for IDF.					

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328a	Little Bealings	Playford Road - east of junction with The Street and Hall Road.	Playford Road and Martlesham Road has become much busier with through traffic between Ipswich and Woodbridge making it less unpleasant and much less safe to cycle on. The road was very popular during the lockdown when there was little or no traffic, as those new to cycling and those wanting to encourage their children to cycle found out.	Close the road to the east of the junction along with closure further to the west so that cyclists have a safe and attractive route between Ipswich and Woodbridge, whilst allowing car drivers to reach Bealings from the A1214 if necessary.	Modal Filter comments from the community comment section have not been assessed in IDF.					
353a	Melton	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Melton Road	No Cycling and Walking infrastructure improvement suggested.					
353b	Melton	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Chapel Street	No Cycling and Walking infrastructure improvement suggested.					
353c	Melton	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Castle Street	No Cycling and Walking infrastructure improvement suggested.					
353d	Melton	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Bredfield Road	No Cycling and Walking infrastructure improvement suggested.					
353e	Melton	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge	Car parking provision further out of Woodbridge. Enforcement parking on Seckford Street and Theatre Street	No Cycling and Walking infrastructure improvement suggested.					
410b	Kesgrave	Kesgrave School	Doesn't appear to be a safe route for children and other cyclists to get to Dr. Watsons Lane (to Playford) and Hall Road (to Bealings) from the Northern (School) side of the road or indeed the existing cycle path on the South side. Hence limiting the opportunity for children and parents from the villages to cycle to the school in safety.	1). Provide a proper crossing and short section of cycle/footpath on the northside of the road where the central refuge is on the A1214 at Hall Road. 2). Extend the existing cycle path beyond the Bell Lane traffic lights past the Doctor Watsons lane junction and provide a separate crossing integrated with the existing traffic lights. 3) This would also help all cyclists wishing to travel from the Kesgrave development north into the villages and beyond.	High	£1,100,000-£1,200,000	SCC, DFT	SCC, DFT	N/A	Beneficial
531C	Martlesham	Martlesham retail and business park, Old Felixstowe Road, Main Road Martlesham, Sandy Lane into Woodbridge	Volume of motorised traffic make this route unsafe for cyclists and pedestrians	1 Traffic management scheme within the retail/industrial area channeling traffic onto A12 2 Restoration of Old Felixstowe Road to a safe cycle priority route by limiting motorised through traffic to buses and emergency vehicles 3 Traffic calming chicanes in The Street, Martlesham 4 No through motorised traffic on Sandy Lane achieved by a physical barrier at the railway bridge	Highway matter not assessed by IDF.					
673a	Southwold	Southwold Town Council	<Null>	<Null>	No Cycling and Walking infrastructure improvement suggested.					

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673b	Southwold	Southwold Town Council	<Null>	<Null>	No Cycling and Walking infrastructure improvement suggested.					
673c	Southwold	Southwold Town Council	<Null>	<Null>	No Cycling and Walking infrastructure improvement suggested.					
684 (category 1 - Point 1)	Martlesham	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.	Not assessed for IDF.					
684 (category 1 - Point 2)	Martlesham	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.	Not assessed for IDF.					
684 (Category 3)	Martlesham	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.	Not assessed for IDF.					
739a	Halesworth	Halesworth	Comment 306	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).	No Cycling and Walking infrastructure improvement suggested. (comments already assessed)					

Ref	Parish	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
739b	Halesworth	Halesworth	Comment 303	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).	No Cycling and Walking infrastructure improvement suggested. (comments already assessed)					
739c	Halesworth	Halesworth	Comment 302	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).	No Cycling and Walking infrastructure improvement suggested. (comments already assessed)					
739d	Halesworth	Halesworth	Comment 480	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).	No Cycling and Walking infrastructure improvement suggested. (comments already assessed)					

Ref	Parish	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
739e	Halesworth	Halesworth	Comment 304	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).	No Cycling and Walking infrastructure improvement suggested. (comments already assessed)					
761B	Stratton Hall	See attached documents - Stratton Hall	See attached documents	See attached documents	Comment too generalised to assess in IDF.					
X1	Felixstowe	ELMCROFT LANE /WESTMORLAND ROAD TO CLIFF ROAD	FOOTPATH 8 REPLACE STAGGERED BARRIERS WITH BOLLARD AND SIGN ROUTE.	FOOTPATH 8 REPLACE STAGGERED BARRIERS WITH BOLLARD AND SIGN ROUTE.	Not assessed for IDF.					
X12	Felixstowe	LANGER ROAD	SAFE ROUTE TO SCHOOL CONVERT WESTERN FOOTWAY BETWEEN WALTON AVE TO HOLLAND ROAD	SAFE ROUTE TO SCHOOL CONVERT WESTERN FOOTWAY BETWEEN WALTON AVE TO HOLLAND ROAD	Very High	£800,000-£850,000	SCC	SCC	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
X16	Felixstowe	GARRISON LANE	ADD ADVISORY CYCLE LANES BETWEEN ITS JUNCTION OF UNDERCLIFFE ROAD WEST AND HIGH ROAD WEST.	ADD ADVISORY CYCLE LANES BETWEEN ITS JUNCTION OF UNDERCLIFFE ROAD WEST AND HIGH ROAD WEST.	Not assessed for IDF.					
X18	Felixstowe	PRINCES ROAD/ SOUTH HILL	SIGN AS CYCLE ROUTE TO PIER	SIGN AS CYCLE ROUTE TO PIER	Not assessed for IDF.					
X19	Felixstowe	CRESCENT ROAD	BETWEEN GARRISON LANE & COBBOLD ROAD EXISTING SIGNED AS NCR51 ADD ADVISORY CYCLE LANES	BETWEEN GARRISON LANE & COBBOLD ROAD EXISTING SIGNED AS NCR51 ADD ADVISORY CYCLE LANES	Not assessed for IDF.					
X2	Felixstowe	LOCAL ROUTE 1 COLNEIS ROAD	ADD ADVISORY CYCLE LANES BETWEEN JUNCTION OF CHURCH ROAD AND BEATRICE AVE	ADD ADVISORY CYCLE LANES BETWEEN JUNCTION OF CHURCH ROAD AND BEATRICE AVE	Not assessed for IDF.					
X20	Felixstowe	PRIORY ROAD	BETWEEN HIGH ROAD WEST & GOLF ROAD SIGN AS CYCLE ROUTE	BETWEEN HIGH ROAD WEST & GOLF ROAD SIGN AS CYCLE ROUTE	Not assessed for IDF.					
X21	Felixstowe	CARR ROAD	BETWEEN BEACH STATION ROAD & DOCK GATES SIGN AS LOCAL CYCLE ROUTE	BETWEEN BEACH STATION ROAD & DOCK GATES SIGN AS LOCAL CYCLE ROUTE	Not assessed for IDF.					
X22	Felixstowe	ORFORD ROAD	BETWEEN CARR ROAD & SEA ROAD REMOVE NCN SIGN REPLACE WITH LOCAL ROUTE SIGNING	BETWEEN CARR ROAD & SEA ROAD REMOVE NCN SIGN REPLACE WITH LOCAL ROUTE SIGNING	Not assessed for IDF.					
X23	Felixstowe	MANOR ROAD & MANOR TERRACE	REMOVE NCN SIGNAGE BETWEEN CARR ROAD WORK ITEM 13	REMOVE NCN SIGNAGE BETWEEN CARR ROAD WORK ITEM 14	Not assessed for IDF.					
X28	Felixstowe	HIGH ROAD WEST	EXTEND ADVISORY CYCLE LANE THROUGH TRAFFIC ISLAND TOWARDS RNDBT TO START OF OFF ROAD CYCLE TRACK.	EXTEND ADVISORY CYCLE LANE THROUGH TRAFFIC ISLAND TOWARDS RNDBT TO START OF OFF ROAD CYCLE TRACK.	Not assessed for IDF.					
X29	Felixstowe	WALTON AVE EXTENSION WEST	ET06180	ET06181	Not assessed for IDF.					

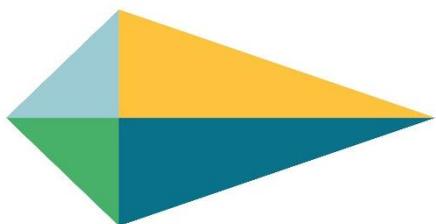
Ref	Parish	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
X3	Felixstowe	CHURCH ROAD	SIGN ROUTE	SIGN ROUTE	Not assessed for IDF.					
X33	Felixstowe	FERRY LANE	FROM END OF OFF ROAD CYCLE FACILITIES ADD ADVISORY CYCLE LANES TO HODGKINSON ROAD/DOOLEY INN PH	FROM END OF OFF ROAD CYCLE FACILITIES ADD ADVISORY CYCLE LANES TO HODGKINSON ROAD/DOOLEY INN PH	Not assessed for IDF.					
X34	Felixstowe	GRANGE FARM AVENUE	BETWEEN LANGLEY AVE & SUDBOURNE RD ADD CYCLE LOGOS AND ADVISORY CYCLE LANES THROUGH ISLAND PINCH POINTS	BETWEEN LANGLEY AVE & SUDBOURNE RD ADD CYCLE LOGOS AND ADVISORY CYCLE LANES THROUGH ISLAND PINCH POINTS	Not assessed for IDF.					
X35	Felixstowe	GRANGE FARM AVENUE (GFA)	AT CROSS ROADS FORMED BY BRACKLEY & POND CLOSE. TERMINATE CYCLE PATH AT POND CL ADD SPUR TO CROSS GFA WHERE BUILD OUT NARROWS ROAD. CONSTRUCT CYCLE BYPASS TOWARDS BRACKLEY CLOSE AND ADD CYCLE LANE ACROSS ITS MOUTH.	AT CROSS ROADS FORMED BY BRACKLEY & POND CLOSE. TERMINATE CYCLE PATH AT POND CL ADD SPUR TO CROSS GFA WHERE BUILD OUT NARROWS ROAD. CONSTRUCT CYCLE BYPASS TOWARDS BRACKLEY CLOSE AND ADD CYCLE LANE ACROSS ITS MOUTH.	Not assessed for IDF.					
X36	Felixstowe	GRANGE FARM AVENUE	EXISTING CYCLE FACILITY ADD GIVE WAYS & SIGNS - DO WHAT TO THEM?	EXISTING CYCLE FACILITY ADD GIVE WAYS & SIGNS - DO WHAT TO THEM?	Not assessed for IDF.					
X39	Felixstowe	WESTMORLAND ROAD	SIGN AS LOCAL CYCLE ROUTE	SIGN AS LOCAL CYCLE ROUTE	Not assessed for IDF.					
X4	Felixstowe	ROSEMARY AVENUE	REVISED ROUTING OF LOCAL ROUTE 1, SIGN & ADD ADVISORY CYCLE LANES	REVISED ROUTING OF LOCAL ROUTE 1, SIGN & ADD ADVISORY CYCLE LANES	Not assessed for IDF.					
X41	Felixstowe	NATIONAL CYCLE ROUTE 41	SUFFOLK COASTAL CYCLE ROUTE	SUFFOLK COASTAL CYCLE ROUTE	No Cycling and Walking infrastructure improvement suggested.					
X42	Felixstowe	NORTH SEA CYCLE ROUTE	FORMERLY NCN 1 NOW NCN41 &51	FORMERLY NCN 1 NOW NCN41 &52	No Cycling and Walking infrastructure improvement suggested.					
X43	Felixstowe	MILL LANE	ADD ADVISORY CYCLE LANES BETWEEN GARRISON LANE AND GRANGE ROAD. AT BRIDGE REDUCE VISUAL RUNNING LANE BY WHITE LINE & HATCHING.	ADD ADVISORY CYCLE LANES BETWEEN GARRISON LANE AND GRANGE ROAD. AT BRIDGE REDUCE VISUAL RUNNING LANE BY WHITE LINE & HATCHING.	No Cycling and Walking infrastructure improvement suggested.					
X44	Felixstowe	TRIMLEY ROAD KIRTON	ADD CYCLE LOGOS (1057) 100M NORTH OF SCHOOL TO ROSELEA NURSERY	ADD CYCLE LOGOS (1057) 100M NORTH OF SCHOOL TO ROSELEA NURSERY	To Cycling and Walking infrastructure improvement suggested.					
X48	Felixstowe	HIGH ROAD TRIMLEY ST MARTIN	ADD CYCLE LOGOS 1057 FROM EGRESS OF CYCLE PATH TO MILL LANE	ADD CYCLE LOGOS 1057 FROM EGRESS OF CYCLE PATH TO MILL LANE	Not assessed for IDF.					
X49	Felixstowe	HIGH ROAD TRIMLEYS & HIGH ST WALTON	REPLACE THE MISSING SECTION OF ADVISORY CYCLES & ADD NEW TO PROVIDE CONTINUOUS LANES BETWEEN GARRISON LANE AND HOWLETT WAY.	REPLACE THE MISSING SECTION OF ADVISORY CYCLES & ADD NEW TO PROVIDE CONTINUOUS LANES BETWEEN GARRISON LANE AND HOWLETT WAY.	Not assessed for IDF.					
X5	Felixstowe	TAUNTON & EXETER ROADS	SIGN ROUTE	SIGN ROUTE	Not assessed for IDF.					
X51	Felixstowe	NATIONAL CYCLE ROUTE 51	HARWICH TO CAMBRIDGE	HARWICH TO CAMBRIDGE	No Cycling and Walking infrastructure improvement suggested.					

Ref	Parish	Where is the matter/improvement located?	What is the matter/improvement?	Please suggest a possible solution / improvement	C&WS priority	Approximate cost	Potential Funding Sources	Potential Funding Mechanisms	Local Plan IDF Project	IDF Priority
X52	Felixstowe	MAIDSTONE ROAD & GRANGE ROAD	ADD CYCLE LOGO 1057 BETWEEN RAISED TABLE BETWEEN HIGH ST WALTON AND WESSEL AVE /PEWITT HILL	ADD CYCLE LOGO 1057 BETWEEN RAISED TABLE BETWEEN HIGH ST WALTON AND WESSEL AVE /PEWITT HILL	Not assessed for IDF.					
X54	Felixstowe	SEA ROAD	ADD CYCLE LOGOS BETWEEN UNDERCLIFF ROAD & ORFORD ROAD	ADD CYCLE LOGOS BETWEEN UNDERCLIFF ROAD & ORFORD ROAD	Not assessed for IDF.					
X55	Felixstowe	HAMILTON ROAD	CONTRA FLOW CYCLING BETWEEN COBBOLD ROAD & ORWELL ROAD	CONTRA FLOW CYCLING BETWEEN COBBOLD ROAD & ORWELL ROAD	Not assessed for IDF.					
X56	Felixstowe	HIGH ROAD EAST	EXTEND ADVISORY CYCLE LANE FROM PRIORY Road TO CLIFF ROAD	EXTEND ADVISORY CYCLE LANE FROM PRIORY Road TO CLIFF ROAD	Not assessed for IDF.					
X57(1)	Felixstowe	MAIDSTONE ROAD -SEATON ROAD RNCBT	OPTION 1 REDUCE ROAD ENTRY WIDTH OF THE 3 ARMS BY LINING AND HATCHING ADD CYCLE LOGOS.	OPTION 1 REDUCE ROAD ENTRY WIDTH OF THE 3 ARMS BY LINING AND HATCHING ADD CYCLE LOGOS.	Not assessed for IDF.					
X58	Felixstowe	SEATON ROAD	ADD CYCLE LOGO 1057 BETWEEN HIGH RD WALTON AND MAIDSTONE ROAD	ADD CYCLE LOGO 1057 BETWEEN HIGH RD WALTON AND MAIDSTONE ROAD	Not assessed for IDF.					
X59	Felixstowe	BACK LANE	ADD CONTRA FLOW CYCLE LANE BETWEEN SEATON ROAD AND HIGH ST WALTON	ADD CONTRA FLOW CYCLE LANE BETWEEN SEATON ROAD AND HIGH ST WALTON	Not assessed for IDF.					
X60	Felixstowe	FELIXSTOWE LEISURE CENTRE	CONSTRUCT NEW OFF ROAD FACILITY FROM UNDERCLIFFE ROAD TO SEA ROAD BEHIND SEA FLOOD WALL. SCDC ASPIRATIONAL ROUTE	CONSTRUCT NEW OFF ROAD FACILITY FROM UNDERCLIFFE ROAD TO SEA ROAD BEHIND SEA FLOOD WALL. SCDC ASPIRATIONAL ROUTE	N/A	£1,500,000-£1,600,000	SCC, DFT, Developer, ESC	SCC, DFT, S106, S278, CIL	Sustainable transport, traffic management and cycle route improvements at Felixstowe	Essential
X61	Felixstowe	CRESCENT ROAD /HAMILTON ROAD JUNCTION	AT TRFFIC LIGHT INSTALL ADVANCED STOP LINES (ASL)	AT TRFFIC LIGHT INSTALL ADVANCED STOP LINES (ASL)	Not assessed for IDF.					
X7	Felixstowe	PICKETTS ROAD	SIGN ROUTE	SIGN ROUTE	Not assessed for IDF.					
X9	Felixstowe	BETWEEN GARRISON LANE & MAYBUSH LANE	SIGN AS LOCAL ROUTE 7 ST ANDREWS ROAD & FOXGROVE LANE AS CYCLE ROUTE	SIGN AS LOCAL ROUTE 7 ST ANDREWS ROAD & FOXGROVE LANE AS CYCLE ROUTE	Not assessed for IDF.					

Consultation Statement

East Suffolk Cycling and Walking Strategy

October 2022



EASTSUFFOLK
C O U N C I L

Contents

1 Introduction	1
2. Who was consulted?	2
3. How were they consulted?	2
Appendix 1: Initial Consultation Responses	5
Appendix 2: Initial and Draft Consultation Bodies.....	193
Appendix 3: Initial Consultation Promotion Material	195
Appendix 4: Draft Consultation Promotion Material.....	198
Appendix 5: Formal Consultation Responses	203

1 Introduction

The purpose of the East Suffolk Cycling and Walking Strategy (the Strategy) is to create safe, coherent, direct, comfortable, and attractive cycling, walking and wheeling environments that lead to improvements in health and wellbeing, facilitate greater social interaction and play, encourage more environmentally sustainable lifestyles, reduce road congestion, and support economic growth. In order to create these environments, the Strategy identifies cycling and walking infrastructure recommendations across East Suffolk (including the Broads Authority Area), focussing on the identification of new and improved infrastructure rather than the maintenance of existing infrastructure.

Once adopted, the Strategy will replace the Waveney Cycle Strategy (2016) and serve as the Local Cycling and Walking Infrastructure Plan (LCWIP) for East Suffolk.

While the Strategy is not a Supplementary Planning Document and is therefore not bound by the Town and Country Planning (Local Planning) (England) Regulations 2012 in the same way that a Supplementary Planning Document would be, East Suffolk Council has sought to follow a similar procedure to that set out in these regulations throughout the preparation of the Strategy. The reason for this is to facilitate effective community engagement and ensure the Strategy's recommendations are robust.

The Council's approach to engagement in the preparation of a Cycling and Walking Strategy is not set out in the Statement of Community Involvement¹, however the approach to engagement in the preparation of a Supplementary Planning Document is. This approach has broadly been followed, as far as reasonably practicable, throughout the preparation of the Strategy. At the start of preparation of the SPD the Statements of Community Involvement adopted in September 2014 were in place (covering the former Waveney and Suffolk Coastal districts). The Council adopted a new Statement of Community Involvement in April 2021 which applied to the consultation on the draft Strategy and to the adoption of the Strategy. While preparing the Strategy East Suffolk Council has consulted with relevant organisations and members of the public. Details of this consultation process are set out below.

An initial stage of consultation was held for 7 weeks between 19th October and 7th December 2020. A formal consultation on the draft Strategy was held for 10 weeks between 1st November 2021 and 10th January 2022.

¹ How to get Involved in Local Planning – Statement of Community Involvement (April 2021)

2. Who was consulted?

Consultation was split into two stages: an initial stage that informed the preparation of the draft Strategy; and a formal stage of consultation that sought views on the draft Strategy.

An initial stage of consultation was held for 7 weeks between 19th October and 7th December 2020. A formal consultation on the draft Strategy was held for 10 weeks between 1st November 2021 and 10th January 2022. At the initial consultation stage, all of those registered on the Council’s planning policy mailing list were directly consulted. Steps were taken to advertise the consultation to others, as set out below.

The same approach was taken at the formal consultation stage, although those that had registered an interest in being kept up to date with the Strategy at the initial consultation stage were also consulted.

Both consultations were also made available to the public on the Council’s website.

3. How were they consulted?

There were two stages to the consultation process, which are set out below.

Initial consultation

The initial consultation ran from 19th October to 7th December 2020 and the interactive map was made available on the East Suffolk Council website via the pages below:

<https://eastsuffolk.maps.arcgis.com/apps/webappviewer/index.html?id=810e5f8977e144509f13120a00a341d6>

The consultation was advertised on the Council’s website, as well as on social media (see Appendix 3). All of those registered on the Council’s planning policy mailing list were directly consulted, and posters were sent to Town and Parish Councils to place in their notice boards if they so wished.

Hard copies of the comments form were also made available free of charge by post by contacting the Planning Policy and Delivery team as the usual locations for viewing documents were closed to the public, due to the Covid-19 pandemic.

The consultation sought views on existing cycling and walking infrastructure issues and opportunities to improve and for new infrastructure across East Suffolk, and asked the following questions:

1. Where is the matter / improvement located? Please provide as much detail as possible e.g. Junction between The Street and London Road, Lowestoft.
2. What is the matter / improvement? Please provide brief details.
3. Please suggest a possible solution / improvement. This is not required, but will help us investigate the matter/improvement.

In total 897 comments were submitted to the initial consultation. All comments submitted to the initial consultation have been incorporated into the Strategy as Community Recommendations, except for one comment that used inappropriate language.

Full copies of the responses have been published on the Council's website at

<https://eastsuffolk.maps.arcgis.com/apps/webappviewer/index.html?id=810e5f8977e144509f13120a00a341d6>

Formal consultation

The draft Strategy consultation ran from 1st November 2021 to 10th January 2022 and the consultation documents were made available on the East Suffolk Council website via the pages below:

<https://eastsuffolk.inconsult.uk/cyclingandwalkingstrategy2021/consultationHome>

The consultation was advertised on the Council's website, as well as on social media. 4,069 emails and 583 letters were sent out at the start of the consultation to the consultees on the planning policy mailing list which includes town and parish councils, individuals, and organisations including those who were previously contacted or responded to the informal stage of the consultation. The list of consultation bodies can be found at Appendix 2.

The consultation was advertised through the use of posters (provided to Town and Parish Councils), a press release and social media posts. The poster, press release and example social media posts that accompanied the consultation can be found in Appendix 4.

The draft Strategy was highly interactive, however a pdf version could be downloaded and printed if needed.

Due to the ongoing impacts of the Covid-19 pandemic, for those unable to view the documents online, an offer of assistance along with contact details for the Planning Policy and Delivery Team was included on letters, emails and the poster.

In total 434 individuals and organisations responded to the consultation. Between them they made 1,207 comments.

In addition to these comments, Suffolk County Council submitted comments after the close of the consultation, on 22nd June 2022. These comments have not been incorporated into this Consultation Statement at Appendix 5 due to their lateness. However, Suffolk County Council have played an important role in contributing to the preparation of the Strategy through their position on the officer steering group.

Full copies of the responses have been published in Appendix 5 below.

Appendix 1: Initial Consultation Responses

The table below lists the consultation responses to the initial consultation. All consultation responses, except one that used inappropriate language, have been incorporated into the Strategy as community recommendations.

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
A Rouse	503	B1084 between Bawdsey and Alderton	We need a continuous footpath from Bawdsey through to Alderton for walkers to feel safe. Currently there are 4 isolated sections of path that don't join up. Its already a 30mph road, and we dont expect nor do we want street lights, but we do need somewhere safe to step back clear from 2 way passing traffic on narrow roads.	A new 120m section of footpath (with elevated or rumble strip kerbing edging) should be created on the west side of the road to join up between the exit from the new Orwell Housing Development, and Pitcairn Cottage where the next section begins. If the road needs to be widened to accomodate it, then extend it into the verge/bank /hedge on the east side so that pedestrians dont have to keep crossing from one side to the other.
Aaron Taffera	644	At Oulton Broad South rail station adjacent to Bridge Road near Dell Road	A foot path / cycle path under the Bridge Road overpass connecting Oulton Broad South station to Dell Road.	The construction of a short foot path/ cycle path to go through an existing archway in the road bridge to connect Oulton Broad South rail station to Dell Road. The new route would open up the rail station to neighbourhoods north of Bridge Road for both cyclists and pedestrians who have no dedicated route to the station that is not step-free and segregated from road traffic entering/exiting via the station forecourt. The footpath would also create step-free and safe access to the Bridge Road foot-crossing via an existing archway in the bridge; presently two sets of steps must be navigated to make this journey. The scheme also negates the need for pedestrians and cyclists to use the busy junction at the station entrance.
Adam Schwarz	662	Woods Lane	Despite the 30mph zone, vehicles seldom adhere to it making this necessary pedestrian and cycling route very unpleasant and dangerous. In addition, for those wanting to turn into Woods Lane from side streets, the	Additional signage to ensure all drivers are aware of 30mph zone, and installation of a speed camera to ensure vehicle compliance. Potential taffic calming measures, including siganage and a pedestrian crossing point. Alternatively, and better still, reducing the speed

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
			speed combined with the volume of traffic make this dangerous. There T-intersection with Leeks Hill is a public right of way frequented by walkers a school children and requiring them to cross.	to a 20mph zone would vastly improve this stretch of road for other users while only adding 60 seconds to vehicle journeys and reducing local noise and pollution.
Alan Collett	62	Thorpe Rd Aldeburgh, the full length of this road between Aldeburgh and Thorpeness.	<p>Many cyclists use this road as it is difficult to cycle all the way to Thorpeness along the beach/foreshore, both because of the terrain and the number of people using the footpath. This road has a 60mph speed limit and because it is straight many people drive fast. It is therefore a dangerous road for cyclists and families to use.</p> <p>It should also be noted that this road runs along side a nature reserve and the risk to wildlife is significant. Deer are also a danger to drivers.</p>	Get the speed limit reduced to 30mph so that it becomes safer and links the 30mph limits in Aldeburgh and Thorpeness together.
Alan Comber	516	Woodbridge Road across Rushmere Common	The whole of Woodbridge Road and Main Road Kesgrave is too narrow to accommodate both cars and cyclists safely. To improve the situation widening the footpath across Rushmere Common so it can take cyclists and pedestrians would significantly help to encourage people to cycle (and walk) in to Ipswich.	Widening the footpath across Rushmere Common so it can take cyclists and pedestrians. Alternatively/additionally find another route across the common. There is a bridle way across the common which can be linked to longstrops in Kesgrave which if upgraded (surfaced) would provide a route and not encroach on any common land.
Alan Comber	517	Full Length of Main Road Kesgrave	The main road is too narrow to take both cycles and cars safely, The cycle path along the length of the road is also extremely uneven and crosses to many road to make it a practical through cycle route. This makes it unsuitable as a safe/fast through route into Ipswich.	The cycle path needs improving (levelling and better signage) and an alternative through route needs providing through Ksgrave - this could be along long strops bridle way. The only other way would be to provide a cycle route along the northern side of the main road - but assume this is not practical due to all the land that would need to be purchased. Pilboroughs Walk is too busy and has too many junctions to make it a viable through route either.

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
Alan Comber	518	Longstrops, Dobbs Wood and Foxhall Heath Bridleway - (Sandlings Walk)	This brideway can be used as a cycle way through Kesgrave but is currently grass / soil so isn't fast. It is also not lit. It is also narrow across Foxhall Heath. If the route was upgraded it could help relieve through cycling along the main road which isn't safe.	If a suitable surfaced cycleway was laid along the length, with possibly lighting, it would provide a fast, safe, traffic free route for cycling though Kesgrave. It would however need to be joined up at the Rushmere and Martlesham ends to amke it a continuous fast route into Ipswich.
Alan Comber	521	tarmaced private drive to lux farm	If a footpath or access could be provided up this drive it would help connect Kesgrave to playford, grundisburgh and beyond via footpaths. There is a footpath from main road, all Saints Church passing heath cottages to Playford Road. It needs extending to Lux Farm. At the moment to get to Playford and beyond you have to take footpaths either via Rushmere St Andrew or via Little Bealings. This is a significant diversion out of your way by a couple of miles.	Provide a public right of way or negotiate public access up the drive to Luz farm so you can join footpath leading on the playford etc. It would encourage more peopel to walk to Playford and beyond.
Alan Swerdlow and Jeremy Greenwood (originally submitted by email)	519	Pathway from Martlesham Creek to Kyson Point and on to Woodbridge	Having made much use of the pathway from Martlesham Creek to Kyson point and on to Woodbridge over the last lockdown months we have often been forced to step aside into less than safe areas to let cyclists pass. They should not be on these narrow paths at all - signs are inadequate. There have been talks about making this route more accessible for cycling which would cause considerable work and disruption and cost a very large sum. We are against such a proposal.	
Aldringham-cum-Thorpe Parish Council	654	B1353 running from Aldringham to Thorpeness	This road is heavily used by families to cycle to and from Thorpeness. The speed of traffic combined with the ever reducing width of the road makes this activity very dangerous.	A new cycle path/footpath linking these two villages would reduce the ever increasing risk to cyclists and pedestrians.

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
Alexandra Stone	101	Cycle route between Bungay and Beccles	Not currently a safe direct cycle route to Beccles from Bungay. The main road is very fast and cars often overtake on hills and blind corners, the smaller roads are equally fast with blind corners and generally poor road condition.	Cycle path along the B1062 road
Alice Taylor	48	The end of Hamilton Road and the steps that connect it to the North Parade (Lat: 52.47643 Lon: 1.76064)	The steep steps from the end of Hamilton Road to the North Parade create a severe hazard and obstacle for cyclists and disabled who otherwise could have an uninterrupted route from the north end of Lowestoft down to Pakefield in the south. Replacing the steps with a ramp will allow tourists to travel from one end of the town to the other on a scenic route and one that follows the route of the coastal pathway.	a ramp
Alice Taylor	49	Old High Street in the north and Kirkley in the south, business district	Lack of places to secure bikes whilst in shops, making people tie up bikes to lamp posts, benches and drain pipes. Even where there are some bike racks (in front of HSBC for instance) there are too few of them and often there is no place to properly secure a bike.	Where the paths are very narrow, narrow horse hitch style posts can be put next to buildings all (not the wider Sheffield bike racks).
Alison Coote	791	East Suffolk	See below.	<p>I would like to ask that when compiling your cycling and walking strategy, you also take into account the wishes and needs of horse riders, for the following reasons:</p> <ul style="list-style-type: none"> • Horse riding is also a healthy form of outdoor exercise • Horse riders share rights of way (bridlepaths and byways) with cyclists, and their needs may be different. For example putting down a hard surface to make a right of way better for cyclists would be detrimental if not dangerous for horse riders • Horse riding contributes significantly to the local economy, such as riding schools, livery yards, farriers, vets, feed merchants, tack shops etc

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				<ul style="list-style-type: none"> Horses have to be kept all year round, we don't just put them in a shed for the winter and get them out again when the weather improves! <p>Local horse riding organisations, and the British Horse Society, should be consulted for their views on any proposed changes to bridlepaths and byways.</p>
Alison Vickers	528	There is no safe way for pedestrians to cross between Howlett Way and Kirton Road.	When crossing from Trimley St Martin on Howlett Way, the verge on the right hand side is totally overgrown with brambles, forcing the user onto the road which is very unsafe. Having crossed the slip road of the A14 from Felixstowe, crossing the sliproad to Felixstowe is difficult because of poor visibility of vehicles coming round the roundabout and onto this sliproad fast.	A pedestrian crossing controlled by traffic lights would be the only safe way. The brambles would also need to be cut back too regularly.
Alison Vickers	635	Between the footbridge over the A14 in Trimley Saint Martin and Capel Hall Lane.	There is no safe way for pedestrians of reaching Capel Hall Lane from the footbridge. There is a footpath marked on the ordnance survey map 197 which is part of the Stour and Orwell walk. There is no safe way to reach this footpath from the footbridge.	Create a safe route from the footbridge to the footpath. Only a short section is required. This could be done by having steps put in between the bridge approach and the path. Alternatively, cutting back the brambles along Kirton Road so it is safe to walk on the verge.
ALLAN DOUGLASS	69	road from hollesley village (rectory road) , moors farm corner to shingle street.	The road to Shingle Street from Moors farm, which is a minor road, has 5 very dangerous blind corners, yet it is sign posted at national speed limit. This road has become very busy with walkers and cyclists (including many children), horse riders and dog walkers, tourists including campervans, 'boy racers' and large heavy vehicles. It also includes a national cycle way and is used as a Duke of Edinburgh Award walk.	Reduce speed limit to 30 or less and please look at the corners before their is fatalities

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			Further information on request as I have lived on this road for 35 years.	
Amy Rayner	421	Many of the pavements in Saxmundham (particularly the high street and the roads off the cross roads at the traffic lights on town.	The pavements in Saxmundham are in many places very narrow and not fit for purpose. In many places they are too narrow for mobility scooters and pushchairs or even for two pedestrians to pass safely. This is especially true on the high street.	Making a section of the high street pedestrians/deliveries and disabled access only.
Amy Rayner	422	The B1121 between Kelsale, Saxmundham and Benhall	Lack of safe cycling route along this road which links two primary schools, two villages and the town centre and is used by motorists and lorries to access town/A12. It also has a very narrow pavement between Benhall and Saxmundham which forces pedestrians very close to the fast moving traffic.	Implementation of the Three Communities Link proposal. Providing an inclusive and safe cyclist and pedestrian route for vulnerable road users including those with children, pushchairs and mobility scooters. The plan already exists, just requires funding.
Andrew Burtenshaw	27	On the Reckford Road between Westleton and Middleton	It would be extremely useful and much safer for pedestrians if there was a footpath from the Southern end of Black Slough to the junction of Reckford Road and Back Road (Middleton. This is a popular walk and would link up with several other footpaths in the area.	
Andrew Duncan	336	The junction between the Market Place and Bridges Street and the contraflow cycle lane.	The junction going uphill is rather dangerous because cyclists must give way to unpredictable traffic. The turn from the market place makes larger cars/vans/lorries swing into the cycle lane round a blind corner. The 20 mph speed limit in Bridge Street is frequently ignored. Cars and vans park in the cycle lane, pushing cyclists into the path of oncoming traffic. Bridge Street is a rat run for traffic going to Norwich.	"No Entry (except cycles)" at the Market Place/Bridge Street junction, preferably with a planter partially blocking the access for vehicles. "Access to Bridge Street via Nethergate Street" , enabling deliveries and residents access while quietening the road. Widening the pavements, initially with paint and identified loading bays to enable street life to take place safely.

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			The noise levels and vibration are unacceptable.	
Andrew Duncan	350	The A144 between the Bungay Bowling Club and through St Mary's Street, Bungay	<p>Lower Olland Street, Bungay is two way with on street parking. As a result it is often congested and dangerous. It is unpleasant for all users (including motorists) but especially for pedestrians and cyclists.</p> <p>There is a notice telling motorists to "consider pedestrians" and "courtesy crossings" with a 20 mph speed limit. So cycling is often subjected to intimidatory driving, the speed limit is not enforced and nobody knows where it is safe to cross the road.</p>	<p>Make Lower Olland Street one way northbound with Beccles Road one way southbound both with a contraflow cycle lane. Dual use pavements even widened ones, turn cyclists into a hazard.</p> <p>Enforce a 20mph speed limit by camera if need be.</p> <p>Clearly mark and identify pedestrian crossings.</p> <p>The roads in the centre of Bungay were built as multi use roads for pedestrians and horse drawn traffic. To make them more pleasant (and IMPROVE the sacred cow of traffic flow) you need the courage to reallocate some road space. The alternative is doing nothing or demolishing half the town to improve traffic flow.</p>
Andrew Eastaugh	114	The B1127 between Wrentham and Reydon	It is extremely unfriendly for walkers and cyclists. In spite of it being a minor road with double bends and poor visibility cars come at speed making it very unsafe.	There should be speed restriction and a cycle lane
Andrew Jolliffe	130	Street between Duck Corner and Woodbridge Walk, Hollesley	<p>main road between two parts of the village, but no cycle or footpath. Both parts of the village are within a cycling distance but the 60mph speed limit and no pathways make it too dangerous.</p> <p>Has been spoken about for at least twenty years but no positive outcome.</p>	Some cycle or footpath to allow people to safely walk from one part of the village to another.
Andrew Jolliffe	131	Main Road, Martlesham - south end of road	There are no zebra crossings along the entire road. however there are a high number of elderly and disabled residents on the north side of the road. this restricts their ability to walk to the local shops such as Tesco.	a zebra crossing to be installed creating a link between both sides of the busy road.

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			There are a small number of traffic islands, however six weeks ago a man with limited vision was knocked down by a car in this area. he believes this was partly due to a lack of safe spaces for him to cross and excess speeding.	
Andrew White	606	Between Somerleyton and Blundeston	I live in North Oulton Broad, and would love to go on country bike rides with my young children, but cannot risk them biking on the country roads. This said, although the villages of Blundeston, Somerleyton etc are very close, it is near impossible for us to bike there.	To expect a change in road infrastructure is impractical, therefore I can only suggest that a review of public footpaths in this area (as well as other similar areas) are made in view of bolstering these to provide the potential to cycle along them. This may require some compulsory purchase to widen footpaths, and a form of deterrent for motorcycles, but I believe it would be an excellent means of safely connecting the local villages and allowing families a better means of exploring these areas (which in itself can help with increasing trade/footfall in local businesses)
Andy Bebington	30	Southwold; south End of main road, in Market Place	Lack of cycle parking, leading to passive-aggressive signs "not to park here" on various buildings	Provision of Sheffield racks (other designs of that sort are acceptable, designs holding only a wheel are not, whether bolted to the ground or to a wall)
Andy Bird	129	Footpath between Longstrops and Dobbs lane	Increase and improve cycle network	Turn footpath into bridleway and if need be turn bridleway into footpath - suitability is the opposite of designation.
Andy Smith	658	Clickett Hill Road at junction thereof with entrance to new Unilever development and existing footpath / Cycle Route 51 to east & north	This point should be seen as the Core Hub for a range of improved (short term) or new (long-term) routes around North Felixstowe and Trimley, and to Ipswich and to Martlesham / Woodbridge. It has good but not always well maintained access to Western Felixstowe, although of uncertain public access status. The access towards Trimley is generally useable, but of varying quality, as well as status.	Options which should be explored: a) A new safe crossing of the now busy Clickett Hill Road as HGV access to the current Unilever development b) A new route adjacent to the western perimeter of the Unilever development to link with Footpath 30 railway crossing to the north and then onwards to the Deben valley, including linking with the forthcoming Felixstowe Garden Village development. b) Provision of a new Pedestrian / Cyclist route adjacent to Clickett Hill Road to the south to improve safety of access to the port employment area

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				c) A consistent standard of surface and access rights on the existing Route 51 to Trimley High Road d) A major new strategic initiative to provide a much more cycle friendly route to Ipswich than the current Route 51. Specifically the lanes through Levington and Nacton are not seen as cycle friendly due to the combination of their twisting nature and traffic levels / speeds. However the challenges of this are recognised to be significant.
Andy Smith, Councillor, Felixstowe Town Council	438	Area bounded by Candlet Rd, Gulpher Rd and approximately the track to Candlet Farm	This area is the subject of major proposals for development of housing and a sports centre in the East Suffolk Local Plan containing significant walking & cycling proposals. Those interested in this area may wish to look at that for information. The relevant policy is at pages 215-221.	All Walking and cycling matters in this area and those to the East planned for development in the East Suffolk Local Plan should be considered in the future context of the entire area.
Andy Smith, Councillor, Felixstowe Town Council.	440	Area bounded by Links Avenue, Upperfield Drive, Ferry Rd, Gulpher Rd to The Grove	This area is the subject of major proposals for development of housing in the East Suffolk Local Plan containing significant walking & cycling proposals. Those interested in this area may wish to look at that for information. The relevant policy is at pages 215-221.	All walking and cycling matters in this area and those to the East planned for development in the East Suffolk Local Plan should be considered in the future context of the entire area
Andy Smith, Town Councillor, Felixstowe	437	Area bounded by Candlet Rd, Gulpher Rd, The Grove	This area is the subject of a major planning application for 560 houses, ref DC/20/1002/ARM, containing significant walking & cycling proposals Although the formal comment period for that is closed, those interested in this area may wish to look at that for information, and possibly also add a comment there.	All Walking and cycling matters in this area and those to West and East planned for development in the East Suffolk Local Plan should be considered in the context of the entire area.
Angie Buggs Levington and Stratton Hall Parish Council	369	Levington and Stratton Hall	Public footpaths are enjoyed by many walkers but are increasingly being plagued by cyclists who endanger the use by walkers and erode narrow coastal paths, delicate in many places	Although the misuse of footpaths contravenes the tort law of trespass, it is highly unlikely to be enforced by any landowner. Any strategy needs to make clear that cycling on public footpaths is unacceptable and unlawful.

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			<p>as previous breaches will testify.</p> <p>Once the strategy is adopted, the bridleways and cycle paths must be properly maintained to encourage their use. The poor state of the A14 cycle way is an example of poor maintenance.</p>	<p>Parishes like ours who welcome considerate walkers to the footpaths are becoming increasingly inundated by rubbish dumped. Although litter picks clear up their rubbish, it needs to be clear that rubbish dumping is an increasing nuisance and that measures should be introduced to eliminate it. The provision of cycle paths seems to be less than public footpaths and this needs to change to avoid clashes between those on foot and those on cycles.</p>
Angus Montgomery	589	Wilford Bridge Road, between Melton Station and the roundabout	Wilford Bridge Road - in particular between Melton station and the roundabout, is becoming increasingly busy, with large amounts of lorry traffic. It is the only access route to the peninsula for cyclists and is extremely narrow and congested.	Cycle lane to be added
Ann Bradburn	406	Yarmouth Road footpath adjacent to Ufford Park Hotel.	Due to the encroachment of soil and grass and other plants over the concrete footpath, the footpath is now extremely narrow. This has resulted in pedestrians having to walk very close to the road side. The footpath is only wide enough for pedestrians to walk in single file thereby making it impossible to safely hold a young child's hand or to push a toddler's buggy. It is extremely uncomfortable and dangerous to walk this part of the footpath as being so close to the road is dangerous.	The soil/grass/plants need to be dug or scraped back so that the full width of the concrete footpath is available.
Anna Pickering	162	Felixstowe Road	It's not safe to cycle or walk along this road with the heavy traffic usage, blind bends and excess speeding. The overgrown plants, narrow path and cycle lanes, and lack of speed awareness are not helping the situation. There's also nowhere safe to cross from the footpath into the community centre.	Speed signs, possibly even reduce it to 20mph, maintain/cut back roadside plants, provide crossings at crown point and community centre. Also widen the footpath and cycle lanes, making it a one way road would assist this and create a more pleasurable journey.

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Anna Pickering	163	The A12 underpass by PHQ	Underpasses are the only way to cross the A12 from Martlesham village, they're both dark, dirty and uninviting. When it's raining/snowing/icy it's difficult and unsafe to use them as they're so slippery, you could slide down but getting back up the other side can be akin to climbing a mountain. Those of us with disabilities want to get out and walk/cycle rather than travel short distances in cars but this is a massive obstacle.	Widen the underpasses to build steps as an alternative to the foot and cycle paths, lay an anti slip surface, hand rails, better lighting, discourage undesirables from hanging around
Anna Spencer	342	Roundabout too narrow for cars and bikes	Rushmere Road/Colchester Road Roundabout is too narrow at peak time to allow safe cycling. The junction needs improvement	
Anna Spencer	343	Cycle lane along Woodbridge road east	The cycle path/lane on the pavement along woodbridge road is a joke: it is old, raid surface is terrible, too narrow and occupied by pedestrians, blocked by driveways making it very dangerous and cars d not stop	
Anna Spencer	344	Terrible bike path	The shared bike path pedestrian lane past Suffolk Constabulary has very poor surface with holes and rotten leaves	new surface regular clearing
Annabel Walker	354	Riverside Beccles	The path becomes very muddy in autumn and winter. It would be excellent if path could be maintained ie adding grit or building a broadwalk. This would encourage many more people to use the path.	Add grit or build broadwalk
Anne Cox	588	Sandy Lane, Woodbridge, Ipswich Rd junction to railway bridge	Sandy Lane is a dangerous place to walk because there is no escape from speeding traffic! The stretch from Broomheath Rd to the Railway Bridge (part of Circular River Walk) is especially dangerous. The narrowness and blind bends make it unsafe.	Sandy Lane needs a footpath! And a 20mph speed limit.

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Anne Neill	616	The Promenade	May I please ask you to consider allowing cycling on the lower promenade during off peak times. For example, not during the peak holiday season or any Bank Holiday weekends. Additionally, when cycling in the designated cycle path on the top of the promenade, pedestrians who wander aimlessly across the path also give cyclists a great amount of abuse.	
Anne Neill	617	Sparrows Nest	cycling North up the High Street, but heading to The Sparrows Nest park, involves crossing lanes of traffic, around the central island where the garage is. As I want to get to Gunton Cliff and down Links Hill to cycle back to town along the Cycle path along North Beach, I find this section really dangerous.	
Anne Neill	618	Pakefield	From a leisure point of view, cycling Pakefield to Southwold would be excellent. Kessingland is a complete no-go, and beyond that, on the A12 would be nothing short of life threatening, yet there are many country footpaths that with a bit of care could be opened up to the cyclist.	
Anne Smith	480	The thoroughfare, Halesworth. Between Halesworth Library and the Thoroughfare/London Road junction	Cycling to be allowed in both directions, thus allowing both local and visiting cyclists to travel through The Thoroughfare and use its facilities	
Anne Tooze	366	Footpath entrance adjacent to the railway crossing at Darsham station	The public footpath exit on to the A12 is dangerous. It opens directly onto the A12 with poor steps, no visibility or waiting place for crossing. The pavement is the other side of the road with no direct means to access it other than either go back up the road or over the banked verge. The exit has been marked as	Work needs to be done to the steps, waiting area, visibility for crossing the road and allowing access onto the pavement the other side of the road. or investigate a pavement in front of Darsham Nurseries leading back towards the garage and shop where visibility may be better.

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			closed for some time but needs to be re-opened to allow access to the station and the shop at the garage.	
Anne Tooze	367	between the A144/A12 junction and the Hinton lane/A12 junction (in front of the 2 Magpies bakery)	The formal footpath ends opposite the A144 junction with no where to walk safely next. It is dangerous to walk or cycle to the bakery beside the A12. Create a new stretch of path from the end of the existing path to the Hinton turn off to access the bakery and High Lodge. This would also create a safer link out to Dunwich, Walberswick (and then Southwold via the Bailey Bridge) along the Hinton Road. This could link in with the cycle routes from Willow Marsh Lane.	Create a new stretch of foot and cyclepath from the end of the existing path to the Hinton turn off to access the bakery. There is a wide verge between the end of the existing path in front of the bakery to the Hinton lane turn off. It is only a very short distance and would make the existing footpath very useful.
Anne Tooze	368	Between Garden House Middleton and Middleton Moor	There used to be a permissive path from opposite Garden House towards Middleton Moor this is now closed. To get to Middleton Moor from the footpath that comes out next to Fordley Road you have to walk on the B1122 which although is supposed to be 30 mile per hour limit the visibility is not good and the lorries do not give way. The addition of a short piece of footpath would allow the footpaths and lanes towards Kelsale or Yoxford to link up with the paths and lanes out from Middleton.	Create a short piece of off road footpath beside the B1122 between Fordley Road and the Middleton Moor footpath
Anne Westover	374	A section of permissive footpath on our circular walks route, south side of B1078 The Gallows Route developed with SCC (Discover Suffolk)	A section of permissive footpath on our circular walks route, blue The Gallows Route developed with SCC (Discover Suffolk) has been closed by the landowner forcing people to walk along the dangerous B1078.	Liaise with landowner and SCC Highways to arrange re-opening please. Raised several times this year with SCC and a Cllr.
Anthony Ellam	661	There are pinch points on the Hill at Wickham Market, at the Post	Cyclists to feel safe these areas to encourage them to cycle in and around the village	The introduction of 20mph speed limits and 'shared space' for cyclists, pedestrians and vehicles.

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		Office and at The Teapot Tea Rooms. The hill coming up from Bordercot Lane on to The Hill		
Antony Barrett	87	Ipswich to villages (this issue also applies to every town in Suffolk)	There are no safe cycle routes between Ipswich and and villages within a 15 miles radius. Where they exist few drivers keep to the 30mph limits and there are far to many stretches with just the National Speed Limit. On relatively narrow roads this leaves cyclists and pedestrians very close to vehicles doing up to 70mph. Safety concerns are a major reason that more people do not cycle or walk.	Create dedicated cycle and pedestrian routes to link villages with Ipswich. Where possible these routes should exclude vehicles except for access or have enforced speed limits. The routes should also have the sort of cycling safety features that Holland has introduced
Antony Barrett	88	Woodbridge to villages (this issue also applies to every town in Suffolk)	There are no safe cycle routes between Woodbridge and and villages within a 15 miles radius. Where they exist few drivers keep to the 30mph limits and there are far to many stretches with just the National Speed Limit. On relatively narrow roads this leaves cyclists and pedestrians very close to vehicles doing up to 70mph. Safety concerns are a major reason that more people do not cycle or walk.	Create dedicated cycle and pedestrian routes to link villages with Ipswich. Where possible these routes should exclude vehicles except for access or have enforced speed limits. The routes should also have the sort of cycling safety features that Holland has introduced
Antony Barrett	143	Helmingham Rd from the centre of Otley to the White House pub and the houses at the edge of the village.	The road is fast and straight despite the 30mph limit. There is no foot or cycle path. This splits the village and makes it dangerous for cyclists and pedestrians to move to and from the village.	A shared cycle footpath would encourage both cycling and walking and reduce car use. This would be especially effective if it included traffic calming measures
Arthur Stansfield	577	A1214 cycle route through Kesgrave plus other locations	Like many of the cycle routes alongside roads in Suffolk cyclists need to give way at junctions. This requires looking over the right shoulder to look for cars turning left. This is dangerous and is also a major inconvenience having to slow down or stop at junctions. If cycling on the road	I lived in Munich for 2 years and cycled there. Cycle routes had a right of way over side roads that they crossed. It worked well all vehicles gave way as needed.

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			the cyclist like vehicles has a right of way across the junction. Also pedestrians have a right of way at junctions according to the highway code.	
Arthur Stansfield	578	Public footpath from Mill Lane Wickham Market to Mill Lane Campsea Ashe	The path between the bridges gets very muddy and when the river is in flood mode the bridge closest to Wickham Market can become unreachable due to high water levels. This route could also provide a good cycle route from the centre of Wickham Market to the railway station	Improve the entrance to the bridge. Provide a decent surface along the public footpath.
Arthur Stansfield	579	The path along the river defence	The path is too narrow, people walk either side of the path leaving an often muddy stretch on each side of the path.	Widen the surfaced path. This would improve the experience of walkers. If the path was wider it would become possible for the path to be shared with cyclists
Arthur Stansfield	580	General comment about public footpaths	Officially public footpaths are not for use by cyclists. A lot could probably be opened up to cyclists and would provide safe off-road routes.	Open suitable public footpaths to cyclists
Arthur Stansfield	581	Speed of cars on country lanes endangers cyclists and pedestrians	cars travel too fast on country lanes and endanger cyclists and pedestrians	For many country lanes (especially single track lanes) a realistic speed limit would be 30mph. The speed limit on country lanes should be reduced to 30mph. It would probably have a minimal effect on journey times along the country lanes for cars. It would also improve villages if the speed limit within the settlement boundary is 20mph. This would also reduce CO2 emissions etc.
Ash Tadjrishi	570	Trimley St. Mary to Kirton via Howlett Way (and return)	There is a known history of accidents involving motor vehicles and cyclists on this route, sadly including the recent death of a cyclist as a result of a collision with a motor vehicle entering the roundabout via the A14 eastbound off-slip junction. The roads connecting the Trimleys to Kirton via	Provision of a clearly marked cycleway along the roads connecting the Trimleys to Kirton via Howlett Way, including the roundabouts, to give better protection to cyclists and improving drivers' awareness of other road users.

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			Howlett Way, including this roundabout, could conceivably accommodate a safe and clearly marked cycleway offering better protection to cyclists and improving drivers' awareness of other road users.	
Ash Tadjrishi	571	Junction from Felixstowe Road (A1156) and Straight Road	Cyclists seek westbound on the popular A1156 Felixstowe Road seeking to turn north up Straight Road have a limited opportunity to safely merge to the centre of the road with fast moving traffic behind them.	Provision of a cycleway along the A1156 and any additional safety features to enable cyclists to be able to turn right in to Straight Road (and potentially right from Straight Road on to the A1156).
Austen Gilbert	240	Along A1071 between hadleigh road and A1214	No cycle route provided along this way for cyclists coming from south of Ipswich and needing to get to Hadleigh Road.	With new estate being built a route through could be planned there is an existing foot path across fields that could be upgraded or an extra lane on either side of the existing A1071
Austen Gilbert	241	Underpass under the A14	Lack of cycling access through to Sproughton meaning cyclists either have to go to central Ipswich or the very busy Sproughton High Street if attempting to get to the Sproughton Road/Morrisons areas of Ipswich	The current underpass be redesignated as having cycling access, and the steps on the Hadleigh Road side replaced with a ramp which will help cyclists, pedestrians with pushchairs/trolleys and those with walking difficulties
Barry Ford	351	main road between Rendlesham and Tunstall	It is too dangerous for children even with adult supervision to cycle to Rendlesham school from Tunstall and Blaxhall. Road is very busy and has narrow 2 lanes with limited visibility due to the bends.	Off road cycle path would be best solution this could also be extended to Tunstall Forest where the Viking cycle trail is located allowing the public to cycle there instead of having to take their bikes on vehicles.
Beccles Town Council	663	N/A	Beccles Town Council, noting that as Suffolk County Council also have a cycling and walking strategy, the ESC cycling and walking strategy should not duplicate this and that the two strategies should link together, particularly as Suffolk County Council are responsible for the highways and transportation infrastructure. The linking of both strategies is also important to ensure that all comments received by the	

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			separate strategies, are duly considered when the overall strategy is reviewed.	
Beccles Town Council	664	London Road, from Wash Lane to the new bypass	With respect to the proposed routes, it was considered that urgent consideration be given to new cycles path from Wash Lane to the new bypass.	
Beccles Town Council	665	Ellough Road to Cedar Drive	With respect to the proposed routes, it was considered that urgent consideration be given to new cycles paths from the new bypass along Ellough Road to Cedar Drive.	
Ben Crossman	28	Cycle path between BT and towards Brightwell	Path is narrow, overgrown and dual carriageway is next to it and unprotected. Rationalisation of path required perhaps in conjunction with Brightwell lakes Development. Linkage of current national and local paths required in this area more generally.	
Bernard John Guymer	277	East side of A47 Yarmouth Road, Lowestoft between Gunton Church lane & Weston Road	Running parallel with the A47 are two slip roads that are closed for vehicular traffic as shown. Between the two slips was access for pedestrians & cycles, frequently used by children from Benjamin Britten High & Gunton Primary together with many pedestrians. Although this access is most likely privately owned access has been available for 40 years that I'm aware of. Access was blocked last March by a tree stump and barriers.	Application has been made to Highways to have the route classified as a footpath
Bill Camplin	175	Between Halesworth and the sea at Southwold	To deliver a cycle route which is safe for children and adults and would enable them to get from the town of Halesworth and nearby villages to the seaside at Southwold (10 miles). It would avoid busy main roads. It would make use of existing bridleways and would link into the national cycle route 1 at Halesworth	Suggested route: start Halesworth Town Park, take National Route 1 along Millennium Green to rail level crossing on Walpole to Mells road. Follow road to Wenhaston and the then to A12 at Blythburgh 644900 274900. Take Bridleway Blythburgh 1and 9 eastwards. Take Bridleway Walberswick 28 and 29 eastwards and join Blythburgh to Walberswick road. Leave road on

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			Millennium Green. It would benefit from signage and a few improvements in path surface to make the route easy to use. Great for fitness, great for all ages, great for building tourism.	Bridleway Walberswick 5 to the river bridge. Cross river and take Bridleway Southwold 25 to Harbour Inn. Then road to Southwold. Main improvement surface of Bridleway Blythburgh 1 and security of short section adjacent to A12. Survey needed for all bridleway surfaces. Route shown in image file attached.
Bob	64	Footpath leading to steps to the beach at the end of Martello Lane, Felixstowe. Known as Jacobs Ladder I believe	The footpath is overgrown. You need to weave your way along avoiding weeds, plants, dead foliage etc along with overhanging branches from neighbouring houses	
Brian Ferrand Williams	168	Chimer Lane/Hall Lane/Honeypot Lane junction near Charsfield	This whole area not just this confluence of c - roads is an exceptionally rich completely rural area which offers outstanding cycling. The nature of the roads is that of restricted width and with many blind bends. Unfortunately motorists seem to think it is a racetrack and often are moving at unsafe speeds for cyclists. At least once in last month I have been almost brushed by a passing car at speed, unsafe for him/her and me	The diversity of nature is outstanding in this area. Just today cycling that route I encountered a young stag with approximately 8 points on his antlers, several buzzards, hunting; various other birds and rabbits. An upper speed limit of 40mph on such roads whilst not making them safe would reduce some of the risk. Could we have a countryside limit please in Suffolk or lobby for such nationally on roads of a diminished width?
Brian Ferrand Williams	371	Bus stop opposite Penzance Road in Bell Lane Kesgrave	there is a sign here stating pedestrians and cyclists allowed. Cyclists assume they are able to cycle from here to Foxhall Road on the pavement as they have been allowed so to do from the Woodbridge Road end of Bell Lane. Pedestrians are of a different opinion, and there is contention	If cyclists are allowed to cycle all the way to Foxhall Road from the last sign at the junction of PenzanceRd/Bell Ln then more signs are needed. If they are not then a sign saying cycling ceases/stops/not permitted is needed to stop confusion and a likely future accident
Bryan Frost (Cllr for Trimley St Mary PC but	495	Cycle path adjacent to Trimley to Levington link road	This path is in a very poor state with many uneven bumps and potholes, and is also dangerously close to a fast section of the A14.	Ideally, the path should be re-sited to run alongside the link road, far safer. In short term, it should be resurfaced and a sturdy barrier placde to shield it from the A14

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writing personally)			As a result, many cyclists choose the link road, slowing vehicular traffic and causing drivers to be impatient.	
Carole Wilson	158	Rendlesham has no safe walking or cycling connectivity to anywhere else...	Rendlesham is accessible only from the A1152 - all entry/exits are along that road which has no foot/cycle path. There is no signage to indicate cyclists/walkers may be present. The speed limit of 40 stops before Rendlesham Mews - and is frequently exceeded by drivers who presume it's a safe-for-them straight stretch, they can see the upcoming increase of speed permission sign. Vehicles passing the Mews at 60 mph+ makes it unsafe for cyclists to turn into the Mews and lanes beyond.	Create a path along the A1152 to extend from the roundabout to the Mews. Extend the speed limit to 40 all the way to Eyke. This would remove the dangerous 60 stretch that includes turnings to the Mews and to the lanes that lead to Friday Street/the forest on one side and to Rendlesham St Gregory's Church/Campsey Ash/Wickham Market on the other. Put up signage on the A1152 that indicates to drivers that they are passing through a residential area where cyclists and walkers may be present.
Caroline Ley	378	Howlett Way, Trimley St Martin, along its full length	This road carries traffic travelling to and from the A14 junction 59. The volume of traffic and the 40mph speed limit discourages cyclists. A new development of 340 houses is planned with vehicular access off Howlett Way with the result that Howlett Way will become very much more busy. Cyclists travelling from the new development to Trimley St Mary, Walton and Felixstowe, including pupils travelling to school, will have to negotiate a stretch of Howlett Way in order to reach the High Rd.	Install a separate, kerbed cycleway
Caroline Ley	379	The village of Trimley St Martin and its links to neighbouring villages	As a result of local plan allocations the number of dwellings in Trimley St Martin will increase by 630 which is over 50%. This is likely to result in traffic congestion and increased danger for those walking and cycling, but it also provides the opportunity to make significant improvements to encourage cycling.	The first step should be to conduct a full and detailed review of cycling within and around the village looking at the possibility of creating new off-road cycle routes as well as improving the provision for sections where on road routes are unavoidable.

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Caroline Ley	380	Old Kirton Road, Trimley St Martin, Footbridge over A14	The existing pedestrian bridge across the A14 is not cycle-friendly	Widen the bridge and create a cycleway which would join both sides of the A14
Caroline Ley	381	Gulpher Road, Felixstowe	Provide an improved surface and access to create an accessible cycleway which would link Gulpher Road and the bridleway to provide an effective High Rd bypass for cyclists	
Caroline Ley	636	Between Kirton village and the site adjacent to Reeve Lodge, High Rd, Trimley St Martin	Trinley St Martin Primary School is currently located in Kirton Rd, in easy walking distance of Kirton village. In 2023, or thereabouts, it will be relocating to a site on the opposite side of the A14 adjacent to Reeve Lodge, High Rd, Trimley St Martin which is much further away.	A safe, segregated cycle track is needed to enable Kirton children to cycle to the new location.
Caroline Topping	488	This used to be a road. It is now a very important green corridor. Whole length of Rigbourne Hill Lane	The surface needs updating. The hedges need cutting back. The bank needs taking back. Important cycling/walking link from the new garden community.	This will be a main route from new Garden Community into town. We need to encourage walking and cycling and this is an existing safe route that needs upgrading, rather than a new route putting in.
Carolyn Gibbins	463	The roundabout top of Woods Lane / A12	1. impossible to see oncoming traffic coming from south on A12 when crossing A12 on the path from the north 2. Impossible to see oncoming traffic when crossing Woods Lane from North to South on the path	In both instances, the path could be closer to the roundabout
Carolyn Gibbins	464	river path woodbridge to Melton	little room for both pedestrian and cyclist although most cyclists dismount for pedestrians	where the path splits into 2 levels, make one for cyclists and one for pedestrians. Visiting cyclists to woodbridge cannot believe cyclists are not allowed along the whole of the river path
Carri Adams	92	Anson Road in Martlesham at the small Tesco roundabout between Tesco and Pets at Home	This is the perfect place for a crossing. A lot of us that like to walk to the shops from Martlesham IP12 there is not a safe place to cross to get to the other side where all the other shops are. We have to put our lives at risk twice trying to cross this busy road and wait for	A traffic light crossing with a button to physically stop the traffic when someone needs to cross. This would keep the traffic flowing and only be used as and when the public needed it. I have witnessed a few people now nearly get hit by cars not stopping for the people using the zebra crossing further up and so due to the high

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			a car to stop. Trying to park is sometimes a nightmare so walking is so much easier and this could be made a lot easier and safer for us all to do so and encourage more to do so by putting in a crossing at this roundabout.	volume of traffic this is the only safe way to cross. We always have to wait for ages to cross or for one kind person to stop for us and wave us across when safe to do so. It's a matter of time before someone gets hits trying to cross this area between Tesco's and Pets at Home.
Charmaine Biggle	530	The junction with The Street/Wiford Bridge and Melton Hill Road	<p>The crossing from The Street to the primary school is very narrow and there is considerable congestion during school hours. The traffic is also very heavy at these times,</p> <p>The Street should have light vehicles only using the road between Woodbridge and Ufford except for access to and from business in the area. As a walker I have nearly been struck several times by large vehicles passing along the road close to the pavement</p>	<p>Re landscape grass verges on the junctions with the lights and the crossings to Melton Primary School.</p> <p>Erect sign asking motorists to switch of engines when idling by lights.</p> <p>Prohibit large vehicles from using the road between Woodbridge, Melton and Ufford unless for delivery only to local business.</p>
Charmaine Biggle	532	Improve public footpath signs for walking between Melton and Woodbridge from Melton Fields	Lack of clear signs and way marks inviting people to walk away from road along footpath from Melton Fields to Woodbridge	Provide waymarks and show distance between Melton Fields and Woodbridge as part of exercise and well being campaign
Chris Adelson	110	A1094 This is the only link between Woodbridge/Snape to Knodishall/Leiston.	The traffic is fast and frequent. The undulating road means people take risks when overtaking. Riding a bike feels unsafe and you have to cross both lanes of traffic.	Half a mile of cycleway beside the carriage way.
Chris Adelson	112a	Kessingland to Southwold	To make this journey by bike you have to go inland through Henstead to avoid the A12. A long way out of your way.	Provide a cycle route between Kessingland beach to Benacre village or a cycle route beside the A12 between Kessingland Wildlife Park roundabout to the Benacre turn on the A12.
Chris Nice	145	Felixstowe Road, Martlesham - the entire length	Although the road is supposed to be a cyclists priority route it often feels less safe than a regular road with a single lane marker. I regularly cycle up and down the road to work and have witnessed many near misses,	Either make the road one way and provide much improved cycle lanes and footpaths or install traffic calming, either speed humps or island/priority sections to reduce the speed of traffic and increase its cycle friendliness.

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			particularly as the road has become much busier in the last 15yrs with the development of the industrial estate. Not only cyclists but pedestrians are also at risk when using the road/footpaths.	
Chris Taylor	146	Gibraltar Road / Ipswich Road & Thomsons Lane	FYI - These three lanes have been proposed by Otley as potential 'Green Lanes' under SCC's latest initiative. They make an ideal cycle / walking /horse riding route between Otley, Ashbocking & Swilland avoiding the B1078 / B1077 & B1079 Road triangle.	Extend the 40mph Speed limit on the B1078 from Ashbocking towards Otley encompass the "Swilland" cross roads"....
Chris Taylor	147	Thomson's Lane, Otley.	FYI - Proposed by Otley as a potential Green Lane under the current SCC Initiative	Please support this proposal...
Chris Taylor	148	Ipswich Road, Otley	FYI - Proposed by Otley as a potential Green Lane under the current SCC Initiative	Please support this proposal...
Chris Taylor	149	Suffolk New Rural (Otley) Campus	There is no dedicated footpath from the College to Otley Village. Students are often see wandering across the fields.	As a minimum reinstate the permissive path that used to exist between the college and Otley Bottom. This has been fenced off by the Land owner / user. Consider a further permissive path option connecting the college with the path that runs along the 'gull' and on to the church / village
Chris Taylor	150	B1078, Otley College to Swilland	No footpath / wide verge making it unsafe to walk along	Consider making the 'permissive footpath' that runs along the northern edge of the large field permanent
Chris Taylor	151	Footpath across the A12 from Seckford (Bealings) to Woodbridge	Crossing the A12 on foot / bike is perilous here.	Consider upgrading to full traffic lighted crossing, underpass or bridge. It could be part of a longer useful & safe cycle/walking route to the Bealings, Grundisburgh and beyond...
Chris Taylor	152	A12 end of Seckford Hall Road (Woodbridge side of A12)	Wooden fence at end of Seckford Hall Road where path starts (out to A12) Difficult to negotiate for anyone on a bicycle, pushing a pram or a using mobility scooter	Redesign 'barrier' to allow easier access. This could be part of a bigger scheme to create a cycle / walking route from Woodbridge (south) to the out lying villages.

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Chris Taylor	153	Seckford Hall Road (West of A12 Woodbridge)	Consider incorporating this lane into a designated cycle route from woodbridge to the Bealings and out lying villages.	Some sort of protected status such as Green Lane, no HGV' route, reduced speed limit, currently national speed limit status
Chris Taylor	154	A12 Footpath north of Melton Roundabout, no	The A12 is a busy (& dangerous) road for cyclists...there is no dedicated cycle route out to Bredfield and the outlying north western villages (particularly from the point of view of cyclists travelling from those villages into Woodbridge and having to negotiate the A12 dual carriageway)	Consider upgrading (widening) the existing footpath that runs along the west side of the A12 to a combined foot/cycle path.
Chris Taylor	155	Footpath / cycleway from Farlingaye Coach park to Woods lane	In places the path is not wide enough for cyclists and pedestrians to pass safely.	Consider widening the path to minimum national standards for combined cycle/footpath, in places there appears to be significant grass verge to allow this to be done. Ensure rigorous pruning of path side vegetation.
Chris Taylor	156	Footpath west of A12 bypass, between Seckford Hall Lane & Dobbies (Wyevale) Roundabout	Path can be overgrown at times and is not wide enough to cycle along. Cyclist will come from Grundisburgh via B1079 to Wyevale roundabout and then want to travel south towards 'Melton End' of Woodbridge. This would be a more direct route connecting with the Footpath Crossing just south of Seckford Hall lane	Widen path to cycle / footpath standard
Chris Taylor	157	Chapel Road, Otley	Land allocated for significant housing development within the village. Increases in the number of houses within the village will inevitably increase the amount of motorised traffic within the village, which in turn will make the roads feel less safe for cyclists, parents of children and other road users (Mobility Scooters, Horse riders etc). This will have a detrimental effect on the plan to increase cycling and walking...	1. Install a 'Full sized' roundabout on Chapel Road at the point of this development (where the Primary School, Village Hall and Doctors Surgery are currently located). This would help significantly to reduce 'speeding' traffic along Chapel Road. 2. Reduce the Village 30mph speed limits to 20mph...

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Chris Taylor	159	Ufford Junction with A12 at Woodbridge	Cycling from Ufford to Bredfield and vice versa involves negotiating a big and fast road junction. the old section of road can be used but is not ideal, it is only a basic path on the side of the A12 southbound and on the other side of the A12 at the Ufford Road junction	Create a dedicated cycle/footpath path along the old section of roadway and then extend it down the A12 verge to a proper crossing point opposite the house just north of Ufford Road (meeting the footpath on the west side of the A12 at that junction)
Chris Taylor	160	B1438 Woodbridge to Wickham Market	This is a direct route between the two towns, avoiding the A12 Dual Carriageway. Local traffic uses this road in preference to the A12. With increased housing being seen in Wickham traffic levels will rise hence increasing the vulnerability of cyclists using this route, Including any young persons wishing to cycle to/from school in Woodbridge.	Create a dedicated cycle lane the whole route, improve cycling related signage and reduce speed limits. Make Melton traffic lights a cycle friendly road junction and extend the cycle route up Woods lane to the Melton A12 roundabout (connect with existing cycle route/path). Continue the cycle route into Woodbridge via Melton hill as per other suggestions. Maybe connect it with a riverside foot/cycle path at Wilford Bridge
Chris Taylor	161	Grundisburgh to Woodbridge	Cycling the B1079 between Grundisburgh and Woodbridge is perilous and not suitable for children, inexperienced cyclists and those using mobility scooters.	Consider creating a cycle friendly route using the back lanes, either via Burgh and Hasketon and the existing A12 crossing, or via Great Bealings and Seckford with a new one at Seckford Hall Road. Ensure 30mph speed limits, restriction of HGV's Cars and suitable signage. Connect with existing Woodbridge Cycle/foot paths on East side of A12
Chris Taylor	172	Aldeburgh...et al	Like many of our towns Aldeburgh high street is often full of cars...especially during holiday seasons..making life difficult for pedestrians, cyclists and mobility scooter users.	Promote the idea of regular car free days across the district....where cars are banned from the centre of towns such as Aldeburgh, Woodbridge, Southwold, Framlingham, Halesworth, Beccles, Bungay etc...Maybe one Sunday per month..in support of World Car free day..it works in London why not in Suffolk
Chris Taylor	180	Footpath B1078, Swilland	Footpath comes out on side of B1078 without any protection for walkers, there is no option but to walk on the carriageway of this busy (fast) B road.	Provide some sort of roadside path to the next footpath or at least the swilland crossroads. This path is part of a local network of paths which are regularly used by dog walkers etc. Could form part of a footpath connection between Swilland and Suffolk rural College

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Chris Taylor	181	Bridleways & Footpaths missing from mapping software	The mapping system does not appear to show 'bridleways' and 'footpaths'. Suffolk has many bridleways which make good offroad routes for walkers and cyclists both for leisure and for local use as connections to local services. The marker is tagging the end of bridleway that connects Gosbeck with Pettaugh as an example, this route is often overgrown and rutted by tractors.	Ensure that all bridleways (RUPP's, BOATs' et al) are maintained to a minimum standard of width and firm surface to enable cyclists and less abled walkers to use them safely.
Chris Taylor	182	Footpath East of Otley Bottom	Footpath that runs from driveway of Chalet Bungalow at Otleybottom up hill (NE direction) and across to unnamed road from Church Road is often completely overgrown, muddy and lacking any form of maintenance including repair of broken styles and signage.	Maintain footpath to a higher standard....this path represents a viable walking route from Suffolk Rural College to Otley Village.
Chris Taylor	183	Permissive footpath Suffolk Rural to Otley Bottom	At some point in recent history the permissive footpath along the northside of the field has been withdrawn. This was a useful path connecting the end of public footpath at the College with the start of the one at Otley Bottom giving a safe walking route to Otley Village.	In this case reinstating this path would give a viable walking route to Otley Village. Overall consider promoting the idea of 'Permissive Footpaths' again with our farming community
Chris Taylor	184	B1079, Grundisburgh to Otley	This particular section of the B1079 is a narrow, windy and undulating road and poses a real safety challenge to anyone wishing to walk, mobility Scoot, cycle or ride a horse along it. Its common to see organised 'charity' rides using it as part of their route planning to/from Woodbridge, which further puts cyclists at risk as well as making overtaking difficult for following vehicles.	<ol style="list-style-type: none"> 1. Create one continuous 30mph speed limit along its length, Otley to Woodbridge. 2. Develop an alternative 'cycle' route via the parallel smaller lanes. 3. Encourage organised rides not to use this part of the B1079.
Chris Taylor	185	Ipswich Road Otley	Initial Section of Footpath (Bridleway ?) known as Gipsy Lane is overgrown	Upgrade this path to bridleway status to provide a route from Otley towards Helmingham

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Chris Taylor	202	Connection to local footpath Network at Suffolk Rural College	Suffolk Rural (Otley) College does not have footpath access to Otley Village	A short section of 'permissive footpath' from the B1078, past the 'Motte' and down to the 'Gull' would connect up with the public footpath into Otley Village. This is an example where many people who live in Rural Suffolk but outside villages do not have direct and safe access to the local public footpath network. The 'B Road network' is becoming busier with increased levels of mixed traffic (ie. cars, lorries, farm vehicles) travelling at up to the national speed limit (60mph). There is a genuine feeling among local residents that walking and cycling on these roads 'is simply too dangerous' especially for children and less abled persons. This encourages more use of cars for local journeys eg the school run and popping to the local shop and hence the roads become busier.
Chris Taylor	203	Rendlesham to Woodbridge A1152 Road	Provision of a dedicated cycle lane/path. With the intended major housing development at Rendlesham, it will only serve to increase the amount of motorised traffic travelling to and from Woodbridge via Wilford Bridge. This will actively discourage people from cycling.	There is a huge opportunity for a dedicated cycle/footpath lane to be established along this road to encourage people to cycle to/from Woodbridge rather than use their cars. (Similar maybe to the one already in existence between Leiston and Sizewell) There is plenty of room and it could easily connect with other cycle / walking infrastructure at Woodbridge. As well as use for local journeys such as cycling to school it would also be useful for leisure / tourist cycling connecting Woodbridge with the Rendlesham forest area and the coast
Chris Taylor	204	The Thoroughfare, Woodbridge	This is a narrow ancient street where cars pedestrians and cyclists are not segregated, Despite the no access to vehicles at certain times restriction cars and delivery vehicles are still ignoring this, creating a conflict particularly between pedestrians, mobility scooters and vehicles.	Install 'pop up' barriers/bollards at the Melton End (& retain existing one way system) as per the centre of Cambridge to remove all non essential motorised traffic from this street completely. This would make the whole Thoroughfare a more pleasant place to 'be in' both for local residents, shoppers, and visitors to woodbridge. Deliveries to shops could be made overnight, emergency services could have transponders...it works in Cambridge

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				why not Woodbridge or indeed other East Suffolk towns which have a 'thoroughfare' style main street.
Chris Taylor	205	Hasketon Road/Ransome Road, Woodbridge	Recognising that Farlingaye does not have very good access there is often a conflict between cars & cars and cars & bicycles in this part of Woodbridge, particularly during the morning rush hour / School drop off hour. School hours generally coincide with the morning rush hour creating increased numbers of cars and cycles (young cyclist) in this area of woodbridge including the B1079.	<ol style="list-style-type: none"> 1) Consider some form of dedicated 'cycle' route to/from this area. Allowing children to cycle to school (Woodbridge & Farlingaye) on a car free route. 2) Look at the 'on street parking' around this area, maybe some(or less) more yellow lines. 3) Consider making Hasketon Road and the B1079 roads oneway utilising the A12 roundabouts and a roundabout at the Hasketon/B1079 junction. 4) Relocate Farlingaye to a site with more cycle / pedestrian friendly access....
Chris Taylor	206	Waterhead Lane 'Bridleway' Melton	This is a useful 'off road' cycle route for avoiding the Melton traffic lights area, however in places it is not very cycle/wheeled user friendly, the surface is uneven, rutted and overgrown with trees and bushes	Consider upgrading it to a hard surface bridleway making it suitable for mobility scooter users, people with prams and inexperienced / young cyclists.
Chris Taylor	207	Cycle route Snape to Aldeburgh avoiding A1094	Cycling along the A1094 can be perilous at times and not encouraging for inexperienced/young cyclists	Consider upgrading the Suffolk Coastal Route path from Snape to Aldeburgh to a 'gravel' cycle/footpath path from Snape, through marshes to the western fringe of Aldeburgh, continue 'cycle/footpath' into town centre.
Chris Taylor	214	Woodbridge Riverside path, Elmhurst park to Wilford Bridge Section	There is no dedicated cycle route from Woodbridge Town centre to the Wilford Bridge (linking to beyond eg. Rendlesham, Rock Barracks etc.) Cyclists have to travel along the busy Melton Road to the Melton Traffic lights and then turn right on to the even busier A1152 towards the Wilford bridge, there is no segregated cycling provision making the route unsuitable for young or inexperienced cyclists.	Consider upgrading the Riverside path to a combined cycle/footpath, especially the bit from Elmhurst park to the Wilford bridge, this would miss out the roads completely. There is a primary school at the Melton traffic Lights which could benefit from a dedicated cycle route nearby
Chris Taylor	215	Junction of New Road (Saddlemakers Lane) with the A12 North of Melton Roundabout	Crossing the A12 by Bike at this junction to access the road to Bredfield & Boulge is perilous, especially at weekends when the A12 is busy with 'Holiday' traffic. This junction is on	Some sort of formal cycle crossing maybe just south of the junction to allow cyclists to cross the A12 to the footpath on the west side of the A12. Upgrade this footpath to a combined cycle/footpath to remove the

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			a convenient quiet route for cyclists from Melton (& Woodbridge) to Bredfield, Debach, Charsfield & beyond)	need for cyclist to use the 'slip lane' off the A12 to access the road to Bredfield.
Chris Taylor	216	Bridleway 'crosses' A12	There is a bridleway at this point that 'crosses' the A12, there is no provision for Walkers, Cyclists, Horse Riders to cross the A12 safely and continue along its route toward/from Bredfield. There is no path on the east side of the A12 to allow users to travel either north or south. The only option is to cross the A12 to the path on the other side of the road.	Some sort of improved road markings/crossing point/signage and widening of paths
Chris Taylor	217	'Bridleway end of Playford Lane to Playford & Little Bealings	The surface of this bridleway is poor, rutted and uneven in places making it difficult to cycle on or use a mobility scooter	Consider upgrading the surface for the full length of its course. This would provide a very viable and usable cycle path directly to Ipswich from the Playford / Bealings area.
Chris Taylor	218	Westerfield Business Centre / Station	Possible site for an Ipswich northern 'Park & Cycle' car park. There is nowhere to park when using Westerfield Station.	Given the emerging development north of Ipswich this would make a good spot for a park,ride and cycle carpark similar to those seen around the fringes of Cambridge. This would enable those of us travelling into Ipswich from the North (aka East Suffolk District) to park up and then either use the train to go northward towards lowestoft or cycle(or walk) or bus the short distance into the middle of Ipswich.
Chris Taylor	224	Footpath Brock Lane Woodridge to Great Bealings	Path is unsurfaced and difficult to walk / cycle on, especially for the less mobile and buggies	This path could be upgraded to a surfaced cycle/footpath connecting Bealings and Grundisburgh with Woodbridge without having to negotiate crossing the A12 dual Carraigeway. There is already an existing Pedestrian tunnel under the A12 Martlesham bypass for this footpath. The route could easily connect with cycle/footpaths to Woodbridge and Martlesham Heath Industrial area / supermarkets.
Chris Taylor	225	Bridleway connecting 'Green Lane; with 'Tuddenham Lane'	This bridleway is cyclable by someone with a mountain bike, however the surface is not good	Upgrade the surface to allow the bridleway to be used by young and inexperienced cyclists, it provides a route from Tuddenham to NE Ipswich avoiding the ever

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			enough for use by 'normal cyclists' being rutted, muddy and stoney in places	increasing traffic on the C road into Ipswich. It could be particularly useful for children accessing Northgate High School and Rushmere Primary Schools by bike
Chris Taylor	226	A12 / B1119 Junction Saxmundham	Crossing the A12 by bicycle or on foot at this junction is difficult /dangerous for any cyclist or pedestrian regardless of age and experience, there is no segregated provision. The B1119 Rendham to Sax road has effectively been cut in half by the A12.	Provide a dedicated pedestrian/cyclist crossing point with separate foot/cycle path linking the Rendham side of the A12 with the Saxmundham side. Enabling anyone from the Rendham direction to safely cycle/walk to Saxmundham.
Chris Taylor	227	A12 Saxmundham, Carlton Lane junction	There is a cycle path across the A12 at this junction however it is not very wide and not very well laid out, it is just a path really and not suitable for cycles / mobility scooters. It is not that visible to traffic on the A12. Again crossing the A12 is perilous for experienced adult riders let alone young people wishing to cycle into Sax from the villages.	Upgrade the path, make it wider and more pronounced, improve the A12 road markings and signage to show that there is a 'cycle crossing' at this junction.
Chris Taylor	229	Wickham Market, new housing developments	Example of where significant housing development has been, and will be allowed without adequate local cycling infrastructure ie a cycle path to enable young people and their parents to cycle to the local primary school safely or indeed the village centre. Parents will always take the easy option when it comes to the daily school run and without safe infrastructure it will be to drive to school or pop down the local shops.	Create a safe cycle route either alongside the B1438 or along Chapel Lane, with a 20mph limit in the middle of Wickham, make the local streets limited to 20mph to encourage more of a sense of a nice neighbourhood where children can roam the streets free and safely.
Chris Taylor	230	Junction of 'The Street' Darsham, with A12	When cycling from Darsham village up to this A12 junction its not obvious that there is a short cycle path on the righthand pavement. This is effectively on the wrongside of the road and as a cyclist you have to cross the opposite carriageway of the 'Street' at its junction with the A12 to get to it. Which is putting yourself at	Extend the 'cyclepath' around the corner of the verge into 'The Street', make it a decent width and not just footpath sized. Do a similar thing to the one at the Willow Marsh Lane Junction opposite. Some A12 roadside bollards and improved signage to show a 'cycle crossing' would make it 'more obvious' to A12 drivers.

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			conflict with vehicles turning off the A12 into 'The Street'. Its a similiar situation at the Willow Marsh Lane Junction opposite.	
Chris Taylor	231	A1214 Kesgrave, Junction with Dr. Watsons Lane to Playford.	Having negotiated the Bell Lane traffic Lights cyclists then have to make an unprotected right turn across traffic on this busy A road into Dr. Watsons lane when travelling to Playford and beyond.	Consider creating a short piece of cyclepath using the existing footpath' from Bell Lane at the Traffic lights, along the side of the A1214 to opposite Dr. Watsons Lane.
Chris Taylor	232	B1078 & Swilland Crossroads	Turning right off the B1078 for cyclists is perilous, particularly during the rushhour periods when the B1078 is busy with streams of vehicles travelling at the speed limit which at this point is 60mph. Its noticable that there is a tendancy amongst some motorists to overtake at speed along this stretch into the face of oncoming traffic which if you are a cyclist or walker is actually terrifying....Traffic does not 'naturally give way' to anyone attempting to walk along the road.	As a minimum the Ashbocking 40mph limit should be extended to the College 40mph to create one continuous 40mph limit
Chris Taylor	233	Chapel Road, Otley	The School, Village hall and Doctors surgeries are all co-located at this point on Chapel Road. These are magnets for cars particularly at drop off times, this creates an area of local congestion and conflict with pedestrains particularly those with children trying to cross the road or indeed cycle to the school. Through traffic travelling at speed compounds the safety risk as the village hall carpark (which is used as the school drop off area) exit/entrance is on a blind bend.	Given the potential of further significant housing development in this area it would make sense to create a roundabout at this point giving safer access to the Hall carpark and Doctors surgery and also serve to calm the through traffic on Chapel road, a carpark within the development would also ease the congestion and provide some public off street parking within the village.
Chris Taylor	234	Sandy Lane, Woodbridge	Sunday 8th November I found Sandy Lane closed to vehicles and barriered off just north of the nursery entrance due to a burst water main...It was wonderful..there were a number	This shows that by making it a dead end with some bollards at this location a well known rat run can be turned into a pleasant place for people to cycle and walk

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			of people walking and cycling along it in complete safety not a car in sight. I was following NCN 1 from Charsfield to Ipswich Waterfront on my bike.	along in complete safety. Access to the businesses along it would not be affected.
Chris Taylor	235	NCN 1 Junction of Old Barrack Road with the B1438	When approaching this junction from Old Barrack Road cyclists have to use the road junction itself to cross into California. This can be problematical if the B1438 is busy and not suitable for the young and inexperienced rider.	There is a central refuge for the footpath adjacent to the pub. This footpath could be widened into a combined cycle/footpath separate from the actual junction itself, so that there is an obvious route across the road for cyclists/pedestrians into 'California'. Particularly as this junction forms part of NCN 1 and the cycle route to Martlesham
Chris Taylor	236	Cycle path Kesgrave Grange Lane to Bell Lane	An amazing Cycle/footpath that runs from Grange lane to Bell Lane completely traffic free, flat and well surfaced with plenty of space for both Walkers and Cyclists. An exemplar of how combined walking and cycling provision should be in modern housing developments	...Continue the off road segregated cycle path idea towards Ipswich across Rushmere heath. The current Ipswich route follows roads and requires some mixing with cars and buses and a very hilly bit near Brendan Drive.
Chris Taylor	237	Bixley Drive / Gwendoline Road, Ipswich	No obvious signage to show that Gwendoline Drive & Chatsworth Drive is actually a cycle route to Ipswich & NCN 1	Some better cycle signage is all that is required.
Chris Taylor	246	Main Road Martlesham	No cycle lane toward Woodbridge. There appears to be a cycle lane on the uphill side of this road towards Martlesham but not on the downhill, Martlesham to Woodbridge Side	Provide a segregated lane to allow safe cycling in both directions.
Chris Taylor	247	Future Rendlesham / Bentwaters Development	Lack of Public Right of Way's connecting 'Rendlesham' to 'Rendlesham Forest', Wantisden, Butley and the coast.	1. Consider running a new cycle/footpath across Bentwaters Airfield to connect Rendlesham Housing estates with Wantisden Corner road. Provides an off road walking route and removes the need for cyclists to use the local 'B roads'. 2. Consider upgrading the 'path' that runs across the eastern end of the runway towards Friday Street.
Chris Taylor	248	Road Bridge Kirton Road to Old Kirton Road	This bridge and its approaches are not cycle friendly or indeed for anyone using a mobility	Upgrade paths on both sides & bridge to a more cycle friendly standard

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			scooter or pushing a pram. It is on a useful back route from Kirton to Felixstowe.	
Chris Taylor	249	Levington Lane & crossing the A14 at this point	There is a public right of way that crosses the A14 (Levington Lane) at this point via a gap in the central reservation. It is possible to get across without being killed but you have to be quick.... The A14 verges are often overgrown....	Tidy verges so that there is better visibility of the crossing.
Chris Taylor	250	Levington, Felixstowe Road.	Crossing the A14 & travel between the villages on either side.	There is a private farm road and 'Tunnel' under the A14 at this point which could be upgraded to a PROW / bridleway between Felixstowe road and Brightwell Road to allow cyclists and pedestrians to cross the A14 safely and travel between the villages on either side of the A14
Chris Taylor	251	A1156 Nacton to Warren Heath Ipswich	Limited cycle path from Seven Hills / Nacton into Ipswich	Consider providing a full cycle/footpath all the way from Nacton (even Seven Hills Junction) towards Warren Heath (Past the Show Ground)
Chris Taylor	252	Buckleham to Ipswich, walking / cycling	Negotiating the Seven Hills Road Junction by bike or on foot	<ol style="list-style-type: none"> 1. Make Buckleham Road a cycle friendly route into Ipswich 2. Consider upgrading the Bridleway (just West of the Seven Hills A14 junction) that connects Buckleham Road with Felixstowe Road to hard surfaced allowing direct access to Felixstowe Road, Warren Heath and Ransomes Europark avoiding the Seven hills A14 Junction.
Chris Taylor	272	Seven Hills Road A14 Junction	There are no footpaths or designated cycle lanes at this junction and on the A1156 into Ipswich...This precludes cycling and walking from (& to) Buckleham, Kirton, Waldringfield and beyond into SE Ipswich and the Ransomes Euro park areaAccess to the newly built crematorium is only possible by car....As an experienced cyclist it is possible to negotiate this junction on the carriageway but it is not safe due to the speed of the traffic.	Provide some sort of path/cycle path as per the Nacton and Claydon Junctions of the A14 connecting with the existing Ipswich to Felixstowe cycle route
Chris Taylor	273	Woodbridge Maidensgrave area	No dedicated cycle route from the thoroughfare to this part of Woodbridge for	NCN 1 runs along Old Barrack Road from the Thoroughfare...consider making this a local cycle route

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			local cyclists. The B1438 is not a cycle friendly road, especially when turning right into Warren Hill Road.	with 20mph limit, proper segregation and signage to encourage local cyclists, rather than just those following the NCN, to use it as a safe route to and from the centre of Woodbridge (encompassing Kyson Primary School).
Chris Taylor	274	Woodbridge Station	Lack of secure undercover cycle storage....useful for anyone commuting to work or making longer journeys the facility to leave your bike for extended periods of time in a safe undercover facility like the one at Ipswich Station. Rather than just locking it to a 'Sheffield Stand' out in the open, not covered by CCTV and hoping for the best.	Provide a storage facility similar to that at Ipswich Platform 1
Chris Taylor	278	Brightwell lakes development Martlesham	Very little to indicate how this development will connect to the local cycling/walking infrastructure, especially on the west (Ipswich) side of the A12...how will a cyclist ride to Ipswich? How will a cyclist ride north to the retail park and beyond to Woodbridge. How will cyclist be protected when cycling along 'Ipswich Road' Brightwell	<ol style="list-style-type: none"> 1.) provide an independent cycle / pedestrian bridge over the A12 connecting with Lancaster Drive. 2.) provide some form of safe route to NCN 1 connection at the Gloster Road / Betts Avenue junction and upgrade (widen) the current pedestrian bridge across the A12. 3.) Ensure that all roads within the development have combined cycle / footpaths such as seen at Stowmarket Mortimer Road, such that a young child does not have to cycle on a road to get to school or the local park / shops.
Chris Taylor	279	Land allocated for Housing 'Humber Doucy Lane & Rushmere'	Land allocated for housing will increase the number of vehicles on the local roads particularly 'Tuddenham Road' & 'Humber Doucy Lane', this already a cut through road, but also popular with cyclists travelling out of Ipswich towards Tuddenham and the villages beyond. There is limited pavement and no cycle lane/protection along its route.	Humber Doucy lane could be widened to incorporate a dedicated footpath / cycle track connecting the development with Ipswich's cycle infrastructure. There is an opportunity to upgrade the bridleway at the end of Tuddenham lane to provide a safe cycling and walking route to Tuddenham avoiding 'Tuddenham Main Road' which is a commuter route into Ipswich for cars.
Chris Taylor	326	New Housing development, Woods Lane Woodbridge	Example of where significant new housing has been allowed without provision for safe cycling to the local shops, centre of Woodbridge and the local primary school. The housing is disconnected from Woodbridge by the A12 &	<ol style="list-style-type: none"> 1). Upgrade the footpath along Bredfield Road into Woodbridge to cycle/footpath standard. 2.) Create a cycle route down Woods lane to the Melton Traffic lights to connect with Melton Road

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			busy Woods lane, necessitating car ownership to access local services.	
Chris Taylor	347	Bridleway A12 to Dobbs lane	Surface not suitable for cyclists or mobility scooter users. Possible route for a cycle/footpath from new Brightwell development.	This bridleway could be upgraded to give a cycle/footpath route from the new 'Brightwell' development south of BT towards the centre of Ipswich. Connecting with the one that runs south of Cedarwood Primary School and mentioned by others as being upgradable to allow cycling, mobility scooters and buggies.
Chris Taylor	348	Ribbans Park Development, Ipswich	Exemplar & Award winning example of a new housing development with a Modeshift STARS "Residential Travel Plan" https://www.modeshiftstars.org/first-residential-development-achieves-national-stars-accreditation/	This requirement should be included with all new housing developments within Suffolk.
Chris Taylor	349	A12 Loer Hacheston / Wickham Mark Roundabout	Given the likely hood of this being a SXC park and ride facility with increased road traffic, there will be increased risk for local cyclists using the roads, roundabout and crossing the A12	Provide suitable segregated cycle/footpaths to allow cyclists/pedestrians to transit from the B1116 to the B1078 and vice versa.
Chris Taylor	372	B1078 junction with Charity Lane, Otley	B1078 Traffic turning right into Charity Lane often cuts across the junction ignoring the road markings which if you're a cyclist or car waiting to turn right out of it is quite disconcerting. The road markings have been rubbed away. This is typical of many junctions along this road where the mouth of a minor road is narrow. Vehicle drivers naturally cut the corner, rather than making the full 90 degree manoeuvre.	Improved markings on the B1078 & at the junction itself on Charity Lane.
Chris Taylor	408	Darsham Station	Lack of connecting cycle/footpath to/from Darsham station towards Westleton, towards Yoxford	With land allocated for development why not include a dedicated cycle/foot path connecting Darsham Station with Westleton Road through this development and Darsham Station to Yoxford by widening the A12 footpath to cycle/footpath specification

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Chris Taylor	410	Kesgrave School	Doesn't appear to be a safe route for children and other cyclists to get to Dr. Watsons Lane (to Playford) and Hall Road (to Bealings) from the Northern (School) side of the road or indeed the existing cycle path on the South side. Hence limiting the opportunity for children and parents from the villages to cycle to the school in safety.	<ol style="list-style-type: none"> 1). Provide a proper crossing and short section of cycle/footpath on the northside of the road where the central refuge is on the A1214 at Hall Road. 2). Extend the existing cycle path beyond the Bell Lane traffic lights past the Doctor Watsons lane junction and provide a separate crossing integrated with the existing traffic lights. 3) This would also help all cyclists wishing to travel from the Kesgrave development north into the villages and beyond.
Chris Taylor	419	Cycle path A1214 Kesgrave Road	A typical example of a 'stop start' cycle path where motor vehicles are given priority at each minor road junction and property driveway entrance, hence impeding the steady progress of cyclists and pedestrians	<p>Consider giving cyclists & pedestrians the right of way at minor junctions by removing the 'giveaway' from the cyclepath and moving the road 'giveaway' lines back from the junction to before where the cycle path crosses it. Also where a cyclepath crosses the front of a property entrance put the giveaway lines across the entrance to ensure that anyone leaving the property gives way to the cyclist, rather than relying on the cyclist having to dodge vehicles sticking their nose out onto the cycle path. This is common practice in countries where cyclists are given priority over vehicles, rather than in the uk where vehicles are given priority over cyclists (and pedestrians, mobility scooter users etc).</p>
Chris Taylor	457	Proposed Bentwaters park development area.	<p>Pedestrians walking / cycling across the A1152 from Rendlesham to Bentwaters.</p> <p>Its important that these two developments are 'connected' and not divided in two by the A1152. Crossing an A road on foot is always 'risky' and not safe for children walking to school or trying to access the local facilities within Rendlesham</p>	<ol style="list-style-type: none"> 1) Upgrade the paths at the roundabout to cycle paths or even create a 'dutch style' roundabout such as the one in Cambridge where vehicles are required to give way to Cyclists / Pedestrians. 2) Provide a second Pedestrian/cycle crossing point at the end of the existing lane near to the Rendlesham Day Nursery.
Chris Taylor	458	Brendan Drive	NCN 1 & the cycle route into Ipswich is via an estate road at this point and sections are cluttered with parked cars, and a couple of	It would make sense to upgrade the footpath that runs across Rushmere Common to Heath Road to a Cycle/footpath there by giving cyclists a section of the

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			short hilly sections where less abled and older riders have to get off and push.	route that is traffic free and relatively flat. It would also connect in the other direction with the bridle way that runs east towards Bell lane and beyond... giving a continuous traffic free cycle route from the Hospital to almost the Brightwell Development Area.
Chris Taylor	459	A12 north of Wickham Market	As already commented regarding walking, there is not a safe way of cycling from the Wickham Market roundabout to Marlesford and beyond.	By providing a short section of Cycle/footpath this will allow cyclists direct access to Bell lane which in turn leads towards villages around Parham Airfield, which is a designated industrial development area. This would allow somebody to cycle to work from Wickham to Parham without using the B1116 which is a busy route to/from Framlingham. Children from the villages could also safely cycle to school or access shops in Wickham.
Chris Taylor	489	Riduna Park / East Suffolk Council Offices / Melton Train Station	There is no sign of any dedicated cycling infrastructure connecting East Suffolk Councils Offices & Riduna Park or Melton Train Station to central Woodbridge and other residential areas within the town. Anyone wishing to cycle to & from must do via a busy A road.	Widen the footpaths along Wilford Bridge Road and a cycle lane into woodbridge
Chris Taylor	491	Proposed 80 house development in Grundisburgh	A proposed large housing development accessed only via two minor roads with no direct access to the 'B' road network. Increased motorised traffic during construction and when inhabited will increase the risk factor for cyclists, pedestrians and other vulnerable road users trying to negotiate Park, Chapel, Lower & Ipswich Roads all of which have limited if any pavements. This will actively discourage walking and cycling in the area, particularly with regard to those less abled...	Motorised traffic on these local roads need to be forcefully restricted to allow more vulnerable road users to safely walk, cycle, scoot or trot along them to/from local amenities The developer should be instructed to provide suitable cycle/footpaths along the roadside boundaries of the development and off site connecting with the School and local amenities. There is considerable local opposition to this development as per the comments on the current planning application.
Chris Taylor	504	A1152 & Wilford Bridge	Lack of a cycle path, Melton traffic lights to Bromeswell Quiet lanes...	Having cycled along the footpaths on this route, there does seem to be enough room on the verge to widen the existing footpaths to create a cycle/footpath pretty much

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				all the way along, past the station and across the bridge and round to the Bromeswell 'Quiet lane'
Chris Taylor	505	Riduna Park. Woodbridge	Example of new industrial development with... No obvious cycle parking facilities for... 1) Members of the Public Visiting East Suffolk Council Offices 2) Employees cycling to work at each unit 2) Cyclists wishing to use units providing food and drink such as Honey & Harveys.	1) Encourage developers to give up one car parking space per unit as a dedicated cycle parking space with stands or provide secure storage as per the Councils own staff facility. 2) Encourage developers to give up unit space to a dedicated indoor bike storage space including showers and lockers. This could be a shared facility for all on the park 3) A few sheffield stands outside the front door of the Council Offices would be useful and look good to passers by. Include a dropped kerb at the roadside end of the main entrance path so that disabled users / buggies can easily access it from the Melton direction.
Chris Taylor	506	Melton	Well done to Melton Parish Council for converting this short length of footpath into a cycle/footpath. It might win the prize for the shortest cyclepath in East Suffolk but it is an example of where a small 'parish council' have been able to upgrade the designation of a footpath to a cyclepath.	East Suffolk DC to proactively support and encourage Parish Councils to upgrade footpaths to foot/cycle paths.
Chris Taylor	507	Sutton Heath	Walking & Cycling along 'Heath Road'..As already noted this is a fast and straight road which makes it unsafe to walk or cycle along	Create a path parallel to the road but on the 'heath' side of the fence line where possible. The path could be a simple woodland style path suitable for walkers or those using mountain bikes. The verges are wide in places as well although it might mean some crossing of the road in places, but thats safer than walking down the road as I saw someone doing the other day.
Chris Taylor	633	Woods Lane, Junction with A12	With increasing traffic on Woods Lane trying to cross the road at this point is difficult / dangerous at times especially for the less abled. The footpath crosses the road at this point via	Provide a proper pedestrian and cycle crossing at this point, continue the cycle path up the A12 to where it then crosses it.

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			gaps in the verge, it is not highlighted as a crossing point to drivers. The footpath is also designated for cycles on the Ipswich side of the road, but not the north bound side.	
Chris Taylor	634	A12 Approach to Melton Junction	Crossing the A12 using the path at this point is difficult / dangerous and involves crossing three lanes of fast moving traffic. The lack of an adequate crossing point here and on the A1152 entry effectively cuts the paths in half and deters walkers & cyclists from using the A12 north bound path towards Bredfield (this path could be a ready made cycle route towards Bredfield and Debach. It would allow children to cycle from the villages to school at Farlingaye & in Woodbridge.	Provide a suitable crossing on the A12 at this point & A1152 Entry Connect the A12 north going path with the A12 south side cycle route to Farlingaye. Upgrade paths to Cycle / footpaths.
Christian Newsome	418	Ness Point	No cycle storage or racks whatsoever at Ness Point for people to lock up there bike! Britains most Easterly Point	More bike racks
Christian Pawsey	607	General	The issue is that most if not all the few existing cycle paths are marked poorly. There is no right of way marked for pedestrians or cyclists on the existing paths (ie A12 path or Martlesham to Ipswich). Most byways and other footpaths positively discriminate AGAINST cyclists, with for example, much protest about mostly harmless cycling on the river wall and bars to prevent cycles passing at most town footpath entrances and exits.	Campaigns to promote a cycle 'economy' around new cycle routes, recognising that every cyclist reduces congestion for road users, reduces pollution, increases the mental and physical health of the cyclists themselves, which in turn saves more money for NHS and authorities. Promotion of positive recognition of cyclists who deliberately commute to better their health and lower local pollution, (combatting climate emergency) vs the negative/destructive effect of driving short distances to school and work. School promotion of cycling within a certain distance instead of driving, especially where onward commute to work is not a consideration. Enforce existing traffic legislation designed to promote

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				the safety of cyclists. (ie speed limits, distances for passing cyclists, parking on cycle paths).
Christian Pawsey	608	General	Nearly all cycle paths stop abruptly at some point with direction onto a busy road with poor direction and often no further option but to stay on the road. Even the poorly marked cycle paths on main roads are usually blocked at some point by parked cars. Hurried commuters often have little time for slower cyclists who are often viewed as a non-road-fee-paying nuisance. Walkers are well catered for in most areas but can view the bike as an unwelcome nuisance also.	Support this campaign by creating and investing in a considered and continuous infrastructure of cycle paths and facilities, such as marking paths with cycle and pedestrian areas, widening existing paths, traffic reduction schemes citing the reason for promoting cycling. (such as the welcome sign for the Thoroughfare which says 'except cycles'). These paths should connect outlying villages as well as provide cross-town routes, cycling off road wherever possible. (ie routes from Bromeswell to Woodbridge using part of the river wall, which is wide enough to accommodate cycles and pedestrians. Rendlesham to Woodbridge, Bredfield to Woodbridge, Hasketon to Woodbridge, and so on).
Christian Pawsey	609	General	Encourage a cycle lock or loop fixed to walls outside certain shops, where appropriate. Invest in wider recreational cycle route creation to enhance the area for local cyclists, pedestrians and (staycation) tourism. (ie river wall route from Wilford Bridge to Felixstowe Ferry).	Further interconnection between towns and villages of the area, including tackling awkward areas where there is seemingly less space for cycle paths, such as from the outskirts of Woodbridge towards Martlesham where routes into Ipswich are found.
Claire Cook	8	Footpath between Elmdale Drive and Wannock Close	Metal railings obstructing the footpath, slowing down cyclists and making it difficult for people with mobility issues to get through.	Remove railings. These are not required as they are approximately 10 metres from either Elmdale Drive and Wannock Close so do not help with safety. Also, there are many other similar footpaths in the area without these.
Claire McBurney	420	Station Road Melton	This is part of the main pedestrian route through the village. In places, the pavement is less than 1m wide. The road is used on a daily basis by HGVs and agricultural vehicles. This is not safe and is very polluting.	Work with other authorities e.g. Suffolk County Council to introduce weight/width restrictions. Work with satnav providers to direct heavy vehicles to more suitable routes.

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Claire McBurney	622	The Street, Melton	This is a historic route. The road is narrow and so are the pavements. Many of the buildings are hard against the pavement. At peak times, the vehicles are nose to tail. Pedestrians, including families on their way to school, have to run the gauntlet between the vehicles and the buildings, wreathed in exhaust fumes.	Measure the air pollution in real time to better understand the scale of the problem. Make The Street a no idling zone.
Clare Astbury	29	Main road kesgrave	Cycle track not fit for purpose, especially around Windrush Road where potholes on road are dangerous. Very uneven and old cycle track surface, many cyclists forced to use Road.	Resurface section from police station to Kesgrave fisheries.
Cllr J Jeans on behalf of Southwold Town Council	508	Pier AVenue and Station Road Junction -- this roundabout has heavy traffic in all directions and there is no dedicated crossing area which is safe for pedestrians	A safe crossing point. This will become even more important as the west side of Station Road and Might's Road are developed with new housing, community facilities, and employment space.	
Cllr James Mallinder	20	Wilford Bridge round about up to entrance of Sutton Hoo	The footpath is getting smaller as the hillside is slowly creeping over on to the path Not only that but excessive amount of weeds growing on the curb The main issue - the footpath needs widening and allowing cyclists - many want to cycle to woodbridge from the peninsula but dont due to this bottle neck on the hill and the roundabout is dreadful and is desperate for an up grade	cut back into the side of the 'hill' to widen the footpath split the footpath with markings to allow cyclists and people and then make clear signage from the railway station to sutton hoo of a cycle path Engage with National trust to see if they can help - we need a better sustainable travel option to a world heritage site
Colin Hedgley	135	C324 (The road between the B1079 and Butts Road Playford).	The part of the C324 between Boot Street and Tuddenham is part of the National Cycle route system Stowmarket to Woodbridge. During the week this road is a Rat-Run between	"Cyclists in Road" signs on bends as part of the road is single lane.

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			Woodbridge and Ipswich and is very busy and at times highly dangerous for cyclists. Weekends see a great number of cyclists on this route, although still dangerous it is a lot more cycle friendly. Some signs along the route stating "Cyclists in Road" especially on bends would be very helpful.	
Councillor Margaret Morris on behalf of Suffolk Coastal Disability Forum	641	Cycle paths and footpaths throughout East Suffolk	Concerns about the surface and width of footpaths and cycle paths.	Cycle paths and foot paths should be at least 2 metres wide to allow for two wheelchairs to pass. The surface should be tarmac so that all people can walk / use wheelchairs easily. They should be reasonably level, with no hills or steps, or gates. They should be regularly maintained.
Cycling UK	777	Bascule Bridge, Lowestoft	The Bascule bridge is the biggest obstacle to more cycling due to being perceived as dangerous. It is ironically unfortunate because it is what links south and central Lowestoft, thus the town's main facilities. I am unconvinced the cycle/pedestrian bridge would be the best way to resolve it. I refer you to the enclosed copy of Cycling UK's (CUK) Hierarchy of Measures for Cycling Facilities. CUK's stance is that the priority should be to make the road environment comfortable for cycling.	The road over the Bascule bridge could be made so if there is the political will for radical interventions. Parts of the carriageway could be exclusively for cyclists by 'blocking off' with 'armadillos'/planters/bollards. It might require some realignments and widenings, however, it would be extremely disappointing if it was argued something to encourage active and sustainable travel cannot be afforded because of the amount spent on a facility for motor vehicles, which are unsustainable. Cyclists are currently allowed to share the footway over the Bascule bridge and then along Station Square. I think the Hierarchy of Measures in effect explains why CUK does not regard that as satisfactory. Indeed, at the point where the footway turns sharp left outside Lowestoft station toward Denmark Road, it surely goes completely against the point about sufficient sightlines.
Cycling UK	778	Pier Terrace, Lowestoft	Concerning Pier Terrace, it is more problematic. However, there is an off-road facility and, I think, as access is traffic lights controlled, many cyclists find the short on-road distance	In conclusion, the most pertinent point is that a facility actually on the Bascule bridge and improvements to Station Square would enable cyclists to use the absolute direct route linking the main parts of Lowestoft, albeit

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			tolerable. That said, I do not think it should be a reason not to have brain storming discussions on possible improvements for cycling along Pier Terrace, particularly as the off-road facility is clumsy.	with possible very minimal deviation due to realignments, It could make cycling quicker and less stressful than driving for short journeys
Cycling UK	779	Lowestoft	The third crossing will mean even less excuse for not having more 20 mph speed limits. There is plentiful evidence they create more cycling. I particularly argue Yarmouth Road would be a good candidate. Come the third crossing, I guess its classification could change. There is arguably a precedent in that in south Lowestoft stretches of Marine Parade/Wellington Esplanade/Kirkley Cliff Road, which are the A12 are 20 mph.	I realise that the A47 is the responsibility of Highways England. Frankly, the cycling provision is a shambles. For a lot of the way it is shared with pedestrians on PARTICULARLY narrow footways, passing bus stops, driveways and crossing roads without priority, i.e. it goes completely against CUK's guidance. There are points where the shared path stops so cyclists have to continuously temporarily rejoin the carriageway. That can increase danger as drivers do not expect it. Ironically, the one reasonable stretch of the cycle path, which is segregated from the footway and runs between Sussex Rd and Hollingsworth Rd, passing Ormiston Academy, gets parking on it at school run times. In my opinion, as the Northern Spine Road is part of a route to bypass Lowestoft centre to reduce congestion, there is no reason why Yarmouth Rd should not already be 20mph to the roundabout with the Northern Spine Road/Corton Long Lane/Blundeston Road. It could encourage compliance with using the bypass route.
Cycling UK	780	East Suffolk	CUK's position is that priority should be to consider whether the road environment can be made comfortable for cycling and that sharing with pedestrians should be the last resort. The latest guidance from the Department for Transport is in agreement stating improved facilities for pedestrians and cyclists should be separated and road-narrowing to enable correct width cycle lanes should be considered which is in effect saying making roads	Considering the nature of many of Lowestoft's busier roads, I understand why on-road facilities would be difficult. I hope there will be proper consultation (CUK would probably accept off-road facilities are more appropriate anyway). Many cyclists will say they want more cycle paths and they don't mind sharing with pedestrians as anything is better than being on road. It is impossible for there to be off-road facilities everywhere. The more cyclists on the roads the safer on-road cycling is, especially if there are 20mph limits. Routes need to be

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			comfortable for cycling should be the first consideration.	as direct as possible, perhaps even giving cycling time-saving, advantages over driving. Many off-road routes involve time-consuming waits at toucan crossings etc. There are pedestrians who dislike sharing with cyclists, so even considerate riders on shared facilities experience hostility. Having to slow for pedestrians, and possibly dismount and walk, works against cycling being quicker than driving for short journeys.
Cycling UK	781	Saltwater Way, Oulton Broad	Lowestoft's off-road facilities are a 'mixed bag.' A number of the more recent cycle-paths are quite good but some of the older ones are extremely bad and poorly thought through and, in some cases, not necessary. The legal position is that pedestrians can walk on cycle-paths but cyclists cannot ride on footways. However, it is reasonable to expect both to respect each others space.	Considering all the complaints about cyclists on footways, I feel peeved when I see far more pedestrians walking on cycle-paths alongside footways than vice-versa. That said, on a number of them, the pedestrian part is so narrow one could not reasonably expect them to not drift onto the cycle path. That is particularly the case for the cycle path/footway alongside Saltwater Way, Oulton Broad, continuing as the underpass. Indeed, at points, particularly close to the junction with Victoria Road, there is greenery that protrudes onto the footway section. The facility also changes from segregated to shared use and back to segregated, which is confusing. There is also the point the underpass is prone to flooding.
Cycling UK	782	Oulton Broad	There are good and bad things about the short stretch of cycle path running from the traffic lights just south of the Bridge Road/Saltwater Way/Victoria Road roundabout, past the fish and chip shop and former Spar store into Oulton Broad centre. The good point is that it gives cyclists a geographical advantage to/from the centre and links, via the toucan crossing, with the shared facility to/from the railway bridge.	Ironically, ideally it should be shorter, avoiding passing the fish and chip shop and former Spar. I cannot exaggerate how many more pedestrians walk on the cycle path instead of the footway, despite, in this case, being reasonably wide. Also, cars regularly park on it and when the Spar was open, it included lorries. The nature of the road means there would be no harm in cyclists having to ride it a little further, especially as a 20 mph speed limit would be easily enforceable.
Cycling UK	783	Lowestoft	Concerning cycle lanes, i.e. white lines on roads, many of them in Lowestoft are not the	For cycle feeder lanes to advanced stop line arrangements, a minimum width of 1.2m may be

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			stipulated minimum width of 1.5 metres. Local Transport Note (LTN) 2/08, paragraph 7.4.2 states: "Cycle lanes should be 2 metres wide on busy roads, or where traffic is travelling over 40 mph. A minimum width of 1.5 metres may be generally acceptable on roads with a 30 mph limit.	acceptable. Cycle lanes less than 1.2 metres wide cannot easily accommodate tricycles or child carrying cycle trailers wholly within the lane." A pertinent point is that the Highway Code advises cyclists to ride 0.5 metres away from the kerb. Cycle lanes less than 1.5 metres can, ironically, increase cycling danger by misleading drivers into thinking those are safe distances to overtake cyclists. LTN 2/08 was withdrawn on 20 July because it has been superseded by LTN 1/20. However, paragraph 6.4.2 indicates 1.5 metres is now only acceptable for one-way roads.
Cycling UK	784	Bascule Bridge	The railway bridge is a close second to the bascule bridge in being the location in Lowestoft that most discourages cycling. As you know, there are "no cycling" signs on the footbridge but the vast majority of cyclists ride. I am uncomfortable about it.	It is inconsistent that on both sides of the bridge there is a shared cycle route and that cyclists have, strictly speaking, to dismount and walk, even though it is only a short distance. By that, I mean I accept the footbridge is narrow so a separate cycle bridge should be a priority.
Cycling UK	785	Nicholas Everitt Park	Considering the size of the Nicholas Everitt Park car park, I would think there is room for some quality covered cycle parking or, if not, in the park itself.	
Cycling UK	786	Horn Hill and Belvedere Road to/from Pier Terrace	I would like discussion on the cycle paths along Horn Hill and Belvedere Road to/from Pier Terrace. They were originally segregated but are now shared. The different coloured surfacing indicates they are segregated and although the signs indicate they are shared, it is confusing. I am not clear why they were changed. Possibly it relates to the fact they pass bus stops, which are supposed to be bypassed.	I am aware there was a cyclist/pedestrian collision at the Horn Hill bus stop in the easterly direction and I note cyclists now have to rejoin the carriageway for the short distance to the roundabout. I realise many cyclists cut through the Asda car park but that is not a good situation.
Cycling UK	787	Ormiston Academy	I hope there will be discussion to resolve the issue of parents parking on the cycle path outside Ormiston Academy.	

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Cycling UK	788	East Suffolk	20 mph speed limits just outside schools do not encourage more cycling of the school run.	Where there are not off-road facilities on popular school routes, often along residential roads, there need to be 20 mph limits. They have been proved to work.
Daniel Wareing	312	Traffic light controlled cross roads of Langer Road and Beach Station Road, Felixstowe.	The traffic lights are activated by sensors in the road. However, they are not activated by cyclists. If a cyclist approaches the junction during quiet times, they face the choice of either waiting for a car to come along and activate the sensor, or jumping red lights. It is incredibly frustrating watching the lights on the intersecting road change through multiple cycles of green orange and red whilst the lights controlling your own progress remain fixed on red.	The sensors need either to be adjusted to ensure that a lone cyclist will be detected and will activate the traffic lights, or the whole system needs to be changed to a simple timer with the requirement for a vehicle to activate a sensor being dispensed with completely.
Daniel Wareing	313	Cross roads controlled by traffic lights, at High Road West and Garrison Lane, Felixstowe	The traffic lights are activated by sensors in the road. However, they are not activated by cyclists. If a lone cyclist approaches the junction during quiet times, they face the choice of either waiting for a car to come along and activate the sensor, or jumping red lights. It is incredibly frustrating watching the lights on the intersecting road change through multiple cycles of green orange and red whilst the lights controlling your own progress remain fixed on red.	The sensors need either to be adjusted to guarantee that a lone cyclist will be detected and will activate the traffic lights, or the whole system needs to be changed to a timer with the requirement for a vehicle to activate a sensor being dispensed with completely.
Daniel Wareing	315	The bridleway which passes Hill House Cottages and Candle Farm between Gulpher Road and Thurmans Lane	Someone else has suggested diverting cyclists from the High Road to this bridleway. This would be a significant and grossly unreasonably lengthy diversion for cyclists needing to transit between eastern Felixstowe and Trimley. That said, the improvement of the bridleway is a good idea to benefit cyclists who already use it, but it should not be on condition that cyclists who would otherwise use the High Road being	

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			expected to divert, as the likely net result would be a reduction in cycling.	
Daniel Wareing	316	Level crossing from Fagbury Road	On occasions the gates governing access across the level crossing are electronically locked for no apparent reason. It is not seem possible to predict when this may occur. This results in a significant detour to the nearest available level crossing which is a considerable distance away. The risk is that frustration will lead to persons crossing the railway when unsafe to do so.	If there is a need for the gates to be temporarily locked for safety reasons, there needs to be a way for a pedestrian or cyclist to find out how long the delay will be and/or to contact someone in control of the locking mechanism to request access.
Daniel Wareing	317	Crossroads of Mill Lane and Garrison Lane, Felixstowe.	The placements of the pedestrian crossings force pedestrians to make a significant detour from the natural line particularly if trying to cross Garrison Lane on either side and either direction.	Locate an additional crossing point to allow pedestrians to cross directly from the NW corner to the SE corner to enable a more direct approach for pedestrians travelling along Mill Lane to cross Garrison Lane in both directions.
Daniel Wareing	370	Pedestrian-only junction of Upperfield Drive and Links Avenue, Felixstowe.	This is currently only for the permitted use of pedestrians, however Links Avenue and Upperfield Drive could form a quiet and suitable alternative route for cyclists travelling between Ferry Road and Beatrice Avenue avoiding Colneis Road.	If the junction of Upperfield Drive and Links Avenue could be upgraded to a full cycle link as well as pedestrian link, whilst maintaining the barrier to through-traffic by motor vehicles, this could create an additional option for cyclists travelling in this part of town.
Daniel Wareing	373	Junction of Chaucer Road and Garrison Lane	Cyclists travelling northward along Garrison Lane wishing to then head towards Western Felixstowe are compelled to continue along the busy Garrison Lane all the way to the crossroads with Mill Lane to turn left onto Mill Lane. There is a junction however with Chaucer Road which is exit only to all traffic including cyclists.	Alter the junction between Chaucer Road and Garrison Lane to permit cyclists bound for Western Felixstowe to turn left from Garrison Lane onto Chaucer Road so that they can avoid the busy part of Garrison Lane approaching the crossroads. Chaucer Road is much quieter and suitable for cycling as well as slightly shortening the distance travelled. The junction would require physical work to safely permit cyclists, but not motorists, to enter from Garrison Lane. It should also permit cyclist travelling south along Chaucer Road to turn right onto Garrison Lane or straight over onto Orwell Road.

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Daniel Wareing	547	Mill Lane railway bridge	The carriageway here is restricted to a single lane where traffic heading east has priority over traffic heading west. However many westbound motorists do not give way to eastbound cyclists when the cyclist has priority and this has the potential for head-on collisions, I personally find this junction scary to approach on a cycle with the right of way as you never know if the oncoming motorist will or will not respect your right of way.	Signage facing west-bound traffic reminding them of the need to give way to oncoming cyclists.
Daniel Wareing	548	Grange Farm Avenue, close to junction with Brackley Close	There is a traffic-calming measure here which requires east bound traffic to give way to westbound traffic. However some motorists often do not respect oncoming cyclists when the cyclist has right of way and this has clear potential to cause a head-on collision. (There is a sign that reads "think bike" however it faces traffic that DOES have right of way so I am not sure what its purpose is). There is a similar issue with Mill Lane at the point where the bridge crosses the railway line.	Either - reverse the sign that reads "think bike" so that it faces traffic that is required to give way; Or, preferably, remove the aforementioned sign and replace with a sign that more specifically reminds traffic that they need to give way to cyclists when the cyclist has right of way.
Daniel Wareing	549	South Hill, Felixstowe	Due to parking of cars on both sides the width of carriageway available on South Hill is limited and it is not possible for a car to pass a cyclist safely, and many motorists especially those descending refuse to slow down or wait for cyclists and pass dangerously, there is the risk that a speeding motorist coming down the hill will have a head on collision with a cyclist climbing the hill.	Make South Hill one way for motor vehicles, I suggest this should be uphill only (and retain two-way passage for cyclists) reflecting the solution arrived at for Bent Hill several years ago as a response to a serious accident. Convalescent Hill is the only one of the three roads ascending the cliff in this area between Sea Road and the Spa Pavilion that is suitable for through motorised traffic.
David Adelson	111	Sutton Hoo to Hollesley Village (Melton Road/Heath Road)	Road is unsafe for cyclists due to large volume of fast traffic. As the road is straight it gives the impression that you can drive fast. It is undulating and very narrow. Alternative routes	A separate lane for cyclists. Maybe through the forest or making use of bridleways across Sutton Common (with surface for normal bikes).

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			to Hollesley or Hollesley Common are a long way round.	
David Adelson	113	Snape to Aldeburgh	The A1094 is too busy and there is no other way of cycling to Aldeburgh.	Use of the coastal path for cyclists as well as walkers. Surfacing in some places, fencing of livestock and extending from Hazlewood Common into Aldeburgh.
David Adelson	407	Footpath from Martlesham to Waldringfield along River Deben	For many years it has simply been accepted that part of the path was washed away by natural erosion, so the only way to walk to Waldringfield from Martlesham is along the road. This is shown by signposts at the access points to this section of footpath.	Re-instating this footpath (by mending the breach at TM279461 or providing a diversion following the high-water mark) would provide a continuous off-road footpath route along the entire west bank of the Deben estuary, with several suitable entry/exit points.
David Adelson	409	Waldringfield	No WC accessible to the public walking or cycling in the area. It would get more people out walking and/or cycling if they could feel sure that they would be able to find WCs en route. Waldringfield is a classic example of a place in a prime location for walkers, but no toilets. This applies to most villages these days so Waldringfield is just one example.	Public WCs should be brought back in villages. Funding could perhaps be eased by charging, and since there is little call to carry coins these days, perhaps this could be arranged via a mobile phone app similar to car-parking. Pubs and cafes (in Waldringfield the Maybush is perfectly located) should be encouraged, or even compelled, to allow passers-by to use their toilets for a small charge (which they might even refund if the user then decides to buy something) - rather than walkers "go" in the bushes.
David Beavan	102	b1127	I agree that the B1127 is dangerous for cyclists and pedestrians. It would also be great to have a cycle route from Reydon to Kessingland, rather than crossing the A12	Make the Coastal path suitable for mountain bikes?
David Beavan	103	southwold and reydon main roads	Congestion in the tourist season makes it difficult for cyclists.	More cycle lanes.
David Carne	334	Westerfield Business Park/Westerfield Station	With reference to the comment of having a cycle park for using the railway , the last time I wanted to use it to take my cycle to Woodbridge I found that the majority of Lowestoft trains do not stop at Westerfield. Could there be liaison with the railway	Request to make Westerfield Station at least a request Halt Station for all users.

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			companies to make Westerfield Station at least a request Halt for cyclists to use all trains.	
David Carne	337	Westerfield Railway Station	Liaise with rail operating company to have all trains stop at lease on a request Halt basis for use by cyclists. As far as I am aware very few Lowestoft trains stop at Westerfield whereas they used to.	Provide parking facility for cyclists and request all passenger trains at least be available to pedestrians or cyclists.
David Findley	397	Footpath between Ufford and Wickham Market alongside B1438	This footpath is very narrow and in poor condition. The path surface has fractured and it is overgrown with weeds. In places the path is non-existent or is heavily rutted. Pedestrians and particularly those with children are in danger from passing traffic and from trip and slip hazards. The path is quite well used but could see much greater footfall if improvements were made.	Widen and resurface this footpath and make sure that the missing sections are filled in. Cut back overhanging bushes to avoid pedestrians having to step into the road
David Findley	465	Footpath on the A12 slip road between High Street and the A12	This footpath is overgrown and the tarmac surface is cracked. the path and verges have not been cut so it means that pedestrians and cyclist have to use the busy road. The path links Ufford with a footpath across to Bredfield and to the site of the Sogenhoe Chapel.	Cut the overgrown grass verges and recondition the overgrown and worn pathway. Make the path wider to allow cyclists to use it.
David Findley	466	Hawkeswade Bridge on road from Ufford to Eyke	This bridge is on a narrow lane with a blind corner, making visibility poor for both vehicles and pedestrians. The footpath and area nearby is used by walkers and cyclists so is often hazardous. Although there is 30 mph sign just before the bridge, there is no road narrows sign and traffic often speeds or has to back up. The road is used by traffic cutting through to the A12 as well as by lorries and tractors from nearby farms.	Improve signage at this dangerous point and also near Melton hamlet where this snother blind corner for pedestrians. Consider adopting a 20 mph limit on this difficult section.
David Foster	66	Broomfield to Eagle way,	The path is too narrow to safely support both cyclists and walkers due to a very tight bent. .	Cyclists should be re routed via Broomfield to Eagle Way

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			There have been collisions in the past at this point.	
David Foster	67	Grange Farm Cycle way	Very poorly maintained and by end of summer is badly overgrown. Additionally people enter the combined Cycle / walkway from hidden junctions.	Need a better maintenance and clearance so its possible to see people entering the cycle track.
David Foster	169	Bridge crossing A12 from Eagle way landing next to Martlesham Leisure	The Cycle / shared pedestrian path is totally unacceptable and has been from the day it was conceived, the landing from the bridge at Martlesham leisure is far too narrow as is the whole path . Cyclists come off the bridge at high speed with little regard to pedestrians .	The Path should be widened or the cyclists diverted onto the road leading to Gloster Road leaving the path for pedestrians. The landing area at the bottom of the bridge must be widened.
David Foster	179	Riverside path from Broomfield to Woodbrige	This is a single track path suitable only for walkers, and I believe cyclists are not permitted. However over the past year more and more cyclists are using it and it is plainly not suitable for mixed use.	Widen the path to permit a cycle lane to be built or prevent cyclists from using it with physical barriers.
David George	198	There is currently no safe or semi-direct safe route for cyclists or walkers between Halesworth and Walberswick/Southwold	There exists currently an 'unsurfaced' footpath running in most parts alongside the River Blyth from Halesworth to Walberswick and then on to Southwold via the river 'Bailey Bridge'. This tends to become overgrown in spring and summer months. It follows a similar path to the ex-Southwold railway track bed (disused and removed early 1900's).	It is suggested that this route be the basis for an improved combined cycle and walkway between these two market towns. This would provide such benefits as alleviating considerable traffic and parking from Southwold and Walberswick, and sharing the abundant tourist and leisure opportunities available at these and along the whole route as it passes through beautiful Suffolk countryside and wildlife. An additional significant benefit is that Halesworth already lies on the Sustrans NCN route 1, plus the benefit of the national rail network, and so passing cycle and rail traffic can detour easily towards the coast. This would require safe provision of a crossing of the A12 at Blythburgh.
David George	293	A144 roundabout joining Quay Street and Saxons Way (Hooker House), up to the Triple Plea	The current main south-north cycle and pedestrian route up Norwich Road to businesses to the north of the town, and importantly to the Edgar Sewter Primary	From the Norwich Road/Quay Street roundabout (A144), move the existing cycle route from the east side of the A144 across to the west. Create a 'Copenhagen' or similar vastly improved crossing at Wissett Road

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		Roundabout where Sparrowhawk Road joins the A144 Norwich Road	School, is dangerous, too complex (multiple road crossings with varying priorities) and does not serve the primary school for sustainable transport	junction, widen what would become the shared pedestrian/cycle path on the west side, remove all existing parking where necessary on the west side (especially near Wissett Road junction, and up A144 past the police station), and replace with single yellow lines with waiting limits of 1 hour (to support school visits and drop-offs). This route must link from the Quay Street Hooker House roundabout up as far as the Sparrowhawk Road roundabout near the Triple Please Road and pub. Suggest NCR1 route is also amended to utilise this new safer less complex route, once established, and once connected to other proposals entered onto the interactive map. Agreed with the Halesworth NPSG Cycle Advisory Team
David George	294	A144 - East side of Saxons Way and London Road in Halesworth, from the Quay Street/Norwich Road roundabout south to the junction between London Road and Bramfield Road	Current NCR1 cycle route through the town Thoroughfare requires dangerous mixing of cyclists with pedestrians and is too complicated. Importantly it routes through the busy central car park which is hazardous for riders to mix with multiple/reversing parked vehicles. The proposal links safely with the separately proposed shifting of the A144 Norwich Road cycle path to the west of the road, via the use of the existing pelican crossings on Saxons Way and/or Norwich Road	Pavements along Saxons Way, from Quay Street roundabout to the Coop/London Road roundabout should become safe, shared cycle and pedestrian paths. There is adequate council-owned land to provide this on the east side of Saxons Way and east side of London Road. Route should continue along the east side of London Road to the Bramfield Road junction (main route into Halesworth from the A12) This route creates the key movement corridor through the town that enables connections to all major destinations – school, Thoroughfare, Doctors Surgery, Sports Centre (in development), industrial estates, residential areas Suggest rerouting of NCR1 away from the Thoroughfare/Bridge Street between the Quay Street and the entrance to the car park removes a confusing and badly signposted national route. The Saxons Way route would remove the confusing one way cycling in the Thoroughfare and the dismount instruction at the

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				southern end of the Thoroughfare. Agreed by the Halesworth NPSG Cycle advisory group.
David George	295	A144 Halesworth, Bramfield Road from junction with London/Walpole Road to Blyth Road Industrial Estate, and possibly to existign NCR1 at Mells/Walpole crossroads on A144	No safe cycle path exists at present, meaning cyclists heading along here must use the main busy road up a steep hill if travelling south-north	Create a route along the south-west side of Bramfield Road (A144), making use of Durban Close if required. This would connect to the proposed north/south route on London Road and to Blyth Road and the entrance to the industrial estate and on into the Millennium Green (hence back towards NCR1. Ideally, this should extend slightly further south-east along the A144 just a little way so that it links with NCR1 where it crosses at the Mells/Walpole crossroads (Wenhaston Grange Road - this creates a far safer route into town for neighbouring Walpole cyclists/families, etc). Blyth Road-London Road section agreed by NPSG Cycling Advisory group, with an additional beneficial extension to Mells/Walpole crossroad to the south
David George	296	Halesworth, existing access route between Chichester Road/Uplands Way housing estate	Current pedestrian-only access between Norwich Road and Uplands Way is narrow and doesn't promote safe cycling of households and children between the Chichester Road estate and town or Primary School. The only current legal cycle route is along busy Wissett Road which is dangerous, has a steep hill for young riders, and has an extremely dangerous junction with Norwich Road (lacking a pushchair/wheelchair width footway). Some young children cycle this route to school but is far from ideal.	Requires possible inclusion of land from Edgar Sewter Primary School to enable wider cycle path. Upgrade the existing footpath between Uplands Way and the Norwich Road alongside the school fence to create a shared pedestrian and cycle path, with signage. Agreed with NPSG Cycle Advisory group.
David George	297	Halesworth - Loam Pit Lane, cemetery area, Harrison's Lane and Hill Farm development	No cycle route linking current and proposed housing development in Harrison's Lane and Hill Farm/Blyth Vale. This will inadvertently encourage riders to use Holton Road and/or Bungay Road and cross the railway line, and/or Norwich Road which is a longer route for young	Loam Pit Lane – include/provide a cycle route so connecting Harrison's Lane to Holton Road, serving the new housing on Harrison's Lane (planning permission given), connecting the new sports centre on the Campus site, with a planned cycle route within it, into Loam Pit Lane. This may partially utilise/link into the development

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			riders. When Campus project is delivered for more social and leisure facilities, it's vital that such a safe route exists, and minimises car use.	intentions by Hopkins Homes Ltd at Blyth Vale (off Hill Farm Road), by linking across the west end of the cemetery and Loam Pit Lane, onto open space areas created by the Hopkins development, and linking towards Holton.
David George	298	Halesworth - link Briar Close with Quay Street/Holton Road	From Quay Street /Holton Road is currently difficult to ride from the road up to the railway station. There is a pedestrian footpath linking from just beside the railway bridge to the end of Briar Close.	Improve access into Briar Close and the route to the Station by improving the pavement under the railway bridge and its connection to the end of Loam Pit Lane. Convert the existing footpath to a shared cycle/pedestrian path. Consideration will have to be given to negotiating the dangerous traffic flow under the Quay Street/Holton Road railway bridge. Part of the NPSG Cycle Advisory group review.
David George	299	Halesworth - new link required between Hill Farm Road development and Loam Pit Lane	Currently it is not clear there is any safe cycle/pedestrian link proposed between the new Hill Farm development (Hopkins Homes Ltd), Loam Pit Lane, and the east side of town towards Holton. Without this the natural route will be a less safe one down Hill Farm Road and onto Holton Road, which is busy for younger and other riders, some of which could be to and from the primary schools in Holton and/or Halesworth.	Hill Farm Road development – create a path from this new estate and the proposed playground west into Loam Pit Lane (possibly linking across the north side of the cemetery) to connect to the proposed new path east to Holton Orchards Road so improving cycling access to and from the east of town and from Holton.
David George	300	Halesworth - new route between Allington Road and Roman Way to support proposed housing development	The current route into town heading west-east (Chediston, Metfield and Harleston) currently takes a cyclist off Chediston Road and up the steep (HGV route) and often busy Roman Way hill which is a steep climb. The proposed new development at Chediston Street/Roman Way includes suggestion for a cycle route up Chediston Street into the town centre. This is considered a dangerous and narrow street with parking both sides, and an alternative route	From the estate create a route into either Barley Meadow, Dakings Drift and/or Allington Road connecting into Dukes Drive, cross Roman Way to connect to the existing cycle route in Holmere Drive and into Church Farm Lane. This creates a relatively safe cycling route into the Market Place and town centre via the quiet northern end of London Road around the St Mary's Church yard.

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			should be planned starting from Allington Road.	
David George	301	Halesworth - from Saxons Way through River Lane to the town park and Millenoum Green	Improve cycling connectivity from the town centre to the Millenium Green and east. River Lane (past George Maltings) is currently only a pedestrian footpath (ownership unknown), but this would be a good short cut from the ANgel Link end of town into the park and Millenium Green and east sides of the town, using the existing Millenium Green cycle path. The existing river bridge in the lane is too narrow to accomodate cyclists.	Make River Lane into a cycle route. This would connect the Angel Link roundabout on Saxons Way to the park. The bridge over the river is too narrow at present but could be given pedestrian right of way or cyclist dismount signs until the bridge can be widened. River Lane is a private road and is unregistered on the land registry, so investigations needed to start the process of redesignation.
David George	302	Halesworth - Blyth Mews link between Quay Street and the town park	Cycling from the east of town (e.g. from Holton Road and Holton village) currently can only use the main Holton Road and Quay Street which links onto Norwich Road (A144) at Hooker House roundabout. This is a dangerous section of road with multiple constricted parking areas (mainly residents), ending in a very busy Hooker House roundabout and confusing pedestrian crossings	<p>Make Blyth Mews off Quay Street into a cycle route with appropriate signage. The bridge over the Patrick Stead Lock at the end of Blyth Mews would benefit from improving. Note - with the addition of 'cyclists give way to pedestrians' signs, the bridge is wide enough in its current form if funding isn't available for widening, until the bridge can be replaced and widened with possible signage giving pedestrians right of way.</p> <p>Review how the Blyth Mews/Quay Street cycle and pedestrian routing could be improved so safer/easier access could be made into Station Road, given giving access to the railway sStation and the 'The Cut'. If the car showroom (currently MR King Ltd) site opposite Blyth Mews was developed this could give an opportunity. Agreed by NPSG Cycle Advisory group</p>
David George	303	Halesworth - Millenium Green and Folly, joining Holton Road to the town park and centre	Currently the natural cycling route from Holton Road and Holton (east side of town) is via Holton Road and Quay Street, which are dangerous and regularly used by HGVs and emergency service vehicles. This should be	Create a new cycle route through the Folly in and the Millennium Green. This would connect Holton Road opposite the new housing development at Hill Farm Road into the Millennium Green and into the town centre or out on the NCR1. The details of the route have been mapped out by the Millennium Green trustees,

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			relieved such that cyclists can divert away from Holton Road onto a new parallel route	<p>who are responsible for much of the land through which the proposed route passes. Footpaths off the Holton Road (towards 'Rails End') would need to be redesignated as cycle routes. The owners would need to be approached.</p> <p>This would then link with the proposed River Lane and Blyth Mews routes into and then through to the town park and Millennium Green, also giving an alternative to the Saxon Way route for less confident cyclists and conversely, a route out to the east of town.</p> <p>Agreed with the NPSG Cycling Advisory group.</p>
David George	304	Halesworth - provide new 20mp speed limit through town to calm traffic and promote safer cycling and low speed vehicle use	Unlike many Suffolk and National towns and villages, Halesworth has no reduced speed limits to 20mph even outside the Edgar Sewter Primary School. This is creating direct danger to cyclists and pedestrians alike, particularly being combined with very poor parking practices in London Road, Norwich Road, Holton Road and Quay Street.	<p>Halesworth requires traffic calming/slowing measures, and the popular and effective way like other nearby market towns would be to provide 20mph speed limiting as follows:</p> <ol style="list-style-type: none"> 1. The main A144 north-south route from Bramfield Road/London Road junction (Kerridges garage) all the way along London Road, Saxons Way, and Norwich Road as far north as "The Avenue". 2. Eastwards from the Norwich Road Hooker House roundabout along Quay Street and Holton Road, as far as "Castle House" at the top of Holton Road hill. 3. Westwards from the Angel Link roundabout and London Road (Coop roundabout) to the junction of Roman Way and Chediston Road. 4. Roman Way from its junction at Chediston Road, to the junction at London Road near the Rifle Hall.
David George	306	Halesworth - Remove parking and apply waiting limits to Norwich Road between its junctions with "Wissett Road" and "The Avenue"	Current unrestricted parking is posing an immediate safety hazard to other road users - cyclists and people/children crossing Norwich Road. It is believed the current prolonged parking may be businesses and Police Station employees. Current parking risks doors being opened into other road users' paths, and	Provide double yellow lines between Wissett Road junction and opposite Hammonds Ford Garage, and from there northwards to the junction with "The Avenue" provide single yellow line restricted parking for 1 hour to enable school drop-off and school visit parking.

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			pedestrian/children crossing between parked cars onto the main through-route including HGVs, is very dangerous. This is a site of previous cyclists being knocked off cycles by cars.	
David George	309	Holton - Triple Plea road from Sparrowhawk Road/Norwich Road A144 roundabout, towards Butts Road (NCR1)	The NCR1 route from Halesworth heading north through Holton, currently is quite complex in places, and if other suggested improvements to north-south routes through Halesworth take place, NCR1 would need slight re-routing from Sparrowhawk Road (Triple Please roundabout) to link up to Butts Road where NCR1 then heads north via the railway Mill Post Crossing towards Westhall and Bungay.	Suggest a crossing from Sparrowhawk Road near the Triple Plea pub to safely cross/cycle onto Triple Plea Road, then signing Triple Plea Road as NCR1 cycle route to the junction with existing NCR1 at Butts Road heading north. This would link the proposed Halesworth area cycle way improvements back onto NCR1 heading north towards Bungay, and vice versa improve cyclist access south to the business and industrial areas at the north end of the town.
David George	310	Heart of Suffolk - Cycle loop passing through Halesworth, Framlingham, Debenham, Eye, Hoxne and Bungay requires improved signage and route granting	This beautiful previously published loop ("The Heart of Suffolk") passes through unspoilt countryside on minor roads and passing churches and other historic points of interest, linking several old market towns. The brown waymarked signs has fallen into real disrepute over the last 5 years or so, and should be granted a formal county route number plus get better signage. The loop can boost local tourism and cafe/craft visits along its whole length.	Review the whole loop and grant a formal route 'number' for the county. Replace existing deteriorated and erroneous direction signs, and republish the loop on an appropriate map and/or website to include GPS files which can be downloaded by other cyclists. Promote links to nearest rail and bus services enroute, to ease the way for shorter distance or less able cyclists. The originator of this request has cycle navigation files which could be used as a basis for publicising online via relevant cycling internet sites.
David George	311	Halesworth - suggested new waymarked county cycle loop (Halesworth, Beccles and Bungay)	This suggested loop follows all back/minor roads and links three prominent market towns, plus would join the route from Beccles to Southwold at Stoven/Sotterley. It would enable joining the loop by train links at either Halesworth, Brampton or Beccles	The originator has navigation files that could be used to illustrate and publicise this route which is a family-safe and beautifully scenic route that can be done in parts or as a while (total 35-40 miles). Heads north from Halesworth through Holton, Brampton, Stoven, Sotterley, Ellough, Beccles, Ringsfield, Ilketshall St Andrews, Mettingham, Bungay, St Peters, St Margarets, Rumburgh and back to Halesworth. Granting of a formal

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				route number and signage would be required - navigation files are available for this very safe route that also piggy-backs a part of NCR1.
David George	361	Link to bypass Wissett Road by joining Norwich Road and Wissett Road	Currently, Wissett Road is a very hazardous route for all road users, but particularly for cyclists and pedestrians, plus the Edgar Sewter Primary School. It is too narrow even for an acceptable pedestrian path at the end near the Norwich Road junction, yet is a route often used by HGVs and farm traffic. Wissett Road in its current form is a dangerous hazard to all.	Norwich Road and Wissett Road should be linked by a new road AND combined cycle/pedestrian route from Broadway Drive (i.e. off Norwich Road) down across the railway line to Wissett Road on the Wissett/north-west side of Halesworth. This would require funding for a railway crossing, but if the field between Norwich Road, Old Station Road and the railway line is (as believed) to be developed for residential or elderly care, then such a crossing should be made an essential part of the development permission process. As a trade-off, perhaps the Old Station Road Mill Post Crossing could be removed to make this proposal more palatable to Network Rail.
David Hayhow	178	B1079 between Grundisburgh and Otley	Twisty narrow road with considerable lorry traffic is not safe for cyclists or walkers.	Newly developed cycling routes should avoid this road.
David Hugh Thomas	392	New Street, Woodbridge	Introduce a 20mph speed limit throughout the centre of Woodbridge. Divert through traffic away from New Street. Introduce a chicane half way down New Street to slow the traffic.	
David McGinity	795	Butley	What is not shown are the number of footpaths in existence. Surely if you want to get people to get out walking and use the footpaths you need to identify them! In the EADT last week it commented that 1904 miles of footpaths had been lost in SUFFOLK alone. They could not have just disappeared! There has been an erosion of the rights of walkers by farmers ploughing up the ways. Establish where these paths are and get them re-established.	As chairman of Butley PC I have raised the issue of farmers ploughing up paths and never even received any answer from Suffolk CC. So lets have some joined up thinking and action. Otherwise this is all a waste of time and money.

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David Neil Steptoe	254	Sandy Lane, Woodbridge	This is a National Cycle Route and could be improved by closing the road to through traffic by bollarding off underneath the railway bridge.	Bollarding off the carriageway can be achieved as there are adjacent turning areas. We achieved this on another site in the West Midlands. I have submitted a report to you covering Woodbridge and Melton on walking and cycling and am happy to give suggestions free of charge.
David Richard Green	90	From Felixstowe Road junction with Mill Lane (track to the RSPCA) to just before Crown Point	Cars passing cyclists on 2 blind bends and having to cut back in across the path of the cyclist as a car comes the other way round the bend. I have personally had several 'near misses'. The area is a serious accident waiting to happen.	Increasingly busy as a 'rat run', the cars need to be slowed down. Suggest 2 speed humps: one by the Mill Lane/RSPCA junction and one further down near Crown Point to slow cars in both directions where the blind bends are.
David W Foster	68	Path alongside the A12	By mid summer the path becomes overgrown reducing it to single file.	If you cannot cut during bird nesting you should really cut back hard at the beginning of the summer or clear the vegetation altogether
Dermot Louth	238	The junction of Warren Hill Road with Ipswich Road.	When cycling up the hill from the Cherry Tree Road mini roundabout it is extremely difficult and dangerous to move across in order to turn right into Warren Hill Road. When waiting at the junction in the middle of the road for a gap in the traffic in order to turn right is very hazardous.	Road markings need to mark out a right turn lane and a illuminated bollard would provide some protection/safety when waiting to turn.
Dermot Louth	239	The traffic lights at the junction of The Thoroughfare and Melton Road.	When cycling into Woodbridge you may need to turn right at these traffic lights to either go straight over into the Thoroughfare or right into St. Johns Street. There is nothing marked on the road to show where cyclists should wait and nothing to protect you from oncoming traffic. The filter system of the lights often mean that you are waiting in the middle whilst traffic squeezes by on your inside and is also passing you on the other side.	A space for cyclists to wait, a bollard to protect and make traffic keep their distance. A mini roundabout may help.
Dr Paul Cope (originally)	739	Halesworth	I have been looking at the plans for the Cycling and Walking Strategy for Halesworth and I think these are all good ideas.	The plans for the Norwich Road (306) are pressing as that is a dangerous route and currently the existing cycle path is quite dangerous in itself with a lot of roads and

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submitted by email)				entryways cutting across. Connecting the town to the Sparrowhawk Roundabout safely would be a huge improvement. I think that connecting the Millenium Green paths to the Holton Road (303) and making the Blyth Mews path (302) into a cycle path are particularly good ideas. Connecting the town to Southwold with a safe cycle route would be great. Halesworth is popular with cyclists and there are often groups in the town, particularly at the cafes (well, pre-covid anyway). Helping cyclists get into the town would be good for business and further opening (480) up the Thoroughfare to cycle access would help both tourists and utility cycling. I would say that car speeds have increased in the town recently and the town needs 20mph zones and traffic calming to make it safer to walk and cycle around (304).
drew whitmore	72	Old felixstowe road, Martlesham	This road is supposed to be cycle friendly but the opposite is true as large quantities of traffic use it as a cut through to the industrial estate and are allowed in the cycle lanes. Very dangerous for cyclists and hence underused.	strict enforcement, separate cycle lanes with kerb.
Drew Whitmore	73	Sandy lane, Martlesham	This lane is the connection between the cycle lanes of Martlesham/Kesgrave and Woodbridge. It is used as a short cut for traffic to and from Woodbridge and is national speed limit which creates dangerous conditions for all cyclists particularly those who don't know the road well and children.	20 or 30 MPH limit. Access only for motorised vehicles?
Drew Whitmore	74	Ipswich Road, Woodbridge	Very dangerous for cyclists on the route into Woodbridge	Dedicated cycle lane, possibly two way alongside/incorporating the wide footpath, as far as the Cherry tree road junction. Provide some quality bike parking in Woodbridge.
Drew Whitmore	75	County wide	The issue for cyclists is a lack of dedicated infrastructure along with having to cycle on	We have a vast network of ancient lanes and byways, many of which are not heavily used by motorised vehicles but do not necessarily join up to go anywhere

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			fast, dangerous small roads alongside drivers who assume entitlement.	<p>safely. Some of these lanes could be connected with new sections built to join settlements as needed.</p> <p>Possible rules along these routes:</p> <ol style="list-style-type: none"> 1. No through traffic 2. A new speed limit of 25mph for all other traffic requiring access. 3. A change in insurance liability similar to the Dutch article 185 of road law along these routes, thus deterring traffic further and encouraging family use. <p>As most of the roads already exist, it could be a cost effective solution with major impact.</p> <p>Such routes, if well planned, may well serve to encourage family cycling holidays, such as are seen in other countries, and if a few campsites or cheap lodgings were encouraged along the way, would likely boost tourism substantially.</p>
Dudley Watson	552	JUNCTION between Warren Hill Road and Ipswich Road.	This is a very dangerous junction for cyclists turning right into Warren Hill Road. Motorists coming down the hill are going faster, also they often fail to see cyclists waiting in the centre of Ipswich Road to turn right; the driver side A pillar of their vehicle obscures the waiting cyclist. Also, vehicles bearing right round the bend tend to move to the centre of the road. This is so dangerous I will no longer make this turn by bike.	There needs to be a safe space for cyclists in the middle of the road. This requires an illuminated island at the junction and line markings on the road indicating cyclist space. NOT just white lines, these could cause more problems by giving the appearance of safe space. There have already been accidents involving cyclists at this junction.
Dudley Watson	554	Cumberland St off the B1438	Pavements are way too narrow here. Since resurfacing, cars go far too fast, often on the school run. Pedestrians have to walk on the	This lovely medieval street should be shared use; space for vehicles should be reduced to one way with passing places and pedestrian space should be made wider by

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			road to maintain social distancing and are constantly at risk from vehicles.	use of bollards and planters; an inexpensive solution. Ideally, resurface at one level.
Dudley Watson	555	The Avenue, north east side of Kingston Field	There is no pedestrian pavement on The Avenue, it is poorly lit. Vehicles now use the new car park adjacent to this road and it is consequently much busier than before. On dark nights pedestrians are all but invisible.	Safe access for pedestrians is required. Pavement?
Dudley Watson	557	Kingston Field	Kingston Field is entirely surrounded by kerbed areas; there is, surprisingly, no disabled access to this field.	Put in flat driveway type access in at least two places. Not too expensive and VERY disabled friendly.
Dudley Watson	560	The Turban Centre, Woodbridge.	There is nowhere to securely leave a bike in the Turban Centre.	Very simple. I appreciate that large cycle hoops will decrease pedestrian access; perhaps some rings in the wall of the Boots store to allow short term parking for, say, three bikes to lie alongside the wall there.
Dudley Watson	562	Quay St, Church St, New St..	All these streets have inadequate space for pedestrians. Pavements are too narrow, vehicles go too fast.	Widen the pavements; if need be with temporary bollards, helping to maintain social distancing. Slow down the cars with obstructions. Better still, shut the cars out.
Dudley Watson	563	Melton Rd, Woodbridge to Melton	Cycle use of this road is dangerous. Cars move too fast and the road has no cycle lanes.	20 mph speed limit would be helpful here. Purpose built cycle path ideally, until then marked cycle lanes on the road.
Dudley Watson	564	The road from Melton cross roads to Sutton Hoo has very poor cycle access.	This is a very busy route. Cyclists are an endangered species.	In the interest of increasing cycle access to Sutton Hoo there should be marked cycle lanes with signs, from the traffic lights at Melton crossroad all the way to Sutton Hoo.
Dudley Watson	566	A12, south of the B1079 roundabout	There is only one pedestrian crossing of the A12 on the entire Woodbridge bypass, opposite Russell Close, this is insufficient. Residents would walk / cycle to the retail and associated areas if they could cross the road safely.	Pedestrians have to cross the A12 at the one crossing opposite Russell Close. There needs to be another crossing south of the B1079 roundabout, this will allow pedestrian and cycle access to the retail area and beyond without making an unnecessary detour. This will decrease car use and increase local shopping.
Duncan Adams	219	The Road surface between The Falcon	The road surface heading south as you leave the cycle lane and head passed the Falcon public house is unsuitable for road bikes. It has	The road needs resurfacing.

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		Public House and Mariners Street.	been patched hundreds of times over a period of many years and is now unfit for cycling without a mountain bike.	
Duncan Adams	220	At the junction of Sussex road and Yarmouth road.	After some light rain the road here floods because of an ongoing problem with drainage. unfortunately there is a serious pothole next to a sunken drain cover which can end up submerged. If a cyclist was to ride through the flood and hit the pothole the accident would be serious.	This has been reported to Highways on a number of times with little effect. The flooding has been continuous for many years. You wouldn't think it would be too hard to drain an area like Yarmouth road which is on the top of a hill! (The Ravine). it needs a new drain and the pothole filling before someone gets hurt.
Duncan Adams	221	Cycle Lane on Corton Road	There is a designated Cycle lane running the length of the Corton Rd, that no one can use because there are always cars parked in it. It feels dangerous as a cyclist to have to constantly overtake these parked vehicles without a designated Cycle Lane.	Move the cycle lane to the outside of the parked vehicles as they do in Holland, and similar to the High Street outside the Lighthouse.
Edward Creswick	400	Ufford	There are many footpaths in and around Ufford that are widely used by residents. While many are across fields and through woodland, walkers are obliged to use the lanes in Ufford to access them. There are very few pavements in the village, obliging walkers to compete with vehicle traffic on single track lanes.	Installing pavements is impractical in most instances due to cost and planning issues. However, there is a simple, cost effect improvement available. The vehicle speed limit within the village is 30 mph. Decreasing this to 20 mph on single lane roadways would dramatically increase safety for both walkers and cyclists, with little effect on traffic flow. Ufford lane road traffic is largely local, with little through traffic.
Edward Gilder	283	The link from Normanton Park to Harbour Road via the rail footbridge	Not only is the footbridge difficult to negotiate with a bicycle or a pushchair once you are on the south side you are dumped in to a sort of no man's land. there is an urgent need for this connection to Harbour road to be sorted out across the waste land rather than down to the foreshore, which of itself is unsatisfactory.	Get a decent, direct and surfaced path across the wasteland at the end of Harbour Road up to the railway bridge. Both East Suffolk and Suffolk CC have adequate powers to secure a route here. it must be possible to engineer a better solution to crossing the railway bridge that exists at present. Improving this route has been a long term aim of the council for years and yet nothing happens. Why not?
Elizabeth Shakespeare	21	Ceder drive towards new roundabout	No cycling or walking path connecting the Ellough Road with the new Beccles bypass	Install a cycle/walking path.

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Emily Springford	133	Between the end of the Walks and the entrance to Sutton Hoo	There is no footpath, which makes it unpleasant and dangerous when walking between Melton station and the peninsula.	Build a foot and cycle way to Melton station. Ideally this would not follow the road down the hill which is steep, bendy and a danger to cyclists; it would be great if a foot/cycle way could be created from Sutton Hoo to the river side and Wilford Bridge: this would make a lovely access route to Sutton Hoo and the peninsula from Melton train station, encouraging sustainable travel and tourism, and reducing congestion in Melton/Woodbridge.
Emily Springford	208	Sutton Heath	The tracks across the heath (especially north/south) are very sandy. This means that they are not practical for cycling. This is a shame as they offer direct routes between the villages and schools on the peninsula and would provide a suitable alternative to road use for cyclists.	Firm up the main paths across the heath with gravel or other hard infill, to facilitate cycling and make the roads safer. It wouldn't be necessary to tarmac them so that the beautiful landscape can be preserved.
Emily Springford	209	The road to Shingle Street	The road is very congested and during the summer a huge number of cars park on the verges, ruining the unique beauty of the beach and marshes. It is difficult and dangerous for walkers and cyclists to navigate the traffic.	The road should be used by vehicles only for access to the homes at Shingle Street. Visitors should be required to park at the Shepherd & Dog pub or the Suffolk Punch Trust and walk or cycle to the beach. Bikes and trailers could be offered for hire to raise funds for the community, and the Trust, village shop and pub would also benefit from increased footfall in the village.
Emma Hollis	120	A14 cycle path Felixstowe to Levington	The cycle path is in a terrible state of disrepair, overgrown and strewn with debris. It is not maintained and the surface is dangerously uneven. It's also frighteningly close to A14 traffic. Because of these issues it's considered by most cyclists to be unusable, and certainly not safe for families with children.	The cycle path was installed prior to the single track link road which now runs beside it. It would be great if the cycle path could be relocated to nearer the quieter link road and away from the A14. Or maybe an alternative route could be considered following the river Orwell to give traffic free access right into Ipswich?
Esther Mower	105	On the shared use cycle path along Lovers Lane towards Sizewell.	The cycle path is great but in a few places there are bollards on the pavement which encroach on the space and make it impossible for a cyclist to pass a pedestrian or other cycle on	The bollards just need removing! I am not sure why they are there. Also, perhaps a guide line on the path for pedestrians/cyclists half of the path?

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			the path. This shared use path is well used by walkers and cyclists but we repeatedly have to join the road here as it is not possible to pass others. It is particularly awkward as this is really well used by families and children.	
Felicity Borwick	478	Moss Lane Westerfield	This road is single track and used by a large range of vehicles as a short cut. It is unsuitable as a rat run and should be closed to through traffic thus protecting cyclist and pedestrians.	
Felixstowe Country Park Group	683	North of Felixstowe	See attached.	See attached.
Felixstowe Town Council	689	Felixstowe	See attached.	
Felixstowe Town Council	690	Fagbury Rd level crossing (Walking)	Both the route to the crossing and the crossing itself have minimal signage, approach is "hostile" – appears to be private haulage yard. Safety issues of the actual railway crossing need investigation and explanatory signage. Are there not Security issues regarding access to the Port railway system? It is also likely to be a critical link on the National Coast Path, underlining the need for safety and signage for non-local users.	
Felixstowe Town Council	X1	ELMCROFT LANE /WESTMORLAND ROAD TO CLIFF ROAD	FOOTPATH 8 REPLACE STAGGERED BARRIERS WITH BOLLARD AND SIGN ROUTE.	
Felixstowe Town Council	X10	FAIRFIELD AVE TO GARRISON LANE/HIGH ROAD WEST JUNCTION	CONVERT FOOTWAY TO CYCLE TRACK REMOVE HOOPED BARRIERS REPLACE WITH BOLLARDS	

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Felixstowe Town Council	X11	WALTON AVE EAST	ET06181 EXTEND OFF ROAD CYCLE TRACK FROM CHURCH OF LATTER-DAY SAINTS TO LANGER ROAD	
Felixstowe Town Council	X12	LANGER ROAD	SAFE ROUTE TO SCHOOL CONVERT WESTERN FOOTWAY BETWEEN WALTON AVE TO HOLLAND ROAD	
Felixstowe Town Council	X15	BEACH STATION ROAD	SIGN AS CYCLE ROUTE TO LANDGUARD & BEACH	
Felixstowe Town Council	X16	GARRISON LANE	ADD ADVISORY CYCLE LANES BETWEEN ITS JUNCTION OF UNDERCLIFFE ROAD WEST AND HIGH ROAD WEST.	
Felixstowe Town Council	X17	CLIFF ROAD WEST	SIGN AS CYCLE ROUTE TO PIER	
Felixstowe Town Council	X18	PRINCES ROAD/ SOUTH HILL	SIGN AS CYCLE ROUTE TO PIER	
Felixstowe Town Council	X19	CRESCENT ROAD	BETWEEN GARRISON LANE & COBBOLD ROAD EXISTING SIGNED AS NCR51 ADD ADVISORY CYCLE LANES	
Felixstowe Town Council	X2	LOCAL ROUTE 1 COLNEIS ROAD	ADD ADVISORY CYCLE LANES BETWEEN JUNCTION OF CHURCH ROAD AND BEATRICE AVE	
Felixstowe Town Council	X20	PRIORY ROAD	BETWEEN HIGH ROAD WEST & GOLF ROAD SIGN AS CYCLE ROUTE	
Felixstowe Town Council	X21	CARR ROAD	BETWEEN BEACH STATION ROAD & DOCK GATES SIGN AS LOCAL CYCLE ROUTE	
Felixstowe Town Council	X22	ORFORD ROAD	BETWEEN CARR ROAD & SEA ROAD REMOVE NCN SIGN REPLACE WITH LOCAL ROUTE SIGNING	
Felixstowe Town Council	X23	MANOR ROAD & MANOR TERRACE	REMOVE NCN SIGNAGE BETWEEN CARR ROAD WORK ITEM 13	
Felixstowe Town Council	X24	MANOR ROAD CAR PARK	ADD CYCLE LOGS (1057) TO HIGHLIGHT ROUTE THROUGH CAR PARK	
Felixstowe Town Council	X25	COBBOLD ROAD	SIGN AS LOCAL CYCLE ROUTE & ADD CYCLE LOGO 1057	

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Felixstowe Town Council	X26	HIGH ROAD WEST (HOWLETT WAY RNDBT)	EXISTING FACILITY, AT START REQUIRES CYCLISTS DIRECTION ARROW FROM HIGH RD TO FACILITY REQUIRES DROP KERB AND MARKING TO REJOIN HIGH RD ON WESTERN SIDE OF RNDBT	
Felixstowe Town Council	X27	HIGH ROAD WEST (HOWLETT WAY RNDBT)	EXISTING FACILITY CYCLE TRACK REQUIRES BOLLARDS AND LINE GIVE WAY LINE MARKING.	
Felixstowe Town Council	X28	HIGH ROAD WEST	EXTEND ADVISORY CYCLE LANE THROUGH TRAFFIC ISLAND TOWARDS RNDBT TO START OF OFF ROAD CYCLE TRACK.	
Felixstowe Town Council	X29	WALTON AVE EXTENSION WEST	ET06180	
Felixstowe Town Council	X3	CHURCH ROAD	SIGN ROUTE	
Felixstowe Town Council	X31	GRANGE FARM AVENUE	WIDEN FOOTWAY TO EXTEND CYCLE FACILITY BETWEEN WESTLETON WAY TO THE RNDBT JUNCTION WITH WESSEL AVENUE	
Felixstowe Town Council	X32	GRANGE FARM AVENUE & WESEL AVENUE	WIDEN FOOTWAY TO EXTEND CYCLE FACILITY ACROSS EASTERN ARM OF RNDBT TO MEET LOCAL ROUTE 6 CYCLE TRACK .	
Felixstowe Town Council	X33	FERRY LANE	FROM END OF OFF ROAD CYCLE FACILITIES ADD ADVISORY CYCLE LANES TO HODGKINSON ROAD/DOOLEY INN PH	
Felixstowe Town Council	X34	GRANGE FARM AVENUE	BETWEEN LANGLEY AVE & SUDBOURNE RD ADD CYCLE LOGOS AND ADVISORY CYCLE LANES THROUGH ISLAND PINCH POINTS	
Felixstowe Town Council	X35	GRANGE FARM AVENUE (GFA)	AT CROSS ROADS FORMED BY BRACKLEY & POND CLOSE. TERMINATE CYCLE PATH AT POND CL ADD SPUR TO CROSS GFA WHERE BUILD OUT NARROWS ROAD. CONSTRUCT CYCLE BYPASS TOWARDS BRACKLEY CLOSE AND ADD CYCLE LANE ACROSS ITS MOUTH.	
Felixstowe Town Council	X36	GRANGE FARM AVENUE	EXISTING CYCLE FACILITY ADD GIVE WAYS & SIGNS - DO WHAT TO THEM?	

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Felixstowe Town Council	X37	LOCAL ROUTE 5 (MORRISONS LAND)	CONSTRUCT NEW OFF ROAD FACILITY ACROSS CAVENDISH PARK NORTH TO MEET CYCLE FACILITY ON CAVENDISH PARK SOUTH.	
Felixstowe Town Council	X38	CAVENDISH PARK NORTH	CONSTRUCT NEW OFF ROAD FACILITY ACROSS CAVENDISH PARK NORTH TO MEET CYCLE FACILITY ON CAVENDISH PARK SOUTH.	
Felixstowe Town Council	X39	WESTMORLAND ROAD	SIGN AS LOCAL CYCLE ROUTE	
Felixstowe Town Council	X4	ROSEMARY AVENUE	REVISED ROUTING OF LOCAL ROUTE 1, SIGN & ADD ADVISORY CYCLE LANES	
Felixstowe Town Council	X40	GRANGE FARM AVENUE	BETWEEN HINTLESHAM DRIVE & POND CLOSE WIDEN EASTERN FOOTWAY & CONVERT TO SHARED USE WITH PRIORITY CROSSING OF REYNOLDS CLOSE. REMOVE TRAFFIC ISLAND & REPLACE WITH RAISED CROSSING FROM NEW CROSSING WIDEN FOOTWAY TOWARDS BRACKLEY CLOSE. (SEE 35)	
Felixstowe Town Council	X41	NATIONAL CYCLE ROUTE 41	SUFFOLK COASTAL CYCLE ROUTE	
Felixstowe Town Council	X42	NORTH SEA CYCLE ROUTE	FORMERLY NCN 1 NOW NCN41 &51	
Felixstowe Town Council	X43	MILL LANE	ADD ADVISORY CYCLE LANES BETWEEN GARRISON LANE AND GRANGE ROAD. AT BRIDGE REDUCE VISUAL RUNNING LANE BY WHITE LINE & HATCHING.	
Felixstowe Town Council	X44	TRIMLEY ROAD KIRTON	ADD CYCLE LOGOS (1057) 100M NORTH OF SCHOOL TO ROSELEA NURSERY	
Felixstowe Town Council	X45	KIRTON ROAD ROAD TRIMLEY	WIDEN FOOTWAY LEADING TO FOOT BRIDGE OVER A14, & CONVERT TO SHARED USE.	
Felixstowe Town Council	X46	HOWLETT WAY TRIMLEY ST MARTIN	WIDEN FOOTWAY & CONVERT TO SHARED USE.	
Felixstowe Town Council	X47	HIGH ROAD TRIMLEY ST MARTIN	ADD CYCLE LANE BETWEEN MILL LANE & HOWLETT WAY	

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Felixstowe Town Council	X48	HIGH ROAD TRIMLEY ST MARTIN	ADD CYCLE LOGOS 1057 FROM EGRESS OF CYCLE PATH TO MILL LANE	
Felixstowe Town Council	X49	HIGH ROAD TRIMLEYS & HIGH ST WALTON	REPLACE THE MISSING SECTION OF ADVISORY CYCLES & ADD NEW TO PROVIDE CONTINUOUS LANES BETWEEN GARRISON LANE AND HOWLETT WAY.	
Felixstowe Town Council	X5	TAUNTON & EXETER ROADS	SIGN ROUTE	
Felixstowe Town Council	X50	FAULKENERS WAY (EAST) HIGH ROAD JUNCTION	CONSTRUCT CYCLE TRACK PRIORITY CROSSING	
Felixstowe Town Council	X51	NATIONAL CYCLE ROUTE 51	HARWICH TO CAMBRIDGE	
Felixstowe Town Council	X52	MAIDSTONE ROAD & GRANGE ROAD	ADD CYCLE LOGO 1057 BETWEEN RAISED TABLE BETWEEN HIGH ST WALTON AND WESSEL AVE /PEWITT HILL	
Felixstowe Town Council	X53	GRANGE ROAD	ADD CYCLE LANES AT SCHOOL ENTRANCE BETWEEN VICARAGE RD & MILL LANE	
Felixstowe Town Council	X54	SEA ROAD	ADD CYCLE LOGOS BETWEEN UNDERCLIFF ROAD & ORFORD ROAD	
Felixstowe Town Council	X55	HAMILTON ROAD	CONTRA FLOW CYCLING BETWEEN COBBOLD ROAD & ORWELL ROAD	
Felixstowe Town Council	X56	HIGH ROAD EAST	EXTEND ADVISORY CYCLE LANE FROM PRIORY Road TO CLIFF ROAD	
Felixstowe Town Council	X57(1)	MAIDSTONE ROAD - SEATON ROAD RNDBT	OPTION 1 REDUCE ROAD ENTRY WIDTH OF THE 3 ARMS BY LINING AND HATCHING ADD CYCLE LOGOS.	
Felixstowe Town Council	X57(2)	MAIDSTONE ROAD - SEATON ROAD RNDBT	OPTION 2 REDESIGN AS SHARED SPACE.	
Felixstowe Town Council	X58	SEATON ROAD	ADD CYCLE LOGO 1057 BETWEEN HIGH RD WALTON AND MAIDSTONE ROAD	
Felixstowe Town Council	X59	BACK LANE	ADD CONTRA FLOW CYCLE LANE BETWEEN SEATON ROAD AND HIGH ST WALTON	

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Felixstowe Town Council	X6	HIGH ROAD EAST	CONVERT TO SHARED USE BOTH EAST BOUND FOOTWAYS TOWARDS THE EXISTING PED ISLAND. LENGTHEN THE ISLAND AND EXTEND DROP KERBS TO PROVIDE A CYCLE CROSSING BETWEEN ROSEMARY AVENUE & PICKETTS ROAD	
Felixstowe Town Council	X60	FELIXSTOWE LEISURE CENTRE	CONSTRUCT NEW OFF ROAD FACILITY FROM UNDERCLIFFE ROAD TO SEA ROAD BEHIND SEA FLOOD WALL. SCDC ASPIRATIONAL ROUTE	
Felixstowe Town Council	X61	CRESCENT ROAD /HAMILTON ROAD JUNCTION	AT TRFFIC LIGHT INSTALL ADVANCED STOP LINES (ASL)	
Felixstowe Town Council	X7	PICKETTS ROAD	SIGN ROUTE	
Felixstowe Town Council	X8	A1021 HAMILTON ROAD ROUNDABOUT	CONVERT TO SHARED USE THE FOOTWAYS AND FOUR PEDESTRIAN ISLAND CROSSING AT THE ROUNDABOUT ARMS	
Felixstowe Town Council	X9	BETWEEN GARRISON LANE & MAYBUSH LANE	SIGN AS LOCAL ROUTE 7 ST ANDREWS ROAD & FOXGROVE LANE AS CYCLE ROUTE	
Felixstowe Town Council	691	Foxgrove Lane / High Rd (Walking)	Poorly signposted, heavily overgrown, poor surface	
Felixstowe Town Council	693	Brook Lane / Park Avenue (Walking)	Signposting, maintenance	
Felixstowe Town Council	696	Church Rd / St. Georges Rd / Western Ave / Roman Way / Clifff Rd (Walking)	Signposting, maintenance (not bad) Ils this cable of upgrading to Cycle Route?	
Felixstowe Town Council	697	Martello Lane / beach (Walking)	Signposting, maintenance	
Felixstowe Town Council	699	Quintons Lane Ferndown Rd / Colneis Rd (Walking)	Poorly signposted, heavily overgrown, poor surface capable of upgrade to cycling?	
Felixstowe Town Council	700	Ferndown Rd / Gosford Way (Walking)	Poorly signposted, heavily overgrown, poor surface capable of upgrade to cycling?	

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Felixstowe Town Council	701	York Rd / rear St. Felix Church (Walking)	Signposting, maintenance	
Felixstowe Town Council	702	Ranelagh Rd Car Park to Spa Pavilion & Garden via steps on Hamilton Gardens (Walking)	Signposting	
Felixstowe Town Council	703	Garrison Lane roundabout to Coronation Drive via Railway bridge (Walking)	Signposting, maintenance	
Felixstowe Town Council	704	Beach Station Rd through Langer Park (Walking)	Signposting, significant enhancement	
Felixstowe Town Council	705	Peewit Hill (Walking)	Poorly signposted, heavily overgrown, poor surface capable of upgrade? Ownership issues?	
Felixstowe Town Council	706	Footpath 41 Haven Exchange to Coronation Drive	Signposting, maintenance Was closed due to slippage. What is current status?	
Felixstowe Town Council	707	Footpath xx Philip Avenue to Coronation Drive	Was closed due to slippage. What is current status?	
Felixstowe Town Council	708	Elmcroft Lane / Colneis Rd / Westmorland Rd x2, Ferry Rd	“No Cycling” sign at Westmorland Rd? No Cycling sign near Whinyard Way. Overgrown, part poor surface. This could surely be a Cycle Route?	See attached map - references W6B
Felixstowe Town Council	709	Elmcroft Lane Western Ave (Walking)	Poorly signposted, heavily overgrown, poor surface Is this cable of upgrading to Cycle Route?	
Felixstowe Town Council	710	High Row Field / High Road (Walking)	Status? Created as part of High Row Field development. Signposting, maintenance. NB reference effects of potential redevelopment of Brackenbury Sports Centre site.	

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Felixstowe Town Council	711	College Green / Maybush Lane (Walking)	Status? Created as part of College development. Signposting, maintenance. Ownership & rights complex. Reference correspondence about Planning Application DC/20/4188/FUL	
Felixstowe Town Council	712	Quintons Lane Sunray Ave / Links Ave (Cycling)	Signposting, maintenance	
Felixstowe Town Council	713	Left turn Chaucer Rd (Cycling)	A short cycle track, c. 3m length across the Chaucer Rd island would allow cyclists to turn left when travelling North West, to access Mill Lane rail bridge, avoiding heavy traffic on Garrison Lane, and traffic lights at Mill Lane junction.	
Felixstowe Town Council	714		Open connection beneath Leisure Centre walkway to promenade between Pier Bight Car Park existing route and the Events Area (Cycling)	Although not obvious, careful informal survey appears to indicate this is feasible. Would need negotiation with Leisure Centre operator. Previously identified by SCC 2015. Also a good principle to establish ahead of potential future development of Leisure Centre site.
Felixstowe Town Council	715	Exit Martello Park to Manor Terrace - See also map Cycle Route 51B & Insets	Track ends at boundary of Martello Park development. Cycle Route 51 continues onto Manor Terrace to Landguard via the Car Park. The large area of unmade ground is without known ownership.	This needs to be researched again (ESC did some work c . 1999 as part of South Sea Front project) and ESC should seek to claim it, as was done recently nearby on corner of Manor Road & Terrace. Could then serve as Cycling and Walking Route, and also possibly additional residents parking for Manor Terrace properties, frequently requested. But it is also a critical access route for both ESC and EA for plant access to 2 vehicular flood gates for flood defence maintenance. Protection is believed to be formalised for EA by flood defence regulations. Layout must recognise that. NB the land cannot be built on, for that reason.

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Felixstowe Town Council	716	New recreational Cycle Route The Dip to Felixstowe Ferry	<p>Enable cycling on:</p> <p>A) ESC Coast defence “promenade”.</p> <p>B) Environment Agency sea wall adjacent golf course</p> <p>Would require permissions ESC, EA And Golf Club? (own the land on which sea wall is built?)</p> <p>Some improved surfacing required at northern end.</p>	See attached map - references C21A
Felixstowe Town Council	717	Hawkes Lane / footpath to Maidstone Rd & Runnacles Way via railway foot bridge	The short stretch of Hawkes Lane between High Street and the school entrance road, and its continuation as a footpath along the West and South of the school site to the new railway bridge and beyond is poorly maintained, partially overgrown and has negligible signage.	<p>If also upgraded for cycling use, it could constitute a significant cross town route to the Orwell Green area, the port area and towards Trimley and Ipswich via the A14 footbridge. Additionally it would link with access across the forthcoming Walton North development to Candlet Road, and then to the North Felixstowe Garden Village and the countryside beyond as a major strategic cycle route, potentially from the Deben to the Orwell estuaries.</p> <p>It should also be made accessible directly from the South Eastern corner of the new Walton Hall Drive, giving access from that estate to the south and west as above.</p> <p>(See attached map - references C23B)</p>
Fiona Baker	460	The entire Riverside of Woodbridge and Melton from Kyson Point to Wilford Bridge	Tourism is vital to Woodbridge's economy and the river is a major tourist attraction. I know that cycle tour companies have expressed amazement that it is not possible to cycle through Woodbridge along the river bank. It is scandalous that we do not make the most of our beautiful river and actively discourage	<p>From Kyson Point to The Avenue there is a rough narrow grass track below and to the left of the raised river path that could be made into a cycle path.</p> <p>From just beyond Deben Road to Wilford Bridge in many places there are already two clear paths and it should be possible to convert and extend one of these into a cycle path.</p>

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			cyclists . There is no safe provision anywhere in the town for them.	<p>In the few places where this would not be possible could there not be signs saying 'cycling permitted but priority must always be given to pedestrians'. In my experience if you are a polite careful cyclist, pedestrians have no objection to cyclists along the part of the river.</p> <p>Between The Avenue and Deben Road there should be signs diverting cyclists along the road. A 20 mph limit should be established on the Avenue, Cherry Tree Road, Kingston Farm Road, Kingston Road and Station Road, so that where there is not a dedicated route along the river cyclists can be diverted to a cycle friendly route.</p>
Fiona Baker	461	the junction of the Thoroughfare and Lime Kiln Quay Road, Woodbridge (traffic lights)	dangerous junction for cyclists	provision of a cyclists' box marked out in front of the car traffic - particularly necessary if travelling from Melton Hill and going right or straight on at the lights and if travelling up Lime Kiln Quay Road going right.
Fiona Baker	462	Riverside path leading from railway bridge on Sandy Lane to the river at Kyson Point.	Currently there is no provision for cyclists to cycle close to the river in the Woodbridge area. We are missing an opportunity to promote glorious cycling in our region.	Please could it be permitted for cyclists to use the river path with priority for pedestrians or permitted at certain times of day or weekdays only? We really need to have more shared usage tracks to encourage good manners and cooperation between cyclists and walkers rather than pitting them against each other always. Why can't we make East Suffolk lead the way in this country, - we are the gateway to the parts of Europe where cycling is king and we have so much to offer. At least make Sandy Lane a quiet Suffolk lane with priority for cyclists and pedestrians as when the tide is in the footpath at the bottom of the creek is impassable.
Fran Crowe	718	East Suffolk	See attached.	See attached.
Fran Crowe	719	Orford to Aldeburgh via Snape	I would like to see off-road cycle paths from Orford to Aldeburgh via Snape (sections of this exist already, for instance the Sailor's Path);	

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Fran Crowe	720	Between Orford and Woodbridge	I would like to see off-road cycle paths from Orford to Woodbridge (and Sutton Hoo).	This would link many local facilities and heritage attractions and also join up with local train stations for those wanting to come to the area with their bicycles by rail.
Fran Crowe	721	Between Snape and Saxmundham	I would like to see off-road cycle paths from Snape to Saxmundham.	This would link many local facilities and heritage attractions and also join up with local train stations for those wanting to come to the area with their bicycles by rail.
Gareth Prosser	582	Cars parked near the shop	Highly dangerous to cycle past the shop area (in particular in the east direction) due to slowing / stopping cars that are parking for the shop, also cars pulling out after using the shop. Frequent near misses due to poor awareness of cycling traffic. Cycle lane is constantly parked on. The road is also very narrow at this point.	20 mph zone? mandatory cycle lane? Dedicated parking bay surrounded by double yellow lines? parking enforcement?
Gareth Prosser	587	The track beyond Cordy's lane that goes as far as the nature reserve.	The surface has improved recently but is still not suitable for running/cycling due to the inconsistent surface and large stones. This is a huge missed opportunity for recreation for this part of Trimley St Mary.	Durable resurface suitable for light foot traffic.
Garry BOOTH	314	Saxon's Way and Thoroughfare	The Saxon's Way A144 thru road is a very busy road for cyclists and has no cycle path. Cycling is only allowed one way thru the Thoroughfare, which is busy with peds. A cycle path is badly needed on Saxon's Way to connect with Bungay Rd A144 where there is a cycle path. (This one needs extending to the quiet lane at the Triple PLea Roundabout at Sparrowhawk Lane.) It is worth remembering that the Edgar Sewter School is on the A144.	
Gavin Reeve	116	High Road East, Felixstowe	Very poor road surface in cycle lane	Road needs resurfacing, not just another top dressing, which makes matters worse for cyclists
Gavin Sharpin	364	Road over Wilford Bridge	Road is narrow and busy and cars sometimes drive very close to cyclists.	Would be very useful to have a cycle path off-road to allow safer access to the coast / Bromeswell.

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Geoff Farrell	346	Between Aldeburgh and Thorpeness	As in a previous comment, the road is unsuitable for riding a bike comfortably, safely and pleasantly. The path is really a footpath not a cycle path. Shared use paths are against LTN 1/20 so the best thing to do is build a new cycle only path. This will be welcomed by people who walk and cycle there.	So that the new cycle path has greater currency, there is a need to link with cycle routes at either end. If there aren't any, then either build them or designate a new route using existing infrastructure.
Geoff Farrell	619	Between Potsford Brook and the footpath that goes to the Gallows on the B1078 west of Wickham Market.	There is already an improved suggestion but if the landowner declines to allow walking along the field edge on the north side of the 1078, then consider opening up a part of the woodland on the south side as a right of way or permissive path.	
Geoff Farrell	620	Just north of Letheringham (the Street) on the way to the Hoo/Easton road.	There is a huge run off of wet mud from the field there and this creates an uneven, rippled and potentially hazardous surface for people on bikes.	Persuade the owner of the land/field to clear the mud on a regularly and frequently.
Geoff Farrell	621	From the bridge follow the line of the old railway up to Halesworth.	Although good footpaths and bridleways, the line of the old railway is not immediately apparent.	Join up the various footpaths and bridleways to create a cycle route between the River Blyth and Halesworth to follow the route of the railway.
George Redpath	22	B1532 (Marine Parade) in Lowestoft	This route is part of the Suffolk County Council Lowestoft Cycle route and designated a On-Road signed cycle route and approx 2km in length. Unfortunately due to lack of upgrading or maintenance around 80% of the white lines separating vehicles from cyclists have faded into the tarmac and now indistinguishable for motorists and cyclists. The only short parts of the cycle route which have been painted are those where the highways agency have completed recent road repairs see attached photo's.	Paint the white lines please along the length of Marine Parade which will link Pakefield in the South to Lowestoft town centre in the North.

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George Redpath	23	From Arbor Lane to Pakefield Rd along the current cliff top footpath	Link Pakefield (Arbor Lane) to Pakefield Road along the scenic cliff top and waterfront, with some will and a little modification to existing pedestrian infrastructure along a 1km section we could have a continuous 3km cycling route linking up to the traffic free sea-front and onto Lowestoft town centre, that is a winner for all.	Currently as you can see in the attached photographs this 1km section is narrow along parts of the route and even passing pedestrians have to step off the footpath which is also a popular route for cyclists especially school children cycling to local schools, yes I know cyclists are supposed to dismount and walk this 1km section but lets move on and grasp the nettle and make it a harmonious link for both pedestrians and cyclists from Pakefield and into Lowestoft, a win-win for all especially school children.
Gerald Wilson	31	Roundabout A47 and Corton Long Lane - to Suffolk Border before Hopton!	Cycle path ends with no path from this roundabout to the Suffolk Border above Hopton. Where on the Norfolk side there is from Gt Yarmouth a cycle path from Gorleston to Hopton and this is where it ends.	A12 upgrade to A47 never improved the cycle ways infrastructure.
Gerald Wilson	32	Lowestoft Town centre	No cycle path through precinct like there is marked out on sea front.	Designate a marked out path through Town Centre for cycles.
Gill Armstrong	391	Church Road and the Coast Road from Corton to Hopton	This is a dangerous stretch of road for cyclists and walkers as it is narrow and has several blind corners	A dedicated cycle/footpath would improve it immensely. Some years ago Sustrans proposed using the old railway lines but it never happened, this would be a good solution, if that is not possible then creating a separated route along the road would help
Gordon Fudge	78	Alderton Road just out side Hollesley	As soon as motorists leave the 30 mph zone they accelerate hard to the full 60 mph. Pedestrians have no protection. There are no pavements, the agricultural vehicles are destroying the verges and there are no footpaths through the fields that could be used as alternatives. The road is so narrow and the vehicles so fast (even the tractors drive at 60mph here and they're HUGE) that we don't dare let our 14 year old out on the road on her bike.	This is your job, not mine. Widen the road? Reduce the speed limit? Ban agricultural vehicles of a certain size or power from public roads? Build pavements?

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Graeme Borley	653	Beccles Road, Carlton Colville between Ivy Lane and the roundabout linking A1145	The footpaths linking Oulton Broad (eastern Beccles Road) with the western end of Beccles Road are not safe. The Northern footpath has become excessively narrowed by the lack of maintenance to the hedgerows between Burnt Lane and Ivy Lane resulting in impossible for a parent to walk side by side with a young child. The southern footway does not link the whole way and is hidden from the road by trees providing for an unsafe environment.	Removal of overgrown vegetation and excessive amounts of soil on the verge for the length of footpath adjacent to the field used as paddocks. Cut back the trees immediately west of Burnt Hill Way to provide a clear view of the footway to passing traffic and making a safer environment. (note Martineau Lane, Norwich incident and action taken). Extend the footpath and create cycleway link past Chaulkers Crescent all the way to the roundabout with the A1145 and Anchor Way estate. Prevent unauthorised off-road / verge parking along this route.
Graham Johnson	37	Road from A12 Blythburgh to Southwold. and most Suffolk B roads.	Country roads not suitable for cyclists. Long hold ups behind cyclists who cannot be safely overtaken on narrow winding roads with or without opposing traffic. Put simply the increase in leisure cycling is a menace to other traffic on our local roads, causing traffic jams , prolonged journey times and inefficient use of fuel when stuck in low gears behind cyclists ,and should not be encouraged. People living in the country need to get about by car. We do not need people 'playing' on our roads,	Separate cycle ways BUT not along existing footpaths. The Sustrans cycle path along Halesworth Millenium Meadow is a classic example of pedestrians and cyclists not mixing. . Cyclists all too often approach walkers(often with dogs) from behind at great speed and give no warning as they hurtle past nearly injuring pedestrians and their pets. It became so bad at one stage that we stopped walking there.
Graham Newman	199	Old Felixstowe Road (formerly A45) between Felixstowe Road/Seven Hills and Levington slip road off A14	Ideal stretch of road to introduce segregated cycle lanes &/or reduce speed limits &/or prohibit through vehicular movement other than if required for public transport or "Operation Stack" An alternative is needed to Cycle route 51 (via Stratton Hall, Levington Church and Nacton village, which although is a picturesque leisure	This was once the main A45 (now A14), the speed limit is still 60mph or 70mph in the dual carriageway near Bridge Road. This 2-mile length of road could be provided with a separated cycle lane in both directions &/or have the speed limit reduced to 20 or 30mph as it runs completely parallel with the A14 dual carriageway. I appreciate the road has historically been used for "Operation Stack", but Port of Felixstowe's Vehicle Booking System has

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			ride, is considerably longer than the direct route, and is also quite hilly in several places	largely removed the need for the road to be designated in this way 24/7/365.
Graham Newman	258	A154 Candlet Road between Garrison Lane Roundabout and Gulpher Road overbridge	The improvement required is a segregated cycle lane - an essential component for a continuous safe route between Hamilton Road (Town Centre) and the new North Felixstowe Garden Village Development and planned new leisure centre	There is ample room on both sides of the A154 Candlet Road for a segregated cycle lane between the locations suggested, but preferable on the south west side. This would link in with the existing cycle/pedestrian crossing across Garrison Lane, to link with the existing Grove Road cycle path to the Grove Medical centre, access to the Town Council's Cowpasture Allotments and my proposed segregated cycle lane alongside Garrison Lane (east side) from this point to Fairfield Avenue.
Graham Newman	259	A154 Garrison Lane (from Fairfield Avenue northbound to Grove Road roundabout) - segregated cycle lane	The suggested IMPROVEMENT is a segregated cycle route alongside the southbound side of the A154 Garrison Lane, between the Grove Road roundabout and the pedestrian entrance to Fairfield Avenue.	A safe cycle route is desperately needed between Hamilton Road (Felixstowe Town Centre and Railway Station) to the new North Felixstowe Garden Village Development and proposed new leisure centre. Part of this could be a segregated cycle lane, which is possible on the east side of the A154 between Fairfield Avenue and the Grove Road roundabout, which would link in with the signalled crossing to Taunton Road, the crossing to Cowpasture Allotments and the cycle way along Grove Road to the medical centre, Eastward Ho sports facilities and Abbey Grove woodland
Graham Newman	260	Between Glenfield Avenue and Fairfield Avenue	Signposting a cycle route	A safe cycle and walking route is desperately needed between Hamilton Road/Town Centre, the railway station and the new North Felixstowe Garden Village development and proposed new leisure centre. This is possible by using the route: Hamilton Road (Great Eastern Square) to the Railway Station, thence Station Approach, across High Road West into Glenfield Avenue, left into Fairfield Avenue. At the northern end of Fairfield Avenue, open up existing pedestrian access onto a segregated cycle route alongside the A154 Garrison Lane northbound to the Grove Road roundabout, linking in with the existing signalled pedestrian crossing to

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				Taunton Road/Candlet Road (with proposed segregated cycle lane as far as Gulpher Road overbridge) - also linking in with the crossing to the Cowpasture Allotments and existing Grove Road segregated cycle lane to the medical centre, Eastward Ho and Abbey Grove.
Graham Newman	265	Blofield Track (from Cordys Lane, Trimley St Mary to Nicholas Road, Port of Felixstowe Campus	Upgrade to decent surface for the whole extent - this is a bridleway (BW12) much used by cyclists from Trimley to the Port which avoids busy main roads, but the surface is very poor.	The first "section", as far as the north-west entrance to the new Gun Club site (near the junction with FP30), is very rough, with large stones exposed. The second "section", from the north-west entrance to the Gun Club to the railway overbridge is basically a mud track, which is often flooded at, and near, the railway bridge - making it barely passable. The third "section" from the railway bridge to Pentalver's Yard on Nicholas Road is generally in better condition, although it does puddle in places. The entire extent needs surfacing with an all weather surface.
Graham Newman	266	Upgrade Bridleway 12 (Trimley St Mary) - from Cordys Lane, Trimley St Mary to Nicholas Road on the Port of Felixstowe campus -to all weather surface	Very poor surface on this bridleway, much used by cyclists and walkers avoiding the busy Trimley High Road/High Street/High Road West/Garrison Lane/Langer Road/Walton Avenue route from the Trimley villages to the Port of Felixstowe	The first "section", as far as the north-west entrance to the new Gun Club site (near the junction with FP30), is very rough, with large stones exposed. The second "section", from the north-west entrance to the Gun Club to the railway overbridge is basically a mud track, which is often flooded at, and near, the railway bridge - making it barely passable. The third "section" from the railway bridge to Pentalver's Yard on Nicholas Road is generally in better condition, although it does puddle in places. The entire extent needs surfacing with an all weather surface.
Graham Newman	267	Capel Hall Lane/Brook Lane/Back Lane/Lower Road	Create a network of Quiet Lanes between Trimley St Martin (Capel Hall Lane) and Falkenham Church via Brook Lane/Back Lane/Lower Road/Falkenham Sink	As above - requires only designation and signage.
Graham Newman	270	Trimley St Mary Bridleway 14: Clickett	The area immediately to the west of Clickett Hill Road becomes very damp and muddy over	As above

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		Hill Road to Nicholas Road	the autumn-winter-spring period and needs to be surfaced - as part of Suffolk Cycle Route 5	
Graham Newman	425	Entrance to Peewit Caravan site to former Beach Station (past Felixstowe Beach Holiday Park)	Unclear as to whether cycling is allowed on the "footway"	Cycling is allowed on the footway between McDonalds/Dock Gate 1, in front of Lidl's supermarket, the JW Kingdom Hall, up as far as Peewit Caravan site approach road. It is then unclear whether cycling is allowed alongside Beach Holiday Park, although there is no difference in the width of the footway. Solution: clarification/additional signage needed
Graham Newman	426	Walton Avenue (A154) between a point SE of Dooley Road NW towards Dock Gate 2 roundabout	For no apparent reason, the ability to cycle on the footway stops just short of Dooley Road (in front of Wincanton depot), along the frontage of China Shipping House, as far as just before Dock Gate 2 roundabout. No significant change in the width of the footway	Link up these two sections of cyclistoute, to avoid having to cycle on the highway between these two points: Walton Avenue (A154) is heavily trafficked with HGVs and other Port related traffic (but very few pedestrians). This (and my other proposals) would lead to a continuous off-road cycleway all the way from the railway crossing at the NW end of Fagbury Road through to the former Beach Station and Beach Station road, around the busy environs of the Port.
Graham Newman	797	Wilford Peninsula	I have completed the map based consultation with several suggestions on the Felixstowe peninsular, but I also wonder about a possibility on a rather grander scale... It all rather depends on how ambitious you want this cycling and walking strategy to be!!	<p>What are the barriers to creating a ground-breaking "Cycle Country" on the Wilford Peninsular? Starting at Wilford Bridge, bounded by the River Deben to the south/south-east; the coast line from Bawdsey to Aldeburgh; the A1094 to Snape Crossroads; and the B1069/A1152 back to Wilford Bridge.</p> <p>I appreciate this is a massive area, but it would create an equally massive leisure and tourist attraction in what is arguably some of the best Suffolk countryside. Other than agriculture and the Bentwaters Airfield business park (accessed via the A1152), there is precious little industry requiring fast-moving access within this area. It could perhaps comprise of a 30mph designation for classified roads within the area (eg on the B1063, B1078</p>

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				& B1084) and quiet lane status with a 20mph recommendation elsewhere.
Great Bealings Parish Council	670	East Suffolk	<p>We are of the opinion that in a period of scarce resources we consider that the three priorities for walking should be as follows:</p> <ol style="list-style-type: none"> 1. Improve existing PROWs by maintaining gates, stiles, finger posts and signage and clarify who can and cannot access PROWs. 2. Ensure land owners co-operate with this maintenance and engage with SCC highways on how to improve condition of PROWs on their land. 3. Ensure Town and Parish councils appoint PROW officer and make sure public are aware who to contact. 	<p>We note that there are already a number of cycling routes supported by ESC and SCC and these should also be given greater publicity. Greater use of cycling is a much more complex issue that requires a high level strategic approach across all relevant councils. As a small parish we can support and publicise these routes as required.</p>
Great Glemham Parish Council (John Cross)	792	East Suffolk	<p>Good Issues</p> <ul style="list-style-type: none"> - Cheap - Good lights & clothing - Short journeys don't take longer than car - Healthy - Fun in good weather - Reduced oil consumption - Panniers & back packs assist shopping <p>Bad Issues</p> <ul style="list-style-type: none"> - Punctures - Hills and inclines (e-bikes help!) - Bad weather - Aggressive driving - Most local roads have 60mph limit - Hard verges and kerbs reduce vehicle options when being over-taken or vehicle approaching from opposite direction (cars rarely wait for cyclists, agricultural vehicles NEVER do) 	<p>I suggest that we start by looking at short journeys of 5 miles or fewer. This could include travelling to work or school and daily and intra weekly shopping trips for most people in East Suffolk. To provide encouragement, the following notes may help:</p> <ul style="list-style-type: none"> - Direct cyclists to cycle-friendly routes? - Can we provide shopping discounts for people who arrive by cycle or walk? - Encourage more frequent shopping trips for lighter, smaller loads - Shop close to home – you're saving on fuel to compensate for any higher prices - Have we got enough cycle racks? - Are they fit for purpose and in a suitable place? <p>Cycle routes don't require lots of infrastructure, but the following help greatly:</p>

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				<ul style="list-style-type: none"> - Appropriate signage at each end of the route - Preferably NOT along 60 mph roads - Quiet lanes are perfect - Soft level verges (assuming single carriageway roads) - Well-maintained road surfaces - No hedge cutting using flails!!!
Guy Foscett	399	between The Avenue and Loudham lane Ufford. the hedge on the west side need cutting back. there are branches and brambles that stick out which cars coming down loudham lane push you into.ut	the hedge on the west side need cutting back. there are branches and brambles that stick out which cars coming down loudham lane push you into.	cutting hedge
Halesworth (NP policies)	802	Loam Pit Lane	Link residential areas to the main town destinations and the NCR1	Loam Pit Lane - make it into a cycle route so connecting Harrisons Lane to Holton Road, serving the new housing on Harrisons Lane (planning permission given), connecting the new sports centre on the Campus site, with a planned cycle route within it, into Loam Pit Lane. This may partially utilise/link into the development intentions by Hopkins Homes Ltd at Blyth Vale (off Hill Farm Road), by linking across the west end of the cemetery and Loam Pit Lane, onto open space areas created by the Hopkins development, and linking towards Holton.
Halesworth (NP policies)	803	Briar close	Link residential areas to the main town destinations and the NCR1 - Improve access into Briar Close and the route to the Station by improving the pavement under the railway bridge and its connection to the end of Loam Pit Lane.	
Halesworth (NP policies)	804	Hill Farm Road Development	Link residential areas to the main town destinations and NCR1	Hill Farm Road development – create a path from this new estate and the proposed playground west into Loam

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				Pit Lane to connect to the proposed new path east to Holton Orchards Road so improving cycling access to and from the east of town and from Holton.
Halesworth (NP policies)	805	new development at Chediston Street/Roman Way	Link residential areas to the main town destinations and the NCR1	The proposed new development at Chediston Street/Roman Way includes suggestion for a cycle route up Chediston Street into the town centre. This is considered dangerous and an alternative route should be planned. From the estate a route should be created into Allington Road. This makes best use of the contours of the land and connects into Dukes Drive near to the bus stop. It would then cross Roman Way to connect to the existing cycle route in Holmere Drive and into Church Farm Lane. This creates a relatively safe cycling route into the Market Place and town centre via the quiet northern end of London Road around the St Mary's Church yard.
Halesworth (NP policies)	806	Bramfield Road (A144)	Link residential areas to the main town destinations and the NCR1.	Create a route down Bramfield Road (A144), to the Mells/Walpole Grange Road crossroads, making use of Durban Close if required. This would connect directly to the NCR1 route going south towards Walpole and into the Blyth Road industrial estate and on into the Millennium Green.
Halesworth (NP Policies)	808	River Lane	Improve cycling connectivity into the Town Park and the Millennium Green which has NCR1 running through it and out into the countryside beyond.	Make River Lane into a cycle route. This would connect the Angel Link roundabout on Saxons Way to the park. The bridge over the river is too narrow at present but could be given pedestrian right of way or cyclist dismount signs until the bridge can be widened. River Lane is a private road and is unregistered on the land registry, so investigations needed to start the process of redesignation.
Halesworth (NP Policies)	809	Blyth Mews / Quay Street	Improve cycling connectivity into the Town Park and the Millennium Green which has NCR1 running through it and out into the countryside beyond.	Make Blyth Mews off Quay Street into a cycle route with appropriate signage. The bridge over the Patrick Stead Lock at the end of Blyth Mews would benefit from improving with 'cyclists give way to pedestrians' signs, if

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				<p>funding is not available for widening).</p> <p>Look at how the Blyth Mews/Quay Street cycle and pedestrian routing could be improved so safer/easier access could be made into Station Road, giving access to the railway station and 'The Cut'. If the car showroom site opposite Blyth Mews was developed this could give an opportunity.</p>
Halesworth (NP Policies)	810	The Folly / Millennium Green	Improve cycling connectivity into the Town Park and the Millennium Green which has NCR1 running through it and out into the countryside beyond.	<p>Create a new cycle route through the Folly which is a part of and the Millennium Green. This would connect Holton Road opposite the new housing development at Hill Farm Road into the Millennium Green and into the town centre or out on the NCR1. Footpaths off the Holton Road (towards 'Rails End') would need to be redesignated as cycle routes. The owners would need to be approached.</p> <p>Routes into and then through to the town park and Millennium Green also give an alternative to the Saxon Way route for less confident cyclists and a route out to the east of town.</p>
Halesworth (NP Policies)	811	Roundabout at Quay Street up the Norwich Road to Sparrowhawk Road	<p>Create a direct and safe 'key movement' cycle route from the Sparrowhawk Road roundabout in the north to the Bramfield Road/London Road intersection in the South.</p> <p>This would reduce the 'inconsistent and confused approach for cyclists and pedestrians' and thereby reduce conflict for all users' as they navigate the Town Centre (Waveney Local Plan). Rerouting of NCR1 would be needed.</p>	<p>Cycle route from the roundabout at Quay Street up the Norwich Road should be on the west side of the road. The partial and inadequate cycle route that goes up to Harrisons Lane on the east should be decommissioned as dangerous.</p> <p>The west side of the road would solve some of the issues for children cycling to school. At present they cannot cross safely from the present cycle route to the school. Poor parking on the west side of Norwich Road (from Edgar Sewter Primary School to 'The Avenue'), caused by overspill from the Police Station, businesses in town, and by parents dropping children off at school, would need to be resolved.</p>

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				This route would become a re-routed NCR1 doing away with the confusing route down Harrisons Lane into Holton and then up to Sparrowhawk Road. At Sparrowhawk Roundabout the NCR1 route could go up the road in front of the Triple Plea pub and join the present NCR1 route at Butts Road in a more direct and straightforward route towards the railway Mill Post Crossing.
Halesworth (NP Policies)	812	Saxons Way	Create a direct and safe 'key movement' cycle route from the Sparrowhawk Road roundabout in the north to the Bramfield Road/London Road intersection in the South.	<p>The pavements along Saxons Way, from Quay Street roundabout to the Coop/London Road roundabout should become safe, shared cycle and pedestrian paths. (the east side of Saxons Way may be the best option as it links with the proposed east side route on London Road and would not impinge on the entrance to the new development on the west side or the entrance to the car park).</p> <p>The Saxons Way route would remove the confusing one way cycling in the Thoroughfare and the dismount instruction at the southern end of the Thoroughfare.</p> <p>The route should then continue along the eastern side of London Road to the turning with Bramfield Road (the main route into Halesworth from the A12)</p>
Halesworth (NP Policies)	813	Thoroughfare / Bridge Street	<p>Reroute the NCR1 away from the Thoroughfare / Bridge Street.</p> <p>The rerouting of NCR1 away from the Thoroughfare/Bridge Street between the Quay Street and the entrance to the car park removes a confusing and badly signposted national route from a semi pedestrianised shopping street and allows for the</p>	

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			Thoroughfare to become safer and more pedestrianised route.	
Halesworth (NP Policies)	814	Market Place	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance cyclist groups to use Halesworth as a stop off destination.	An option is to provide E-chargers along the wall of the 'Boarding House' café, where there are currently market stallholder electrical outlets already provided. An alternative could be along the wall of the Wine Shop/public toilets on the opposite side of the Market Place.
Halesworth (NP policies)	815	Central (main) Thoroughfare carpark	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance cyclist groups to use Halesworth as a stop off destination.	There is a substation in the central carpark, plus numerous businesses, that potentially could facilitate E-charging points. Ideally these could be along the river side wall (north) of the car park.
Halesworth (NP policies)	816	Angel Link carpark	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance cyclist groups to use Halesworth as a stop off destination.	Discussions highlight this car park as considerably underused, and there remains the potential for a bus terminus here, despite past failed attempts (which should be refreshed). In addition there is plenty of scope here for E-chargers to be positioned in numerous places, to attract town centre visitors to use this under-utilised space. An ideal position might be along the boundary to the Angel Hotel private carpark. Alternatively, there could be scope for E-chargers in what I believe is called 'Angel Lane South' carpark behind the EACH charity shop.
Halesworth (NP Policies)	817	Bridge Street	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance cyclist groups to use Halesworth as a stop off destination.	Cyclists would benefit from the addition of perhaps a 3-4 cycle toast rack positioned on the town river bridge, which is the widest section of the main street. If carefully positioned on the upstream side of the bridge, it was felt these wouldn't encroach on vehicular flows or the pedestrian access across the bridge and viewing the river.

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Halesworth (NP Policies)	818	Market Place	Increase and improve cycle parking, including e-bike parking and charging, at key destinations and in the Thoroughfare/Market Place to encourage cyclists to make short journey's into town, support the cafes and businesses and the encouragement of long-distance cyclist groups to use Halesworth as a stop off destination.	There is currently a 3-4 bike toast rack store adjacent to the Market Place pump. The storage capacity could be significantly boosted in the Market Place, possibly by taking up a parking bay adjacent to the Wine Shop. This would provide enough space for a 10 (or more) bike toast rack.
Halesworth (NP Policies)	807	Wissett Road junction	Create a direct and safe 'key movement' cycle route from the Sparrowhawk Road roundabout in the north to the Bramfield Road/London Road intersection in the South	The Wissett Road junction should be made into a Copenhagen style junction giving priority to cyclists and pedestrians. This would encourage safer cycling to the primary school by children and parents.
Halesworth Neighbourhood Plan Group	738	West and north of Halesworth	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Formalise newly devised circular walks to the West and North East of the town, that use existing public rights of way through SCC map creation. (working with the SCC PROW team to commission new maps).
Halesworth Neighbourhood Plan Group	740	Town Centre to Millennium Green	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Support the improvement to the routes and connectivity from the Town Centre to the Millennium Green (see Objective 7) so encouraging more use.
Halesworth Neighbourhood Plan Group	741	Green corridor / walking route	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Designate land that would support the creation of a green corridor/walking route around the South/Western edge of the town. This supports Objective 1 (biodiversity) and 5 (protection of entrance views to the town if future development of farming land was permitted). It could run from Chediston St, behind the backs of Dukes Drive, crossing Walpole Road and continuing behind the backs of Kennedy Avenue and Close linking to the proposed nature reserve and footpath to the Basely sports ground.

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Halesworth Neighbourhood Plan Group	742	Blyth Valley towards Walpole	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Explore the possibility of the above linking to a footpath along the Blyth valley west towards Walpole. (flood risk may make this unviable and land ownership not known).
Halesworth Neighbourhood Plan Group	743	Halesworth	Make Halesworth a 'walking hub' with a network of walks within the town, circular walks around the town and footpaths out into the countryside connecting to neighbouring villages, improving the health and wellbeing of residents, and supporting the town as a tourist destination.	Rationalise the walking maps available so they can form a suite of information online and in leaflet form and that reference each other. Some are signposted. Some need updating. Some have a specific historical focus. NB Subsequent agreement to work with Green Access team at SCC to produce a leaflet of circular walks for the Discover Suffolk website and to digitise the other leaflets so they can be accessed on the same website. Erect well designed and coherent signage once the maps and routes are finalised. (not a planning matter but a potential use of CIL money).
Halesworth Neighbourhood Plan Group	745	Harrisons Lane to Loam Pit Lane	Hill Farm Road, Fairview Road,(being built) Chediston St/Roman Way, Harrison's Lane/Town Farm (with outline planning) are the new developments. Attention has been given to walking connections from Hill Farm Road (this better connects Halesworth and Holton and tries to ensure children can walk to the two primary schools) and to Fairview (a rather disjointed pavement/cycle track around the corner of Fairview Road that doesn't really connect).	Where gaps exist ensure safe and connected pavements from the main residential areas to the town centre and key destinations. Harrisons Lane housing and sports developments need to connect with Loam Pit Lane and the east west routes. There is concern about the poor considerations given to walking and cycling in the Chediston St development plans.
Halesworth Neighbourhood Plan Group	746	Allington Road to Dukes Drive	Where gaps exist ensure safe and connected pavements from the main residential areas to the town centre and key destinations.	Routes need to make use of the lie of the land and connect with Allington Road onto Dukes Drive to connect with the bus stop. The proposed cycle route up Chediston Street is strongly opposed by the Cycling Group as the road is too narrow).

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Halesworth Neighbourhood Plan Group	747	Wissett Road down to Old Station Road	Where gaps exist ensure safe and connected pavements from the main residential areas to the town centre and key destinations.	Create a new pavement down the northeast side of Wissett Road from the entrance to the Children’s Centre down to Old Station Road. This can improve the existing tarmac path at the top end, better connect it to the pavement in Wissett Close which goes down to Chichester Road and then use the wide verge down to Old Station Road. This would give safer walking for the Chichester Road estate and help connect with country footpaths around Wissett.
Halesworth Neighbourhood Plan Group	748	Pavement down Norwich Road	The existing pavement from Norwich Road down to the Children’s Centre entrance is very narrow in parts and should be looked at to see if some widening could be done.	Where gaps exist ensure safe and connected pavements from the main residential areas to the town centre and key destinations.
Halesworth Neighbourhood Plan Group	749	Entrance to Wissett Road	Make walking, cycling and scootering to Edgar Sewter primary school a safe and healthy option for children and parents. Based on consultation with years 5 and 6 children and with parents in the walking expert group the following are the suggestions as to how to achieve this policy. The planned extension to the school gives an opportunity to make changes. The proposed rerouting of the cycle track up the West side of Norwich Road could support these changes.	Create a safe crossing (zebra, pelican, toucan, Copenhagen) across the entrance to Wissett Road – this is a busy and narrow junction with Norwich Road especially at school start and finish times, with cars backing up down Wissett Road, and cars turning into Wissett Road from both north and south into the very narrow entrance.
Halesworth Neighbourhood Plan Group	750	Norwich Road School Entrance	Make walking, cycling and scootering to Edgar Sewter primary school a safe and healthy option for children and parents. Based on consultation with years 5 and 6 children and with parents in the walking expert group the following are the suggestions as to how to achieve this policy. The planned extension to the school gives an opportunity to make changes. The proposed rerouting of the cycle	Create a safe crossing on Norwich Road in front of the main entrance to the school – at present the nearest crossings are at the Quay Street roundabout and at Harrisons Lane (installed for the former middle school). This would support walking options from the east of the town and new developments at Harrisons Lane where walkways will enable children to commute onto Bungay Road and up the path just north of the school but on the ‘wrong side’.

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			track up the West side of Norwich Road could support these changes.	
Halesworth Neighbourhood Plan Group	751	Wissett Road	Make walking, cycling and scootering to Edgar Sewter primary school a safe and healthy option for children and parents. Based on consultation with years 5 and 6 children and with parents in the walking expert group the following are the suggestions as to how to achieve this policy. The planned extension to the school gives an opportunity to make changes. The proposed rerouting of the cycle track up the West side of Norwich Road could support these changes.	Create a safe crossing across Wissett Road to connect the footpath from Rectory Street to the Children’s Centre entrance to school – this makes for a safe and healthier route from the South of the town along the Thoroughfare and connects with the pre-school in School Lane.
Halesworth Neighbourhood Plan Group	752	Norwich Road	Make walking, cycling and scootering to Edgar Sewter primary school a safe and healthy option for children and parents. Based on consultation with years 5 and 6 children and with parents in the walking expert group the following are the suggestions as to how to achieve this policy. The planned extension to the school gives an opportunity to make changes. The proposed rerouting of the cycle track up the West side of Norwich Road could support these changes.	Make a 20mph zone along the Norwich Road in front of the main school entrance preferably from the Quay Street roundabout to The Avenue or beyond.
Halesworth Neighbourhood Plan Group	753	Thoroughfare	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Pedestrianisation of the Thoroughfare, (Objective 7 and 6).
Halesworth Neighbourhood Plan Group	754	Saxons Way from Lansbury Road estate	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Dangerous crossings identified across Saxons Way from the Lansbury Road estate, which has several homes for elderly people. Make the crossing from Swans Lane, presently a central island, into a zebra or pelican.
Halesworth Neighbourhood Plan Group	755	Roman Way / London Road	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Dangerous crossings identified across Roman Way where it joins London Road which is the main route out of town to the A143 and on towards the A14. A crossing is

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				needed to help walking from the estates down Walpole Road.
Halesworth Neighbourhood Plan Group	756	Halesworth	Support elderly and less mobile residents with safe accessible footpaths, pavements, and crossings.	Styles, gates and seats – better designs required to make walking in the countryside easier for the less mobile and more seats around town to encourage more walking to shops etc. (advice needed on what a NP can do on this)
Halesworth Town Council (NP policies)	801	footpath between Uplands Way and Norwich Road	Link residential areas to the main town destinations and NCR1 - Upgrade the footpath between Uplands Way and the Norwich Road alongside the school fence to create a shared pedestrian and cycle path, with signage. This would connect the Chichester Road residential area, support cycling to school, help cyclists coming from the Wissett area to avoid the dangerous and steep Wissett Road.	
Harry Grainger	666	Lowestoft	The improvement that I feel needs making is that whilst it is reasonably possible to cycle within Lowestoft it is virtually impossible to cycle away from Lowestoft to any significant or interesting destination.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.
Harry Grainger	667	Between Haddiscoe and Reedham via Somerleyton	The marshes between Haddiscoe and Reedham via Somerleyton involves cycling along 'car fast' narrow lanes which have no provision whatsoever for cyclists, and any attempt to avoid fast roads involves miles of detours with in real terms no real gain in safety.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.
Harry Grainger	668	Lowestoft to Southwold	Lowestoft to Southwold involves large detours to avoid the A12 from Kessingland but eventually arriving at a very dangerous crossing of the A12 at Wrentham followed by several miles of very dangerous travel along the B road to Reydon and Southwold. again there is no provision whatsoever for cyclists.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.

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Harry Grainger	669	Lowestoft to Ellough	Cycling to the Ellough farmers market from Lowestoft. The majority of this route is currently satisfactory despite no obvious provision for cyclists once out of Lowestoft, but at the end cyclists are deposited onto a very fast busy B road with no provision for cyclists.	If cycling is to really be taken seriously we need to take the European approach and simply stop prioritising cars over pedestrians and cyclists. Cycle routes need to be delineated from beginning to end and where there are issues of space cycling and walking should be given clear priority.
Haydn Morris	39	B1121 main road linking Benhall, Saxmundham, Kelsale	Lack of safe pedestrian/cycling route between Benhall, Saxmundham, Kelsale, Lack of cycling infrastructure (signs, secure parking.cycle lanes) East Suffolk Council, Suffolk County Council Highways Dept, Planning Dept do not seem to communicate with each other - a perfect example of this is the new train station in Saxmundham has no provision for secure bicycle parking.	The 3 Communities Link project report was completed in 2017 - it detailed a safe route between Benhall, Saxmundham, Kelsale for pedestrians and cyclists. It also linked to the local schools and Saxmundham railway station. The report is currently sitting with Suffolk County Council and has been included in their list of 100 cycling projects to be delivered in the next 5 years (see EADT article.) The report has been ratified and costed by SCC/Highways and is still awaiting funding. It is an "oven-ready" solution to the transport infrastructure issues in and around Saxmundham I am the author of the report file:///media/fuse/drivefs-234088169dc1f109c9a130868367d4ad/root/THE%20%20COMMUNITIES%20LINK%20Impact%20Audit%20&%20Report.pdf Our FB page: https://www.facebook.com/SaxTCCFocusGroup
Helen Hudson	383	Denmark Road cycle path from station to Rotterdam Road	This must be the worst and most dangerous cycle path in the country. It is extremely uneven and shakes bones and bikes unbearably. There is also a concrete	Re-lay the path and drop the kerbs where required. Not sure what the obstruction is so unsure if it can be moved. Maybe designate the path on the opposite side as a

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			obstruction along with at least one place where the kerb has not been dropped.	shared footpath/cycle path as it is plenty wide enough along most of its length.
Historic England	794	East Suffolk	As a result of the number of consultations we are currently receiving, we regret that we are unable to comment specifically at this time.	
Iain Johnston	624	Wilford Bridge - Access to the peninsula/ Suffolk Coast AONB	The Suffolk Coast AONB is becoming more and more popular for cycling, both on trail and the road. There is minimal signage and road marking to highlight or protect the cyclists on this network of roads and trails.	Additional signage on the main routes onto the peninsula, Wilford Bridge being one, to warn motorists that they are entering a high cycle area. Motion activated signage akin to the speed warning signs that are prevalent on entry to low speed limit areas. Central Bedfordshire Council have used Swarco Ltd signs of this nature. There are also many "high risk" sections of road that comments have already been placed on. eg uphill stretches, entry into wooded sections, blind summits and corners. Again, road markings or signage to highlight additional awareness for bikes would be of real benefit. Finally, as has been noted in other comments, the villages on the Suffolk AONB lack a safe / marked cycle route on the main roads such as B1083, B1084, Heath Road for commuting cyclists. These users may be distinctly different from recreational users and travel at slower speed and so require better protection.
Iain Johnston	625	At Red Lodge, where the road becomes bordered by the wood	As noted in other comments, this is a very fast section of road, popular with cyclists. I have been witness to near misses with cars on a number of occasions. The change in light as a result of coming into or leaving the trees, leaves cyclist or other road users extra vulnerable.	Signage or road markings to highlight this would be of benefit.
Iain Johnston	626	The corners and ascent into the forest at Spratt's Street	High risk point for cyclists: fast driven corners meeting slow moving cycles and change in light conditions as a result of the trees.	Signage or road markings to highlight this would be of benefit.

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Ian Day	41	A1214 Rushmere / Kesgrave	Great historic cycle lane adjacent to this road that would be greatly improved by changed priorities on minor road junctions to prioritise cycles. The route reduces in width to an ordinary (shared) pavement at Rushmere Heath creating a significant gap in infrastructure.	Changed priorities on side roads and new, protected cycle lane at Rushmere Heath.
Ian Day	42	B1438 Melton Road Woodbridge	This road is a significant link between Woodbridge town centre and Melton. The road is very wide but has no cycle infrastructure or any reasonable alternative routes.	Provide good quality cycle infrastructure and Cycle advanced stop lines at traffic lights either end. May need to consider on street parking and the narrowing road at the Woodbridge end.
Ian Day	45	Wilford Bridge Road, Melton	Popular route for recreational cycling without any cycle infrastructure. This road provides access to the railway station at Melton and is the only direct route between the populated areas of Ipswich / Woodbridge and the coast and forests that are so important for recreation. Very hostile road for cyclists with blind bends and double white line no overtaking restrictions. May be possible to open up the riverside path as alternative from Woodbridge?	Good quality cycle infrastructure replacing existing pavement between roundabout and the level crossing.
Ian Day	54	Retail areas, Martlesham Heath	All of the retail developments on Martlesham Heath have been created with large car parks and have each been created independently with no joined up approach to movement from one development to another. For example moving from B&M to Tesco is only a short distance but a lack of pavements and crossing points means that walking is a very hostile experience. People drive very short distances from car park to car park.	A new pedestrian / cycle crossing between Tesco and Pets at Home. New crossings between Poundland and Next. New crossings between M&S and B&M. The B&M development has no pavement access at all.

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			MH has good foot cycle access until you actually get to the shops!	
Ian Hopgood	170	Cumberland Street	Drivers consistently ignore the time restrictions and use this route as a rat-run.	Turning the road into fully 1-way from North-East to South-West would reduce it's desirability as a rat-run - but continue to allow 2-way bicycle traffic
Ian Hopgood	171	The Thoroughfare	Cars using the road as a rat-run	Reversing the one-way direction would remove the routes desirability as a rat-run.
Ian Hopgood	213	River Wall path between Wilford Bridge and Woodbridge	This is currently a footpath, but could be changed to allow bikes.	Keeping the current surface would help to limit bike speed. Having a green cycle route between Melton& Woodbridge would provide relief from the poor road conditions.
Ian Humphreys	605	Colneis Road from Ferry Road to Beatrice Avenue	Parked cars on both sides, especially near Kingsfleet and Colneis schools, also high speed of traffic at any time. Children, from the expanding Laureate Fields development, will be in danger when cycling to the Academy.	Mandatory cycle lanes would improve safety for all cyclists.
Ian Poole	85	A1214 between Playford Road and Bent Lane	No cycle lane but one exists to the east and to the west	Widen footways to create dedicated cycle path
Ian Shaw	799	East Suffolk	<ol style="list-style-type: none"> 1. despite recent resurfacing work there are many pot holes on back lanes 2. again on many lanes there is a build up of sand, gravel or tree debris 3. in autumn when farmers cut hedges the tractor powered methods strew the road with sharp fragments which create a very significant risk of punctures not only to bikes but also cars 4. signposts, so valuable to anyone not relying on sat-nav, are increasingly corroded through and lying in the verge 	<ol style="list-style-type: none"> 1. Cyclists are aware of road condition and promoting the existing SCC online reporting tool amongst them would reduce the need for staff to carry out road surveys. 2. Reduce verge cutting, which is prejudicial to wildlife, spend it on sweeping roads free of sand and flints which are a particular problem with the local geology. 3. Anyone strewing a road with tacks would soon be subject to enforcement action so it seems strange that there are no moves to deal with the hacking of hedges with no regard to the state the road is left in. 4. Signposts are in a poor state. If there is insufficient money to replace, an imaginative solution needs to be found. Perhaps a plastic insert to reconnect the tubular uprights on an interim basis? 5. A new, imaginative look at our roads needs to be

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				promoted rather than just doing, or not doing, what always has been. While much of the direct responsibility for remedial work lies with higher tiers, is E.S.C. supportive of the objectives?
Isla McMillan	585	Junction of Lime Kiln Quay Road, St John's Street and Thoroughfare	I use this junction several times a week as I'm cycling home from work. I have to go straight over onto the main bit of the Thoroughfare, so I have to wait on the right-hand side of my lane, which is absolutely terrifying. There is no space for cyclists and the traffic turning from Lime Kiln Quay passes so close to me – it's particularly scary if it's a bus!	
Isla McMillan	586	Theatre Street and Burkitt Road	I either cycle or walk my son to playgroup at St Mary's Primary School before I head off to work. Walking or cycling are both a bit hairy as the traffic often zooms by on this stretch – there's no indication of what the speed limit is so people take that as licence to go as fast as they please – often speeds in excess of 30mph. This is a busy stretch filled with children on the way to Farlingaye and St Mary's – please put up a 20mph sign!	Please put up a 20mph sign!
J Jacobs	136	New cycle lane barriers	The barriers are an improvement of sorts except that they seem to give drivers the impression at they can drive as close to them as they like! If you have a bike with 2 full panniers, it is difficult to join and exit through the barriers.	Make the cycle lanes wider and improve entrance and exit areas especially near roundabouts. General comment for ALL cycle lanes - STOP any vehicles parking in them!
jackie catterwell	79	River path Kyson to Wilford Bridge	Thank you for the no cycling signs on the Kyson part of this path. Some clear ones are needed on the Wilford Bridge section.	If you are going to allow cycling here then you need to keep cycle and pedestrian paths separate as very dangerous otherwise, as I have often found!
jackie catterwell	80	Melton to Martlesham road	Not a problem for me but many others say they won't cycle on the main road from Melton to	Provide a designated cycling space on main road from Melton, though Woodbridge, meeting up with the

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			Woodbridge as there is no designated space for them.	cycling section in Martlesham, which then goes to Ipswich.
Jacqueline Tricker	268	The pedestrian crossing island near Pytches Road does not give priority to cyclists who feel vulnerable as motorists try to narrowly overtake even if cyclists take up a central position to prevent this. Nicknamed "Cycle crushers"	Problem is cars overtaking cyclists too narrowly through the gap between the island. Either spend lots of money, like the Dutch, on engineering a proper cycle way or put a sign up giving cyclists priority over motorists. I have been the victim of a road rage incident here. The Police blamed me for hogging the road. I was preserving my life.	Highway code change imminent to support cyclists who take up central position? Sign to prioritise Cyclists. Better (eg more expensive) planning/cycle way engineering as in NLs.
Jake Daws	63	Main road Kesgrave from Martlesham to Ipswich hospital	You talk about cycling strategies to improve access- I have reported this many times over the years about the poor state of the cycle path and poor condition potholed surface on Kesgrave to Ipswich main road cycle path. It's simple- improve cycling numbers by providing Dutch style standard surfaces to cycle on. No more cycle repairs due to rubbish poorly maintained cycle paths like this one!!!!	I've mentioned this as above
James Hall	167	X-roads on B1078 with Gibraltar Rd. Otley and High Rd. Swilland.	V. dangerous junction because of speed of traffic and overtaking on B1078 .	Extend the speed limit of 40 mph at the Ashbocking x-roads so that it continues all the way to the 40 mph limit near Otley College.
James Richards for Ramblers Association, Alde Valley Group	97	Westleton. Between Reckford Bridge (TM436677) and the start of Black Slough (TM438679)	Walkers wishing to link between Footpath Westleton 25 (Reckford Bridge) and Bridleway Westleton 26 (Black Slough) have to walk along a dangerous stretch of the B1125 where there is no space for pedestrians around a tight bend.	A public footpath of 0.12 mile between Reckford Bridge (TM436677) and the start of Black Slough (TM438679) must be created inside the hedges of the farm land to provide a safe alternative to walking along the busy carriageway of the B1125 between Public Footpath Westleton 25 and Bridleway Westleton 26 and enable valuable circular walks around Middleton, Eastbridge, Minsmere and Westleton to be walked safely. The danger here will be worsened even more if the B1125 is to carry construction traffic for Sizewell C.

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James Richards for the Ramblers Association, Alde Valley Group	468	River Wall - eastern side of Butley River. The path along the river wall between the points TM 393 505 and TM 396 485	This section of river wall is blocked off to the public by fencing. Its omission from the Definitive map could simply be an anomaly as the route recorded on the Definitive Map as Chillesford Footpath 18 stops abruptly at the Chillesford/Gedgrave parish boundary which is absurd.	This route must be added to the Definitive map by way of a Creation Order or Agreement. The proper recording of this route would enable a fine circular walk linking Chillesford and the Butley Ferry.
James Richards, for Ramblers Association, Alde Valley Group	475	Ramsholt to Bawdsey – The stretch of river wall from Ramsholt to Bawdsey on the Deben	There is no public access along this stretch river wall	This should be made available to the public to connect with existing routes and become part of the England Coast Path. This section of river wall is not currently open to the public but could be made a public footpath with a minimum of alteration and expenditure with no inconvenience to the landowners. A Creation Order or Agreement is required. It will have a good deal of support from local residents as well as visitors.
James Richards, for Ramblers Association, Alde Valley Group	483	9 points on Saxmundham bypass: TM380656 Kelsale FP 10; TM373646 Kelsale FP 38; TM376644 Kelsale FP 1; TM375639 Kelsale FP 3; TM375636 Sax FP 5; TM375632 Sax FP 11; TM376630 Sax FP 13; TM377621 Benhall FP 22; TM378616, Benhall BR 25	Paths severed by A12 bypass with no thought for walkers. Crossings lethal- single carriageway with 60 speed limit. No warnings to motorists- no central refuges- in two instances (TM 376 644 and TM 375 636) one must climb over Armco-type barriers on each side. TM 375 632 crossing is oblique requiring a considerable walk alongside the carriageway to cross it at a right angle. Traffic increased many fold by new housing on western edge of the town. Sizewell C traffic would exacerbate more.	These crossings must be made safer and easier through speed limits, warning signs to motorists, provision of gaps in the Armco barriers and the installation of central refuges and waiting areas.
James Richards, for Ramblers Association, Alde Valley Group	486	Bridleways Sudbourne 12 and 13 near the site of the old Marsh House. On Sudbourne Marshes linking Sudbourne village with the river wall.	Near where Bridleways 12 and 13 meet they cross dykes one of which is difficult and dangerous to cross even in the driest of weather. A bridge is required. Attempts were made in the 1990s to downgrade the path to a footpath so that a new footbridge would solve	A bridleway bridge needs to be constructed to enable these paths to be linked.

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			the problem at a much lower cost. This was objected to and never took place.	
James Richards, for Ramblers Association, Alde Valley Group	487	Drive from Thorington Road at TM 4175 7421 to Walnut Tree Farm and beyond to meet Bramfield Footpath 7 at TM 4146 7329.	Bramfield Footpath 7 is recorded as coming to a dead end just short of Walnut Tree Farm. It should continue north to the Thorington Road along the existing farm road.	The missing link needs rectifying by means of a Creation Order or Agreement.
James Richards, for the Ramblers Association, Alde Valley Group	469	Clayhill Road, Kelsale – between the points TM 3924 6410 and TM 3965 6416 (between Saxmundham Footpaths 34 and 33).	Safe connectivity is required for walkers along this road between the points TM 3924 6410 and TM 3965 6416 so that they can walk safely between Saxmundham Footpaths 34 and 33.	Creation of a new footpath between these points.
James Richards, for the Ramblers Association, Alde Valley Group	470	Route from Eastbridge Road to Leiston Footpath 20. –between TM454652, through Black Walks and Lower Abbey to TM458661	Much of this route is believed to be in the ownership of EDF . There are notices denying public access along it but it is believed to have been a freely available route for walkers in the past.	This route should be added to the Definitive Map by way of a Creation Agreement or Order as a safe alternative to the Eastbridge Road and between Footpath 20 and Bridleway 19 at the Round House.
James Richards, for the Ramblers Association, Alde Valley Group	471	River Wall – Butley River, The Gull, River Ore. Butley Ferry to Tide Guage (TM393481 to TM415484).	This is another section where there is no apparent reason for the route not to be recorded on the Definitive Map. It is freely used (possibly on a permissive basis) but is another instance where a Creation Order or Agreement should be funded.	Path should be added to the Definitive Map by way of a Creation Order or Agreement.
James Richards, for the Ramblers Association, Alde Valley Group	472	Alde River wall east of Iken Church (TM412567 - TM443556)	This is another section of river wall that should be opened to the public as a public footpath to link Iken Church with Public Footpath Iken 7. We are recommending to Natural England that it becomes part of the England Coast Path.	A Creation Order or Agreement is needed.
James Richards, for the	473	The British Energy permissive path	This path forms part of the important recreational route known as The Sandlings	It should be made into a permanent public right of way by means of a Creation Order or Agreement. The other

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Ramblers Association, Alde Valley GRoup		between the small car park off Lovers Lan 6452.	Walk. Currently it is permissive only and as such can be withdrawn at any time.	adjoining permissive paths on British Energy's estate through Sizewell Belts should also be made permanent public rights of way.
James Richards, for the Ramblers Association, Alde Valley Group	474	The old railway track bed between TM 4601 5745 and TM 4622 5945.	This forms part of much walked circular routes taking in Aldeburgh, Thorpeness, the Aldringham Fen and Aldringham Walks. It also presents for walkers and cyclists a safe alternative to the B1122 which is a fast and extremely dangerous road and the only other direct link between Aldeburgh and Leiston Much of the track bed appears to be in private ownership but is open, presumably as a permissive path. Permissive paths are unsatisfactory because the permission can be withdrawn at any time.	Creation Agreements or Orders should be funded to secure the route as a permanent public right of way. An ideal solution would be for a bridleway to be created over the track bed as this would provide a multi-user facility for walkers, horseriders and cyclists.
James Richards, for the Ramblers Association, Alde Valley Group	476	Verge of the A1094 near Aldeburgh Golf Course forming part of "the Sailors' Path",	Until recently there was no safe link at the Aldeburgh end between the small car park at TM443581 and the footway at TM448577. Walkers were expected to walk in the carriageway of a fast and dangerous road after it leaves the 30mph limit. Verges are narrow, sloping and uneven with drainage channels - totally inadequate. SCC has secured a licensed path but this is understood to be a ten-year agreement only.	A permanent right of way is required over this licensed path. The verges on the southern side of the road fronting the gardens between the Golf Club and the small car park also need dedicating.
James Richards, for the Ramblers Association, Alde Valley Group	484	Northern end of Footpath Sibton 1 near Wood Farm(TM 3644 7031)	The recorded footpath comes to a dead end and should continue further north or west.	1903 Ordnance Survey Map shows the path continuing west from TM 3644 7031 along the southern edge of Northgrange Farm to the Halesworth Road at TM 3597 7030. This path should be reinstated by way of a Creation Order or Agreement in order to restore the through-route.
James Richards, for the	485	Sizewell Cliffs- Cliff-top path Thorpeness to	Strengthening work needs urgently to be carried out just to the south of the junction	This part of the problem is eased by the fact that people have for many years been able to walk freely over the

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Ramblers Association, Alde Valley Group		Sizewell (Aldringham FP 31) - serious incidents of erosion along this path which have caused the Suffolk Coast Path recreational route to be re-routed. The path affords outstanding beautiful views	with footpath 32 (TM475616) where the path edge is falling away. Footpath 31 seems now to have been lost between points TM474599 (Old Homes Road) and approximately TM476604. The footpath below the cliffs (footpath 33) is also impassable at high tide in the vicinity of TM475601 where gabions have been installed.	grassland between Thorpeness Common and the cliffs and along the existing tracks to reach Byway 20 or North End Avenue, Thorpeness. However, this area is not recorded as Access Land nor are there any public rights of way over it recorded on the Definitive Map. Creation of permanent rights of way over these tracks should be funded to enable signage to be installed and them to become part of the Suffolk Coast Path recreational route.
James Winterbotham	439	Wangford Road and Halesworth Road	1. There is no East West pedestrian access between Reydon and the A12 north of the estuary. Walking on either road is extremely dangerous as the roads are relatively narrow and traffic will only increase as more houses are built in Reydon (200 at Copperwheat with no possibility of improving the road infrastructure); double decker buses at speed; blind corners. 2. From the Hen Reed Beds to the A12 old footpaths have disappeared under the estuary. A solution needs to be found to reach Blythburgh.	1. Established hedges mean that road verges cannot be widened to create footpaths. The only solution is to incentivise the landowners to create footpaths inside the field hedges (c 1m wide?). This may be doable at national level as EU subsidies are replaced by a new UK system; but local initiatives need to be developed. 2. This requires negotiation with local landowners. To be born in mind when SCC has any dealings with landowners.
James Winterbotham	441	No access to Easten Bavents beach	Suffolk Coastal path takes a huge inland diversion between Southwold and Covehithe. The latter is now spilling over with people trying to access the beach.	Safe steps over the breakwaters at the north end of Southwold Parade would meet a need, avoid people taking risks on the rocks and allow escape if stranded by rising tides.
James Wright	124	The non car section of Raglan street, outside Jacobs Court, Lowestoft	This area is a designated cycle way but the bollards preventing cars from using the area for parking have not been replaced and cars park on here sometimes completely blocking the way for cyclists to negotiate through.	Replace the bollards so cars cannot be parked on the paved section. Maybe make signage more obvious.
James Wright	125	Dip Farm football pitches off Corton Road, Lowestoft	There is no where secure to lock a bicycle by the changing rooms car park area. With the popularity of the facility growing with the use	Install a generous number cycle racks

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			by Waveney FC this has seen the car park heavily congested on busy match days and cars also create a hazard by parking along Corton Road often blocking the pavement. Putting a decent numbers of cycle racks here may encourage match goers to cycle instead of drive.	
James Wright	126	Corton Road, Lowestoft	The painted on cycle lanes along the length of Corton Road have been allowed to fade (like a lot of other cycles lanes on other roads in Lowestoft) and have not been repainted. The presence of these lanes and provide reassurance to cyclists using the road.	Repaint and maintain the cycle lanes.
James Wright	127	High Street between Camden Street and Mariners Street, Lowestoft	Cycles are permitted to ride south along this part and there is no contraflow cycle lane painted onto the road. If one was here it would give confidence to people cycling in that direction and also remind motorists this is permitted. The southern end of high street between Dukes head street and the Triangle market area, also needs resurfacing as its becoming very uncomfortable and bumpy when cycling over.	Paint a contraflow cycle lane and resurface the High street where it needs doing.
James Wright	128	Gunton church lane near Yarmouth Road, Lowestoft	Accessing the cycle path can be difficult at busy times such as the school run as queues of traffic build up past Glebe close and sit too close to the kerb to be able to get past.	Make the pavement between Glebe Close and Yarmouth road shared use or paint a cycle land on Gunton church lane to try and encourage motorists to leave a gap for cyclists.
James Wright	242	Cycle path and Footpath from Salehurst Road to Bucklesham Road	Cyclists have worn away much of the surface making it very hazardous for walking and almost impossible with a mobility scooter	From Salehurst Road the first section is either concrete or tarmac. After that it is basically compressed soil. This route is very popular and would benefit from a complete overhaul to establish a good quality walking route which can also be used safely by those with mobility issues.
Jamie MacDonald	50	The issue concerns the full length of a bridleway	This long public bridleway (aka 'Scalesbrook Lane') leading from Holton to Westhall could be	The central section of the route was diverted, following WW2, along the perimeter of the former airfield, and so

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		<p>which passes through the parishes of Holton (BR2), Sotherton(BR4), and Westhall (BR16). The point pinned on the map is the (new) section that would need the most work to make it suitable for cycles.</p>	<p>improved to make it more suitable for cyclists – remembering public bridleways carry cycle rights as well as equestrian rights over them. If Network Rail (as it appears they will, eventually) ever close the Millpost Crossing further to the west (which many cyclists use), then this would be the only direct route from Halesworth/Holton to Westhall, and beyond, that avoids use of the A144 'Bungay Straight'.</p>	<p>is fairly even. As is the first section adjacent to the turkey factory. However, there is a short section at its north end, through a copse, that has recently been (re)added to the Definitive Map; which because of its being newly clear as a through-route would not be suitable for cyclists, even though it is passable by those on foot and probably by those on horseback as well. Therefore, if this section could be made up in some way that would make it more usable by cyclists, then I'm sure it would be used more readily by them. Especially, (and more especially with any future closure of the Millpost Crossing), as this could end up being the ONLY safe route for cyclists to use between Halesworth/Holton and Westhall making the latter parish feel even more isolated than it already is. It then being the only option that avoids two busy and dangerous roads, the A144 and the B1244.</p>
<p>Jane Cochrane (originally submitted via email)</p>	<p>544</p>	<p>Melton Road / Melton Hill</p>	<p>Cycling into Woodbridge via Melton or the A12 is too unsafe or unpleasant. When cycling along past the Coach & Horses at Melton you have to pass numerous parked cars and twice now I have nearly been knocked off my bike by stationary motorists opening their doors. Also, as the incline steepens (near the old council offices) there are numerous cars parked on both sides of the road so, as a cyclist, you become something of an impediment to traffic because you tend to slow down as the hill steepens.</p>	<p>It is too far for me to walk (in terms of time) from Ufford to Woodbridge but I would frequently cycle IF there was a safer/pleasant route.</p> <p>The ideal solution, from my perspective, would be to create a cycle path along the riverbank but from the comments about this on Nextdoor.com it's easy to see that this is controversial topic! I do believe however that if the path was widened walkers and cyclists could amicably share the space. It would need the council to make clear that the route is legally open to walkers and cyclists.</p> <p>https://nextdoor.co.uk/news_feed/?post=17592194269906&comment=17592205235927</p> <p>I would really welcome a cycle path all the way along the</p>

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				riverside to Martlesham Creek - creating a sustainable transport option to the Martlesham retail sites.
Jane Millar	52	Old Felixstowe, walk to Felixstowe Ferry	The pathway by the sea down to Felixstowe Ferry is hard core or gravel, which makes walking difficult and renders it almost impossible for wheelchair users or buggies to complete the walk to the ferry and the cafes at Felixstowe Ferry.	To replace the rough walking surface with a smooth surface to encourage walkers to reach Felixstowe Ferry.
Janet Perry	395	Melton and Woodbridge	Aside from cycling in the parks and A12 (cycle path) there are no family friendly or safe routes. No exclusive cycling options. I feel the narrow streets and way people drive is unsafe for children of primary age to cycle. Exclusive areas would improve children's and parents confidence and encourage families to get on bikes.	Research locations for family safe cycling routes and designate land where you could create this. Partner with land owners.
Jasmine Barrick	435	Felixstowe road, especially between mill lane and main road	The road is not safe to cyclists or pedestrians, regardless of the time of day. I walk this road frequently for work and groceries and cars whizz past at dangerous speeds. The section between mill lane and main road is very overgrown which forces pedestrians closer to traffic, it is also poorly lit compared to further up the road and littered with debris which makes it difficult to see where the path ends and the road begins.	Deterring speeding, clearing the greenery and widening the foot path would be a good start however making the road one way would be the best option to make the road safe for cyclists as well. I avoid Felixstowe road altogether when cycling as the road is even less safe than the foot path.
JENNIFER MARY KING	33	Sailors' Path, Snape	Too many cyclists who are so quiet that you don't hear them approaching. They don't appear to have a bell, so they shout at you to get out of the way. Is this a designated cycle track, or simply for pedestrians. Cyclists have already taken the roads and pavements, now they want the FOOTPATHS.	Please clarify which paths are purely for pedestrians by marking on signs.

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Jenny Eckersley	98	Ipswich Road Woodbridge	Pedestrians have to cross the road 3 or 4 times walking in or out of Woodbridge (. from the duke of York) The road is very busy and it's dangerous	Make new footpath so that there is a footpath on both sides of the road. Provide a safe crossing place at the Framfield house surgery
Jenny King	776	East Suffolk	More and more cyclists are riding on footpaths and some are very arrogant and dangerous with it. Can we PLEASE have signs saying that these are FOOT PATHS and therefore cycling is forbidden.	
Jenny Morcom Assistant Town Clerk, Saxmundham Tonw Council	192	B1121 between Benhal Saxmundham and Kelsale	Three villages cycle path	the three villages cycle path should be put in place ASAP
Jenny Morcom, Assistant Town Clerk, Saxmundham Town Council	193	A12 to the west of Saxmundham	safe crossing for cyclists and walkers	The local plan proposes a new housing development of 800 homes on the easdtern side of the A12 just south of Saxmundham. it also proposes development of an employment area just north of the A 12. There must be a safe crossing for cyclists and walkers between the new housing development and the employment area. preferably in the form of either a footbridge or underpass.
Jeremy Smith	411	There needs to be a safe cycle route between Benhall and Saxmundham, and preferably on to Kelsal	The B1121 between Benhall and Saxmundham is dangerous and absolutely unwelcoming for cyclists. A safe and properly constructed cycle path is needed	There is a public footpath on the inside of the hedge for much of the way. This should be made into a good quality cycle path as well as footpath. The 3C cycle route from Benhall to Sax to Kelsale was developed as concept several years ago and the Sax-Benhall part should be implemented as it forms part of site allocated for South Saxmundham Garden Neighbourhood and fits the policy for the site perfectly (including promoting cycling). Photo shows road looking south from South Entrance Saxmundham, with footpath parallel behind the hedge.
Jeremy Smith	412	A12 Saxmundham bypass.	It is extremely unsafe at present for cyclists and pedestrians to cross the A12 bypass e.g. to	We need underpasses, effective pedestrian crossings, or even step-free bridges at all relevant crossings. The

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			roads, bridle paths or footpaths on the west side. Safe crossings are essential. This is all the more so given the Local Plan designation of the South Saxmundham Garden Neighbourhood which will, amongst other matters, mean that local residents will need to cross the A12 to access rural areas, as the existing much-used rural paths to the east of the bypass will become semi-urbanised.	attached photo showing a footpath crossing was taken in full lockdown when, almost uniquely, there was zero traffic - usually going 60 mph. Impossible for people who cannot move fast to cross without extreme danger. These crossings become even more essential if Garden Neighbourhood proceeds.
Jessica Jeans	510	Wrentham Road entering Reydon	Footpath ends before the Reydon Business Centre, meaning there is no safe way to walk between the Business Centre and Reydon and Southwold. There is no safe place to wait for the bus going into Southwold from the Reydon Business Centre.	Create a continuous length of pavement of pavement safely linking pedestrians to both the business centre and the bus stop on the east side of the road. Create a bus waiting area on the verge by the bus stop on the east side of the road.
Jessica Kirby	222	Lowestoft road coming into Blundeston Village	The walking/cycling links into and out of the village are awful, especially for kids who frequently use this road to access the skate park in the summer and vice versa with those venturing out. A pathway along the entire road would vastly improve access out of the village for those of all ages. There is a large development of houses about to be built near that road, meaning this worse is even more essential.	Investigate the safety of pedestrians in Blundeston entering and existing the village, especially children. Think about how it could improve social isolation. Also factor in this matter when giving permission to large housing developments.
Jill Painter	109	Felixstowe Road, Martlesham leading to Main Road/TheStreet/Top Street Martlesham	Felixstowe Road is shown as a priority cycle route. It is not. It is a heavily used rat run which has made it nigh on impossible for cyclists to safety use it and the other roads listed above. The cycle lanes are dangerous and hardly used due to consistently heavy traffic and HGVs ignoring the weight limits. The speed limit of 30 is ignored (Police Speed Detection surveys	Either close Felixstowe Road to through traffic (buses don't need to use it..and Highways will put every objection possible to this as they see F Rd as a relief road for their failed traffic schemes for the retail park and A12, and have treated residents complaints and concerns with utter contempt) or make it one way. Then it will become a usable cycle and walking route instead of in name only. Put the traffic back onto the A12 instead of

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			prove this). Highways are aware and ignore complaints every time re concerns about ratrunning.	making cycling a dangerous and not very enjoyable pastime, and that may encourage the long suffering residents to get on their bikes. Because at the moment, nothing will encourage me to use the roads where I live other than by car.
Jo Mullett	436	Felixstowe Road, Martlesham between Crown Point and junction with Anson Road	I've noticed a large increase in the volume of vehicles using Felixstowe Road in recent years. I regularly walk along this route but feel increasingly unsafe doing so. Traffic passes very close, if there are puddles at the road edge there is nowhere to move out of the way, as the path is narrow /overgrown in places. The street lighting is inadequate to see the path edge, I worry about slipping off the kerb into the road. I feel safer walking down Mill Lane and around the field edge in the dark.	Make the road one way for motor vehicles, with improved cycling lane. Widen the footpath, and introduce traffic calming measures. Additional street lighting.
Joanna Abbott	390	Main Road Kesgrave	the cycling path which runs along Main Road is an asset to Kesgrave. The High School, which is located along the Main Road has one of the highest amount of pupils who cycle to school in the County. This cycle path is in great need of repair. the markings, signage and surfacing all need updating, re instating and re tarmacking. If ESC wish to encourage cycling and walking in East Suffolk then these issues need to be addressed ASAP.	As above.
Joanne Peters	655	Cycling and Walking Improvements	Walking: 1) On the Snape Road to the north of the village (from the most northerly 30 mph sign in Sudbourne to the jumps at Tunstall Forest gate 23) which is particularly dangerous and regularly used by pedestrians. There is a combination of a narrow twisty road, shadow from over hanging trees and at times a low	1) A short foot path (approx. 300m) along this stretch could be introduced it would safely connect the pavement in Sudbourne, access to the footpath to Iken Boot (Sudbourne no 4) and access to the Tunstall Forest at gate 23. This would make a significant difference in both improving safety and would facilitate better use of footpath no4. 2) A short footpath (approx. 100m) along this stretch

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			angle of light where a number of close incidents have been witnessed where pedestrians have been in danger of being hit.	would connect the pavement to the two footpaths which being on a bend, un-sights motorists to the frequent local walkers and dog walkers. This would also create additional safe round walk options in the village. 3) The crossing of the B1084 needs to have improved visibility / or a different location as it is situated on both a bend and at a road junction. In addition, the footpath needs to be signposted at the Rustic cottage end of the path and for a new access and a clear route from the road crossing to the foot path network in the forest is required.
Joanne Peters	656	Sudbourne	2) On the Snape Road immediately south of the village from the most southerly house to footpaths number 30 to the east and 42 to the west. 3) The safety of walkers crossing from the Rustic Drive footpath (linked to footpath 18) at Rustic Cottage to / from Tunstall Forest.	2) A short footpath (approx. 100m) along this stretch would connect the pavement to the two footpaths which being on a bend, un-sights motorists to the frequent local walkers and dog walkers. This would also create additional safe round walk options in the village. 3) The crossing of the B1084 needs to have improved visibility / or a different location as it is situated on both a bend and at a road junction. In addition, the footpath needs to be signposted at the Rustic cottage end of the path and for a new access and a clear route from the road crossing to the foot path network in the forest is required.
Joanne Peters	657	Sudbourne	Cycling: 1. Time trial and organised events: The B1084 (Melton to Orford) is a popular route for time trial and organised events but has many narrow sections which can put cyclists and other traffic in conflict. In particular large agricultural vehicles with restricted speed, manoeuvrability and driver visibility can be hazardous for cyclists. This is a particular problem in mid / late summer with long	1. Time trial and organised events: Organiser of these events should pre-warn affected Parish Councils of their intention to hold these organised events and routes in advance: to enable landowners / farmers in particular to ensure their vehicle movements are planned to ensure that there is minimal or reduced mixing of cycles and large agricultural vehicles. The onus has to be on the event organisers to ensure this is done in a timely manner. Information and advice for cyclists should be located at strategic locations such as Honey and Harveys in Melton

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			daylight hours when they are on the same road at the same time.	a frequent meeting point for cycling groups. Event organisers should include their contact details on all roadside signage and once cycle events have been completed, they are responsible for its removal of all to reduce the amount of roadside litter created.
Joanne Peters	659	Sudbourne	2. Condition of Suffolk Coastal Cycle Route 41 (Orford to Iken / Snape via Ferry Road through Sudbourne): This promoted rural route is quiet, picturesque and in many ways ideal for cyclists. However, the route suffers from multiple large areas of sand that have run-off from fields in particular near gate / road ways. This sand surface is especially dangerous for cyclists with smooth road tyres who have no grip on such surfaces.	2. Condition of National Cycle Route 41 (Orford to Iken via Ferry Road in Sudbourne): There are potentially three solutions that may be used individually or in combination. 1. Information should be added to publicity of the route that this is a hazard for cyclists to be aware of. 2. Information on the actual route should highlight the hazard in advance for cyclists 3. The land owners / Suffolk County Council should ensure the roads are clear of this washed off material.
Joanne Peters	660	Sudbourne	3. Snape road and B1084 Snape to Orford. This road in particular is often very busy with frequent blind spots and drivers who drive too fast for the prevailing road conditions and don't anticipate individual and multiple cyclists. There are few safe passing places for cars and other vehicles on this road. An alternative for cyclist route should be investigated and implemented as a matter of urgency.	3. Snape road and B1084 Snape to Orford. In order to remove the hazards from the route from Orford to Snape an alternative route with a suitable surface should be built and clearly marked through Tunstall Forest. This would provide a safe cycling environment that would be enjoyed by a wide range of cyclists and reduce the hazard on the road. There are a variety of potential routes that can be explored in more detail which would enhance the riders experience and improve safety. 4. Information signs to bikers could be Tangham campsite , Snape Maltings , car park at Iken and Sandgalls
John Richmond	51	the entire A1094 from Friday street to Aldeburgh but especially the stretch between Frisyon and Alfeburgh.	fast road with cars doing 60mph, having to brake heavily when coming upon bikes. road is often busy both ways and insulates meaning it becomes difficult to pass the cyclists safely.with the increase in hgvs traffic expected for the	I have no solution but as a motorist I'm petrified of slow moving cyclists going up.hill and meeting them before I've been able to brake sufficiently.

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			wind farm installation something needs to be done to protect the cyclists	
John Benjamin Harvey Box	165	Chapel Road, Otley, and its continuation towards Crettingham	The fields around Otley have a good network of footpaths. Many are easily accessible for walkers with children and dogs, but those that lead off to the left and right of Chapel Road beyond the derestriction sign at the edge of Otley can only be reached by walking along the road itself or on a high, narrow verge. With cars passing at speed outside the 30 mph limit, this is not safe.	Continuation of the pavement from Otley village at least to the turn-off to Villa Farm; even better, continue the footpath to Shrubbery Farm.
john clark	24	Forge Cottage, Walpole, IP19 9AZ	Walking from one village to another is extremely dangerous especially where there are bends and hills with high banks and no escape for pedestrians. Some drivers exceed the 30 mph speed limit and others drive into the winter sun unable to see the road at all, Other rural roads that are NSL are narrow and should be 20 or 30 mph. Walking and cycling should be encouraged. We have no 'bus service to our nearest shops which are over 2 miles away, as are schools, pubs and active churches.	Walking and cycling, especially between towns and villages should be made safer. Narrow roads should be 20 or 30 mph. Attention should be given to improving the visibility of cyclists and pedestrians especially on hills and bends and where there are high banks. New footpaths at such points through adjacent fields would reduce the risks. Banks could be cut back at key points.
John Cross	194	Framlingham - New Road to B1120 Brabling Green	Road is crying out to be a Quiet Lane. Heavily used by both cyclists and walkers pretty much the whole length. Also, the 60 mph speed limit should be reduced and appropriate signage installed at each end plus repeaters at appropriate intervals.	
John Dickerson	493	The Bascule bridge in Lowestoft	This is a pinch point for cyclists & pedestrians crossing from south Lowestoft to North Lowestoft and vice versa. It is not easy to cycle or even push your cycle across this bridge at busy times. On the north-east side there is	There are currently 3 lanes for motorised traffic crossing this bridge. It would be better if there were only 2 lanes for traffic and a half-lane on either side for cyclists.

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			rather a lot of "street furniture" to contend with.	
John Dickerson	522	Battery Green road in Lowestoft, as it approaches the bascule bridge crossing it is dual carriageway.	There is no cycle lane along Battery Green road which is an approach road to the bascule bridge, the only crossing point between North & South Lowestoft.	To help cycling could the nearside lane be restricted to buses, taxis and cyclists.
John Dickerson	524	The A12 approach to the bascule bridge in Lowestoft	The bascule bridge is the only crossing point for cyclists between north and south Lowestoft. The cycle route from the bridge to Tom Crisp Way is not an easy route with many road crossings.	Could the inside lane of the dual carriageway be restricted to buses, taxis and cyclists only. This would make the route from the bridge to Tom Crisp Way a much easier and safer route for cyclists.
John Dickerson	525	Lowestoft to Hopton	The Suffolk Coastal Path starts/finishes at Royal Plain in Lowestoft. The Norfolk Coastal Path starts/finishes at Hopton.	Could some serious consideration be given to connecting the Norfolk Coastal Path at Hopton to the Suffolk Coastal Path at Lowestoft.
John Dickerson	526	East coast of Suffolk	The longest single signed cycle route in the world, approx. 6,000Km, is signed along the north Lowestoft sea wall and around the Gunton St Peter's estate. Each year many people travel from all around the world to cycle this route. Currently, the route goes from Norwich to Beccles and stays inland to Harwich missing out on the Suffolk coast.	In conjunction with Sustrans could some serious consideration be given to routing the North Sea Cycle Route from Beccles to Lowestoft and follow the Suffolk coast down to Harwich.
John Dickerson	527	Junction of the A47 Yarmouth Rd and Gunton St Peters Ave or anywhere cycle paths cross side roads.	Cyclists are required to stop at each side road.	This may be too radical for 2021 Lowestoft but it would be good to start thinking as the Dutch do - priority to cyclists. Instead of cycle paths stopping each time they cross a side road make the traffic stop and make the cycle path the priority. This would encourage cyclist to use cycle paths. As you probably know Cambridge are trialling a "Dutch" style roundabout giving priority to cyclists. One day we will catch up with the Dutch and cycling in the UK will be safe. Priorities will be cyclists, pedestrians, motorised traffic.

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John English	223	WWestleton Road, Dunwich between access tracks to Mount Pleasant and Raceground Housee.	Walking on a busy road makes this circular walk dangerous.	Create a short footpath along the edge of the National Trust field to link the two existing footpaths.
john Laughlin	261	Deben riverside path from Wilford Bridge to Martlesham	There is no cycling permitted along this route along the Deben. It would be the obvious choice for cycling due to the flat nature of the terrain and the hilly nature of Woodbridge. This would encourage children and parents to cycle to the Melton primary school.It would possibly help alleviate the pollution at the junctions in Woodbridge and Melton. Cycling to the stations from areas of Melton and Woodbridge would be much easier and would relieve pressure on traffic and station parking.	A shared track with pedestrians would be an improvement. In the short term allowing cycling as it is but with signs informing cyclists that pedestrians have the right of way. If this is done it would help ES to monitor the situation to asses the pros and cons.
John Milne	25	High Road , Trimley.	Cars parked on cycle lane, necessitating cyclists moving out and in from main road repeatedly. Cycle lane disjointed with many short sections.	Ban parking in cycle lane. Have one continuous cycle lane. Similar problem exists in many other areas in Felixstowe with disjointed cycle lanes.
John Pilgrim	307	The entire stretch of 'The Walks' plus Sutton Road to Wilford Bridge roundabout.	Very busy, fast, unsafe traffic, yet this is one of two main access routes to/from the peninsula for cyclists.	With a large proportion of the land to the north of The Walks being publicly-owned (Forestry Commission), there is surely an opportunity to establish a safe all-season paved cycle (and walking) way through the forest between the peninsula villages (notably Boyton/Hollesley) and Melton. This would encourage commuting to Melton/Woodbridge/the stations by bicycle, and would also increase recreational cycling by families daunted by the busy main road.
John Pilgrim	308	Alderton Road/Hollesley Road between the two villages (60mph section).	This is, not unreasonably, a 60mph stretch of road, so has fast cars upon it. It is, however, too narrow in all places to allow vehicles to pass at speed, let alone for cyclists to be/feel safe.	There appears to be significant potential on farmland on the east side to both expand the road and to add a cycle/footpath adjacent to the road.
John Rayner	243	This whole redundant railway line should be surfaced and rebuilt as a	Could be a dedicated cycleway with funding from the windfarms perhaps? You know - like a	

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		cycleway between Leiston and Aldeburgh	proper dedicated route like they have in other parts of the country.	
Jonathan Hawes	490	On the cycle path running adjacent to Tom Crisp Way, South West of the main traffic light junction with Carlton Road and Long Road.	Steel post erected on the cycle path. This is a part of a sign (which consists of two posts) notifying road users of the distance to various destinations. One post is in the cycle lane, the other is in the grass verge. This post poses a heightened risk of a collision with it, especially in the dark where it can become near enough impossible to see it with the glare from oncoming vehicles when travelling North East on the cycle path.	To remove the post and if possible the sign. If it is still needed, have a smaller sign which would only need the use of one post.
Jonathan Hawes	492	On the cycle path running adjacent to Tom Crispway.	The use of multiple posts in the middle of the path to notify users of what the path is for.	These posts seem to offer little or no purpose. But what they do offer is an increased risk of a collision due to a cyclist crashing into a post which has no need to be there in the first place. In contrast, you wouldn't have a post in a road for no particular reason. One improvement would be to remove all the posts that have little or no reason for being there. I recognise the purpose of some of these to cause an obstruction to vehicles potentially using the paths, but ones like these are a danger.
jonathan valentine	375	the thoroughfare woodbridge.	walking/shopping on this street at times when motorised vehicles have unrestricted access can be a very unpleasant experience, it becomes a noisy, dangerous and polluted area, and pavement parking further limits the safe public space, forcing vulnerable pedestrians/shoppers onto the space remaining to compete with powerful industrial machines. this is in complete contrast to the safer, relaxed, more sociable atmosphere that prevails when motorised vehicle movement is restricted.	consider making this street safe for shoppers/ walkers / cyclists / vulnerable people like children, elderly and disabled at all times, not just for a few hours each day. if you need to know how its done look at other towns and cities, much bigger and more complex than Woodbridge, that confronted and resolved this conflict years ago. this has to be considered low hanging fruit for any council developing a cycling and walking strategy.

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Joy Clutten	40	path linking Old Lane and Gunton Avenue Corton	is very narrow for shared use by cycle and pedestrian traffic has become rather overgrown making things worse, its difficult to get out of the way of cyclists and problem to social distance.	Keeping undergrowth cut back, while appreciate not possible to widen for whole distance some widening would make it safer for all
Joy Porter	38	B1083 from Wilford roundabout up towards Sutton Hoo	Tarmac footpath is often overgrown + narrowed due to bank subsiding. Road busy with traffic. Insufficient room to pass each other on path or for the less fit cyclist to walk a bike up in order to prevent cars trying to overtake on this steep, blind hill. This is a popular area for walkers + cyclists accessing Deben, Rendlesham forest, National Trust and coast.	Either 1. Provide a cross country path linking the roundabout with the extensive bridleway network in this area (so it can be used by cyclists too) or 2.Widen path and reinforce bank to provide safer access up hill. I dislike cycling to shops in town as it feels too dangerous.
Judi Hallett	61	Bromeswell, cycling up Wilford Hollows	The hill is steep and many cyclists have to travel slowly. A separate cycle path would be a great idea	cut in to the bank
Judith Hedges	722	Proposed Felixstowe Garden Village Areas	There should be a shared walkway and cycleway connecting all the proposed Felixstowe Garden Village Areas and linking into the town. Kesgrave is an excellent example of what can be achieved. These routes should be wide, well lit, welcoming.	
Judith Hedges	723	The Grove and Abbey Grove	Access to The Grove and Abbey Grove needs to have kissing gates to prevent cycling. Mountain bikes would soon ruin the pathways for walking.	
Judith Hedges	724	Beatrice Ave, Beatrice/Colneis roundabout and The High Rd/Beatrice Ave/Hamilton Rd roundabout	A safe cycle way along Beatrice Ave is essential. The cycle way must not push cyclists into riding on the camber of the road as is often the case. There also needs to be a safe cycle route around the Beatrice/Colneis roundabout and The High Rd/Beatrice Ave/Hamilton Rd roundabout.	

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Judith Hedges	725	Hamilton Rd junctions with St Andrews Rd, Cobbold Rd, Orwell Rd and then across Hamilton Gardens and into Bent Hill	A safe crossing with priority for cyclists should be available at Hamilton Rd junctions with St Andrews Rd, Cobbold Rd, Orwell Rd and then across Hamilton Gardens and into Bent Hill. This will be a safe route from Garden Village to the prom. Cyclists can then cycle along the prom to pier and Landguard area.	
Judith Hedges	726	Beatrice Ave/Colnies roundabout to Taunton Rd	A high standard cycle path on the verge from Beatrice Ave/Colnies roundabout to Taunton Rd and into Ataka and then Gulper would work very well.	
Judith Hedges	727	The Candlet Track	The Candlet Track needs to be upgraded to enable cyclists to leave North Felixstowe and reach Trimley St Martin and Kirton on a traffic free route.	
Judith Hedges	728	Cycle ways in Felixstowe	Many of the so called cycle ways in Fx are too narrow given the road camber and gutter to make for safe and comfortable cycling. Too many allow cars to park in them. Cars expect cyclists to be in the lanes when they are unsuitable.	Maybe the pavement on one side of the road should be a cycle way. Again Kesgrave is very good in this respect. These lanes need to be kept clear of grit and debris that cars push into them. Better signage needed for cyclists and cars. Thought needs to be given at junctions.
Judith Hedges	729	Garrison Lane traffic lights	It is dangerous for a cyclist at Garrison Lane traffic lights if a vehicle behind at the lights turns left in front of the cyclists.	There needs to be a period during the light change that is for cyclists only. I realise this would make the lights even slower but if we want more cyclists on the road it is needed.
Judith Hedges	731	Links Avenue and Upperfield Drive	Links Avenue and Upperfield Drive should become cycle ways.	Cars could be confined to Colneis Rd unless for access. This would aid pupils reaching Colneis and Kingsfleet Schools
Judith Hedges	732	Quiet lanes	Quiet lanes should have enforceable restrictions placed on them. Motorists do not seem to take any notice in Gulpher Rd.	It needs a mandatory scheme. Many more warning cyclists signs would help, the flashing speedo signs are good. Maybe the tarmac could be a different colour. Could the roads be access only for vehicles to stop the joy riders.

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Judith Hedges	733	Newbourne, Hemley and Waldringfield	The lanes out towards and through Newbourne, Hemley and Waldringfield need to be 'quiet lanes'. Maybe they could be for access only by cars.	
Judith Hedges	734	Woodbridge, Campsea Ashe, Snape, Iken and Bawdsey	The area between Woodbridge, Campsea Ashe, Snape, Iken and Bawdsey could become a 'Cycling paradise area' for visitors and residents with the correct restrictions on the roads, ie 'quiet lanes'.	
Judith Hedges	735	'Old' Felixstowe Rd between the Levington turn off / junction with the current Felixstowe Road	Cars travel at great speed along the 'old' Felixstowe Rd between the Levington turn off and the junction with the current Felixstowe Road.	There needs to be a dedicated cycle lane which continues through the layby area onto the dedicated cycle path on the 'current' Felixstowe Rd.
Judith Hedges	736	Cycle way along A14 from Goslings	The cycle way along A14 from Goslings onwards is poorly maintained.	
Judith Hedges	737	Cycle way approaching Warren Heath Sainsburys roundabout	Also the cycle way approaching Warren Heath Sainsburys roundabout from Felixstowe is poorly maintained (often seriously overgrown) and this encourages cyclists to stay on the road which is not sensible with the road layout at the roundabout.	
Judith Hedges	730	The prom and onto the Landguard Reserve	Cycling on the prom and onto the Landguard Reserve cycle way and onto the viewing area needs to be well signed and the surface maintained.	
Judith Margaret Humphreys	365	Ferry Road from Golf Club to Gulpher Road	Cars travelling too fast, particularly at the sharp bends, dangerous for both cyclists and walkers. Road is too narrow for increased volume of traffic.	Speed limit 20mph, warning signs, possibly cycle & foot priority in the area.
Julian Cusack	362	Yoxford to Saxmundham	Cycleway alongside A12 from Yoxford to the B1121 turnoff to Saxmundham is poorly maintained or non-existent. This could provide a direct route to access important local services	

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			in Saxmundham such as the medical centre, shops and pharmacy for cyclists from Parishes to the north	
Julian wiseman	424	Legitimise cycling between Snape and aldeburgh.	To be able to cycle safely from Snape to Aldeburgh (and the other way of course) would be a major improvement and add to the economy by all the holidaymakers and second homers being able to cycle with children to Snape or vice versa and the route is almost there, along the river wall, down the sailors path and along the verge to Aldeburgh. Just a small spend to improve the river wall and the verge and you are there.. it would also be a fantastic addition for local folk to cycle it.	Maybe just a bit of edging along the river and verge to contain some road planings and a few signs to be respectful of pedestrians.
Juliet Redding	164	Between Woods Lane lights, Melton to Bromeswell Roundabout to Sutton Hoo	Road is extremely busy, narrow and has blind bends. It is the only way into Woodbridge (and beyond) for cyclists coming from villages on Bawdsey peninsula and yet there is no cycling infrastructure. The stretch between Melton level crossing and the junction on the Hollesley and Alderton roads near Sutton Hoo are particularly dangerous for cyclists with cars overtaking on blind bends and not giving space to cyclists.	Cycle lanes on all roads into Woodbridge from surrounding villages.
Juliet Redding	166	Road between Sutton Hoo and Rock Barracks	No pavement or cycle lane - vehicles travel extremely fast on this road (60mph) and yet there is no cycle lane or pedestrian route from the barracks into Woodbridge. Many people walk this route (especially from the Travellers Site) and it is very dangerous - especially in the dark. There should be a safe cycle route from all the villages into Woodbridge to enable people to commute by bicycle instead of	Cycle lane from villages into Woodbridge plus pavement/pedestrian footpath between Barracks and Melton.

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			driving, especially as the bus services are so infrequent and do not connect with trains.	
K Cooper	393	A12 at Grove Farm Ufford where cycle routes from Ufford need to continue to the WEST side of the A12 to access cycle path South to Woodbridge or villages West of A12.	cycling with my children from Ufford, west along the old A12 cyclepath in Ufford towards Bredfield or to access the cyclepath south along the A12 to Woodbridge, involves a dangerous crossing of the A12 at Grove Farm Ufford. We have to dash across a busy dual carriageway which is terrifying. There desperately needs to be a way for cyclists and pedestrians to cross the A12 at this point - or there is no safe cycle path access out of the village of Ufford towards the South or West.	A pedestrian crossing of the A12 at Grove Farm Ufford where the dual carriageway starts.
K Cooper	394	The footpath in Ufford, going North towards Pettistree and Wickham Market.	The footpath from Ufford towards Wickham Market is overgrown and too narrow for pushchairs and children's bikes, with numerous potholes and stinging nettles.	Clear, widen and resurface the footpath from Ufford towards Pettistree. It is too narrow, overgrown with stinging nettles in the summer and full of potholes. It is too narrow for a pushchair, and children's bikes - their legs also get stung and scratched. The path has been resurfaced from Pettistree to Wickham market, but the Ufford stretch has not been. There is no shop or services in Ufford, so pedestrian and cycle access Wickham Market is essential.
Keith Derham	501	A12 between Ufford Road junction to Bredfield and Woods Lane roundabout	There is only a pedestrian path alongside the main road, not authorised for cyclists.	1. Authorise making this a shared user (pedestrians & cyclists) and thus legitimise current practise. 2. Widen the path
Keith Derham	502	Woodbridge Road, Bredfield, between pump at junction with Scott's Lane, and A12	This stretch of road is busy and highly frequented by HGV traffic. It is made hazardous by the presence of several blind bends. There is no safe and separate path for cyclists & pedestrians.	With landowner permission create a shared user path of about 900m to the A12. Surface a strip of the track eastwards from Pump Corner past Blue Barn Farm (picture 1) and extend it (picture 2) alongside and past Horse Close Wood (aka Jubilee Wood) to meet the path running alongside the A12

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Keith Wink	86	Cycle path... no cycle paths shown on the map so difficult to locate!	No dropped kerb on cycle path at this location	Install dropped kerb
kelvin roffe	58	many places	<p>on narrow FOOTPATHS cycles and buggy(go carts) creep up on walkers or ride at speed towards and fail to give warning before speeding up from behind. cyclists along the sea front seem to prefer to ride on the footpath rather than the designated cycle path never dismount at the pier - ride like hooligans on the bascular bridge regardless of pedestrians</p> <p>social distancing is more important now than ever</p>	<p>keep bikes and walkers separate in well defined areas</p> <p>in the last 10 years I have walked 77million steps mainly in the Lowestoft oulton broad area footpaths need to be safe for us walkers</p>
Ken Allen	325	Widen and improve the current footpath to make it a shared pedestrian and cycleway.	Cyclists are currently sharing a dual carriageway with fast moving traffic.	If the path was widened to make a shared footpath/cycleway, it would to separate cycles from traffic using the dual carriageway. This would be especially effective where slow moving cyclists are riding up the hill from Ipswich to Copdock.
Kevin Archer	363	Main A1214 from Martlesham to Ipswich (Kesgrave Town section	<p>Being frank the entire cycle path from Martlesham to Ipswich is a disgrace. The surface is worn due to car traffic crossing it to access the many houses along its length. The path is dangerous and cyclists are at more risk of collision with cars from the many side roads because the Stop lines are painted on A1214 not on the cycle lane and Give Way signs on the cycle path are worn away. It is therefore safer to cycle on the main road as the least dangerous option defeating the need for a path.</p>	Maintain the cycle with a good surface, clearly mark give way signs. Improve visibility because you cant see cyclists when approaching the A1214 from the numerous side roads Mark "Give way" before the Cycle path on all sideroad junctions rather than on the main road which is some 10 to 15m further away ; cars are still slowing down and not stopped so a 10 to 15 mph side on collision is very likely.
Kim smith	122	Cycle pathway alongside A14	It's over grown and VERY uneven	A significant tidy up, re tarmac pathway

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Laurence Forgiel	396	Footpath along B1438	The footpath for almost the whole way from Melton up to the top of Yarmouth Road is too narrow. In places this appears to just be overgrown where the vegetation has been allowed to reclaim the footpath - especially at the upper end around Ufford Park entrance. This leaves pedestrians walking perilously close to the road.	Cut back the vegetation and hedges, widen the path properly. Then keep the path cleared regularly to avoid this in future.
Leo Borwick	138	Lower Road, Westerfield	Lower Road and Church Lane are used as a rat run by large numbers of motorists seeking a short cut to main routes West of Ipswich. This is made worse when there are closures of the Orwell Bridge. There is no footpath along much of this route, forcing pedestrians to mix with often speeding traffic. As a resident of the village, I know that a number of other residents are afraid to walk there, particularly the more elderly. This results in both unnecessary car journeys and social isolation.	My suggestion would be to make both Lower Road and Church Lane one-way for motor traffic, as there are viable alternative routes into and out of the village. Proper footways could then be installed and a contraflow cycle lane, preferably with grade separation, or, at minimum, flexible wands or similar.
Leo Borwick	140	Playford Road/Martlesham Road/Bealings Road	This is used as a rat run by drivers seeking to avoid congestion on the A1214 and the NSL applies over large parts of it, resulting in speeding vehicles and a hostile environment for cycling and walking. It is an obvious quiet route for cycling between Ipswich and Woodbridge.	Close the road to through motor traffic and provide a signalised cycle crossing at the western end to enable Ipswich-bound cyclists to continue on their way.
Leo Borwick	141	Rushmere Heath	Currently cycling along the footpath is not permitted here. There is a clear opportunity for a traffic-free route lining Kesgrave/Grange Farm with The Hospital and onward cycling route to the centre of Ipswich.	Install a surfaced cycle track alongside the footpath.
Leo Borwick	142	A1152 Rendlesham	I note that there are planned developments for both housing and employment at Rendlesham	Build grade-separated cycle paths along the main routes into and through both the village and the employment

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			and Bentwaters and yet there is little or no provision for cycling. There is plenty of space and a golden opportunity to make this area a "mini-holland" by providing Dutch-style cycling infrastructure.	area. Convert the roundabout to a Dutch-style configuration, with proper provision for cyclists and pedestrians. Provide secure cycle parking at all the main facilities in the village (care centre, school, shopping area) and employment area.
Lesley white	36	Kessingland to lowestoft	One path to use Only on one side of the road. This is a shared pedestrian and cycle path which is used by people going north and south - it's not enough room. There needs to be a substantial cycle path so that people wishing to cycle to Lowestoft can do so safely.	
Lewis Treloar	121	Bridleway at Carlton Marshes (Suffolk Wildlife Trust) ends in the middle of a field	The bridleway ends in the middle of the field. This could be extended at the bottom of the flood wall to the river.	By extending the bridleway at the base of the flood wall there will be no risk of injury to walkers and still allows cyclists to be able to ride from Oulton across the Waveney and on towards Norwich..
Linda Dowe	333	Southwold	At the present time the only cycle lane 'in' Southwold is the approach road from the Lowestoft Road junction to the North Road junction. This is completely useless as it is not a solid white line hence parking seems to be acceptable anywhere along it thus completely stopping cyclists from using it and further increasing the hazard of an accident as they swing out round parked cars. Southwold has a problem with speeding which is never picked up by the local town council.	I suggest the cycle lane be removed as it serves no purpose and a strictly enforced 20mph speed limit be put in place from St Felix School and also implemented in Reydon to make sure the whole, very popular cycling and walking area, is safer for cyclists and pedestrians alike.
Lindsay Daws	59	Main Rd Martlesham near junction of Holfen Close	Main rd Martlesham is extremely busy with traffic and has become impossible to cross safely since Martlesham Retail Park has expanded. Crossing safely so that I can enjoy the countryside walks by the river Deben is almost impossible and creates much anxiety	Please, please may we have a pedestrian crossing along Main Rd Martlesham so that all the local residents, leisure walkers, disabled users,school children, dog walkers can cross safely. We have such beautiful countryside here but we cannot get to enjoy it safely.

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			when taking your life into your hands with speeding traffic.	
Lisa Simmonds	357	All over Suffolk	Your footpath signs are rubbish, they keep falling over and have to be reported and a worker brought out to stand them up again. Change to metal? Sit them inside some kind of flange plate with soil on top. Label with the footpath number. Could even have suggestions where they lead to! Look at Kent system.	As above
Lisa Simmonds	358	All over Suffolk	Stiles	Get rid of them and have metal kissing gates that the less able and dogs can use.
Lisa Simmonds	359	Footpath that runs beside the River Blyth from Halesworth to Blythburgh	Someone has suggested turning this into a combined footpath and cycle way. I think that would completely destroy a beautiful piece of countryside. I do not want to walk always having think is there a cyclist hurtling towards me? The hard surface is completely out of keeping with the location. It will ruin it.	Cycle ways should be provided alongside roads, with a hedge inbetween. It is I fact possible to cycle on quiet road between the two places, just not directly.
Lisa Simmonds	360	Round Halesworth	A Councillor has suggested a list of cycle route round the town. I support all of the councillors ideas and am not going to write all out again on this cumbersome system.	Do, what the Councillor suggests.
Little Bealings Parish Council	550	Playford Road and Martlesham Road, Little Bealings	The Parish Council is aware that both these roads are used regularly by cyclists, including cycling clubs at weekends, and by walkers passing between footpaths. The route is a rat run to Ipswich for vehicles seeking to avoid the A1214 and there has long been concern over the volume and speed of traffic	Traffic calming, such as width restriction or a barrier across part of the road. There was hatching in Martlesham Road, but this faded and SCC did not replace it. There was also a surface change introduced in Playford Road at one time, but this has also gone due to resurfacing.
Liz Gifford	533	Gloster Road	The cycle lanes on this stretch are too narrow, a lot of cars drive exactly next to them and so leave far less space than the recommended 1.5m. It's especially worrying cycling next to big	Widen the cycle lanes

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			articulated lorries going to/from the shops/industrial estate.	
Liz Gifford	535	Right turn onto Sandy Lane	It is a hairy right-hand turn coming down the hill to turn right onto sandy lane.	Speed limit or separate waiting space would help
Liz Gifford	536	Ipswich Road	There isn't any provision for cyclists here and the traffic moves very impatiently. There's a lot of unsafe overtaking, especially when there are two cyclists going in different directions and motorists on each side trying to overtake.	Cycles lanes and wider pavements would be great on this stretch. If it felt safe walking or cycling between woodbridge and martlesham I'm sure many more people would do it.
Liz Gifford	583	Burkitt Road	It feels unsafe walking on the pavement here between st mary's primary and market hill with little ones. The pavement is narrow in places and the traffic moves very quickly and very close to the kerb. Sometimes cars pull on to the kerb because the road is narrow for 2 cars to pass each other. Apparently there is a 20 mph limit outside the school but it doesn't seem to be marked properly.	Traffic calming measures, clearer marking of / enforcement of 20mph limit
Liz Gifford	534	Felixstowe Road	The road markings are completely bonkers. Cars sometimes drive in the middle very near to oncoming traffic as if they think it's one-way. Also, traffic moves too fast, often far quicker than 30mph which I guess is the limit. At rush hour, cars sit in the cycle lane in a long line queuing at the t-junction. The pavements are too narrow too. I cycle daily between martlesham and woodbridge and this is one of the bits which I think could be made much safer for cyclists and pedestrians.	Impose a speed limit, sort out road markings, possibly chicanes (things that stop motorists using it as a rat run and really make it a cyclist priority route as intended).
Liz Muniandy	34	Along the B1127, towards Potters Bridge.	The Suffolk Coastal Path comes onto this busy road and you have to walk along it in order to get to the next footpath past Potters Bridge. You actually have to walk along the road as	I feel it should be possible to make this much safer for everyone to use by having a path alongside the road and not in the road, to join up the different footpaths.

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			there is no footpath at the side or anywhere else to walk. it is very scary as it is often very busy with cars and lorries, it is not a straight road either. it is impossible to do with children or dogs without putting them in danger.	
Louise Potter	271	Willford Bridget to Martlesham creek. Waldringfield along the river front to Woodbridge	We walk these areas and are passed by cycles on these footpath routes, it is a bone of contention for walkers and cyclist. In Scotland I believe that footpaths can be used by cyclist as well as walkers, why can we not just adopt this policy, The paths can be used by both as long as cyclist pass with caution and slow down. I like to cycle also but in Woodbridge we are restricted to the roads as the only safe cycle route is by the bypass, and you have to cycle the roads to get there.	solution make the footpaths for cycles as well, with the emphasis that the walker has the right of way with the cyclist either dismounting or passing with care.
Lowestoft Town Council	793	Lowestoft	The Town Council is aware that more people in Lowestoft than the national average use the bicycle as a form of transport. Connectivity of routes through and around town should be reviewed and the East Suffolk Council should scrutinise and strongly lobby the County Council on lack of funding being allotted to Lowestoft as opposed to other Suffolk towns.	It is hoped the public will submit their individual comments to East Suffolk Council in response to this consultation, however, again, it is noted that a digital consultation is not inclusive to the whole community.
Lucy Macgreor	89	westerfield lane and high street tuddenham st martin	this lane is used as a rat run throughout the day and quite often speeding motorists, HGVs petrol tankers brewery lorries. This is a single track lane and during lockdown it was very pleasant to cycle, walk down this lane as then you didnt have to dive for cover when an annoyed motorist would want you to jump out their way asap. Which is quite dangerous at times....little lane has pull ins and these are	make this lane a QUIET LANE and NO access to HGV's only for local traffic its even worse when orwell bridge is shut as its like the M25 !!!! with alot of near misses

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			being made bigger by the heavy traffic that tries and push forward, so ruining the verges	
Lucy Williams	327	Playford Road - west of its junction with Butts Road.	Playford Road used by motorists wanting to avoid speed limit on A1214 making it unpleasant and less safe to cycle as many of them drive far to fast.	This route was really popular during the lockdown when there was much less traffic and cyclists felt safe. Closing the road here and at junction further east would provide an excellent cycle route to Woodbridge and yet allow motorists to travel between Playford and/or Bealings and the A1214.
Lucy Williams	328	Playford Road - east of junction with The Street and Hall Road.	Playford Road and Martlesham Road has become much busier with through traffic between Ipswich and Woodbridge making it less unpleasant and much less safe to cycle on. The road was very popular during the lockdown when there was little or no traffic, as those new to cycling and those wanting to encourage their children to cycle found out.	Close the road to the east of the junction along with closure further to the west so that cyclists have a safe and attractive route between Ipswich and Woodbridge, whilst allowing car drivers to reach Bealings from the A1214 if necessary.
Lucy Williams	329	Junction of Top Street Martlesham with Sandy Lane in conjunction with proposal further east.	This section of road is used as a rat run and alternative route for car drivers making it less pleasant and less safe for cyclists and walkers.	Close road to through traffic here as well as further east to provide cyclists with part of a safe and attractive route between Ipswich, Martlesham and Woodbridge.
Lucy Williams	330	Sandy Lane, south of junction with Broomheath.	Sandy Lane is used as a rat run or alternative route for car drivers which makes cycling and walking a less safe and less attractive option.	Close road here to through traffic to provide part of a safe cycle route between Woodbridge, Martlesham and Ipswich.
Lucy Williams	331	Morston Hall Road	Cyclists and motorists and sometimes bus drivers come into conflict on this stretch of road which can be intimidating and off-putting.	Use physical measures to deter motorists from using the road e.g. traffic calming. Provide segregated cycle/pedestrian track to one side.
Lucy Williams	332	Felixstowe Road	The road is used by motorists as a rat run making it very unattractive to cyclists. The road layout does not appear to give cyclists priority but causes confusion to cyclists and motorists.	Close road to north of the community centre to through traffic and provide cyclists and pedestrians with a safe and attractive route.
Lucy Williams	431	From Elmham Drive, eastwards to Straight Road, north side of A1156.	Cyclists wanting to travel from this part of Ipswich towards Martlesham via Straight Road are meant to cross the A1156 here and then re-cross to access Straight Road or continue along	Either improve and add crossings of A1156 to make it safer or provide quality path/cycle lane between Elmham Drive and Straight Road. This could be continued to the cemetery.

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			narrow, poorly maintained footway and a short section on the main carriageway.	
Lucy Williams	432	East/west footpath across Rushmere Common.	For many cycling between Kesgrave and Ipswich is not seen as safe due to the section of route between Linksfield and where the ring road starts to the west.	Provide a section of shared use path east/west across the common.
Marilyn Mackley	93	The road between Otley and Crettingham	There are safe and pleasant routes for pleasure cycling around Monewden and Framsdon. The only way to access these routes from Otley is via Chapel Rd towards Crettingham. This road is narrow and has no speed limit. Vehicles drive very fast on this road. This road is a major reasons that families and children cannot cycle in safety around Otley	Add cycle lanes, reduce the speed limit, add warning signs
Marilyn Mackley	94	Junction Gibraltar Rd and B1078	This is on route from Otley to Swilland and towards Ipswich. The B1078 is fast and straight with only NSL. Crossing on foot or bike from Otley is very dangerous. I do it by myself but would not risk it with a group especially if it included inexperienced cyclists or children	Better signage, speed limit, central reservation
Mark Gilbert	210	Where the cycle route crosses the A12 just west of Farnham (Tinker Brook)	The 30mph limit stops just short of this crossing. If it was extended a 100 metres or so toward Glemham it would be safer to cross the A12 by bicycle.	
Mark Gilbert	211	Bannocks Lane Cransford	This is on a marked cycle route. When the road was resurfaced pot holes were not filled prior to coverage with chippings. This makes the the pot holes more dangerous as it is much more difficult to see them. This applies in many other areas of the region and is potentially very dangerous both to cycles and cyclists.	All pot holes should be repaired prior to any surface dressing being applied. Contractors work needs to be thoroughly checked by council officials.

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Mark Gilbert	212	Thompson Lane Ashbocking/Otley	Road surface is falling apart making it difficult to cycle	Resurface and reduce crowning/camber to make cycling safer
Mark Parker	186	Footpaths in and around Waldringfield, and elsewhere throughout East Suffolk	With the rising popularity of cycling we seem to have lost respect for the differences between footpaths and bridleways. Cyclists seem to no longer acknowledge that footpaths are not for cycling along, making it potentially dangerous for walkers and causing damage to footpaths. In the same way that cyclists wish to see improvements to the road infrastructure to feel safe from vehicles we need to acknowledge that there are similar issues on footpaths...which are NOT rights of way for cyclists.	A campaign of education about the differences between footpaths and bridleways coupled with improved signage and potentially sanctions for non compliance
Mark Sims	228	Section of Ellough Road south of Cedar Drive.	No footpath/cycle path.	Provide a footpath/cycle path.
Martin Walker	119	School traffic	At school start time there is a lot of contention when parents park on the double yellow lines across the cycleway or crisscrossing the cycle way to drop off kids.	Why can't they use the drop off circle that was designed for this within the school freeing up the high road . And the school should reopen the Maidstone entrance for cyclist
Martin Walker	118	No entry in to th ASL from 2 directions	The Garrison lane traffic lights has no entry lane into the box either from the south bound direction or the west bound	Your the engineers work it out. Last time I commented on the west bound and you removed the north bound. The whole system needs a rethink. Painted advisory cycle lanes are continually parked on rendering them useless, they are often not wide enough especially when they contain drains
Martlesham Parish Council	681	Felixstowe Road, Main Road and Sandy Lane, Martlesham	Felixstowe Road, Main Road, and to a lesser extent Sandy Lane, Martlesham, have become a rat run making them dangerous and unattractive routes for pedestrians and cyclists. Unless traffic is reduced on Main Road, it would benefit from safer crossing points for people of all abilities.	An MPC paper on Felixstowe Road, "Felixstowe Road traffic calming", is attached which was previously circulated to the principal authorities and the developer of Brightwell Lakes; the points raised remain pertinent. We have also been pushing for improvements to Sandy Lane via our County Councillors; an MPC paper, "Sandy Lane Speed Limit 2017 – briefing paper" is attached.

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Martlesham Parish Council	682	Martlesham Retail Park	The Martlesham Retail Park needs measures to allow safer circulation for pedestrians & cyclists. As with most retail parks, the emphasis is on the car, but many shoppers move between the different shopping areas on foot. In particular crossing Anson Road for pedestrians between Tesco & Pets At Home is difficult. There is a lack of dropped kerbs on Beardmore Park making it difficult for wheelchair users to move between the areas.	
Martlesham Parish Council	684	Brightwell Lakes (BL) to the retail and business areas	Safe links from the Brightwell Lakes (BL) to the retail and business areas must not be overlooked.	Attached is a map, "Martlesham pedestrian improvement opportunities", drawn up by ESC officer, Ben Woolnough, as a result of a site visit between members of the Parish Council, SCC, SCDC (now ESC), the BL developer & a resident with a guide dog.
Martlesham Parish Council	685	Manor Road crossing point of Eagle Way, near the Tesco roundabout	The Manor Road crossing point of Eagle Way, near the Tesco roundabout, is dangerous with traffic leaving the A12 at speed making it difficult for pedestrians and cyclists to cross.	
Martlesham Parish Council	686	A12 underpasses at the Tesco and Park & Ride roundabouts	The two A12 underpasses at the Tesco and Park & Ride roundabouts are poorly lit, in particular the one between the Police HQ and old Martlesham. They are main cycle/pedestrian routes, but they are unattractive, appear to be infrequently cleaned and the vegetation can encroach. The slopes on the approaches, as well as on the footbridge between Martlesham Heath, make these dangerous routes for pedestrians and cyclists alike in icy weather.	
Martlesham Parish Council	687	Safe crossing of the A12 for Brightwell Lakes	We have lobbied for a safe crossing of the A12 for Brightwell Lakes and suggested an upgrade of the existing bridle path to form safe links into Kesgrave/Rushmere/Ipswich and to the local national cycle networks.	

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Martlesham Parish Council	688	Martlesham	<p>The feedback by local parishioners shown on the ESC interactive map reinforces many of the issues raised by MPC over several years, in particular about the need to make improvements to encourage sustainable and safer travel between Martlesham and Woodbridge. This is all the more important given the climate emergency which SCC, ESC and MPC have declared.</p> <p>We refer you to the Martlesham NP which has a section on 'Getting Around' – see Cycling, walking and disabled access, p43, policies MAR13 & 14.</p>	
Matthew English	26	Road between Ceder Drive and Relief Road	<p>Lack of safe walking path between the end of Ceder drive and the roundabout at the end of the relief road. Pedestrians are forced to walk down the neighbouring field to walk safely. This is a major route between a large number of housing (Ceder Drive and Ellough Road and surroundings) and the industrial areas at Ellough. Alternative routes are a significant distance on foot. Waiting for potential works for new housing is not practical as it is likely 10's of years until this happens.</p>	<p>Surfaced path from Ceder drive to roundabout to provide safe walking route along side road. Would complement planned extension of cycleway from relief road to next roundabout</p>
Maurice Parish (CTC right to ride officer)	132	Howlett way to roundabout -over roundabout into kirton road and vice versa	<p>This is the only route out of Felixstowe and the Trimley to the villages of Kirton Newbourne etc. Howlett road is a busy route and the roundabout is dangerous due to its size which allows traffic to negotiate at speed.</p>	<p>There is ample room to accommodate segregated cycle lane on the roundabout and on the wide verges leading to the roundabout along both sides of Howlett road.</p>
Meg Amsden	84	Junction between Bulcamp Drift and the A1095 to Southwold	<p>A fast, dangerous road for cycling and walking! No footpath from A12 to Wolsey Bridge, so no link up possible between footpaths from Southwold and to Halesworth. No appreciable</p>	<p>1: Extend the 40mph speed limit to Southwold. 2: Create a foot/cyclepath on the south side of the road on Henham Estate land between Wolsey Bridge and the A12. Put pressure on them?</p>

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			verge and a very dangerous bend about 1/4 mile east of Bulcamp Drift - many accidents, several fatal. Living on the Bulcamp peninsular is like being on an island - we have to go everywhere by car. The bus stop at the end of the Drift has lost its designation and it's hard to persuade drivers to stop, though they should.	3: Reinststate the bus-stop at the end of Bulcamp Drift, cutting the bushes back on the north side of the road to make it visible - there's a farm track/opening into the woods.
Melanie Poole	642	River wall footpath from near Wilford Bridge to Martlesham	The path is narrow, in some parts hard to comply with social distancing. With steep slopes either side, often walked by children and elderly, sometimes even crowded, dogs on and off leads (either of which being potentially tricky for cyclists) - it is not safe for dual use at present. Not all pedestrians expect the presence of cyclists, cyclists need pedestrians to step aside, and to keep their dogs out of their way etc.	<p>If the route is to be improved for cyclists, ideally the track should be separate from the pedestrian path.</p> <p>Meanwhile and as soon as possible:</p> <ul style="list-style-type: none"> - make a decision about path etiquette, - Clarify with notices to users, sited at the path (as soon as possible and regardless of any future decision on improvement): whether or not cyclists are permitted to cycle on this route as it is. <p>If they are already permitted, please make it clear that cyclists must dismount when passing pedestrians. In the interests of clarity and safety, this cannot be left to individual judgment.</p>
Michael Crowther	137	Felixstowe, Undercliffe Rd at the Leisure Centre car park	Section of road (part of national cycle route 51) extremely dangerous for cyclists due to uncontrolled parking along the road on the Leisure Centre car park side.	Double yellow lines along this section of road on the car park side. Could provide some 30 minute free parking spaces in the nearby leisure centre and Convalescent Hill car parks to mitigate any impact on the businesses facing the leisure centre car park.
Michael Irwin	529	A12 crossing out of Brightwell Lakes.	I fully endorse comment 278 relating to connections for new development. The opportunity should be taken to view the whole area from Brightwell Lakes/Martlesham to the hospital/Ipswich as a single cycle friendly zone containing housing, employment, retail, educational facilities etc ideal for developing cycling priority routes	Safe crossing under A12, upgrades to existing bridleway from crossing into Kesgrave, linking with cycleways to hospital and Ipswich and national cycle network

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Michael Irwin	531	Martlesham retail and business park, Old Felixstowe Road, Main Road Martlesham, Sandy Lane into Woodbridge	Volume of motorised traffic make this route unsafe for cyclists and pedestrians	<p>1 Traffic management scheme within the retail/industrial area channeling traffic onto A12</p> <p>2 Restoration of Old Felixstowe Road to a safe cycle priority route by limiting motorised through traffic to buses and emergency vehicles</p> <p>3 Traffic calming chicanes in The Street, Martlesham</p> <p>4 No through motorised traffic on Sandy Lane achieved by a physical barrier at the railway bridge</p>
Michael Law	623	Levington around the A14	There is no safe place for pedestrians / cyclists to cross the A14 in the vicinity of Levington, Bucklesham, Kirton et.c, except the underpass at Walk Farm opposite Stratton Hall Drift.	This lack of a crossing could be solved by making the track between the two minor roads either side of the A14 (including the underpass at Walk Farm) a public right of way.
Michael Pett	115	Trinket high road	Cycle lane markings are virtually invisible and need re painting.	Re mark cycle lanes
Michael Urey	269	The length of the Woodbridge Thoroughfare.	Frequency and speed of traffic is unacceptable and totally unreasonable.	<p>Vehicles & cycles need to be banned and the Thoroughfare made pedestrian only.</p> <p>Residents would need to be given access at certain hours.</p> <p>The car park could increase disabled parking to assist but at present the speed and frequency of traffic is unacceptable and totally unreasonable.</p> <p>There are plenty of examples of where this has been successfully implemented.</p>
Michelle Golding	104	Heavy traffic down Northgate	In order to access the proposed cycle path along the disused railway line from the opposite bank (as identified in this strategy and on the interactive map) all walkers and cyclists would need to use Gillingham Dam and Northgate, where their safety is an issue due to lack of pavements and the narrowness of the roads	Link with the highways strategy. Consider linking bus and rail services and redirect the heavy traffic away from this area to make it safer and more accesible for Walkers and Cyclists
Mike Morley	139	Morston Hall Road between Levington and Trimley	This is mostly a single track road with passing places used by cyclists as a commuting and leisure route between Ipswich and Felixstowe.	There is a very wide verge along the whole length of Morston Hall Road which could be converted to a dedicated cycle path or shared use path.

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			The width of the single lane sections does not leave a lot of room for vehicles to overtake or for oncoming vehicles to pass and a large proportion of drivers see no reason to slow down when passing, so it can often feel unsafe for cyclists.	
Mike Nicolson	173	Beside the Westbound A14 from where the High Road joins it to where it meets Felixstowe Road.	The cycle/walking path alongside the A14 is not only very unpleasant but dangerous with no barriers between cyclists and pedestrians and very fast moving large container trucks and cars. I have personally experienced angry car drivers, who believe that the road belongs to them, when cycling along this “passing places” road. There is adequate land alongside this road on the south side.	As described above. The safer and more pleasant route (and that which most cyclists take) is along the Morton Hall Road where a separate path could be constructed alongside this road. If the path was moved to this location a lay-by could be constructed beside the A14 to allow for parked container trucks, etc.
Mike Nicolson	174	The bridleway which passes Hill House Cottages and Candle Farm between Gulpher Road and Thurmans Lane	This bridleway is a perfect route to take cyclists off the High Road and High Street through the Trimleys. There has already been comment on the issues facing cyclists travelling along High Road and High Street where they have to move in and out of moving traffic because of parked cars in the dedicated cycle lane.	Improve the bridleway surface and provide adequate signage to divert cyclists onto this route. This would greatly improve the safety and encourage more people to use their cycles when travelling to work and for pleasure.
Mr C M Petty	433	Warren Heath where Ransomes Way joins Felixstowe Road close to the railway line	Over the last few years changes have been made on both Felixstowe Road and Ransomes Road to increase speed of traffic. This has made crossing Ransomes Road a difficult and dangerous manoeuvre. Each side of the road is shared use paths. To safely negotiate this crossing cyclists have to take the road. Pedestrians have no choice but to take a	Provision of a Puffin crossing as has been provided on the two approaches on Felixstowe Road.

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			chance as the alternative crossings are very long detours.	
Mr Peter W Watson	134	Frostenden Hall	Cyclists using footpaths putting walkers, employees and contractors in danger. It is illegal for a cyclist to cycle along a public footpath without the land owner's permission. Very few cyclists are aware of this.	Educate cyclists . Identification numbers on cycles will help deter persistent offenders. Inform navigation apps that some of their information could be incorrect
Mrs Della HUghes	177	B1078 junction with Manor Road at Clopton IP13 6QN	Traffic coming up the hill in Easterly direction is often speeding and also often overtakes on the brow of the hill where the driver can have no view of road ahead. At the top of the hill is a road junction, a blind corner, a village hall, a childrens' play area and a bus stop. Cycling and walking along this stretch of road is made suicidal by speeding traffic, and HGVs. It is necessary to cross this road to access local footpaths, the childrens play area and the village hall.	A speed limit through the village of 30mph would be a good idea to start with. At the very least, double white lines (no overtaking) up the hill to prevent blind overtaking would be a step forward.
Mrs J M Pryce	195	End of combined cycle-way/footpath from North Cove church to The Street	Cyclists exit the cycle way at speed without stopping to give way at the end sometimes going over the bonnets of cars travelling from the A146 towards Pinewood Gardens and Marsh Lane.	Just repainting the Give Way lines and triangle so that it shows up more to see if that helps resolve the problem.
Mrs J M Pryce	196	At the end of The Lowestoft old road which runs from North Cove Church to Marsh Lane Worlingham, the crossing of the A146 is from a sloping blind path onto the exit of the roundabout	The cycle crossing across the A146 slopes down and is blind just as cars come off the roundabout. from the other side of the A146 it is difficult. and vegetation can make it blind. It needs to be made safer for children cycling to the schools in Worlingham and Beccles. it is difficult for walkers to cross as well.	Light controlled crossing.

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Mrs Karen Langdon	398	The level of traffic on the small lane to Shingle Street	It is dangerous to walk down this lane to Shingle Street in the summer months because of the number of visitor cars to the area. It is a popular route for walkers, local families, ramblers groups, D of E groups to visit Shingle Street. The large volume of cars using the lane makes it very dangerous for non-vehicle users because it is narrow, with unmarked 90 degree bends and there is nowhere to escape if a car is travelling too fast or misjudges the space available to safely pass	Register the lane under the Quiet Lane Scheme. Mark out on the road surface a lane for walkers/cyclists to reduce the speed of the cars by highlighting the lack of space for the cars to pass other users Ban cars parking from the bridge down to Shingle Street, except resident vehicles during the summer months.
Mrs Margaret Goffin	321	From Church Rd, Ellough left to Moor Business park.	It's impossible to walk safely from Church Rd Ellough to the Moor Business park. Theoretically you would need to cross over to Walkway/cycle way towards roundabout but cannot cross over Benacre Rd again opposite entrance to Moors Business park as there is a ditch to traverse. There is enough space on the side of the road as the business park to provide a walkway/cycle way.	
N. Winship	590	Ufford Road junction with A12 single carriage way has a bridle way Xing	Dangerous to cross A12 as cars very fast to & after dual carriage way	A Toucan Crossing. Also resurface & remove foliage from cycle way
Natural England	796	East Suffolk	Natural England has no comments to make at this time. However, we will be happy to comment on future forward planning consultations which come forward.	
Neil Winship	591	Saddlemakes Lane /A12 junction a GR 278514	Dangerous to cross A12 from cycle way to Saddle Makers lane	A Toucan Crossing. Also resurface & remove foliage from cycle way
Neil Winship	592	GR 267504 Immediately north of roundabout A12/ 52	Dangerous to cross A12 to reach cycle way beside the A12	A Toucan Crossing. Also resurface & remove foliage from cycle way
Neil Winship	594	GR 282 504 along B1083 to 294 496	Risky cycling all along B1083 to & from Bawdsey Ferry	Provide a separate cycle way that could encourage AT & visitors

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Neil Winship	604	GR 247 459 GR 248 454 & GR 193 453	Mainline buses at Tesco, Mrtlesham Heath & BT at southern end Gloster Road, are bus 'nodes' offering frequent services to & from Ipswich, Felixstowe & Woodbridge. They could complement cycling and walking to and from nearby rural settlements. But there are no hoops to which to secure bikes, and no urinals. Similarly there are no public toilets near the P&R bus stop at the roundabout north of the Hospital for ATs enroute to and from Ipswich, but I didn't flag it on your map.	Provide hoops to which to secure bikes, and toilets mainly for older ATs. Men only need urinals and now women likewise with advent of 'SheWees'! This may seem trivial to younger and middle aged persons but lack of them can be a serious deterrent to elderly Active Travelers.
Neil Winship	593	GR 282 504 to GR 294 496	Risky shared pedestrian & cycle way from Melton lights over rails, Wilford Bridge and up hill to access Bawdsey Peninsula.	Widen shared way that is beside busy highway and provide some safe crossing at Riduna and the A1152/B1083 roundabout.
Neil Winship	596	GR 254481	Dangerous to cross A12 to /from cycle way, especially just to west of roundabout and the B1458 into Woodbridge	A Toucan Crossing. Also resurface & remove foliage from cycle way
Neil Winship	601	GR 265 450	When Brightwell Lakes are developed, ATs will want to enter the AONB to reach the R. Deben & Maybush Inn. The permissive footpath from GR 264452 to the Quiet Lane at 273454 is not a PROW	If Waldringfield Heath Golf Course new owners do NOT provide a footpath in due course, then ESC & WPC might negotiate with Howes Farm owner of that permissive path, for it to become a PROW.
Neil Winship	597	GR 248 447	Brightwell's bway12 cross A12 to 6 unusable for years by all except at night. When safe, day-time crossing for ATs is provided, then Brightwell bridleway 6 needs connect to safe cycle & walkway to Ipswich Hospital, town, buses, coaches & rail NB: two way requirement Quiet Lanes Suffolk point to need to encourage the 200,000 living Ipswich & suburbs, to benefit by AT on PROWs in countryside, without needing to come by car.	Brightwell Lakes coming Pegasus Crossing of A12: although a bridge like that at GR 246453 (I find fully acceptable unlike 169) would be better, as doubt any horse & rider will use and many ATs will be reluctant to stop busy & fast traffic. But If the smart lights & vehicles in platoons system are adopted in lieu of widening the 4 roundabouts, then the Pegasus crossing will probably be best
Neil Winship	598	GR 239432 and 238431	Foxhall' footpaths #18, #26 and #27 have been severed by the A12 although #18's finger posts	It could have an inexpensive walkway through, but H&S will probably veto. But #27/#25 seems very suitable for a

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			<p>are still in place. #26 has a car-sized culvert through which a stream flows.</p> <p>Also the A12 (T) has severed the #27/#25 crossing, which has an AT suitable road to the west and #25a lane to houses beside a track to the east.</p>	<p>Toucan crossing which would provide an attractive and relatively direct route for ATs in both directions. Indeed this and the Bucklesham/Levington bridleway #21 crossing of the A14 could provide a good AT route</p>
Neil Winship	599	GR 242407	<p>Bucklesham/Levington bridleway #21 has been severed by the A14. As the only safe AT crossing for 6.5 Kms between Trimley pedestrian bridge and the A1156 road bridge at GR 223433, this seriously deters AT</p>	<ol style="list-style-type: none"> 1: As per reference 272, provide a safe way across the A12/A14 junction at Seven Hills. 2: Provide a pedestrian/cycle bridge at #21. 3; Negotiate with the owner of Walk Farm for ATs' to use the tunnel at GR 252396. This might need traffic type lights at each end so that farm vehicle drivers are forewarned
Neil Winship	600	GR 242 464 to GR 198 453	<p>The A1214 Woodbridge Road's cycle way is reasonable except:</p> <ol style="list-style-type: none"> 1. For most of its length, vehicles joining from side roads tend to halt on the cyclists' way crossing that side road before the junction. 2. Where it passes alongside the Rushmere Golf Course, it co-uses the narrow pavement and the kerbstone prevents cyclists getting on/off to avoid walkers. 	<ol style="list-style-type: none"> 1. Side roads surfaces should be painted with 'zebra crossing patches' and maybe a warning sign 2. Widen the foot & cycle way
Neil Winship	602	GR 260 451	<p>At present, ATs aiming for the Martlesham Retail Park and to cross the A12 via the foot & cycle bridge or either of the tunnels in order to reach the Martlesham P&R, Kesgrave High School, Ipswich Hospital, Town, buses or rail station, and visitors coming the other way, tend to cycle along the tarmac strip as footpaths #23 & 43 are very rough.</p>	<p>When Brightwell Lakes are developed, good cycleways to the A12 crossings, must be provided</p>
Neil Winship	603	GR 256 429	<p>Newbourne #1/Brightwell #19 was un-signed and ploughed last time I tried to walk from Waldringfield to Bucklesham</p>	<p>Reinstate signs and ensure link to A12 (T) crossings at GR238431 and the tunnel at GR 241 433</p>

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Neil Winship	595	GR 260 492 just South of A12/Grundidburg roundabout	Dangerous to cross A12 to /from cycle way on west side of A12	A Toucan Crossing. Also resurface & remove foliage from cycle way
Nicholas Mayne	76	Footpath marking around Stoven Wood , Brampton, also North Green and also footpath from Stoven to North Green	Several years ago I walked these paths with an 'official footpath lady' I think from Ipswich. She undertook to get new wayposts installed and direction markers replaced. This never happened.	Replace defective waymarks, put official direction posts at North Green and mark the path from Stoven to North Green. The marker on the map is indicative only as there are several issues.
Nicola Halton	324	A safe cycle crossing to the path on west side of A12 at Aldburgh/Friday St junction would enable cyclists to access roads on this side from the Snape Rd.	The path needs to be kept clear of vegetation and allocated as a shared use path. It is currently overgrown and not fit for purpose. Cyclists frequently cross here to cycle either north or south to access the roads to Ben hall and other villages west of the A12.	As above
Nigel Cottee	574	Southwold to Felixstowe via Woodbridge	Following a good deal of British success at the elite level and a general desire to improve mental and physical health, cycling has become an increasingly popular activity, whether it be commuting or for leisure. However, poorly lit roads and busy traffic prevent it becoming more commonplace with people still opting for four wheels rather than two.	There is enough open space to build a cycle path from Lowestoft to Felixstowe via Woodbridge. This could be done quickly and at modest expense.
Pam Watson	553	Junction of Ipswich Road with Warren Hill Road	When cycling up the hill along the Ipswich Road it is very dangerous turning right into Warren Hill Road. The oncoming traffic is fast, often breaking the 30mph speed limit, because the road is wide and the traffic is going downhill. Visibility for both traffic and cyclist is poor because it is on a blind bend. The cyclist is forced to wait in the middle of the road, between lines of traffic.	A safe space for cyclists in the centre of the road. Painted white lines as these are not visible enough to traffic, and could even make the problem worse by creating an illusion of safety for cyclists. Cyclists need to feel safe. An island is the only solution.

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Pam Watson	556	Cumberland Street off B1438	Cumberland St is a beautiful medieval street which should be a pleasure to walk along. Instead it is an intimidating place because the pavements are very narrow. Cars go very close by at 30mph (and sometimes more) as there is nothing to slow them down, since the road is very smooth and the double yellow lines keep the road generally free of parked cars. People frequently need to walk in the road, if they need to pass each other, or walk two abreast for example.	Shared space for vehicles and pedestrians. Traffic could be slowed easily by putting planters alongside the pavement at intervals, narrowing the access for traffic and making it slow down. Drivers should be made aware that they need to share this space with other road users.
Pam Watson	558	The Avenue off Kingsdon Farm Road, Woodbridge	There is no pavement along here despite traffic increasing as a result of the new car park at the bottom of this Street. People walking along it have to dodge parked cars as well as traffic and have no space to stand or walk and feel safe.	Create a pavement
Pam Watson	559	Kingston Field, Woodbridge	No disabled access on to this important and intensively used council owned leisure space.	There should be two points of access, I suggest one at the bottom of Cherry Tree Road and another near the car park entrance on The Avenue.
Pam Watson	561	Turban Centre, Woodbridge	Nowhere to leave cycles in the Turban Centre. Nearest cycle stores are too far away (next to Nero's in Thoroughfare or outside car park WCs)	Hoops in the wall of Boots, as standard cycle stores would take up too much pedestrian space.
Pam Watson	565	The whole of Quay Street, Church Street and New Street, Woodbridge	Here we have beautiful medieval town centre streets which are impossible to walk along feeling safe because the pavements are so narrow. Priority is given to the traffic using these streets, with pedestrians having to get out of the way. This traffic goes close by at 30mph (or more if it's breaking the current speed limit). As well as being dangerous it is polluting and noisy, especially HGVs. People must be allowed to feel safe, and be able to social distance from other pedestrians.	The traffic must be slowed down, and much more emphasis must be placed on traffic giving way to pedestrians. Pavements could be widened and the roads narrowed until the traffic can be shut out completely. Even Quay street could be treated in this way. The other two roads are one way so could easily be narrowed. Chicanes along New Street (one on South side of B1079, one near Mariners Pub) would slow traffic coming down the hill here. Another solution is to take away all distinctions between pavements and road, levelling the whole space in order to make the dominant hierarchy of

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				road usage by cars less clear, forcing traffic to slow down for pedestrians,
Pam Watson	567	Melton Road between The Thoroughfare, Woodbridge, and Melton Traffic lights at junction of A1152	This is a popular route for cycles as it's the only way to get from Woodbridge to Melton and across to the Bawdsey peninsular. The road is dangerous for cyclists because there is no space for them. It is heavily used by vehicular traffic and parked cars on the route are a real problem, since car doors can open suddenly as cycles attempt to pass.	A purpose built cycle path kept clear of parked cars. 20mph speed limit for motor vehicles.
Pam Watson	568	B1083 between A1152 and Sutton Hoo entrance	Very hard to cycle up the hill to this beautiful site of national importance owned by the National Trust. Better access needed for cyclists.	Cycle lane, white paint with signs.
Pam Watson	569	A12 to South side of B1079	There is currently only one pedestrian/cycle crossing across A12 from Woodbridge. A12 is an extremely busy road and impossible for cyclists and pedestrians to otherwise cross. They need to be able to get from Woodbridge, with its sizeable population, to the other side, to access the Garden centre and other shops in the development. Only car users can currently access. The only crossing at present is too far away to be of practical use.	A pedestrian and cycle crossing
Patricia Garrod	382	There need to be a safe cycle track from Carlton Colville to Beccles on the A146	Several people cycle the A146 and it is very dangerous especially by the Barnby Bends. The back rounds are hazardous in the dark morning and evening so there is no safe route. If there was a cycle track I'm sure more people would cycle rather than use cars.	Decent cycle track to link towns and villages
Patricia Williams	353	Various access roads into Woodbridge from North and Melton	Melton road into Woodbridge Chapel Street Castle Street Bredfield Rd	Car parking provision further out of Woodbridge.

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			<p>Seckford Street Theatre Street</p> <p>These roads have become one big car park making cycling access into Woodbridge dangerous. These roads could be made no parking making it safer for cyclists. Car parks should be provided further away which would mean people would get more exercise walking and there would be a lot less pollution in and around Woodbridge</p> <p>Sandy Lane should be made safer for walkers and cyclists</p>	
Patricia Williams	584	Woods Lane	Children use this route for cycling from Melton to Farlingaye school. It is very busy with huge lorries coming to and from Rendlesham Bentwaters. Needs shared cycle/footway or cycle Lane to make safer for cyclists.	
Paul Beardsley	53	The old river crossing ,north gate, Beccles	May not be East Suffolk, but there is a disused railway line goes from the old railway river crossing in Beccles,to Gillingham,geldeston,ellingham,bungay. I tried to cycle a small section recently, impossible, very overgrown... But as in Derbyshire, a reclaimed railway line are brilliant for traffic free walking and cycling	Talk to the land owner / set up a charity work party
Paul Charles Richardson	106	Between Suffolk town centre of Beccles and Suffolk town centre of Bungay (in partnership with Norfolk).	Having no direct route between the Suffolk towns and having the old railway route unused.	Between Suffolk town of Beccles and Suffolk town of Bungay (in partnership with Norfolk). Reconnect the town's by making use of the old railway route as a new cycle path. This would be away from roads, existing infrastructure (bridges, embankments and cuttings), minimal / no gradients, countryside views, direct route between town centres and for the majority of their route

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				likely to be unused and already furnished with trees, hedges and the odd bit of history along the way.
Paul Charles Richardson	107	Between Low Road and Puddingmore / Ballygate	Busy road between Beccles and Bungay with no cycleway and only a broken bit of pavement could see a combined cycle/foot path added (as long as it doesn't destroy hedgerows / trees)	Low Road is an ideal and pleasant route into Bungay that avoids the hills and much of the main road from Beccles. However, to get to Low Road from Beccles there is no cycle path and only a patchy / unsuitable pedestrian path.
Paul Jordan	43	Junction of Linksfild and Woodbridge Road to Ipswich border.	An adequate cycle route runs along the south side of the A1214 Woodbridge Road until Linksfild junction but cyclists riding to Ipswich must then join the busy caridgeway or illegally use the footway. This is a serious gap in the route network.	Widen footway onto common to allow space for shared use path with dividing line. Ideally allow bothway cycle use so that east bound riders from Glenavon Road do not have to cross Woodbridge road at Glenavon Road and again at Beach Road
Paul Jordan	44	Re-route NCN1 to avoid retail park in Martlesham	The area around Gloster Road has become much busier since NCN1 was planned as has Felixstowe Road.	It would now be safer, shorter and more plesant to route NCN1 straight on at the point shown on the map, along Main Road under the junction of A12/A1214 to rejoin the existing route at the junction of A1214 and Deben Avenue.
Paul Jordan	46	Recreation Ground Martlesham	Fynn Valley Walk out of alignment. Walking East on the Fynn Valley walk at present means walking South from the junction of Post Office Lane and The Street, along School Lane before turning onto a footpath to Martlesham Creek.	If a permissive path could be negotiated with the land owner the route would be much improved by a link from the recreation ground at the point shown on the map to join the existing footpath round Martlesham Creek
Paul Reeve	335	Cycle paths in Ipswich	There is a lack of clarity in Ipswich as to where cycle paths begin and end and which footpaths are shared space.	Paint all cycle tracks to increase visibility for pedestrians and cyclists
Pete Whelan	355	The whole of the river path from Martlesham to Melton is unsuitable for dual use (pedestrians and cyclists). Cyclists are currently prohibited, but very few take notice of the fact and push past	The path is only just wide enough for pedestrians to pass in a lot of places. To widen it to the necessary regulation width for dual use would likely not be possible and would also spoil the area. Enforcement is necessary before someone is seriously injured.	enforcement action against cyclists using the path

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Pete Whelan	356	Cycle lanes anywhere in the east suffolk region	Can you make sure that any cycle lanes (road or pavement) that are installed are to the regulation width and not too narrow to use (some parts on Felixstowe Road Martlesham are about 60cm). If any of the plastic wands/bollards are used then the 2m width of the cycle lane should be used. I have a tricycle and cannot use the lanes in Ipswich which have wands installed without either hitting the kerb or wands as they are too narrow,	keep to the planning guidelines and standard for all cycle lane provision. That way motor vehicles can give some clearance to cyclists, even if driving right onto the white line or wand
Peter Crofts (originally submitted by email)	514	Road between Woodbridge Thoroughfare and Melton cross roads	<p>The all day parking on both sides has reduced the width of this road by about half. Mostly shoppers or commuters are seeking to travel but the all day parkers are an obstruction and a danger to any under aware pedestrian.</p> <p>The other day I had an appointment in Common Lane, Melton and the traffic was gridlocked, from Woodbridge to Melton. I thought there must have been an accident but no. On the bike I was able to nimble past them it was a ridiculous situation.</p>	Is it time for bikes only for trips under 10 miles? Some days a week. It is moving that way.
Peter Crofts (originally submitted by email)	513	Sandy Lane, Martlesham as far as Ipswich Road, Woodbridge	<p>Many motorists tend to drive too fast and show their reluctance to slow down for less powerful craft such as a bicycle. The railway bridge often results in a last second lurch for many.</p> <p>For a cyclist to exit the bottom of the hill from Broomheath on the way to Woodbridge, it has become quite difficult to exit onto Ipswich Road going to Woodbridge.</p>	Possible solution might be to widen the pavement thus curbing the motorists and allow cyclist to share with the few pedestrians. I would be interested to hear your views.
Peter Crofts (originally	515	Felixstowe Road	As you will know, it is marked as a "cyclist priority route" at both ends. But in practice it is not. The painted lines provide no latitude for a	I offer the following solution which has virtually no cost. Introduce vehicle free sundays, so that family cycles can

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submitted by email)			wobble. During the lockdown there was a reappearance of young families on bikes unthreatened by cars, but now the 4 x 4 are out in force again with their largely single occupants hell bent on going shopping. Their speeds are often estimated at 40/50mph. Coming out of Mill Lane one has about 2 seconds to exit.	explore and travel this short distance without the threat of extra danger.
Peter Dutton	280	A separate cycle/pathway along the south side of Halesworth Road from Wissett to Halesworth.	A separate cycle/walkway alongside the Halesworth Road from Wissett to Halesworth would make walking and cycling a lot safer for non-vehicle users along this narrow twisty country road which has a high bank and big hedges along its northern side. Many potential users do not use this route due to its obvious dangers for walkers and cyclists.	
Peter Dutton	281	Lack of connectivity	There is no easy way for cyclist and pedestrians to walk/cycle into Halesworth except along the busy B1123, Holton Road. The new 160 unit housing estate will add pressure to the need for a surfaced track to link this part of Halesworth through the Millennium Green to the town centre thus avoiding the B1123. Such a route will encourage people to cycle/walk along this attractive cross country route.	
Peter Dutton	282	Remove the confusion facing cyclists using Route 1 through the Thoroughfare in Halesworth	The Thoroughfare in Halesworth is part of Route 1 but it's a one-way mainly pedestrianised shopping street.	Move Route 1 to the east side footpath of Saxons Way to enable two way cycling. This footpath is little used by pedestrians. This foot[path is a little less than 2 metres wide and so will require to be widened.
Peter Dutton	284	Halesworth Road from Wissett to Halesworth is very dangerous for cyclist and pedestrians	This Halesworth Road is narrow, twisting and bounded by a high bank on the north side. There is space on the south side of this road for a dedicated cycle/pathway which would	

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			encourage more people to cycle or walk the short distance into Halesworth. Currently it is too dangerous, except for the brave and the foolhardy to risk it. The number of bends means that drivers are often suddenly confronted with a walker or cyclist in a road that is only just wide enough for two cars	
Peter Dutton	285	A footpath along the northern verge of Wissett Road Halesworth	From the rear entrance to Edgar Sewter school and Old Station Road there is not a continuous footpath. Pedestrians have to keep crossing the road and for part of this stretch they have to walk in the road. A continuous footpath alongside the road would greatly increase safety and improve the connectivity of this part of Halesworth with the town centre.	
Peter Dutton	286	Lack of connectivity	A short cycle/pathway linking Bramblewood Way with Loam Pit Lane. This short connecting link would enable cyclists/pedestrians to avoid having to go along Holton Road if they were going to the station, surgery, Edgar Sewter school or the north of the town.	
Peter Dutton	287	No continuous footpath along the east side of the A144 from Fair View Road up to the Sparrowhawk Road roundabout	There are short lengths of path which need to be linked up for convenience and safety reasons. There is space along the verge and a path should have been installed at the time of the new housing developments were being built.	
Peter Dutton	289	Upgrade the footpath from opposite the Beech Close junction on Chediston Street	Improving this unmade path would make a pleasant country walk around the west side of Halesworth. If it can be widened to accommodate cycles it would add an	

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		through the fields to the end of School Lane.	interesting connection for cyclist around the town without having to go through it.	
Peter Jackson	482	Footpath/cycleway, bridge and roundabout	There is in informal footpath around the edge of this field which allows pupils from the High School to access East Bungay without going along the busy main road. Turn this into a legal right of way with footpath and cycle way and a bridge over the Tin River. Also to enable safe crossing of the main road put a round about or at least a median island at the junction of Kings Road and St Johns road	
Peter Lux	65	New Road	A general issue that reports of road problems which affect cyclists are not taken seriously by the highways department. At this location there is a big dip in the road where the telegraph line crosses the road. It is a downhill stretch and if you do not know about it then it could lead to a cyclist being dismounted or coming off the road (this has happened).	The highways department to take cycling issues seriously and fix accordingly.
Peter Lux	610	Barnby Bends	Large dip on westbound although road surface not broken. Possibility of dismounting cyclist since it is downhill and cyclists could be travelling at reasonable speed. Almost dismounted cyclist in front of me yesterday - I am aware of dip so can avoid	
Peter Marett	496	Marlesford Lane dips beneath railway line at Bucks Head bridge.	Road often floods after rain in winter and from irrigation run-off in summer. Existing drain usually blocked. Water depth often sufficient to prevent access by walkers and cyclists - sometimes deep enough to cause abandonment of motor vehicles.	New drainage works.
Peter Marett	497	Lane leaving B1078 adjacent to where	Register as a quiet cycling route to Campsea Ashe, avoiding the B1078 which can be busy with motor traffic and which, in places, is	

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		southbound A12 slip road joins B1078	narrow with high banks. This also gives access at Well Cottage to a lane which crosses the railway line via the Blackstock level crossing to give a quite cycling route via Station Road to Blaxhall and on to Snape.	
Peter Marett	498	Blackstock Crossing	Register as quiet walking and cycling route between Wickham Market and Blaxhall and on to Snape.	
Peter Marett	499	Ashe Road between Campsea Ashe and Eyke / Rendlesham	Register as a quiet walking and cycling route between Campsea Ashe station and Eyke or Rendlesham. Give priority to walkers and cyclists.	
Peter Marett	500	Ivy Lodge Road between Campsea Ashe and Rendlesham / Bentwaters	Register as a quiet cycling route.	Frequently used as a short cut by lorries accessing Bentwaters from the A12. Road not suitable for HGVs and potentially dangerous for walkers and cyclists Prohibit HGVs from using this route (with exception of agricultural vehicles).
Peter Webb	200	North of Melton Old Church	Road frequently flooded. This is especially dangerous for cyclists because there are often potholes that cannot be seen under the water. Also there is a thick layer of mud along the centre of the road. This is an important route for those wishing to cycle between Ufford and Melton/Woodbridge.	Flooding and mud has been reported numerous times but SCC Highways have failed to provide any drainage.
Peter Webb	201	Junction of A12 and New Road between Melton and Bredfield	At busy times it is very difficult and hazardous for cyclists to cross the A12 when travelling between Melton and Bredfield. The A12 carriageway is very wide at this junction	Provide central reservation for cyclists and pedestrians. This could also make the junction safer for motorists.
Phil Ridgway	276	"Private" road from East side of Melton Bridge through to Sutton Hoo .	Define the "Private" road from East side of Melton Bridge through to Sutton Hoo to be a Bridleway or similar. I believe there is already an outstanding request to confirm that this should be a Public Footpath. This will allow an	Define the "Private" road from East side of Melton Bridge through to Sutton Hoo to be a Bridleway or similar. I believe there is already an outstanding request to confirm that this should be a Public Footpath. This will allow an easy access to Sutton Hoo from Woodbridge and the Melton Railway Station

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			easy access to Sutton Hoo from Woodbridge and the Melton Railway Station	
Philip Raiswell, Planning Manager, Sport England	494	This is a general comment, Sport England, as a non-statutory consultee, supports the development of this strategy, which will improve opportunities for physical activity, in line with Sport England's Active Design principles.	n/a	n/a
Rachel Knight	197	Barnby bends	The road is far too narrow and winding and it needs a cycle path/lane that follows the same route but takes cycles off the main road as it is dangerous and causes huge tailbacks. The only cycle route takes cyclists so far off this route that they just don't use it! I would not dare cycle to work because it is just dangerous and any other route is far too far round (via Mutford)	Totally bypass the Barnby bends and include a cycle path - this has been needed for decades! At least widen the road to include a proper cycle path on each side of the road
Rachel Smith-Lyte	479	Wilford Bridge Road leading onto Sutton Road onwards	In an ideal world separate coned cycle lanes would be in operation but due to roads being too narrow and in order for cyclists to feel reasonably safe, speed limits must be reduced for motorised traffic from 60 mph to 40 mph maximum on rural roads between 30 mph towns and villages to help avoid potentially fatal accidents involving cyclists and horse riders too. Ultimately we want more people on bicycles for commuting as well as leisure but safety is paramount if this is to happen.	As above. Will obviously also benefit pedestrians/those trying to cross increasingly busy roads.

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Rachel Summers	253	Easton to Kettleburgh Road, big dip in road about 0.75m from verge going up the hill into Kettleburgh, catches cyclists and motorbiked out.	Raise grate and level road	
Richard Atkins	318	Bike paths via Murrills Road park	The barriers at Murrills Road & Bucklesham Road are tight to get a cargo bike through. Cars are often parked at the Meadow Crescent entrance/exit.	Increase gap of barriers at Murrills Road & Bucklesham Road. Add 2m of double yellow line at Meadow Crescent.
Richard Atkins	319	Edge of A1156 adjacent to path through from Murrills Road	Lack of footpath to the pedestrian lights to cross the A1156, worn grass track (sometimes muddy), in danger of being overgrown by gorse bushes.	10m length of path to connect the North-South path from Murrills Road to the piece at the pedestrian controlled traffic lights.
Richard Atkins	320	High Road Trimley nr Faulkeners Way	Cars parked in cycle lane and even on cycle path approaching mini roundabout.	Solid white lines and no parking in bike lanes with enforcement.
Richard Atkins	322	High Road East, Felixstowe, & out through Trimleys	Cars regularly parked in cycle lanes	Change from dotted to continuous white line and enforce no parking in bike lanes.
Richard Dolan	292	All Schools.	If we want to increase safe cycle usage it should start with young people so that it becomes absolutely normal to cycle, and especially to school.	I suggest that all schools have a cycling policy produced by stake holders eg teachers, parents, students, police, local council, etc. The policy would include among other things: Suggested safe routes to school from all the main centres of population that feed into the school. And perhaps roads that should be avoided as unsafe for cyclists to use. The council should consider providing suitable signage for cyclists and other users along the routes. Safe dry cycle storage within the school. Safe storage of helmets, hi-vis clothing. Cycling competency certification schemes.

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Richard Dolan	288	From Melton Station to the roundabout near Wilford Bridge and onwards up the hill towards Sutton Hoo.	This is a very busy piece of road. Many motorists seem impatient and overtake inappropriately. I have had several close calls along this road. I now find it too dangerous to cycle which means I can no longer cycle to Shingle Street except by a roundabout route or I go early Sunday morning. Ideally there should be a cycle lane separated from traffic but this is not a cheap solution.	Cycle lane.
Richard Hugh Morling	537	Tonning Street/Bevan Street East Junction	When following the 517 (30) cycle route along Tonning Street there is no drop kerb at the traffic lights to go across to Bevan Street	Make a drop kerb near the traffic lights
Richard Hugh Morling	538	Denmark Road, South side cycle track	Concrete bunker makes it difficult when passing, not to go into the road	Remove bunker
Richard Hugh Morling	539	Denmark Road, south side. near junction with Rotterdam Road	When reaching the end of the cycle track you have to go on to the road. You cannot cross to the cycle track on the other side as there is no drop kerb at this point on the north side.	This may all change with the construction of the new bridge. All the cycle tracks at this point should be reconsidered
Richard Hugh Morling	540	From the roundabout at the junction of Corton Lone Lane and A47	In addition to the lack of cycle lanes to the north of this junction on the A47 to Hopton. There are very few direct cycle lanes along the A47 to the centre of Lowestoft. There are good lanes along the new Millennium Way and also around the back roads into Lowestoft, but not a direct route down the A47	Please see if you can introduce lanes south, along the existing A47
Richard Hugh Morling	541	Gorleston Road, west side between Mobbs Way and Dunston Drive. Oulton	A build up of vegetation and leaves over the past 2 years has reduced the width of the footpath. This means that if a mobility scooter is coming on this path any other scooter, buggy or pedestrian has to walk into the road to get past.	Remove all debris from the tarmac footpath. The footpath extends to just behind the lamp posts and this will double the width of the footpath. My wife has rung up a number of times about this.
Richard John Dolan	467	Footpath alongside Woods Lane heading	Observed Farlingaye School students going home to Melton village. Some were walking, others cycling. There were also other	1. Find an alternative safe route for school children who live in Melton village and beyond. This might involve new permissive paths, resurfacing, etc. Basically Woods Lane

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		down towards Melton traffic lights.	pedestrians. Those on bikes had chosen to ride on the pavement as the road is busy and often has large vehicles and is not wide. It is therefore safer on the pavement. However the pavement is not wide enough to accommodate everyone safely. The problem is aggravated by the steepness of the hill. I am a regular cyclist and don't use Woods Lane.	is unsafe for cyclists. 2. Have a proper dedicated cycle lane. This would probably involve widening the road or the pavement.
Richard Millner	652	It is a pointless exercise suggesting improvements to local infrastructure unless there is a coherent plan for cycling in Lowestoft.	<ul style="list-style-type: none"> - Lowestoft with its relatively flat terrain and low car ownership should be leading the way. - Instead there is a mish-mash of side streets and a few reasonable cycle routes. Few join up and almost all end in dangerous exit points at roundabouts and junctions. - Few routes are safe for children - No attempt to encourage cycle tourism, such as routes from the station to Oulton Broad or Carlton Marshes, or even signage to the beach! 	<p>First, come up with a proper co-ordinated strategy for cycling in Lowestoft not just minor cosmetic improvements (I would be happy to contribute).</p> <p>Secondly prioritise safe direct routes into town that you would be happy to let your children use.</p> <p>Thirdly, encourage cycle tourism by making Lowestoft a hub for routes to the Broads, and along the river Waveney.</p> <p>Fourthly get Sustrans and Lottery funds to make safe cycle tracks not dotted lines on the main road.</p> <p>Finally where there are shared routes with pedestrians, look at ways of separating the activities (eg different coloured surfaces) to increase pedestrian safety and acceptance of dual use routes.</p>
Ricky Andrews	671	Bridge Road, Oulton Broad railway crossing	Can you tell me if there will be provision in your new, Cycling and walking strategy to modify the existing footpath over the railway bridge on Bridge Rd, Oulton Broad? Cycling over that bridge on the road is very off putting to many cyclists including myself. This is actively discouraging cycling in Oulton Broad.	Can the existing pedestrian footpath be modified/widened to accept cycles as well as pedestrians?

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Robert Brockbank	384	Junction of the top (i.e. west end) of Market Hill and west-bound Seckford Street	Firstly, visibility from the top of Market Hill into west-bound Seckford Street is non-existent. One has to pull out across the junction to see if there is anything coming, and if there is, then there is little space for the oncoming vehicle. Secondly, vehicles coming up the south side of Market Hill and turning across the top of Market Hill cut the corner, right into the path of any cyclist waiting to turn right into Seckford Street.	Make the Market Hill a one-way street all the way round, clockwise. This will clear the problem completely.
Robert Brockbank	385	Junction of the top (west end) of Market Hill and the east side	Cyclists going north along the top of Market Hill and wanting to turn east down the side of the Shire Hall have no visibility of oncoming traffic coming down Theatre Street, and so have to pull out to look, into the path of any oncoming vehicle. As vehicle exiting from the top of Angel Lane tend to cause vehicles travelling down Theatre Street to pull out, this means these vehicles are already on the wrong side of the road when they meet the Market Hill junction, thus compounding the problem.	Make the Market Hill a one-way street all the way round, clockwise. This will allow cyclists to get into the right hand lane at the top of Market Hill and have greater visibility up Theatre Street. This will clear the problem completely.
Robert Holder	60	Gorleston Road (as an example)	The cycle lanes throughout Lowestoft all need repainting.	Paint plus workers
Robert Willers	401	Mill Lane Campsea Ashe	Narrow road, high hedges, no footpaths, heavy traffic from agriculture	Mark as unsuitable for cyclists/walkers
Robin Biddle	545	Kirton Road, parallel to A14, Trimley St Martin. Unlit country road.	Trees growing to road edge, leaving no walking space, also forces cyclists out further out into traffic The verges have been mown, but under the trees	Cut back trees as far as ensibly possible
Robin Leonard Sanders	376	Footpath 10 coming from Old Martlesham to the roundabout to the Duke of York pub at	There is a poorly maintained and overgrown footpath that goes right from Footpath 10 and crosses the B1438 close to the roundabout to	With a more easterly crossing of the B1438 and upgrading this route would provide a safe footpath to Woodbridge from Old Martlesham and could be widened for cycle use as well.

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		Ipswich Road/Barrack Road junction.	continue along to the north side of the B1438 to the Duke of York.	
Robin Leonard Sanders	377	Just east of the Seckford Hall roundabout ((A12/B1438)	Footpath 10 from Martlesham crosses the A12 here on a derestricted section of dual carriageway with no marking or warning for drivers of the crossing - extremely dangerous as unsighted for southbound drivers on the A12 until they accelerate west out of the roundabout. On the north side it also connects with a poorly maintained footway up to the B1079/A12 roundabout.	Install a pedestrian control traffic light crossing as per the current footpath crossing the A14 just west of the Dock spur roundabout outside Felixstowe. Upgrade the path up to the B1079/A12 roundabout to pedestrian and cycleway.
robyn Marshall	405	The cycle access at Bloodmoor Road bridge	No cycling access from under the footbridge onto the cycle path. To use a cycle you need to either carry up steps to access or ride cycle over bridge to other side of the road which does not have a cycle path.	Place cycle path from Dale End area of estate on the cycle path of A12 which will take children to Pakefield High School Main entrance. Or place cycle path alongside A12 on School side of the road. Children walk to school because the only other cycle rout is from Bloodmoor roundabout which when coming from the Dales housing estate doubles the journey
Rodney Gibson	290	The service road/cycle lane that runs the southern length of Main Road A1214 along the settlement boundary of Kesgrave.	The cycle path was created from a service road with pedestrian access to shared cycle use. Due to neglect it is unfit for purpose and is dangerous and therefore unused. The surface is poor and the many side roads are hazardous. Cars frequently drive straight out over the cycle path exiting shops/garages. Give Way signs have worn away or are non-existent. Cars park on it (esp near shops and school) again making the case for cyclists to choose the main road.	This is a golden opportunity to do something to put cycling and walking at the centre of transport policy for the future while not actually preventing other road users having access. The land is there to be properly utilised and turned into a modern cycling freeway on a major through route into Ipswich. It needs real imagination and investment.
Rodney Gibson	291	Long Stropps Bridleway, Kesgrave	This is a 2.2km bridleway and walking route with rough surface cycle tracks. This could provide an opportunity for a major cycling through route path to Ipswich.	This is an opportunity to provide a cycling route along the length of Kesgrave which if coordinated with neighbouring villages could be part of a through route from Martlesham to Ipswich.

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Roger Waterfall	305	A12 from Marlesford Road to B1116 (NW side of A12)	To walk to Wickham Market from Marlesford requires several crossings of the A12. The path is often narrow and obstructed. A safe pedestrian and cycle way is required between Marlesford and Wickham Market. I'm sure many of the Council will have driven through Marlesford on the A12. Has anyone tried to walk from Bell Lane to the Framlingham Road (B1116)?	A combined cycle/pedestrian track is required from Marlesford Road to the B1116 roundabout. This should be away from the highway, on the NW side of the hedge.
Ros Jones	81	junction of felixstowe road with main road martlesham	whole of Felixstowe Road dangerous for cyclists due to excessive traffic and buses. road is narrow with narrow cycle lanes. Cyclists have to cycle down middle of the road alongside traffic queues as cars fill the cycle lanes. blind bends and heavy traffic mean many near misses. some collisions have happened with cars cutting in front of cyclists pushing them into the hedge. Turing right at the junction is dangerous for cyclists as cars sometimes pass the cyclist on the RH side during the turn.	Make this one way for cars and buses and make half the width of the road into 2 way cycle lanes. This would mean solving the congestion from the retail park onto the A12 to force traffic to use the bypass instead of running through here to Woodbridge and the A12. But you would need to allow 50cc mopeds through as they are restricted to 28 mph which is dangerous on a dual carriageway.
Ros Jones	82	alongside felixstowe roaad	when walking alongside this road on the footpath in or after rain pedestrians get soaked by cars spraying water from puddles. There is no where to get away from this and it can be significant. I carried shopping home along here one day and my shopping bag was drenched inside with puddle water and I had to throw away fresh bread and some fresh produce because of this.	make the road one way for cars and the other half of the road for cyclists and mopeds. The car lane could be furtherest away from the footpath.
Ros Jones	83	main road martlesham	lack of safe crossing places for elderly and vulnerable persons. The road down hill from Crown Point is heavily used (88000+ cars per week) and uphill has a high % of speeding traffic. No pedestrian crossing anywhere along	pedestrian crossing over Main Road near Black Tiles (upgrade the existing refuge ?) and another across junction with Felixstowe Road at Crown Point.

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			this road. There would also need to be pedestrian crossing across the junction of Felixstowe Rd by the fish shop to connect up a safe route to rural martlesham homes. blind man and his guide dog hit by car as he tried to cross main road downhill on 14 september 2020.	
Rose Westall	70	End of pier avenue (town end) Southwold	When walking to southwold from Reydon where I live with my young family it is very difficult and dangerous to the cross the road at pier avenue. Southwold is very busy with traffic making it very difficult to cross over, with or without a buggy and a toddler on a bike.	A pedestrian crossing or similar,.further up pier avenue for safety.
Rose Westall	71	Jermyns road, entire length	Jermyns road is a road with Reydon primary school just off it, it is very dangerous with fast traffic. My son rides his bike to school but I am fearful of the traffic and would appreciate some traffic calming measures, as in most areas with a school on/near the road	Traffic calming, 20 mph limit
S WJ Hodgkiss	262	Road between Martlesham and Woodbridge	Currently there is no continuous footpath between Martlesham village and the outskirts of Woodbridge Town. This leads to many unnecessary vehicle journeys as use of a car is the only safe way to move between the two centres, a distance that many would be happy to walk if a safe pedestrian route existed.	Provision of a footpath along those sections of the road that currently do not have a footpath.
S WJ Hodgkiss	263	The entirety of the Martlesham retail development.	There is no pedestrian walkways between the myriad of large shops on the new retail development at Martlesham. Whilst the lack of footpaths was acceptable when this was a mainly commercial area, the explosion of retail outlets and consequential increase in footfall has meant both pedestrians, cyclists and	Provision of a complete footpath network linking all the parking and shopping areas such that by parking anywhere within the retail park area you can walk to any of the retail stores without having to walk along a roadway, with safe crossing places provided where any paths ways cross the road network.

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			motorists are now at considerable risk as they move about this area.	
S WJ Hodgkiss	264	General consideration of the motorist as a part of the cycling and walking strategy	The growing positive bias in council policies and strategies towards walking and cycling seems at times to be bordering on a demonisation of all motorists. Any new initiatives should take into account Suffolks rural environment and the need for many people - including the aged or disabled - to make journeys that are not viable on foot or by cycle. These people and their needs do not seem to be given due consideration in some of the rushed often ill-conceived initiatives that are proposed.	<p>Ensure full and due consideration is given to all classes of road users when creating any schemes that seek to offer improvements to the built environment. Fulfilling the demands of any particular pressure group will undoubtedly lead to a less than optimum solution for the general populous who after all are the majority...</p> <p>In respects to all proposals there should be full consultation with all user groups prior to any initiative being taken forward, its especially important to reach out proactively to those who do not have the technical knowledge or access to the mainly internet focused mechanisms that currently form the backbone of the consultation process.</p>
Sally Adams	575	Between Corton Long Lane, Lowestoft, NR32 5, GBR going northwards to Hopton roundabout lack of cycle path/footpath	Cyclists/pedestrians/currently use the busy A47 or the bendy coast road B1385 which has no footpath. As a motorist I see the dangers of cyclists using this fast dual carriageway, even if they are entitled to, but people make bad choices. I have even seen a person in a mobility scooter using this road. Death wish. As a cyclist and pedestrian I use the coast road every time, but it is bendy, there is no footpath and it is a bus route.	It would be a great amenity and so much safer to have a cycle/footpath between Corton Long Lane, Lowestoft, NR32 5, GBR going northwards to link with the existing one at Hopton. It might help mobility scooter users too. Near the roundabout there would ideally have to be some kind of crossing point to link users into the existing track past St Margaret's Church, Hopton (grid ref: TG 5241 0004) on the old Lowestoft Road.
Sally Adams	576	The clifftop cycle path/footpath at Pakefield going from The Jolly Sailors. Pakefield Street, NR33 OJS, to Arbor Lane	It's rather narrow for the amount of users it gets, especially at weekends and peak holiday times. The path is used by pedestrians, dog walkers, people in mobility scooters and cyclists and there has to be a lot of give and take between them. It can be snail pace for cyclists.	Widening of the route and having a dedicated cycle path would make life a lot easier for all concerned and allow cyclist to make progress.

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Sally butcher	117	Morston Hall Road. Trimley	This link road between the old A14, Felixstowe road and Trimley st. Martin. It is used by busses, local residents, cyclists and speeding motorists that would be better off using the actual A14. I suggest that it be used as a cycle and bus lane only with local residents access. It would give a safe route for the above to travel between Felixstowe and Ipswich. The cycle lane actually alongside the A14 is not fit for purpose. It's rough, bumpy and has heavy traffic thundering past making it unsafe.	Local residents of Morston hall road , bus and cycle lane only 20 mile an hour speed limit
sally louth	255	Wilford Bridge Melton	This is a dangerous road to cross for pedestrians using the footpaths either side of the river and also bad for cyclists too.	Slowing traffic down so pedestrians get a chance to cross the road ,or narrow the road to slow traffic down and widen the pavements which could then accommodate a bike lane.
sally louth	257	Grundisburgh road B1079 and Grove Road roundabout , close to garden centre	It is very difficult to cross the roundabout on foot or by bike to get from Woodbridge town to the garden centre and/or beyond. The pedestrian lights further up the A12 are not a direct route for pedestrians .Traffic does not always stop at these lights as it tends to speed up after the roundabout.	A better crossing for bikes and pedestrians , closer to the roundabout. Or reduced speed restrictions on this stretch of road between the roundabout and existing traffic lights
Sam McDonald	99	New road junction.	A very ill thought out cycle path. If coming towards the a146 down new road you have to cross over the road to get onto the cycle path. You have to look out for drivers turning right onto new road, and left onto new road. Visibility is poor to see if a driver is turning left off a146.	Extend cycle path up new road so you can get on it before the junction or a new path and crossing on the left of the road.
Sam McDonald	100	Roundabout A145	Poorly thought out cycle path for cyclist. Safest way to get onto the cycle path is heading south along the B1062. If heading north onto the roadabout from the a145, you have two choices head straight onto the B1062 then stop	If heading heading west along the cycle path to join traffic you have to cross over a busy road with limited visabilty from the left. Dropped kirb to join cycle path on the a145. A middle

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			in the middle of the road to cross onto the cycle path. Or turn right onto the a145 heading east then get stuck on the road or hop up the kirb at the safest opportunity.	island on the b1062 to wait and cross in to rejoin to head north.
Samantha Coe	414	Access to Wickham Market Train Station in Campsea Ashe from Tunstall	Dangerous road for cyclists and walkers, pot holes are uneven surface on edge of road on Ashe Road, very sharp blind corners and road is regularly used by lorries. This means poor access for both cyclist and walkers to the train station. Public transport in this area is poor so access to the train station is vital for allowing people greener methods of transport.	The best solution would be cycle lanes and footpaths that allow direct access between Tunstall and Campsea Ashe or alternatively follow the road. Alternative solution would be improving Ashe Lane and adding protected cycle lanes.
Samantha Coe	415	Cycle access between Tunstall and Woodbridge	There is poor cycle access between Tunstall and Woodbridge the next proper sized town. The main road is busy, poorly lit and fast moving and not particularly safe for cyclists or walkers for that matter. Many people in villages have to rely on cars when proper cycle access may encourage people to be greener. There is also a lack of access to local schools in neighbouring villages and the high school in Woodbridge.	Dedicated cycle paths linking Woodbridge and Tunstall would be valuable as it would allow village residents access to the facilities of the town centre while reducing traffic in Woodbridge. It would also allows those in Woodbridge dedicated cycle lanes linking them to Tunstall forest. This would give more people in Woodbridge the chance to enjoy the countryside and forest. It could also provide safe access for children to go to school by cycling rather than car or bus.
Samantha Coe	416	Snape Road, Tunstall Footpaths	There is a big gap between the footpaths on Snape Road meaning walkers have to walk on a blind bend to get to the next footpath. There is currently a footpath to the forest between Walk Farm Road and Snape Road. The next footpath on Snape road is much further down the road and you have to walk round a blind bend. This is one of the quickest access points to the forest from the village for walkers.	The current right of way could be made into a t shape rather than an l shape allowing for 2 points of access in the field. Alternatively a path could be installed on Snape road.
Samantha Morley	123	Purdis Heath SSSI - Purdis Farm Lane at the junction with Purdis Avenue	New fences with stiles have been erected in the past few weeks along with a large gate across the wide path. It looks like the plan is to be able to close the gate to prevent any	Stiles should not be being installed on any footpath without also providing a gate big enough for a large wheelchair or mobility scooter. This applies to all areas.

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			vehicle/bike access but it's not clear whether there will be access for wheelchairs or buggies. We regularly use this path with a wheelchair buggy.	
Sandra Tricker	338	Junction of A12 and The Street, Darsham	Twice we have used the train from/to Ipswich to/from Darsham Station to ride out to the coast. We used the cycle path beside the A12 to get to 'The Street'. It was extremely difficult to cross the A12, traffic in both directions was continuous and travelling fast (possibly faster than the 40mph speed limit) and we had to wait for a considerable time for a gap in both directions before being able to cross SAFELY. My suggestions for improvement are shown below. Not safe for adults let alone children	<ol style="list-style-type: none"> 1. A signalised crossing for pedestrians and cyclists 2. Lower speed limit on the road at this point 3. Advance signs warning of cyclists and/or pedestrians crossing. 4. Painting SLOW PEDESTRIANS/CYCLISTS CROSSING on the road in each direction. 5. Install a speed camera at this location. 6. Install a central refuge to allow the road to be crossed in two stages.
Sandra Tricker	339	Cycle path alongside A14 dual carriageway near Morston Hall Road	Using this cycle path is unpleasant and very scary being so close to fast moving traffic on the A14 with NO crash barrier. I prefer to use Morston Hall Road but this is not wide enough for cars to pass cyclists.	Provide a cycle path adjacent to Morston Hall Road away from A14.
Sandra Tricker	340	End of Thurmans Lane, Trimley & Bridleway to Gulpher Road	The bridleway provides a safe link from Thurmans Lane to Gulpher Lane towards north Felixstowe and the ferry. The condition of the path is poor and rutted in places and becomes muddy.	Improve surface of the Bridleway
Sandra Tricker	341	NCN 51 link between Manor Road and the southern end of Promenade, Felixstowe.	The gravel surface of the link between Manor road and the promenade (part of NCN 51) is unsuitable for cycling. The surface is uneven and the gravel is deeper in places and difficult to ride through and could be dangerous for inexperienced cyclists, especially children. After rain there are a number of deepish puddles. This would certainly not be acceptable as a promoted cycle route in the Netherlands and	<p>Provide a suitable surface on one side of the path, clearly marked for cycles and on which car parking is banned.</p> <p>The promenade and path across Landguard Common provide one of the few offroad routes available for parents to introduce their children to cycling and this poorly surfaced link needs improvement.</p>

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			nor should it be in the UK! Parked cars can also obstruct the track.	
Sandra Tricker	345	Ferry Road to Felixstowe Ferry	This route is popular with cyclists and is part of the NCN with the ferry link across the River Deben. The C class road is quite narrow, twisting and tightly hemmed by the golf course on each side. It is quite scary being overtaken by close passing and relatively fast moving motorised traffic (cars have grown in size over the years).	A 20mph speed limit would be more appropriate for this road which forms a dead end for motor traffic. The road could be marked with cycle lanes each side and a central lane for motor vehicles with drivers having similar to Felixstowe Road between Anson Road and Main Road at Martlesham.
Sara Barratt	413	Cycle path/pavement along Tom Crisp Way into Lowestoft	Separate pedestrian and cycle ways. Dog walkers, people with children and prams/pushchairs etc have very different requirements from cyclists. This is particularly bad over the Bascule bridge which is marked as combined cycle path/pavement but not really suitable	Clearly mark the pedestrian and cycle parts separately. Consider adding dedicated cycle lane on the road where pavement can't be widened
Sara Barrett	789	See attached.	See attached.	See attached.
Sarah Barrett	108	A 12 cycle path from Kelsale to Hinton is not maintained and is largely therefore unsafe to use.	Both the surface and surrounding hedgerows etc are not maintained and the cycle path in many places isn't usable, so you have to cycle on the A12, which is often quite unpleasant on a bike among fast, heavy traffic	Maintain the cycle paths
Sarah Morrison	640	Enable footbridge to take cyclists so they do not use A14 Roundabout	The A14 Roundabout is perilous for cyclists - enable the footbridge to take cyclist and pedestrians safely	Widen foot bridge and encourage cyclists to use bridge rather than attempting the A14 Roundabout from Kirton to Trimley St Martin, cars need to slow down. This is where a number of accidents have taken place with cyclists.
Seamus Bennett	389	walkway/promenade from Cobbolds Pt to Felixstowe Ferry (especially from the Dip toilets northwards) .	This can be an ideal shared use route for cyclists to reach the Ferry off-road, avoiding fast-moving traffic and other hazards (!) on the road through the golf course. Cycle access easy at the Dip.	As with the prom south of Cobbolds Point, more clear signage is needed to ensure safety and consideration of all users, especially cyclists being considerate of and giving way to walkers, but also walkers looking carefully when joining prom or changing direction while walking.

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Seamus Bennett	442	Cycle path alongside A14 between Goslings Farm & Levington turn-off	cycle path surface quality is VERY poor throughout this stretch - very bumpy, strewn with debris, high risk of punctures. This increases temptation to ride along the parallel bus route (Morston Hall Road) which is fast and smooth, but not intended for cyclists and probably slightly dangerous and may cause delays for buses etc.	properly resurface (not just patch up) this fairly short stretch of cycle path, with a slight camber to keep surface clear of debris and standing water. Also trim back adjacent hedges. At same time consider widening the cycle path to allow two cycles to pass in opposite directions - there appears to be sufficient space for this, along most of the stretch at least.
Seamus Bennett	443	Mill Lane into town centre - lack of safe cycle route	there is no safe cycle route into town centre from the Coronation Park / Wesel Ave / Grange Road area - one of the most deprived areas of the town. Such areas have been shown to have far lower than average access to a private car. e.g. in poorest areas of Lowestoft up to 48% of households have no access to private car (2011 Census.)	create / build a high quality cycle route connecting Grange Farm / Coronation park area to town centre, potentially via Mill Lane. Due to the high prevalence of on-road parking on Mill Lane, it may be necessary to utilise the existing pavement(s) to allow shared or dual use between cyclists and pedestrians.
Seamus Bennett	651	from the Dip northwards to Felixstowe Ferry along sea wall/ promenade	to be consistent with Prom south of Cobbolds Point, allow cycling access as shared use with pedestrians along prom/sea wall north off Dip. This will mean children / families won't have to use fast section of Ferry Rd through golf course if they wish to get to Fx Ferry - a popular spot for families. Also, Fx Ferry as a dead end, has a traffic and parking congestion problem, which improved cycle access to the hamlet would help mitigate.	Give permission for considerate cycling, while maintaining pedestrian priority. Narrow stretch near Cliff car park may need widening or signs for cyclists to dismount for this short stretch.
Selena Levermore	434	Old railway line between Aldeburgh and Leiston	Absence of safe cycling route for tourists and residents between Aldeburgh and Leiston. Roads are too dangerous and existing cycle route along coast path isn't accessible for most. We don't have an easily cycled tourist route like other parts of the country.	The old railway line between Aldeburgh and Leiston provides an ideal route. Starting from the caravan park, heading along the old line, across the road at Thorpeness holt, continuing along the line route until Crown Farm, this would join the existing cycle path along Lovers Lane, a new extension proposed by EDF (DCO) and Leiston's Cycle Strategy route into Town. A tarmac track (Suffolk's version of the 'cinder trail' - route 1 of National cycle

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				network) would give access to many more residents who cannot currently cycle easily or safely between the two towns for work/recreation. It would be a boost for tourism as more people would access the route as a flat and easily cycled surface. E Bikes could be promoted to reduce car journeys.
Selena Levermore	551	Old rail line running between aldeburgh and crown farm, lovers Lane, leiston, sizewell	Restore old rail line route from Aldeburgh to leiston (crown farm junction) a hard surfaced cycle route for tourists. This could then be extended through to Southwold	Suffolk's own cinder track for cyclists. Smooth hard surface available to all and not just hardcore 'off roaders'
Seth Bannister	611	Langer road safety issues for cyclists and pedestrians alike	Langer rd is a straight length of road with a primary school & playgroup located on it. Due to the lack of any speed restrictions, traffic calming or cycle lanes, children cycling to school & workers cycling to & from work at the port are at risk on this road. Drivers consistently speed leaving cyclists at risk and forced to use the paths. The schools lollipop lady is in constant fear of speeding motorists. Residents are blighted by speeding cars & children travelling to school will be hurt.	The road should be a 20's plenty as a minimum!!! The safety of the children is most at risk. Most schools have this measure but Langer Academy on Langer road does not. Speed cameras should be installed or police monitoring increased. One side of the road has a wider pedestrian path than the other. It should become a mixed cycle/pedestrian path to aid children in their travel to school, this could then should be connected to Langer park's path via marina gardens to encourage walkers and cyclists to stay away from the dangerous roads and use the facilities on Langer park which is looking to be improved by the council.
Seth Bannister	612	Felixstowe Promenade	Lack of continuation of cycling and walkway connecting Felixstowe to Old Felixstowe	The promenade should be continued for the full length of the coast line between Felixstowe and old Felixstowe encouraging runners Walker and cyclists.
Simon Daws	57	The whole of Sandy Lane from old Martlesham to Woodbridge	There is currently no safe pedestrian access from Old Martlesham to Woodbridge. Would strongly recommend installing a footpath full length of Sandy Lane from Top Street Martlesham to Ipswich Rd Woodbridge.	
Sorrel Sweeney	144	Footpath	Students walking through Otley bottom to Post office are a road hazard and often cannot be easily seen. Would also encourage locals that work at the college to walk to work.	To encourage locals to walk to work and to provide safety for students who always walk to the post office, provide a footpath. This will get them off the road, and

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				reduce road hazards where traffic is fast through Otley bottom.
Southwold Town Council	673	Southwold Town Council	STC would like to support references that have been submitted already, namely: Refs: 333, 34 and 102 combined, and all references to the Coastal Path from north of the pier through Eastern Bavenes.	
Southwold Town Council	674	B1127 Lowestoft Road	The B1127, Lowestoft Road is particularly dangerous for walkers and cyclists and safety measures to improve the lot of each would be welcome.	
Southwold Town Council	675	Rear of St Felix School	A new cycle route from the rear of St. Felix School to Southwold is worthy of examination although there is no preferred route, per se.	
Southwold Town Council	676	East Suffolk	In seeking to improve the cycling and walking experience in East Suffolk the safety of each is paramount - from separating them from motorised transport to ensuring that mountain and trial bikes do not despoil the environment by increasing erosion.	The most essential aspect for me, from a cyclists' viewpoint, has a to be ensuring that no parking is allowed in any cycle lane; it's crazy and euphemistic! Any mitigation, by way of educating and persuading car users to reduce their dependence upon the motor car, would be welcome, even to the extent of escalating car parking charges, perhaps?
STC Cllr J Jeans	509	Reydon-Southwold cyclingpedestrian links	<ol style="list-style-type: none"> 1. The existing cycle lane Rt 31 goes over the bridge and stops before the most dangerous junction which is crossing into the Wrentham Rd. 2. No safe cycle crossing point onto the Wangford Rd. 3. No cycle route linking the proposed Copperfield Road development and the development proposed on land owned by the NHS around the surgery to Southwold. 4. Cycle lane on the East and West sides of 	<p>Improve cycle crossing points to Wangford Road and Wrentham Road by extending marked cycle land and showing the cross point with signage giving cyclists & pedestrians right of way.</p> <p>Create an unbroken foot path cum cycle path linking the new developments to Southwold via Keen Lane, the footpath from St Felix School to the Bund footpath on Botany Marsh through to the Blyth footpath leading to Station Rd and the Bailey Bridge. This would create a car-free linkage between the three parish/towns of the Southwold Ward. It would also benefit the proposed development on the St Felix playing fields. We would like to discuss this in more detail with you as this</p>

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			Mights Road has broken lines, creating risk to cyclists overtaking parked cars.	infrastructure improvement could have the greatest impact for cyclists and pedestrians. Distinguish cycle routes from car routes with unbroken lines to prevent parking. SCC should keep cycle lanes clear of debris and localised flooding from blocked drains.
STC Cllr J Jeans	511	Cycle Rt 31 between Beccles and Southwold in the area between Clay Common and A12	Road is in a poor condition and difficult to cycle on -- dropped drainage, etc. There is insufficient signage on the A12 that this is a cycle route crossing the A12.	Sign on north side of the A12 indicating designated cycle path crossing point. Modest repairs to the stretch of road identified above.
Stephen Denton	91	The Old Felixstowe Road is part of the national cycle network and is also a commuter route for cyclists between Woodbridge and the employment area at Martlesham	It is marked with cycle lanes on each side but they're far too narrow, especially at the north end where they're overgrown and there's a blind bend Some motorist assume that the lane markings means that it's safe to pass close to the lane marking, not so! It's 30 mph but there are no signs to remind users of this and although there are street lights - they're dim at night and scarcely visible during daytime. Spacing between some is too long to be legal indication of the 30mph limit.	1) clear out the over growth 2) remove the cycle lane markings and - they are more dangerous than having none 3) make the speed limit 20mph with proper signage to indicate this is a cycle route 4) improve to the lighting 5) ideally put chicanes in place to discourage motorist from using the route. See also my separate comment re the Sandy Lane speed limit which is part of the same Cycle network Route
Stephen Denton	96	Sandy Lane between The Street and its junction with California north of the railway bridge	This is a derestricted section connecting two 30mph areas. It's part of the National Cycle Network serving commuters and businesses on Sandy Lane south of the railway. The Parish council has been asking for several years to have this made 30mph on safety grounds. Nothing has happened. To encourage sustainable transport this key part of the only viable cycle route between Woodbridge and Martlesham need be improved, as does the Old Felixstowe Road.	Make the section of Sandy Lane between The Street and California a 30mph area. The attached satellite view gives a good impression of the number of business along that road.

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Stephen Mayhew	402	The whole of Byng Hall Road but particularly where it passes the houses up to the underpass of the A12	Concerns around visibility here particularly around the Public Rights of Way path that has its entrance/exit on the inside of the bend outside "Wayside", and the visibility along Byng Hall Road for both vehicles travelling in opposing directions and the pedestrians/cyclists/equestrian users. 2 speed roundels (outside Wayside & Woodcott) that have been consumed by the vegetation. The encroachment of the verge onto the carriageway on the eastern side of Byng Hall Road.	To complete the work highlighted from the site visit and then either introduce 20 mph speed limits or designate as a Quiet Lane
Stephen Mayhew	403	Spring Lane from the High Street to Lower Ufford	Single track road often used by pedestrians, cyclists and equestrians that is very tight with some blind bends. Danger of accidents with some of the aforementioned parties with vehicles. Often overgrown and often not able to drive down in a car without the vegetation coming in to contact with the vehicle	Vegetation control (cutting) and Categorise as a Quiet Lane
Stephen Mayhew	404	Lower road Ufford - the entire length.	Single track road often used by pedestrians, cyclists and equestrians that is often flooded and muddy.	Look at improving the drainage and because of the frequent use by pedestrians, cyclists and equestrians designate as a Quiet Lane.
Stephen Read	427	Felixstowe Road, Martlesham.	Ref 145 already reported	Totally agree with comments. Priority for Cyclists route is now dangerous due to volume of traffic. Needs to be one way with cycle lanes each side. Part of National Cycle Route 1 so should be a high priority.
Steve Barnes	35	Trimley St Martin	6 pathways leading to open countryside have been closed across the railway line. This hardly promotes improved walking and cycling access.	Reinstate those crossings where there is still only one track to cross so not making the pathways any less safe than before.
Steve Falvey	423	Exampler - Snape Maltings but applies to towns, villages and popular visitor locations.	Provide or assist businesses in providing sufficient good quality and secure cycle parking. These need to be in high footfall areas with	As above.

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			<p>CCTV and good lighting to discourage theft. Cycle lockers at station and other transport hubs would be ideal.</p> <p>Unless cyclist feel confident that there are good cycle parking facilities that are safe they just won't visit these places.</p>	
Steve Read	615	Carlton Marshes	<p>creating a safe cross-country cycle route between Oulton Broad and Norwich making use of the re-established ferry crossing of the River Waveney at Burgh St Peter and the ferry crossing of the River Yare at Reedham. This continues to be a high priority objective of the BLAF.</p>	<p>Within Suffolk the route could commence at Nicholas Everitt Park in Oulton Broad and following either Footpaths 15 or 14 westwards to the newly established Suffolk Wildlife Trust Centre at Carlton Marshes. At Carlton Marshes these FPs link into Bridleway No 4 which goes northwestwards towards the River Waveney. Some 500 metres from the River Waveney the route to the ferry follows FP No 10 which sits on top of the Floodbank.</p> <p>The use of Footpaths for cycling may require upgrading the status of the highways to Bridleways although it is understood that there are other options available to allow cyclist to use Footpaths.</p>
Steve Rogers (Originally submitted by email)	546	the Denes to Kessingland	<p>Great to see this subject being considered, particularly at a time when cycling & walking are likely to play more important roles in all our lives.</p> <p>Being a keen cyclist, I've always been impressed with the amount of cycling paths and lanes but, understandably, a number of these were put in place probably decades ago and the town has changed around them.</p>	<p>The Third Crossing will obviously impact traffic volumes and flows, and hopefully be one factor in providing opportunities for improving cycling and walking paths, particularly where these can be provided alongside, rather than necessarily sharing, the same road as vehicles.</p> <p>In that respect, there could be an opportunity to join up, or create, a coastal cycle & walking path, running from the Denes to Kessingland? That would potentially allow people to travel safely from one end of town to the other, mostly away from traffic. And something to support the promotion of the Sunrise Coast, too.</p>

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STEVEN BROWN	47	Between Nacton and Trimley	Lack of safe walk routes between Nacton and Trimley	use 1/2 of the Felixstowe road as a cycle track and walkway
STEVEN BROWN	543	Gaymer's Lane	A safer way to cycle to Trimley was via a path on to Gaymer's lane (then the new Bridle way) from St Stennetts Close, (come up the Avenue) but someone has now blocked this.	removal of barrier
Stuart Pryce	542	Ellough Road between Cedar Drive, Worlingham and Ellough Industrial Estate	There is no direct walking/cycle route between Beccles/Worlingham and the major employment area of the Ellough Industrial Estate. The road is a 60MPH limit with bends and the brow of a hill which obscure vision. It's a significant diversion to avoid this section of road. Pedestrians currently use the verge and adjacent fields which is obviously exceptionally dangerous, especially in the dark. Cyclists suffer close passes as motorists frequently overtake only to encounter oncoming cars.	A shared use cycle and pedestrian path from Cedar Drive to the Industrial estate. Ideally this would continue through the estate, connecting residential areas with individual places of work in this major centre of employment. This path would also link up with the eastern end of the cycle/pedestrian path adjacent to the relief road, creating a traffic free circular recreation route for families/exercise etc.
Stuart Whayman	429	Walking path required along side Orford Road besides Woodbridge Rugby Club to provide safe walking from path between path emerging opposite from Eyke Road to track to Potter's Woodyard.	The Orford Road is a busy road with fast traffic and at times lorries. There is a path which links the Eyke and Orford Road's which emerges opposite the Club but to reach the path opposite one has to walk up the busy road side. This is far from safe.	Clear a passable footpath in the grass verge alongside the Rugby Club
Stuart Whayman	430	Orford Road opposite Bromeswell School Lane	There is no defined path from the bridle way over Woodbridge Golf Club to the Bus Stop. This is part of the Sandlings Way and yet is not a well defined path and is very dangerous given the speed of traffic on this busy road	A very short well defined path to connect the Sandlings way on the Bridleway over Woodbridge Golf Club to the bus stop opposite School Lane
Sue Barnard	187	Lowestoft High Street, south of A47 near petrol	The High Street has some interesting shops such as a zero waste shop, a bakers, Post Office, but the number of cycle racks there is	

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		garage and north of A47 near Artillery Way	extremely limited. It is a main route into Lowestoft from the wards of Gunton and St Margaret's and really should be better served with bike racks.	
Sue Barnard	188	Hopton to North Lowestoft lack of a cycle route either along the A47, the coast road from Corton to Hopton or on bits of the old railway line.	There is no dedicated cycle route from north Lowestoft to Gorleston or Yarmouth. There is a dedicated cycle path alongside the A47 in Norfolk, from Gorleston to Hopton, after that there is nothing. Cyclists either have to go along the busy A47 or the coast road, which has high hedges, sharp bends and adds distance to the journey. This road is used by tourists staying at facilities in Corton and Hopton, who are not used to tight bends and cyclists. It is a real health and safety issue.	The options are either a continuation of the cycle path alongside the A47 from Hopton to the Corton Long Lane roundabout and possibly a spur off to Oulton Broad or a dedicated cycle route alongside the coast road.
Sue Barnard	189	Top of Lowestoft High Street at its junction with the A47 heading south and the junction with the north bound 2 lanes of the A47 there	If cycling north up Lowestoft High Street, when one comes to the A47 junction, there is no dedicated cycle route north. There is a cycle route south along the A47, but nothing the other way. Cyclists then have to traverse 2 lanes of the south bound A47 at a sharp bend by the petrol station, then cycle to the 2 lanes of the north bound A47 cross these and then get to head north. Crossing 4 lanes of a Highways England road, the main artery from Lowestoft to Yarmouth is a health and safety issue.	Provide a cycle route northwards from the High Street that does not involve crossing 4 lanes of A47 traffic.
Sue Barnard	190	Lack of sufficient cycle racks in the pedestrianised London Road North	There are insufficient cycle racks in the main retail area of town. There should be significantly more to encourage people to cycle into town.	
Sue Barnard	191	Beccles Road to Suffolk Wildlife Trust's Carlton Marshes	There should be provision of cycle hire at Oulton Broad South railway station for visitors to the Carlton Marshes reserve who arrive by	Either a dedicated cycle route by the Angles Way route from the reserve to Oulton Broad or a dedicated cycle route along Beccles Road.

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			train, also a dedicated cycle route from the station to the nature reserve. This would assist ecotourism, visitor numbers to the reserve and assist locals cycling in the area as well.	
Sue Barnard	244	Slip roads gap beside A47,	Purposefully blocked footpath and cycle path that has been in use for a minimum of 40 years. The access between the slip road from Gunton Church Lane going north west has been blocked by Heras fencing, soil and twigs, even though there are 2 concrete bollards denoting where there is access. The blocking of this path, means that cyclists and pedestrians, including school children now have to go on to a very narrow path beside the A47. This does not fit with the active travel policy.	Unblocking of the gap to allow access and so maintain active travel away from the narrow A47 and narrow paths to the side of this road that are often overgrown with vegetation that narrows them further.
Sue Barnard	245	At the end of the sea wall, at the end of Hamilton Road by the Onward.	Only steep step access, which is very steep, at the end of the sea wall, which is supposed to be part of the national coastal path. Bicycles using the sea wall to gain access to and from the town have to cycle through an industrial estate to Ness Point to get to the sea wall. It is near impossible to get a bike up these steps by yourself.	A ramp would be ideal for cyclists and pedestrians, including those who have mobility difficulties.
Sue Ford	352	Part of the Sandling walk, from Blaxhall to Snape on the busy Snape road (B1069)	Part of the Sandling walk that goes from Blaxhall to Snape is signposted down the busy Snape road with no footpath option. Very unsafe to walk or cycle to Snape Maltings down this stretch	There is a wide overgrown banking on one side of the road which could maybe be removed to make a footpath/cycle lane. If possible, a path from Blaxhall Common through the woods joining up with this would also be advantageous instead of walking the road into Blaxhall too.
Sue Hall	627	Sandy Lane	This is a key desire line for cyclists between Ipswich - Kesgrave - Woodbridge. There really isn't any other choice for on-road cycling. The A12 is even more dangerous for cycling and so is the "hairpin bend" route going through old	Please can Sandy Lane be closed off to motor vehicles part way along this route as motorists have an alternative through route they could use instead of using Sandy Lane. Please can it also be made a 20mph zone which would make it safer for pedestrians/walkers (e.g.

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			Martlesham. But Sandy Lane is unsafe and unattractive to use due to motorists - including van drivers - trying to squeeze past at speed. A particular area of concern is under the railway bridge where the road is narrow and turns sharply and cyclists get squeezed.	it's a leisure route for those walking along the riverside area, walking a circuit). A reduced speed limit would also help those of us who struggle to get back up the hill at the Woodbridge end and of Sandy Lane and sometimes walk with our bikes!
Sue Hall	628	The A1214 between Ipswich and the A12 junction and the cycle footways alongside the A1214 that's used for Kesgrave High School access	1) The A1214 between Ipswich and the A12 junction is a key route for everyday transport cycling but is congested/polluted and on-road improvements are needed. 2) The design of the cycle/footways by Kesgrave Fisheries and Kesgrave High School are not fit for purpose and also need repair/resurfacing 3) Damage to the cycle/footways is exacerbated by vehicles driving and parking on them and vehicles also cause obstructions 4) The side road cycle priority crossings have also deteriorated.	1) Make the whole of the A1214 between Ipswich and the A12 junction a 20mph zone with priority for cyclists. It runs past a school and residential housing and lower speeds would make it safer /more attractive for cyclists/pedestrians 2) Widen the road across Rushmere Heath to create dedicated cycle lanes on either side, separated from the footway. And plant Oak/Birch etc trees along the Heath edge 3) Turn the sections of shared cycle footway by Kesgrave Fisheries, Kesgrave High School etc into wide attractive pedestrian-only routes - they are too narrow /dangerous for shared use by cycles/pedestrians/mobility scooters/wheelchairs/buggies 4) Where space allows e.g. by KHS the new pedestrian-only route could be designed and built as a wide and pleasant tree-lined boulevard to accommodate the very high level of foot traffic at school times including buggies, dogs etc. Trees would also help soak up some of the traffic pollution and help improve health, the environment and visual amenity.
Sue Hall	629	A1214 Kesgrave especially its junction with Bell Lane and the section up to All Saints Church and Ropes Drive West roundabout and in the other direction going to Heath Road roundabout	1) The cycle/footway is too narrow on south side of A1214 and at Bell Lane junction and is heavily used for walking and cycling to/from Kesgrave High School 2) There is no pedestrian crossing of the A1214 and this is needed to enable people to cross the road from All Saints Church to access the Cemetery, Carpet Cuts and the bus stop 3) High level of air pollution by The Bell caused by traffic congestion and	Redesign A1214 corridor as safe and attractive for people to walk, cycle and use a bus. Helps address the climate emergency and public health crisis (reduces NHS burden if people can choose active travel). Make the A1214 a priority route for cyclists, buses and disabled users who need to use thier cars. It's a key bus route and First Bus have previously asked for improvements to A1214. In return, ask them - with support from local councils/central government funding - to offer free bus

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			queuing here which creates health risks for everyone- especially car drivers and occupants	use for a month (+ ongoing offers) to persuade people out of cars e.g. The Park and Ride bus service is excellent but few people have tried it. More bus use = less single occupancy car use +less congestion and pollution. Turn A1214 into a 20mph road to encourage cycling, offer free cycle training and bike repairs locally. Redesign the cycle/footway on the south side of A1214 as a pedestrian-only route with pedestrian crossing of A1214 and ped/cycle/bus friendly redesign of the Bell Lane/a1214 junction.
Sue Hall	630	A1214 junction with Bent Lane and Linksfield	1) Pedestrians find it difficult to cross the A1214 here - there are lots of people including dog walkers going to and from Rushmere Heath. There are also people trying to cross here to access the bus stops. 2) Cyclists find it difficult to turn right into Bent Lane (if travelling from the Woodbridge direction). Turning right into Bent Lane involves sitting in the middle of the road waiting for a gap in the oncoming traffic. Feels very unsafe.	Some redesign of the junction to slow traffic down and enable people to cross the road/enable cyclists to turn right into Bent Lane. Perhaps a toucan crossing? It's not enough to put in a right turn lane and traffic islands as experience at the A1214 / Cambridge Road junction and A1214 / Edmonton Road junction indicates that motorists rarely give way and you can wait in the middle of the road a very long time for a gap in the traffic. It feels unsafe. Some priority for cyclists and pedestrians would be welcome. They seem to always be at the bottom of the pile.
Sue Hall	631	A1214 across Rushmere Heath	Key section of route in the corridor between Ipswich - Kesgrave - Woodbridge. Cyclists have no alternative routes available which are safe and convenient e.g. the footpath across the Heath is a footpath - a sandy track across which there is no legal right to cycle and there is also a risk of being hit by golf balls. And the route via Rushmere village is a long detour. If we are to encourage more people to cycle then this key section of route needs some cycling provision. It's a missing link.	Widen the A1214 here to create dedicated cycle lanes on either side of the road, segregated from the pedestrian footway. Widen the footway on either side so it's suitable for mobility scooters, wheelchairs, buggies etc. Plant suitable trees along the edge of the footway and Heath - Birch, Oak etc? and a shrub layer - gorse? to create an attractive and sheltered route for pedestrians and an attractive feature in the landscape. I think the land either side of the A1214 here is Common Land - if so, then can the Council find an area of land, comparable in size and in quality in terms of wildlife/landscape quality and public amenity/access in East Suffolk to dedicate as Common Land to substitute/compensate for

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				that taken? And as an enhancement, perhaps East Suffolk could discuss with the owners any appropriate support for wildlife e.g. a wildlife tunnel underneath the A1214 road if helpful for connectivity for amphibians /reptiles other creatures in lowland heath habitats or other support?
Sue Hall	632	Playford Road between junction with Bent Lane and Hall Road and along Martlesham Road	High traffic speeds. Feels very dangerous to cycle along Playford Road. Also drivers often play chicken - overtaking me on my bike when there is oncoming traffic and they cut in front of me. There have been far too many near misses... It must be terrifying for the oncoming cars too.	I am very impressed with the recently installed speed cushions further down Playford Road between Humber Doucy Lane and Bent Lane. A big thank you to whoever initiated/funded/implemented these. There is just enough space between the cushion and side of the road for cyclists to pass and the cushions are successful in slowing traffic speeds. Also, the new mini-roundabout by Bent Lane /The Street /Playford Rd seems to have helped slow traffic speeds too. Can speed cushions be installed all the way along Playford Road and Martlesham Rd please? It is a key cycling route, but too terrifying for many people to use. And lower speed limits would hopefully benefit pedestrians too?
Sue Kershaw	613	Lowestoft Promenade	I read there are several items on the agenda for safety, need and encouragement for even more cycle lanes to be improved, eg new lines to be re painted along the promenade. Surely this is such an easy task, low cost and needs no consolidation, as the cycle lane is already in use?	So, I ask this is to be given priority, after all there is no money issue, as I also researched the funding that central Government had given to you, I believe the sum of three million, this was to spend to fast track for cycle corridors, in the wake of the Covid 19.
Sue Kershaw	614	Pakefield High School (opposite)	My last request, for the spending of the money given to Lowestoft, for the high demand and in identifying the NEED for a new cycle lane opposite Pakefield High School, NR337AQ. I travel on London Road frequently, either on my bike, walking or by my car. Last Thursday afternoon, when the student	I can see from your plans that Arbour Lane, MAY be improved? Look at taking this new cycle lane from Mc Donald's roundabout to Pakefield road and connects to the existing track along the promenade. There are over three hundred students at this school, the

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			were finishing school, I witnessed a child stumble into the road, he was very lucky not to be injured.	new safety improvements need to happen promptly. The safety of everyone in that area should not purely be down to luck.
Suffolk County Council	680	East Suffolk	See attached.	See attached.
Suffolk County Council	798	East Suffolk	The County Council supports the underlying sustainable aims and objectives of the emerging Strategy and would suggest that engagement is made with neighbouring authorities in Norfolk (i.e. Great Yarmouth Borough Council and Norfolk County Council) to ensure that the maximum benefits can be made through cross-boundary working in respect of cycling and walking routes to Norfolk Settlements.	
Susan Harvey	572	Kirton Village Green to Reeve Lodge Trimley St Martin	Trimley St Martin Primary School is being moved from its present position on Kirton Rd Trimley to a piece of land by Reeve Lodge SCLP 12.65. This school is attended by children from Kirton many of whom do not have cars. There needs to be a safe segregated cycle path from Kirton to the new site.	The land opposite Kirton Village Green is owned by Trinity College as is the land where the new school is to be built. If land could be acquired from Kirton Green crossing Croft Lane and beyond it would be possible to put in a new segregated cycle path virtually up to the existing footbridge over the A14. There is a wide footpath past Roselea Nursery which could easily be increased in width. The path would then link into Old Kirton Road. There would have to be some kind of crossing to get children to the new school over Many adults cycle over the footbridge as a means to get to Felixstowe. This could be a very valuable route to decrease road traffic and meet East Suffolk's climate change Greener agenda. It also connects to other major cycle routes in the area.
Susan Harvey	573	SCLP12.65 New Primary School	A new safe cycleway (preferably segregated) will be required to get Kirton and Falkenham Children to and from the new Primary School at	The footpath over the A14 is the obvious route. There is adequate land from Kirton Green on the western side of Trimley Road(in the same ownership as land that

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			SCLP12.65 . Many children from Kirton go to the existing Trimley St Martin Primary school and as it is being moved provision needs to be made for a safe access cycle path from the new site to Kirton and Falkenham.	the school is being built on) to accommodate a segregated path through to Roselea Nursery and thence to the footbridge. A new safe route would then be needed to access the school. This could form the basis of an interconnected route which would benefit East Suffolk's Climate change and Greener Future Agenda
Susan Steward	637	going from High Street north on A47 (towards Corton)	I have no idea what I am supposed to do at the top of the High Street on a bicycle. There is a cycle lane coming south but I do not want to use it going into on-coming traffic. There is confusion about what pavement cycling as sometimes marked and then disappears. I don't want to cycle on the A47 as it is too fast but there is no alternative but more importantly NO SIGNAGE at all. The DENES HIGH SCHOOL is on the A47 and currently no cycle path from south to allow pupils to cycle safely.	Proper cycle ways that are NOT on the road and NOT on the pavement. Cyclists need to be protected from traffic on A roads. A cycle way along the whole of the A47
Susan Steward	638	Kessingland + A12 going south	There is no cycle route at all. There is no way for cycles to travel safely along the A12. How do we even get to Benacre from Lowestoft? Why no cycle way along the A12?	Cycle way along the A12. At present no way of getting to Lowestoft until Kessingland is reached (and then it's not very good)
Susan Steward	639	Whole of Lowestoft	Cycle routes are good in the town of Lowestoft (compared to other UK cities/towns but NOT when compared with most of Europe). However there is NO WAY of getting OUT of LOWESTOFT to other places: Yarmouth unsafe, Southwold unsafe/non-existent; Beccles not great; Hadiscoe very unsafe and non-existent. How do people work in other places and commute by bike (or even public transport)?	You need to work with Highways and Norfolk. There is just no investment to go from place to another. Why not work with Sustrans? (who have pulled out of Suffolk because not enough funding). More strategic thinking about cycling as a mode of transport NOT just a Sunday jolly.

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Susan Stone	323	Easton, Suffolk	<p>The roads out of Easton to surrounding villages do not have pavements and the increasing through traffic in Easton, particularly at rush hour and during school run means it is increasingly unsafe to walk/cycle.</p> <p>Neither Wickham Market or Framlingham is far from Easton and would be easily walkable if it weren't for the danger of the roads. While it is not possible to provide pavements, I suggest that permissive paths on the edge of farmland could be instigated which link public rights of way.</p>	<p>Pursue a series of permissive paths on the edge of farmland that link the village with Wickham Market and Framlingham and public rights of way so providing a safe walking network in and around the village separated from roads. Such paths would not need to be wide - possibly only 1-2m wide and once created could be maintained by footfall.</p> <p>This idea is not applicable just to Easton it could be rolled out across many rural villages to encourage walking.</p>
Sylvia Atkinson	520	The Street, Kettleburgh	<p>It is a fairly well used road by all manner of vehicles. It is also a well used cycle route but alas not ideal for walkers as there is no path and no street lighting.</p> <p>I was saddened three weeks ago, whilst I was walking down the road in the early evening when I lost my footing and fell to the ground, sprained my ankle very badly and hurt my left knee and arm.</p> <p>I noted exactly where this happened and have attached photographs of the damaged road there and further unacceptable and unsafe areas.</p>	<p>Please try to address this road safety situation as a matter of urgency as I believe it is only a matter of time before a more serious incident could occur to cyclist and walkers alike.</p> <p>I know the government is encouraging more activity in these areas so safety has got to be the priority.</p>
Sylvia Robbins	428	Ellough road from college lane to the Industrial park	<p>No public footpath/ cycle path to link College lane to the Industrial Park. Many pedestrians use the grass verge to walk to work and this is clearly hazardous. There is a partial cycle path linking college lane to the A146roundabout but this needs to extend to the industrial park and also back towards Beccles as far as Cedar Drive</p>	<p>Convert the grass verge to a foot/ cycle path</p>

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Tamsin Anderson (originally submitted by email)	523	Ufford	Ufford residents are currently rather trapped in the village by busy roads and unable to safely leave the village for trips to school or the shops. I'm part Danish and long to be able to use my bike instead of the car for school, shopping, etc, as my family do in Denmark. However with small children there's no way I'd venture onto the roads to Woodbirdge or Wickham Market. I hate how much I have to use the car.	I would love a cycle friendly route between Wickham Market and Woodbridge. It would enable so many children to get to school safely.
Terry & Elizabeth Smith	275	Pavement through Bredfield	Much of the "pavement" is now too broken or overgrown for safe walking, particularly for anyone with a buggy, a wheeled walker. or a wheelchair People are forced to walk in the road.	The "pavement" needs to be resurfaced and parts of it need to be remade.
The British Horse Society	790	See attached.	See attached.	See attached.
Tim Butler	77	Melton Rd Woodbridge.	Road surface is very bumpy/rutted for the length from Pythches Rd junction to near Dock Lane junction, causing cyclists to ride erratically. This is a main through route for cyclists to the Suffolk Coastal region from Ipswich and surrounding areas.	Resurfacing
Todd Strehlow	417	Castle Street btw Double Street and Fore Street	Castle Street is one-way eastbound which reduces access to the town centre and church from estates on the east side of the town	Suggested contraflow cycle lane. There wouldn't be any loss of parking as the only parking currently is the widest section - there are two exit / queuing lanes and you only need one. West of Double Street may well be too narrow but not a problem as cycles can turn down Double Street which is 2-way
Todd Strehlow	477	B1078 / B1116 junction Lower Hacheston	Very limited pedestrian and no cycling facilities at the B1078 / B1116 junction. Bus stops for	Pedestrian island on SW arm of junction Secure cycle parking at bus stop (next to the shelter) Lay-by where cars can safely pull over and wait, if

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
			the main no. 64 bus and also 963 school bus to Thomas Mills HS	collecting / dropping people Markings and dropped kerbs to facilitate segregated path between the bus stop and Station Road (the lane just next to the southbound slip road)
Todd Strehlow	643	Rendlesham / Ivy Lodge Road	Distance and highway conditions from Rendlesham to Wickham Market station. Currently cycling between the two involves navigating the roundabout at the N end of the village and a 60mph stretch of the B1069, then the full length of Ivy Lodge Road.	Providing pedestrian and cycle access on the estate road within Rendlesham Park / old estate, which would remove the most dangerous part of the journey and also reduce the distance by 25%. Most of the route exists, though may need a new access point from Ivy Lodge Road.
Todd Strehlow	650	lack of cycling facilities Framlingham - Parham - Hacheston - Wickham Market station	The B1116 is a very busy road, and parts have a national (60mph) speed limit. Some has 30/40mph but from Brick Lane to The Street in Parham there is no alternative. There is a back-lane route from Hacheston to Campsea via Marlesford but there is no safe crossing of the A12. A significant number of cyclists do use the A1116 but only fit and fast ones.	Re-create the Framingham branch railway line for walking and cycling. For much of the way from Framingham to Marlesford there are public footpaths paralleling the old railway alignment, or very near by. These could be diverted, through negotiation, and joined up to follow the track bed, and be reclassified as bridleway or cycle track. In the longer term the track bed could be acquired and the surface upgraded. As an extension - though more complex - path could be extended along the old freight railway line to Snape Maltings. There are very few truly traffic-free cycling facilities in this part of Suffolk (that are not muddy). This could develop into a fantastic and very well-used facility for leisure and other purposes.
Todd Strehlow	677	River Waveney, Beccles	Lack of cycle / walking access from Beccles towards Burgh St Peter, Aldby peninsula	acquire and restore the former railway bridge over the Waveney, that used to carry trains towards Haddiscoe. Work jointly with South Norfolk Council to create a walk/cycle way, and also protect the corridor for possible future rail service
Todd Strehlow	678	B1078 between Campsea Ashe and Five Ways / Lower Hacheston	very dangerous conditions for pedestrians and cyclists trying to access the key bus stops at Five Ways from Campsea Ashe	Pavement / footway-cycleway; some can be done as pavement adjacent to kerb (e.g., in front of houses and Lower Hacheston) some as segregated track parallel to the road, behind hedgerows

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
Tony bibby	481	The high street and the A143 junction	We live between bungay and spexhall, we have no pathways at all, it would be fantastic to have a walkway or cycle path put in between bungay where we do our shopping and spexhall where our local public house is situated that we use for social events, I cycle but feel very unsafe riding on the main road as it is very dangerous, my partner has a mobility scooter that she could never use between these two points on the map, so we have to always use the car but would much rather use our cycle and scooter	Pathway or cycle lane from bungay to spexhall along the A143
Tony Cooper (originally submitted by email)	444	B1122 Abbey Road / Lovers Lane junction to Valley Road. All offroad.	Safe route, avoiding Abbey Road, bringing workers into Town. Legacy route for residents and tourists accessing Aldhurst and route onto Suffolk Coast Path. 3. Links to route 2 and on to all other proposed routes.	
Tony Cooper (originally submitted by email)	445	LOVERS LANE - VALLEY ROAD - ALLOTMENTS - EXITING AT SIZEWELL ROAD/KING GEORGES AVENUE.	Lovers Lane via EDF route. Close part of Valley Road to sewage works. Then on road via Valley Road to allotments. Then across allotments on FW and across private land to King George Avenue. Route 2b from allotments to High Street Closure of Valley Road will facilitate safe route from camp site for construction workers. Legacy route for residents and tourists accessing Aldhurst and route onto Suffolk Coast Path to Aldeburgh via new tourist cycle route along the old railway line.	
Tony Cooper (originally submitted by email)	446	LOVERS LANE - SIZEWELL ROAD - KING GEORGES AVE - EXITING AT GRIMSEY ROAD	Sizewell Gap / Lovers Lane Junction Via King Georges Avenue to Sizewell Road / Grimsey Road junction. Off road cycleway on south side of King George Avenue as far as eastern	

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
			entrance to Sports Field/Recreation ground. Then private tracks / footways behind houses. Links back to King George Avenue with off road cycleway on Sylvester Road.	
Tony Cooper (originally submitted by email)	447	CROWN FARM JUNCTION - NEW TOURIST ROUTE - GRIMSEY LANE EXITING AT LEISURE CENTRE	Sizewell Gap via track south to join Grimsey Lane. West via Grimsey Lane to the Leisure Centre. Off road (tracks) but on road from Leisure Centre along Red House Lane to Poppy Way. More direct cycle access for workers to the Leisure Centre. Legacy route for residents and tourists accessing new tourist route to Aldeburgh and route to Sizewell.	
Tony Cooper (originally submitted by email)	448	GRIMSEY ROAD (Sylvester Road?)- THROUGH TOWN CENTRE - CROSS STREET - VICTORY ROAD - WATERLOO AVENUE	King George Avenue / Sylvester Road junction via Sizewell Road, Cross Street and Victory Road (all on street) then via public footway to Waterloo Avenue (off road) Main route through town linking east with west, avoiding busy/unsafe routes; Haylings Road, Park Hill and White Horse junction.	
Tony Cooper (originally submitted by email)	449	GOLDINGS LANE - ALDEBURGH ROAD - THROUGH TOWN CENTRE TO WHITE HORSE (WATERLOO AVENUE/STATION ROAD JUNCTION)	B1069 Haylings Road via Goldings Lane (part on, part off road) to B1122 Aldeburgh Road the north on Aldeburgh Road, High Street and then west to Waterloo Avenue / Station Road junction. On road with short diversion onto service road. Main route through the town from south to north. Route from Knodishall into Town or to Leisure Centre via 6b or Sizewell via route 4.	
Tony Cooper (originally submitted by email)	450	ALDEBURGH ROAD - HOPKINS ESTATE - LEISURE CENTRE AND ALDE VALLEY ACADEMY	B1122 Aldeburgh Road via track to Daisy Drive, then on road via Foxglove End and Prevett Way to Red House Lane.	

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
			<p>Safer route avoiding traffic in Red House Lane. Links to route 8</p>	
Tony Cooper (originally submitted by email)	451	ALDEBURGH ROAD - SEAWARD AVENUE - SYLVESTER ROAD	<p>Off road cycleway from Aldeburgh Road along Seaward Avenue to Sylvester Road. Then on road (contra flow) on Sylvester Road north to join route 3 south of Sizewell Road. Extension 7b on Seaward Avenue to Alde Valley Academy and route 8.</p> <p>Safe route to travel to Alde Valley Academy (Secondary School) and Avocet Academy (Primary School) avoiding Town centre.</p> <p>Important link for route from south to north of town for workers and residents/tourists.</p>	
Tony Cooper (originally submitted by email)	452	LEISURE CENTRE- ALDE VALLEY ACADEMY - AVOCET ACADEMY	<p>Route 3 south of King Georges Avenue across recreation ground and then via public footways to Red House Lane/ Linking to route 4</p> <p>Safe link between all three sites and access to all routes.</p>	
Tony Cooper (originally submitted by email)	453	WATERLOO AVENUE (CHURCH ROAD) - PATH BEHIND MASTERLORD ESTATE - BUCKTON PLACE	<p>Waterloo Avenue north on public footpath and then west across recreation ground to Harling Way.</p> <p>Safe route from west boundary into Town avoiding Waterloo Avenue and White Horse junction.</p>	
Tony Cooper (originally submitted by email)	454	WESTWARD HO (PEDESTRIAN RAILWAY CROSSING) - BUCKLESWOOD ROAD - ABBEY LANE	<p>Route 9 where it turns west to recreation ground along public footpath to Buckleswood Road then on road west along Buckleswood Road to Harrow Lane</p> <p>Links route 9 to route 1 from west of Town. Avoids single track, rat run route of Abbey Lane.</p>	

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
Tony Cooper (originally submitted by email)	455	PATH AROUND VICTORY ROAD RECREATION GROUND TO BE UPGRADED	PATH AROUND VICTORY ROAD RECREATION GROUND TO BE UPGRADED	
Tony Cooper (originally submitted by email)	456	Abbey Lane	From B1122 Abbey Hill to Aldhust Farm Off road cycleway to avoid narrow section of Abbey Lane	
Tony Neill	386	Cycle path outside Claremont Pier	Cyclists are asked to dismount for the short section passing the pier. I can see this may have been done for the safety of pedestrians, but think a warning to go slow and also for pedestrians to be aware of cyclist would be better.	
Tony Neill	387	Bridge	Cyclists and pedestrians share the path on both sides of the bridge. It's not very clear to pedestrians as they often give me abuse!	Better signs or separate lanes for bikes & pedestrians
Tony Neill	388	Peto Way heading towards Wickes	No cycle path on left of road so have to ride on the road. The cycle path on the other side is difficult to get to as you have to cross 2 lanes.	Cycle lane, or make it easier to get to lane on other side
Tracy Rogers	176	access to woodbridge from Melton for cyclists.	The towpath between Melton and Woodbridge is pedestrians only. The road between Melton and Woodbridge is getting increasingly busy with many more parked cars, hazards for cyclists. A cycle path next to the pedestrian footpath along the river, or one wide enough for both would make access to Woodbridge practical for cyclists, decreasing parking needs and increase shoppers. A proper cycle path on the road between Melton primary and the thoroughfare would be an improvement, if not ideal.	described above
Tricia Higgins	95	In and around Martlesham/Martlesha	Few, if any, footpaths are accessible for wheelchair users, which means that I cannot	Make more footpaths accessible for wheelchair users (and parents with prams/buggies) especially in local

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
		m Heath and Woodbridge	accompany my friends and family when they go for walks. Shared footpaths with cyclists are a problem because often I can't hear cyclists coming from behind me, and they ride too close.	beauty spots Separate pedestrians from cyclists, or provide a barrier so that cyclists can't ride so close.
Trimley St Mary Parish Council	672	Extend the temporary 'mandatory' cycle lane through Walton and then through Trimley St Mary / Trimley St Martin	There are a couple of areas on this road that are pinch points and of particular danger to cyclists, not least outside the school entrance on the High road and near McColls shop. If there are no plans to re-paint or enhance the cycle lane provision in this area, are there any other plans to address road safety issues in these areas?	The Parish council are also keen to find out if there would be any funding available to introduce a mini roundabout at the High road / Station road junction. This would reduce speeding in the immediate area as well as improve the road junction.
Walberswick Parish Council	800	Walberswick	The Council strongly supports developing a cycling and walking strategy. We support putting in additional cycling and walking routes and increasing the level of maintenance that ESC and SCC spend on maintaining routes. Walberswick Parish Council has already objected to Sizewell C including that its construction period will make it impossible to cycle on the roads in and around the area as huge increases in traffic, HGVs and rat running will make roads busy and dangerous for cyclists and walkers.	Should Sizewell C go ahead, ESC should address this particular issue in the Cycling and Walking Strategy along with the ongoing work in the rest of the District.
Waldringfield Parish council	645	Footpath from Martlesham to Waldringfield along River Deben	The breach prevents walking between Woodbridge and Waldringfield without going on roads	Waldringfield Parish Council agrees that the footpath should be re-instated but disagrees that this should be done by mending the breach. We support the new inland footpath proposed by Natural England, and shown below:
Waldringfield Parish council	647	River Wall north of Waldringfield (Footpath 11)	There is a serious problem on the river wall footpath north of Waldringfield (FP11), where cycling damages the structure of the river wall	Barriers would be effective but are problematic because they make access for mobility vehicles difficult. Better signage might help.

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
			and could eventually result in a breach. No cycling signs are regularly ignored by cyclists.	
Waldringfield Parish Council	648	Waldringfield (Ref 409)	Waldringfield Parish council agrees with this, except that we do not support compelling pubs such as the Maybush to provide toilets – encouragement is far better. The absence of public toilets leaves walkers with little choice if they are ‘caught out’, resulting in health hazards as well as being offensive and off-putting.	A public toilet in the Maybush car park. There should also be far more litter bins at the start and end of public footpaths.
Waldringfield Parish council	649	Bridleway from Waldringfield to the Waldringfield Heath crossroads (FPs 24 & 29)	When the Brightwell Lakes development is completed there will be far more people using this route to/from Waldringfield. When the new school is operational, pupils are likely to cycle from Waldringfield to it every day. The bridleway seems to end at the crossroads, where there is no choice but to use the road. In fact FP35 is a bridleway, but isn’t signposted as such, and is very narrow, and where it crosses the Ipswich Rd (turning into FPs 27, 8 & 34) is dangerous and also poorly signposted.	Widen FP35, improve the signposting, and provide proper road crossing facilities for cyclists so that the route FPs 24-29-35-27-34/8 can be cycled with minimal interaction with road traffic. (There is currently no signpost at the crossroads end of FP29)
Waldringfield Parish Council	646	Footpaths in and around Waldringfield, and elsewhere throughout East Suffolk (Ref186)	Waldringfield Parish Council agrees with this. WPC has put up No Cycling signs on several footpath following complaints by residents, and most of these have been destroyed, presumably by cyclists.	
Waldringfield Parish Council (originally from email)	698	Bridleway from Waldringfield to the Waldringfield Heath crossroads (FPs 24 & 29)	When the Brightwell Lakes development is completed there will be more people using this route to/from Waldringfield. When the new school is operational, pupils are likely to cycle from Waldringfield to the new school. The bridleway seems to end at the crossroads, where there is no choice but to use the road. In	Widen FP35, improve the signposting, and provide proper road crossing facilities for cyclists so that the route FPs 24-29-35-27-34/8 can be cycled with minimal interaction with road traffic. (There is currently no signpost at the crossroads end of FP29)

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
			fact FP35 is a bridleway, but isn't signposted as such, and is very narrow, and where it crosses the Ipswich Rd (turning into FPs 27, 8 & 34) is dangerous and also poorly signposted.	
Waldringfield Parish Council (originally submitted by email)	692	footpath from Martlesham to Waldringfield along River Deben	<p>For context we have included the comments taken from the map, WPC's responses are labelled as 'our response'.</p> <p>For many years it has simply been accepted that part of the path was washed away by natural erosion, so the only way to walk to Waldringfield from Martlesham is along the road. This is shown by signposts at the access points to this section of footpath.</p>	Re-instating this footpath (by mending the breach at TM279461 or providing a diversion following the high-water mark) would provide a continuous off-road footpath route along the entire west bank of the Deben estuary, with several suitable entry/exit points. We agree that the footpath should be re-instated but disagree that this should be done by mending the breach. We support the new inland footpath proposed by Natural England, and see attached.
Waldringfield Parish Council (originally submitted by email)	694	Footpaths in and around Waldringfield, and elsewhere throughout East Suffolk	<p>With the rising popularity of cycling we seem to have lost respect for the differences between footpaths and bridleways. Cyclists seem to no longer acknowledge that footpaths are not for cycling along, making it potentially dangerous for walkers and causing damage to footpaths. In the same way that cyclists wish to see improvements to the road infrastructure to feel safe from vehicles we need to acknowledge that there are similar issues on footpaths...which are NOT rights of way for cyclists.</p>	<p>For context we have included the comments taken from the map, WPC's responses are labelled as 'our response'.</p> <p>A campaign of education about the differences between footpaths and bridleways coupled with improved signage and potentially sanctions for non compliance</p> <p>Our response: We agree with this. WPC has put up No Cycling signs on several footpath following complaints by residents, and most of these have been destroyed, presumably by cyclists.</p> <p>We also have a more serious problem on the river wall footpath north of Waldringfield (FP11), where cycling damages the structure of the river wall and could eventually result in a breach.</p>
Waldringfield Parish Council (originally	695	Waldringfield	For context we have included the comments taken from the map, WPC's responses are labelled as 'our response'.	For context we have included the comments taken from the map, WPC's responses are labelled as 'our response'. Our response: We agree with this, except that we do not

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
submitted by email)			No WC accessible to the public walking or cycling in the area. It would get more people out walking and/or cycling if they could feel sure that they would be able to find WCs en route. Waldringfield is a classic example of a place in a prime location for walkers, but no toilets. This applies to most villages these days so Waldringfield is just one example.	support compelling pubs such as the Maybush to provide toilets – encouragement is far better. The absence of public toilets leaves walkers with little choice if they are ‘caught out’, resulting in health hazards as well as being offensive and off-putting. There should also be far more litter bins at the start and end of public footpaths.
Wendy Brooks	256	From Triangle market to top of High st. From Yarmouth Road to Yarmouth	There is no route that continues from the High St to the villages of Blundeston, Lound and Hopton. Lanes are faded and poorly maintained.	Enforce parking rules in the High St, repair the cycle path between Sussex Rd and Harris Avenue. Create a shared path through to Blundeston Roundabout, there are few pedestrians except when the schools comes out .and this is made worse by parents parking on the cycle path to collect their children. Create a purpose built cycle track either side of the Yarmouth Rd through to Yarmouth, Introduce a signal that allows cyclists to leave a traffic light before cars.
Westerfield Parish Council	765	Church Lane and Lower Road	An East/West route, Church lane (unclassified) and Lower Road (C Class), is used by many vehicles as an alternative to busy roads across the North of Ipswich. This route in many places is only 5 metres wide and has no footpaths and no walkable verges while the peak hour flow of traffic has been measured at over 500 vehicles per hour.	
Westerfield Parish Council (originally submitted by email)	766	Westerfield footpaths	The Parish Council have sought to apply for definitive status for a number of footpaths that were known to be used by residents but in all cases access to these routes for a circular walk includes use walking along dangerous local roads.	
Westerfield Parish Council	767	Westerfield	The only recognition of cycling in the village is that a section of the East/West route from	

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
(originally submitted by email)			Lower Road and Church Lane and then Moss Lane to Tuddenham is part of a Long-Distance Cycle Route.	
Westerfield Parish Council (originally submitted by email)	768	Section of track leaving the B1077 going west between Mill Farm and High Acre	Section of track leaving the B1077 going west between Mill Farm and High Acre, not on the definitive map but currently used as a footpath to be adopted as a public right of way to link with Footpath 18 (Fonnereau Way) as part of the Ipswich Garden Suburb and hence enable access to the proposed footbridge over the Railway line and the footpath towards Ipswich.	
Westerfield Parish Council (originally submitted by email)	769	Section of track leaving the B1077 going east and then passing under the two railway bridges	Section of track leaving the B1077 going east and then passing under the two railway bridges to be adopted as a public right of way to join with the network of routes passing Red House Farm within the Ipswich Garden suburb and giving access to Tuddenham Road. This would enable residents of Westerfield to gain access to Northgate High School and Northgate Sports Centre without having to use heavily trafficked roads.	
Westerfield Parish Council (originally submitted by email)	770	Lower Road, Westerfield	This road is unsuitable for cyclists and pedestrians due to the amount and the speed of traffic. This narrow road does not have footways or walkable verges and where the minimum width is 5 metres a drainage ditch is immediately adjacent only protected by reflective marker posts.	Although Speed indicators are present it is obvious that physical measures are needed to improve reduce traffic speeds and enable cyclists and pedestrians to use this road in safety. Consideration should be given to traffic management measures such as restricting vehicles to single lane working alongside pedestrian/cycling facilities and/or any other provision to decrease the number and speed of vehicles.
Westerfield Parish Council (originally submitted by email)	771	Church Lane, Westerfield	This road is unsuitable for cyclists and pedestrians due to the amount and the speed of traffic. This narrow road does not have footways or walkable verges and limited visibility is an additional hazard.	Although Speed indicators are present it is obvious that physical measures are needed to improve reduce traffic speeds and enable cyclists and pedestrians to use this road in safety. Consideration should be given to traffic management measures such as restricting vehicles to

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
				single lane working alongside pedestrian/cycling facilities and/or any other provision to decrease the number and speed of vehicles.
Westerfield Parish Council (originally submitted by email)	772	Moss Lane	This road is single vehicle width and used by a large range of vehicles as a short cut. It is unsuitable as a rat run and should be closed to through traffic thus protecting cyclist and pedestrians. The SCC ROW Improvement Plan referred to possible classification as a Green Lane (Similar Comment to that already registered No478)	
Westerfield Parish Council (originally submitted by email)	773	B1077 near Westerfield Railway Station	Parked vehicles near the level crossing are a hazard to cyclists and pedestrians.	A solution is to work with Greater Anglia (re current usage) and Ipswich Borough Council as part of the Ipswich Garden suburb to provide facilities for off road parking.
Westerfield Parish Council (originally submitted by email)	774	Westerfield Railway Station and Greater Anglia	In order to make better use of rail services and reduce dependence of local residents on car travel there's a need for East Suffolk Line services to stop at Westerfield. In the past it has been possible to use this service to or from Woodbridge as part of a cycle ride or a ramble, in fact it's listed as an East Suffolk Line walk. Stopping trains on the East Suffolk line would therefore help to encourage walking and cycling while also eliminating car journeys and contributing to "Green" policies.	
Westerfield Parish Council (originally submitted by email)	775	Sandy Lane and Route of Bridleway (Westerfield ROW No 1) from Lower Road, Westerfield to Henley	It is suggested that this route could be upgraded to be suitable for all classes of cyclist. This would enable social/recreational links between the two villages to be enjoyed while not having to mix with fast moving traffic on roads with no footpaths or verges.	

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
Westerfield Parish Council (originally submitted by email)	764	Main Road B1077	The Main Road B1077 connects Ipswich with Debenham and villages to the North of the County and for most of its length in there is frontage development and a 30mph Speed limit. A suitable width footway exists between the Railway Level Crossing and The Swan PH but northwards this footway is of inadequate width.	
Will Windell	512	Southwold High Street	Due to the large number of pedestrians using the narrow pavements, and the large number of cars going through the High Street, pedestrians are forced to walk in the roads, creating a safety hazard. Covid has accentuated an existing problem.	A large sign/banner. Go slow, make way for pedestrians in the road. Or some such language. Widen pavements Or other traffic calming measures
Yoxford Parish Council	679	N/A	See attached.	See attached.
Yvonne Smart	757	Convoy riding on highway	Although riding in large groups is no doubt a pleasant experience, riding in convey without occasionally pulling in to allow build up of traffic to pass does put riders at risk of car drivers taking chances to pass. I have on more than one occasion been stuck behind such a convey from Martlesham through to Woodbridge with little opportunity to pass. One has to be patient but as said, some car drivers may try and overtake inappropriately risking themselves and cyclist to injury.	
Yvonne Smart	758	Bent Hill, Felixstowe	Cyclist riding at speed down the middle of Bent Hill thus risking themselves, walkers and car drivers to injury. An accident waiting to happen (but should it wait?) Incidentally the same goes for skateboarders.	
Yvonne Smart	759	Hamilton Road shared space	Cycling one way, same as traffic, would help with safety of walkers especially the deaf and	

Respondent name	Comment ID	Where is the matter / improvement located?	What is the matter / improvement?	Please suggest a possible solution / improvement
			poor sighted. Cyclists/skateboarders play in this area.	
Yvonne Smart	760	Cycle lanes along highway	A white line separating cyclist from vehicles is not a safe option. Cars parked in cycle lanes requires cyclists to move around cars in the hope no one opens a car door as rider passes. I appreciate the solution is not an easy one but one has to be found if we are to encourage more cyclists to use network of roads. I personally have ceased cycling into Felixstowe from Trimley.	
Yvonne Smart	761	See attached documents - Stratton Hall	See attached documents	
Yvonne Smart	762	See attached documents	See attached documents	
Yvonne Smart	763	Cycling - general comments	See attached document. Points 2 to 5 are plotted on the map in the relevant area which relates to the matter.	

Appendix 2: Initial and Draft Consultation Bodies

The following organisations and groups were consulted during the preparation of the Strategy

- Elected members
- Developers / landowners / agents
- Suffolk County Council
- Broads Authority
- Historic England
- Natural England
- Environment Agency
- Members of the public

Specific consultation bodies

The Coal Authority

- Environment Agency
- English Heritage
- Marine Management Organisation
- Natural England
- Network Rail
- Highways Agency
- Suffolk County Council
- Parish and Town Councils within and adjoining East Suffolk
- Suffolk Constabulary
- Adjoining local planning authorities – Ipswich Borough Council, Mid Suffolk District Council, Babergh District Council, Great Yarmouth Borough Council, Broads Authority, South Norfolk District Council
- Anglian Water
- Essex and Suffolk Water
- Homes England
- Electronic communication companies who own or control apparatus in the Suffolk Coastal District
- Relevant gas and electricity companies
- NHS England
- Ipswich and East Suffolk Clinical Commissioning Group
- Great Yarmouth and Waveney Clinical Commissioning Group

General consultation bodies

- Voluntary bodies some or all of whose activities benefit any part of the District
- Bodies which represent the interests of different racial, ethnic or national groups in the District
- Bodies which represent the interests of different religious groups in the District
- Bodies which represent the interests of disabled persons in the District
- Bodies which represent the interests of persons carrying on business in the District
- Bodies which represent the interests of environmental groups in the District

Other individuals and organisations

Includes local businesses, individuals, local organisations and groups, planning agents, developers, landowners, residents and others on the Local Plan mailing list.

Appendix 3: Initial Consultation Promotion Material

Twitter – 19th October 2020

You Retweeted

 **East Suffolk Council**
@EastSuffolk

We want to make it safer & easier for people to cycle & walk to school, work or just for fun.

Have your say on what could be improved for cyclists & pedestrians in East Suffolk - we'd also like your ideas on how to encourage more people to cycle & walk:
eastsuffolk.gov.uk/news/comments-...



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Twitter – 16th November 2020

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 **East Suffolk Council Planning**
@eastsuffolkplan

The consultation for the East Suffolk Cycling and Walking Strategy is currently underway. Only 3 weeks remain to submit comments before the consultation ends on Monday 7th December 2020.
eastsuffolk.maps.arcgis.com/apps/webappvie...



HOW CAN WE IMPROVE CYCLING & WALKING?

Consultation period
19 October to 07 December 2020

What are we doing?

East Suffolk Council is at the early stages of preparing a Cycling and Walking Strategy to help make cycling and walking in East Suffolk, whether for work, school or leisure, more convenient and more desirable.

With your input, we can improve the cycling and walking experience in East Suffolk.

How can you get involved?

- HIGHLIGHT ISSUES**
Help identify cycling and walking infrastructure matters in East Suffolk which you think could be improved
- SUGGEST SOLUTIONS**
Let us know solutions that could help encourage more people to cycle and walk
- INTERACTIVE MAP**
An online map shows opportunities and potential solutions raised by other users

Find out more and give your views:
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Alternatively, please send ideas to:
East Suffolk Council, Planning Policy & Delivery Team, Riverside, 4 Canning Road, Lowestoft, Suffolk NR33 0EQ

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Consultation Poster



Consultation period
19 October to 07 December 2020

HOW CAN WE IMPROVE CYCLING & WALKING?

What are we doing?

East Suffolk Council is at the early stages of preparing a Cycling and Walking Strategy to help make cycling and walking in East Suffolk, whether for work, school or leisure, more convenient and more desirable.

With your input, we can improve the cycling and walking experience in East Suffolk.

Find out more and give your views:
[www.eastsuffolk.gov.uk/
planningpolicy](http://www.eastsuffolk.gov.uk/planningpolicy)

Alternatively, please send ideas to:
East Suffolk Council, Planning Policy &
Delivery Team, Riverside, 4 Canning Road,
Lowestoft, Suffolk NR33 0EQ

How can you get involved?



HIGHLIGHT ISSUES

Help identify cycling and walking infrastructure matters in East Suffolk which you think could be improved



SUGGEST SOLUTIONS

Let us know solutions that could help encourage more people to cycle and walk



INTERACTIVE MAP

An online map shows opportunities and potential solutions raised by other users

planningpolicy@eastsuffolk.gov.uk
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Appendix 4: Draft Consultation Promotion Material

Twitter – 1st November 2021

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Consultations begin today on three draft planning documents:

- the Affordable Housing Supplementary Planning Document,
- the Sustainable Construction Supplementary Planning Document,
- and the East Suffolk Cycling and Walking Strategy.

Have your say! [eastsuffolk.gov.uk/news/have-your...](https://eastsuffolk.gov.uk/news/have-your-...)



East Suffolk Council Planning

 1  12  11 

Twitter – 22nd November 2021

 **East Suffolk Council**  @EastSuffolk · Nov 22 ⋮

There's still time to have your say on the draft East Suffolk Cycling and Walking Strategy!

The consultation is open for responses until 5pm on 10 January 2022:
eastsuffolk.gov.uk/planning/plann...



East Suffolk Council Planning

  3  3 

Facebook – 24th November 2021



East Suffolk Council
2 mins · 🌐

Have your say! 🗨️

Consultations on three draft planning documents are still open. You can comment on:

- the Affordable Housing Supplementary Planning Document,
- the Sustainable Construction Supplementary Planning Document,
- and the East Suffolk Cycling and Walking Strategy.

View the documents and comment at
<https://www.eastsuffolk.gov.uk/.../planning-policy-consultat.../>



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Press Release – 1st November 2021

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Home > News > Have your say on draft planning documents

Have your say on draft planning documents

Posted by on 1 November 2021 | Comments

Tags: [planning](#), [cycling](#), [environment](#)

East Suffolk residents are invited to have their say on three new planning documents covering affordable housing, sustainable construction and cycling and walking.

Consultations on three draft documents have launched today (Monday 1 November):

- the draft Affordable Housing Supplementary Planning Document
- the draft Sustainable Construction Supplementary Planning Document
- and the draft East Suffolk Cycling and Walking Strategy.

The Affordable Housing Supplementary Planning Document (SPD) will provide guidance on the implementation of East Suffolk Council's affordable housing planning policies. The draft SPD covers a range of matters including types of affordable housing, identifying an appropriate mix of affordable housing, the design of affordable housing, legal agreements, carrying out local housing need assessments and making planning applications.

This consultation is now closed.

The Sustainable Construction Supplementary Planning Document (SPD) will provide guidance on the Council's planning policies on sustainable construction, including guidance on a range of topics relating to the construction of environmentally sustainable buildings. Examples include increasing water efficiency in buildings, increasing energy efficiency and reducing carbon emissions from buildings, use of responsibly sourced and renewable materials, reducing construction waste, orienting buildings for solar gain, and using green infrastructure to increase biodiversity.

This consultation is now closed.

The draft East Suffolk Cycling and Walking Strategy provides a district-wide shortlist of cycling and walking infrastructure opportunities. The strategy aims to encourage greater use of sustainable forms of transport, reduce contributions to climate change, support the growth of the tourism industry, and to improve the health and wellbeing of residents by ensuring more trips to work, school, leisure, day-to-day errands, or public transport hubs, such as train stations or park and ride sites, are accessible by bike or on foot. Once adopted, this strategy will replace the Waveney Cycle Strategy (2016) and will cover the whole of the district.

Cllr David Ritchie, Cabinet Member for Planning and Coastal Management said: "We want to increase the opportunities for people to walk and cycle safely around the district, reducing reliance on vehicles, improving the environment, and benefiting people's health. Our Cycling and Walking Strategy sets out ways we can do this, and we would welcome any feedback on this and on the Supplementary Planning Documents which will enable us to make decisions on future developments."

This consultation is now closed.



Consultation Poster



Have your say

Draft Sustainable Construction Supplementary Planning Document

Draft Affordable Housing Supplementary Planning Document

Consultation period
1st November to 5pm 13th December 2021

Draft Cycling and Walking Strategy

Consultation period
1st November 2021 to 5pm 10th January 2022

What are we doing?

East Suffolk Council is in the process of preparing a new Sustainable Construction Supplementary Planning Document and a new Affordable Housing Supplementary Planning Document which will provide guidance to support the Council's planning policies on these matters.

We are also preparing a Cycling and Walking Strategy, which identifies potential cycling and walking infrastructure opportunities across the district.

How can you get involved?

 **GIVE YOUR VIEWS**

Visit the website below to view and comment on the draft documents. Hard copies of the three documents are also available to view in libraries and the Council's Customer Service Centres. Please contact us if you need any assistance in viewing the documents.

Find out more and give your views:
[www.eastsuffolk.gov.uk/
planning-policy-consultations](http://www.eastsuffolk.gov.uk/planning-policy-consultations)

Alternatively, please send comments to:
East Suffolk Council, Planning Policy & Delivery Team, Riverside, 4 Canning Road, Lowestoft, Suffolk NR33 0EQ

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Appendix 5: Formal Consultation Responses

The two tables below list the consultation responses to the consultation on the draft Strategy. Please note that in the ‘Document Ref’ column of the first table any page and paragraph numbers relate to the draft Strategy (November 2021). Please note that in the second table, ‘Community Recommendation Ref’ column relates to the draft Strategy (November 2021).

Appendix 5 Formal Consultation Responses - Draft Strategy

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
Paragraph 1.1 - 1.2		2	Yes	Just because.	Support noted.
Paragraph 1.1 - 1.2	Andy Smith	785		<p>I believe this approach to any such long term global strategy for Felixstowe and its hinterland is fundamentally flawed..</p> <p>While I accept that the purpose of this document is not to address issues of lack of maintenance to existing footpaths and bridleways, they should remain the start point for a cost effective and fully segregated approach to C&W, certainly within existing developed and urbanised areas. The open countryside should have a different approach, suitable to either large</p> <p>A. proposed allocations for future development, or B. to improvement or new provision in open countryside.</p> <p>New infrastructure should surely be designed to link</p>	<p>Whilst the maintenance of existing cycling and walking infrastructure falls outside the scope of the Strategy, the importance of maintenance is not underestimated and a thorough understanding of the existing cycling and walking network has been of fundamental importance to the identification of recommendations for new and improved infrastructure. Urban and rural cycling and walking can differ in nature and so different infrastructure provision may be necessary depending on the particular context. Nevertheless, throughout the preparation of the Strategy recommendations in urban and rural areas seek to identify high quality cycling and walking infrastructure to meet the needs of all users. Throughout the preparation of the Strategy recommendations have been identified that seek to introduce infrastructure improvements that avoid conflict between users in such a way that also encourages cycling and walking trips, converting</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>to and make better use of existing, often fragmented paths. If these are ignored, there is a danger of</p> <p>A. Duplication B. Excessive costs, on potentially a large scale</p> <p>And , where major new shared paths are proposed</p> <p>C. Loss of existing green verges and the like, significantly detracting from the originally planned quality of place in development from the 1970s onwards.</p> <p>D. Increases in traffic congestion, hence in turn to increased queueing, degraded air quality and significant increase in journey times, and enomic cost.</p> <p>E. Very significant increase in conflict between pedestrians and cyclists The great majority of the population do not expect, and will not accept, “people cycling on the path”. And in fact there are many potential dangers arising irectly from that, in particular in relation to older and / or less agile people, parents with pushchairs or several small children, and others. The aim should be to avoid conflicts, not to create them</p> <p>Looking at the proposals as a whole, the great majority of the core traffic routs in the town will be significantly degraded as to the routine flow of traffic, before even taking into account increased delays at junctions.</p>	<p>potential vehicle trips into cycling and walking trips thereby reducing road congestion and supporting economic growth.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>Many of these will be core links from development on the north of the town through to sea front and town centre destinations. We would request that this aspect be significantly re-considered.</p> <p>The core purpose of the highway is to facilitate the social and commercial life of the community in the widest sense. The transport network (in its broadest sense) has evolved, and must continue to evolve, to provide safe, attractive and efficient movement for all network users, with a wide and improved choice of modes, ideally separated wherever possible I support the aim of achieving a safe, integrated network of routes which encourages and supports all those who are able to use cleaner / quieter / healthier forms of transport (for utility and leisure), while still allowing motor vehicle users to go about their business without unreasonable inconvenience.</p> <p>This aspect is especially significant or exaggerated in Felixstowe, by comparison with most market towns, in that it has the good fortune to have been a wholly planned town from its conception from around 1885 onwards. All of the key routes are thus designed, as a whole, for the purpose, with a much more efficient and convenient existing structure than in older settlements..</p> <p>I suggest most strongly that the above aspect should have a far greater weight in considering individual proposal than is the case in this draft, while</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>recognising that will not be the simple solution in every case.</p> <p>Where dedicated routes cannot be provided, and redesign of existing roads is essential to fulfil a clear need for an improved route for cyclists and /or pedestrians, these should seek to minimise disruption to overall traffic flows, avoiding causing congestion, as demonstrably the greatest cause of air pollution and inefficient use of fuels of all types, and consequent diversion of traffic to unsuitable minor roads or streets.</p> <p>In all cases, conflicts, or perception of conflicts, between alluser classes must be minimised.</p>	
Paragraph 1.1 - 1.2	Benhall and Sternfield Parish Council (Melanie Thurston)	660		<p>As there are significant numbers of horseriders in Benhall, we would ask that all cycle routes be available for horses (at walk only, and with clear priority for pedestrians, wheelchair users, child buggies etc). There are a number of such shared routes in the UK: whilst 3 m is recommended as the ideal width when horses are permitted, we have been in touch with the BHS (British Horse Society) who have many examples of shared usage on 2.5 m. The BHS have offered their support and advice in getting horseriders added to the permitted users of these routes. for further information the contact is (name and address supplied)</p>	<p>Where appropriate equestrian users were considered in the formation of any new or improved infrastructure. The British Horse Society were a consultee and have been directly spoken to.</p>
Paragraph 1.1 - 1.2	British Horse Society (Lynda Warth)	668		<p>It is unfortunate that the original consultation was only aimed at pedestrians and cyclists:</p> <p>Purpose of the Strategy</p>	<p>Whilst the primary purpose of the Strategy is to create safe, coherent, direct, comfortable and attractive cycling and walking environments, ensuring equestrian users are not disadvantaged is recognised. For this reason, the opening paragraph of the Strategy has been amended to</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>The East Suffolk Cycling and Walking Strategy identifies potential cycling and walking infrastructure opportunities across the district. The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure. It provides context and information to support detailed infrastructure</p>	<p>specifically reference equestrian users and the need to ensure they are accounted for in detailed infrastructure proposals. Throughout the preparation of the Strategy consideration has been given to the needs of equestrian users in ensuring that where improvements are recommended to bridleways, they meet the needs of all users.</p>
<p>Paragraph 1.1 - 1.2</p>	<p>British Horse Society (Lynda Warth)</p>	<p>671</p>		<p>The information above is to support the principle and some of the detail of the need to include equestrians in the LCWIP project and to protect the amenity of the bridleway network for equestrians since that is the only network available to them.</p> <p>I am conscious that I have not addressed the question of the actual routes included in the consultation. I cannot provide this information as I do not know the area well enough. This information needs to be provided by local horse riders however, due to the lack of BHS Access Officers in Suffolk, it may well take time to collate. Caroline and I would appreciate guidance on which information you require as a priority.</p> <p>However, the following boxes in the consultation do raise concern because they all refer to bridleways and potential ‘improvements’ or ‘resurfacing’ to the path surface, ostensibly for the benefit of cyclists. Care must be taken to ensure that any changes do not discriminate against the intended bridleway user – equestrians - and that shared use does not result in the displacement or reduction in amenity of existing users – this includes the allocation of path width for a hard top path to</p>	<p>Throughout the preparation of the Strategy consideration has been given to the needs of equestrians in ensuring that where improvements are recommended to bridleways, that they meet the needs of all users.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>enable cyclists to ride side by side whilst reducing the same social amenity for horse riders.</p> <p>The term ‘improvement’ has been challenged in Court.</p> <p>Section 62 (2) of the Highways Act 1980 gives Local Highway Authorities a power to carry out works to improve highways.</p> <p>The Court of Appeal in the case of Cowen –v- Secretary of State for the Department of Environment Transport and the Regions (1999) 3PLR108 concluded that if the construction of a hard surface changes the character of a way it goes beyond ‘improvement’.</p> <p>It will always be a matter of fact that tarmacing a bridleway will change its character. When deciding to tarmac a bridleway, the highway authority should take safety implications and the enjoyment of current users, including riders, into consideration.</p> <p>Improvements to a bridleway must make the way easier, safer, or more enjoyable its users, enhancing its suitability for use. Any works to a bridleway which detract from its suitability might not be a proper exercise of the highway authority’s power to improve a highway.</p> <p>Any changes to crossings which are proposed to link new routes must be multi user including equestrians. Any barrier along a route renders the</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>whole route useless. All routes need to be RSA assessed from the outset to ensure they are not delivered and then discovered not to be suitable for equestrians.</p> <p>Care needs to be taken in the use of the Strava information – this is not a valid NMU representative group. Also, as we mentioned, the Strava purportedly cycling information, may well be distorted by equestrians who use the app but without the opportunity to register as horse riders rather than cyclists.</p>	
Paragraph 1.1 - 1.2	Broads Authority (Natalie Beal)	156		<p>Summary of response The Strategy is welcomed and well presented. There is a lack of mention of the Broads in this document considering that we were involved in its production to some extent and that some of the routes go through the Broads.</p> <p>Detailed comments It might be worth saying that you have also liaised with us about this Strategy and that we will work with you to promote walking and cycling and where appropriate work with you on delivery of some routes that are in the Broads. You might want to clarify that the strategy applies to all of the district including that area that is the Broads. Indeed, some routes actually go through our area.</p> <p>Steering group – do you want us to be on that group?</p> <p>Policy context – would be useful to mention</p>	In recognition of the fact that the Broads have been involved in the preparation of the Strategy and their important role in future implementation the Strategy has been amended.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				the Broads Plan and Integrated Access Strategy . Also, that our current Local Plan safeguards some routes and that approach could be used to help implement this strategy.	
Paragraph 1.1 - 1.2	Bull Phil and Janet	678		We have one further comment which relates to some of the terminology used in the document (which we have noticed in previous plans we have seen). When reading the document, we found several examples of planning terminology that required us to make internet searches to find the meanings. It is possible that we missed a Glossary of Terms section in the 229 pages, but we could not find one. In future, where planning documents are to be read and commented on by people who are not planning professionals, please can you adopt the good practice of including a Glossary of Terms.	Comment noted. When preparing the Strategy we have tried to use plain English where possible, but at times more technical language is required. The suggestion is considered reasonable, and a glossary has been added to the final document.
Paragraph 1.1 - 1.2	Bungay Cycle School (Mark Elmy)	431	No	<p>People start cycling locally therefore we need more 20mph local restrictions, wider pavements, fewer trucks using local residential, local, community roads as 'cut-throughs - especially when there are far better roads suitable for their usage. This is especially poinient in Bungay where we have huge 44 ton lorries using the centre of the town, through narrow, old, residential and town centre to avoid using more appropriate 'A' roads - like the A145 & A146 Ellough Road.</p> <p>Specifically in Bungay we NEED a 20MPH limit throughout, bounded by the extremities of the town to allow local residents to walk and cycle into the town centre and children to cycle to school.</p> <p>I gave a Bikeability course to St. Edmunds school children and had a conversation with a mother collecting her child, praising how well they had done</p>	The recommendations within the Strategy have attempted where possible to create infrastructure that moves cyclists off-road. An assessment of all roads within Bungay would be required to include a blanket speed limit across a settlement. Request to reduce speed needs to be made through the county councillor.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>to be told the bike would go back in the shed because its too dangerous to cycle to school because of the lorries using the town centre.</p> <p>Within the points of Bottom of Annis Hill Watchhouse hill Ditchingham Dam/Chicken RAB Clays RAB Earsham Dam Flixton Road X road a 20mph limit is NEEDED. Please copy the below into a browser Bungay to 9-81 B1435, Bungay - Google Maps</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
Paragraph 1.1 - 1.2	Felixstowe Town Council (Ash Tadjrishi)	908		<p>A core part of FTC's original comments was to better recognise, then to maintain and publicise, existing infrastructure. New infrastructure should surely be designed to link to and make better use of existing, often fragmented paths. If these are ignored, there is a danger of duplication and wasted costs. Improved maintenance and signing of these should be considered as a candidate for CIL, or other, funding, in parallel with new provision. Many of these will be core links from development on the north of the town through to sea front and town centre destinations. We would request that this aspect be significantly</p>	<p>The existing quality and extent of the cycling and walking network across East Suffolk has been an important source of evidence in the identification of the Strategy's recommendations. The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance of existing infrastructure cannot. CIL spend and the regulations that govern the spending of CIL lie outside the scope of the Strategy.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>re-considered.</p> <p>More fundamentally, it must be recognised that the core purpose of the highway is to facilitate the social and commercial life of the community in the widest sense. The transport network (in its broadest sense) has evolved, and must continue to evolve, to provide safe, attractive and efficient movement for all network users, with a wide and improved choice of modes, ideally separated wherever possible. We support the aim of achieving a safe, integrated network of routes which encourages and supports all those who can use cleaner / quieter / healthier forms of transport (for utility and leisure), while still allowing motor vehicle users to go about their business without unreasonable inconvenience.</p> <p>FTC strongly supports development of dedicated networks for cycling and / or walking wherever possible to foster greater use of these modes.</p> <p>Where dedicated routes cannot be provided, and redesign of existing roads is essential to fulfil a clear need for an improved route for cyclists and /or pedestrians, these should seek to minimise disruption to overall traffic flows, avoiding causing congestion, as demonstrably the greatest cause of air pollution and inefficient use of fuels of all types, and consequent diversion of traffic to unsuitable minor roads or streets.</p> <p>In all cases, conflicts, or perception of conflicts, between user classes must be minimised.</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
Paragraph 1.1 - 1.2	Greener Waldringfield (Chris Baldry)	248	Yes	<p>The strategy as stated simply doesn't go anywhere near far enough. There are a number of exciting and positive things within the recommendations. However, they are limited to the 'Key Corridors', which in respect to Waldringfield means that no new provision is being recommended whatsoever. Further, where recommendations are made on the 'Key Corridors', it is not apparent that cycling and walking routes will be totally physically separate and at a safe distance from vehicular traffic, as the routes seem to utilise existing roads in many places.</p> <p>The recommendations, though positive, would only bring a limited amount of East Suffolk up to the standard of many European Countries some 20+ years ago. As a response to a declared 'Climate Emergency' this strategy is woefully short of what is needed. A modal shift in means of transport from high energy car use to one where walking and cycling dominates, requires:</p> <ul style="list-style-type: none"> • All areas of East Suffolk to be included in new or improved route provision, not just people served by 'Key Corridors' • Brave and innovative changes that seriously incentivise people across all of East Suffolk to favour cycling and walking over other forms of transport wherever possible. Even if that means compulsory land acquisition* and diversion of budgets away from road building. 	<p>Your comments have been noted. There has been a focus on the key corridors which run through and between the major urban areas because these provides the highest potential modal shift. As there is limited amount of resource available the Strategy has tried to focus on the most achievable recommendations, whilst also trying to be ambitious for the whole district.</p> <p>However, we also undertook initial consultation to obtain recommendations from across East Suffolk which included several responses within Waldringfield, which are community recommendations within the final Strategy. Furthermore, a key corridor does pass close to Waldringfield.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				Note. * Land acquisition doesn't appear to be a problem when major infrastructure projects such as the creation of the M25, or HS2 are concerned. Taking such measures in a Climate Emergency must be seen as justified.	
Paragraph 1.1 - 1.2	Historic England (Marsh, Andrew)	316		<p>We welcome the production of these Supplementary Planning Documents and Draft Cycling and Walking Strategy, but do not currently have capacity to provide detailed comments. With regards to the Cycling and Walking Strategy we would refer you to our detailed guidance on successfully incorporating historic environment considerations into the development process, which can be found here: Streets for All https://historicengland.org.uk/images-books/publications/streets-for-all/. This document sets out principles of good practice for street management - such as reducing clutter, co-ordinating design and reinforcing local character. The manuals, covering each of the English regions, provide inspiration and advice on street design which reflects the region's distinctive historic character.</p> <p>To avoid any doubt, this letter does not reflect our obligation to provide further advice on or, potentially, object to specific proposals which may subsequently arise as a result of the proposed plan, where we consider these would have an adverse effect on the historic environment.</p>	In preparing the Strategy we have considered how our recommendations might impact the historic environment. It is important that should a recommendation be taken forward to delivery that the historic environment is taken into consideration in the final design.
Paragraph 1.1 - 1.2	Isobel Fleming	80	No	Whilst I agree that walking and cycling are important to health and wellbeing, horse riding has also been proven to promote a similar or better degree of wellbeing, and the needs of horseriders is blatantly	Where appropriate horse riders were considered in the formation of any new or improved infrastructure. The British Horse Society were a consultee and have been directly engaged with.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				missing from this whole document. In fact, mention is constantly made to resurfacing bridleways to make it more suitable for cyclists, and this would seriously affect the safety and wellbeing of horseriders. If a surface is so suitable for cyclists they can achieve speeds similar to that of cars, with the subsequent and associated safety issues.	
Paragraph 1.1 - 1.2	Jackie Street	584	Yes	<p>I feel that Woodbridge is a beautiful place to live with a love for the community and countryside ,but with the huge amount of walkers and cyclists and vast equestrian facilities ,the roads around are clearly not friendly enough for the public to exercise in a safe manner ,with either vehicles driving too fast or driving past too close.</p> <p>I keep my horse on the Sutton Hoo road, which has lovely track which runs across the road to the forest ,but the road is both fast and with a hidden dip dangerous for anyone to cross .</p> <p>Im sure the council would want to embrace the full potential for the locals to use these paths and roads with confidence instead of fear .</p>	The Ipswich to Melton Key Corridor has been extended to provide a low traffic route to Sutton Hoo. Furthermore, we have identified a leisure route from Woodbridge to Bawdsey which makes best use of the available PROW to create traffic-free cycle and walking routes.
Paragraph 1.1 - 1.2	Jeremy Boyle	643	No		Objection noted.
Paragraph 1.1 - 1.2	John Clark	36		Making is easy to get from one village / town to another on foot or by bicycle will help reduce climate change. Making roads safer for pedestrians should be an aim. If we become fitter so much the better. Wheelchair users also need to be considered. My neighbour in Walpole often travels this way to and from Halesworth along the B1117.He cannot jump onto a verge, into a ditch or a hedge. Lower speed	No change required.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				limits than the NSL are needed outside villages and towns.	
Paragraph 1.1 - 1.2	Kate Cooper	685		With your walking and cycling strategy please can you consider more off road routes for carriage drivers you mention equestrian use - please include us carriage drivers not just riders.	There are challenges in designing infrastructure that can accommodate all users. However, the aim of the Strategy has been to accommodate all users. Further consideration can be given to carriage drivers at a design stage.
Paragraph 1.1 - 1.2	Kate Startin	168	No	Cycling and walking cannot just be lumped together. What suits one group may be totally unsuitable for the other	Both users have been carefully considered and often combined infrastructure represents the best approach given the existing constrained road network. In other cases, separate infrastructure for both users have been considered and recommended.
Paragraph 1.1 - 1.2	Kirk Weir	169	Yes	This is essentially a Cycling Strategy and the title is misleading. There are no proposed pedestrian only footpaths. The idea that pedestrians and cyclists can safely use the same pathway is not addressed. I appreciate that cyclists using Bridleways are expected to give way to pedestrians as a matter of courtesy. I also appreciate the dangers of cycling on roads, especially country roads, and hence the wish to separate cyclists from road traffic.	Both users have been considered throughout the strategy, in many cases combined infrastructure could be the best approach given the constraints of the existing road network. There are instances where pedestrian specific improvements have been recommended, however our approach has been to secure infrastructure improvements for both cyclists and pedestrians.
Paragraph 1.1 - 1.2	Leo Borwick	28	Yes	An important missing element of this very welcome strategic initiative is a set of metrics for the things that the strategy is designed to affect. For example, the Council has recognised a climate emergency and whilst this strategy could be expected to make a positive impact on this, it is difficult to gauge how big this is. Whilst I would accept that direct effects on CO2 emissions might be hard to gauge, mode shares for active travel might be a reasonable way of operationalizing this. Potential mode share is effects are considered in relation to comments, but no baseline is stated and no estimate is made of	<p>The Council has created a vision for cycling and walking throughout East Suffolk based on data including that of modal shift from the Propensity of Cycle Tool, informed by community engagement.</p> <p>The implementation of the Strategy will be monitored and officers will continue work closely with Suffolk County Council in this respect.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>changes flowing from the strategy, or targets set. It is hard to know, therefore whether the strategy goes far enough. in the context of an emergency, it seems to me to be essential to do this kind of assessment.</p> <p>Similar comments might be made about other issues that the strategy is no doubt intended to address, such as the epidemic of I'll health related to inactive lifestyles, promoting active tourism, countering air pollution and so forth.</p>	
Paragraph 1.1 - 1.2	Liz Buckley	23		<p>In the 1st paragraph you mention equestrian use, this is not mentioned anywhere else, you talk about leisure, well being, and green space all of which are essential to everybody, as horse riders, we also need access to bridleways and green spaces, with all the new developments locally we are losing our bridleways, the ones we have are overgrown, other councils have "set aside" where verges are able to be ridden on, please consider horse riders too when you are planning new routes, Sandy or grass tracks, not just concrete or stones</p>	<p>Where appropriate, equestrian users were considered in the formation of any new or improved infrastructure. The British Horse Society were a consultee and have been directly engaged with.</p> <p>The surfacing will be a consideration for the design stage, but the Strategy is clear that the surface must be suitable for its location and intended users.</p>
Paragraph 1.1 - 1.2	Marine Management Organisation (Sir/Madam)	281		<p><u>Consultation response - PLEASE READ</u></p> <p>Thank you for including the Marine Management Organisation (MMO) in your recent consultation submission. The MMO will review your document and respond to you directly should a bespoke response be required. If you do not receive a bespoke response from us within your deadline, please consider the following information as the MMO's formal response.</p>	<p>Throughout the preparation of the Strategy the management of the coast has been considered particularly in relation to routes planned near the coast.</p>

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				<p><u>Marine Management Organisation Functions</u></p> <p>The MMO is a non-departmental public body responsible for the management of England’s marine area on behalf of the UK government. The MMO’s delivery functions are: marine planning, marine licensing, wildlife licensing and enforcement, marine protected area management, marine emergencies, fisheries management and issuing grants.</p> <p><u>Marine Planning and Local Plan development</u></p> <p>Under delegation from the Secretary of State for Environment, Food and Rural Affairs (the marine planning authority), the MMO is responsible for preparing marine plans for English inshore and offshore waters. At its landward extent, a marine plan will apply up to the Mean High Water Springs (MHWS) mark, which includes the tidal extent of any rivers. As marine plan boundaries extend up to the level of MHWS, there will be an overlap with terrestrial plans, which generally extend to the Mean Low Water Springs (MLWS) mark. To work together in this overlap, the Department of Environment, Food and Rural Affairs (Defra) created the Coastal Concordat. This is a framework enabling decision-makers to co-ordinate processes for coastal development consents. It is designed to streamline the process where multiple consents are required from numerous decision-makers, thereby saving time and resources. Defra encourage coastal</p>	

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				<p>authorities to sign up as it provides a road map to simplify the process of consenting a development, which may require both a terrestrial planning consent and a marine licence. Furthermore, marine plans inform and guide decision-makers on development in marine and coastal areas.</p> <p>Under Section 58(3) of Marine and Coastal Access Act (MCAA) 2009 all public authorities making decisions capable of affecting the UK marine area (but which are not for authorisation or enforcement) <u>must have regard to</u> the relevant marine plan and the UK Marine Policy Statement. This includes local authorities developing planning documents for areas with a coastal influence. We advise that all marine plan objectives and policies are taken into consideration by local planning authorities when plan-making. It is important to note that individual marine plan policies do not work in isolation, and decision-makers should consider a whole-plan approach. Local authorities may also wish to refer to our online guidance and the Planning Advisory Service: soundness self-assessment checklist. We have also produced a guidance note aimed at local authorities who wish to consider how local plans could have regard to marine plans. For any other information please contact your local marine planning officer. You can find their details on our gov.uk page.</p> <p>See this map on our website to locate the marine plan areas in England. For further information on how to apply the marine plans and the subsequent</p>	

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				<p>policies, please visit our Explore Marine Plans online digital service.</p> <p>The adoption of the North East, North West, South East, and South West Marine Plans in 2021 follows the adoption of the East Marine Plans in 2014 and the South Marine Plans in 2018. All marine plans for English waters are a material consideration for public authorities with decision-making functions and provide a framework for integrated plan-led management.</p> <p><u>Marine Licensing and consultation requests below MHWS</u></p> <p>Activities taking place below MHWS (which includes the tidal influence/limit of any river or estuary) may require a marine licence in accordance with the MCAA. Such activities include the construction, alteration or improvement of any works, dredging, or a deposit or removal of a substance or object. Activities between MHWS and MLWS may also require a local authority planning permission. Such permissions would need to be in accordance with the relevant marine plan under section 58(1) of the MCAA. Local authorities may wish to refer to our marine licensing guide for local planning authorities for more detailed information. We have produced a guidance note (worked example) on the decision-making process under S58(1) of MCAA, which decision-makers may find useful. The licensing team can be contacted</p>	

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				<p>at: marine.consents@marinemanagement.org.uk.</p> <p><u>Consultation requests for development above MHWS</u></p> <p>If you are requesting a consultee response from the MMO on a planning application, which your authority considers will affect the UK marine area, please consider the following points:</p> <ul style="list-style-type: none"> • The UK Marine Policy Statement and relevant marine plan are material considerations for decision-making, but Local Plans may be a more relevant consideration in certain circumstances. This is because a marine plan is not a 'development plan' under the Planning and Compulsory Purchase Act 2004. Local planning authorities will wish to consider this when determining whether a planning application above MHWS should be referred to the MMO for a consultee response. • It is for the relevant decision-maker to ensure s58 of MCAA has been considered as part of the decision-making process. If a public authority takes a decision under s58(1) of MCAA that is not in accordance with a marine plan, then the authority must state its reasons under s58(2) of the same Act. • If the MMO does not respond to specific consultation requests then please use the 	

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				<p>above guidance to assist in making a determination on any planning application.</p> <p><u>Minerals and Waste Local Plans and Local Aggregate Assessments</u></p> <p>If you are consulting on a minerals and waste local plan or local aggregate assessment, the MMO recommends reference to marine aggregates, and to the documents below, to be included:</p> <ul style="list-style-type: none"> • The Marine Policy Statement (MPS), Section 3.5 which highlights the importance of marine aggregates and its supply to England’s (and the UK’s) construction industry. • The National Planning Policy Framework (NPPF), which sets out policies for national (England) construction mineral supply. • The minerals planning practice guidance which includes specific references to the role of marine aggregates in the wider portfolio of supply. • The national and regional guidelines for aggregates provision in England 2005-2020 predict likely aggregate demand over this period, including marine supply. <p>The minerals planning practice guidance requires local mineral planning authorities to prepare Local Aggregate Assessments. These assessments must consider the opportunities and constraints of all</p>	

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				mineral supplies into their planning regions – including marine sources. This means that even land-locked counties may have to consider the role that marine-sourced supplies (delivered by rail or river) have – particularly where land-based resources are becoming increasingly constrained.	
Paragraph 1.1 - 1.2	Marlesford Parish Council (Richard Cooper)	400	Yes	Marlesford Parish Council is pleased to see that the proposals contained in the strategy are not exhaustive. Marlesford PC wants to take this opportunity to re-state its case for a new pedestrian and cycleway from Marlesford (Bell Lane) to the proposed SZC Southern Park and Ride - at which point it will link to the proposed pedestrian and cycleway from Wickham Market to the park and ride. The requirement for the new path from Marlesford is in part driven by the potential development of SZC and the resulting increase in bus and HGV use of the A12 between Marlesford and Wickham Market of up to 1,000 movements per day. This could potentially be further exacerbated if the Scottish Power Renewables Friston project goes ahead. This will result in significant cumulative impact in terms of traffic and will increase fear and intimidation for- vulnerable road users (as recognised by both East Suffolk Council and Suffolk County Council). Marlesford PC welcomes the recognition that funding for pedestrian and cycleways could be linked to the development of Nationally Important Infrastructure Projects. We have a particular concern that rural areas can be "left behind" whilst urban area pedestrian and cycling facilities are improved. We want to make the case for allocating funding to rural areas and want to	<p>The Strategy has considered the suggested improvements through the initial consultation where this improvement was suggested and scored forming part of the Strategy as a Community Recommendation.</p> <p>We recognise the importance of rural routes which are included both within parts of the Key Corridors and throughout the wider Leisure Routes. Accordingly a route between Framlingham and Campsea Ashe forms part of the Leisure Routes.</p>

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				underline the importance of providing safe and accessible active travel opportunities in non-urban areas.	
Paragraph 1.1 - 1.2	Marlesford Parish Council (Richard Cooper)	906		<p>2. Comments under Policy Context</p> <p>The weight of support for walking and cycling in national and local policy is clear. As a Parish Council, we endorse all policies designed to provide safe access to walking and cycling opportunities, particularly where these opportunities enhance connectivity between local communities. We welcome the ESC initiative to develop an East Suffolk Cycling and Walking Strategy on the basis that active travel will have a positive effect on health and wellbeing and contribute to a reduction in car journeys.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	Support is noted.
Paragraph 1.1 - 1.2	Miles Thomas	243		<p>Thanks for the meeting the other night. It was a shame that the cyclist path people hadn't looked at the B.L.S. side of things. I believe there is scope for a new cycle path between the new development in Blundeston and the proposed new development in Somerleyton along "Waddling Lane". This would also connect to Somerleyton Station allowing access to the rail network. It could also be extended from Blundeston into the North Lowestoft network allowing cycling to the high Schools in the area.</p> <p>It might also be a project that the Somerleyton</p>	The comments have been noted and are supported. A route utilising Waddling Lane has been included within the Leisure Routes, connecting Somerleyton with Oulton.

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				<p>Estate might be interested in given their green credentials?</p> <p>I've attached a map with a Yellowish Line alongside the suggested route.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
Paragraph 1.1 - 1.2	National Highways (Sir/Madam)	283		<p>We welcome the fact that the promotion of sustainable travel, the provision of facilities and services is promoted in your plan despite the challenges thrown up by the rural nature of the district. As this will not only help reduce CO2 emissions but also improve the health of those who choose to travel by foot or cycle.</p> <p>It is recognised that these routes are in some cases parallel to the Strategic Road Network (SRN) and provide a much safer and pleasant route for those who choose sustainable travel modes this is welcomed.</p> <p>There are a number of places where proposed routes interface with the (SRN), these will need careful design and we would like to be involved in the development of these schemes, as we have experience from elsewhere, in due course.</p> <p>Other policy such as to provide adequate secure cycle storage for individual dwellings. Changing, secure storage and changing facilities need to be</p>	<p>Support is noted. The detailed design stages will need to consider the relationship between the proposed cycling and walking infrastructure and the Strategic Road Network.</p> <p>This Strategy sets recommendations that can be used as an evidence base for plan making.</p>

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				available at destinations. Needs to be in other policy documents to promote sustainable travel, if the plan is to be successful.	
Paragraph 1.1 - 1.2	Paul Reynolds	235	No	<p>I am a cyclist and as far as I am concerned the present cycle tracks do not need to be upgraded. We do not need another bridge and to fell 25 mature trees is crazy. Spend your funds on planting more trees and clearing the weeds along the interlinking alley ways and pathways! Also</p> <p>recently I had a very bad fall in Carlford Close as I tripped over a BT inspection hatch which was 5 cm off the level of the pavement and</p> <p>caused me to severely damage my left elbow and right knee and I had to go to Hospital. Sought the pavements out! Thank you.</p>	<p>IM12 has been amended to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath.</p> <p>Whilst the Strategy's primary focus is on new cycling and walking infrastructure, it is recognised that the maintenance of existing infrastructure is important for the safety of users.</p>
Paragraph 1.1 - 1.2	Paul Roshier	623	Yes	I think this is an excellent initiative. If it happens then it will encourage people to take more exercise, safely and enjoy our beautiful countryside.	Support noted.
Paragraph 1.1 - 1.2	Peter Hudson	61		<p>You have invited comments re the above.</p> <p>As a first step, can East Suffolk adopt Vision zero? This has already been adopted by London and Essex https://www.essexhighways.org/news/vision-zero-no-more-deaths-on-essex-roads</p> <p>I look forward to your comments</p>	<p>One of the key aims of the Strategy is to improve the safety of cyclists and pedestrians and aim to create infrastructure segregated from the road where possible.</p> <p>An East Suffolk Council commitment to Vision Zero is outside the scope of this Strategy, but has been notified to relevant officers.</p>
Paragraph 1.1 - 1.2	Peter Kerridge	402	No	The process of commenting on this document is inherently flawed. It is disproportionately aimed at people who are computer owners and computer literate. It does not comply with the Disability	The formal consultation on the draft Strategy sought to give the public the opportunity to view the document online and printed versions at the ESC customer service centres, as well as the opportunity to comment on the

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				Discrimination Act. The fact that you get automatically thrown out of the response after a relatively short period of time and lose all of the comments you have spent time doing is appalling.	document via the online web portal, email, and/or letter. In doing so, the consultation was open to all and a significant number of consultation responses were received.
Paragraph 1.1 - 1.2	Peter Kerridge	602	No	So many of the current cycle paths are just not maintained. Pre pandemic I regularly used the cycle path between Woodbridge and Ufford and found it overgrown with brambles, low tree branches, covered in broken glass, and in parts the path is barely discernible as it had deteriorated so badly. If you can't maintain the current paths what chance is there for even more paths.	Whilst the primary focus of the Strategy is new cycling and walking infrastructure maintenance of existing infrastructure is also important. General maintenance is outside the remit of this Strategy and should be reported directly to the infrastructure owner. The Suffolk County Highway Department has a tool on their website site to report such maintenance issues.
Paragraph 1.1 - 1.2	Rachel Smith-Lyte	630	Yes	Really pleased equestrian use has been included in this. Horse riders take their lives in their hands trying to cross our rural roads in particular.	Support noted.
Paragraph 1.1 - 1.2	Reydon Parish Council (John Roger Cracknell)	182	Yes		Support noted.
Paragraph 1.1 - 1.2	Reydon Parish Council (John Roger Cracknell)	183	Yes	Seems sensible	Support noted.
Paragraph 1.1 - 1.2	Rob Mabey	588	Yes	Brilliant initiative. As a former Aldeburgh business association we were always looking for safe recreational areas. This prompted the Aldeburgh triathlon to show off the areas sporting possibilities . If the proposals can be realised it would be wonderful for the locals who like me enjoy the triathlon sports.	Support noted.

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				<p>when visiting France there were lots of paths for biking and walking through woods and the coast, very well used and safe .</p> <p>keep up the good work</p>	
Paragraph 1.1 - 1.2	Saliscrown Limited (David Gilbert)	1		 <p>Having granted planning permission for the construction of a crematorium within the Beccles areas you seem to have no regard that one of the only bicycle hearses in the country are based here and have struggled to negotiate the roads to the crematorium. The bicycle hearse cannot use the cycle routes as you seem adamant to erect bollards at the centre of the cycle lanes which do not leave sufficient space for the hearse to pass. We need wide, smooth cycle lanes with steady gradients. Otherwise we have to negotiate the normal traffic and risk getting killed, or would this be like double jeopardy!</p>	<p>Beccles is situated on a key corridor so recommendations for improvements can be found throughout the town. Where possible wider cycle tracks have been considered which are segregated from the footway before shared paths are considered if the space renders this approach unfeasible.</p>

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Paragraph 1.1 - 1.2	S Hall	622	No	<p>Comment regarding:</p> <p><i>1.1 The East Suffolk Cycling and Walking Strategy identifies potential cycling and walking infrastructure opportunities across the district. The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure.</i></p> <p>I am concerned that the strategy focuses on new infrastructure opportunities only. Unless I missed a reference to this somewhere, I didn't see any reference to how any new routes and adjacent connecting routes would be maintained to a high standard in the future. This is really important for safe cycling and for the ongoing delivery of most of the criteria / outcomes in your Multi-Criteria Assessment Framework (MCAF). The draft Strategy needs to address this.</p> <p>There would be no point in going to the trouble of assessing all of the route options according to your MCAF, then going ahead and building routes with the highest scores – only for them to be abandoned on the day they are opened to an undefined future fate.</p> <p>Sad to say, this fate, based on current experience, is likely to be a fate of future of neglect. I attach a few photos taken in Kesgrave in recent years which I hope highlight some of the problems. I have lots</p>	<p>The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance of existing infrastructure cannot. Throughout the preparation of the Strategy recommendations have been identified that seek to improve existing and propose new infrastructure, based on a thorough understanding of the current quality of existing cycling and walking infrastructure. In order to provide clarity as to the importance of planning for a coherent cycling and walking network, the opening paragraph of the Strategy has been amended to emphasise the need for cycling and walking infrastructure to be safe, coherent, direct, comfortable and attractive.</p>

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				<p>more but your system only allows 20 photos.</p> <p>I don't describe the future as one of neglect lightly. A case in point is the shared pedestrian / cycle route alongside the A1214 in Kesgrave which has been in a poor state for years. If I was to assess this route based on your MCAF – which could be used as an assessment tool for cycle routes in use - this is what I might say: -</p> <p>Safety, (in which you say: <i>the quality of the proposed infrastructure will be a factor</i>);</p> <ul style="list-style-type: none"> • The consultation comments speak for themselves as people point to the safety hazard caused by the poorly maintained and potholed surface which is described as unfit for use. I attach a photograph. • People comment that in places the route is so poorly maintained that it has become unsafe to use and people cycle on the road instead. • They also point to the dangerous situation of the cycle route crossing multiple side roads where a cyclist is forced to give way at each side road so the route does not deliver the safety and convenience required by current cycling policy. • They point out that cars are often parked on the cycle path causing obstructions and meaning the route cannot be used and people end up cycling on the road instead. 	

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				<ul style="list-style-type: none"> • The above indicates that the quality of the infrastructure and it’s management is poor and it would have a very low score or fail the assessment <p>Modal Shift</p> <ul style="list-style-type: none"> • I doubt people would wish to leave the comfort of their car to cycle over a bumpy, potholed and dangerous cycle route! Where is the incentive for that?! • The above indicates that the quality of the infrastructure is poor and it would have a very low score or fail the assessment <p>Connectivity & Growth</p> <ul style="list-style-type: none"> • A bumpy, potholed and dangerous cycle route will in many cases, simply not be used so will fail to deliver connectivity and sustainable growth. • The above indicates that the quality of the infrastructure is poor and it would have a very low score or fail the assessment <p>Optimisation</p> <ul style="list-style-type: none"> • I doubt that a bumpy, potholed and dangerous cycle route would encourage users of motor vehicles to take more trips by sustainable means. 	

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				<ul style="list-style-type: none"> • The above indicates that the quality of the infrastructure is poor and it would have a very low score or fail the assessment <p>Leisure</p> <ul style="list-style-type: none"> • I doubt people would choose to use a bumpy, potholed and dangerous cycle route to access leisure destinations when other options are available. • Such a poorly maintained route would not be a high value attraction in itself. • The above indicates that the quality of the infrastructure is poor and it would have a very low score or fail the assessment <p>Some consideration of the maintenance of routes in the future is essential.:</p> <ol style="list-style-type: none"> 1. Your own assessment criterion acknowledges under the category “Safety” that the <i>quality of the proposed infrastructure will be a factor</i>). But the Strategy does not acknowledge that good maintenance is the key to the ongoing quality of infrastructure once it is built However clean and safe a new route is when it is first opened – it will sooner or later require maintenance to keep it clear of encroaching vegetation, surface debris and to keep it operating safely. 2. It’s also really important to understand that any new cycling infrastructure provided through this Strategy will link into existing routes as part of a 	

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				<p>cycling and walking network. It is the network as a whole which enables access to work, school, leisure activities etc. We can't look at new routes in isolation. We have to take a network wide approach. This means considering how we look after both existing and new routes in the cycling and walking network.</p> <p>For example if a new cycle route is provided – it may not be used to its full potential – and indeed it may hardly be used at all - if adjacent/connecting routes remain in a poor state of repair or are not maintained to a high standard and people see them as too dangerous to use. The Cycling & Walking Strategy needs to address connectivity within the whole network to provide safe, direct, convenient and attractive routes to the places people need to travel to and from.</p> <p>3. The evidence from your initial consultation highlights that some existing cycle routes are dangerous and in some cases they are not being used because they are poorly maintained and are viewed as being of poor quality. This is the view from people who want to use them. And it's important to hear those views. If people aren't able to use new and existing routes due to poor maintenance, that undermines the success of the Walking & Cycling Strategy.</p> <p>A New Policy is Needed:</p> <p>There is currently a policy gap in the draft Strategy</p>	

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				<p>as there is no explanation of how high quality maintenance of infrastructure will be carried out. It would be helpful to include a new pro-active policy commitment in the draft Strategy to acknowledge and address all of the above. This could usefully include:</p> <ol style="list-style-type: none"> 1. An explanation of the different functions of the County and District Councils regarding maintenance – to identify who does what. 2. Identifying how partnership working between the District and County Council and any other partners will work to deliver high quality maintenance of infrastructure. (To be written ideally, in collaboration with the partners). 3. Identifying ESCs commitment to ensuring high quality route / infrastructure maintenance in order to deliver your MCAF outcomes as a measure of quality in the longer term. And ideally a commitment from the partners too. 4. To refer to / identify management and maintenance schedules that are transparent to taxpayers and show how new and connecting infrastructure in the walking and cycling network will be maintained. E.g. How often will the surface condition be checked / swept of glass, leaves and other debris? How often will grass verges and other vegetation that grows over the route be monitored and cut back? How will the surface be kept smooth, safe and pothole free? How can users of the routes report back any problems and receive a timely response and timely action? 5. To identify budgets that will be used for the above 	

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				<p>Closing the Funding Gap – Maintenance Budgets</p> <p>Over the last decade the County Council has had its maintenance budgets cut. When I've requested pedestrian and cycle route maintenance, it's taken a long time to get a reply and it seems to me that this issue is a low priority for the County Council because issues have often remained unresolved. The maintenance of the <u>road</u> network is the top priority and cycling and walking route maintenance seems to fall down something that I would describe as a funding gap.</p> <p>How can the District Council help here? A few suggestions that could be considered / agreed and included in the Strategy are as follows: -</p> <ol style="list-style-type: none"> 1. Maintenance could be a key factor to discuss when speaking with developers / national infrastructure providers. If new cycling and walking infrastructure is to be provided, can they provide a commuted 25 year sum (for example) to ensure the route is looked after? This represents a commitment that the infrastructure (asset) will be looked after in the long term to provide maximum value for their investment. 2. When grant applications are being made to central government or others for active travel infrastructure, as above, can a 25 year sum be included in the revenue budget bid, for ongoing maintenance? 3. Can the County and District agree that <u>both</u> will 	

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				<p>contribute funding to meet an agreed high quality management/maintenance schedule for the maintenance of all walking and cycling routes please? Support from ESC may ease the burden on the County Council's budget and help ESC to meet its own outcomes for safety, modal shift etc. It's all taxpayers money, afterall, whichever Council raises it.</p> <p>For example: I know that on request, ESC has arranged for the cycle slip route on the A1214 by Ropes Drive West to be swept free of debris. But this does not seem to be part of any regular maintenance schedule. Unfortunately the cycle slip route almost always has a lot of debris in it which is hazardous to those of us on two wheels. These details are important.</p> <p>4. Can you create a protected and ring-fenced budget for the maintenance of walking and cycling infrastructure please?</p> <p>Without all of this I fear that any new routes created will suffer the same fate as the shared pedestrian cycle route along the A1214 – falling into a state of disrepair and representing an asset that has now become a liability and a danger to pedestrians and cyclists.</p> <p>Attachments: https://eastsoffolk.inconsult.uk/gf2.ti/af/1322978/455158/PJP/</p>	

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				<p>/11774901%201%20footway%20by%20milsoms%20bus%20stop%2Ejpg</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455159/PJP/-</p> <p>/11774901%202%20overgrown%20hedeg%20cycle%20footway%20Halls%20Drift%20to%20Bartrum%20Lane4%2Ejpg</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455160/PJP/-</p> <p>/11774901%203%20footpath%20and%20cycle%20route%20by%20All%20Saints%20roundabout%2Ejpg</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455161/PJP/-/11774901%204%20P7160112%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455162/PJP/-</p> <p>/11774901%205%20overgrown%20hedeg%20cycle%20footway%20Halls%20Drift%20to%20Bartrum%20Lane%2Ejpg</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455163/PJP/-/11774901%206%20P7160108%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455164/PJP/-/11774901%207%20P7160111%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455165/PJP/-</p> <p>/11774901%208%20overgrown%20hedeg%20cycle%</p>	

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				<p>20footway%20Halls%20Drift%20to%20Bartrum%20Lane%2Ejpg</p> <p>https://eastsoffolk.inconsult.uk/gf2.ti/af/1322978/455166/PJP/-/11774901%209%20P7160056%2EJPG</p> <p>https://eastsoffolk.inconsult.uk/gf2.ti/af/1322978/455167/PJP/-/11774901%2010%20P7160110%2EJPG</p> <p>https://eastsoffolk.inconsult.uk/gf2.ti/af/1322978/455168/PJP/-/11774901%2011%20close%20up%20by%20Kesgrave%20buslink%2Ejpg</p> <p>https://eastsoffolk.inconsult.uk/gf2.ti/af/1322978/455169/PJP/-/11774901%2012%20footway%20near%20KHS%2Ejpg</p> <p>https://eastsoffolk.inconsult.uk/gf2.ti/af/1322978/455170/PJP/-/11774901%2013%20P7160109%2EJPG</p> <p>https://eastsoffolk.inconsult.uk/gf2.ti/af/1322978/455171/PJP/-/11774901%2014%20overgrown%20hedeg%20cycle%20footway%20Halls%20Drift%20to%20Bartrum%20Lane%2Ejpg</p> <p>https://eastsoffolk.inconsult.uk/gf2.ti/af/1322978/455172/PJP/-/11774901%2015%20footpath%20west%20of%20Bell%20Lane%20junct%2Ejpg</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455173/PJP/-/11774901%2016%20A1214%20foot%20cycle%20way%2EJPG</p> <p>Kesgrave Fisheries cycle footway parking Dec 29th Photo 4 - <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455175/PJP/-/11774901%2018%20overgrown%20hedeg%20cycle%20footway%20Bartrum%20Lane%20to%20play%20area%2Ejpg</p>	
Paragraph 1.1 - 1.2	Suffolk County Council (Andrew Woodin)	114		I see PRoW are covered in the draft strategy, I'd like to see acknowledgement PRoW can be created if it is seen to be in the public interest.	The comment is noted and some new PROW's have been recommended within the Strategy. The detailed design stage will consider the best approach in achieving cycle/walking connections and this may involve the create of new PROW routes.
Paragraph 1.1 - 1.2	Susan Mason	249		Why does the council think it has the right to destroy woodland that is not owned by them to provide a cycle/footpath for people who will live on the other side of the A12 who will have plenty of areas that they can walk/ride. If they need to go to Martlesham/Kesgrave there is already ample cycle paths along the A12 that come out by BT and dual paths from Deben Avenue that can be used without destroying further habitats for our wildlife and precious trees.	The importance of the natural environment to wildlife and the health and wellbeing of residents is recognised within the Strategy, and various amendments have been made to recommendations to reduce the impact on the natural environment, whilst providing for high quality cycling and walking environments.

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				We are supposed to be protecting our green spaces not destroying them.	
Paragraph 1.1 - 1.2	Timandra Howell	267	No	I live in the local area and regularly walk in the woods. It is quiet, tranquil and safe. The addition of a cycle path will be detrimental to the wildlife of the area as the numbers of users is significantly increased. The walkers would need to be constantly looking over their shoulders to ensure they are not in the line of cyclists. At present there is no restriction and people are on the whole very sensible and respectful of others when they are cycling in the woods. Making a designated cycle path would almost certainly change these habits.	The approach of the Strategy has been to avoid harm to the natural environment where possible.
Paragraph 1.1 - 1.2	Trimley St Mary Parish Council (Debra Cooper)	287	Yes	It would make sense to follow up this strategy with a published implementation plan. This can draw on the funding mechanisms outlined within the strategy but overlay with delivery details from known infrastructure and development activity across the region. Details submitted within planning applications would enable a priority and timeline (even if only indicative) and a funding source to further the effectiveness of the strategy and closely align it to development activity being actively planned.	The implementation section of the Strategy sets out how the Strategy will be implemented and monitored. An Infrastructure Delivery Framework is being created linking recommendations to potential funding mechanisms. We are also creating a prioritisation methodology with SCC where recommendations will be prioritised post-adoption.
Paragraph 1.1 - 1.2	Woodbridge Town Council (Greg Diaper)	432		General Comments Woodbridge Town Council ('WTC') has reviewed the draft strategy in the light of the data collected by ESC and WTC councillors' knowledge of the use by cyclists and walkers of roads and footways within Woodbridge and its immediate environs.	Ongoing SCC projects have been accounted for throughout the preparation of the Strategy so as to avoid duplication of and abortive infrastructure recommendations. Comments relating to specific recommendations have been addressed in the section of the Consultation Statement relevant to the specific recommendations.

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				<p>Further WTC has taken into consideration the following:</p> <ul style="list-style-type: none"> • The likely significant impact of the current design being finalised on its behalf by Suffolk County Council Highways for several 20mph zones within the town boundaries as well as the likely increasing of the speed limit by SCC along the B1438 between the A12 and the junction with Old Barrack Road/California from 30 to 40mph. • The likely significant impact of the SCC proposed A12 improvements between Seven Hills and Woods Lane, specifically the revision to junctions at the three roundabouts on the outskirts of Woodbridge and the dualling of the single lane section between the B1438 and B1079 junctions. <p>WTC, in its review of the consultation, has also examined the ESC proposals based on its belief that IM 15 and 17 should be the primary strategic route for cyclists travelling from Ipswich/Kesgrave/Martlesham to Woodbridge and/or Melton. WTC is of the view that further cycling accesses into Woodbridge can be added from IM17 which provide an alternative option that may better meet the core design objectives in section 4 of the Department of Transport LTN1/20 than some of the routes in the ESC draft strategy. These will be commented upon in detail under the comments section for individual routes.</p>	

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Paragraph 1.3	Anthony Webb	10	Yes	<p>My interest is primarily facilitating the crossing of the A14 between Bucklesham and Levington for cyclists and pedestrians along Levington Lane</p> <p>Thankyou for your attention</p>	<p>Comments noted. The Strategy looks to provide a connection between Levington and Bucklesham over the A14 in Key Corridor recommendation IF14.</p>
Paragraph 1.3	British Horse Society (Lynda Warth)	669		<p>Initial map-based consultation (19 October 2020 to 7 December 2020)</p> <p>Consultees were invited to identify existing cycling and walking issues across East Suffolk and, where possible, suggest solutions to them. Respondents were encouraged to plot their response on an online map. Over 800 comments were submitted, and these can be viewed on the initial consultation map.</p> <p>The map did not show the rights of way network so users of that network were unable to comment easily whilst it was made clear from the outset that this was a walking and cycling consultation. The fact that that the bridleway network was to form part of the new provision and changes were intended was not made clear to horse riders nor anyone else. That is to be regretted.</p> <p>Steering group</p> <p>In recognition of the fact that East Suffolk Council (ESC) is the Local Planning Authority and Suffolk County Council (SCC) is the Local Highways Authority, a steering group of ESC and SCC officers was established to inform and guide the development of the Strategy.</p>	<p>The initial consultation map provided the opportunity for suggestions to be made in relation to cycling and walking issues and opportunities across the whole of East Suffolk. These could have been, and were, made on Public Rights of Way.</p> <p>Consideration has been given to the needs of horse riders throughout the preparation of the strategy particularly in relation to bridleways. The County Public Rights of Way team were part of the steering group.</p>

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				Are there any horse riders on this group? The BHS as the representative body for horse riders clearly should be included. Please could you advise us how we can ensure that we are represented?	
Paragraph 1.3	Felixstowe Town Council (Ash Tadjrishi)	909		<p>A core part of FTC's original comments was to better recognise, then to maintain and publicise, existing infrastructure.</p> <p>New infrastructure should surely be designed to link to and make better use of existing, often fragmented paths. If these are ignored, there is a danger of duplication and wasted costs.</p> <p>Improved maintenance and signing of these should be considered as a candidate for CIL, or other, funding, in parallel with new provision.</p> <p>Many of these will be core links from development on the north of the town through to sea front and town centre destinations.</p> <p>We would request that this aspect be significantly re-considered.</p> <p>More fundamentally, it must be recognised that the core purpose of the highway is to facilitate the social and commercial life of the community in the widest sense. The transport network (in its broadest sense) has evolved, and must continue to evolve, to provide safe, attractive and efficient movement for all network users, with a wide and improved choice of modes, ideally separated wherever possible. We support the aim of achieving a safe, integrated network of routes which encourages and supports all those who can use cleaner / quieter / healthier forms of transport (for utility and leisure), while still allowing motor vehicle users to go about their</p>	<p>The existing infrastructure was carefully considered throughout the preparation of the Strategy to create the optimum and most efficient network possible.</p> <p>The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.</p> <p>The Strategy recognises that streets have multiple functions and all users have been considered. The Strategy focuses on cycling and walking which are higher on the user hierarchy, but ultimately the detailed design of the recommendation will have to be considered if the recommendation is taken forward.</p> <p>The Community Recommendations in the Strategy do not generally show who submitted them.</p>

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				<p>business without unreasonable inconvenience.</p> <p>FTC strongly supports development of dedicated networks for cycling and / or walking wherever possible to foster greater use of these modes.</p> <p>Where dedicated routes cannot be provided, and redesign of existing roads is essential to fulfil a clear need for an improved route for cyclists and /or pedestrians, these should seek to minimise disruption to overall traffic flows, avoiding causing congestion, as demonstrably the greatest cause of air pollution and inefficient use of fuels of all types, and consequent diversion of traffic to unsuitable minor roads or streets.</p> <p>In all cases, conflicts, or perception of conflicts, between user classes must be minimised.</p> <p>See above.</p> <p>The 24 comments from FTC were NOT attributed to FTC, but as general Community Comments, which is inappropriate.</p> <p>And in line with above, some not recognised in the proposed Strategy. E.G. Footpath from Brook Lane to Park Avenue: FTC W5B / SCC Map 693.</p>	
Paragraph 1.3	Michelle Golding	565		<p>Re: Identification of Key corridors</p> <p>While the East West route through Beccles is considered Key, the North South route, linking the Town with Norwich is given scant attention.</p> <p>The link to Norwich is just as important for Beccles</p>	<p>The role of Northgate has been considered in the formation of the Strategy. Accordingly a recommendation relating to Northgate has been included.</p> <p>As you note the route between Beccles and Norwich has not been included within the key corridors. However,</p>

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				<p>(and East Suffolk) as is the East Wes route and there is already a waymarked route from Whitlingham Broad to evidence this.</p> <p>These routes transect at the foot of Northgate, itself a vital Walking /Cycling route through the town, part of the Angles Way long distance footpath and National cycle route 30.</p> <p>The current prioritisation of this narrow medieval route, for buses and heavy vehicles, is a serious safety concern for the many walkers and cyclists who currently use it. If preservation of the historic heritage of the town as well as encouragement of it's tourist potential are to be taken seriously this route (104 on the current plan), needs to be taken much more seriously than on this draft and I urge you to reconsider.</p> <p>I have previously sent the attached photo, but reattach as evidence of just how narrow the street is, and just how much heavy traffic is making the route so difficult for Pedestrians and Cyclists</p> <p>Attachments: https://eastssuffolk.inconsult.uk/gf2.ti/af/1322978/454666/PJP/-/11769237%201%20Busses%20in%20Northgate%20Beccles%2Ejpg</p>	<p>there have been discussions with neighbouring authorities including the Broads Authority and Norfolk County Council about improving links across Authority boundaries.</p>
Paragraph 1.3	Reydon Parish Council (John Roger Cracknell)	184	Yes	No comments	Support noted.

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Paragraph 1.3	Sue Tansley	5	Yes	It is an excellent idea but it seems our parish council were unaware of this and as such no one has commented on behalf of our parish. As the footpath officer in the parish of Pettisttee & Loudham I would have liked the opportunity to make comment.	Support noted. The Parish Council was notified of the consultation.
Paragraph 1.3	Suffolk Coast & Heaths and Dedham Vale AONBs (Simon Amstutz)	381		The Suffolk Coast Ltd Destination Management Organisation and AONB commissioned Tourism Strategy funded by the EU Balance project would provide useful background into the development of cycling in the Suffolk coastal area. Similarly the Suffolk Coast & Heaths AONB Management Plan-a statutory document required of LAs to produce for those with AONBs within their area. I would suggest a wider grouping on the steering group to reflect voices beyond the local authorities voices	Various Suffolk Coast and Heaths AONB documents have been used throughout the preparation of the Strategy, including the AONB Management Plan, and numerous walking and cycling guides.
Paragraph 1.4 - 1.9	Andy Smith	786		<p>Without a clear Implementation Plan, and more detailed consideration of potential funding streams it is hard to see how at least those proposals within the existing urban area can be achieved in anything approaching a desirable time scale.</p> <p>Conversely, many of the proposals will represent significant changes to the transport infrastructure and the local environment, so subject to various regulations and / or may require planning consent. We seek firm assurances in the eventual published Strategy, that while it remains a relevant consideration in respect any specific scheme, all firm proposals for implementation must remain fully subject to normal consultation procedures under various relevant legislation including transport and planning regulations, National (NPPF), Local and</p>	<p>The implementation of the recommendations is an important facet of the project and a section detailing the likely methods of implementation has been created. As part of the Strategy we will be working collaboratively with Suffolk County Council in prioritising recommendations.</p> <p>This document provides recommendations and doesn't negate the need to undertake all appropriate assessments and procedures.</p>

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				Neighbourhood Plans, environmental assessments where appropriate and others.	
Paragraph 1.4 - 1.9	David Adelson	418	Yes	Nothing is gained by a strategy that doesn't have the funding or even the commitment to deliver it. I understand there may be sources of funding from UK government and possibly others. It's actually a key part of the strategy to say how the proposals are to be funded. This should be included in this section.	It is recognised that the funding and implementation is an important part of the project. A section of the Strategy is included relating to implementation and further work will be undertaken with SCC following adoption to implement the recommendations.
Paragraph 1.4 - 1.9	Felixstowe Town Council (Ash Tadjrishi)	910		<p>No mention is made of the SCC Review of 2014/15, of which details were submitted by FTC - and similarly, few if any have been implemented.</p> <p>We suggest that without an Implementation Plan, and more detailed consideration of potential funding streams, this unfortunate situation is unlikely to be resolved to any scale.</p> <p>Conversely, many of the proposals will represent significant changes to the transport infrastructure and the local environment, so subject to various regulations and / or may require planning consent. We seek firm assurances in the eventual published Strategy, that while it remains a relevant consideration in respect any specific scheme, all firm proposals for implementation will remain fully subject to normal consultation procedures under various relevant legislation including transport and planning regulations, National (NPPF), Local and Neighbourhood Plans, environmental assessments where appropriate and others.</p>	<p>ESC has worked extensively with SCC during the production of the Strategy. SCC are part of the steering group and shared cycling and walking infrastructure plans (LCWIP) with us.</p> <p>It is recognised that funding and implementation is an important part of the project. A section of the Strategy is included relating to implementation and further work will be undertaken with SCC following adoption to implement the recommendations.</p> <p>This document provides recommendations and doesn't negate the need to undertake all appropriate assessments and procedures.</p>
Paragraph 1.4 - 1.9	Isobel Fleming	81	No	If funding is available it should be used to promote other forms of exercise, such as maintaining and	Throughout the preparation of the Strategy we have looked to identify improvements to the PROW network

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				improving the bridleway network across the county. Where there is major building in every town and village bridleways will be even more important to keep horses off the roads and safely on bridleways,RUPPS and Byways.	as well as the public highway for all users. Where appropriate equestrian users were considered in the formation of any new or improved infrastructure. The British Horse Society were a consultee and have been directly engaged with.
Paragraph 1.4 - 1.9	Leiston Together (Helen Greengrass)	847		Overall strategy To consider within it cycle storage – as many people make positive choices to have electric bikes, or invest in expensive bicycles, they are unlikely to make use of them unless there is sufficient safe storage facilities.	Cycle storage has been recommended at various places within the Strategy where appropriate.
Paragraph 1.4 - 1.9	Mr Dunn	714		I believe that any development of cycleways should come with training. It has become apparent that cyclist think all areas that are not road are their exclusive preserve to the detriment of pedestrians. Although the witnessed accidents are still low in numbers the number of near misses couple with excessive industrial language are growing to an unacceptable level. In view of the above I am strongly opposed to any new cycle ways.	Whilst cycling proficiency is important it falls outside the remit of the Strategy which focuses on new infrastructure.
Paragraph 1.4 - 1.9	Nik Bestow	96		<ul style="list-style-type: none"> National Cycle Routes The maps and the scheme make no mention of the National Cycle Routes (specifically National Cycle Route 1 and Regional Cycle Route 41 for my local area). These routes are important to cyclists and should be included on the reference maps and taken into consideration when planning new paths. In some cases it may be appropriate to redirect 	The National Cycle Networks were a consideration in the formation of the Strategy and reference to the NCR/NCN has been included.

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				the National Cycle Routes to take into account the changes you are proposing.	
Paragraph 1.4 - 1.9	Reydon Parish Council (John Roger Cracknell)	185	Yes	Agree	Support noted.
Paragraph 1.4 - 1.9	S Hall	647	No	<p>To implement this strategy a whole new section / focus is needed around “Education, Communications & Promotion”. With a commitment and funding allocation to support this work. I think the strategy could fail. I don't recall this being mentioned so far - apologies if I missed it. But it would need commitment and funding.</p> <p>I doubt that many members of the public and even many local councillors will be aware of the massive change in government policy towards cycling last year. And I doubt they would be aware of the details in the Suffolk-Design-Streets-Guide which puts pedestrians as the first priority and cyclists as the second priority etc. See attached.</p> <p>This is all very helpful for planners to work with and it's great for active, healthy and sustainable transport – in theory. But it is such a step change in policy that in order to put it into practice, I think some really clear and effective communications with the public and with local councillors is needed to enable people to understand these new priorities and the background to them.</p> <p>Unless people have cycled in places such as the Netherlands they may not understand the really exciting and practical possibilities for mass transit by</p>	While education, communication and promotion are important elements in a well-rounded approach to cycling and walking, the purpose of the Strategy is on new and improved cycling and walking infrastructure. If we create safe, coherent, direct, comfortable and attractive cycling and walking infrastructure the public will use it.

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				<p>bike. And they may not know that it is only since the 1970s that the Netherlands has had this policy and practice. The Netherlands transformed its polluted, congested and dangerous road environment (the one we have here in East Suffolk today) into a cycle friendly space in which people actively wanted to cycle and choose to do this – often as their first transport choice.</p> <p>The Netherlands highlights the key role which well-designed infrastructure plays in enabling people to cycle. It is a sheer joy to cycle on well designed cycle routes, getting about quickly and conveniently along safe, direct and attractive cycle routes. And the massive health and wellbeing benefits and feel good factor which come from this are well documented. Good communication of all of this is needed. Another angle for communications is in the context of the climate crisis. Many people want to do something to help in the crisis and cycling rather than driving is one thing that people can consider.</p> <p>Can East Suffolk Council and Suffolk County Council please do some really effective educational and promotional work aimed at the general public and local councillors? It could show the great potential for cycling – and especially for commuter cycling. Perhaps presentations could be made to local councillors and drop-in events organised with/for local residents? It may also help if there were some online resources with UK case studies showing “before” and “after” photos. Examples from the continent would help too. There are some great</p>	

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				<p>online videos of the Netherlands before and after their “cycling revolution” (pun intended ;) The point is that the right infrastructure was transformative in the Netherlands and if the Dutch can do it, so can we! Many people think the Dutch have always had cycling infrastructure and a cycling culture – but they haven’t. It’s worth making that point. And also, before motor vehicles dominated out roads, the UK used to have a very strong cycling culture too. I remember in the 1960s and some of the 1970s thousands of people cycling to and fro work everyday. The humble bicycle provided transport for the masses. There are some great photos online of this cycling culture which could be included in communications, as it might just spark others to remember... and then the idea of mass cycling may feel less “alien” and more achievable.</p> <p>One key point for communications is to raise the profile of cycling as a means of everyday commuter transport e.g. travel to work, for shopping etc. I think there is an educational/knowledge gap around this. All too often people seem to consider cycling as a leisure activity (your paragraph 3.3). A commonly held view is that you might cycle to school/college until you are 17 years old, but then, you will get your driving licence and you will be driving a car to college or work and no longer cycling.</p> <p>On this point - Local Transport Note 1/20 says:</p> <p>“This updated national guidance for highway authorities and designers aims to help cycling</p>	

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				<p>become a form of mass transit in many more places. Cycling must no longer be treated as marginal, or an afterthought. It must not be seen as mainly part of the leisure industry, but as a means of everyday transport. It must be placed at the heart of the transport network, with the capital spending, road space and traffic planners' attention befitting that role. The guidance delivers on our commitment to boost design standards and improve safety. It sets out the much higher standards now expected, and describes some of the failings common in the past, which will be strongly discouraged in future".</p> <p>Another point to make in any communications work by ESC is what current travel statistics show. They indicate that fewer young people are choosing to obtain a drivers' licence and to use a car and that this is a significant new trend. The statistics show that it is older age groups who drive the most - and they are often the decision makers! So, I think it would be extremely helpful if East Suffolk Council could engage the public (and councillors) with good communication on these statistics and trends so that everyone is aware of them.</p> <p>One final suggestion for communications is to develop/deliver Smarter Choices projects. I worked on the Ipswich TravelSmart Individualised Travel Marketing (ITM) project back in 2010 – a multi-stakeholder project that targeted 17,000 local households with information about smarter travel choices.</p>	

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				<p>The aim of the TravelSmart project in Ipswich was:</p> <p>“to promote greater use of sustainable and active travel modes (walking, cycling and public transport) as alternatives to car travel among a target population of 17,000 households”.</p> <p>A 71% response rate meant that the project was successful in engaging with some 12,000 residents. It was successful in, (amongst other things), reducing car trips (with single driver) by 11% and increasing cycling trips by 55%.</p> <p>Smarter Choice projects could be developed with schools, workplaces and towns in East Suffolk.</p> <p>Smarter Choice projects are specifically referred to in the ISPA Transport Mitigation Strategy prepared by SCC in 2019 Suffolk Coastal Local Plan which you reference in your draft Strategy.</p> <p>Attachments: https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455267/DOCX/-/11775701%201%20Suffolk%20Design%20Streets%20Guide%20Priorities%2Edocx</p>	
Paragraph 1.4 - 1.9	Suffolk Coast & Heaths and Dedham Vale AONBs (Simon Amstutz)	382		<p>other funding and delivery opportunities could include AONB grants (eg Sustainable Development Fund, Amenity and Accessibility Fund, Farming in Protected Landscapes programme (that includes an access element) and working in Partnership with Sustrans and the AONB team. Building a broader partnership would allow a wider debate and ideas to</p>	<p>Working with partners and seeking funding opportunities is an important aspect of the Strategy and we welcome the identification of the AONB's funding opportunities. Reference to funding and delivery options at the local and national level is referenced within the Strategy as an important element of implementation.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				bring to bar in meeting the ambition. RAMs may also be a consideration in funding opportunities	
Paragraph 1.4 - 1.9	Timandra Howell	268	No		Objection noted.
Paragraph 1.4 - 1.9	Ufford Parish Council (Judi Hallett)	726		<p>Introduction to Consultation Response</p> <p>East Suffolk Council(ESC) published their Cycling and Walking Strategy and this is out for consultation with a deadline for comments of Monday 10th January 2022. Ufford Parish Council did not respond formally to the earlier ESC/ Suffolk County Council (SCC) consultation but several individual residents flagged issues in and around the village and beyond, such as the poor state of footpaths alongside major and minor roads, inadequate signage and the possibilities of increasing both walking and cycling usage if improved infrastructure could be developed.</p> <p>Sections 1 and 2: Overall Strategy</p> <p>Ufford Parish Council is supportive of the concept and the direction of travel within the Cycling & Walking Strategy. There is a clear need to improve and extend the infrastructure within East Suffolk to provide cyclists and walkers with safer access to our towns, villages and the countryside. However, within the proposals there seems too have been insufficient thought given to providing an integrated network with safe cycling and walking routes. There appears to be still too much reliance on using busy major and minor roads where cyclists and walkers are at risk or</p>	<p>Whilst the importance of maintenance to the cycle and walking network is recognised this Strategy focuses upon providing new infrastructure.</p> <p>The Strategy looks to identify improvements not only to the Highway network but also the PROW network. It is recognised that many roads lack the space to provide new infrastructure however there are various opportunities to improve the cycling and walking experience without requiring more space i.e. modal filter. The highway network offers opportunities in relation to delivery given it is in public ownership.</p> <p>In reference to the remaining points. No comment has been discounted and each was given due attention and scored appropriately. These community recommendations form part of the strategy.</p> <p>Urban areas represent higher potential for modal shift benefiting a larger population. However, we have sought to incorporate rural areas on the edges of the key corridors and identified a number of leisure routes throughout East Suffolk.</p> <p>IM23 has been amended to incorporate a better cycling connection between Melton and Ufford.</p> <p>Taking a 'joined-up' approach has been an important part of the Strategy with SCC and PROW representatives</p>

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				<p>are discouraged due to the volume of car and lorry traffic. More specifically:-</p> <ul style="list-style-type: none"> • Some of our busy minor roads and lanes are too narrow to allow two cars to pass each other; hence safe cycling is Equally, where the footpaths are overgrown or too narrow then walkers are at risk walking on the highway or are discouraged from trying. From the map- based consultation, several of these routes around Ufford and Melton flagged as “needing improvement” have been discounted. • In the Key Corridors criteria, insufficient attention appears to have been given to “countryside corridors” which are used already by cyclists but are not The Strava Metro data used to assess issues flagged in the initial consultation appears flawed as it gives insufficient to the rural aspects. • There seems to be a lack of “joined up” thinking between Planning, Highways, Public Rights of Way and related ESC needs to work harder to build a coordinated approach across Departments and with Suffolk County Council. • Little attention has been paid to the potential impact of Sizewell C, to the proposed duelling of the A12 around Woodbridge or the proposed duelling around A12 villages resulting from EDF’s plans. 	<p>part of the steering group.</p> <p>Consideration has been given to both the impacts and associated infrastructure improvements of Sizewell C and the A12 works.</p>

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Paragraph 1.4 - 1.9	Ufford Parish Council (Judi Hallett)	735		<p>Missing Issues - Rights of Way Improvement Plan (ROWIP)</p> <p>This Suffolk County Council (SCC) initiative is mentioned with the ESC document but is given scant attention in the Recommendations. As part of SCC's Green Access Strategy, it is acknowledged and should align with the ESC Cycling and Walking Strategy actions.</p> <p>The ROWIP is already being implemented by SCC and Ufford is involved as a couple of important walking trails (notably the Rendlesham and Deben Valley Trail) pass through the village. We are in touch with SCC to add to their data base, for example by adding the award-winning Ufford Heritage Trail, to the emerging network. It is suggested that ESC give greater credence to the ROWIP in its final strategy.</p>	<p>The Green Access Strategy was considered in the preparation of the Strategy and PROW officers are part of the steering group.</p> <p>IM23 has been amended to incorporate a better cycling connection between Melton and Ufford.</p>
Paragraph 2.1	Andy Smith	787		<p>Many useful relevant documents are quoted. However, notably, not the National or regional Cycle Routes or the National Coastal Path and Local Walking paths. Both should be:</p> <p>(i) recognised as part of context, with links needed to and from</p> <p>(ii) Certainly shown of all the maps for clarity. See more detail in Note A</p> <p>Proposals for major development of underground or multi-storey car parks are inappropriate for inclusion in this context. They should be removed.</p>	<p>The National Cycle Network was referenced in the list of evidence sources used to create the recommendations and a website address is included. The Local Walking Paths is assumed to mean the Public Rights of Way Network which again is listed as an evidence source and is available to view as a layer on our interactive map.</p> <p>The National Coastal Path/England Coast Path is referenced in individual sections where applicable.</p>
Paragraph 2.1	Felixstowe Town Council (Ash Tadjrishi)	911		<p>Many useful relevant documents are quoted. However, notably, not the National or regional Cycle Routes or the National Coastal Path and Local</p>	<p>Throughout the preparation of the Strategy consideration has been given to the National Cycle Network and the England Coast Path. However, these</p>

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				<p>Walking paths. Both should be: (i) recognised as part of context, with links needed to and from (ii) Certainly shown of all the maps for clarity. See more detail in Note A</p> <p>We would question whether the Strategy should be accompanied, as with most Planning or Transport Policy document, should be accompanied by an Environmental, Impact Assessment.</p> <p>The draft Strategy contains two proposals for major development of underground or multi-storey car parks. We suggest strongly that such significant aspects are beyond the scope of “Cycling and Walking” and are inappropriate for inclusion in this context. They should be removed.</p>	<p>are not documents and so are not set out in the Policy Context. The Strategy has been subject to Strategic Environmental Assessment screening, and there is no need for the Strategy to be subject to Environmental Impact Assessment (EIA). If the implementation of recommendations within the Strategy triggers the need for EIA this will need to be undertaken as the recommendation is taken forward to delivery.</p>
Paragraph 2.1	Margaret Rogers	30	Yes	<p>It is essential that the rural routes are inspected and maintained regularly by SCC, ensuring there are no potholes and that verges are cut back sufficiently to enable view of oncoming traffic. Tractors and buses use the rural routes as well, those tractors used in my area often fill the entire width of the road and there is no where for anyone else to go as verges are higher than the road. Rural roads are only narrow lanes.</p> <p>Unfortunately this year due to heavy rain the re-growth of weeds and grass in the verges is a hazard to cyclists and walkers alike.</p> <p>It would appear that SCC have no plans to re-cut verges due to lack of funding, not being on their</p>	<p>The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure.</p>

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				<p>schedule or just because we are rural areas, this also applies to pot holes and holes at the sides of roads. There are also roads in rural areas that have not been cut and just left. SCC have no funds (This is according to the media) God help any cyclist that meets a tractor or even hits a pot hole.</p> <p>When WDC (Waveney Norse) used to cut verges it was done twice during the summer months if it was necessary.</p>	
Paragraph 2.1	Peter Kerridge	403	No	<p>You claim to be complying with the strategic priority to mitigate human impact on the environment but are doing the exact opposite by making all your walkways and cycle paths tarmac or other unnatural surfaces. If we wanted to live in a tarmac environment we would move to a town. All you are doing is ruining the special natural environment and as to the putting in of street lights, this just beggars belief when these are actually being turned off at night by your own Highways Department - in order to reduce carbon emissions.</p> <p>Not everyone wants to walk or cycle on tarmac there are plenty of roads and footpaths for that already without destroying the natural environment. Just look at your own photographs which presumably you have included as exemplar to try and justify these proposals. The opening photo shows an horrendous swathe of tarmac looking like a throw back to the 1970s but the most telling part is that there is absolutely nobody using it. Then there is a photograph of a plethora of huge signs showing a cycle route - do you really think these don't impact</p>	<p>The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p> <p>Whilst cycling on-road may be suitable for some cyclists it won't meet the needs of all users. Safe and attractive routes that are appropriately segregated from vehicles will encourage more cycling and reduce vehicular trips.</p>

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				<p>on the environment. What a waste of money and terrible visual impact.</p> <p>You will probably want to justify putting tarmac everywhere by claiming it is more disabled friendly but it isn't as simple as that as people with fallen arches and plantar fasciitis find walking on a natural surface much easier.</p>	
Paragraph 2.1	Trimley St Mary Parish Council (Debra Cooper)	290	Yes	<p>There is some excellent work contained within the Suffolk Design Streets Guide and the ES Cycling & Walking Strategy would benefit from directly evidencing this guidance. For example, the Design Guide 'promotes and encourages the designer to explore and develop designs that respond to user needs and circumstances'. Yet, in the strategy, it is hard to find references to recommendations based on user needs. Journeying to the High School is a key user need that must be properly considered in the Cycling & Walking Strategy.</p> <p>From the Suffolk Design Street Guide: "Designers need to consider not only movement and place but also the variety of users. This requires deeper consideration to the proper functions of the street to consider users 'vulnerabilities". The Strategy would benefit from evidencing this consideration in its approach to scoring. For example, 1000+ pupils need to travel to and from the High School every weekday. This is a primary function of the High Road. School children: users. The Strategy should do more to directly address these movement considerations.</p>	<p>The Suffolk Design Streets Guide is referenced in our policy context and the Strategy draws from the principles of the Suffolk Design Streets Guide throughout its recommendations. Particularly with reference to the user hierarchy whereby pedestrians and cyclists are prioritised over other transport modes.</p> <p>The exact design of the infrastructure will be considered at the design stage.</p>

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Paragraph 2.2 to 2.7	Felixstowe Town Council (Ash Tadjrishi)	952		<p>National and Local Coastal pedestrian Paths.</p> <p>Designated and signed long distance walking paths are an increasingly important element in encouraging walking for promoting tourism, for leisure and for health.</p> <p>It follows therefore that any Cycling and Walking Strategy, at County, District or local level should have as a core element the recognition, promotion and mapping of relevant paths, and a Strategy Objective to expedite their use both in their own right and for local routes to link to and use them wherever that can be achieved.</p> <p>In Felixstowe (and the whole Orwell North bank) we have:</p> <ol style="list-style-type: none"> 1) The long standing Stour and Orwell Estuaries Walk. 2) The Suffolk Coast Path 3) And in the final stages of Parliamentary approval, 2 sections of the National Coast Path Shotley Gate to Felixstowe Ferry Felixstowe Ferry to Bawdsey <p>Further details of the National Coast Path can be obtained from Natural England</p> <p>We strongly request that these are classified and mapped as part of the ESC Strategy, and that the final Strategy expedites and overtly recognises those, including local signage.</p> <p>Relevant proposals include, but certainly not limited</p>	<p>Throughout the preparation of the Strategy we have considered the England Coast Path, Public Rights of Way and the National Cycle Network amongst other evidence sources set out in the Strategy.</p> <p>The importance of cycling and walking for tourism, leisure and health is recognised within the Strategy and for this reason a number of cycling and walking leisure routes have been set out within the Strategy.</p> <p>The points raised under individual recommendations have been addressed under the relevant individual reference.</p>

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				<p>to those below, however we have not individually detailed the many seafront links to the Suffolk Coast Path and the National Coast Path</p> <p>Proposals F8 F9 F40 F95 F113 Objectives 2 Policy Context</p>	
Paragraph 2.2 to 2.7	Felixstowe Town Council (Ash Tadjrishi)	955		<p>National Cycle Routes 51 and 41 are important, signed and widely used within and visiting to or through Felixstowe. They can be viewed at any scale on the National Cycle Routes map here , and more information can be obtained from Sustrans https://www.sustrans.org.uk/find-a-route-on-the-national-cycle-network/route-51</p>	The National Cycle Network and it has been considered throughout the preparation of the Strategy.
Paragraph 2.8 to 2.11	Jonathan Clyne	387	No	<p>The LCWIP states that it relies on statistics for 2020 and boasts of a 76.9% increase in cycle trips during this period. In March 2020 the first Covid-19 lockdown occurred and, as has been widely reported, as part of their daily exercise people took cycle trips or walks which they ordinarily would not have done. The statistics relied on are therefore misleading and are not representative of the cycle trips and walks that would take place under normal circumstances. No where in the voluminous documentation have I seen (so far) it acknowledged that the data relied upon is not representative of any year in which Covid-19 restrictions did not</p>	<p>The data used in the Suffolk County LCWIP was just one source of data used in the production of the Cycling and Walking Strategy and has not been exclusively relied upon.</p> <p>It is recognised that Covid19 will have an impact on some data, but this does not negate the need for improved infrastructure.</p>

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				apply. Perhaps it should, and it certainly would be more honest.	
Paragraph 2.8 to 2.11	S Hall	627	No	<p>2) Policy Context – County</p> <p>These are all really helpful documents, thank you for including them. But I think a More Joined Up Approach Would Be Helpful</p> <p>For the Walking & Cycling Strategy to be successful it would help if ESC specified policy support for additional measures and collaborations, such as a commitment to:</p> <ul style="list-style-type: none"> • The future maintenance of cycling infrastructure (please see my comments under 1.1 • Allocating funding to support new cycle training for all age groups – not everyone has the confidence or skills to cycle. • Supporting initiatives that help people buy bikes, recycle bikes that are not needed, and initiatives that help people keep their bikes repaired and regularly serviced and in good condition etc. • Review its spatial policies to reduce the need to travel by motor vehicle and review its car parking policy and pricing mechanisms as part of this. A carrot/stick approach is needed if modal shift is to happen. If car parking is cheap and plentiful where is the incentive to cycle! • Support for secure, undercover, well supervised cycle parking where it is needed 	<p>Whilst cycle training, bike repair and maintenance, and car parking policies are important elements of a successful cycling and walking environment, they lie beyond the scope of the Strategy, which is to make recommendations in relation to cycling and walking infrastructure. Cycle parking and storage is included within recommendations across the Strategy, however the detailed design of recommendations will need to consider the need, exact location and design of cycle parking and storage. The Strategy has been prepared with the needs of all users in mind, including the wide variety of cycles on offer.</p>

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				<p>and managed cycle parks/repair/service hubs.</p> <ul style="list-style-type: none"> • Promote the benefits of cycling for public health and long term savings in the health budget, perhaps mentioning the Suffolk Health and Wellbeing Strategy etc which recognises the role of walking and cycling in improving health and wellbeing. • Considering the role of pedal powered/electrically assisted taxis and cargo bikes can play locally. I attach a photo from many years ago of a Pedicab we used as a taxi service to raise money for charity. These vehicles and other similar ones are used elsewhere and it would be great if ESC could support these kind of initiatives locally. And local routes / infrastructure need to be designed to accommodate them. A case in point is that I use a bicycle trailer but find that it will not fit on cycle paths due to their narrowness and the presence of bollards and other obstructions. Cycle paths need to be wide enough with sufficient turning radii for the different types of cycles and trailers people use. • Considering the role that other kinds of cycle such as "wheelchair bikes" can play in opening up cycling opportunities to a wider audience. I attach photos as examples. Sorry I can't find anything better at the moment. But these cycles are such fun and I have really enjoyed "piloting" them with 	

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				<p>others. The joy they can bring to others is huge! And local routes / infrastructure need to be designed to accommodate them.</p> <ul style="list-style-type: none"> The Strategy would benefit from a more joined up approach and wider collaborations. Cycling doesn't happen in a vacuum. And it encompasses a wide range of different pedal powered vehicles which need to be taken into account. <p>Attachments: Trev Amy and Dave - <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p> <p>Trev and Matthew - <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455207/PJP/-/11775061%203%20pedicab%20rear%20view%2Ejpg</p>	
Paragraph 2.12 to 2.18	Felixstowe Town Council (Ash Tadjrishi)	953		<p>National and Local Coastal pedestrian Paths.</p> <p>Designated and signed long distance walking paths are an increasingly important element in encouraging walking for promoting tourism, for leisure and for health.</p>	<p>Throughout the preparation of the Strategy we have considered the England Coast Path, Public Rights of Way and the National Cycle Network amongst other evidence sources set out in the Strategy.</p> <p>The importance of cycling and walking for tourism, leisure and health is recognised within the Strategy and</p>

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				<p>It follows therefore that any Cycling and Walking Strategy, at County, District or local level should have as a core element the recognition, promotion and mapping of relevant paths, and a Strategy Objective to expedite their use both in their own right and for local routes to link to and use them wherever that can be achieved.</p> <p>In Felixstowe (and the whole Orwell North bank) we have:</p> <ol style="list-style-type: none"> 1) The long standing Stour and Orwell Estuaries Walk. 2) The Suffolk Coast Path 3) And in the final stages of Parliamentary approval, 2 sections of the National Coast Path Shotley Gate to Felixstowe Ferry Felixstowe Ferry to Bawdsey <p>Further details of the National Coast Path can be obtained from Natural England</p> <p>Further details of the National Coast Path can be obtained from Natural England</p> <p>We strongly request that these are classified and mapped as part of the ESC Strategy, and that the final Strategy expedites and overtly recognises those, including local signage.</p> <p>Relevant proposals include, but certainly not limited to those below, however we have not individually detailed the many seafront links to the Suffolk Coast Path and the National Coast Path</p>	<p>for this reason a number of cycling and walking leisure routes have been set out within the Strategy.</p> <p>The points raised under individual recommendations have been addressed under the relevant individual reference.</p>

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				Proposals F8 F9 F40 F95 F113 Objectives 2 Policy Context	
Paragraph 2.12 to 2.18	Felixstowe Town Council (Ash Tadjrishi)	954		Designated Local Cycle routes in Felixstowe are on the Visit Felixstowe website . In the time available we have not been able to fully cross reference these, but strongly suggest that this should be done, and the routes integrated wherever possible in the final Strategy.	The National Cycle Network and existing cycling and walking infrastructure in East Suffolk have been used amongst other data sources in the preparation of the Strategy.
Paragraph 2.12 to 2.18	Leo Borwick	27	Yes	A key element of context that appears to be missing is coordination with neighbouring districts, particularly Ipswich to the south and west and Great Yarmouth to the north. As recognised in the scoring criteria, developments are only likely to be useful and effective if they provide coherent and connected routes to desired destinations. A second potentially valuable, though informal, source of context is the Slow Ways network, which provides links between centres of population for walkers. See beta.slowways.org.uk .	Neighbouring authorities have been engaged through the preparation of the draft Strategy both before and after the formal consultation. The Slow Ways network is an important collection of routes, which have been arrived at through community engagement. The Slow Ways network has therefore been considered as part of the preparation of the Strategy.
Paragraph 2.12 to 2.18	Suffolk Coast & Heaths and Dedham Vale AONBs (Simon Amstutz)	384		The AONB Management Plan should also be a consideration	The AONB documents have been considered in the preparation of the Strategy and reference to the AONB Management Plan as well as the AONB Walking Cycling Guides have been added to the policy context.

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Paragraph 2.19	Martlesham Parish Council (Diane Linsley)	877		<p>Martlesham Parish Council (welcomes the Draft Walking and Cycling strategy, and in line with the Martlesham Neighbourhood Plan (MAR 13, supports proposals to improve cycling and walking in the parish. MPC particularly endorses the key points set out in paragraphs 2.19 and 3.17 of the consultation document. MPC would like to thank Anthony Taylor (Senior Planner, Policy and Delivery ESC), for meeting with representatives from MPC prior to submission of this response.</p> <p>MAR13 of the adopted MNP, states that proposals to improve cycling and walking will be supported, and that provision of cycle and pedestrian routes which are separated from vehicular traffic, and from one another will be supported. Furthermore, the MNP states that such proposals should seek to ensure that they link up with existing cycling and walking networks and contribute to the formation of a more comprehensive integrated network across the NP area where possible.</p> <p>Proposals to address the following priority routes will be strongly supported:</p> <p>Additional crossings of the A12 Access to Kesgrave High School Access to Woodbridge along Sandy Lane Access to the Recreation Ground</p> <p>Since the adoption of the MNP, Martlesham Parish Council (MPC) has, alongside East Suffolk Council,</p>	<p>The Strategy seeks to identify recommendations for the introduction of segregated cycling and walking infrastructure that form part of a coherent cycling and walking network, in alignment with the Martlesham Neighbourhood Plan. The importance of avoiding harm to the natural environment is recognised and recommendations have been identified and amended to avoid such harm. The National Cycle Network has been an important element of our evidence base in the identification of infrastructure improvements. Comments relating to individual recommendations have been addressed within the specific sections for those recommendations.</p>

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				<p>declared a climate emergency, so is now also of the view, that any new proposals should focus on improvements to existing routes where possible, and avoid impact on woodland, nature reserves and heathland SSI. Improved transport sustainability at the expense of the avoidable loss of these environmentally valuable areas would be counterproductive.</p> <p>A review of existing unclassified roads within the parish which are already part of the National Cycle Network, to include traffic speed limits and flooding problems, would also be helpful, an example being NCN 41, frequently impassable following heavy rainfall and often dangerous on foot.</p> <p>Furthermore, MPC is of the opinion that the draft strategy does not go far enough to address the very real problems for local residents walking to and around the retail area. The MNP household survey highlighted that the Beardmore Park retail area was both difficult and dangerous for cyclists, pedestrians and disabled users. This area has been a source of complaint from residents and other users for many years since the retail park expansion. The author of the draft strategy is therefore invited to tour that area with us to get a better understanding of the problems and dangers that the current arrangements create.</p> <p>At present, too many short journeys to and from the</p>	

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				<p>retail park are made by car because of the lack of safe walking and cycling provision in this area. Such improvements should be a key objective of this strategy.</p> <p>Much of the open space in Martlesham is managed by Martlesham Parish Council, Martlesham Heath Householders Ltd* (MHHL) and a range of voluntary groups as nature reserves, formally designated or otherwise, sites of special scientific interest or wildlife friendly areas and corridors. Therefore, a sensitivity towards the protection and enhancement of these areas must be shown. Our proposals to moderate the IM10 route and eliminate the need for IM12 reflect this.</p> <p><i>* MHHL is a not for profit company (run by volunteers) which owns or leases (on 999 year terms) most of the open space in Martlesham Heath including the SSSI. Householders are, by deed of covenant, required to be shareholders in the company and pay an annual fee towards the land maintenance cost.</i></p> <p>Overall Constraints</p> <p>Martlesham is a significant and strategic parish for the longer journeys under consideration, due to its comprehensive employment, leisure and retail facilities. Following completion of Brightwell Lakes (BL), the proposed development at the Police</p>	

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				<p>Headquarters, and other significant infrastructure projects currently under consideration, the current retail and leisure facilities are set to become more popular, even if no significant modal shift takes place. Therefore, longer range routes will be busy in both directions at peak times, unlike the unidirectional tidal flow normally associated with commuters. Furthermore, traffic to and from Brightwell Lakes and commuting to the industrial estate and BT from Ipswich, need special consideration.</p> <p>Superimposed upon this, is the need to facilitate shorter journeys within the parish, including to BL and Kesgrave High School. Much of this local traffic at peak times will be associated with primary and secondary schools. Therefore, as far as possible, these should be segregated from the longer distance traffic to minimise conflict between these types of users.</p> <p>In view of the climate emergency, this strategy should focus more on the needs of residents who are keen to make short journeys on foot or to cycle safely, but who are fearful of doing so because of lack of safe provision.</p>	

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				<p>Principles behind the Parish Council's Position</p> <p>The best way of achieving ESC's strategy within these constraints is to identify three distinct types of route dependent upon their main purpose.</p> <ul style="list-style-type: none"> • Routes for longer distance, primarily commuting traffic, highlighted in particular a direct EXPRESS route crossing the A12 at the proposed T junction (see map below [see attached pdf]) would be a major benefit to BT commuters and Brightwell Lakes residents alike. See our comments against IM12 and IM25. • Local routes principally within Martlesham used for specific journeys, highlighted green • Other routes as shown (NB proposed routes IM12 and IM10 east of the Tesco underpass do not exist and are opposed - see later) <p><i>The images have not been published due to potential data protection concerns, but these were still fully considered and assessed in forming the Strategy.</i></p>	
Paragraph 2.19	Reydon Parish Council (John Roger Cracknell)	186	Yes	<p>Reydon has a 'made' Neighbourhood Plan and a Footpaths and Cycleways Group looking to link shorter and longer Footpaths to provide an integrated network for residents and visitors.</p> <p>The Parish Council is also mindful of the safety issues that face walkers and cyclists and is seeking to address these hazards. Some additional funding will be needed to make these improvements.</p>	<p>Support noted. The Council welcomes local groups engaging in cycling and walking in the preparation of neighbourhood plans.</p> <p>ESC likewise considers safety a key priority.</p>

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Paragraph 2.19	Stephen Denton	477		<p>The Martlesham Neighbourhood Plan is generally not in favour of shared pedestrian/cycle routes based on practical experience.</p> <p>See pages 42 - 44 of the NP.</p> <p>At busy times this type of shared route creates problems and frustration for both types of users.</p> <p>It is worth noting that in the NP a survey showed that 55% of those using cycle paths would not use a marked cycle lane on the highway. This is a serious consideration as there quite a few of these - eg Gloster Road, old Felixstowe Road and the A1214 through Martlesham. See photo of Gloster Road.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	<p>Shared paths are one of many types of cycling and walking infrastructure. In most cases segregation between users (vehicles, cyclists and pedestrians) will be the most appropriate solution. However, within existing, constrained streets occasional compromises will need to be made and shared paths may be the most appropriate solution. That said, throughout the preparation of the Strategy the highest standards of cycling and walking infrastructure have been sought, in accordance with LTN1/20.</p>
Paragraph 3.1 to 3.5	Andy Bird	118		<p>General:</p> <p>This shows a progressive approach to making use of under utilised infrastructure - upgrading key routes to bridleways. These are critical positive changes to provide safe active travel routes.</p> <p>Stakeholders:</p> <ul style="list-style-type: none"> No clear cycling stakeholder for the area. 	<p>Support noted. Cycling stakeholders had the opportunity to comment at both the initial and formal consultation stages. Parish and Town Councils, and Community Partnerships were engaged throughout the consultations. SCC PROW team were on the steering group and provided valuable insight into PROW matters. Implementation of the Strategy is of fundamental importance and available funding and delivery opportunities will be explored.</p>

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				<p>Delivery:</p> <ul style="list-style-type: none"> • PROW upgrade can happen now - start process. Any path surface upgrade is secondary 	
Paragraph 3.1 to 3.5	Andy Smith	788		<p>The concept of key corridors is very much welcomed, and those identified in Felixstowe and Trimleys are generally supported, urgently in some cases around new developments currently in the Planning System. However, certain instances of these may not be feasible or appropriate.</p> <p>Also the concepts of the Local Plan Allocations as a core target for improved C&W is very welcome, again urgent in our local context.</p> <p>C&W aspirations cannot be addressed in isolation, and main core routes for all traffic supporting the everyday life of the community must not be prejudiced. This should be one of the criteria for evaluation of all proposals.</p>	Support noted. Detailed design will need to be considered as recommendations are taken forward to delivery. Throughout the preparation of the Strategy the appropriate user hierarchy has been followed, which prioritises pedestrians and cyclists, whilst also realising the needs for private cars.
Paragraph 3.1 to 3.5	Arthur Stansfield	580	No	<p>The corridors are very limited concentrating on the south and north of East Suffolk. Little is offered for the band from Halesworth to Southwold or from Framlingham to the coast. Villages and towns in these areas have seen recent significant population growth.</p> <p>Corridors linking to stations on the East Suffolk line and improving cycle carrying capacity of the trains would offer increased leisure and commuting access.</p>	<p>While the Key Corridors focus on the more urban areas of East Suffolk, a number of Leisure Routes throughout East Suffolk have been identified in response to consultation comments. Such Leisure Routes include routes between Framlingham and Wickham Market Railway Station, as well as between Halesworth and Southwold.</p> <p>Throughout the preparation of the Strategy we have sought to link the railway stations to the recommendations. Whilst the importance of cycle carrying capacity on trains is recognised this sits outside</p>

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				<p>The potential for cycling across the whole of East Suffolk shown in this document lacks ambition. Do any of the authors cycle? Is there a steering group that includes cyclists?</p> <p>The fixing of potholes and other damage to roads reflects poorly on the councils support of cycling. What may be insignificant for a car can be extremely dangerous for a cycle.</p>	<p>the scope of the Strategy.</p> <p>The Strategy seeks to set out an ambitious set of recommendations through both the urban and rural parts of East Suffolk.</p> <p>Maintenance of roads is a matter for SCC as the Highways Authority and falls outside the scope of the Strategy.</p>
Paragraph 3.1 to 3.5	British Horse Society (Lynda Warth)	667		<p>See attached.</p> <p>Attachments: https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/583126/PDF/-/British%20Horse%20Society.pdf <i>(Some of the images have not been published due to potential data protection concerns, but these were still fully considered and assessed in forming the Strategy.)</i></p>	<p>While the primary purpose of the Strategy is to aid the creation of safe and attractive cycling and walking environments, it is recognised that this should not be achieved at the expense of equestrian users. Throughout the preparation of the Strategy, and in response to your comments, the role that equestrian users play across East Suffolk and the need to accommodate such users is recognised, particularly on the PROW network. Comments addressing specific recommendations have been responded to in the relevant sections.</p>
Paragraph 3.1 to 3.5	Broads Authority (Natalie Beal)	157		<p>Para 3.1, 3.8 - and the Broads Authority</p> <p>We could incorporate the relevant parts into our Integrated Access Strategy and Local Plans. We could endorse this strategy and use it as well. Please get in touch when you are finalising this to discuss our comments and our endorsement and use. We would need the GIS files as well.</p> <p>When it comes to access through the Broads we should always be pushing for LTN 1/20 as the minimum standard rather than as “best practice”.</p>	<p>In the spirit of cooperation between Local Authorities, ESC would support the endorsement of the Strategy by the Broads Authority in so far as it is relevant to the Broads Executive Area. High quality cycling and walking infrastructure that takes account of local context is supported.</p>

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Paragraph 3.1 to 3.5	Campsea Ashe PC (Richard Fernley)	190	Yes	Leisure route would be particularly useful. Campsea Ashe PC have been trying to achieve this in the adoption of Quiet Roads following recent initiatives.	Support for Leisure Routes is noted. Throughout the preparation of the Strategy Quiet Lanes have been used as an important evidence base in establishing recommendations.
Paragraph 3.1 to 3.5	David Tricker	179	Yes	<p>I'm a 59 year old cyclist who has always cycled to work my whole working life. I would describe myself as a Utility cyclist.</p> <p>No Cycling Route from Rendlesham to anywhere. It's still cut off with the only direct route to the south along the busy road through Eyke. I cycle from Upper Melton to Rendlesham daily (Via Ufford) so know how unpleasant and at times dangerous this road is to cycle. I consider this a priority.</p> <p>None of the routes shown are direct. We do not all cycle for just leisure, often we want to get somewhere quickly and directly. None of the routes shown achieve this.</p> <p>The existing Cycle path along the Main Kesgrave road is not fit for purpose due to poor surface and the fact that it crosses numerous roads with no right of way. This does not make for efficient pleasant cycling</p> <p>Route IM24 Route shown is through some very boggy ground.</p> <p>Route IM1 very difficult to find. Currently not suitable for road bikes.</p> <p>Route IM19 CemeteryLane/Fen walk is Very narrow,</p>	<p>A number cycling and walking infrastructure recommendations are made in the Rendlesham area, particularly in relation to the site allocations. The Strategy seeks to provide a number of cycling and walking routes throughout East Suffolk that meet the needs of both commuter and leisure cyclists and walkers.</p> <p>IM5 recommends the existing cycling and walking track is continued over Main Road side streets to ensure that cyclists and pedestrians have priority over vehicles. IM24 has been removed from the Strategy as it would require a significant area of vegetation clearance. IM19 has been removed from the Strategy owing to the difficulty with introducing comprehensive cycling and walking infrastructure along Old Barracks Road and Meadow Walk. IM20 seeks to avoid conflict between cyclists and pedestrians, and recognises the primacy of pedestrians in the Thoroughfare. Strava Metro provides useful data to help understand the current state of walking and cycling across East Suffolk. Strava provides a valuable source of information, however there are limitations to the data on its own. Therefore other data sources have also been used, which taken together provide a more comprehensive picture of the current state of cycling and walking in a given location.</p>

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				<p>cannot see how this could be made suitable for cycles.</p> <p>Route IM20. Route through thoroughfare very difficult due to large numbers of pedestrians often unaware of bikes.</p> <p>Route from Melton Station to Wilford Bridge is very dangerous due to narrow road and blind corners on which cars will often try to pass cyclists.</p> <p>I'm not convinced of the use of Strava data. Strava will tend to be used by Sports cyclists not Utility Cyclists. The Strava data could give a skewed view of where people want to cycle.</p>	
Paragraph 3.1 to 3.5	Felixstowe Town Council (Ash Tadjrishi)	912		<p>The concept of key corridors is very much welcomed, and those identified in Felixstowe and Trimleys are generally supported, urgently in some cases around new developments currently in the Planning process. However, certain instances of these may not be feasible or appropriate, which we have detailed in our comments.</p> <p>Equally, the concepts of the Local Plan Allocations as a core target for improved Cycling & Walking is very welcome, again urgent in our local context.</p> <p>However, we note that under the heading of "Leisure Routes", only the circular route in the Saxmundham/ Snape / Aldeburgh area is identified. Given that Felixstowe offers a unique potential for a leisure route utilising the ferry connections between Bawdsey and Harwich, we request that this</p>	<p>Support noted. Detailed design will need to be considered as recommendations are taken forward to delivery. Throughout the preparation of the Strategy the appropriate user hierarchy has been followed, which prioritises pedestrians and cyclists, whilst also realising the needs for private cars.</p> <p>A Leisure Route between Melton and Bawdsey has been included in the Strategy, in response to comments received through the formal consultation on the draft Strategy. The Felixstowe Internal Key Corridor includes recommendations for cycling and walking infrastructure between Felixstowe Ferry and the Port of Felixstowe.</p>

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				<p>opportunity be considered within the Strategy as part of an additional Leisure Route. To support this, we have made specific comments to some of the relevant routes, both inland and coastal, in our consultation response.</p> <p>We would also suggest that Cycling & Walking aspirations cannot be addressed in isolation, and main core routes for all traffic supporting the everyday life of the community must not be prejudiced. This should be one of the criteria for evaluation of all proposals. See certain detail comments.</p>	
Paragraph 3.1 to 3.5	Fiona Powell	412	No	<p>I am deeply concerned that this document mentions equestrian use - horse riders and carriage drivers - once in 44 pages, and that Strava is being used as a source of data about usage.</p> <p>Given that no tracking app is used by everyone, I think it needs to be recognised that Strava excludes equestrians. This means that equestrians who do record their trips use different apps, which do not feed into Strava, or they use, typically, cycling, running or walking to record their activity in Strava.</p> <p>What I want is a genuinely shareable series of safe off-road routes for all non-motorised users - including disabled users, for example, and carriage drivers, who are normally only allowed on byways.</p> <p>The cycling community is 75% male and men cycle four times the mileage of women. Equestrians are about 85% female and carriage drivers include older people, families and disabled, many former riders</p>	<p>Whilst the Strategy focusses on cycling and walking infrastructure, equestrian users have been considered throughout the preparation of the Strategy, with particular regard in respect of the PROW network.</p> <p>Strava provides a valuable source of information, however there are limitations to the data on its own. For this reason, a number of sources of evidence have been used in order to provide a fuller picture of current and potential future cycling, walking and equestrian use across East Suffolk.</p> <p>The demographic imbalance in cycling is recognised, which strengthens the case for safe cycling and walking infrastructure across East Suffolk.</p>

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				<p>who want to drive safely away from traffic. Women also report feeling safer with a horse than alone on a bike or walking. I believe about 96% cycling is for leisure. Please see CyclingUK and BHS for statistics.</p> <p>Given that the equestrian industry (excluding racing) contributes almost £5bn to the British economy, mainly benefiting rural areas, it seems obvious that equestrian access to PRoW is of major importance, for East Suffolk's economy, tourism and for the health and safety of women who choose horses as their 'exercise'. See the British Equestrian Trade Association 2019 survey: Sport England.</p> <p>I attach a picture of my carriage driving pony, and of the Strava heatmap showing where I exercise my pony (recorded as a cycle) in the arena, below centre.</p> <p>Attachments: https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454115/PJP/-/11763765%201%2025704DED%2D6CDC%2D4C78%2D8FFA%2DE729E77E5721%2Ejpeg</p> <p>Strava heatmap - <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
Paragraph 3.1 to 3.5	Halesworth Neighbourhood Plan Steering	130	Yes	There is a need to connect market towns. In particular a route from Halesworth up to Beccles, Bungay and Southwold would support the	A number of Leisure Routes throughout East Suffolk have been identified in response to consultation comments. Such Leisure Routes include routes between

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	Group (Joyce Moseley)			economies of the towns as well as provide recreational routes.	Halesworth and Beccles thereby connecting into the Lowestoft to Bungay Key Corridor, and Halesworth and Southwold.
Paragraph 3.1 to 3.5	James Winterbotham	497		There is a huge gap between the Aldeburgh/Leiston corridor and the Kessingland/Lowestoft corridor. This needs to be addressed as the roads are narrow and suffer increasing traffic as new housing is built.	A number of Leisure Routes throughout East Suffolk have been identified in response to consultation comments, which include areas between Aldeburgh, Leiston, and the Lowestoft to Kessingland Key Corridor.
Paragraph 3.1 to 3.5	Leo Borwick	29	Yes	<p>Whilst the main recommendations are all welcome, there appears to me to be a missed opportunity to address issues arising from through traffic and poor access for active travel through residential neighborhoods. There is a range of interventions such as modal filters, school streets, 20mph speed limits and so on that can help this, but perhaps more important is to apply these in a coordinated way to make a real difference to a neighbourhood.</p> <p>Whilst most UK examples are in urban areas, such as "mini Holland" initiatives in such places as Waltham Forest and Hackney, it seems to me that there is a great opportunity to something similar in smaller settlements.</p> <p>In the context of your earlier consultation, I suggested Rendlesham as a promising candidate and the specific interventions suggested by others in that area would be a good starting point.</p> <p>I suspect that there would be a considerable appetite for similar initiatives in historic market towns like Woodbridge and Framingham, where recent car-oriented peripheral developments have created increased traffic pressure.</p>	<p>Throughout the preparation of the Strategy a variety of cycling and walking infrastructure types have been recommended across East Suffolk. While the Strategy makes specific cycling and walking infrastructure recommendations there are alternative types of infrastructure that could be used to create the same cycling and walking outcome, such as greater use of modal filters to create Low Traffic Neighbourhoods.</p> <p>Outside the Strategy, although related to it, SCC are preparing a 'mini Holland' feasibility study for Woodbridge.</p> <p>It is acknowledged that cycling and walking infrastructure improvements have great potential to generate modal shift, improve public realm, and support high streets.</p>

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				It should be borne in mind that the best opportunities for modal shift lie with short journies and these are most likely to be facilitated by a systematic improvement of a local area, so that people can confidently and safely opt for active travel for a wide range of local journeys. Systematic improvements can also lead to visible improvements to the public realm that encourage better footfall for local businesses, attract visitors and act as a desirable exemplar for other communities.	
Paragraph 3.1 to 3.5	Marlesford Parish Council (Richard Cooper)	401	Yes	Marlesford PC supports the creation of corridors which provide longer connections between and within urban areas and we also support the creation of imaginative recreational routes such as that proposed for Yoxford/Aldeburgh/Benhall areas.	Support noted. A number of Leisure Routes throughout East Suffolk have been identified in response to consultation comments.
Paragraph 3.1 to 3.5	Marlesford Parish Council (Richard Cooper)	901		<p>1. Summary</p> <ul style="list-style-type: none"> • Marlesford Parish Council (MPC) is arguing for a new combined pedestrian and cycleway between Marlesford and the Sizewell C (SZC) Southern Park and Ride at Hacheston. The new path would connect to the proposed path from Wickham Market to the Southern Park and Ride. • We ask for East Suffolk Council (ESC) to support the provision of a new pedestrian and cycleway by including it in their emerging Cycling and Walking Strategy. • Rural communities lag behind their counterparts in urban areas in having easy access to safe active travel facilities. We ask ESC to recognise this and address a real need for this rural community. • The existing path is unsafe for use by pedestrians 	Community Recommendations 305 and 459 seek the introduction of a cycling and walking track between Marlesford Road and the B1116. The provision of this track is supported and incorporated into the Strategy in the form of Community Recommendations. Transport East's Active Travel Strategy represents a good piece of evidence that has been taken into account through the preparation of the Strategy and has been added to the policy context section of the Strategy.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>and unusable by cyclists. We therefore argue for the new path on the grounds of improving the safety of vulnerable road users.</p> <ul style="list-style-type: none"> • We argue that the potential increase in traffic resulting from the cumulative impact of SZC and energy developments in the Friston area will make the provision of a new path essential and its funding should come from the energy project developers. • We require the path to be appropriately surfaced to allow its use by wheelchair users. • MPC wants to promote the health benefits of active travel and we also recognise that the provision of a new path will contribute to fewer car journeys. <p>2. Introduction</p> <p>MPC welcomes the opportunity to comment on ESC’s Draft Cycling and Walking Strategy 2021 and supports the broad principle behind enhancing access to active travel opportunities with the aim of improving health and wellbeing and reducing car journeys. We have provided some comments in the on-line consultation document (see Appendix 2), but this submission should be taken as our comprehensive consultation response.</p> <p>We will argue below for a new pedestrian and cycleway to be constructed between Marlesford (Bell Lane) and the entrance to the proposed SZC Southern Park and Ride at Hacheston.</p> <p>Marlesford lies 2.7 miles northeast of Wickham</p>	

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				<p>Market which is Marlesford’s nearest service centre and provides supermarket, post office, doctors’ surgery, library and other amenities all of which are regularly used by our residents. The A12 dual carriage bypass of Wickham Market ends to the southwest of Marlesford at which point the A12 becomes single carriageway. The A12 severs Marlesford village with part of the community situated to the north of the A12 and part to the south.</p> <p>There is an existing path (which is incomplete) which runs from Marlesford towards the Fiveways roundabout (junction of B1116 and B1078) – the path is on the northwest side of the A12, adjacent to the carriageway. The path is not continuous, is narrow, overgrown and so close to the carriageway that it is considered to be dangerous.</p> <p>Because of a revetment which would prevent widening of the existing path, MPC is arguing that the only option is for a new pedestrian and cycleway to be created on the northwest side of the existing path behind the existing hedge. The land required for the creation of the path is outside Highways ownership and the Parish is about to enter into talks with the landowners in order to secure commitments to provide the land required for the new path.</p> <p>Figure 1. Route of Proposed Pedestrian and Cycleway from Marlesford to Southern Park and Ride.</p>	

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				<p>Red route shows part of the proposed EDF-funded pedestrian and cycleway from Wickham Market to the entrance to the proposed Southern Park and Ride.</p> <p>Yellow Route shows the new pedestrian and cycleway from Marlesford (Bell Lane) to the entrance to the proposed Southern Park and Ride.</p> <p>Photographs of the existing path are included at Appendix 1.</p> <p>3. Policy</p> <p>We note the weight of policy at national, county and district level designed to encourage active travel and produce a modal shift from car usage. We are encouraged to note Policy SCLP7.1 in the District’s local plan which states “Development proposals should be designed from the outset to incorporate measures that will encourage people to travel using non-car modes to access home, school, employment, services and facilities.” We would ask ESC to support the provision of a new pedestrian and cycleway as part of the mitigation measures to be delivered by EDF in Marlesford and Little Glemham.</p> <p>We also welcome Transport East’s report on Active Travel Strategy which was published at the end of October 2021. The Parish Council is particularly encouraged by Transport East’s statement that it “has ambitions to propel Active Travel across the</p>	

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				<p>region; extending the bold vision described in Gear Change (UK Government's vision for walking and cycling) beyond urban areas to people in market towns, coastal communities and local neighbourhoods." We endorse the principle of ensuring that local neighbourhoods and particularly rural communities, are as well served by pedestrian and cycling infrastructure as the communities in urban areas.</p> <p>We note that many of the policy statements recognise the health benefits (as well as the reduction in car journeys) that result from access to well-integrated active travel facilities. The health benefits of active travel are recognised in Suffolk County Council's (SCC) Suffolk Local Transport Plan 2011-2031 Part 2 - Implementation Plan, but we lament the fact that so much emphasis is put on urban schemes and not enough focus is directed towards rural active travel.</p> <p>4. Current Situation</p> <p>As stated in the Introduction above, Marlesford currently has a narrow and incomplete path running from just west of Marlesford Road in Marlesford, southwest towards the Fiveways roundabout and Wickham Market. We argue that because of the narrowness of the path, it is dangerous and therefore not fit for purpose. This appears to be recognised by ESC in its response to Community Recommendations from its Initial Cycling and Walking Strategy Consultation (October – December</p>	

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				<p>2020). Councillor Dr Roger Waterfall made recommendations which have been numbered 305 and 459 in the current consultation document. Against both items (under the safety heading) ESC states “This section of the A12 is a busy ‘A’ type road with a national speed limit and appears to be often utilised by HGVs. Despite this section of the A12 having an existing small footway adjoining the NW side, removing cyclists and pedestrians away from the road will have considerable safety benefit. A score of 3 is deemed reasonable.” We entirely endorse this view as one of our major concerns about the current path is its safety.</p> <p>The A12 (particularly the single carriageway stretch through Marlesford) is not safe for cycling and the existing path is too narrow and close to the carriageway to allow use by cyclists. In addition it is overgrown.</p> <p>We have stories of parents pushing pushchairs along the existing path in order to get to Wickham Market. This is extremely dangerous as the path is so close to the carriageway that the air displacement from passing HGVs and buses could lead to a serious accident.</p> <p>Both ESC and SCC acknowledge the problem in their Local Impact Report (LIR) to the Planning Inspectorate regarding SZC where they say at Para 17.33 “Marlesford has been identified by the Councils as a location of particular concern with regard to increased severance, fear and anxiety of vulnerable road users and reduced amenity.”</p>	

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				<p>5. Impact of Nationally Important Infrastructure Projects</p> <p>In May 2020 NNB Generation Company (SZC) Limited submitted its Development Consent Order (DCO) application to the Planning Inspectorate. MPC fully engaged with the examination process and has consistently argued, in its various representations, the need for improved cycle and pedestrian connectivity between the village and Wickham Market. There is a need now, but it will become even greater if the SZC project goes ahead as the project will introduce up to an extra 1,000 HGV and bus movements per day on the A12 through Marlesford. The Marlesford submissions which cover active travel can be found in the PINS Document Library under the following references: [REP2-365] [REP5-237] [REP5-238] [REP6-065] [REP7-207] [REP10-333]</p> <p>We are hugely disappointed that EDF has not been required to include a new pedestrian and cycleway between Marlesford and the Southern Park and Ride. They argue that it would not be proportionate for them to provide such a path and even if they were minded to create a new path, they regard the required land being in private ownership as an obstacle. We completely disagree with EDF on these</p>	

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				<p>issues and argue that the level of fear and intimidation currently experienced by Marlesford residents as a result of traffic on the A12 (let alone the adverse impacts from the SZC and other energy project traffic in the future) justifies the provision of a path, and the fact that land is in private ownership should not be seen as an insurmountable barrier to delivery.</p> <p>At Annex L of the Draft Deed of Obligation (Construction Worker Travel Plan) at Para. 4.2.6 the Deed refers to “a Wickham Market Scheme to fund pedestrian, cycle and public realm improvements in Wickham Market; and a Little Glemham and Marlesford Scheme to fund improvements for vulnerable road users in Little Glemham and Marlesford;” We welcome the A12 mitigation measures already agreed with EDF for Marlesford (and Little Glemham) but argue that they do not go far enough as they don’t include the requested pedestrian and cycleway.</p> <p>We are also unsure that EDF has enshrined in the Deed of Obligation (DoO) their commitment to a Cycle Connectivity Fund, as requested several times by SCC.</p> <p>The Joint LIR (PINS Library Ref [REP1-045] submitted to PINS by ESC and SCC, is supportive of active travel measures as both mitigation for adverse impacts caused by increases in traffic resulting from the cumulative impact of SZC and other energy projects, and as legacy benefits once the energy projects have</p>	

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				<p>been completed. In addition to the comment quoted from Para. 17.33 under Current Situation above, the same paragraph goes on to say “The Change submission assessment considers that during the peak years amenity reduces as a result of the changes. Where it increases the impact on severance, mitigation is proposed in the form of a shared footway/cycleway thus reducing the effect on severance to not significant. The Councils will expect this mitigation to be incorporated in revised work plans.”</p> <p>But in response to the Councils, EDF said “In Marlesford the effect on fear and intimidation increases, however the transport assessment addendum [AS-266] considers the overall effect to be minor adverse - not significant”. ESC and SCC do not agree with this assessment - as noted in the transport section (paragraph 15.103) [of the LIR]. Marlesford has been identified by the Councils as a “location of particular concern with regard to increased severance, fear and anxiety of vulnerable road users and reduced amenity.” We agree with this assessment by the local authorities but are very disappointed that the recognition of the adverse impacts that will be experienced by Marlesford have, as yet, not resulted in a commitment from EDF towards the proposed pedestrian and cycleway.</p> <p>It is clear from 18g in Table 18 of the LIR that the Councils intended that EDF would provide cycling and pedestrian infrastructure for Marlesford as well as Wickham Market in order to connect with the</p>	

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				<p>Southern Park and Ride. At 18g the Councils say that they require “Improvements of footway and cycling infrastructure linking the site to Wickham Market and Marlesford for [access to] the Southern Park and Ride, if provided (by which we understand the Councils to mean “if constructed”). Under the same line in the table, the Councils go on to say that these facilities should be “secured by obligation / through DCO plans”. In the case of Wickham Market, it is believed that these plans are in hand, but in the case of Marlesford the pedestrian and cycle infrastructure envisaged by the Councils for our village have not been incorporated in the Draft DCO or the Draft DoO. It is not too late to hold EDF to account on this issue and greater weight could be given to the argument in favour of a new path if ESC supported its provision within the emerging Cycling and Walking Strategy.</p> <p>A further safety concern for vulnerable road users, if SZC and the Scottish Power Renewables projects go ahead simultaneously, is the prospect of rat-running. There is a real fear that some drivers will avoid using the A12 because of the traffic on it and the difficulty of turning onto the A12 from minor roads. This anticipated added traffic on rural roads (often single track) will add to the safety issues faced by pedestrians and cyclists. This is particularly true for Marlesford cyclists who currently use the minor roads in order to get to Wickham Market.</p>	

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				<p>6. Proposal</p> <p>In meetings with EDF and SCC (which ESC also attend) to discuss A12 mitigation measures in Marlesford and Little Glemham, the issue of the pedestrian and cycleway has been regularly discussed, and Marlesford’s views are well known to all parties. EDF argues that it cannot fund the path as it is outside the DCO and DoO. SCC, whilst tacitly supporting the idea, see the issue of the required land being in private ownership (rather than the ownership of SCC Highways) as being problematic. Both EDF and SCC raise concerns about how the proposed path might be funded. This Parish Council believes that all these issues are capable of being overcome if all parties work together to achieve delivery of the proposed path.</p> <p>Our proposal is for a combined pedestrian and cycleway starting at Bell Lane Marlesford and running (in the first instance) to the junction with Marlesford Road (both junctions are shown in Fig. 1 above). The newly created path would be on the north side of the A12 and would be part of the highway improvements already committed to by EDF.</p> <p>A dropped kerb crossing should be provided at Marlesford Road and the path would continue towards the Southern Park and Ride on the northwest side of the existing hedge so that the hedge would provide a separation from the A12 northbound carriageway for cyclists and pedestrians.</p>	

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				<p>The path would link with the proposed path from Wickham Market to the Southern Park and Ride and would therefore provide a continuous active travel route between the two villages.</p> <p>We are advised by SCC Highways that the width of the path should be 3m.</p> <p>We require the path to be properly surfaced to a standard that will allow wheelchair users and those pushing prams and buggies to use the new path safely.</p> <p>MPC is about to open discussions with the landowners who would be required to put land into the project when it goes ahead, but it recognises that it will require technical support from SCC Highways once discussions are under way.</p> <p>We do not see funding as an obstacle, although we don't underestimate the effort that will be required to assemble it. Our preferred option is that EDF should fund the project in its entirety in the way that is anticipated for other "off-site" infrastructure – we believe that it is not too late to put this obligation on EDF. However, if the EDF funding were to fall short of a full 100% coverage, we believe that other "pots" of funding will be available to facilitate the delivery of the scheme, some of which are set out in ESC's Draft Cycling and Walking Strategy 2021 consultation at Para 1.7.</p>	

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				<p>7. Conclusion</p> <p>Whether SZC and other energy projects go ahead or not, MPC argues that the A12 between the village and Wickham Market is too dangerous for use by cyclists and the existing footpath immediately adjacent to the northbound carriageway is too narrow for safe use by pedestrians and contributes to a sense of fear and intimidation for vulnerable users.</p> <p>The development of SZC and the cumulative impact of other energy projects would make a new pedestrian and cycle route between Marlesford and Wickham Market absolutely essential. We regard the provision of a new path, funded by EDF and the other energy projects, as a valuable legacy contribution and a small price to pay for the upheaval that local residents will suffer over the next 10-12 years if the projects go ahead.</p> <p>We urge ESC to include the Marlesford to Southern Park and Ride and Wickham Market to Southern Park and Ride combined pedestrian and cycleways in their schemes which will form part of their emerging Cycling and Walking Strategy.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
Paragraph 3.1 to 3.5	Reydon Parish Council (John Roger Cracknell)	187	Yes	Proposals noted.	Support noted.
Paragraph 3.1 to 3.5	S Hall	631	No	<p>You have defined the following:</p> <p>3.2 Key Corridors - Key routes between, and through, settlements where there are significant opportunities for modal shift to arise from improved cycling and walking infrastructure.</p> <p>3.3 Leisure route - A potential Leisure Route has been identified, connecting a number of more rural settlements and projects.</p> <p>For the sake of clarity, the definition of “Key Corridors” needs to be re-named “Key Commuter Routes”. People would then have a clear understanding that some routes are mainly used by commuters going to work, school, shopping etc and others are used mainly by leisure users out for a Sunday bike ride etc. The route function determines its design (“form follows function”) so this is extremely important.</p> <p>E.g. you wouldn’t want to put a commuter route along an unsealed track with no street lighting because few people would use it in rainy, icy, dark winter evenings cycling home from work. But you may consider designating such a route as a leisure route as it might be well used during the daytime at weekends by mountain bikers, for example.</p> <p>For the sake of clarity I suggest that the following</p>	<p>While the primary purpose of the Key Corridors is to facilitate modal shift in relation to commuting, school and shopping trips, and the primary purpose of Leisure Routes is to provide opportunities for safe and attractive cycling and walking for recreation and tourism, they can and will likely be used for all purposes. That said, it is of fundamental importance that the detailed design of key corridors and leisure routes reflect their primary purposes. LTN1/20 and the Suffolk Design Streets Guide are key supporting documents and are set out in the policy context section of the Strategy. These documents must be read in conjunction with the Strategy.</p>

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				<p>new text is used instead:</p> <p>3.2 Key Commuter Routes - Key routes between, and through, settlements where there are significant opportunities for modal shift to occur at commuter times from improved cycling and walking infrastructure</p> <p>I think it's important for members of the public - and for local councillors – who will probably be the decision makers in all of this - to understand that people do actually use bikes for transport purposes to travel to work etc! And, that they need direct, safe, convenient, continuous and attractive cycle routes to get them there.</p> <p>Unfortunately there seems to be a huge educational/knowledge gap when it comes to cycling. All too often councillors seem to consider cycling as a leisure activity (your category 3.3).</p> <p>And a commonly held view is that you might cycle to school/college until you are 17 years old, but then, you will get your driving licence and you will be driving a car to college or work and no longer cycling.</p> <p>Even worse, a prevalent view seems to be that cycling policy and design for cyclists needs to take bikes off the road and divert people along bumpy unlit bridleways / improved public rights of way across fields. The view is that public rights of way are the right place for cycling. I think this is a</p>	

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				<p>mistaken view in urban areas when we are providing for commuter cycling and modal shift and this view runs directly against government policy which changed significantly in 2020.</p> <p>We need to make a very clear distinction between commuter and leisure cycling so that it is properly understood. A conversation about modal shift can be more meaningful. In practice, if more people choose to cycle to work rather than drive to work this leads to a reduction in peak hour congestion, a reduction in air pollution and a reduction in traffic dangers – and these are all key issues.</p> <p>All too often I hear people trivialising the role of cycling because they think of it only as a leisure activity! They have no idea of the potential that exists for modal shift at peak commuter times and mass transit by bike! This is the experience of cycling on the continent where cycling is often the “first choice” to get to work etc because the good infrastructure to enable this is already in place. We have to get the good infrastructure in place first. That’s exactly what Local Transport Note 1/20 says. But that will never happen until people understand the potential for modal shift to cycling and especially for commuter cycling. There’s a massive role for good communications and education here. (See my comments later on this subject).</p> <p>I’d like to share an extract from Local Transport Note 1/20 to highlight this point. Perhaps this extract can be included in your Strategy: -</p>	

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				<p>“This updated national guidance for highway authorities and designers aims to help cycling become a form of mass transit in many more places. Cycling must no longer be treated as marginal, or an afterthought. It must not be seen as mainly part of the leisure industry, but as a means of everyday transport. It must be placed at the heart of the transport network, with the capital spending, road space and traffic planners’ attention befitting that role. The guidance delivers on our commitment to boost design standards and improve safety. It sets out the much higher standards now expected, and describes some of the failings common in the past, which will be strongly discouraged in future”.</p> <p>It would also help to include the attached extract from Suffolk-Design-Streets-Guide in your Strategy</p> <p>Attachments: https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455224/DOCX/-/11775189%201%20Suffolk%20Design%20Streets%20Guide%20Priorities%2Edocx</p>	
Paragraph 3.1 to 3.5	Stephen David	204		<p>No mention of Woodbridge-Felixstowe corridor</p> <p>No mention of Bungay-Halesworth-Southwold corridor</p> <p>Bus shelter at Woodbridge Turban Centre - a disgrace, and means we do not get access to timetables. It has been like this for some time.</p>	<p>The Strategy includes a cycling and walking route between Woodbridge and Felixstowe, between Halesworth and Beccles, and Halesworth and Southwold, as well as an extension of the Ipswich to Melton Key Corridor to Sutton Hoo.</p> <p>Whilst the importance of multi modal trips is recognised, the Strategy focusses on cycling and walking infrastructure improvements.</p>

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				<p>Something should be done to reduce traffic on Wilford Bridge Road, including having better pedestrian access to Sutton Hoo. Even a ferry from Woodbridge.</p> <p>Better maintenance of footpaths throughout the area, including signage</p> <p>Better bus services - I don't cycle, but I do walk. However, some of these routes are too long for walking both there and back, so a reliable bus in the other direction would be very useful</p> <p>Who in their right mind is going to opt to cycle anywhere on dark and rainy nights in winter, even with good well-lit cycle routes?</p>	
Paragraph 3.1 to 3.5	Suffolk Coast & Heaths and Dedham Vale AONBs (Simon Amstutz)	385		Short circular leisure routes should be considered alongside potential for a longer distance route that are found in several other areas eg Camel Trail in Cornwall	A number of Leisure Routes throughout East Suffolk have been identified in response to consultation comments.
Paragraph 3.1 to 3.5	Trimley St Mary Parish Council (Debra Cooper)	294	Yes	'The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure.' The opportunity along the High Road is not to 'maintain' the existing, limited infrastructure, but to develop new infrastructure that serves traffic to and from key sites - in particular the High School. Traffic along the High Rd is increasing as a result of increasing development on the peninsular. There are many pinch points along this route. Simply building a cycle by-pass from Trimley St Martin to Old Felixstowe will not resolve local traffic along the High Road. In	High Road is a key route for cyclists, pedestrians and motorists and is also subject to significant constraints in what can be achieved by way of cycling and walking infrastructure improvements. For this reason the identification of cycling and walking infrastructure recommendations on High Road has been challenging. However, there are opportunities to improve the route for cyclists and pedestrians, with regard to school access, and these are set out in the Felixstowe Internal Key Corridor.

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				<p>particular, children do not currently feel safe cycling to and from school. This results in lots of unnecessary car journeys taking children to school which could be reduced if the High Road was designed to be safe with these journeys in mind. Not addressing this would be a missed opportunity.</p> <p>There is also concern that pedestrian provision (along the High Road) appears to have minor consideration. There are narrow stretches of pavement and parked vehicles often result in pedestrians having to walk on the carriageway to pass by, which isn't safe.</p> <p>3.20 The view that 'internal routes' are sufficient is too dismissive. Appropriate detail needs to be provided to support the enhancement of these routes to safely support cycle traffic. These 'Internal routes' are already overburdened roads where cars and cyclists already compete for space on the highway, not to mention inadequate pedestrian provision at pinch points along the High Road.</p> <p>Trimley St Mary Parish council have no objection to the route planned that connects Trimley St Martin to the North Felixstowe Garden development but it should be made clear that this will not resolve the issues that exist on the High Road currently and should not take place in lieu of works to improve pedestrian and cycling safety on the High Road. Not least as the High School sits on the High Road and children will always cycle to school along this road from both directions as it will always continue to be</p>	

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				<p>the most direct route. Safe walking and cycling routes to and from the school along the High Road must be secured as a minimum by this initiative. It may not be possible to develop 'perfect' segregation for all highways users (vehicles, cyclists and pedestrians) but these areas are constrained, which only adds to the need to deliver a better solution. Perfect should not be the enemy of progress.</p> <p>The single High School that serves the whole of the Felixstowe peninsula sits on the High Road. Routes to and from the High School need to be properly considered from a user's perspective and this analysis evidenced within this strategy. This seems to be overlooked currently.</p> <p>There is a well-known pinch point on the High Road near the Post Office. The road narrows here, cars frequently park (and pull out) dangerously. For cyclists, in particular, it is a dangerous point on the High Road. It does not make any sense for this section of the High Road to be omitted from the Very High Priority cycle route. Whilst it may be considered an option to design an alternative solution to this section of the road, users will always adopt this because it is the most efficient route.</p>	
Paragraph 3.1 to 3.5	Whitehouse, Robert	26		Felixstowe to Martlesham and Martlesham to Woodbridge should be identified as a Key Corridor	Felixstowe to Martlesham was part of the wider Ipswich-Felixstowe Key Corridor, but has now been identified as a key corridor in its own right. Martlesham to Woodbridge is part of the wider Ipswich-Melton Key Corridor.

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Paragraph 3.1 to 3.5	Woodland Trust (Oliver Newham)	637	No	<p>The Woodland Trust was only notified of this cycle strategy consultation in the past few days. In the time available, we have done a very quick check of some of the proposed routes for possible threats to ancient woods or ancient/veteran trees. But we have not had time to look at all of the suggested routes for new cycleways. I am sure you will appreciate that ancient woods and ancient trees are irreplaceable habitats and para 175c of the National Planning Policy Framework says that they should only be damaged or lost to development in wholly exceptional circumstances.</p> <p>So, whilst we fully recognise the importance of new cycleways in achieving modal shift and helping to tackle the climate emergency, we do not wish to see them created at the expense of irreplaceable habitats. So we would urge you to reconsider the proposed routes and seek to avoid ancient woods and ancient trees, or, failing that, to put in place adequate buffering so as to protect them from damaging impacts.</p>	Throughout the preparation of the Strategy the importance of the natural environment is recognised and recommendations have considered potential impacts on the natural environment, including ancient woods, and ancient/veteran trees.
Paragraph 3.6 to 3.14	Andy Smith	789		<p>The recognition of Ipswich to Felixstowe as a Key Corridor – will be welcomed by many – and should be a prime focus in the early steps of implementation</p> <p>However, the Corridor Felixstowe / Martlesham / Woodbridge is increasingly a major route for Felixstowe residents, many of whom now perceive Martlesham as a much more accessible and attractive retail destination than Ipswich. It should be similarly recognised.</p>	Key Corridors are identified between Ipswich and Felixstowe, as well as Martlesham and Felixstowe.

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Paragraph 3.6 to 3.14	Broads Authority (Natalie Beal)	158		<p>Para 3.1, 3.8 - and the Broads Authority</p> <p>We could incorporate the relevant parts into our Integrated Access Strategy and Local Plans. We could endorse this strategy and use it as well. Please get in touch when you are finalising this to discuss our comments and our endorsement and use. We would need the GIS files as well.</p> <p>When it comes to access through the Broads we should always be pushing for LTN 1/20 as the minimum standard rather than as “best practice”.</p>	In the spirit of cooperation between Local Authorities, ESC would support the endorsement of the Strategy by the Broads Authority in so far as it is relevant to the Broads Executive Area. High quality cycling and walking infrastructure that takes account of local context is supported.
Paragraph 3.6 to 3.14	Colin Maunder	159		<p>I'm sure the Strava data provides a useful insight but the trouble is that it is far from being either complete or representative. I'm a keen cyclist and walker, but I don't use it. I doubt many of those you see cycling to and from local shops do either. Neither will the many teenagers heading to and from schools, friends houses, etc. The likelihood, then, is that this data is biasing your proposals towards lycra-wearing cyclists, near-professional walkers, etc.</p>	Strava provides a valuable source of information, however there are limitations to the data on its own. For this reason, a number of sources of evidence have been used in order to provide a fuller picture of current and potential future cycling, walking and equestrian use across East Suffolk.
Paragraph 3.6 to 3.14	Elaine Everitt	136	No	<p>3.9 Site visits have not always been possible.</p> <p>Consultants, officers and other outsiders cannot possibly understand what is necessary, what is practical, what is damaging to nature and wildlife without site visits at various times of day and on different days.</p> <p>As a resident of Martlesham Heath for 35 years, I have much more valid and valuable knowledge.</p>	Local knowledge from the community is recognised as being of vital importance to the success of the Strategy. For this reason, two consultations have been undertaken which generated approximately 2000 comments. These comments have been considered and recommendations amended accordingly.

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Paragraph 3.6 to 3.14	Felixstowe Town Council (Ash Tadjrishi)	913		We greatly welcome the recognition of Ipswich to Felixstowe as a Key Corridor - this is already much used, but in need of significant improvements to make it less challenging, safer and therefore more attractive. There is a large potential for increased usage if those difficult elements can be achieved. However, we would suggest also that the Corridor Felixstowe / Martlesham / Woodbridge should be similarly recognised, not least given the increasing role of Martlesham as a retail and employment centre, indeed linking directly to the Ipswich / Melton proposal. This is in addition to, and stands beside, the need for Leisure access, probably over more rural locations, serving the coast and estuaries as a leisure resource of ever growing popularity and scale. That access appears to be given insufficient recognition in the proposed Strategy.	Key Corridors are identified between Ipswich and Felixstowe, as well as Martlesham and Felixstowe.
Paragraph 3.6 to 3.14	S Hall	636	Yes	Very pleased that Ipswich to Melton is identified as a key corridor. See earlier comment about "Key corridor" definition - suggest this should instead be defined as 3.2 Key Commuter Routes - Key routes between, and through, settlements where there are significant opportunities for modal shift to occur at commuter times from improved cycling and walking infrastructure	The language of the Key Corridors has been retained as they function as cycling and walking routes for commuters as well as to and from services and facilities.
Paragraph 3.6 to 3.14	Water Management Alliance (Ellen Moore)	54		Our ref: 21_05540_P Thank you for your email. As you may be aware areas of East Suffolk are partially within the Internal Drainage District (IDD) of the East Suffolk Internal Drainage Board (IDB) and the Waveney, Lower Yare	Comment noted. As recommendations are taken forward to delivery the impact on and design of drainage features will need to be considered.

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				<p>and Lothingland IDB. Please see our website (https://www.wlma.org.uk/uploads/ESIDB_Index_plan.pdf and https://wma-idb.maps.arcgis.com/apps/webappviewer/index.html?id=98e25174c8404ed28dbe0a0aed77700d) for detailed mapping of each Board's District. These maps also show which watercourses have been designated as 'Adopted Watercourses' by the Board. The adoption of a watercourse is an acknowledgement by the Board that the watercourse is of arterial importance to the IDD and as such will normally receive maintenance from the IDB. This maintenance is not necessarily carried out on an annual basis but on a recurrence deemed necessary to meet water level management requirements. Please be aware that the designations are made under permissive powers (meaning there is no obligation for IDBs to fulfil any formal maintenance requirement and there is no change in the ownership or liability associated with the watercourse).</p> <p>We note that the key corridors identified may coincide with both of the above mentioned Boards' IDDs, therefore we have the following comments to make in relation to the draft Sustainable Construction Supplementary Planning Document, draft Affordable Housing Supplementary Planning Document and draft Cycling and Walking Strategy. In order to avoid the potential for future conflict between any future projects and the Board's</p>	

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				<p>regulatory regime and consenting process please be aware of the following.</p> <ul style="list-style-type: none"> • For any development site within the Board’s Internal Drainage District (IDD), the Board’s byelaws apply. The Byelaws for the Board are available on the development pages of our website (https://www.wlma.org.uk/uploads/WMA_Planning_and_Byelaw_Policy.pdf). Specifically please be aware of the following byelaws: <ul style="list-style-type: none"> ○ If a surface water (or treated foul water) discharge is proposed to a watercourse within an IDD (either directly or indirectly), then the proposed development will require a land drainage consent in line with the Board’s byelaw 3. Any consent granted will likely be conditional, pending the payment a surface water development contribution fee, calculated in line with the Board’s charging policy. ○ If the proposals include works within 9m of a Board adopted watercourse, consent is required under byelaw 10. Byelaw 10 restricts works within 9 metres of drainage or flood risk infrastructure (including adopted watercourses), the principle aim being to ensure watercourses can be maintained by the Board now and in the future without restrictions being placed on the Boards access, and to ensure operatives 	

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				<p>are aware of third party structures when undertaking maintenance.</p> <ul style="list-style-type: none"> • If proposals include works to alter a watercourse (including culverting for access) then Land Drainage Consent is required under Section 23 of the Land Drainage Act 1991. If inside the IDD then the IDB would be the consenting authority. If outside the IDD, then Suffolk County Council (Lead Local Flood Authority) would be the consenting authority. • We support the view that all new developments should incorporate water efficiency and water re-use measures to reduce water use, and that all major residential developments should incorporate sustainable water management measures such as SUDs. <p>I hope the above is useful, may I request that the WMA is consulted on any future proposals within our Boards' IDD's so that we can provide more tailored, site specific responses.</p>	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Andrew and Simone Moore	855	No	<p>I have read with interest your proposed strategy and have some questions and objections regarding the impact on the village of Martlesham Heath.</p> <p>The Village was designed as a self contained new village with paths linking all the open spaces owned by the householders. The few cycle paths which were originally incorporated were never designed</p>	<p>IM12 has been amended to remove reference to a formal cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This</p>

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				<p>for transient use but to facilitate safe and convenient movement for residents of the village.</p> <p>The Path and Cycle path which cuts through Broomfield is one such and is bearily 6ft / 2mtrs wide. It is ever increasingly used as a cut through by cyclists to access BT and the industrial areas surrounding it. The cyclists generally have scant regard for life or limb, cycling at pace with no warning along this narrow pathway and in the view of many residents and ourselves this combination is an accident waiting to happen.</p> <p>General Your proposals to open up further tarmac and lit paths crossing the heath which is widely used for recreation and well-being is not helpful.</p> <p>New access across Martlesham Heath land will encourage cyclists to take short cuts through SSSI land and the construction of paths through the woods will necessitate the felling of many mature trees, disruption and destruction of habitat.</p> <p>The whole essence of the village will be diminished and the heath and this natural asset damaged.</p> <p>I referred earlier in this letter to plenty of existing tracks - Most of these cycle paths are very used well used but their maintenance is non existent - natural encroachment making them narrow - graffiti vandalism and broken bottles being a feature of these fine assets.</p>	<p>recommendation avoids Martlesham Heath entirely, passing to the south of Martlesham Heath.</p>

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				<p>I fear the same would happen to all the new cycle paths you put in, poor maintenance and the encouragement of transitional traffic where it is unsuitable.</p> <p>Please take this letter as constructive I am far from a NIMBY but I am concerned that these proposals don't even dovetail into the National Cycle Route 1 thereby giving me greater concerns and confirmation that these plans are generally ill thought out.</p>	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Andy Bird	119	Yes	<ul style="list-style-type: none"> • IM1/IM4 - very good safe route • Opposite Bell lane to IM1 is a good route to join up • IM4 does not require resurface - good all year round • IM14-to martlesham - should try to avoid funneling to the IM15 woodbridge road until as late as possible. Brightwell lakes via felixstowe road is safer and joins near martlesham red lion. • IM18 - good safe route 	Support noted. Resurfacing of IM4 has been retained as this will enable more users, including those using wheelchairs and mobility scooters to enjoy the route. Moreover, there remains space along IM4 to retain space for walking and cycling on a natural surface.
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Bridget Burke	89		<p>I am writing in response to IM12, marked as a medium priority on your plan. I want to object in the strongest terms.</p> <p>It cannot be justifiable to destroy our beautiful wood when there are safe , well lit routes already in place. It is ENVIRONMENTAL VANDALISM. In this day and age when we are making commitments to our children and grandchildren to preserve the trees and protect the animals, it's criminal to even think about IM12. It's a simple solution on paper but in reality it's unnecessary.</p>	IM12 has been amended to remove the recommended formal cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath.

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				<p>This route also completely disregards human nature. School children from the new estate will head for the shops before and after school, it's what teenagers do and they will go via the BT boundary path IM13. They will go to Greggs and McColls and use the old bridge to get to school.</p> <p>Families will also head to the bowling, soft play etc all using the IM13 route. IM12 doesn't go anywhere, even the parks are better accessed over the old bridge.</p> <p>Finally, my children love the wood and my grandchildren now use it every week. During lockdown it was our go to place, it's a haven for wildlife and it's rough paths are perfect for children to explore. The area is rich in wildlife, we know a family of foxes have a den right next to the route and deer live in the centre of the wood. Dumping a concert path and adding lighting would ruin the area completely for NO GAIN. Cyclists can use Eagle Way safely to access the existing cycle route.</p> <p>I hope that even if none of these objections touches a nerve, planners will see IM12 will be costly and bring no benefits.</p>	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Chris Adelson	506		Please consider provision for public conveniences along this route.	The Council recognises the need for planned cycling and walking routes to align with existing services and facilities, and hopes that new services and facilities will come forward along the routes, thereby supporting the desirability of using the planned routes.
Ipswich to Melton Key	Christine Kendall	259	No	I must strongly object to the routes IM1,IM2 and IM3 as they would severely adversely affect Rushmere Common. As you should know Rushmere	Recommendations IM1, IM2, and IM3 provide highly desirable cycling and walking routes between Ipswich and areas further east and therefore have been

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Corridor Paragraph 3.15 to 3.19				<p>Common is privately owned and managed by trustees, as a charity, for Rushmere Commoners. Most of the common is used by Rushmere Golf Club too.</p> <p>As you state in the consultation document these paths currently provide 'a safe and desirable walking route' but if the status of these paths were to be changed to cycle routes they would most certainly not be safe for any users. The conflict of interests of walkers, golfers and cyclists would be too great and accidents, and claims for injuries, would be inevitable. At present both walkers and golfers can look out for each other but a cyclist travels at a greater speed and would be unaware that they were cycling in line of play on a golf course!</p> <p>The consultation document also states that 'users would feel safe and comfortable' but this is far from the truth.</p> <p>On top of the conflicts of various groups of people these proposals would harm wildlife and damage a valued area of common land.</p>	retained. Cycling and walking tracks can be designed to avoid conflict between users. In order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Joyce Atkins	562	No	<p>I strongly object to both the routes through Rushmere Common and Martlesham woods as this will destroy the natural look of both areas.</p> <p>Also there would be many safety issues across Rushmere Common as it is also used as a golf course.</p>	Recommendations IM1, IM2, and IM3 provide highly desirable cycling and walking routes between Ipswich and areas further east and therefore have been retained. In order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2. Conflict between users is an important consideration, which is why traffic free cycling and walking routes are so important for the

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					<p>safety of pedestrians, cyclists and those that require use of a wheelchair or mobility scooter.</p> <p>IM12 has been amended to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath.</p>
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Karl Kitchener	592	No	<p>With reference to sections IM12 and IM9, I have several issues in both of these sections as both disregard the environment and current movement for a greener environment. It is totalling baffling why a paved cycle path is proposed to cut through 'Martlesham Woods/Birch Woods destroying trees, endangering wildlife and totalling spoiling a natural habitat. The same argument applies to the route proposed through Portal Woods. When we are all encouraged to plant trees, you want to destroy them, putting in lights that would deter wildlife and even prove dangerous for children using existing routes in these woods travelling to and from school and for leisure walking. There is an existing bridge linking Brightwell to Martlesham Heath, why build a new one saving a minimal distance. If the existing bridge is inadequate, surely a better way would be to upgrade this one. The destroying of existing woodland cannot in my option be justifiable when existing routes could be improved and enhanced with much less environmental impact . Your</p>	<p>IM12 has been amended to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath.</p> <p>IM9 reflects the existing hard surfaced track through Portal Woods and therefore does not propose the removal of any vegetation. Lighting is proposed to only be introduced where strictly necessary.</p>

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				proposals are nothing short of environmental vandalism.	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Lesley Vince	334		The Martlesham Heath neighbourhood is well served by cycle and pedestrian routes which are not overused even at peak times from what I have seen. Additional routes would be at the expense of valuable woodland.	The existing cycling and walking network through Martlesham Heath provides a valuable cycling and walking environment for cyclists and pedestrians. However, improvements can still be made. Throughout the preparation of the Strategy the importance of preserving the natural environment is recognised and amendments have been made to recommendations within the Martlesham Heath area to reflect the need to further support the natural environment.
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Lesley Vince	337	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this area are minimal in comparison to the loss of habitat/habitat changes.</p>	In preparing the draft Strategy, officers considered the benefits to cycling in respect of improved journey times, and an attractive and safe route through the wooded area away from vehicles to outweigh the negative aspects in relation to low level lighting through the wooded area between Eagle Way (west and east), increased human presence in the wooded area harming wildlife, and potential conflicts with pedestrians. Having reflected on consultation responses and considered these matters further, officers have amended IM12 to avoid recommending the wooded area for cycling and instead recommend cycling and walking improvements to Eagle Way. The recommended cycling and walking bridge over the A12 remains part of IM12, but is proposed to be accessed from Eagle Way and not through the wooded area between Eagle Way (west and east).
Ipswich to Melton Key Corridor Paragraph	Lynne Hill	607	No	I oppose the lit tarmac cycle/footpath, including the felling of trees, on land owned and managed by Martlesham Heath Householders Ltd and a Site of Special Scientific Interest. Martlesham Woods are immersive and a place of awe, clam and curiosity. I	IM12 has been amended to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling

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3.15 to 3.19				walk through these woods regularly with friends and family and I vehemently resist any suggestion to disrupt their natural state.	and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath. There are no recommendations that propose introducing infrastructure to the Ipswich Heaths Site of Special Scientific Interest.
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Martlesham Conservation Group (Phil Smith)	893		<p>This response has been produced on behalf of Martlesham Conservation Group and relates to proposed routes within the Martlesham area. Although the group supports greener ways of transport and encouraging people to enjoy nature, as a general comment the Group believes that insufficient attention has been paid to the Council's own policy on the natural environment. We would ask the council to pay particular attention to Policy SCLP10.1: Biodiversity and Geodiversity in the Suffolk Coastal Local Plan.</p> <p>Policy SCLP10.1: Biodiversity and Geodiversity in the Suffolk Coastal Local Plan.</p> <p>"Development will be supported where it can be demonstrated that it maintains, restores or enhances the existing green infrastructure network and positively contributes towards biodiversity and/or geodiversity through the creation of new habitats and green infrastructure and improvement to linkages between habitats, such as wildlife corridors and habitat 'stepping stones'. All development should follow a hierarchy of seeking firstly to avoid impacts, mitigate for impacts so as to make them insignificant for biodiversity, or as a last resort compensate for losses that cannot be avoided</p>	Throughout the preparation of the Strategy consideration have been given to the need to follow the hierarchy of avoiding harm, mitigating harm, or as a last resort compensating for harm.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>or mitigated for. Adherence to the hierarchy should be demonstrated.”</p> <p>The approach adopted by the council seems to have been to pick existing paths and as a result lead to degradation of the ecology of existing wildlife sites and corridors. These sites include an SSSI, County Wildlife sites, Local Nature Reserve and a registered village green.</p> <p>The Council could have chosen to investigate new routes which would provide an opportunity to introduce new wildlife corridors instead of surfacing over existing ones. The approach adopted by the Council seems out of step with the Government’s vision for enhanced biodiversity by pushing more farmland towards wildlife conservation.</p>	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Martlesham Conservation Group (Phil Smith)	900		<p>Alternative Routes to Kesgrave and Ipswich</p> <p>Why has the Council not considered re-routing the existing bridleway marked PROW6, so that it runs next to Welham’s Plantation? It could then be extended to cross Dobbs Lane and continue on the field edge adjacent to Foxhall Road towards Bell Lane, then turn parallel to Bell Lane, again along the field edge, to link with IM4. An extension around the area required for the basic pathway/cycleway could provide a wildlife corridor, which would be beneficial for wildlife. A bridge over the A12 at the eastern end of the existing bridleway could provide access to Brightwell Lakes.</p> <p>As an alternative the Council could consider a route</p>	A new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>that runs adjacent to the Foxhall Road and Bell Lane to link with IM4 as above, but continues at the eastern end to the junction of the Foxhall Road with the A12. A bridge could be provided over the A12 north of the roundabout for access to Brightwell Lakes, IF33 etc. via a new pathway between the roundabout and IM13. An extension around the area required for the basic pathway/cycleway would again provide a wildlife corridor beneficial to wildlife.</p>	
<p>Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19</p>	<p>Martlesham Heath Householders Ltd (Sir/Madam)</p>	<p>284</p>		<p>Martlesham Heath Householders Ltd (MHHL) as Landowners</p> <p>1. When Martlesham Heath Village received planning permission in 1975, it was a condition that a separate company limited by guarantee was set up to manage the open spaces of Martlesham Heath. Some 46 years later, Martlesham Heath Householders Limited is still active in managing the open spaces and other assets, such as the Control Tower, on Martlesham Heath Village. The set-up of Martlesham Heath is unusual, in that the 1400 households of the village are all shareholders of the company and every year pay an annual charge of £35 for the upkeep of our 140 acres of open spaces and other assets. MHHL is run by unpaid Volunteer Directors and Volunteers. The shareholders individually are all part owners of our private land and assets.</p> <p>2. The shareholders on the Heath take great pride in their village and indeed there is great emphasis in protecting the open spaces which includes a site of</p>	<p>Comments noted.</p>

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				<p>special scientific interest (SSSI). MHHL is continually enhancing the landscape of the Heath by planting bulbs, trees and shrubs as well as on-going maintenance. Whilst the open spaces belong to the shareholders, access is allowed to the general public in a wider community spirit.</p>  <p>3. MHHL has an informative website which shows, amongst other things, our land ownership. Home Martlesham Heath Householders Ltd (mhl.co.uk)</p> <p>4. The stance of the Directors of MHHL is that we wish to engage positively with East Suffolk Council (ESC) in their consultation as we acknowledge that if there is money available to improve cycling routes, then our shareholders would benefit.</p> <p>5. If any proposals we as Directors make as part of this consultation and are accepted by ESC, these proposals would have to be ratified at an annual general meeting or an extraordinary general meeting.</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				6. The Directors are encouraging all shareholders to make their own individual comments via the consultation website.	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Martlesham Heath Householders Ltd (Sir/Madam)	285		<p>Overview of Ipswich to Melton Corridor Proposal</p> <p>7. We note the purpose of the strategy is:</p> <p>The East Suffolk Cycling and Walking Strategy identifies potential cycling and walking infrastructure opportunities across the district. The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure. It provides context and information to support detailed infrastructure proposals and inform decision making to support cycling, walking, and equestrian use.</p> <p>8. Without being too cynical, we question why you are not maintaining existing infrastructure at present rather than concentrating on new projects. Below are 3 photographs which show the lack of maintenance on a key cycle footpath near the underpass to the A12.</p>  <p>9. We note that a key feature of this corridor is the conversion of the Long Strops Bridleway into a combined cycle/footpath, which has a tarmac surface. If indeed you wish to support equestrian use, how is this possible if the Bridleway is to be</p>	<p>The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot. The importance of Long Strops Bridleway for equestrians, leisure cycling, walking and dog walking, and other uses is recognised. There is space along Long Strops to introduce cycling and walking infrastructure that meets the needs of disabled users, commuting cyclists and pedestrians, equestrians, as well as retaining the attractive natural setting for leisure cycling, walking and dog walking. Materials have not been prescribed as the most appropriate surfacing solutions will need to be considered as the recommendations are taken forward. The safety of users is of paramount importance, which is why it is often appropriate for cyclists and pedestrians to be segregated between themselves, as well as segregated from vehicles. Detailed design that complies with the Department for Transport's LTN 1/20 guidance will need to be considered as the recommendations are taken forward. Long Strops Bridleway has an advantage over Main Road in that it is wholly without vehicles and as safety is of paramount importance IM4 has been identified as very high priority. The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge would not be necessary in close proximity to an existing cycling and</p>

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				<p>tarmaced? Near Mildenhall in 2013 there was outrage by local horse-riders when a nearby Bridleway was to be tarmaced over. Horses do not like a tarmac surface. In this situation Suffolk County Council reached a compromise with the riders where a grass strip was to be left available for the horses to use. The fundamental question which must be answered by ESC is; will equestrians be able to use the Long Stropps Bridleway? If there is to be an adjoining grass strip to the tarmac, would the existing Bridleway be wide enough?</p> <p>10. The proposals for this corridor show most routes as a combined cycle/footpath. In Holland, the tendency is to keep cycle/footpaths separate. For walkers, tarmac is not necessary and walking on an unmade surface through woods is part of the experience. The Sandlings Walk, as it passes through MHHL land, is largely on natural heathland. We urge ESC not to be fixated on combined cycle/footpaths as speeding cyclists can cause serious injury to pedestrians when a collision occurs.</p> <p>11. The ESC proposals give different priorities to different routes. It is our view, given the expected usage of the routes to be taken by cyclists, especially schoolchildren, is that the Main Road (A1214) IM5/IM8 route should be “very high priority” and the IM4 route should downgraded to “high priority”.</p> <p>MHHL’s position on proposed new routes across our land</p>	<p>walking bridge. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>12. The Board notes that the total distance of MHHL land ESC wish to use for new cycle and footpaths would total 1 mile. We wish to point out that in the past we have been very accommodating with Local Authorities for access to our land. Indeed, we have the long distance Sandlings Path across our land. The route is shown below in blue.</p> <p>13. Besides the Sandlings path, MHHL also has 2 permitted, combined cycle/footpaths across its land. One route follows the perimeter track around the Police HQ and the other is from Birchwood School to the Grove. Issue 1 25/12/2021</p>   <p>14. The Board is happy to meet officers from ESC and walk routes on our land as appropriate. However MHHL is against the imposition of any more tarmac and lit cycle paths on our land. Since the village was built there has been continued pressure on our open</p>	

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				<p>spaces from more recent neighbouring developments and the flora and fauna on our open spaces is deteriorating accordingly.</p> <p>15. The Board has familiarised themselves with the compulsory purchase process but would hope the Council would not attempt to use such a process. Compulsory purchase process and the Criche Down Rules - GOV.UK (www.gov.uk)</p> <p>16. The Board has also familiarised themselves with the 2020 Cycle Infrastructure Design Publication and notes the minimum footpath/cycle path dimensions as quoted in chapter 5. Cycle infrastructure design (LTN 1/20) - GOV.UK (www.gov.uk)</p> <p>17. The Board is also aware of the Brightwell Lakes development and its emphatic view is that the existing bridge over the A12 should be used as the only connection to Martlesham Heath Village. The Board's view is that the existing bridge is perfectly adequate and would easily cope with increased usage from the completed Brightwell Lakes development. By using the existing bridge, damage to our woodlands will be avoided and people will be encouraged to visit the Martlesham Heath village centre thereby providing potential integration between the two communities</p> <p>18. National Cycle Route 1 is already in existence but its route is not integrated into the ESC draft strategy. Route 1 - Sustrans.org.uk</p>	

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				<i>The maps have not been published due to potential copyright concerns, but these were still fully considered and assessed in forming the Strategy.</i>	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Martlesham Sea Wall Group (Thomas O'Brien)	876		<p>Here are my comments for the East Suffolk Cycling and Walking Strategy Draft 2021. Please can you pass onto the team responsible.</p> <p>I am a house holder living in Martlesham Heath. I enjoy walking around about my local countryside and would like to see more paths opened to provide circular walks near my home. Martlesham parish is a growing community which makes the need for local inhabitants to get out in the outdoors just as important as the housing count is to planners. Currently Martlesham Heath and Grange Farm are boxed in. No access to fields to the South, dissected by the A12, and to the north blocked by the Woodbridge Road. All this makes it difficult to access the outside countryside. I don't own a dog myself but within the boxed-in area there are what I call some 'dog hot spots'. More access to the open countryside would mean more places to walk dogs and so take pressure off nearby nature reserves etc. Also I believe our community is short changed and it seems most of our Council rates are spent in the more established towns of Felixstowe, Woodbridge and Lowestoft who have received large sums for sports centres, formal gardens, piers, restoring pavilions etc. (Why not a formal garden like Elmhurst Park in Martlesham parish?) So some investment in better access on foot to the surrounding countryside is only fair recompense.</p>	<p>Access to open space is an important aspect of healthy living and is recognised within the Strategy. The purpose of preparing the cycling and walking infrastructure recommendations is to enable the public to cycle and walk for their day to day needs, including access to open space. IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath. The cycling and walking connections across the A12 in this area are recognised, and this is reflected in recommendations IM25 and IM26. Taken together, these recommendations seek to introduce cycling and walking routes between Martlesham and Ipswich.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>I have attached three maps. Map 1 is copied from the Ramblers Don't Lose Your Way interactive map. It shows possible rights of way that could be reinstated. These routes seem to be derived from old OS maps such as the 1921 edition. (I bought a copy in Ipswich Museum). In particular it seems many rights of way were lost when the Martlesham Heath airfield was created. So the paths were not included in the original definitive map of public footpaths in the 1950s.</p> <p>Map 1 South Martlesham Heath</p> <ol style="list-style-type: none"> 1. A footpath that would enable Grange Farm residents access further south. This path would link up the Long Strops path with the bridal way moving west to east to the south of Martlesham Heath . 2. Here Martlesham Heath residents would have access southwards to the same west to east bridal way. Paths 1 & 2 would help create a circular walk. 3. I agree a foot crossing of the A12 is necessary here. Brightwell Lakes is planned to have its own schools and shops and other amenities which Martlesham Heath residents would enjoy also. A method of easily walking across the A12, especially if a secondary school is built, would be frequently used. Also I am in favour of a made up path across the Birch Woods. But I can understand the annoyance local residents feel about the construction of a lit cycle way across the woods. I walk there often. I enjoy listening to the chiff chaffs, nightingales, and I've heard young sparrow hawks calling from their nest. A lit path would certainly be 	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>an intrusion. Also these woods are definitely a dog hot spot so the inclusion of racing cyclists doesn't mix and is unsafe.</p> <p>4. This points to the foot crossing planned when the new Brightwell Lakes A12 junction is created. It means the west to east bridal way to the south of Martlesham Heath can safely continue across the A12 and create a circular walk. Also note that at present this bridal way is constructed so walkers can cross the A12 on foot. The dual carriageway central barrier has been left open at this point to enable walkers to cross. But as traffic flow is so high it is only safe to cross on foot at night.</p> <p>Map 2 shows the A12 South of the Foxhall Rd roundabout. Here a number of paths were dissected by the construction of the A12.</p> <p>5. An A12 crossing alternative to 4 above should the new A12 Brightwell lakes junction not be created. A pelican foot crossing could be included with the planned traffic lights on the A12/ Foxhall Road junction enabling a circular walk. This crossing will not be heavily used so will not impede traffic significantly.</p> <p>6. Here a number of footpaths were dissected when the A12 was constructed. As in 4 above they were created so that walkers could cross the A12 on foot. Once again central barriers were removed to allow foot access across the A12. But nowadays only safe to cross at night. A foot bridge or tunnel crossing would mean walkers could enjoy the full beauties of this hidden valley. This valley is so picturesque l</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>don't like telling people about it.</p> <p>7. Here is the rather crude and precarious cross road junction between Kennels Road and the busy A12 (see attached picture). This junction was created when the A12 was originally built. At the far end of Kennels Road (east of the A12) are some cottages and a farmstead. These residents face a dangerous task when joining the A12 at this junction with A12 cars travelling at 70mph or more and no long lead in to merge with traffic. So for safety's sake it would be reasonable to propose a single track car bridge here. The bridge would also be accessible to cyclists, walkers and even horses. Especially as Kennels Road leads to the east to a land rover type track to Brightwell in the East and Bucklesham to the South.</p> <p>Map 3 shows the A12 Martlesham bypass north of the Woodbridge Road.</p> <p>8. This points to another place to the north of Martlesham Heath where footpaths have been dissected by the A12 Martlesham Bypass. And again arrangements were made so that walkers of these paths can ascend the steep bank and cross the A12 on foot. A gap was created in the A12 central barrier. A tunnel was built so the stream could pass underneath the A12 but not made big enough for walkers to pass also. A lost opportunity. This is another beautiful valley where I walked often before the bypass was built. Houses are planned to the south east of this valley. So an A12 crossing at point 8 would give nearby residents more access to open countryside.</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>Martlesham Heath and Grange Farm Kesgrave certainly lack large parkland and access to open spaces. For instance, Ipswich has Christchurch Park, Landseer Park, splendid Holywells Park and more. Also I have noticed a rather negative attitude to my part of Suffolk. For example I've heard people in Waldringfield calling us 'the hordes' and 'those on the other side of the A12'. So I would like you to study my proposals and think of Martlesham as an entity where people have as much right to a pleasant environment as those in Waldringfield. As it is, new housing and retail and industrial development is being liberally imposed within Martlesham parish so now this is East Suffolk Council's chance to show it cares about the environment for the inhabitants of Martlesham parish, rather than thinking of our parish simply for meeting housing quotas and at the same time regarding the resulting population as a problem that needs containing.</p> <p><i>The images/maps have not been published due to potential copyright/data protection concerns, but these were still fully considered and assessed in forming the Strategy.</i></p>	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Mary Odam	861		<p>It seems to me that, in seeking to create faster and easier routes for a hypothetical band of cyclists who may wish to travel from Brightwell Lakes to Ipswich, the needs of the current users (children, pedestrians, ramblers, youth groups, wildlife) are being sacrificed and the wellbeing of all users put at risk. There are existing suitable cycle routes, which could do with</p>	<p>The safety and wellbeing of existing and future users is of fundamental importance in the consideration of new or improved cycling and walking infrastructure. While existing cycling infrastructure may be considered suitable by some, this is not the case for all users, some of which seek safer cycling and walking routes away from traffic. Segregation between cyclists and</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>some maintenance, and the money would be better spent in upgrading these.</p> <p>Finally, it is a fact that shared spaces for cyclists and pedestrians are rarely an ideal solution as anyone who has used IM7 in either capacity during the morning rush hour will testify.</p>	<p>pedestrians is sought as the highest standard of infrastructure provision. However, IM7 is one of the few high quality pieces of cycling and walking infrastructure throughout the Ipswich to Melton Key Corridor. Various changes have been made to recommendations between Ipswich and Brightwell Lakes. IM12 has been amended to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath.</p>
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Mike Gilson	644	No	<p>The rushmere sections go over common land. This is object to and can not support.</p>	<p>Recommendations IM1, IM2, and IM3 provide highly desirable cycling and walking routes between Ipswich and areas further east and therefore have been retained. In order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.</p>
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Mr & Mrs D Evans	903		<p>Please find below our comments on the proposals for the Ipswich to Melton Corridor.</p> <p>Whilst we would wish to support any move to increase the safe provision of cycling route in the area we would suggest that some of the proposals in this paper are unnecessary and in some cases detrimental to the environment and against other East Suffolk Council policies namely:-</p> <p>The East Suffolk Environmental Policy revised in 2017 which on the web page states “Financial self-</p>	<p>Throughout the preparation of the Strategy the impact of recommendations on both the natural and historic environment has been considered, and harmful impacts avoided wherever possible. The purpose of the Strategy is to help provide high quality cycling and walking routes in order to give individuals the opportunities to cycle or walk rather than drive, thereby reducing the harmful impact that vehicles have on our natural and historic environment.</p> <p>SCC's Major Road Network A12 proposals have been considered throughout the preparation of the Strategy.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>sufficiency sets out the Council’s commitment to the responsible use of our natural resources, sustainable strategies for council assets, as well as protecting and enhancing our districts [sic] biodiversity.</p> <p>We would suggest that some proposals do not meet the aims of this policy statement.</p> <p>You state that the purpose of this Draft CyclingPolicy is to:-</p> <p>“.....identify potential cycling and walking infrastructure opportunities across the district. The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure. It provides context and information to support detailed infrastructure proposals and inform decision making to support cycling, walking, and equestrian use’.</p> <p>It is not clear from the consultation document whether the plan has been developed in conjunction with the Suffolk County Council (SCC) document on improvements for the A12. This has been out for consultation but as far as we are aware no final strategy has been agreed. Your consultation seems to rely on a bridge (for route IM12) crossing the A12 (known in the SCC consultation as the northern option) yet SCC are consulting on both a bridge and an underpass at this point plus a southern option which does not meet the requirements of your plan and a Pegasus Crossing at the proposed A12 traffic lights which once again does not meet your plan’s requirements . Should SCC decide not to pursue the</p>	<p>The implementation of the Strategy will need to be informed by further evidence gathering and detailed discussions with SCC as the Local Highways Authority. It will also be important to draw upon various funding and delivery opportunities, including SCC proposals.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				northern option will East Suffolk Council fund the provision of this crossings facilitate the introduction of this plan?	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Pauline Kitchener	109	No	Why are you planning to destroy the natural habitat that is Birch woods, not Martlesham woods as you mistakenly describe them in the document. There is adequate provision for cyclists through the village including a serviceable bridge over the A12. The proposed cycle path through the woods could make it dangerous for other woods users ie families, walkers and dogs. Lighting this path would go against advice we are all receiving in limiting light pollution. At this time when we need to be preserving our green spaces, rewilding our gardens and planting more trees to help our dwindling wildlife I feel it is most important to protect these areas from creeping urbanisation.	IM12 has been amended to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath.
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Peter Ross	116	No	As a keen cyclist, walker and a countryside conservation manager of some 38 years I support the general ethos of improving cycling and walking facilities and opportunities. But, this should not be at the expense of damage and loss of natural greenspace and habitat. In particular I am concerned about the IM12 link through The Birch Wood (Martlesham Woods) as owned and managed by the residents of Martlesham Heath, via MHHL of which I am a member. Whilst supporting in principle the provision of a new bridge across the A12 I object to the construction of a new surfaced and lit cycle/footpath through The Birch Wood for 3 reasons.	IM12 has been amended to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath. IM4 has been amended to recommend the provision of a signalised crossing on Dobbs Lane where IM4 and IM10 meet, as well as a village gateway for northbound vehicles, forcing them to slow down before the signalised crossing and entering Kesgrave.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>1. Loss of natural habitat.</p> <p>2. Visual impact of an engineered link.</p> <p>3. Detrimental affect of light pollution . See Sustrans 'Traffic Free Routes and Greenways Design Guide' : Section 10.</p> <p>I question the practical and cost benefits of creating IM 12 as this will only save approximately 300 metres of cycling on Eagle Way, which is a relatively quiet road and a small percentage of the cost of constructing IM12 would be better spent on any necessary traffic calming measures instead. E.g, Speed bumps and digital speed monitors, etc.</p> <p>As a regular walker between Martlesham Heath and Kesgrave, I would draw your attention to the dangerous crossing point where IM 10 meets IM4 at Dobbs Lane. This is already a well used route by walkers and cyclists of all ages and there is an issue here with poor visibility, due to the sharp bend and speeding cars , despite the 30 mph speed limit. A pedestrian crossing and speed bumps at this point are as much needed as they are a few hundred metres north at Gorseland Primary School.</p> <p>I am also concerned about the impact that the upgrade of IM4 would have on the natural environment of both the woodland, the Millenium field and beyond to Bell Lane.</p>	<p>The importance of Long Strops Bridleway along the south of Kesgrave for leisure cycling and walking, and the natural setting to this route is recognised. The significant width of Long Strops Bridleway, between the built edge of Kesgrave and the agricultural land to the south, enables the IM4 recommendation to provide infrastructure for all users, including a hard smooth cycling and walking surface as well as cycling and walking on a dirt track, while retaining the natural setting of the area.</p>

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Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Rachel Smith-Lyte	639	Yes	Good to see you including Melton Road between there and Woodbridge - thats key. Also Melton Station to National Trust Sutton Hoo where there is currently no safe way of transferring between there and Melton station safely, other than in a car, which, given the proximity of each site to the other is inadequate.	The Ipswich to Melton Key Corridor has been amended to incorporate a new recommendation between Melton Railway Station and Sutton Hoo, providing a low traffic cycling and walking route along as much of the route as possible.
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Richard Boother Consultancy Ltd (Richard Boother)	624		<p>Generally,</p> <p>Overall, I support the much-needed improvement to the pedestrian/cycle routes. I make this in context of being a keen cyclist for both leisure and commuting for over 40 years.</p> <p>I note the following</p> <ul style="list-style-type: none"> • Cycling is quickly moving from a leisure and short distance pursuit, to now include a sports element, the use of electric bikes for both pleasure and commuting and electric scooters. These can be difficult to integrate into the footpaths/cycle ways designed 30 years ago. • Also, to be considered will be the growth of elder age groups and person of limited mobility in the use of electric “invalid” transport. <p>See attached map for visual comments</p> <p>IM11</p>	<p>Throughout the preparation of the Strategy attention has been paid to current and future technology enhancements that will make cycling and walking more accessible and desirable, as well as how these enhancements can support those with more limited mobility such as older people.</p> <p>Support for IM11 is noted.</p> <p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p> <p>The proposed alternative IM10 route would have a more harmful environmental impact than the route set out in the Draft Strategy. IM10 therefore has not been amended to align with the suggested route.</p>

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				<p>Overall I support this route, but with the following comments</p> <ol style="list-style-type: none"> 1. The Cycle/Footbridge, need to be either widened or better still to have a pedestrian bridge and a cycle bridge. 2. The “winding through Martlesham centre is a great idea as it will enhance the shopping and other facilities. <p>IM12</p> <p>Overall, I am totally opposed to the following aspects of IM12</p> <ol style="list-style-type: none"> 1. The crossing of Birch Wood (incorrectly noted Martlesham Woods) 2. Is a second bridge required? <ol style="list-style-type: none"> 1. Surely IM13 and IM11 should be designed to be adequate. 2. The travel distance using the proposed IM12 over the distance using IM13 & IM11 is negligible. 3. This would massively reduce the amenity value to the residents and visitors to Martlesham Heath 4. The design of hard surfacing and lighting is anathema to wilding the countryside It is privately owned by the residents and would therefore have to be subject to a compulsory purchase order. 	

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				<p>5. I am not being a NIMBY on this as there are adequate solutions for good design and that sufficient money will not solve.</p> <p>6. The residents of MH will fight this “tooth and nail”.</p> <p>3. I support The upgrading of the section between Valiant Way to the junction with the M10 opposite the entrance to Carlford Close up to the “Tesco roundabout” underpass.</p> <p>4. Do not be tempted to rush ahead with IM12 & IM13 a design it to small, then coming back a few years’ latter saying there is now no option but to go ahead with IM12 as you need the additional capacity.</p> <p>IM10 & IM9</p> <p>I have no comments of IM9</p> <p>My comments for IM 10 relate to the point where it goes from the north of Wingfield and heads roughly southwest.</p> <p>From the north of Wingfield along Eagle Way to Crown Point I have no comments.</p> <p>1. The need to cross t area of outstanding beauty to the point it crosses Dobbs Lane is totally unacceptable.</p>	

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				<p>1. Please refer to my comments above in IM12 which equally apply here.</p> <p>2. IM9 could be extended and “tuck in” tightly against Goresland Schools southern boundary. It could run south down along Dobbs Lane which would need to be upgraded, and cross Dobbs Lane as shown.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Rosemary Nunn	860	No	<p>I Have viewed the Key Corridors and Public Rights of Way of the Draft Cycling and Walking Strategy proposed for Martlesham Heath, and strongly object to the invasive nature of the Very High and Medium routes.</p> <p>Therefore I wish my objections to be noted as too invasive to the area of Special Scientific Interest.</p>	<p>The very high priority routes of IM10 and IM11 have been retained in the Strategy. However, IM12 has been amended to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath. No recommendations propose the introduction of cycling and walking infrastructure on the Ipswich Heaths Site of Special Scientific Interest.</p>
Ipswich to Melton Key Corridor Paragraph	S Hall	642	No	<p>Regarding</p> <p>3.15 The Ipswich - Melton Key Corridor seeks to provide a cohesive cycling and walking network, which maximises off-road routes and delivers improvements of the highest quality. The route</p>	<p>The Strategy has sought to provide a range of ambitious solutions to the issues that exist along our transport network, in order to generate modal shift by making it more attractive to walk and cycle rather than drive. In some circumstances the best solution will be a traffic free cycling and walking route with appropriate</p>

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3.15 to 3.19				<p>encompasses the parishes of Rushmere St Andrew, Kesgrave, Martlesham, Woodbridge, Melton, Brightwell, Bucklesham and Foxhall.</p> <p>Government policy now places cycling in the centre of a balanced road-based transport policy. We need to focus on providing for cycling within road spaces and not sidelining cyclists onto off road routes which often means taking space away from pedestrians.</p> <p>I don't think off road routes will deliver high quality improvements for commuter cyclists. I disagree with this policy.</p> <p>If I am cycling home from work on a dark, cold winters day I need a well surfaced route with street lighting not an off road track running through remote areas or a along a narrow route shared with pedestrians, buggies, dogs, parked cars etc</p> <p>I attach an extract from Cycle infrastructure Design guidance to highlight new government policy.</p> <p>In particular I want to highlight the poor design of the current A1214 shared cycle/pedestrian route currently running through Kesgrave. When it was built it no doubt fitted in with government policy. But government policy has now changed and this kind of design is no longer acceptable. Your consultation highlighted complaints from cyclists about the danger of having to give way at multiple side road junctions and the new government guidance says this is not good design. Instead,</p>	<p>segregation between cyclists and pedestrians, and in others it will be an on road cycling track segregated from vehicles and pedestrians. The constraints within the network will usually dictate the most appropriate solution.</p> <p>It is also the case that existing cycling and walking infrastructure will not always meet the higher standards we have today, as set out in Local Transport Note 1/20 and in changes to the Highway Code.</p> <p>As regards Main Road, Kesgrave, recommendation IM5 proposes the cycling and walking track to be continued over the side streets to clearly indicate priority over vehicles, and IM5 has been amended to recommend segregation to be introduced between cyclists and pedestrians should appropriate space be available.</p>

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				<p>cyclists need to be accommodated in dedicated space on the road. See attached extracts from government policy.</p> <p>There seems to be a proposal to simply widen the existing A1214 pedestrian/cycle route. I cannot see how an extra 2 feet of width (which may be all that is possible in some locations) can possibly deliver on government policy which seeks to provide for mass cycling. This proposal would not be value for money at all. There would be negligible benefit. It's tinkering at the edges providing some possible cosmetic benefit and not providing the real step change and bold vision and policy that we now need to implement. We need better provision for both pedestrians and cyclists - not more of the same awful compromises.</p> <p>From a practical point of view too - I've lost count of the times I've had dogs running out in front of me on these kind of paths, people walking in a line along the whole width of the path, cars parked in the pathway causing obstruction etc etc. It's not good for pedestrians or for cyclists - and I am both of these.</p> <p>For the A1214 through Kesgrave, the footpath needs widening for pedestrians and dedicated space on the road - protected with wands or similar needs to be provided for cyclists. Ideally a whole carriageway needs to be given to cyclists. This is what other places in the UK are doing. And the road could be designed as a 20mph zone.</p>	

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				<p>Attachments: https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455256/DOCX/-/11775541%201%20Cycle%20infrastructure%20Design%2Edocx</p>	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Terry Duffell	709	No	<p>As a resident of Martlesham Heath, I am very concerned about the proposals for the implementation of this strategy for the local area. Collectively, they will have a serious and detrimental impact on the environment and adversely affect the character of the village, and I wish to register my opposition to these plans.</p> <p>The plans in general would urbanise a unique area, diminish its character and alter the profile irreparably, and I implore the Council to reconsider these unnecessary and potentially very damaging proposals.</p>	In preparing the Strategy the character and natural and historic environment of each area has been considered. In the Martlesham Heath Area, IM12 has been amended to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath.
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Ufford Parish Council (Judi Hallett)	728		<p>Section 3 2 onwards incl. 3.15 to 3.19 - Key Corridors: Ipswich to Melton</p> <p>This set of recommendations identifies the Ipswich to Melton corridor as a critical area for improvement and innovation to support increased use of cycling and encourage more walking. This is welcomed although the extent of the corridor locally seems to be based on current and potential usage, notably on Melton Road and Wilford Bridge Road. Extension to cover Ufford should be considered to encourage greater cycling and walking.</p> <p>We support the new off-road routes planned which</p>	The Ipswich to Melton Key Corridor has been amended, at recommendation IM23, to extend the route along Bridleway 10 and Footpath 6 to provide a cycling and walking route between Melton Railway Station and Lower Road, thereby avoiding the heavily trafficked Yarmouth Road and enabling cycling and walking between Ufford and Melton via Lower Road and along the improved Footpath 6.

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				would make it easier and safer for cyclists and walkers and it is interesting to note that the Melton Neighbourhood Plan contains a section devoted to cycle lane and footpath/ walking enhancements.	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Wendy Sexton	132	No	Please do not destroy anymore woodland , trees etc. Feel the council always take the easy way out with regard to Martlesham Heath. Bombarding us with everything. What we have here on the Heath is very precious and fragile. please leave our woodland areas as they are and try to come up with alternatives or not at all. We have put up with Adstral Park, McCarthy & Stone etc etc etc. LEAVE our Woods etc as they are please!!!	The importance of avoiding harm to the natural environment is recognised throughout the preparation of the Strategy. A number of recommendations within Martlesham Heath have been amended in order to avoid harm to the natural environment.
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Whymark Nichola	78	No	Yes. I find it very sad that hard earned tax payers money is being considered on such an unnecessary concept. I see cyclists in the woods all the time. Funnily enough they are on mountain bikes and are there because they want to ride through the woods as it is. The woodland has been there way before any of us residents. How very sad that damage and abuse of the natural environment is even being considered. It does not make sense. It comes across like a tick box exercise rather than being sensible. Perhaps time and effort should be put into ensuring the roads, pathways and cycle paths in the village are fit for purpose. They currently are not. In fact this has been neglected for years. Why start a new project when such basic things have not already been addressed? If people wish to cycle in the woods then they would use the correct sort of bike that does not require a flashy (not in keeping) pathway. As for the damage to the trees, nature, animals I dread to think. Please consider an	Throughout the preparation of the Ipswich to Melton Key Corridor careful consideration has been given to avoiding harm to the natural and historic environment, whilst providing for the needs of cyclists and pedestrians. The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.

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				alternative. Look at the land on Eagle Way and give that some thought to aid cyclists. Not trounce through natural beauty which has been left unscathed for all this time. One more thing to add. The woods is used constantly by dog walkers, elderly and children. Cyclists bombing their way through the woods is not conducive to the masses using it. Please delete my previous comment. This is the correct one I wish to submit.	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Woodland Trust (Oliver Newham)	632	No	<p>Woodland Trust is very concerned about the potential for damaging impacts on areas of woodland and in particular ancient woods and ancient/veteran trees in the vicinity of the new cycleway. We have only been notified of the consultation at a very late stage but we have already, from a very quick analysis, identified the following potential threats.</p> <p>We would urge that you look either divert the cycleway away from these trees/woods or put in appropriate buffering.</p> <p>Woodland Trust owned wood – Porter’s Wood - IM16 and IM18 adjacent</p> <p>Notable Pedunculate oak TM23824613, ID 84936 – IM5 on A1214</p> <p>Notable Pedunculate oak TM23804611, ID 84937 - IM5 on A1214</p> <p>Notable Pedunculate oak TM22774540, ID 29592 - IM7</p>	<p>Throughout the preparation of the Strategy careful consideration has been given to avoiding harm to the natural environment. In taking forward recommendations for delivery the detailed design of cycling and walking infrastructure must demonstrate consideration of and avoidance of harm to the natural environment.</p> <p>IM18 has been rerouted away from Porter’s Woods and instead follows Ipswich Road to avoid harm to the natural environment. IM16 involves the introduction of a modal filter on Sandy Lane and will therefore not result in any harm to ancient, veteran, or other trees.</p>

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				<p>Ancient Pedunculate oak TM21694458, ID 96901 – IM4 west side of Bell Lane</p> <p>Veteran pedunculate oak TM21894456, ID 96900 – IM4 east side of Bell Lane</p> <p>Ancient pedunculate oak TM21914456, ID 38765 – IM4 east side of Bell Lane</p> <p>Veteran pedunculate oak TM21934456, ID 96899 – IM4 east side of Bell Lane</p> <p>Veteran pedunculate oak TM21944455, ID 96894 – IM4 east side of Bell Lane</p>	
Ipswich to Melton Key Corridor Paragraph 3.15 to 3.19	Zac Barnes	600	Yes	<p>Despite 3.18 explicitly identifying Sutton Hoo as an opportunity, no mention is made of making the private road (mentioned by Comment 276 – score of 7) a bridleway. It would seem a shame to get so close to making a good connection from Ipswich to Sutton Hoo (a nationally important tourist destination) and fall over at the last hurdle.</p> <p>A footpath will have to be created along the road anyway as part of the England Coast Path and a footpath previously existed along the east bank of the Deben (though has now been eroded by the river) so it seems reasonable to make it accessible to bikes at the same time.</p> <p>The annoying lack of public right of way for this stretch has been mentioned to me as a gap in the path network by everyone from scout leaders to a</p>	The Ipswich to Melton Key Corridor has been extended to Sutton Hoo as it was an omission from the Draft Strategy. The recommendation (IM30) links to Sutton Hoo along a tarmac track which is proposed to form part of the England Coast Path.

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				cyclist doing an international tour so I think that it really ought to be fixed.	
IM1	Chris Adelson	444	Yes		Support noted.
IM1	David Adelson	463	Yes		Support noted.
IM1	John Adams	379	No	<p>Bridleways on Rushmere would be disasterous for a well loved and used open space by the general public and Rushmere Golf Club.</p> <p>Upgrading Public Footpaths to Bridleways would too dangerous as many of the fairways cross the paths. Walkers tend to check to see if golf is in play and wait, but illegal cyclists just wizz along expecting people to jump out of their way, and ignore golfers. More traffic would impinge on public safety. The prospect of equestrians cutting across fairways would also be dangerous. Golfers will not wish to use such a dangerous and uninviting course so will play elsewhere forcing the club to close.</p> <p>With no income from the golf club the Rushmere Commoners would have no income to manage the common, which is too large an area to rely on volunteers. Very soon the common would turn into a scrub and wooded area, a centre for fly tipping and other criminal activities, thus making it an unsafe and unhealthy place to enter. An eyesore would also have a detrimental effect on the value of properies surrounding it.</p> <p>The destruction of this public amenity when there are other options is totally unacceptable.</p>	The creation of bridleways through Rushmere Common would act to formalise cycling and walking and therefore, through careful design, reduce the conflict between cyclists and pedestrians. The safety of users has been considered throughout the preparation of the Strategy, and IM1 would provide a much safer walking and cycling route for all users than currently provided for elsewhere on roads alongside vehicles.
IM1	Kev Driver	93	No	Cycle and footpaths across the common must be mindful of the wildlife and environment. The	The creation of bridleways through Rushmere Common would act to formalise cycling and walking and

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				<p>common is also used to play golf and the proposed upgrades to bridleway etc would come with an increased safety risk. There are already a number of near misses with golf balls and upgrades could lead to more users and heighten the risk of injury.</p> <p>The surface of the common should also be natural and not made of man made materials and as such with heavier use could lead to an impact on the wildlife including the pond which one route passes nearby.</p>	<p>therefore, through careful design, reduce the conflict between cyclists and pedestrians. The safety of users has been considered throughout the preparation of the Strategy, and IM1 would provide a much safer walking and cycling route for all users than currently provided for elsewhere on roads alongside vehicles. The most appropriate surfacing material will need to be considered as the recommendation is taken forward to delivery.</p>
IM1	Martyn Shakespeare	487	Yes	<p>Could be very useful if IM4 implemented. Suggest a rolled stone surface across Heath (and no lighting) to fit in visually. Need IBC to sort out Heath Road connection to path around hospital to make maximum benefit of this path.</p>	<p>Connections east and west of IM1 are of fundamental importance to the delivery of the route, and the most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward to delivery.</p>
IM1	Mike Gilson	645	No		<p>Objection noted.</p>
IM1	Nik Bestow	12	No	<p>This should be left as a rough track. Cycle Route 1 (which you have omitted from the map) provides a more than adequate route around the bottom of the heath. Don't forget the Heath Road crossing point is further up, not opposite Heath Lane. Having a suitable off road cycle path to the North of the heath is more important.</p>	<p>The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward to delivery. While National Cycle Route 1 provides a good route, the intention with the Ipswich to Melton Key Corridor is to provide a direct and therefore more desirable cycling and walking route.</p>
IM1	Ray Whymark Construction Consultant (Raymond Whymark)	272	No	<p>I have read the County Council's proposals for creating bridleways IM 1,2 and 3 on Rushmere Heath and I must object to these in the strongest possible terms.</p> <p>I have resided adjacent to the Heath for 55years and have enjoyed the wonderful facilities that it offers to members of the public and golfers. It is the only green lung between Ipswich and Woodbridge that</p>	<p>The creation of bridleways through Rushmere Common would act to formalise cycling and walking and therefore, through careful design, reduce the conflict between cyclists and pedestrians. The safety of users has been considered throughout the preparation of the Strategy, and these routes would provide much safer walking and cycling routes for all users than currently provided for elsewhere on roads alongside vehicles.</p>

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				<p>can be enjoyed by all including walkers, runners and the public at large as well as members of the Golf Club. If this facility is allowed to be eroded then there will soon be an urban sprawl from Ipswich through to Martlesham and Woodbridge.</p> <p>If the proposals to create these bridleways were to proceed then it is likely that the Golf Club would close due to the way in which the course would become crossed many times by the bridleways making playing golf impossible with additional danger caused by golf balls. If the Club were to leave then the Heath would become dangerous with little or no control of what happens and misuse of the area would be rife including increases in the current drug abuse and other illegal actions.</p> <p>I must urge you to reconsider the entire question of the bridleways so that the Heath is not affected, many local people are angry at the proposals and the Heath must stay as common land in its entirety, any erosion of the wonderful facility will be regretted for many years to come.</p>	<p>In order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.</p>
IM1	Rushmere Commoners Committee (C A Kendall)	373	No	<p>Objection Letter - Draft Cycling And Walking Strategy Nov 2021</p> <p>As managing trustees of Rushmere Common we strongly object to the proposals to create bridleways on our common. We have not been informed of such proposals and have been contacted by a number of concerned commoners.</p> <p>The land is not public land, it is privately owned and</p>	<p>Recommendations IM1, IM2, and IM3 provide highly desirable cycling and walking routes between Ipswich and areas further east and therefore have been retained. Such routes would enable a greater number and diversity of individuals to access the common, convert trips to cycling and walking that might ordinarily be made by private car thereby reducing environmental impacts on people and wildlife, and help to support active and healthy lifestyles. The creation of bridleways through Rushmere Common would act to formalise</p>

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				<p>is registered common land held for the benefit of the commoners. It is managed by the Rushmere Commoners Committee as a registered charity.</p> <p>Large parts of the common are let to the Rushmere Golf Club, under licence, and the income from that pays for the overall common maintenance. We also have a number of charities and organisations involved in helping us to maintain and improve the heathland environment.</p> <p>The upgrading of the rights of way as proposed for IM1 , IM2 and IM3 in terms of status and surface and the extension of the route of FP59 would seriously harm the commoners' rights and the interests of the owners, occupiers and users.</p> <p>Our initial main objections are:</p> <ul style="list-style-type: none"> • The commoners have a number of rights and, although not all are exercised, this would mean that these rights cannot be fully exercised in the future. • Loss of valuable common land and consequently loss of the character and amenity value. • Loss of biodiversity value and adverse effect on wildlife as walkers move from established paths further into the common to avoid cyclists. • The common would no longer be a safe environment for the general public walking on paths and golfers would be unaware of approaching cyclists. This would no doubt result in injuries and claims for those injuries. • Rushmere Golf Club is a heathland-based course 	<p>cycling and walking and therefore, through careful design, reduce conflict between cyclists and pedestrians. The safety of users has been considered throughout the preparation of the Strategy, and IM1 would provide a much safer walking and cycling route for all users than currently provided for elsewhere on roads alongside vehicles. However, in order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.</p>

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				<p>that is well-regarded throughout Suffolk. This proposal would massively impact the course, which is the oldest in the Ipswich area, and these plans could result in its closure.</p> <ul style="list-style-type: none"> • If Rushmere Golf Club were forced to take their business elsewhere the commoners would lose this income and be unable to maintain the common as a whole in its current form. The impact on this valuable green space would be catastrophic. This would have a knock-on effect and be detrimental to the surrounding area. • We do not allow cycling on the common and creation of such access will start to legitimise such access to the detriment of the many people who enjoy this pedestrian only space. <p>We trust that you will take into account our strong objections and the concerns of the Commoners of Rushmere and that this plan will be rejected out of hand.</p>	
IM1	Rushmere Golf Club (Bob Tawell)	371	No	<p>Objection to East Suffolk Councils</p> <p>“East Suffolk Cycling and Walking Strategy Draft 2021”</p> <p>Golf has been played on Rushmere Common (The Hidden Gem of Suffolk) since 1895 at which time known as Ipswich Golf Club, from 1927 Rushmere Golf Club was established. This long-established Heathland Course has become one of the most respected courses in the County, as we have built up a reputation of being one of the most</p>	<p>Recommendations IM1, IM2, and IM3 provide highly desirable cycling and walking routes between Ipswich and areas further east and therefore have been retained. Such routes will enable a greater number and diversity of individuals to access the common, convert trips to cycling and walking that might ordinarily be made by private car thereby reducing environmental impacts on people and wildlife, and help to support active and healthy lifestyles. The creation of bridleways through Rushmere Common would act to formalise cycling and walking and therefore, through careful design, reduce conflict between cyclists and pedestrians. The safety of users has been considered</p>

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				<p>welcoming of clubs, which has a course that drains so well, making it playable all year round. Visitors and societies are welcome and many return year after year to enjoy the course and our excellent hospitality. Our club is a Community Hub which would be lost if this project goes ahead.</p> <p>The proposals put forward by East Suffolk County Council to upgrade rights of way through the common IM1, IM2 and IM3, would destroy the existence of Rushmere Golf Club for the following reasons.</p> <ul style="list-style-type: none"> • IM1 will cause playing issues at the 2nd, 8th, 9th and 12th • IM3 will cause playing issues at the 10th, 11th, 12th, 14th, 15th, 16th, and 18th • IM2 will cause playing issues at the 17th • This would have a major impact on membership, at present we have over 600 members of all ages, male and female, young and old. Due to this we would expect a minimum of over 50% of members to leave should this project go ahead, as the course would become a Health & Safety hazard and become an unenjoyable experience. If this occurs, then the Golf Club would become commercially unviable, and we see no alternative but to cease trading. If this occurs then we would be required to sell our land, clubhouse and part of the course 	<p>throughout the preparation of the Strategy, and IM1 would provide a much safer walking and cycling route for all users than currently provided for elsewhere on roads alongside vehicles. However, in order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.</p>

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				<p>to a commercially acceptable bidder as per our constitution.</p> <ul style="list-style-type: none"> • At present all our members and visitors are insured, however with introducing hard surfaces where golf balls will accelerate off the surface and the footfall would increase dramatically, the insurance company would want to relook at the premiums again, this would mean extra cost to the golf club. • The Golf Club has become a community center over the years where we have welcomed outside groups like the Deaf Club, Blind Club and the Health Centre at Kesgrave, it has become a function area for local parties' weddings and wakes, all of this will be lost if the Golf Club closes. • The Golf Club employs 7 full time employees and circa 20 part time people, all these jobs would go as the Club would not be able to survive on half a membership. • If the Club folds, then the income the Commoners Trustees receive from us as a rent to play golf on the Common would be lost. This in turn would see the Common turn into one of the biggest dumping grounds and drugs haven that I am sure Ipswich & East Suffolk Council does not want, you would have new paths, but then not safe for walkers to use! I am sure that 	

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				<p>a travelling community would soon set up camp on this beautiful Heathland Site!</p> <ul style="list-style-type: none"> • One of the big fears about introducing hard surfaces is that after a very short while, those with motor bikes, electric scooters would soon see these pathways as racetracks, bearing in mind families take their children to school through the common and this would in turn become a major hazard and health and safety concern. • Over the years Rushmere Golf Club have worked very closely with the Commoners Trustees and have dealt with many clear ups after major fires, this has been done with the help of Hollesley Bay and the Green Light Trust. Again, we have seen this as helping the Community. • Rushmere Golf Club Green Staff and volunteers pride their-selves on how safe they make the Common for both walkers and golfers. Walkers in turn always feel safe knowing golfers are on the course, we have a good relationship with over 95% of both parties making the common work for them. Should the paths go ahead this I am sure will cause major issues as you can see by item 1,2, and 3 as slow play would be major issue. The increased footfall would increase the time it takes to play a round of golf as golfers would need to wait longer to ensure walkers, cyclists etc. are not in range and are safe before hitting a 	

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				<p>shot. Any increase in the time to play would have a detrimental impact on golfers and subsequent membership numbers of the golf club.</p> <ul style="list-style-type: none"> Rushmere Golf Club is 100% against this project going ahead and are open for further communication with the planning department to ensure a comprehensive and thoughtful review is undertaken. 	
IM1	Rushmere St Andrew Parish Council (Stannard)	251	No	Rushmere St Andrew Parish Council strongly objects to these proposals/ recommendations. These proposals will lead to a deterioration, disturbance and damage of the Rushmere St Andrew Commons/ Rushmere Heath. The Commons/ Heath is an important ecological and recreational asset to residents within and outside of the parish. It is important that the Commons/ Heath be preserved as it is and to avoid any damage/ deterioration to it. The recommendations would create a significant public safety issue creating conflict between pedestrians, cyclist and golfers. The Commons/ Heath is very well used and it is a serious concern.	Recommendations IM1, IM2, and IM3 provide highly desirable cycling and walking routes between Ipswich and areas further east and therefore have been retained. Such routes will enable a greater number and diversity of individuals to access the common, convert trips to cycling and walking that might ordinarily be made by private car thereby reducing environmental impacts on people and wildlife, and help to support active and healthy lifestyles. The creation of bridleways through Rushmere Common would act to formalise cycling and walking and therefore, through careful design, reduce conflict between cyclists and pedestrians. The safety of users has been considered throughout the preparation of the Strategy, and IM1 would provide a much safer walking and cycling route for all users than currently provided for elsewhere on roads alongside vehicles. However, in order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.
IM2	British Horse Society (Lynda Warth)	672		The information above is to support the principle and some of the detail of the need to include equestrians in the LCWIP project and to protect the amenity of the bridleway network for equestrians	Throughout the preparation of the Strategy consideration has been given to the needs of equestrians in ensuring that where improvements are

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				<p>since that is the only network available to them.</p> <p>I am conscious that I have not addressed the question of the actual routes included in the consultation. I cannot provide this information as I do not know the area well enough. This information needs to be provided by local horse riders however, due to the lack of BHS Access Officers in Suffolk, it may well take time to collate. Caroline and I would appreciate guidance on which information you require as a priority.</p> <p>However, the following boxes in the consultation do raise concern because they all refer to bridleways and potential ‘improvements’ or ‘resurfacing’ to the path surface, ostensibly for the benefit of cyclists. Care must be taken to ensure that any changes do not discriminate against the intended bridleway user – equestrians - and that shared use does not result in the displacement or reduction in amenity of existing users – this includes the allocation of path width for a hard top path to enable cyclists to ride side by side whilst reducing the same social amenity for horse riders.</p> <p>The term ‘improvement’ has been challenged in Court.</p> <p>Section 62 (2) of the Highways Act 1980 gives Local Highway Authorities a power to carry out works to improve highways.</p> <p>The Court of Appeal in the case of Cowen –v-</p>	<p>recommended to bridleways, that they meet the needs of all users.</p>

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				<p>Secretary of State for the Department of Environment Transport and the Regions (1999) 3PLR108 concluded that if the construction of a hard surface changes the character of a way it goes beyond 'improvement'.</p> <p>It will always be a matter of fact that tarmac a bridleway will change its character. When deciding to tarmac a bridleway, the highway authority should take safety implications and the enjoyment of current users, including riders, into consideration.</p> <p>Improvements to a bridleway must make the way easier, safer, or more enjoyable its users, enhancing its suitability for use. Any works to a bridleway which detract from its suitability might not be a proper exercise of the highway authority's power to improve a highway.</p> <p>Any changes to crossings which are proposed to link new routes must be multi user including equestrians. Any barrier along a route renders the whole route useless. All routes need to be RSA assessed from the outset to ensure they are not delivered and then discovered not to be suitable for equestrians.</p> <p>Care needs to be taken in the use of the Strava information – this is not a valid NMU representative group. Also, as we mentioned, the Strava purportedly cycling information, may well be distorted by equestrians who use the app but</p>	

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				without the opportunity to register as horse riders rather than cyclists.	
IM2	Martyn Shakespeare	489	Yes	Would enhance safety along this section.	Support noted.
IM2	Mike Gilson	646	No		Objection noted.
IM2	Nik Bestow	13	Yes	<p>Having a segregated cycle path along this stretch of road should be the number one priority. The path on the south side of the road is used by many cyclists and runners but isn't wide enough to pass.</p> <p>The road is a major bus route (Route 66 and Park & Ride) and is used by Ambulances heading towards Ipswich hospital making it a risk (I avoid using it preferring Cycle Route 1).</p>	Support noted. IM1 and IM4 remain the highest priority east-west cycling and walking route as it provides the greatest safety for all users. However, IM2 remains a high priority route.
IM2	Nik Bestow	99		<p>• Paths Across Woodlands and Heath</p> <p>I'm not in favour of building lit cycle paths through the woodland areas in Martlesham or diagonally across Rushmere Heath.</p> <p>These are havens for nature - particularly at night. It's easy to cycle round Eagle Way which has very little traffic.</p> <p>I do cycle across Rushmere Heath occasionally - but this is mainly for leisure and the off-road challenge - as I suspect do most people. A cycle path would remove this enjoyment.</p> <p>Dedicated cycle paths to the North and South of the heath (as you have proposed) would be more than adequate - I suspect there are very few people who start in the South East corner of the heath and need to get to the north west corner.</p>	Recommendations IM1, IM2, and IM3 provide highly desirable cycling and walking routes between Ipswich and areas further east and therefore have been retained. However, in order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.
IM2	Rushmere St Andrew Parish	253	No	The Parish Council also objects to proposal IM2. The Parish Council is concerned that these proposals	Throughout the preparation of the Strategy consideration has been given to the need to avoid harm

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	Council (Stannard)			<p>would mean that the road would need to be widened and that this would result in incursions into the land adjacent to Woodbridge Road that has been identified in the Rushmere St Andrew Neighbourhood Plan as an important gateway into the parish and a rewilding project are currently being implemented on this area of land. The southern side of Woodbridge Road has long stretches of well developed cycle and pedestrian routes and it is considered that this side of the road would be a far better option to develop a cycle route.</p> <p>Cycle routes should be separated from the road to work efficiently and safely.</p>	<p>to the natural environment. There are many ways cycling and walking infrastructure could be introduced to Woodbridge Road and the detailed design will need to further consider the impacts of such infrastructure on the natural environment whilst also providing for the needs of cyclists and pedestrians.</p>
IM2	S Hall	656	No	<p>At the point where Woodbridge Road runs across Rushmere Heath we need wider pedestrian footways. The footways are well used by walkers, dog-walkers, joggers, mobility scooter users etc and are currently too narrow. We also need wide cycle lanes here - separated from pedestrians and road vehicles, because this is a key gap in the cycle network which needs to be filled. However, I think provision needs to be made on both sides of the road and not on the north side only. So I disagree with the proposal for a north-side only provision - if that is what is being proposed.</p> <p>If we had north-side provision only - it means cyclists have to cross back and forth over the road rather than use the road in a direct fashion as a motorist would. This is inconvenient and it reduces the safety and attractiveness of any route provision. e.g. It will</p>	<p>Reference to a bidirectional cycling and walking track along the northern side of Woodbridge Road in the draft Strategy was not intended to advocate for such infrastructure instead of other forms of infrastructure, but simply to note the need for a crossing point at Footpath 57 and Beech Road. IM2 has therefore been amended to remove reference to a bidirectional cycling and walking track along one side of Woodbridge Road, thereby clarifying that cycling and walking infrastructure would not need to be designed as a bidirectional track on one side of Woodbridge Road, but could come forward in a number of ways, including a segregated cycle track and footway on both sides of the road.</p>

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				<p>probably mean stopping / queuing at traffic lights. It doesn't fit in with the new government policy of prioritising cyclists over motorists! It is hard to believe we now have a government policy which actually does that - but we do. We have to make cycling really attractive and cater for mass cycling. We have a "blank canvas" along this stretch of road to design anything we want and I would like to see preference given to having wide footways and cycle routes along both the north and south side of the carriageway. Funding opportunities should hopefully be open to supporting this as it fits in with government policy.</p> <p>Also, as mentioned in earlier consultation comments it would be possible to create something really special here. I suggested planting suitable trees along the edge of the footway and Heath - possibly Birch, Oak etc? and a shrub layer - gorse? to create an attractive and sheltered route for pedestrians and cyclists and an attractive feature in the landscape. I think the land either side of the A1214 here is Common Land - if so, then can the Council find an area of land, comparable in size and in quality in terms of wildlife/landscape quality and public amenity/access in East Suffolk to dedicate as Common Land to substitute/compensate for that taken? And as an enhancement, perhaps East Suffolk could discuss with the land owners/managers any appropriate support for wildife e.g. a wildlife tunnel underneath the A1214 road if helpful for connectivity for amphibians /reptiles other creatures</p>	

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				<p>in lowland heath habitats or other support?</p> <p>Finally - in reply to my earlier comment you said</p> <p>Optimisation: As there is currently no infrastructure for cycling at this point, but segregated cycle lanes are not as effective as pedestrian/cycle tracks, and do not provide pedestrian infrastructure, a score of two is given.</p> <p>Your reply makes no sense to me! Cycle lanes are a highly effective means of delivering government policy, please do read the new policy, thank you.</p>	
IM3	British Horse Society (Lynda Warth)	673		<p>The information above is to support the principle and some of the detail of the need to include equestrians in the LCWIP project and to protect the amenity of the bridleway network for equestrians since that is the only network available to them.</p> <p>I am conscious that I have not addressed the question of the actual routes included in the consultation. I cannot provide this information as I do not know the area well enough. This information needs to be provided by local horse riders however, due to the lack of BHS Access Officers in Suffolk, it may well take time to collate. Caroline and I would appreciate guidance on which information you require as a priority.</p> <p>However, the following boxes in the consultation do raise concern because they all refer to bridleways and potential 'improvements' or 'resurfacing' to the path surface, ostensibly for the benefit of</p>	<p>Throughout the preparation of the Strategy consideration has been given to the needs of equestrians in ensuring that where improvements are recommended to bridleways, that they meet the needs of all users.</p>

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				<p>cyclists. Care must be taken to ensure that any changes do not discriminate against the intended bridleway user – equestrians - and that shared use does not result in the displacement or reduction in amenity of existing users – this includes the allocation of path width for a hard top path to enable cyclists to ride side by side whilst reducing the same social amenity for horse riders.</p> <p>The term ‘improvement’ has been challenged in Court.</p> <p>Section 62 (2) of the Highways Act 1980 gives Local Highway Authorities a power to carry out works to improve highways.</p> <p>The Court of Appeal in the case of Cowen –v- Secretary of State for the Department of Environment Transport and the Regions (1999) 3PLR108 concluded that if the construction of a hard surface changes the character of a way it goes beyond ‘improvement’.</p> <p>It will always be a matter of fact that tarmac a bridleway will change its character. When deciding to tarmac a bridleway, the highway authority should take safety implications and the enjoyment of current users, including riders, into consideration.</p> <p>Improvements to a bridleway must make the way easier, safer, or more enjoyable its users, enhancing its suitability for use. Any works to a bridleway which detract from its suitability might not be a proper</p>	

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				<p>exercise of the highway authority's power to improve a highway.</p> <p>Any changes to crossings which are proposed to link new routes must be multi user including equestrians. Any barrier along a route renders the whole route useless. All routes need to be RSA assessed from the outset to ensure they are not delivered and then discovered not to be suitable for equestrians.</p> <p>Care needs to be taken in the use of the Strava information – this is not a valid NMU representative group. Also, as we mentioned, the Strava purportedly cycling information, may well be distorted by equestrians who use the app but without the opportunity to register as horse riders rather than cyclists.</p>	
IM3	Chris Adelson	445	Yes		Support noted.
IM3	Kev Driver	94	No	<p>See Comment about safety and suitable surfacing in IM1</p> <p>Cycling is not actually permitted on the common, and this would lead to a change in it's use. I believe this would be a death nell in the oldest Ipswich Golf Course an important part of Ipswich history.</p>	In order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.
IM3	Martyn Shakespeare	488	No	Perhaps best left as a walking route, PROW59/66 better as a cycling route. Potential for too much intrusion on common land.	In order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.
IM3	Mike Gilson	648	No		Objection noted.

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IM3	Nik Bestow	14	No	This is unnecessary and spoils the nature of the heath. It would make more sense to connect Linksfield with IM4.	It is agreed that a connection between IM2 and IM4 along the eastern edge of the Common would provide an equally useful cycling and walking route whilst avoid the potential harm to the Common.
IM3	Rushmere Commoners Committee (C A Kendall)	374	No	<p>Objection Letter - Draft Cycling And Walking Strategy Nov 2021</p> <p>As managing trustees of Rushmere Common we strongly object to the proposals to create bridleways on our common. We have not been informed of such proposals and have been contacted by a number of concerned commoners.</p> <p>The land is not public land, it is privately owned and is registered common land held for the benefit of the commoners. It is managed by the Rushmere Commoners Committee as a registered charity.</p> <p>Large parts of the common are let to the Rushmere Golf Club, under licence, and the income from that pays for the overall common maintenance. We also have a number of charities and organisations involved in helping us to maintain and improve the heathland environment.</p> <p>The upgrading of the rights of way as proposed for IM1 , IM2 and IM3 in terms of status and surface and the extension of the route of FP59 would seriously harm the commoners' rights and the interests of the owners, occupiers and users.</p> <p>Our initial main objections are:</p>	In order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.

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				<ul style="list-style-type: none"> • The commoners have a number of rights and, although not all are exercised, this would mean that these rights cannot be fully exercised in the future. • Loss of valuable common land and consequently loss of the character and amenity value. • Loss of biodiversity value and adverse effect on wildlife as walkers move from established paths further into the common to avoid cyclists. • The common would no longer be a safe environment for the general public walking on paths and golfers would be unaware of approaching cyclists. This would no doubt result in injuries and claims for those injuries. • Rushmere Golf Club is a heathland-based course that is well-regarded throughout Suffolk. This proposal would massively impact the course, which is the oldest in the Ipswich area, and these plans could result in its closure. • If Rushmere Golf Club were forced to take their business elsewhere the commoners would lose this income and be unable to maintain the common as a whole in its current form. The impact on this valuable green space would be catastrophic. This would have a knock-on effect and be detrimental to the surrounding area. • We do not allow cycling on the common and creation of such access will start to legitimise such access to the detriment of the many people who enjoy this pedestrian only space. <p>We trust that you will take into account our strong objections and the concerns of the Commoners of</p>	

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				Rushmere and that this plan will be rejected out of hand.	
IM3	Rushmere Golf Club (Bob Tawell)	372	No	<p>Objection to East Suffolk Councils</p> <p>“East Suffolk Cycling and Walking Strategy Draft 2021”</p> <p>Golf has been played on Rushmere Common (The Hidden Gem of Suffolk) since 1895 at which time known as Ipswich Golf Club, from 1927 Rushmere Golf Club was established. This long-established Heathland Course has become one of the most respected courses in the County, as we have built up a reputation of being one of the most welcoming of clubs, which has a course that drains so well, making it playable all year round. Visitors and societies are welcome and many return year after year to enjoy the course and our excellent hospitality. Our club is a Community Hub which would be lost if this project goes ahead.</p> <p>The proposals put forward by East Suffolk County Council to upgrade rights of way through the common IM1, IM2 and IM3, would destroy the existence of Rushmere Golf Club for the following reasons.</p> <ul style="list-style-type: none"> • IM1 will cause playing issues at the 2nd, 8th, 9th and 12th • IM3 will cause playing issues at the 10th, 11th, 12th, 14th, 15th, 16th, and 18th • IM2 will cause playing issues at the 17th 	In order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.

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				<ul style="list-style-type: none"> • This would have a major impact on membership, at present we have over 600 members of all ages, male and female, young and old. Due to this we would expect a minimum of over 50% of members to leave should this project go ahead, as the course would become a Health & Safety hazard and become an unenjoyable experience. If this occurs, then the Golf Club would become commercially unviable, and we see no alternative but to cease trading. If this occurs then we would be required to sell our land, clubhouse and part of the course to a commercially acceptable bidder as per our constitution. • At present all our members and visitors are insured, however with introducing hard surfaces where golf balls will accelerate off the surface and the footfall would increase dramatically, the insurance company would want to relook at the premiums again, this would mean extra cost to the golf club. • The Golf Club has become a community center over the years where we have welcomed outside groups like the Deaf Club, Blind Club and the Health Centre at Kesgrave, it has become a function area for local parties' weddings and wakes, all of this will be lost if the Golf Club closes. 	

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				<ul style="list-style-type: none"> • The Golf Club employs 7 full time employees and circa 20 part time people, all these jobs would go as the Club would not be able to survive on half a membership. • If the Club folds, then the income the Commoners Trustees receive from us as a rent to play golf on the Common would be lost. This in turn would see the Common turn into one of the biggest dumping grounds and drugs haven that I am sure Ipswich & East Suffolk Council does not want, you would have new paths, but then not safe for walkers to use! I am sure that a travelling community would soon set up camp on this beautiful Heathland Site! • One of the big fears about introducing hard surfaces is that after a very short while, those with motor bikes, electric scooters would soon see these pathways as racetracks, bearing in mind families take their children to school through the common and this would in turn become a major hazard and health and safety concern. • Over the years Rushmere Golf Club have worked very closely with the Commoners Trustees and have dealt with many clear ups after major fires, this has been done with the help of Hollesley Bay and the Green Light Trust. Again, we have seen this as helping the Community. 	

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				<ul style="list-style-type: none"> • Rushmere Golf Club Green Staff and volunteers pride their-selves on how safe they make the Common for both walkers and golfers. Walkers in turn always feel safe knowing golfers are on the course, we have a good relationship with over 95% of both parties making the common work for them. Should the paths go ahead this I am sure will cause major issues as you can see by item 1,2, and 3 as slow play would be major issue. The increased footfall would increase the time it takes to play a round of golf as golfers would need to wait longer to ensure walkers, cyclists etc. are not in range and are safe before hitting a shot. Any increase in the time to play would have a detrimental impact on golfers and subsequent membership numbers of the golf club. • Rushmere Golf Club is 100% against this project going ahead and are open for further communication with the planning department to ensure a comprehensive and thoughtful review is undertaken. 	
IM3	Rushmere St Andrew Parish Council (Stannard)	252	No	Rushmere St Andrew Parish Council strongly objects to the recommendations proposed. The recommendations would lead to the deterioration, disturbance and damage of the Rushmere Common/ Heath. The Commons/ Heath is an important ecological and recreational asset to residents within the parish and outside the parish. It is important that this important asset be conserved as it is. The recommendations will create a public safety issue. It	In order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.

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				will create significant conflict between pedestrians, cyclists and golfers. The Commons/ Heath is well used by pedestrians and this would be a serious safety issue.	
IM4	Alasdair MacLeod	537	No	<p>I think IM4 would be damaging to Long Stropps and I suspect would not be used as much as anticipated I do not think it should be "very high". There already exists a tarred path along some of the north side of Long Stropps and I don't see people using it that much. I think this is a leisure route and people will prefer to be "off road" in many cases.</p> <p>If IM4 is to go ahead consider hard-packed aggregate rather than tarred surfaces. Ideally leave out lighting to avoid light pollution. I suggest the north side of long stropps running alongside the housing estate so existing surfaced paths can be incorporated and any lighting is against the backdrop of the lights in houses and street lights rather than creating a new band of lighting on the other side of Long Stropps.</p>	The importance of Long Stropps Bridleway for leisure cycling, walking and dog walking is recognised. There is space along Long Stropps to introduce cycling and walking infrastructure that meets the needs of disabled users and commuting cyclists and pedestrians, as well as retaining the attractive natural setting for leisure cycling, walking and dog walking.
IM4	British Horse Society (Lynda Warth)	674		<p>The information above is to support the principle and some of the detail of the need to include equestrians in the LCWIP project and to protect the amenity of the bridleway network for equestrians since that is the only network available to them.</p> <p>I am conscious that I have not addressed the question of the actual routes included in the consultation. I cannot provide this information as I do not know the area well enough. This information needs to be provided by local horse riders however, due to the lack of BHS Access Officers in Suffolk, it may well take time to collate. Caroline and I would</p>	Throughout the preparation of the Strategy consideration has been given to the needs of equestrians in ensuring that where improvements are recommended to bridleways, that they meet the needs of all users.

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				<p>appreciate guidance on which information you require as a priority.</p> <p>However, the following boxes in the consultation do raise concern because they all refer to bridleways and potential ‘improvements’ or ‘resurfacing’ to the path surface, ostensibly for the benefit of cyclists. Care must be taken to ensure that any changes do not discriminate against the intended bridleway user – equestrians - and that shared use does not result in the displacement or reduction in amenity of existing users – this includes the allocation of path width for a hard top path to enable cyclists to ride side by side whilst reducing the same social amenity for horse riders.</p> <p>The term ‘improvement’ has been challenged in Court.</p> <p>Section 62 (2) of the Highways Act 1980 gives Local Highway Authorities a power to carry out works to improve highways.</p> <p>The Court of Appeal in the case of Cowen –v- Secretary of State for the Department of Environment Transport and the Regions (1999) 3PLR108 concluded that if the construction of a hard surface changes the character of a way it goes beyond ‘improvement’.</p> <p>It will always be a matter of fact that tarmacing a bridleway will change its character. When deciding to tarmac a bridleway, the highway authority should</p>	

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				<p>take safety implications and the enjoyment of current users, including riders, into consideration.</p> <p>Improvements to a bridleway must make the way easier, safer, or more enjoyable its users, enhancing its suitability for use. Any works to a bridleway which detract from its suitability might not be a proper exercise of the highway authority’s power to improve a highway.</p> <p>Any changes to crossings which are proposed to link new routes must be multi user including equestrians. Any barrier along a route renders the whole route useless. All routes need to be RSA assessed from the outset to ensure they are not delivered and then discovered not to be suitable for equestrians.</p> <p>Care needs to be taken in the use of the Strava information – this is not a valid NMU representative group. Also, as we mentioned, the Strava purportedly cycling information, may well be distorted by equestrians who use the app but without the opportunity to register as horse riders rather than cyclists.</p>	
IM4	Caroline Bickers	393	No	<p>Crossing Dobbs Lane at this point where there is a blind corner in the road is an accident waiting to happen. Although in the 30 mph limit cars are travelling from Foxall Road at speed. As some one who crosses here regularly I have seen many near accidents. I also totally disagree with the addition of lighting and a hard surface the woodland area. Stop light pollution and its affect on wildlife.</p>	<p>The safety of users is of paramount importance and for this reason IM4 has been amended to recommend the provision of a signalised crossing on Dobbs Lane where IM4 and IM10 meet, as well as a village gateway to slow northbound vehicles before the signalised crossing and entering Kesgrave. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward to delivery.</p>

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IM4	Chris Adelson	446	Yes		Support noted.
IM4	Fosker	208	No	Love these wood, use them everyday. Love the fact they aren't full of fast cyclist. So much running goes on here! Needs to be soft for running. Fun run every Saturday.	The importance of Long Strops Bridleway for leisure cycling, walking and dog walking is recognised. There is space along Long Strops to introduce cycling and walking infrastructure that meets the needs of disabled users and commuting cyclists and pedestrians, as well as retaining the attractive natural setting for leisure cycling, walking and dog walking.
IM4	Janine Davey	411	No	<p>I would like to object to the planned cycle route through Birch Woods, Martlesham Heath (referred to in your document as Martlesham Woods). This is a precious amenity area for local residents and an area of natural woodland. The proposal of felling mature trees to make way for the cycle route along with construction of lighting will damage the woodland and disturb wildlife. Where the path would exit on Eagle Way, cyclists and pedestrians would be tempted to take a short cut across our SSSI on their way to Dobbs Lane.</p> <p>The same argument applies for the lit tarmac routes through Portal woods and near Dobbs Lane. Our natural areas and wildlife are constantly under threat from these types of schemes and would urge you to re-visit this decision.</p> <p>I see the proposed schemes as an unnecessary waste of public money seeing as there is a perfectly acceptable existing through-cycle route from the A12 to Grange Farm.</p> <p>No doubt you will be conducting the necessary surveys and producing data to justify this expense.</p>	The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward to delivery.

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				<p>The local residents would be very interested in your findings.</p> <p>Please read and carefully consider any objections you receive and not treat this consultation as a 'box-ticking' exercise.</p>	
IM4	Jonathan Clyne	389		<p>Repeatedly, reliance is placed on Strava Metro data, but no indication is given about how many separate users there were and over how long. Is this just a handful of people using it often, or many different users over a long period of time.</p> <p>If the paths are being used satisfactorily now, there should be no need to widen and resurface them. There is always an adverse environmental impact of such widening and resurfacing schemes, the impact of which seems to have been effectively overlooked in the strategy.</p> <p>Furthermore, a bridleway is also for the use of horses. A tarmac or other hard surface would not be appropriate for horses.</p> <p>The addition of cycling and walking crossing point would make Dobbs Lane more dangerous than it is now, being very close to the corner where visibility is limited.</p>	<p>Strava Metro provides useful data to help understand the current state of walking and cycling across East Suffolk. However, it cannot be relied upon on its own and that is why caution is observed when using Strava Metro data and why other data sources have been used, which taken together provide a more comprehensive picture of the current state of cycling and walking in a given location. In order to ensure the safety of users Strava Metro does not provide data in areas where only a small number of users make trips.</p> <p>It may be the case that some routes provide for the needs of some users in their current state. However, in most cases this will exclude some users. The importance of Long Strops Bridleway for leisure cycling, walking and dog walking is recognised. There is space along Long Strops to introduce cycling and walking infrastructure that meets the needs of disabled users and commuting cyclists and pedestrians, as well as retaining the attractive natural setting for leisure cycling, walking, dog walking, and horse riding.</p> <p>The safety of users is of paramount importance and for this reason IM4 has been amended to recommend the provision of a signalised crossing on Dobbs Lane where IM4 and IM10 meet, as well as a village gateway to slow northbound vehicles before the signalised crossing and entering Kesgrave.</p>

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IM4	Margaret Shaw	849	No	<p>I write with particular reference to the proposed cycleway through Martlesham Heath but my comments re MCAF "biodiversity" may be relevant to the other proposed routes in the strategy.</p> <p>MCAF- Safety</p> <p>I strongly oppose the junction of IM10 and IM 4 which is a very high priority route where it crosses Dobbs Lane. The cycle path crosses on a bend which occurs after a straight run from Faxhall Road. As a cyclist, walker and car driver I know that cars travel at a high speed down that road and that the approach to the bend does not give clear visibility to either the cars or to walkers and cyclists crossing the road. A cycle path crossing here will endanger lives and create an accident black spot.</p> <p>Safer alternatives would be</p> <ol style="list-style-type: none"> 1. IM10 ceases with the junction with IM7 2. IM\$ runs down the Martlesham Heath side of Dobbs Lane and crosses where there is clear visibility at the point where there is a sharp right hand turn in IM4 . 	<p>The safety of users is of paramount importance and for this reason IM4 has been amended to recommend the provision of a signalised crossing on Dobbs Lane where IM4 and IM10 meet, as well as a village gateway to slow northbound vehicles before the signalised crossing and entering Kesgrave.</p>
IM4	Martlesham Conservation Group (Phil Smith)	897		<p>IM4 (Long Strops Bridleway)</p> <p>a) The route of IM4 is currently well-used for informal recreation along its route (and formal in the region of the Millennium pavilion) including walking, Park Run and dog walking. Part of the route crosses through an area of woodland close to Dobbs Lane.</p>	<p>The importance of Long Strops Bridleway for leisure cycling, walking and dog walking, as well as wildlife is recognised. There is space along Long Strops to introduce cycling and walking infrastructure that meets the needs of disabled users and commuting cyclists and pedestrians, as well as retaining the attractive natural</p>

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				<p>This area contains a range of wildlife including reptiles especially Adders and also birds particularly Nightingales. Martlesham Conservation Group therefore has serious concerns for the wildlife in this part of the route also. Again the lighting is likely to cause issues.</p> <p>b) The Council should consider enhancing the whole route of IM4 as an expanded wildlife corridor perhaps by using land from the adjacent arable fields. It may then be possible to provide a well-designed cycling and walking route with good separation, wildlife friendly crossing features, enhanced biodiversity, room for informal recreational activities, and screening from the light pollution for residents adjacent to the route. An enhanced wildlife corridor supporting the wildlife of lowland heath between Martlesham Heath, Foxhall Heath and Rushmere Heath would prove of benefit to all three sites.</p> <p>c) IM4 should not link to IM10 for the reasons stated above. Cycle traffic should be directed northwards along Dobbs Lane to link with IM5.</p>	<p>setting for leisure cycling, walking and dog walking, and the sensitive natural environment for wildlife.</p>
IM4	Martlesham Parish Council (Diane Linsley)	878		<p>IM4 has the potential to be one of the main commuting routes into Ipswich as it is convenient for the hospital, Copplestone and St. Alban's school as well as being a direct route into Ipswich. As with IM7 the PC would like to review the informal links to Eagle Way for this route. As this utilises MHHL land they would also need to be consulted.</p> <p>See also our proposal for a cycle Express Route</p>	<p>The most appropriate detailed design solutions will need to be considered as the recommendation is taken forward. The Ipswich-Melton Key Corridor has been amended to incorporate a new recommendation (IM32), which seeks to introduce a traffic free cycling and walking route between Brightwell Lakes and Kesgrave via Bridleway 6 along Welham's Plantation to the south of Martlesham Heath.</p>

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				which would pick up the continuation of IM4 westwards (see comments against IM12 and IM25)	
IM4	Martyn Shakespeare	486	Yes	Broadly in favour, but probably not of great use unless IM1 and/or IM3 are completed.	It is noted that IM4 relies upon IM1 and/or IM3 in order to create a continuous traffic free route to and from Ipswich, which is reflected in the identification of IM1 and IM4 as very high priority recommendations.
IM4	Mary Odam	864	No	IM10/IM4 seem superfluous. There are already two cycleways into Ipswich – one along the A1214 (IM5) and the other through Grange Lane and Grange Farm (IM7). Furthermore Long Strops is a pleasant piece of land much used by dog walkers and other pedestrians. Unlike the cycle paths through the village is it not subject to hoards of schoolchildren walking and cycling at the beginning and end of the day. It is probably the only undeveloped public space in the residential part of Kesgrave and should remain so. The Covid pandemic has shown how important it is for people to have access to open spaces for exercise and recreation.	Main Road and the Grange Farm cycleway do offer good cycling and walking routes, however they also have issues. Main Road lacks priority over side streets and is alongside vehicles, while the western extent of the Grange Farm cycleway connects poorly with routes further west. IM4 would provide a traffic free cycling and walking route between Martlesham and Ipswich, in combination with IM1. The importance of Long Strops Bridleway for leisure cycling, walking and dog walking is recognised. There is space along Long Strops to introduce cycling and walking infrastructure that meets the needs of disabled users and commuting cyclists and pedestrians, as well as retaining the attractive natural setting for leisure cycling, walking and dog walking.
IM4	Michael Farahar	718		With reference to East Suffolk Council's East Suffolk Cycling & Walking Strategy - Draft 2021 consultation, I would like to support the draft's recommendations contained within S 3.15, The Ipswich to Melton Key Corridor recommendations. I would like to suggest though that an innovative and respectful approach is adopted to minimise the impact on both local residents and the natural environment when planning for the implementation of recommendations IP4, IM9, IM10 & IM12, for example by the use of low level lighting rather than	The impacts of cycling and walking infrastructure on existing residents and the natural environment have been considered throughout the preparation of the Strategy. Further consideration will be given to the most appropriate surfacing and lighting solutions as the recommendation is taken forward to delivery.

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				traditional full height lamp posts and/or lighting intensity controlled by movement sensors etc,	
IM4	Nigel Maxwell	810		<p>— please consider a cycle path connecting IF33 and IM4 using the southern perimeter of Martlesham Heath, i.e. without entering Martlesham Heath</p> <p>— this would be a very quick route from Brightwell Lakes to the start of the Kesgrave Park Run</p>	The benefits of an additional cycling and walking connection between Brightwell Lakes and Longstrops Bridleway that would be more direct and attractive than existing infrastructure are recognised. The Ipswich-Melton Key Corridor has been amended to incorporate a new recommendation (IM32), which seeks to introduce a traffic free cycling and walking route between Brightwell Lakes and Kesgrave via Bridleway 6 along Welham's Plantation to the south of Martlesham Heath.
IM4	Pat Lisseman	315	No	<p>Eagle Way has little traffic and there are very rarely cars parked at the kerbside and is perfectly safe and adequate for cyclists.</p> <p>2. IM4 - As above - cyclists already use this route and current surface conditions encourage them to cycle with due care and attention. Again as above frequently used by walkers with dogs off lead.</p>	The importance of Long Strops Bridleway for leisure cycling, walking and dog walking is recognised. There is space along Long Strops to introduce cycling and walking infrastructure that meets the needs of disabled users and commuting cyclists and pedestrians, as well as retaining the attractive natural setting for leisure cycling, walking and dog walking.
IM4	Paul Davey	322		<p>As a keen walker and cyclist I am very much in favour of ESC having a strategy for my benefit.</p> <p>However as a resident of Martlesham Heath I consider considerable concerns regarding the draft proposals published this year.</p> <p>My objections and reasons are listed below.</p> <p>1. IM4 is a duplication of IM7 and would urbanise a pleasant walk in natural surroundings.</p>	Whilst IM7 provides a high quality cycling and walking environment along the existing Grange Farm cycleway, there are limitations to the route in connecting further west. IM4 provides a traffic free route between Martlesham and Ipswich, in combination with IM1. The importance of Long Strops Bridleway for leisure cycling, walking and dog walking is recognised. There is space along Long Strops to introduce cycling and walking infrastructure that meets the needs of disabled users and commuting cyclists and pedestrians, as well as retaining the attractive natural setting for leisure cycling, walking and dog walking.

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IM4	Robin Guy	261	No	<p>Whilst the data may show that Long Stropps is well used by cyclists, it is my experience that most of these are "off road" cyclists for recreation rather than being used for commuting etc. The route is isolated and is unlikely to be used in darkness even if the cycle/path is lit.</p> <p>This area is used extensively by people walking their dogs, again the attraction is open space rather than designated metalled surfaces. There are plenty of this already! There is also a very successful Saturday Park Run and this could be adversely impacted.</p>	No changes required.
IM4	Smith, Carolyn	780		<p>Thank you for consulting on your draft cycling and walking strategy; hopefully this will enable you to learn from local knowledge and prevent you from making expensive mistakes which will damage precious heathland and adversely impact well used green spaces in the Martlesham and Kesgrave areas.</p> <p>I am in favour of encouraging walking and cycling, but not at the expense of damaging scarce habitats and the loss of local amenity, all of which will occur if the routes IM4, IM9, IM12 and IM10 are constructed in their proposed form.</p> <p>Routes IM12, IM10, IM4</p> <p>Why are you proposing surfacing over and urbanising valued green spaces, important for recreation and wildlife?</p> <p>This is in direct contravention of the SC Local Plan</p>	The importance of Dobbs Wood for wildlife and habitat preservation is recognised. A cycling and walking track could be introduced through Dobbs Wood with no or very limited vegetation removal as there exists a wide route through the Woods at presents. The most appropriate detailed design, including surfacing and lighting solutions, will need to be considered as the recommendation is taken forward. The benefits of an additional cycling and walking connection between Brightwell Lakes and Longstropps Bridleway that would be more direct and attractive than existing infrastructure are recognised. The Ipswich-Melton Key Corridor has been amended to incorporate a new recommendation (IM32), which seeks to introduce a traffic free cycling and walking route between Brightwell Lakes and Kesgrave via Bridleway 6 along Welham's Plantation to the south of Martlesham Heath.

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				<p>Policy SCLP10.1 (Biodiversity and Geodiversity).</p> <p>Namely:</p> <ul style="list-style-type: none"> • · the woodland to the west of Dobbs Lane, and Longstrops (IM4) • · Portal Woods (IM9) • · Martlesham Birch Woods (IM12), • · the heathland to the northern end of Martlesham Heath SSSI, and southern end of Martlesham Common (IM10) <ul style="list-style-type: none"> • Improve the existing A12 crossing to the north of the BT roundabout and improve IM13 to encourage the use of this route across the A12 instead of spending additional money on a crossing to the south. <p>Or:</p> <p>Extend IM13 southwards to a new bridge crossing over the A12 north of the Foxhall Road/Waldringfield Road roundabout, and continue on a new route through the edge of the field alongside Foxhall Road to Bell Lane, and continue along the field edge to connect with IM4 west of Bell Lane.</p> <p>Construct a bridge crossing the A12 to connect IM13 with footpath PROW6, re-route PROW6 alongside the wooded area to the south and then across to</p>	

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				<p>Foxhall Road, and continue eastwards along the field edge towards Bell Lane as above.</p> <p>Both of these options give an opportunity to construct a wildlife corridor alongside this route, with screening trees and wildflower planting.</p>	
IM5	-	275	Yes	I would fully support the priority of cyclists over the side streets .I would support overland priority at the roundabouts , rather than divert cyclists down the underpass , this can be reserved for pedestrians.	Whilst the diversion of cyclists and pedestrians to underpasses may not be considered the highest standard of cycling and walking infrastructure, the segregation from vehicles is effective and for this reason the Strategy does not propose to amend these existing underpasses.
IM5	Alasdair MacLeod	536		IM5 is "high priority", it should be "very high" as this is the path taken by most cycle commuters (as it feeds off Woodbridge Rd in Ipswich) and is used by Children traveling to Kesgrave High School.	IM5 currently offers the best cycling and walking experience and therefore generates the greatest cycling and walking trips. However, if IM4 and IM1 were implemented this route would likely generate a greater number of cycling and walking trips as it offers a traffic free route. For this reason IM4, and IM1, are identified as higher priorities than IM5.
IM5	Caroline Bickers	394	Yes	The segregation between cyclists and pedestrians needs to be made much clearer than currently. All road users need to understand who has priority at junctions. The cycle way needs to be maintained and cleaned. This does not happen at the moment.	It is agreed that clarity is required between cycling and walking infrastructure to reduce conflict between users. For this reason IM5 has been amended to provide clear demarcation between cycling and walking infrastructure.
IM5	Chris Adelson	443	Yes	Ensure travel is easy in both directions. Any special provisions needed when school is opening or closing?	Desirable and safe cycling and walking in both directions is an important element of a successful Main Road cycling and walking route. For this reason IM5 has been amended to provide clear demarcation between cycling and walking infrastructure. In relation to provisions for school opening and closing times, the existing Main Road underpass offers continuous access between the school and Main Road cycleway.

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IM5	Martlesham Parish Council (Diane Linsley)	879		<p>As currently constructed on the south side of the A1214, IM5 has too much conflict with motor traffic, including that from side roads, to be a safe through route. Apart from essential safety improvements, it should be left as it is, serving local movements and KHS. However, if a through cycle route could be established on the northern side, we would welcome this, as it would be convenient for residents in the northern part of Martlesham and provide a more direct route along the whole corridor.</p> <p>It would however, require several crossing points (including near the top of Portal Ave). Improved lighting in the underpass connecting to IM8 is essential. It is noted that the potential for improvements to the footway on the A1214 between the top of Portal Avenue and the start of the underpass ramp may be restricted in places by the presence of telecoms equipment. If land was made available from the police site at the corner of Portal Avenue and the A1214, this could allow the connection from IM5 to IM8 to be improved and made safer. This work may be achieved as part of the road improvement required if the Police HQ site is redeveloped, or as part of the A12 junction improvements.</p>	While a cycling and walking route along the northern side of Main Road would provide a more continuous route with less conflict with side streets, there would need to be a number of crossing points to access the route. It would also require significant investment above that which would be required for the proposed improvements to the existing Main Road cycling and walking track, and which therefore would not be able to fund other much needed infrastructure.
IM5	Martyn Shakespeare	490	Yes	Consistency and clarity of what is cycle and pedestrian route and resurfacing would improve this route.	It is agreed that clarity is required between cycling and walking infrastructure to reduce conflict between users. For this reason IM5 has been amended to provide clear demarcation between cycling and walking infrastructure.
IM5	Robin Guy	262	Yes	Moist commuting cyclists do not use the designated cycle way, probably because there are so many	Ensuring cyclists and pedestrians have priority over Main Road side streets is highly important to a safe and

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				road crossings. If those crossings were made more safe with pedestrian & cyclists priority this would reduce congestion on the Kesgrave Road. School children are the largest user of the existing cycle way but tend to cycle more slowly.	desirable cycling and walking route. So too is the need for clear demarcation between cyclists and pedestrians to reduce conflict between users. For this reason IM5 has been amended to provide clear demarcation between cycling and walking infrastructure.
IM5	S Hall	649	No	<p>Comment regarding 3.19 and IM5. IM2 IM1 and IM4:</p> <ul style="list-style-type: none"> • Within this Key Corridor the Propensity to Cycle Tool (PCT) map shows the importance of Main Road, Kesgrave as the most popular route for cyclists and also capable of significant modal shift. <p>If Main Road Kesgrave was identified as the most popular route for cyclists why is it not given the status of Very High Priority? Surely the relative priorities given to Main Road Kesgrave IM5 & Woodbridge Road IM2 (High priority) and IM1 and IMF4 routes across Rushmere Heath and Long Strops (a Very High Priority) are the wrong way round?</p> <p>Can you also clarify whether any work to IM4 would be dependent on there being housing development there? I'm concerned it could pave the way (literally) to housing development.</p>	<p>IM5 currently offers the best cycling and walking experience and therefore generates a significant number of cycling and walking trips. However, if IM4 and IM1 were implemented this route would likely generate a greater number of cycling and walking trips as it offers a traffic free route. For this reason IM4, and IM1, are identified as higher priorities than IM5. The role of the Strategy is to identify new and improved cycling and walking infrastructure, rather than land for development of a particular use. Allocating land for development is the role of Local and Neighbourhood Plans.</p>
IM6	Martyn Shakespeare	491	No	<p>I don't think it will be used. Shared use existing path adjacent Bell Inn/All Saints' Church probably good enough.</p>	<p>IM6 was intended as a potential alternative option to Main Road if space for cycling and walking infrastructure along Main Road between Bell Lane and Ropes Drive would not support high quality cycling and walking infrastructure. However, given IM6 lacks directness it would likely force cyclists onto the carriageway of Main Road. For this reason IM6 has been removed from the Strategy.</p>

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IM6	Nik Bestow	15	No	There is no way I would use this diversion.	IM6 was intended as a potential alternative option to Main Road if space for cycling and walking infrastructure along Main Road between Bell Lane and Ropes Drive would not support high quality cycling and walking infrastructure. However, given IM6 lacks directness it would likely force cyclists onto the carriageway of Main Road. For this reason IM6 has been removed from the Strategy.
IM6	S Hall	652	No	<p>This is a terrible idea if you are suggesting that cyclists should be diverted away from Main Road along a detour north of the cemetery. Is that the proposal?? If so, what happened to the principles of direct and convenient cycle routes? This proposal is an absolute "no" in terms of government policy. And how many people would actually use it in practice? Please abandon it and re-think, if this is what you are proposing.</p> <p>Why not instead, reduce the carriageway width here to a single lane for motor traffic and provide cyclists with dedicated space on the road. There are various ways of managing single track sections of road and I am not an expert on whether traffic lights or other measures would be appropriate here - but please can engineers have a look at this possibility?</p> <p>This would enable pedestrians to have a wider footway here which is also needed - especially on the south side of the A1214 in the vicinity of the crossing at Bell Lane and All Saints Church.</p> <p>And pedestrians also need a means of crossing the road from All Saints Church on the south side to the</p>	IM6 was intended as a potential alternative option to Main Road if space for cycling and walking infrastructure along Main Road between Bell Lane and Ropes Drive would not support high quality cycling and walking infrastructure. However, given IM6 lacks directness it would likely force cyclists onto the carriageway of Main Road. For this reason IM6 has been removed from the Strategy.

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				<p>cemetery, businesses and bus stop on the north side of the A1214. Pedestrians also need a route on the north side which provides good access into the cemetery.</p> <p>A table top design between Dr Watsons Lane and Ropes Drive with zebra crossing/s could possibly work. A table top would slow traffic down - and it would be single lane here only in my proposal. This would make it easier for people to cross the road. This area is well-used by pedestrians and they should, in my view, have much more priority here. All Saints Church is a significant community hub in Kesgrave. If a table top layout ran from just to the west of Dr Watsons Lane this could also make turning in and out of Dr Watsons easier. (It can be problematic). Finally this area by the Bell Public House has high levels of air pollution. In some readings I have seen readings that have exceeded legal limits - before being averaged out. My proposal would hopefully help to reduce air pollution. Can a table top idea with single carriageway and zebra crossings please be considered?</p> <p>The solution to all of this is surely lies within a redesign of the road itself. A northern detour is not the answer.</p>	
IM7	Chris Adelson	449	Yes		Support noted.
IM7	Martlesham Parish Council (Diane Linsley)	880		<p>IM7 has too much conflict between pedestrians, primary and secondary school pupils to be a fast cycle commuter route and has inadequate links at either end to be part of a longer range route. Currently, there are informal links to this from the</p>	<p>While the Grange Farm cycleway provides a high quality cycling and walking route, there are challenges in providing a comprehensive route beyond the cycleway. IM4 provides for continuous cycling and walking</p>

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				<p>Heath, bypassing the Broomfield bottleneck. The PC would like to review this area with ESC on a site visit and MHHL would need to be consulted, as their land is affected. Photograph above [see attached pdf] shows heavy mixed traffic at peak times on IM7 past east side of Gorseland School.</p> <p><i>The images have not been published due to potential data protection concerns, but these were still fully considered and assessed in forming the Strategy.</i></p>	between Ipswich and Martlesham, and for this reason retains the highest priority in this area.
IM7	Martyn Shakespeare	492	No	Lankester Way too narrow and wrong choice for this. I either continue parallel to bus way to join Bell Lane at 12 Acre Approach junction, then use Bell Lane, or use cycle track along Ropes Drive and via Church, which I consider OK. Excellent path through Grange Farm needs some resurfacing in places because of tree root damage.	A shared path is currently in place on Bell Lane between Twelve Acre Approach and the Heath Primary School entrance. The width of Bell Lane presents challenges in introducing further cycling and walking infrastructure improvements without reallocating road space away from vehicles. IM7 has been amended to highlight the opportunities to improve cycling and walking infrastructure on Bell Lane if road space could be reallocated away from vehicles.
IM7	Nik Bestow	16	No	Agree with the importance of IM7 but the route should continue parallel to the guided bus way up to Bell Lane as it does today (Cycle Rote 1) and then turn right along Bell Lane. Add a cycle path along this part of Bell Lane.	A shared path is currently in place on Bell Lane between Twelve Acre Approach and the Heath Primary School entrance. The width of Bell Lane presents challenges in introducing further cycling and walking infrastructure improvements without reallocating road space away from vehicles. IM7 has been amended to highlight the opportunities to improve cycling and walking infrastructure on Bell Lane if road space could be reallocated away from vehicles.
IM7	Pat Lisseman	314	No	* When walking the children to school it is already a hazard with cyclists tearing past Gorseland School - very few slow down.	The existing cycling and walking network in this area is of a high quality, however it can be improved and the wider network could be made more dense lessen the cycling and walking trips along the same route, thereby reducing conflict between pedestrians and cyclists. The

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					recommendations through this area will facilitate a safer walking and cycling environment for all users.
IM7	Paul Jordan	8	Yes	<p>I believe that the existing cycle path south from Gorseland school is currently designated on the definitive map only as a footpath and not a bridleway. This needs to be upgraded. See https://www.bhsaccess.org.uk/dobbin/pathdetails.php?id=east/2026Suffolk&password=2026&pathid=SU-1055&gridref=TM2386945478&notes=</p> <p>I have raised this issue with Martlesham Parish Council</p>	The cycling and walking track alongside Gorseland Primary School is not identified on the Suffolk Definitive Map as a Footpath or a Bridleway.
IM8	Caroline Bickers	395		The A12 underpass here is dirty and poorly maintained. The undergrowth has been allowed to encroach so far that the path and cycle way are severely limited.	The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.
IM8	Chris Adelson	450	Yes	<p>Improve sight line as there is a tight corner when travelling east side from Portal Avenue. Emergency vehicles are known to appear out of Portal Avenue and there is no warning for cyclists who might not know the area.</p> <p>Narrowing Main Road on the Martlesham side of the A12 might encourage people to keep within the speed limits.</p>	Support noted. The intention of IM8 is to improve safety for cyclists and pedestrians along Main Road and in particular at the Portal Avenue/Main Road junction, as well as make the route more desirable to cycle and walk.
IM8	Jonathan Clyne	390	No	There are already 3 crossing points over / under the A12, all of which are already suitable for cyclists and walkers. Time and money would be better spent on maintaining the existing crossings.	The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.

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IM8	Martlesham Parish Council (Diane Linsley)	881		<p>IM8 extends from the A12 underpass to Crown Point.</p> <p>With the exception of the stretch from Portal Avenue to the underpass, this is not a bad route for cyclists. Improvements could be made to that stretch with land taken from the police site at the top of Portal Avenue, which at the same time could create a wider radius turn to improve visibility for cyclists going up Portal Avenue and turning right at the top.</p> <p>Where the subway cycle route joins the A1214 east of the A12, it is often necessary to stop on the slope to wait for a gap in the traffic. Starting off again on that slope can be difficult for some cyclists and an improved arrangement would make it easier to use.</p> <p>East of the underpass, the road is wide and needs to be calmed. Some additional crossing islands would help to change its wide open character and allow residents safer access to facilities on either side of the road, eg near the Crown Point bus stop.</p> <p>We do not agree with the removal of turning lanes. All of them serve a practical purpose and the associated traffic islands are useful for people crossing the road.</p>	<p>The proposal to consider narrowing Main Road and consider removing turning lanes would have the effect of slowing vehicle speeds, thereby making for a safer cycling and walking environment. Removing islands and replacing with cycling and walking crossing points would be an important aspect of such a scheme. IM8 has been amended to recommend a cycling and walking track is introduced between the A12 underpass and the existing infrastructure along the southern side of Main Road to ensure a coherent cycling and walking route.</p>
IM8	Martyn Shakespeare	509	No	<p>Suggest better to make use of existing service lane south of Main Road for majority of route, joining path under underpass via path on edge of common. Support improvement of Portal Avenue side.</p>	<p>IM8 has been amended to recommend a cycling and walking track is introduced between the A12 underpass and the existing infrastructure along the southern side of Main Road to ensure a coherent cycling and walking route.</p>

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IM8	Nik Bestow	17	Yes		This comment of support has been noted.
IM8	Stephen Denton	480	No	<p>I support it in part (especially cycle crossing point where tunnel emerges on to Main Road), also widening of existing cycling and walking infrastructure immediately east of the Portal Avenue junction.</p> <p>I object to removal of turning lanes unless this part of a comprehensive strategy to make the whole of the A1214 unattractive to those rat running through there, and make the A12 more attractive if the planned A12 improvements achieve their objective.</p> <p>The islands at those turning lanes are useful for pedestrians crossing the road to get to shops etc. More island refuges are needed especially near Crown Point.</p>	The proposal to consider narrowing Main Road and consider removing turning lanes would have the effect of slowing vehicle speeds, thereby making for a safer cycling and walking environment. Removing islands and replacing with cycling and walking crossing points would be an important aspect of such a scheme. IM8 has been amended to recommend a cycling and walking track is introduced between the A12 underpass and the existing infrastructure along the southern side of Main Road to ensure a coherent cycling and walking route.
IM9	Alan Pitt	597	No		Objection noted.
IM9	Alasdair MacLeod	538	No	<p>Part of IM9 runs over the original perimeter track of the Martelsham Heath airfield. That surface should be preserved as it is part of the history of Martlesham Heath.</p> <p>Light pollution: I am against adding lighting to the route through the woods. Both because lighting is harmful to wildlife and because light pollution is harmful to residents mental health.</p>	The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whist being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Andrew and Simone Moore	856	No	The present WW2 perimeter track owned by us through Portal woods is wide enough for cyclists and walkers to co exist with safety but having this lit and a better surface laid will enable cyclists to increase	IM9 recommends repairing the existing hard surfaced track through Portal Woods whist being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to

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				their speed and is not conducive to either safety nor the natural preservation of our environment.	only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Beaumont, David	361	No	<p>Response to Public Consultation on Proposed Travel Routes IM9(part), IM10 & IM12</p> <p>I am totally OPPOSED to routes IM9 (part that links Gorseland School to Portal Avenue), IM10 & IM12 as proposed by East Suffolk Council (ESC) on the grounds that they:</p> <ul style="list-style-type: none"> a) endanger a protected Site of Special Scientific Interest (SSSI), known as Martlesham Heath’s ‘Western Corridor’ b) endanger carefully conserved woodlands, known as Martlesham’s Portal Woods, Birch Woods and the woodland between Coopers Road and Lancaster Drive Hamlets c) are contrary to ESC’s objectives to protect the existing ecology and counter Global Warming d) have been proposed with no prior consultation with the landowners, Martlesham Heath Householders Ltd (MHHL), of which the residents of Martlesham Heath Village are shareholders e) are unsafe for both pedestrians and road users of Eagle Way, particularly near Birchwood School and the proposed crossings either end of Birch Woods. Furthermore, locating the crossing of Dobbs Lane by IM10 on a ‘blind corner’ is totally irresponsible. <p>I will now expand on the above issues.</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>

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				<p>A) The SSSI and immediate vicinity As ESC planners well know, the ‘green land’ west of the Martlesham Heath’s housing to the boundary of Dobbs Lane is a protected SSSI. Consequently, this SSSI is very sensitive to ‘overuse’ for recreational purposes and thus efforts are consciously made to not exploit easy access. This land is leased by MHHL, which has set up as a charity, SSSI Ltd, to manage the site with guidance from Natural England. Martlesham residents respect and regard it as a privilege to act as guardians of this SSSI which is one of the few remaining lowland heath sites in the country and the breeding ground for an endangered rare butterfly, the Silver Studded Blue.</p> <p>Whilst route IM10 does not seem to cross the SSSI it does run along its northern border abutting MHHL land which MHHL has purposely kept as natural as possible whilst accommodating access in order to respect the proximity of the SSSI. Reptiles and valuable flora are in abundance on this northern border of the SSSI; in fact it has been reported that there is a relatively large population of Adders located and breeding in this area.</p> <p>I am annoyed that planners regard ‘informal trails’ shown on various maps of the SSSI and nearby area as ‘paths’ which they ARE NOT, unless specifically designated as such. This includes the area of MHHL land between Eagle Way and the footpath/cycleway that run North-South on the eastern boundary of Gorseland School.</p>	

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				<p>Any ‘urbanisation’, with wide paved paths, cycleways and lighting between Dobbs Lane and Eagle Way will encourage ‘overuse’ and endanger the carefully controlled ecology of this area which MHHL and conservancy groups of volunteers have nurtured for decades, on occasion using they own money.</p> <p>B) The Woodlands Firstly, part of Route IM9 that connects Gorseland School and Portal Avenue is part of the old airfield perimeter track and has historical significance. This area is heavily wooded and is conserved by the Portal Conservation Group which has made significant effort to maintain this area as a wildlife sanctuary. This area is also used as an informal education area for school children to understand the importance of nature and the ‘quiet environment’ it bestows encourages flora and fauna like ‘Muntjac deer’, reptiles, ‘glow-worms’ and numerous bats. The transformation of this ‘track’ into a heavily used, paved and worse still, lit ‘travel route’ will damage this habitat.</p> <p>Secondly, part of IM12 that connects the western side of Eagle Way to IM13 scythes through one of Martlesham Heath’s prime ‘carbon sinks’, known as Birch Woods. This wood has been preserved for low impact recreational walking and has a multitude of mature birch trees amongst other varieties which will be destroyed by the proposed wide paved footpath/cycle route resulting in light pollution and possible flooding.</p>	

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				<p>IM12 then proceeds eastwards to further destroy a purposely created natural barrier separating Coopers Road and Lancaster Drive to cross the A12 and link up with IM13. This ‘barrier’ was conceived as a natural barrier between ‘Hamlets’ as part of the original approved plans for Martlesham Heath, as well as acting as a ‘sound barrier’ for the busy A12. A ‘travel route’ through this ‘barrier’ would change the demarcation of the two Hamlets making them appear to be contiguous and worse still act a ‘sound tunnel’ concentrating the noise pollution of the A12 traffic into the heart of Martlesham Heath.</p> <p>Another bridge crossing the A12 is not necessary as one already exists (IM11). Another bridge would be expensive and offer little benefit but cause major disruption and destruction of a mature, natural, ecologically sound environment. Furthermore, the Brightwell Lakes development planning approval stipulated a need for a controlled crossing/junction on the A12 as the main access to the development, connecting it to the existing ‘bridleway’ on the western side of the A12.</p> <p>C) Ecology and Global Warming policy ESC is guilty of sending out mixed messages on policy. On the one hand it is lauding itself as encouraging the protection of the ecology to help fight Global Warming and on the other hand it then proposes ‘sustainable travel solutions’ which will damage the careful nurtured ecology of Martlesham Heath. Martlesham Heath was never designed to</p>	

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				<p>have an east-west urban cycle route running through it: <i>‘throwing the baby out with the bath water’</i> comes to mind.</p> <p>Is ESC serious about protecting the District’s ecology and supporting and encouraging the public who are willing to offer their time and money in helping?</p> <p>D) Lack of consultation I believe that the publication of this “Strategy Proposal” was the first anyone in Martlesham was made aware of the major impact these ‘Travel Routes’ would have on the Parish. I find it astonishing that ESC hasn’t contacted the affected landowners earlier to establish if there were any extenuating circumstances why these proposals should or should not be considered.</p> <p>E) Safety issues Is ESC seriously considering that the northern section of IM12 using Eagle Way is a sensible and safe option for pedestrian, cyclists and other road users? If so, then it is ill-conceived.</p> <p>Anyone who uses that section of Eagle Way could tell ESC that this proposal is a disaster waiting to happen. This section of road is heavily used by young school children, their parents, the elderly, and others. There is widespread street parking on this section of Eagle Way in school-time with parents ‘dropping off’ children, school events, etc. Furthermore, this is a bus route and a major arterial</p>	

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				<p>road for Martlesham Heath residents and their vehicles getting on to the A12. The road itself was not designed to accommodate parked traffic, parked buses, a commercial bus route <u>and now a proposed cycle route.</u></p> <p>Another safety issue is the location of the crossing of Dobbs Lane by IM10 on a 'blind corner' which is totally irresponsible. This is a busy 'rat run' for commuters and anyone using the proposed crossing will be obscured from motorist's view right up until the last moment due to foliage and trees in the vicinity.</p> <p>Conclusion It seems obvious to me that these 'travel routes' have been conceived and proposed as a 'desk based' exercise to get from 'A to B' using any 'available' green space, probably viewed using Google maps' satellite view and without consulting the landowners concerned or environmental agencies like Natural England. I have come to this opinion based on previous suggestions for path routes by planners of ESC and Suffolk County Council (SCC) when trying to link Brightwell Lakes development to Grange Farm. ESC are rightly proud of their respect for protected environments (e.g. SSSIs), why do they insist on using Martlesham to 'mope up' previous strategic planning inadequacies with a 'spaghetti junction' of paved, lit footpaths/cycleways?</p> <p>It should be noted that the Brightwell Lakes planning</p>	

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				<p>approval was based on an 'A12 Access Crossing' as the main entrance to Brightwell Lakes which incorporated a pedestrian/horse/cycle facility to the existing bridleway opposite. No mention or agreement had been made to have another crossing across the A12 linking into Martlesham Heath as one already existed and was part of the National Cycle Network.</p> <p>The proposed crossing of the A12 via IM12 would have probably been adamantly opposed by residents and Martlesham Parish Council to the Brightwell Lakes Development if it had been proposed and now this 'Sustainable Travel' proposal could be seen as a means of the ESC getting a crossing by stealth. Is this how ESC wants to be seen to be planning infrastructure?</p> <p>I hope the above comments are considered and are helpful in shaping ESC's Draft Cycling and Walking Strategy.</p>	
IM9	Caroline Bickers	396	Yes	<p>Again any work here will need to be maintained to a far better standard than is presently the case. Turning in and out of the underpass towards Tesco's (blind corner) is dangerous for both pedestrians and cyclists.</p>	<p>The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.</p>
IM9	Caroline Miller	485	No	<p>There is already a paved route around the police headquarters over to the existing cycling track at Dobbs Lane so I would be against any removal of woodland just to straighten it out.</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation.</p>

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					Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Chris Adelson	451	Yes	Avoid tree removal at all costs. This area has already had trees removed to it's detriment.	The importance of the natural environment and in particular the avoidance of tree and other vegetation removal is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	David Foster	22		This path is the property of Martlesham Heath Householders Ltd from the kink inn the fence towards Birchwood school. Why have you not consulted the land owners?	The purpose of the consultation on the Draft Strategy was to give all interested parties the opportunity to comment on proposed cycling and walking infrastructure recommendations.
IM9	Fosker	209	No	Please don't spoil this beautiful wood they is the jewel of amazing animal life. It's quality is that it is perfect to cycle on for mountain bike and young kids. It is full of history and this will so,spoil it. It shouldn't be allowed, absolutely not, please.	The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Ian Dudley	415	No	I wish to lodge my objections to the above proposals concerning Martlesham Heath / Brightwell	In preparing the draft Strategy, officers considered the benefits to cycling in respect of improved journey times,

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				<p>Lakes development for the following reasons:</p> <p>There is already a footbridge bridge across the A12 which , in my opinion, is not the most used or popular way to cross!</p> <p>To build a new bridge across the A12 is, in my opinion, a waste of public money. Who would, be responsible for the upkeep?</p> <p>Council tax rates are high enough!</p> <p>The proposed new foot/cycle path routes through the village, would create in the region of 1 mile of tarmac cycle/footpaths with them cutting through Land owned and managed by Martlesham Heath Householders Ltd, on behalf of the resident membership.</p> <p>The proposed bridge would necessitate the creation of another path to continue through Birch Woods (Martlesham Woods) and would require the felling of at least 25 mature trees, an action that is not very environmentally friendly!</p> <p>Where the proposed path would exit near Forest Lane on Eagle Way, cyclists and pedestrians would be tempted to take, (on their way to Dobbs Lane)</p> <p>a 'short cut' across our village's SSI (Site of Special Scientific Interest) which is maintained by MHHL and numerous volunteer residents.</p>	<p>and an attractive and safe route through the wooded area away from vehicles to outweigh the negative aspects in relation to low level lighting through the wooded area between Eagle Way (west and east), increased human presence in the wooded area harming wildlife, and potential conflicts with pedestrians.</p> <p>The existing cycling and walking bridge over the A12 is an important connection between the areas east and west of the A12 and lessens the need for a cycling and walking bridge between Brightwell Lakes and Martlesham Heath. However, a new bridge would provide an additional and direct connection between Brightwell Lakes and Martlesham Heath, and west towards Ipswich, which would be of significant value. Having reflected on consultation responses and considered these matters further, officers have amended IM12 to avoid recommending the wooded area for cycling and instead recommend cycling and walking improvements to Eagle Way. The recommended cycling and walking bridge over the A12 remains part of IM12, but is proposed to be accessed from Eagle Way and not through the wooded area between Eagle Way (west and east).</p>

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				<p>Why is there a need for a new lit tarmac pathway route around the perimeter of Portal Woods ?</p> <p>WHY is a new lit tarmac route from Dobbs Lane along the back of Whinfield /</p> <p>Broomfield leading to Eagle Way a necessity?</p> <p>I can think of no reasons that the above proposals should be accepted, indeed, the proposed exercise is another example of humans interfering with the environment when it is unnecessary.</p> <p>If people want to cycle or walk, they are able to do so within the currently available cycle/footpaths!</p>	
IM9	Janine Davey	343	No	<p>I would like to object to the planned cycle route through Birch Woods, Martlesham Heath (referred to in your document as Martlesham Woods). This is a precious amenity area for local residents and an area of natural woodland. The proposal of felling mature trees to make way for the cycle route along with construction of lighting will damage the woodland and disturb wildlife. Where the path would exit on Eagle Way, cyclists and pedestrians would be tempted to take a short cut across our SSSI on their way to Dobbs Lane.</p> <p>The same argument applies for the lit tarmac routes through Portal woods and near Dobbs Lane. Our natural areas and wildlife are constantly under threat from these types of schemes and would urge you to re-visit this decision.</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>

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				<p>I see the proposed schemes as an unnecessary waste of public money seeing as there is a perfectly acceptable existing through-cycle route from the A12 to Grange Farm.</p> <p>No doubt you will be conducting the necessary surveys and producing data to justify this expense. The local residents would be very interested in your findings.</p> <p>Please read and carefully consider any objections you receive and not treat this consultation as a 'box-ticking' exercise.</p>	
IM9	John Kelso	776		<p>Any proposal to provide a lit and hard surface through Portal woods should be denied as it would prove detrimental to existing wildlife.</p> <p>Are ESC aware of SCC highways proposal to create a new bus route between A12 Tesco roundabout and A1214 via Portal Avenue in order to stop bus delays at A12/ A1214 roundabout?</p> <p>If so I would suggest that pedestrians, cyclists and buses are not a good mix to have on this narrow road (Portal Avenue) and infact could prove dangerous.</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whist being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p> <p>The Strategy has been prepared with knowledge of plans to improve bus connectivity in the area. Bus routes and cycling and walking infrastructure can be introduced alongside each other.</p>
IM9	Jonathan Clyne	391	No	<p>The existing track north and west of the Police QQ are perfectly adequate and there is no need to upgrade it. Any lighting is also unnecessary, being especially detrimental to the wildlife which local residents have struggled to encourage. Once the</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whist being</p>

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				trees are cut back, resurfacing has taken place and lights installed, the environment will be destroyed for ever.	sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Justine Lewis	808	No	Lit tarmac paths through established woodland would change the character of the woods. Urbanisation would discourage wildlife. The majority of residents who enjoy the woodland and heathland areas for dog walking and leisure, without having to drive, would instead have to drive to other heathlands such as Sutton Heath and Rendlesham - more cars on the road.	The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Lesley Vince	339	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this area are minimal in comparison to the loss of habitat/habitat changes.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggested that a new cycling and walking bridge would not be necessary in close proximity to an existing cycling and walking bridge. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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IM9	Lesley Vince	340	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this area are minimal in comparison to the loss of habitat/habitat changes.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggested that a new cycling and walking bridge would not be necessary in close proximity to an existing cycling and walking bridge. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM9	Linda Ledgerwood	369	No	<p>I do not support the proposed route through birch woods and portal woods. These woods are used by us all for walking and for children playing. There are lots of deer and wildlife that live in these woods. If a 4.5 metre lit cycle path was put through the woods it would have a detrimental impact on the wildlife. It would also be dangerous for young children playing and the older generation walking if cyclists who can cycle up to high speeds are whizzing through these woods. This is a well maintained natural woodland which has recently had native bluebells planted as well. In this day and age with climate change so high on the agenda any loss of trees and natural vegetation will impact more. The existing cycle path from the industrial estate over the main road through the roads of Martlesham Heath gives a</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>

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				completely viable route to Kesgrave without endangering anyone or any wildlife.	
IM9	Martlesham Conservation Group (Phil Smith)	895	No	<p>IM9 (Gorseland school – Portal Avenue)</p> <p>The Portal Woodlands area has been managed as a wildlife conservation area for many years by the Portal Woodlands Conservation Group, a sub-group of Martlesham Parish Council. Portal Woodlands is an area of woodland that supports a wide range of species including birds (e.g. Nightingales), insects including butterflies (e.g White Letter Hairstreak, Purple Hairstreak, Comma, Holly Blue) and Glow Worms (reported in good numbers over many years), reptiles, and mammals including several bat species.</p> <p>The proposed route through the woodland would lead to degradation of the wildlife due to increased disturbance from extra traffic, and light pollution from any permanent lighting on the pathway would lead to a serious impact on the Glow worm population as well as other species.</p> <p>The area is used by local groups including children from Gorseland School, and the Portal Woodlands Watch Group for natural history education purposes. Creating an official route would encourage more cyclists and encourage them to travel at high speed through this area, which would make it difficult for groups of people to use this area safely.</p> <p>Route IM9 should be withdrawn. The existing</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>

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				atmosphere promoting calm, quiet access to nature should be retained.	
IM9	Martlesham Heath Householders Ltd (Sir/Madam)	303	No	<p>IM9</p> <p>25. The Board also totally rejects the proposal of route IM9. The route is along a WW2 perimeter airfield track and has already permitted route status. From the photographs you will see that the track is wide and the surface is tolerable for bicycles.</p>  <p>26. The Board feels that any development with extra tarmac and lighting through the woods would detract from the area around the Control Tower Museum, which we consider to be a heritage area, giving a flavour of how Martlesham Airfield was. Martlesham Heath Aviation Society has been consulted on the proposal for IM9 and is also totally against it. What we have at the moment is very acceptable. Indeed, Portal Woods Conservation Group, we are sure would not be in favour.</p>	The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward. The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.

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				 <p>Conclusion</p> <p>27. MHHL is happy to take a positive approach to some of the wider ESC proposals and engage constructively with officers from the Council.</p> <p>28. The proposals seek to create new cycle/footpaths but say nothing about maintenance of existing paths. The Board hopes that funds are made available to improve and maintain existing cycle/footpaths along the Ipswich/Melton corridor, thereby meeting the needs of the local communities.</p>	
IM9	Martlesham Parish Council (Diane Linsley)	882		<p>IM9 along the western and northern side of the police HQ site should form the main continuation of IM4 and IM7 to join IM8 as the principal E<>W through route for the corridor. This would avoid conflict on Eagle Way, the bottleneck at the Tesco underpass and routing of through traffic on Martlesham Common Local Nature Reserve.</p> <p>The removal of the current surface seems</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting</p>

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				unnecessary and any problems could be addressed with remedial patching. Any lighting should be at the lowest practical level and have regard to the fauna in the woods (especially bats and glow worms).	solutions will need to be considered as the recommendation is taken forward.
IM9	Martyn Shakespeare	494	No	Yes to resurface route between Portal Avenue and Eagle Way (and better lighting if possible). No to lighting track around Police HQ, nicer to leave this as is.	The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whist being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Michael Farahar	719		With reference to East Suffolk Council's East Suffolk Cycling & Walking Strategy - Draft 2021 consultation, I would like to support the draft's recommendations contained within S 3.15, The Ipswich to Melton Key Corridor recommendations. I would like to suggest though that an innovative and respectful approach is adopted to minimise the impact on both local residents and the natural environment when planning for the implementation of recommendations IP4, IM9, IM10 & IM12, for example by the use of low level lighting rather than traditional full height lamp posts and/or lighting intensity controlled by movement sensors etc,	The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whist being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Moira Weaver	416		I am a frequent cyclist and live in Martlesham Heath. Whilst I absolutely support a revised and improved network as outlined on your site, I absolutely do NOT approve of plans to put your routes as	In preparing the draft Strategy, officers considered the benefits to cycling in respect of improved journey times, and an attractive and safe route through the wooded area away from vehicles to outweigh the negative

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				<p>proposed through the Heath.</p> <p>In particular I and my husband object to the planned route through Birch Woods (you call it Martlesham Woods). This is a precious area of natural woodland where you are proposing the felling of at least 25 mature trees, and lighting that will disturb wildlife. The exit would tempt people to take a short cut across our SSSI. I cannot see the logic of this proposal, particularly in view of the urgency to preserve our threatened wildlife on our planet.</p> <p>The same illogic and I would say, brutality, seems to apply to the suggestion to cut through Portal Woods.</p> <p>I appeal to your consciences to consider the future impact of your proposals. Neither of these paths are necessary for a good cycling experience in this area.</p>	<p>aspects in relation to low level lighting through the wooded area between Eagle Way (west and east), increased human presence in the wooded area harming wildlife, and potential conflicts with pedestrians. Having reflected on consultation responses and considered these matters further, officers have amended IM12 to avoid recommending the wooded area for cycling and instead recommend cycling and walking improvements to Eagle Way. The recommended cycling and walking bridge over the A12 remains part of IM12, but is proposed to be accessed from Eagle Way and not through the wooded area between Eagle Way (west and east).</p>
IM9	Mr & Mrs D Evans	904		<p>Similarly you indicate a possible route for walkers and cyclists from Eagle Way at the “Tesco” roundabout to the A1214 designated IM9 with a spur to IM5. This seems to follow an identical SCC proposed route for the specific 66/66A bus route to by-pass the A12 “police HQ” roundabout. Will these two uses be compatible and more importantly safe for pedestrians and cyclists?</p>	<p>The proposed cycling and walking infrastructure improvements could be implemented alongside a bus route, whilst also prohibiting other vehicle through traffic.</p>
IM9	Paul Davey	325		<p>As a keen walker and cyclist I am very much in favour of ESC having a strategy for my benefit.</p> <p>However as a resident of Martlesham Heath I considerable concerns regarding the draft proposals published this year.</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation.</p>

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				<p>My objections and reasons are listed below.</p> <p>4. IM9 via Portal Woods would be a gross urbanisation of an attractive walking area already threatened by housing development.</p>	<p>Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>
IM9	Philip Wilmot	238	No	<p>Re "Introduce a cycling and walking track and lighting to existing track north and west of Suffolk Police HQ", this proposal risks undermining the wildlife habitat afforded by Portal Woods. Also, would it survive Passmore's attempt to sell the Police HQ and build on the land?</p> <p>Re "Introduce a cycling track along Portal Avenue, avoiding tree removal where possible", surely anyone with half a brain would avoid tree removal where possible simply in order to minimise construction cost. The reference to tree removal is nothing more than an attempt to signal concern for the environment.</p> <p>Re "Resurface route between Portal Avenue and Eagle Way including a modal filter if necessary", as far as I know this is already a route for pedestrians and cyclists only, and there is no modal filter, so the possible need for one suggests significant widening which I would oppose.</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward. The existing cycling and walking track through Portal Woods lies outside the Police HQ site, however it is important that cycling and walking connections are made between the track and the site.</p>
IM9	Robert Fulcher	733	No	<p>Much as I applaud moves to improve cycling routes I must express my objections to two of your planned routes.</p> <p>Route IM 12 through Birch Wood is unnecessary and very damaging to wildlife and nature. The wood in spring and summer has a very high density of nest</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation.</p>

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				<p>sites including many summer migrants. Many of these migrant birds are already suffering a decline in numbers. The removal of many trees and the subsequent construction of this footpath would ruin this particular part of the woods.</p> <p>The need for this path is quite unclear. Cyclists can easily use Eagle Way by adding a cycle lane. The existing paths are more than adequate for walkers to use.</p> <p>All the comments above also apply to the unnecessary part of route IM8 from near Gorseland Primary school and passing through Portal Wood.</p> <p>I hope your above proposals will be reconsidered to help safeguard our fragile environment.</p>	<p>Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>
IM9	Robin Guy	264	Yes	<p>This cycle/path way is adversely impacted by overgrowing vegetation which should be addressed in these proposals.</p>	<p>The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.</p>
IM9	Smith, Carolyn	781		<p>Thank you for consulting on your draft cycling and walking strategy; hopefully this will enable you to learn from local knowledge and prevent you from making expensive mistakes which will damage precious heathland and adversely impact well used green spaces in the Martlesham and Kesgrave areas.</p> <p>I am in favour of encouraging walking and cycling, but not at the expense of damaging scarce habitats and the loss of local amenity, all of which will occur if</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>

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				<p>the routes IM4, IM9, IM12 and IM10 are constructed in their proposed form.</p> <p>Routes IM12, IM10, IM4</p> <p>Why are you proposing surfacing over and urbanising valued green spaces, important for recreation and wildlife?</p> <p>This is in direct contravention of the SC Local Plan Policy SCLP10.1 (Biodiversity and Geodiversity).</p> <p>Namely:</p> <ul style="list-style-type: none"> • · the woodland to the west of Dobbs Lane, and Longstrops (IM4) • · Portal Woods (IM9) • · Martlesham Birch Woods (IM12), • · the heathland to the northern end of Martlesham Heath SSSI, and southern end of Martlesham Common (IM10) <ul style="list-style-type: none"> • Improve the existing A12 crossing to the north of the BT roundabout and improve IM13 to encourage the use of this route across the A12 instead of spending additional money on a crossing to the south. <p>Or:</p>	

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				<p>Extend IM13 southwards to a new bridge crossing over the A12 north of the Foxhall Road/Waldringfield Road roundabout, and continue on a new route through the edge of the field alongside Foxhall Road to Bell Lane, and continue along the field edge to connect with IM4 west of Bell Lane.</p> <p>Construct a bridge crossing the A12 to connect IM13 with footpath PROW6, re-route PROW6 alongside the wooded area to the south and then across to Foxhall Road, and continue eastwards along the field edge towards Bell Lane as above.</p> <p>Both of these options give an opportunity to construct a wildlife corridor alongside this route, with screening trees and wildflower planting.</p>	
IM9	Stephen Denton	481	Yes	<p>Supported with conditions</p> <p>Lighting should be minimal - the woodlands are home to various fauna including bats and glow worms.</p> <p>The track surface is actually pretty sound - minimal patching would save money, reduce damage to woodland and preserve this historical wartime construction of which there is very little left visible in Martlesham.</p> <p>Re the suggestion to resurface the route between Portal Avenue and Eagle Way including a modal filter if necessary - what is a modal filter in this context - what would be filtered?</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whilst being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward. The modal filter could prohibit vehicle through traffic, while allowing walking and cycling. Alternatively, the modal filter could allow cycling, walking and buses through the use of a rising bollard, and prohibit other vehicle through traffic.</p>

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				I am not convinced it would need resurfacing - it is in much more urgent need of the overgrowth being cut back and maintained properly.	
IM9	Steven Moore	483	No	There is already an existing path around the Police HQ from Portal Avenue to Gorseland School. I am happy to support any improvements on the existing path but do not support any shortcuts through existing woodland. I would also be concerned if damage is done to the remnants of the airfield.	The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whist being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Terry Duffell	711		The plans for a lit tarmac route through Portal Woods would decimate its nature and deprive the residents of a cherished and popular facility which provides tranquility and pleasure.	The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whist being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM9	Tony Ellison	1085		Next I have concerns on the lighting in Portal woods. This is a known bat roost and LED lighting is known to cause distress to wildlife. I have cycled through these woods many times at night and although the surface is broken in places I would not like to see these woods dug up to install lighting and the impact that would have on the environment.	The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whist being sensitive to the natural and historic environment, and does not propose the removal of any vegetation.

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				<p>The area I have marked in red does need a new surface as it is just mud at this time.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	<p>Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>
IM9	West	619	No	<p>Currently cyclists and walkers enjoy the existing shared perimeter track in the woodland, part of Martlesham's history. But I believe adding a surface upgrade and lit cycleway will only lead to speeding cyclists shortcutting through the wood to go elsewhere. I myself am a keen cyclist but this proposal is not in the interest of Martlesham residents.</p>	<p>The importance of the historic and natural environments as well as the safety of all users is recognised within the Strategy. For this reason, IM9 has been amended to recommend repairing the existing hard surfaced track through Portal Woods whist being sensitive to the natural and historic environment, and does not propose the removal of any vegetation. Lighting is proposed to only be introduced where necessary. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>
IM10	Alan Pitt	598	No		<p>Objection noted.</p>
IM10	Alasdair MacLeod	539	No	<p>I am against a new tarred surface through MHHL land. Many local paths cross the ground without damage - the ground is dry and hard.</p> <p>Light pollution: I am against adding lighting to the route through the woods. Both because lighting is harmful to wildlife and because light pollution is harmful to residents mental health.</p> <p>This seems to abandon the Broomfield - Eagle Way route but people will continue to use it. Improve the Broomfield - Eagle Way route (ref 66) and link to</p>	<p>The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward. The current cycling and walking infrastructure at Broomfield is of limited width and cannot be widened due to property boundaries. IM10 would provide an high quality alternative route.</p>

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				<p>IM11. Fix the tight turn (there is ground available) and you improve the path that everyone is going to use anyway. Maybe sign-post better so people approaching from the south know that turning into Broomfield gives quicker access to Eagle Way than IM10 would.</p> <p>The document mentions desire lines - but if properly signposted people would have a fast route to Eagle Way via Broomfield without the need to go via the control tower (a longer off-road path).</p>	
IM10	Andrew and Simone Moore	857	No	<p>The link with the Longstrops IM10 proposal again is not required - there are plenty of alternative routes for cyclists. The existing tracks used through the woods are used for recreational walkers who on occasions find themselves at risk by off road inconsiderate cyclists who are determined to use these green tracks as their right of way.</p> <p>If tarmac and lit these tracks would be forever gone their destruction would diminish the environment for no good purpose.</p>	<p>IM10 would provide a direct and safe cycling and walking route between Longstrops Bridleway and the A12 underpass. Cycling and walking infrastructure can be designed to avoid conflict between cyclists and pedestrians. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.</p>
IM10	Beaumont, David	362	No	<p>Response to Public Consultation on Proposed Travel Routes IM9(part), IM10 & IM12</p> <p>I am totally OPPOSED to routes IM9 (part that links Gorseland School to Portal Avenue), IM10 & IM12 as proposed by East Suffolk Council (ESC) on the grounds that they:</p> <p>a) endanger a protected Site of Special Scientific Interest (SSSI), known as Martlesham Heath's 'Western Corridor'</p>	<p>IM10 does not enter the Martlesham Heath SSSI. The safety of users is of paramount importance and for this reason IM4 has been amended to recommend the provision of a signalised crossing on Dobbs Lane where IM4 and IM10 meet, as well as a village gateway to slow northbound vehicles before the signalised crossing and entering Kesgrave.</p>

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				<p>b) endanger carefully conserved woodlands, known as Martlesham’s Portal Woods, Birch Woods and the woodland between Coopers Road and Lancaster Drive Hamlets</p> <p>c) are contrary to ESC’s objectives to protect the existing ecology and counter Global Warming d) have been proposed with no prior consultation with the landowners, Martlesham Heath Householders Ltd (MHHL), of which the residents of Martlesham Heath Village are shareholders</p> <p>e) are unsafe for both pedestrians and road users of Eagle Way, particularly near Birchwood School and the proposed crossings either end of Birch Woods. Furthermore, locating the crossing of Dobbs Lane by IM10 on a ‘blind corner’ is totally irresponsible.</p> <p>I will now expand on the above issues.</p> <p>A) The SSSI and immediate vicinity As ESC planners well know, the ‘green land’ west of the Martlesham Heath’s housing to the boundary of Dobbs Lane is a protected SSSI. Consequently, this SSSI is very sensitive to ‘overuse’ for recreational purposes and thus efforts are consciously made to not exploit easy access. This land is leased by MHHL, which has set up as a charity, SSSI Ltd, to manage the site with guidance from Natural England. Martlesham residents respect and regard it as a privilege to act as guardians of this SSSI which is one of the few remaining lowland heath sites in the country and the breeding ground for an endangered rare butterfly, the Silver Studded Blue.</p>	

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				<p>Whilst route IM10 does not seem to cross the SSSI it does run along its northern border abutting MHHL land which MHHL has purposely kept as natural as possible whilst accommodating access in order to respect the proximity of the SSSI. Reptiles and valuable flora are in abundance on this northern border of the SSSI; in fact it has been reported that there is a relatively large population of Adders located and breeding in this area.</p> <p>I am annoyed that planners regard ‘informal trails’ shown on various maps of the SSSI and nearby area as ‘paths’ which they ARE NOT, unless specifically designated as such. This includes the area of MHHL land between Eagle Way and the footpath/cycleway that run North-South on the eastern boundary of Gorseland School.</p> <p>Any ‘urbanisation’, with wide paved paths, cycleways and lighting between Dobbs Lane and Eagle Way will encourage ‘overuse’ and endanger the carefully controlled ecology of this area which MHHL and conservancy groups of volunteers have nurtured for decades, on occasion using they own money.</p> <p>B) The Woodlands Firstly, part of Route IM9 that connects Gorseland School and Portal Avenue is part of the old airfield perimeter track and has historical significance. This area is heavily wooded and is conserved by the Portal Conservation Group which has made significant effort to maintain this area as a wildlife</p>	

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				<p>sanctuary. This area is also used as an informal education area for school children to understand the importance of nature and the ‘quiet environment’ it bestows encourages flora and fauna like ‘Muntjac deer’, reptiles, ‘glow-worms’ and numerous bats.</p> <p>The transformation of this ‘track’ into a heavily used, paved and worse still, lit ‘travel route’ will damage this habitat.</p> <p>Secondly, part of IM12 that connects the western side of Eagle Way to IM13 scythes through one of Martlesham Heath’s prime ‘carbon sinks’, known as Birch Woods. This wood has been preserved for low impact recreational walking and has a multitude of mature birch trees amongst other varieties which will be destroyed by the proposed wide paved footpath/cycle route resulting in light pollution and possible flooding.</p> <p>IM12 then proceeds eastwards to further destroy a purposely created natural barrier separating Coopers Road and Lancaster Drive to cross the A12 and link up with IM13. This ‘barrier’ was conceived as a natural barrier between ‘Hamlets’ as part of the original approved plans for Martlesham Heath, as well as acting as a ‘sound barrier’ for the busy A12. A ‘travel route’ through this ‘barrier’ would change the demarcation of the two Hamlets making them appear to be contiguous and worse still act a ‘sound tunnel’ concentrating the noise pollution of the A12 traffic into the heart of Martlesham Heath.</p> <p>Another bridge crossing the A12 is not necessary as</p>	

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				<p>one already exists (IM11). Another bridge would be expensive and offer little benefit but cause major disruption and destruction of a mature, natural, ecologically sound environment. Furthermore, the Brightwell Lakes development planning approval stipulated a need for a controlled crossing/junction on the A12 as the main access to the development, connecting it to the existing 'bridleway' on the western side of the A12.</p> <p>C) Ecology and Global Warming policy ESC is guilty of sending out mixed messages on policy. On the one hand it is lauding itself as encouraging the protection of the ecology to help fight Global Warming and on the other hand it then proposes 'sustainable travel solutions' which will damage the careful nurtured ecology of Martlesham Heath. Martlesham Heath was never designed to have an east-west urban cycle route running through it: 'throwing the baby out with the bath water' comes to mind.</p> <p>Is ESC serious about protecting the District's ecology and supporting and encouraging the public who are willing to offer their time and money in helping?</p> <p>D) Lack of consultation I believe that the publication of this "Strategy Proposal" was the first anyone in Martlesham was made aware of the major impact these 'Travel Routes' would have on the Parish. I find it astonishing that ESC hasn't contacted the affected</p>	

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				<p>landowners earlier to establish if there were any extenuating circumstances why these proposals should or should not be considered.</p> <p>E) Safety issues Is ESC seriously considering that the northern section of IM12 using Eagle Way is a sensible and safe option for pedestrian, cyclists and other road users? If so, then it is ill-conceived.</p> <p>Anyone who uses that section of Eagle Way could tell ESC that this proposal is a disaster waiting to happen. This section of road is heavily used by young school children, their parents, the elderly, and others. There is widespread street parking on this section of Eagle Way in school-time with parents 'dropping off' children, school events, etc. Furthermore, this is a bus route and a major arterial road for Martlesham Heath residents and their vehicles getting on to the A12. The road itself was not designed to accommodate parked traffic, parked buses, a commercial bus route and now a proposed cycle route.</p> <p>Another safety issue is the location of the crossing of Dobbs Lane by IM10 on a 'blind corner' which is totally irresponsible. This is a busy 'rat run' for commuters and anyone using the proposed crossing will be obscured from motorist's view right up until the last moment due to foliage and trees in the vicinity.</p>	

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				<p>Conclusion</p> <p>It seems obvious to me that these ‘travel routes’ have been conceived and proposed as a ‘desk based’ exercise to get from ‘A to B’ using any ‘available’ green space, probably viewed using Google maps’ satellite view and without consulting the landowners concerned or environmental agencies like Natural England. I have come to this opinion based on previous suggestions for path routes by planners of ESC and Suffolk County Council (SCC) when trying to link Brightwell Lakes development to Grange Farm.</p> <p>ESC are rightly proud of their respect for protected environments (e.g. SSSIs), why do they insist on using Martlesham to ‘mope up’ previous strategic planning inadequacies with a ‘spaghetti junction’ of paved, lit footpaths/cycleways?</p> <p>It should be noted that the Brightwell Lakes planning approval was based on an ‘A12 Access Crossing’ as the main entrance to Brightwell Lakes which incorporated a pedestrian/horse/cycle facility to the existing bridleway opposite. No mention or agreement had been made to have another crossing across the A12 linking into Martlesham Heath as one already existed and was part of the National Cycle Network.</p> <p>The proposed crossing of the A12 via IM12 would have probably been adamantly opposed by residents and Martlesham Parish Council to the Brightwell Lakes Development if it had been proposed and now this ‘Sustainable Travel’</p>	

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				<p>proposal could be seen as a means of the ESC getting a crossing by stealth. <u>Is this how ESC wants to be seen to be planning infrastructure?</u></p> <p>I hope the above comments are considered and are helpful in shaping ESC's Draft Cycling and Walking Strategy.</p>	
IM10	Brenda Shakespeare	566	No	Felixstowe Road is very necessary as the only way to avoid the chaos often found with cars from industrial estate and leaving Tesco area plus from the Marks and Spencer and Next areas queueing to access A12. This is also a bus route. It is much used and NECESSARY	Felixstowe Road is not needed as a vehicle through route as alternative higher capacity roads exist, namely the A12 and Main Road. The modal filter could be designed to allow buses, cyclists and pedestrians, and would result in a minor detour for motor vehicles.
IM10	Chris Adelson	452	Yes		Support noted.
IM10	Chris Petty	35		Blocking through traffic on Felixstowe Road would restore its status as a cycle priority route, it currently has the status of a rat run putting cyclists off using. It also part of the national cycle network. There are no residential properties on it and few business which have low levels of traffic so not much inconvenience to people on that road	Support noted.
IM10	Clive Roper	573	No	<p>If you prohibit through traffic then traffic from/to BT Adastral Complex will all have to travel via the A12 by joining the at either of the two A12 roundabouts. the A12 is already congested. It will be further congested when the 2000 houses on Brightwell Lakes are built. How will we as residents of Martlesham Heath be able to join the A12, which is extremely difficult.</p> <p>If the Felixstowe Road is well lit, I am all for that.</p>	The proposed modal filter on Felixstowe Road would restrict vehicle through traffic, and result in a minor detour for motor vehicles.
IM10	David Green	271	Yes	I particularly support this. As a regular cyclist along Felixstowe road I take my life in my hands,	Support noted.

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				particularly with the 2 blind bends near the RSPCA entrance. The traffic seems to be getting ever heavier and I regularly get 'cut up' by irresponsible motorists taking a chance overtaking me on the blind bends.	
IM10	Drs Joan and Mike Hudson	803	No	<p>I wish to record our objection of the proposed new lit tarmac route from Dobson Lane along the back of Whinfield/ Broomfield leading to Eagle Way. We have experienced many dangerous incidents involving cyclists who now travel along the existing paths. The cyclists travel at speed, churn up the paths which become quagmires in wet weather and are unmindful of walkers in the woods and heathland. Also a lit route would be a disturbance to us and our neighbours and would encourage even more traffic to pass along the path which was created within the last two years during lockdown.</p> <p>We do not consider these plans to be of benefit to residents on Martlesham Heath as there already exist quiet roads which are more suitable for cyclists rather than spoiling and putting in danger walkers on the Heath.</p>	The introduction of cycling and walking infrastructure in areas where cyclists and pedestrians often conflict can have the effect of reducing conflict as areas are designed for each user. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.
IM10	Ernest John Geary	226	No	Your comments conveniently avoids describing the intended infrastructure between Dobbs Lane and Eagle Way most of which is unsurfaced small paths and through some woodland. What is a ' crossing point ' ? and what infrastructure is to be added to Eagle Way? We do not want the through traffic this route implies, it should go up Dobbs Lane and onto Main Road.	A segregated cycling and walking track is proposed along Eagle Way, connecting the proposed improvements to Longtrots Bridleway and Felixstowe Road. Crossing points come in different shapes and sizes, and the most appropriate crossing point for cyclists and pedestrians over a road would need to be considered as the recommendation is taken forward.
IM10	Ian Dudley	414	No	I wish to lodge my objections to the above proposals concerning Martlesham Heath / Brightwell	In preparing the draft Strategy, officers considered the benefits to cycling in respect of improved journey times,

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				<p>Lakes development for the following reasons:</p> <p>There is already a footbridge bridge across the A12 which , in my opinion, is not the most used or popular way to cross!</p> <p>To build a new bridge across the A12 is, in my opinion, a waste of public money. Who would, be responsible for the upkeep?</p> <p>Council tax rates are high enough!</p> <p>The proposed new foot/cycle path routes through the village, would create in the region of 1 mile of tarmac cycle/footpaths with them cutting through Land owned and managed by Martlesham Heath Householders Ltd, on behalf of the resident membership.</p> <p>The proposed bridge would necessitate the creation of another path to continue through Birch Woods (Martlesham Woods) and would require the felling of at least 25 mature trees, an action that is not very environmentally friendly!</p> <p>Where the proposed path would exit near Forest Lane on Eagle Way, cyclists and pedestrians would be tempted to take, (on their way to Dobbs Lane)</p> <p>a 'short cut' across our village's SSI (Site of Special Scientific Interest) which is maintained by MHHL and numerous volunteer residents.</p>	<p>and an attractive and safe route through the wooded area away from vehicles to outweigh the negative aspects in relation to low level lighting through the wooded area between Eagle Way (west and east), increased human presence in the wooded area harming wildlife, and potential conflicts with pedestrians.</p> <p>The existing cycling and walking bridge over the A12 is an important connection between the areas east and west of the A12 and lessens the need for a cycling and walking bridge between Brightwell Lakes and Martlesham Heath. However, a new bridge would provide an additional and direct connection between Brightwell Lakes and Martlesham Heath, and west towards Ipswich, which would be of significant value. Having reflected on consultation responses and considered these matters further, officers have amended IM12 to avoid recommending the wooded area for cycling and instead recommend cycling and walking improvements to Eagle Way. The recommended cycling and walking bridge over the A12 remains part of IM12, but is proposed to be accessed from Eagle Way and not through the wooded area between Eagle Way (west and east).</p>

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				<p>Why is there a need for a new lit tarmac pathway route around the perimeter of Portal Woods ?</p> <p>WHY is a new lit tarmac route from Dobbs Lane along the back of Whinfield /</p> <p>Broomfield leading to Eagle Way a necessity?</p> <p>I can think of no reasons that the above proposals should be accepted, indeed, the proposed exercise is another example of humans interfering with the environment when it is unnecessary.</p> <p>If people want to cycle or walk, they are able to do so within the currently available cycle/footpaths!</p>	
IM10	Ian Read	563	Yes	<p>My wife and I welcome ES Council's acknowledgement that cycle pathways in our neighbourhood are overdue for increase and improvement. Also that there is a need for a strategic plan, to ensure that cycle paths are joined-up (in the literal sense!) to encourage more cycling.</p> <p>We are long-term residents of Martlesham Heath and cycle users, both for transport and leisure/fitness. Martlesham Heath cycle path provision has always been inadequate (e.g. compared to the newer Grange Farm, Kesgrave development), so in principle we welcome planning and investment to address this.</p> <p>IM10 - We're very pleased to see this route designated as Very High priority. This is on the condition and understanding though that the</p>	Support noted. The proposed cycling and walking infrastructure along Eagle Way will need to be designed in such a way to avoid conflict between pedestrians and cyclists, whilst being segregated from vehicles on Eagle Way.

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				existing footpath around Eagle Way is properly upgraded to a joint pedestrian and cycle path, with the necessary surface markings and signage. Overall there is plenty of verge width to achieve this. The footpath has been used (misused?) as a de-facto cycle path for many years anyway. It is particularly important to halt cyclists cycling in the Kesgrave direction at the entrance to Demesne Gardens, where a house obscures visibility of pedestrians and cyclists for vehicle drivers leaving the road. These improvements would also improve safety for those cyclists who do currently chose to follow the Highway Code and rejoin the road where the cycle path finishes, very close to the roundabout.	
IM10	Janine Davey	413	No	<p>I would like to object to the planned cycle route through Birch Woods, Martlesham Heath (referred to in your document as Martlesham Woods). This is a precious amenity area for local residents and an area of natural woodland. The proposal of felling mature trees to make way for the cycle route along with construction of lighting will damage the woodland and disturb wildlife. Where the path would exit on Eagle Way, cyclists and pedestrians would be tempted to take a short cut across our SSSI on their way to Dobbs Lane.</p> <p>The same argument applies for the lit tarmac routes through Portal woods and near Dobbs Lane. Our natural areas and wildlife are constantly under threat from these types of schemes and would urge you to re-visit this decision.</p> <p>I see the proposed schemes as an unnecessary waste</p>	The proposed cycling and walking infrastructure improvements are wholly outside the Martlesham Heath SSSI. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward.

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				<p>of public money seeing as there is a perfectly acceptable existing through-cycle route from the A12 to Grange Farm.</p> <p>No doubt you will be conducting the necessary surveys and producing data to justify this expense. The local residents would be very interested in your findings.</p> <p>Please read and carefully consider any objections you receive and not treat this consultation as a 'box-ticking' exercise.</p>	
IM10	Jean Rogers	429	No	I oppose the extension of a cycle route from Dobbs Lane along the back of the Whinfield/Broomfield roads leading back to Eagle Way. This current track is used extensively by locals for exercise and dog walking. It would be unsafe to add cyclists to this mix. This will also encourage some cyclist to use the western corridor to continue their journey, cutting across SSSI and causing long term damage.	The introduction of cycling and walking infrastructure would act to formalise cycling and walking routes, and reduce cycling and walking through the SSSI as other more desirable routes exist.
IM10	Jenny Edgerley	476	No	I fully support the need for a cycling strategy however, I believe the location of this route would have a huge impact on what is a very natural environment and for little benefit. The route through long strops, then leading across Dobbs lane on Martlesham Heath, then at the back of Broomfield/Whinfield are quiet but popular areas. They are a haven for wildlife, is mostly unmanaged and is well used by walkers enjoying the peace and quiet which is one of the things so unique about the village. There are existing formal cycleways/footpaths from the footbridge leading across Martlesham Heath village to grange farm	Whilst there are other cycling and walking routes within and around Martlesham Heath, IM10 seeks to provide a direct and safe route between the proposed improvements to Longstrops Bridleway and the A12 underpass.

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				which are well lit and well used so there is no need to destroy the tranquility and beauty of this area when there are suitable routes already in place.	
IM10	John Kelso	777	No	No lighting or hard surfacing should be permitted on Martlesham Common as it is protected heathland.	In order to preserve the natural environment at Martlesham Common the proposed cycling and walking track across Martlesham Common has been removed from IM10.
IM10	Jonathan Clyne	392	No	There is no need to make any changes to Eagle Way, which is already wide enough for and is used by cyclists. Any introduction of new tracks to heathland should be avoided as already far too much heathland has been destroyed by housing developments etc.	Whilst Eagle Way may be considered a safe cycling route for some users, it will not meet the needs of all users, which is why segregated cycling and walking infrastructure is proposed. This will need to be designed to ensure the continued safe operation of the bus stop. In order to preserve the natural environment at Martlesham Common IM10 has been amended to remove the recommended cycling and walking track across Martlesham Common. Instead cycling and walking infrastructure is proposed for improvement along Anson Road, thereby connecting the A12 underpass to Felixstowe Road.
IM10	Margaret Shaw	850	No	<p>I write with particular reference to the proposed cycleway through Martlesham Heath but my comments re MCAF "biodiversity" may be relevant to the other proposed routes in the strategy.</p> <p>MCAF- Safety</p> <p>I strongly oppose the junction of IM10 and IM 4 which is a very high priority route where it crosses Dobbs Lane. The cycle path crosses on a bend which occurs after a straight run from Faxhall Road. As a cyclist, walker and car driver I know that cars travel at a high speed down that road and that the approach to the bend does not give clear visibility to</p>	The safety of users is of paramount importance and for this reason IM4 has been amended to recommend the provision of a signalised crossing on Dobbs Lane where IM4 and IM10 meet, as well as a village gateway to slow northbound vehicles before the signalised crossing and entering Kesgrave.

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				<p>either the cars or to walkers and cyclists crossing the road. A cycle path crossing here will endanger lives and create an accident black spot.</p> <p>Safer alternatives would be</p> <ol style="list-style-type: none"> 1. IM10 ceases with the junction with IM7 2. IM\$ runs down the Martlesham Heath side of Dobbs Lane and crosses where there is clear visibility at the point where there is a sharp right hand turn in IM4 . 	
IM10	Martlesham Conservation Group (Phil Smith)	896	No	<p>IM10 (Dobbs Lane to Felixstowe Road)</p> <p>a) Part of IM10 runs adjacent to the northern end of the Martlesham Heath SSSI. Martlesham Conservation Group is concerned that the proposed approach of applying hard surfaces over heathland soil, combined with lighting, would lead to a degradation of the wildlife at the northern end of the SSSI. It would also lead to increased recreational pressures on an already well used area. This northern area has the same range of wildlife found on the SSSI and acts as a wildlife corridor between the Heath and Portal Woods. It is also important to note that Nightingales are regularly to be found in the area to the south of Gorseland School.</p> <p>b) IM10 also has a children’s play area adjacent to the route towards the south of Gorseland School. The footpath taken by many children and parents to access Gorseland School crosses IM10. With these two features already in place it could not be</p>	<p>IM10 runs wholly outside the Martlesham Heath SSSI. The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward. Cycling and walking infrastructure can be designed to avoid conflict between cyclists and pedestrians. In order to preserve the natural environment at Martlesham Common IM10 has been amended to remove the recommended cycling and walking track across Martlesham Common. Instead cycling and walking infrastructure is proposed for improvement along Anson Road, thereby connecting the A12 underpass to Felixstowe Road.</p>

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				<p>considered safe to encourage cyclists to traverse this area, possibly at high speed.</p> <p>c) Part of IM10 runs across Martlesham Common, which is a County Wildlife site and Local Nature Reserve. The habitat is similar to the Martlesham Heath SSSI and is another fine example of rare lowland heath which also has the nationally rare Silver Studded Blue present, along with Glow Worms, a wide range of insects (e.g. Green Tiger Beetle), reptiles (Adders, Viviparous Lizards, etc.) and birds including Nightingales. The site already suffers considerable pressures due to the close proximity to Tesco supermarket and The Sandlings. The Council should not be seeking to further degrade this area, as indeed this would be in contravention of their own policy.</p> <p>The Martlesham Conservation Group therefore has serious concerns for the wildlife along this route and believes IM10 should not go ahead on that basis alone. When combined with safety considerations mentioned in b) the case for withdrawing this route is overwhelming.</p>	
IM10	Martlesham Heath Householders Ltd (Sir/Madam)	288	No	<p>IM10</p> <p>21. The Board does not accept the proposed route IM10 as a cycle route as it crosses our land. The Sandlings path does cross our land as shown on the photographs below but as we have said previously, we are against any more tarmac paths on our “Western Corridor” which includes a SSSI.</p>	The suggestion of a permitted path connecting the proposed IM4 to the existing cycling and walking track south of Gorseland Primary School is welcomed.

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				 <p data-bbox="846 715 1424 1002">22. The Board may consider offering our path from Dobbs Lane to Cycle Route 1 as a permitted route with an all-weather non-tarmac surface. At the T junction with the existing route 1 as shown in the photographs below, cyclists could then go left or right to their chosen destination. Should cyclists wish to go eastbound along the Ipswich/Melton corridor, we propose they use existing cycle route 1 leading to routes IM7, IM9, IM11and IM13.</p> 	

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IM10	Martlesham Parish Council (Diane Linsley)	883		<p>This should be regarded as a local route only, assuming that connections via IM9 (along the west and north side of the police HQ site) to IM8 from IM7, IM4 and IM8 are made, the principal commuting routes are between Ipswich, Kesgrave, Woodbridge etc.</p> <p>The Tesco pedestrian underpass is unsuitable for volume traffic. Its design predates the creation of the present day retail park; the tunnel itself is too narrow to properly accommodate pedestrians and cyclists, and the T junction between the western end of the tunnel and the down ramps has poor visibility. IM10 east of the A12 should remain a leisure and occasional shopping route as it passes over the sensitive Martlesham Common Local Nature Reserve with the routes that PC has already created.</p> <p>The Parish Council view is that the creation of a wider shared pedestrian path along the north side of Eagle Way between Parkers and the underpass down ramp is unnecessary.</p> <p>Eagle Way is wide enough, with good visibility and few junctions, to accommodate cyclists.</p> <p>However, the Parish Council has on several occasions over the last 6 or 7 years, sought improvements to calm traffic down at the entrance to Eagle Way from the A12 where it changes from national limit to 30mph. There is also a crossing route from Manor Road to the top of the down ramp to be considered and made much safer. It would appear that there</p>	<p>The desire for IM9 to remain unlit, or only lit where necessary means that it cannot be relied upon as the main cycling and walking through route. While the southern Tesco underpass may not be of the desired width it provides an important safe cycling and walking connection across the A12. In order to preserve the natural environment at Martlesham Common IM10 has been amended to remove the recommended cycling and walking track across Martlesham Common. Instead cycling and walking infrastructure is proposed for improvement along Anson Road, thereby connecting the A12 underpass to Felixstowe Road. Whilst Eagle Way may be considered a safe cycling route for some users, it will not meet the needs of all users, which is why segregated cycling and walking infrastructure is proposed. This will need to be designed to ensure the continued safe operation of the bus stop. The proposed modal filter on Felixstowe Road would provide a safer cycling and walking environment by preventing vehicle through traffic. Vehicles would need to take a minor detour along the A12 and Main Road. ESC and SCC have worked closely throughout the preparation of the Strategy, with officers from both ESC and SCC present on the steering group.</p>

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				<p>has been no movement on this as it has consistently been deferred until the long awaited and much consulted A12 improvements have been agreed.</p> <p>The proposal to relocate the bus stop would be a loss to the street scene. It is attractively designed and is flanked by pleasant landscaped beds maintained by MHHL.</p> <p>Although strictly a highway matter, the Old Felixstowe Road is relevant as the principal N<>S cycle route from the retail/industrial areas to old Martlesham and beyond.</p> <p>In view of the longstanding inadequacies of the Old Felixstowe Road, the Parish Council has very serious concerns about the wisdom of routing more E<>W cycle through traffic (in the form of IM10), on to that road, when alternative safer options are available. Whilst the draft consultation talks about the potential closure of that road to through traffic, there has been no evidence presented in last year's A12 consultation, about the impact of the various A12 options on traffic volumes on local feeder roads; in particular will the proposals reduce or increase traffic volumes along Old Felixstowe Road and the A1214 through Martlesham? Its use as a rat-run currently results in very high levels of traffic through old Martlesham causing problems for cyclists and for pedestrians crossing the road. The frontages of some of the properties in this location are very close to the road. When the Old Felixstowe Road was closed for several days for gas mains work about 3 years ago,</p>	

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				<p>gridlock occurred in the retail and industrial areas, with delays in excess of an hour reported by people trying the leave the retail car parks via the A12. We therefore seek confirmation that the increased capacity of the A12, and subsequent A12 improvements, will allow a scheme of calming and traffic management to be applied to the Old Felixstowe Road, especially when proposed new housing developments in the parish are factored in.</p> <p>We would therefore like to see much more joined up thinking between planners at ESC, SCC highways, and those co-ordinating the A12 improvements, in full consultation with MPC.</p> <p>In the circumstances, restoring Old Felixstowe Rd to a safe cycle priority route for its existing N<>S usage should perhaps be the minimum objective. This could involve things like improved lighting (NB some of the distances between the lamp posts are greater than the 30mph standard), speed limit reminders, recovering carriageway width lost to overgrowth resulting from years of neglect, and closure to HGV through traffic. Ideally it should be 20mph.</p> <p><i>The images have not been published due to potential data protection concerns, but these were still fully considered and assessed in forming the Strategy.</i></p>	
IM10	Martyn Shakespeare	493	No	<p>Partially in favour. OK from Dobbs Lane to Parkers Place. Exit via Parkers Place road rather than tarmacking over green opposite Carlford Close, which is being managed as a wildlife area. Use on-road route via Eagle Way which is not busy; replacing most of the grass verge (which I believe is</p>	<p>Whilst Eagle Way may be considered a safe cycling route for some users, it will not meet the needs of all users, which is why segregated cycling and walking infrastructure is proposed. This will need to be designed to ensure the continued safe operation of the bus stop. In order to preserve the natural environment at</p>

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				private land) with a path would detract visually from the road, it's one of the values of the Estate. Better lighting, and some widening, on Tesco side of underpass desirable, difficult to see oncoming bikes/pedestrians after well-lit underpass. I think that it would be preferable to use the existing much wider path at the edge of the common, parallel to the A12, to join the cycleway alongside Main Road Martlesham, than widen other paths across the common. This also avoids directing cycles onto the Old Felixstowe Road, which can then remain open to through traffic.	Martlesham Common IM10 has been amended to remove the recommended cycling and walking track across Martlesham Common. Instead cycling and walking infrastructure is proposed for improvement along Anson Road, thereby connecting the A12 underpass to Felixstowe Road.
IM10	Martyn Shakespeare	534	No	Partial. Old Felixstowe Road is a bus route so needs to be open for that, alongside access to RSPCA, Community Hall and other properties. Blocking the road to through traffic removes the most direct route to the Community Centre from Martlesham Village. This road takes a lot of traffic away from the A12 and Tesco roundabout, which is already very busy and likely to get much more so with the Brightwell Lakes development and Sizewell C traffic, and turning right from the A12 to Martlesham Village is frankly dangerous if there are more than two cars with the daft traffic lights on the roundabout. It is perhaps better to encourage cyclists to use an alternative route (e.g. Main Road Martlesham which is now relatively quiet and then path across common).	The introduction of a modal filter on Felixstowe Road can be designed in a way to restrict motor vehicles whilst allowing buses, cyclists and pedestrians. This would result in a minor detour for motor vehicles.
IM10	Mary Odam	863	No	IM10/IM4 seem superfluous. There are already two cycleways into Ipswich – one along the A1214 (IM5) and the other through Grange Lane and Grange Farm (IM7). Furthermore Long Strops is a pleasant piece of land much used by dog walkers and other	The Strategy recognises the importance in improving the ability of individuals to access open space in a safe and convenient manner. The recommendations seek to connect existing cycling and walking infrastructure with

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				pedestrians. Unlike the cycle paths through the village is it not subject to hoards of schoolchildren walking and cycling at the beginning and end of the day. It is probably the only undeveloped public space in the residential part of Kesgrave and should remain so. The Covid pandemic has shown how important it is for people to have access to open spaces for exercise and recreation.	new infrastructure to provide a coherent and safe cycling and walking network.
IM10	Michael Farahar	720		<p>With reference to East Suffolk Council's East Suffolk Cycling & Walking Strategy - Draft 2021 consultation, I would like to support the draft's recommendations contained within S 3.15, The Ipswich to Melton Key Corridor recommendations.</p> <p>I would like to suggest though that an innovative and respectful approach is adopted to minimise the impact on both local residents and the natural environment when planning for the implementation of recommendations IP4, IM9, IM10 & IM12, for example by the use of low level lighting rather than traditional full height lamp posts and/or lighting intensity controlled by movement sensors etc,</p>	The most appropriate lighting solutions will need to be considered as the recommendation is taken forward.
IM10	Michael Rogers	478	No	I strongly oppose the extension of a cycle route from Dobbs Lane, the current track is used extensively by locals for exercise and dog walking; it would be unsafe to add cyclists to this mix. (If you are in any doubt about this, try strolling across Jesus' Green in Cambridge and be prepared for aggressive attitudes by cyclist who 'know' they have right of way.) This will also encourage cyclist to use the 'Western Corridor' to continue their journey, cutting across SSSI and causing long term damage.	The safety of all users has been of paramount importance throughout the preparation of the Strategy, and it is therefore important that cycling and walking infrastructure is designed in such a way that avoids conflict not only with vehicles, but also between cyclists and pedestrians. The route avoids the Martlesham Heaths SSSI.

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IM10	Nik Bestow	18	Yes	Martlesham Trim Trail uses part of this route - needs to be taken into account.	The detailed design, including things such as incorporation of the Martlesham Trim Trail will need to be considered as the recommendation is taken forward.
IM10	Paul Davey	324		<p>As a keen walker and cyclist I am very much in favour of ESC having a strategy for my benefit.</p> <p>However as a resident of Martlesham Heath I consider considerable concerns regarding the draft proposals published this year.</p> <p>My objections and reasons are listed below.</p> <p>3. IM10 is not required and Eagle Way is already accessed from IM7.</p>	IM10 seeks to provide a direct and safe connection between Longstrops Bridleway and Felxistowe Road, whilst making best use of existing cycling and walking infrastructure such as the A12 underpass.
IM10	Paul Jordan	6	Yes	<p>I support the aim of this policy and would like to see it put into action but I think that including closing Felixstowe Road to through traffic will prove too difficult - motorists will object and they may have a point. As an alternative I suggest that the route is changed so that it runs from the A12 underpass to join Main Road with the proposed changes to Main Road increased to High Priority. There are two viable options for joining the underpass to Main Road. Option 1 would be to use the existing surfaced path towards Black Tiles, this would require widening and segregation. Option 2 is to link to the existing surfaced route to the east of Mill Heath, PROW 52.</p>	While closing Felixstowe Road to through traffic may be a big change for some users initially, the diversion to Main Road and the A12 is relatively short.
IM10	Paul Newman	513	No	<p>I wish to comment on two sections of the East Suffolk Councils Draft Cycling & Walking Strategy.</p> <p>In particular two corridors IM10 & IM12 where they are constructed on open or wooded land owned</p>	Objection noted.

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				<p>by Martlesham Heath Householders Ltd (MHHL) whose shareholders are all residents of Martlesham Heath.</p> <p>I am fundamentally opposed to cutting any 4.5m tarmac artificially lit cycleway through Birch Woods (incorrectly titled Martlesham Woods), Portal Woods & the open land at the back of Whinfield/Broomfield. To cut such a tarmac cycleway would involve the destruction of many mature trees & would fundamentally change the whole concept of open sandy paths that the residents currently enjoy either exercising or walking dogs. The existing cycle/foot bridge over the A12 alongside Martlesham Leisure could be upgraded, if thought necessary, & the existing paths & roads on Martlesham Heath could be altered to accommodate more cyclists. This can be achieved by making the entire Martlesham Heath enclave a 20 MPH Zone from the only two access junctions at the BT & Tesco roundabouts on the A12. The roads could be rebuilt to use the same traffic calming methods used on the Ravenswood development that has the same 20 MPH Zone so that cyclist, walkers & children would be safe.</p> <p>In any event none of the proposed construction will be allowed on any of the open or wooded land owned by MHHL as it is all covered by a Section 52 restriction placed on it by the original developer Bradford Property Trust. In fact Bradford Property Trust over 40 years ago thought to put tarmac</p>	

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				paths through Birch Woods after they had put the Section 52 restrictions in place & found that they were not allowed to do so. Having been in contact with Bidwells Solicitors in Cambridge who drew up the original Section 52 restrictions I was informed only a full act of Parliament could lift the restrictions in the event that one of the parties to the agreement did not agree. As a shareholder in MHHL I have instructed them to oppose any construction, which I understand has the support of all the directors.	
IM10	Paul Newman	859	No	<p>In particular two corridors IM10 & IM12 where they are constructed on open or wooded land owned by Martlesham Heath Householders Ltd (MHHL) whose shareholders are all residents of Martlesham Heath.</p> <p>I am fundamentally opposed to cutting any 4.5m tarmac artificially lit cycleway through Birch Woods (incorrectly titled Martlesham Woods), Portal Woods & the open land at the back of Whinfield/Broomfield. To cut such a tarmac cycleway would involve the destruction of many mature trees & would fundamentally change the whole concept of open sandy paths that the residents currently enjoy either exercising or walking dogs. The existing cycle/foot bridge over the A12 alongside Martlesham Leisure could be upgraded, if thought necessary, & the existing paths & roads on Martlesham Heath could be altered to accommodate more cyclists. This can be achieved by making the entire Martlesham Heath enclave a 20 MPH Zone from the only two access junctions at the BT & Tesco roundabouts on the A12. The roads could be rebuilt</p>	Objection noted.

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				<p>to use the same traffic calming methods used on the Ravenswood development that has the same 20 MPH Zone so that cyclist, walkers & children would be safe.</p> <p>In any event none of the proposed construction will be allowed on any of the open or wooded land owned by MHHL as it is all covered by a Section 52 restriction placed on it by the original developer Bradford Property Trust. In fact Bradford Property Trust over 40 years ago thought to put tarmac paths through Birch Woods after they had put the Section 52 restrictions in place & found that they were not allowed to do so. Having been in contact with Bidwells Solicitors in Cambridge who drew up the original Section 52 restrictions I was informed only a full act of Parliament could lift the restrictions in the event that one of the parties to the agreement did not agree. As a shareholder in MHHL I have instructed them to oppose any construction, which I understand has the support of all the directors.</p>	
IM10	Philip Wilmot	237	No	<p>Re "This will require widening of existing infrastructure along Eagle Way, and giving priority to cyclists and pedestrians when crossing Eagle Way side streets", there is a conspicuous lack of detail, but I sense that everyone except cyclists will be de-prioritised and inconvenienced. In particular residents legitimately using their cars to access Eagle Way from their homes will be.</p> <p>Re "Introduce Eagle Way crossing point into Manor Road", presumably this means traffic lights that will often delay vehicles.</p>	<p>The Highway Code requires cyclists and pedestrians to be prioritised over motor vehicles when crossing side streets. This minor inconvenience for motor vehicles must be reflected in the design of new infrastructure. IM10 has been amended to remove the recommended cycling and walking track across Martlesham Common. Instead cycling and walking infrastructure is proposed for improvement along Anson Road, thereby connecting the A12 underpass to Felixstowe Road. The effect of a modal filter on Felixstowe Road would be to direct motor vehicles onto the A12 and then Main Road, resulting in a minor detour.</p>

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				<p>Re "When introducing track to heathland north of the A12, route should follow existing desire line and avoid vegetation removal where possible", I cannot see how a wide, lit path can be put across a vegetated area without removing vegetation. This statement means nothing more than that unnecessary work should be avoided during construction!</p> <p>Re "Introduce modal filter on Felixstowe Road to prohibit vehicle through traffic". Could we have this in English please? Also doesn't such a radical proposal merit greater visibility? If I understand what is being hinted at you want to stop cars and vans traversing Old Felixstowe Road other than for access. Is this wise? What will be the effect on traffic flows along the A12?</p>	
IM10	PSP Consulting (Patrick Gurner)	245	Yes	<p>We have reviewed the draft 2021 East Suffolk Cycling and Walking Strategy and we have been asked to make the following comments on behalf of the landowners who front the east side of Felixstowe Road in Martlesham, to the north of the RSPCA Unit, running up to Three Stiles Lane.</p> <p>Our Clients' are concerned that the current cycle lane road markings are not fit for purpose and the overall layout, including the road signs saying that 'vehicles may enter cycle lane when clear', creates unnecessary confusion and potential for conflict. Our own traffic surveys on Felixstowe Road have shown that there are more vehicles and less cyclists than one might expect, indicating that the cycleway</p>	Support is noted. While there may be a variety of ways in which Felixstowe Road could be improved for cyclists and pedestrians whilst ensuring vehicle access to all properties is retained, the Strategy's proposed approach is for a modal filter that would restrict vehicle through traffic but allow cycling and walking.

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				<p>scheme as it currently stands is not working.</p> <p>We note from Chapter 3 of the draft Cycling and Walking Strategy, that Ipswich to Melton (IM) is identified as a 'Key Corridor' (paragraph 3.6) and the plan on page 19 of the draft Strategy shows the route of Cycleway IM10 running along Felixstowe Road past our Clients' land. The table on page 20 then identifies IM10 as 'very high priority route' and proposes the introduction of a <i>modal filter on Felixstowe Road to prohibit vehicle through traffic.</i></p> <p>We can confirm that our Clients support this proposal.</p> <p>The final part of the draft Strategy looks at the Community Recommendations from the Initial Map-Based Consultation exercise, which ran from October to December 2020 (Chapter 3 starting at page 151). Felixstowe Road is considered on pages 189 and 190 and the solution/improvement suggested by the Community, is to make Felixstowe Road one-way and provide a segregated cycleway.</p> <p>With Felixstowe Road operating 'one-way', traffic in the opposite direction would need to use the A12. Routing part of their journey along the A12, is likely to be a concern to more vulnerable members of the local community who are travelling by car to/from the Martlesham Community Hall. Cyclists travelling in the opposite direction to one-way car movements along Felixstowe Road could also lead to confusion. The implications of a 'one-way' scheme would thus</p>	

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				<p>need careful consideration to see if it is viable.</p> <p>When the Planning and Highway Authorities are considering the options for the 'modal filter on Felixstowe Road to prohibit vehicle through traffic', there may well be other traffic management measures that should be considered in the round.</p> <p>In summary, our Clients support the draft 2021 East Suffolk Cycling and Walking Strategy and we agree that there is a need for cycling (and walking) improvements along Felixstowe Road in Martlesham.</p> <p>We would be grateful if you could take the above comments in account as you progress your next steps.</p>	
IM10	Robin Guy	265		<p>IM10 proposal is on land owned by MHL on behalf of Martlesham Heath residents and I oppose its use for formalised paths/cycle ways. This land is already used extensively by local people with the attraction being that it is open land. IM10 will cross a very busy 'junction' by the play area by IM7 (especially should BT return to pre pandemic working routines) and again immediately prior to the path swinging towards Dobbs Lane. I would also draw your attention to the area where this path is intended to join Dobbs lane at Dobbs Corner. This is a very tight bend with very poor visibility with an threat to life as cyclists/pedestrians & vehicles meet.</p>	<p>The safety of users is of paramount importance and for this reason IM4 has been amended to recommend the provision of a signalised crossing on Dobbs Lane where IM4 and IM10 meet, as well as a village gateway to slow northbound vehicles before the signalised crossing and entering Kesgrave.</p>
IM10	Simon Shaw	582	Yes	<p>The route from IM4 joining IM10 at the bend in Dobbs lane is dangerous. The bend is blind and drivers approach from Foxhall at high speed. Traffic calming measures would be required to make this</p>	<p>The safety of users is of paramount importance and for this reason IM4 has been amended to recommend the provision of a signalised crossing on Dobbs Lane where IM4 and IM10 meet, as well as a village gateway to slow</p>

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				crossing safe. I use this route to cycle from Martlesham Heath to ransomes europark and on my return do not turn right here I carry on and turn at Gorselans school.	northbound vehicles before the signalised crossing and entering Kesgrave.
IM10	Smith, Carolyn	782		<p>Thank you for consulting on your draft cycling and walking strategy; hopefully this will enable you to learn from local knowledge and prevent you from making expensive mistakes which will damage precious heathland and adversely impact well used green spaces in the Martlesham and Kesgrave areas.</p> <p>I am in favour of encouraging walking and cycling, but not at the expense of damaging scarce habitats and the loss of local amenity, all of which will occur if the routes IM4, IM9, IM12 and IM10 are constructed in their proposed form.</p> <p>Routes IM12, IM10, IM4</p> <p>Why are you proposing surfacing over and urbanising valued green spaces, important for recreation and wildlife?</p> <p>This is in direct contravention of the SC Local Plan Policy SCLP10.1 (Biodiversity and Geodiversity).</p> <p>Namely:</p> <ul style="list-style-type: none"> • the woodland to the west of Dobbs Lane, and Longstrops (IM4) • Portal Woods (IM9) 	IM10 does not run through the Martlesham Heath SSSI. In order to preserve the natural environment at Martlesham Common IM10 has been amended to remove the recommended cycling and walking track across Martlesham Common. Instead cycling and walking infrastructure is proposed for improvement along Anson Road, thereby connecting the A12 underpass to Felixstowe Road.

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				<ul style="list-style-type: none"> • Martlesham Birch Woods (IM12), • the heathland to the northern end of Martlesham Heath SSSI, and southern end of Martlesham Common (IM10) • Improve the existing A12 crossing to the north of the BT roundabout and improve IM13 to encourage the use of this route across the A12 instead of spending additional money on a crossing to the south. <p>Or:</p> <p>Extend IM13 southwards to a new bridge crossing over the A12 north of the Foxhall Road/Waldringfield Road roundabout, and continue on a new route through the edge of the field alongside Foxhall Road to Bell Lane, and continue along the field edge to connect with IM4 west of Bell Lane.</p> <p>Construct a bridge crossing the A12 to connect IM13 with footpath PROW6, re-route PROW6 alongside the wooded area to the south and then across to Foxhall Road, and continue eastwards along the field edge towards Bell Lane as above.</p> <p>Both of these options give an opportunity to construct a wildlife corridor alongside this route, with screening trees and wildflower planting.</p>	
IM10	Stephen Denton	535	No	I strongly object on the following grounds:	IM10 seeks to provide a coherent and safe cycling and walking route linking the proposed improvements to

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				<p>a) using as a main route the Tesco underpass which is too narrow for shared use and has a blind T junction</p> <p>b) creating a route across the commonland which is being managed to encourage flora and fauna</p> <p>c) delivering traffic travelling to and from the west of Martlesham going to and from Woodbridge, onto the Felixstowe Road at the point where it starts to narrow and becomes even more unwelcoming to cyclists. See attached photo of traffic queuing Crown Point - not easy for cyclists to manoeuvre.</p> <p>Bear in mind that a route via Portal Avenue, leading to Main Road via the underpass would be a better proposition, avoiding all the above issues, for this longer distance route.</p> <p>I also object to the creation of an off-highway shared track along the north side of Eagle Way. This is unnecessary as this is a wide road with good visibility and relatively light vehicular traffic.</p> <p>Martlesham Neighbourhood Plan says that shared pedestrian cycle routes are problematical - fast cyclists don't mix well with pedestrians. My son was knocked off his bike by a speedy commuter near Gorseland school.</p> <p>The Martlesham NP survey showed that 55% of those using cycle paths would not use a marked cycle lane on the highway.</p>	<p>Longstrops Bridleway and Felixstowe Road, whilst making use of existing traffic free infrastructure such as the A12 underpass. While the underpass may not be of the desired width it provides a high quality connection across the A12. In order to preserve the natural environment at Martlesham Common IM10 has been amended to remove the recommended cycling and walking track across Martlesham Common. Instead cycling and walking infrastructure is proposed for improvement along Anson Road, thereby connecting the A12 underpass to Felixstowe Road. Felixstowe Road is proposed to be modal filtered, preventing vehicle through traffic while allowing cycling and walking. Whilst Eagle Way may be considered a safe cycling route for some users, it will not meet the needs of all users, which is why segregated cycling and walking infrastructure is proposed. This will need to be designed to ensure the continued safe operation of the bus stop.</p>

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				<p>Also, assuming it refers to the bus stop opposite Manor Road, the suggestion to move that bus stop closer to the road is unacceptable. Firstly it is an attractive setting with nicely maintained beds either side, and secondly it implies closing the bus stop lay-by so the buses stopping there would be on the main carriageway near a junction making it more dangerous for cyclists who chose to use the road.</p> <p>I do agree that the crossing from Manor Road to the subway approach needs to be made much safer.</p> <p>Rat-running through old Martlesham and the Felixstowe Road with the resultant extremely high levels of traffic is the single biggest hindrance to creating a safe cycle corridor between Martlesham and Woodbridge. This needs to be addressed - is there any evidence yet as to the outcome of the A12 improvement consultation. That consultation did not have any information about the effect on local roads of the various options, unlike previous consultations about A12 improvements. This lack of information in the A12 consultation was an insult to the intelligence of the consultees in my opinion.</p> <p>As a footnote, if the councils collectively were serious about modal shift, they would reinstate the massive cuts (I estimate 90%) that reduced the route 66 through Martlesham Heath to an hourly service operating over much reduced hours. Bear in mind that the 66 still goes through the retail/industrial</p>	

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				<p>area but completely by passes Martlesham Heath for most of its timetabled service.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
IM10	Stephen Read	37	Yes	<p>There is no mention of improving the now dangerous section of NCR 1 along Felixstowe Road, between Anson Road and Main Road, Martlesham. The traffic is so heavy now with the increase in the size of the retail park. It should be made one way from Anson Road, with contra flow cycling.</p>	<p>Recommendation IM28 proposes to introduce a modal filter on Felixstowe Road to prohibit vehicle through traffic whilst allowing cycling and walking.</p>
IM10	Tony Ellison	320		<p>Next I have concerns on the lighting in Portal woods. This is a known bat roost and LED lighting is known to cause distress to wildlife. I have cycled through these woods many times at night and although the surface is broken in places I would not like to see these woods dug up to install lighting and the impact that would have on the environment.</p> <p>The area I have marked in red does need a new surface as it is just mud at this time.</p> <p><i>The map has not been published due to potential copyright concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	<p>The area in question would require a new surface. However, the most appropriate surfacing solution will need to be considered as the recommendation is taken forward.</p>
IM10	West	621	No	<p>The crossing point at Dobbs lane is a blind corner and should not be an option for a proposed cycle crossing. The cycle way should be extended up Dobbs lane to a new track south of Gorseland Primary School, or North where the IM7 crosses. This</p>	<p>The safety of users is of paramount importance and for this reason IM4 has been amended to recommend the provision of a signalised crossing on Dobbs Lane where IM4 and IM10 meet, as well as a village gateway to slow</p>

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				would negate the need to add a new tarmac/lit cycleway through woodland and along the back of Broomfield. Why not upgrade or reroute the walkway near the control tower to connect the IM7 to the IM10 this could be changed to a cycle/walkway	northbound vehicles before the signalised crossing and entering Kesgrave.
IM11	Alasdair MacLeod	540		<p>This is being uplifted by the McCarthy & Stone development and with improvement to the tight turn in Broomfield (ref 66, there is ground available) could be continued through to link up with the proposed IM7/IM10 junction and be the natural primary route through Martesham.</p> <p>The Broomfield to Eagle Way path is the main route taken by pupils travelling between Martlesham Heath and Kesgrave High School and Gorselands Primary School. It is unrealistic to downgrade this to a footpath and expect the cycle traffic to reduce - pupils will continue to use it and without proper segregation the path will become more hazardous. A comment highlights the sharp corner on this route. There is a flower bed adjacent the sharp corner. Could the path be shifted into the area of the flower bed to reduce the angle? (i.e. swap ground use so the tight leg is made flowerbed?) Fixing this corner would enable an extension of IM11 through to IM7 and align with natural usage.</p> <p>(and an extended IM11, with proper signage would obviate the need to tar over more ground along IM10).</p>	The permitted development along IM11 will deliver part of the IM11 route. The Strategy does not propose to remove the painted segregation between cyclists and pedestrians on any existing infrastructure. IM10 will provide a safer route for cyclists and pedestrians than the current Broomfield arrangement.

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IM11	Chris Adelson	455	Yes	Gloster Road cycle lanes are very narrow. Slow the traffic down and prioritise cyclists and walkers so that traffic has to give way.	IM13 recommends the introduction of segregated cycling and walking infrastructure along Gloster Road, and has been amended to recommend the introduction of cycling and walking crossing points along Goster Road at appropriate desire lines, and more cycle parking.
IM11	Clive Roper	574	No	<p>This is a quiet route used by local residents who like to get away from the noise of the A12 and A1214. The existing path is in reasonable condition and only minor maintenance being required. The introduction of energy efficient whiote LED lighting would be harmful to all the noctural animals, birds and insects in this wood. There is considerable research stating that the white light seriously affects them.</p> <p>Part of the route is in the SSSI area and what what considerations or permissipons have been obtained for creating a new path.</p>	The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward. None of the Strategy's recommendations enter the Martlesham Heath SSSI.
IM11	John Kelso	778	No	Existing footbridge crossing over A12 should be replaced with a new bridge of a width which would enable the creation of a legal cycle/Pedestrian route. Any new bridge should contain non slip surfacing. Are ESC aware of the SCC proposals to create a bus gate near the North Green carpark in order to create a new bus route a long Valiant Road ? I would suggest that a bus route alongside a cycle/pedestrian route along this narrow section of Valiant Road could prove dangerous especially as this section is widely used by children going to the village centre, village green and Birchwood school.	The existing cycling and walking bridge over the A12 provides an important cycling and walking route, and improvements to the bridge would be welcomed, however are not essential. ESC and SCC have working together closely throughout the preparation of the Strategy. Bus, cycling and walking infrastructure can work seamlessly together and provide for a safe cycling and walking environment.
IM11	Martlesham Conservation Group (Phil Smith)	898		<p>IM11 /IM14</p> <p>a) The bridge across the A12 should be upgraded as it is currently too narrow and would better support</p>	The existing A12 cycling and walking bridge provides a highly valuable connection between Martlesham Heath, the retail park and beyond. It is of sufficient width, however improvements to it would be of benefit. IM11

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				<p>the route of IM11.</p> <p>b) The route of IM11 should join with IM14 and not cross over. Spratt's Plantation woodland should not be totally surrounded with routes. It would be best to retain the west (industrial) side only.</p> <p>c) Why does IM11/IM14 not continue south along the east side of the BT site to link with IF33? There is a missed opportunity here.</p>	<p>has been amended to remove the proposed cycling and walking track along the north and east of Spratt's Plantation and instead join IM14 at the end of Betts Avenue. In order to highlight the important role Brightwell Lakes will play in improving cycling and walking infrastructure in the area, IM31 has been incorporated into the Strategy. IM31 highlights the cycling and walking infrastructure that will be delivered by Brightwell Lakes. IM11 connects to IM31 and other infrastructure proposed to adjoin IM31.</p>
IM11	Martlesham Heath Householders Ltd (Sir/Madam)	286		<p>19. The Board notes that cycle route 1 and route IM11 coincide up to Eagle Way. Although ESC believe the current route through Broomfield in somewhat narrow, the Board is happy to discuss using part of the MHHL land (the mature flowerbed with trees in it), to smooth out the Broomfield "kink".</p> 	<p>MHHL's suggestion to use the planted area near Broomfield to better align the existing cycling and walking route is welcomed. However, the planned route along IM11 would provide a safer arrangement for cyclists and pedestrians.</p>

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				20. The Board notes that as part of the now approved McCarthy and Stone development, the plans show a 3 metre wide shared cycle /footpath along northern side of the site, thereby avoiding the public highway along Eagle Way (IM11/Cycle Route 1).	
IM11	Martlesham Parish Council (Diane Linsley)	886		<p>The section of IM11 around the north side of Spratt's Plantation should be left in its present state as a quiet leisure route as it passes very close to the park homes which are sited almost hard up against the fence separating them from the path.</p> <p>Also see comments under IM14</p> <p>IM14</p> <p>Similarly, IM14 in conjunction with IM11, provides important links within Martlesham; it will be the most convenient route to the retail and industrial areas and the A12 footbridge for residents in the eastern half of Brightwell Lakes. To improve these links, the existing footbridge needs to be widened, and Felixstowe Road restored to its status as a cycle priority route as mentioned elsewhere in this response.</p> <p>Having said the above, the Parish Council is surprised and disappointed that the so-called new northern quadrant route has not been proposed to carry the stretch of IM11 between IM14 and Gloster Rd. This would present the opportunity to create a purpose built route for all user types from scratch, and avoid the multiplicity of business access points along Betts</p>	<p>IM11 has been amended to remove the proposed cycling and walking track along the north and east sides of Spratt's Plantation, and instead end at the end of Bett's Avenue adjoining IM14. The existing cycling and walking bridge over the A12 is sufficient in width, however improvements would be welcomed. A modal filter is proposed for Felixstowe Road in order to prohibit vehicle through traffic whilst allowing cycling and walking. In relation to Betts Avenue, IM11 has been amended to reference the planned cycling and walking infrastructure along the northern Brightwell Lakes access. In order to highlight the important role Brightwell Lakes will play in improving cycling and walking infrastructure in the area, IM31 has been incorporated into the Strategy. IM31 highlights the cycling and walking infrastructure that will be delivered by Brightwell Lakes.</p>

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				<p>Avenue, and would even be a viable alternative route to parts of the retail area and the footbridge for residents in parts of the western section of Brightwell Lakes.</p> <p>Consideration should be given to a part time pedestrian/cycle entrance into the north east corner of the Adastral Park complex to allow north bound commuters from Adastral Park to pick up IM14 thus avoiding the rigours of Gloster Rd at rush hour and the staggered T junction at its northern end.</p>	
IM11	Martyn Shakespeare	499	No	<p>May be better to route via The Drift and Douglas Bader rather than following existing route. Car park is being developed as Care Home; make developers include this within the planning permission. Wider bridge (and slightly wiser and less steep access on industrial site side) would be useful, as would some resurfacing.</p>	<p>The existing A12 cycling and walking bridge provides an important cycling and walking route, and while improvements to the bridge would be welcomed they are not essential.</p>
IM11	Nik Bestow	19	Yes	<p>The East end should be modified. The loop around the woods is unnecessary. the path should continue down the already existing road towards the unused heliport and connect up with PROW7A at the crossroads.</p>	<p>IM11 has been amended to remove the proposed cycling and walking track along the north and east sides of Spratt's Plantation, and instead end at the end of Bett's Avenue adjoining IM14.</p>
IM11	Paul Davey	326		<p>As a keen walker and cyclist I am very much in favour of ESC having a strategy for my benefit.</p> <p>However as a resident of Martlesham Heath I considerable concerns regarding the draft proposals published this year.</p> <p>My objections and reasons are listed below.</p> <p>5. The purpose of IM11 and IM14 (popular walking</p>	<p>IM11 follows one of the few cycling and walking connections across the A12 and is of significant importance in ensuring easy and safe access between Martlesham Heath, the retail park and beyond. The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.</p>

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				<p>routes) is not clear when good alternatives are available.</p> <p>In conclusion I would prefer ESC to properly maintain existing footpaths and cycleways rather than spend large sums on unnecessary projects which are never used.</p>	
IM11	Paul Jordan	9	Yes	<p>I believe that the existing footbridge over the A12 is designated on the definitive map only as a footpath and not a bridleway. This needs to be upgraded to match current use. See https://www.bhsaccess.org.uk/dobbin/pathdetails.php?id=east/2026Suffolk&password=2026&pathid=SU-1059&gridref=TM2470145390&notes=</p> <p>I have raised this issue with Martlesham Parish Council</p>	The cycling and walking bridge over the A12 is not identified on the Suffolk Definitive Map as a Footpath or a Bridleway.
IM11	Peter Kerridge	404	No	<p>The author is showing a very poor understanding of Martlesham Heath and ignoring the already numerous paths and cycle ways that already exist. He/she also seems to think that Eagle Way is some sort of heavily trafficked road that requires special crossing points for cyclists whereas in reality nothing could be further from the truth. In fact Eagle Way is so quiet that cyclists use its circular design as a training area. Valiant Way, barring school drop offs, is quieter still and the proposal to widen the cycle path rather than just allowing cyclists to use an already quiet road seems to be a job creation/justification scheme.</p> <p>The map doesn't show where either footpaths 42 or 51 are. Indeed if I use the search tool on the map</p>	Eagle Way may be considered a safe cycling and walking environment for some. However, it doesn't meet the needs of everyone. The interactive map provides the Public Rights of Way network, which can be viewed across the district, including Footpaths 42 and 51. The planning permission along the route of IM11 has secured cycling and walking infrastructure that deliver part of IM11 and therefore aligns with IM11.

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				<p>and search for footpath 42, it takes me to India!</p> <p>The proposal to put a cycle track on Eagle Way fronting the car park ignore the fact that your own planning dept has given permission for this to be developed by McArthy Stone and they have already fenced the area off.</p>	
IM11	Stephen Denton	550	No	<p>I support parts of this proposal but without a clearer explanation of some of the proposals I cannot support this at it stands.</p> <p><i>1) The existing crossing point on Eagle Way (just north of Eagle Way/Valiant Road junction) may need to be moved further south to facilitate a cycling and walking track on the east of Eagle Way.</i></p> <p><i>I oppose moving the bus stop south as it would place it too close to the Valiant Road Junction. Also where would the track on the east side of Eagle Way run and for what purpose. Children currently coming from the west tend to cross Eagle Way and up onto the pavement and round the corner onto Valiant Road, adults stay on the road. This seems to work OK. Have you visited the area at school opening and closing times when there are cars parked on both Eagle Way and Valiant Road?</i></p> <p>Where do you propose a crossing point on Valiant Way - for pedestrians it would ideally be west of the junction with the drift although it would then be close to the junction with Saddlers Place. For cyclists going through to the bridge it would be need to be</p>	<p>The purpose of IM11 at Eagle Way and Valiant Road is to provide a more continuous cycling and walking environment to ensure the safety of cyclists and pedestrians when crossing Eagle Way and Valiant Road, whilst ensuring the continued safe and desired operation of the bus stop. In relation to Betts Avenue, IM11 has been amended to reference the planned cycling and walking infrastructure along the northern Brightwell Lakes access. In order to highlight the important role Brightwell Lakes will play in improving cycling and walking infrastructure in the area, IM31 has been incorporated into the Strategy. IM31 highlights the cycling and walking infrastructure that will be delivered by Brightwell Lakes.</p>

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				<p>just before the 5mph signs. But on on balance I have never observed a need for such crossing points. However I have seen people walking (some with prams) on the grass verge on the south side of Valiant Road between The Drift and the car park entrance. It needs a simple footway along there. It should be noted that having passed the car park entrance it becomes a cycle route only until the Saplings Nursery where it becomes a shared route. In summary there need to be a continuous footway all along the south side of Eagle Way from Birchwood school as far as the Saplings - this a logical route for parents with children at school and nursery. See streetview screen grabs of this area.</p> <p>Is there any reason why the northern quadrant route into BL has not been proposed for IM11 east of Gloster Road. This will be created from scratch and avoid the entrances to the vehicle intensive businesses along Betts Avenue?</p> <p>Attachments: <i>The attachments have not been published due to potential copyright/data protection concerns, but these were still fully considered and assessed in forming the Strategy.</i></p>	
IM11	Sue Sapsed	618	No	This proposal would cause additional problems within the area of Martlesham Heath which is already a busy road and pedestrian route to essential services in the square.	The recommendation would not impact pedestrians or drivers in accessing the centre of Martlesham Heath.
IM11	Sue Sapsed	867		The existing bridge is already used by many residents and serves its purpose well. It impacts little on the	The Strategy does not seek to move the existing cycling and walking bridge over the A12. The Draft Strategy proposed a new cycling and walking bridge over the A12

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				environment whereas moving it further south would increase its impact.	between Martlesham Heath and Brightwell Lakes. However, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM11	Taylor Wimpey East Anglia (Sir/Madam)	761		<p>Cycling and Walking Strategy</p> <p>Brightwell Lakes is identified within the “Ipswich to Melton Key Corridor” and a number of the recommendations within the Cycling and Walking Strategy relate to existing connections in this part of East Suffolk. Of particular relevance is the following:</p> <ul style="list-style-type: none"> IM11 Valiant Road/Betts Avenue (Very High Priority): this is described as using the existing route which connects the Retail Park to Martlesham Heath via the existing footbridge. The recommendation is to introduce segregation for cycling and walking and upgrade the connection points. It provide access to the northern part of the Brightwell Lakes site. In future phases of development at Brightwell Lakes this can become a route which the residential areas can connect into. 	In order to highlight the important role Brightwell Lakes will play in improving cycling and walking infrastructure in the area, IM31 has been incorporated into the Strategy. IM31 highlights the cycling and walking infrastructure that will be delivered by Brightwell Lakes.
IM11	Tony Ellison	319		Can I introduce myself as a cyclist in the Mratlesham Heath I commute to Adastral park frequently and frequently cycle on and off road for leisure with my 3 daughters and wife.	Support for the Strategy is noted. However, the Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered

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				<p>First of all I do commend the strategy for cycling it is definitely the a great initiative and also commend that work is being planned before any houses are being built in the Adastral Park area.</p> <p>First of all I do have a concern on the safety of the current route I have to cycle to Adastral Park over the A12 bridge. The phot below was taken on a frosty day and a frozen puddle can be clearly seen. I am sure it does not have to explained why this blocked drain is such a hazard when we have cold weather. Please could you report this or tell me how to report it.</p> <p><i>The image has not been published due to potential data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	<p>through the planning system while maintenance cannot. Highway maintenance issues can be reported to Suffolk County Council as the highways authority, via their online reporting tool: https://www.suffolk.gov.uk/roads-and-transport/roads-pavements-and-verges/report-a-highways-issue/.</p>
IM12	A. Thompson & H Phillips	748	No	<p>We are writing to totally oppose the above. We moved to Martlesham Heath 15 years ago and counted ourselves very lucky to have found such a lovely village to live. We were also excited to find somewhere that could not be built on.</p> <p>The natural surroundings, the heathland and woods , are so lovely and not only good for the environment but also our well being. To be able to hear the wide selection of birdsong and see wildlife and plants are a big plus.</p> <p>To cut down trees in the woods would be a disgrace and we should not encourage this in anyway. Especially when there is no good reason and to build</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>a bridge across from Brightwell Lakes to Martlesham Heath and create a tarmac path would be unthinkable. We as residents of Martlesham Heath have the right to enjoy our village and residents at Brightwell Lakes should have their own walks and cycle paths and not interfere with ours.</p> <p>Please, please stop this idea of joining two villages together.</p>	
IM12	Abbie Simpson	221	No	<p>As someone who has lived in Martlesham Heath for over 34 years, I strongly oppose the idea of introducing additional cycle paths to our village.</p> <p>Introduction of these proposed changes will negatively impact village life, natural spaces and wildlife.</p> <p>However a 'cycling and walking strategy' may be dressed-up as being 'eco' or 'green', there is no escaping or excusing the fact that the building and implementation of new cycle paths will permanently damage the nature and wildlife that currently exists in our village. This includes deer, foxes, hedgehogs and birdlife. The Martlesham Woods are also an essential dog-walking route for many residents, an introduction of a cycle path would make this space less safe to do so and for the many children who enjoy the woods.</p> <p>It has been calculate that cycling along Eagle Way will take less than 40 seconds more that the proposed cycle path would take, I cannot see how introducing a cycle path will be of any benefit to our</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				village. It is merely a box-ticking exercise for the council.	
IM12	Alan & Yvonne Laws	698	No	<p>We would like you to know our view on the proposed cycle/footpaths on Martlesham Heath.</p> <p>We are opposed to the IM12 medium scale as we live in Broomfield Mews, when trying to get out onto Eagle Way we will have to stop to let cyclist and pedestrians in both directions. There is no info regarding the cycle/pathway from the western corridor to Eagle Way, is it to be closed or left open for use as it is dangerous the speed people travel through it with its blind corners.</p> <p>We are appalled that Martlesham Heath is having to make accommodation for the proposed bridge over the A12 for the new housing development and disturbing 40 years of designated footpaths, conservation areas.</p> <p>Will we the residents then have to contribute more money to keep the Cycle/ footpaths maintained in the future although we oppose it or will the new Brightwell Lakes development be responsible for the maintenance now and years to come.</p> <p>MHHL have problems keeping Martlesham Heath maintained at the moment ie; footpaths kept clear, overgrown trees blocking the pathways</p> <p>This information is probably a waste of our time due to previous developments being passed although opposed ie: Retirement flats on the Square,Police</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				headquarters, alterations to A12 which will never work after having lived on Grange Farm while traffic lights were at each roundabout, once they were removed traffic flowed with no problems.	
IM12	Alan Pitt	595	No		Objection noted.
IM12	Alan Ridge	745	No	<p>I have lived on Martlesham Heath for over 40 years and would like to comment on the cycle/ walking routes under consideration on the Heath.</p> <p>I strongly object to the Route IM 12 for several reasons.</p> <p>1 It is virtually of no benefit to residents on the Heath, we have managed without it very well for 40 years.</p> <p>2. How can Suffolk County Council square this idea with their environmental policy because it would require the cutting down of trees and the destruction of a well established wild habitat. The 200 trees they are donating to local communities will take a long time to make up for the damage caused.</p> <p>3. The cost of building a bridge over the busy A12 dual carriageway would cover the cost of constructing miles of footpath / cycle ways. The money is better spent on improving what we have.</p> <p>4. Most of the proposed routes appear to concentrate on the North of the village so people coming from Brightwell wanting to cycle towards Ipswich need to get to the North of the village anyway. The existing bridge and cycle path (route IM</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>11) can be used for this making the bulk of IM12 surplus to requirements.</p> <p>5. The walkers amongst us would much prefer to walk the woods using the natural paths that are there already and not a tarmac path that needs maintenance and causes more light pollution at night.</p> <p>Whilst I realise this is only a consultation document, when and how, will we the residents get some feedback, to show our views have at least been considered?</p>	
IM12	Alan Sansom	129	No	<p>I object to route IM12 through “Martlesham Woods” (Birch Woods) in Martlesham Heath.</p> <p>The woods are owned by MHHL on behalf of the residents of the village, and are a valuable rural environment adjoining the green in the middle of the village. There are informal paths through it, in keeping with a woodland environment.</p> <p>There are a variety of walking and cycle routes in Martlesham, and the proposed new route is a few hundred metres from National Cycle Network Route 1.</p> <p>Rather than destroying woodland, cyclists can use existing residential roads in either direction around the village, all of which have a maximum 30mph speed limit and street lighting.</p> <p>Valiant Road, Sidecentre Gate and Eagle Way is</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>approximately 1km, whereas the proposed new route along Eagle Way and through the woods is 800m (and pedestrians can use this route). A 650m cycle path created through the woods to save 200m travel distance on a bike seems completely unnecessary.</p> <p>My objections are based on the following:</p> <p>Amenity</p> <p>A track which will presumably have a tarmac surface with segregation lines, tactile paving, direction signs, mandatory signage on posts, bollards, and artificial lighting is out of character in a natural woodland setting, and more akin to an urban landscape.</p> <p>The construction, infrastructure (electricity, surface water drainage) and maintenance of it would be detrimental to wildlife and ecosystems, and would necessitate the removal of native trees, shrubs and plants. 650m of 3m wide path would sacrifice almost half an acre of land, in this case woodland.</p> <p>Ecology and wildlife</p> <p>The woods are an intrinsically dark landscape that support habitats for native nocturnal animals. Bats can be observed flying close to the woods and nightingales can be heard in the woods throughout spring and summer.</p> <p>Artificial lighting will have a negative impact on</p>	

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				<p>wildlife and ecosystems.</p> <p>Local Planning Policy</p> <p>Neighbourhood Plan Vision 3 Respect and protect the natural environment and the open spaces between the built-up areas states that there is very strong and widespread local support for the continued protection and enhancement of the spaces, and sets the continued protection and enhancement of their qualities and benefits as its principal objective.</p> <p>Character</p> <p>The neighbourhood plan also refers to open spaces as being important both individually and collectively for a variety of reasons, including visual amenity, formal and informal outdoor recreation. Their usage and their management by volunteer groups contributes significantly to a sense of local identity, well-being and social cohesion.</p> <p>Part of the village character is the separation of the residential hamlets, and the carefully planned concept of links between them. A new formal segregated cycling and walking track through the woods would be out of context, and bisecting the village in this fashion would not sympathetic with the original planning and design principles.</p>	

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				<p>Noise</p> <p>Potential noise from commuting cyclists and increased number of pedestrians travelling through what is generally a relatively peaceful area of woodland. The difference would be particularly noticeable between dusk and dawn, when the unlit woods are currently little-used and quiet.</p> <p>Greenest County</p> <p>Destruction of woodland for an illuminated cycle track doesn't seem very green.</p>	
IM12	Alasdair MacLeod	541	No	<p>I am opposed to IM12.</p> <p>This will inevitably lead to pressure on the woods in Martlesham Heath and the SSSI - more footfall and more maintenance needed. The woods are private property (but open to all) and maintained at the cost of the residents. It is unjust to push this route forward.</p> <p>This would degrade the woodland. The woods are enjoyed as a patch of quiet woodland that families enjoy exploring, the proposed route through the woods would destroy that feeling of being enclosed in the woods - it would spoil them. It is effectively driving a road through the woodland.</p> <p>It would introduce light pollution (damaging to animals and our mental health), damage habitats and in general spoil an area of quiet recreation.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>Risk of collision. Young children and dogs could no longer be allowed to wander at will along the path. The risk of collision with speeding cyclists would force young families to keep clear of the route when letting children wander. This would be a massive loss. When they were toddlers, I would take my children into that very area of the woods and they would amble along, picking berries and generally exploring as children do, IM12 would make that too risky.</p> <p>The IM12 route will result in an unofficial de-facto route cutting across the SSSI, bringing irreparable damage to the SSSI. In it is unrealistic to think walkers and especially cyclists traveling from Brightwell to Kesgrave will follow the proposed route through the woods and then turn right onto Eagle Way. It is far more likely they will travel through the woods and cross Eagle Way, continuing in a straight line, cutting across the SSSI. This will put even more pressure on the SSSI Habitat than there is today causing damage that will be impossible to make good.</p> <p>Loss of haven for wildlife. The woods by Lancaster Drive do not have any official paths, they have been left as a haven to wildlife. IM12 would cut through these routes and bring much more human activity into that area to the detriment of wildlife.</p> <p>By-passes Martlesham Village. IM12 would undermine linking of the communities either side of the A12. Cyclists from Brightwell would pass through</p>	

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				Martlesham Heath land ignoring the business and services in the center of Martlesham Heath.	
IM12	Alison Wright	127	No	Whilst I am keen on encouraging cycling and support improvement to the existing cycle route from the existing footbridge over the A12 to the cycle route north of the shops I strongly object to building another footbridge over the A12 so close to the existing one. Even more strongly I object to a wide cycle way with street lighting through the Birchwoods which are a haven for wildlife and safe natural area enjoyed by walkers young and old. There is no rational argument for wasting so much money producing a new cycle route where existing routes are so close and cyclists can easily use Eagle Way. We should be planting more trees not cutting them down unnecessarily. Also we should be limiting artificial lighting in rural areas to the housing and shopping areas rather than introducing lighting through natural woodland. This is a totally unnecessary and irresponsible proposal.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Amanda Shave	69	No	Destroying the woods with a cycle path is unnecessary and a wasteful use of money. The road around the heath isn't busy, you can cycle over the bridge and past the shops without the need to cycle through the woods. The woods are used by many walkers you will just create a hazard. I cannot see the point of it,	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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					Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Andreia Antunes	87	No	In regard to IM12 - in particular the section that goes from Eagle Way east to Eagle Way west. This section is completely unnecessary. It is already possible for walkers to traverse the woods safely, and it is no hardship for cyclists to go an extra quarter of a mile and just follow Eagle Way round its southern section. This is not a busy road, and is already well lit. It makes no sense to instead opt for destroying a wide section of the woodland as you propose, especially given the cost of installing a suitable surface and lighting. In the autumn, that surface through the woods would be completely covered in leaves and potentially some mud making it hazardous for cyclists. Plus, the lighting would be very disruptive for the wildlife. Cutting a wide path through a section of amenity woodland for no good purpose at a time when we are supposed to be more conscious than ever about protecting our planet doesn't seem a well thought decision. I can't see a good justification to save cyclists a 5 minute ride round a perfectly safe road.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Andrew and Simone Moore	858	No	A new bridge over the A12 and a link metalled and lit wide path through the established Martlesham Woods. The only use for this I can ascertain is to facilitate the speedier transient cycle traffic through Martlesham Heath. The construction and use of this path will cause the destruction of established trees and long term damage to the habitat.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and

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				<p>There is a perfectly good bridge further up the A 12 for local access to the Martlesham Heath Village centre which then links to all the pathways and roads on the village.</p> <p>If it is deemed that the future Brightwell Lakes development needs a further crossing over the A12 to access a link with Ipswich, may I suggest that this is built to link up with the PROW6 (parallel to the Foxhall road.) which could be developed into a lit and paved path without the destruction of woodland and habitat.</p> <p>My fear regarding IM12 is that where it will exit onto Eagle Way that cyclists instead of following the road,(Eagle Way) they will take a short cut across the SSSI land onto the proposed cyclepath IM10 to Kesgrave.(Longstrop wood) SSSI is protected but this seems to be ignored now by cyclists who “off road “ so more encouragement will create even more damage to this special area.</p> <p>Beside this most important point I cannot reconcile the thought process of having cut though a wood and put cyclists back on Eagle Way. Why they could not have followed Eagle Way which is little used and lit - all be it longer.?</p> <p>The crossing from the bridge over Eagle Way is also a recipe for accidents so all looks very poorly thought through.</p> <p>The main Kesgrave Road has cycle paths the full way</p>	<p>walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				but on so many occasions I witness that cyclists prefer to use the road than the facility constructed for them.	
IM12	Andrew Bailey	712	No	<p>I refer to the above subject, I have recently learnt that there is a plan add approximately 1 mile of tarmac'd light cycle way across MHHL land identified on the map known as IM12. This proposal seems to be a complete waste of money, not to mention the disruption to plants and trees in the path of the proposed route. There is no need for an additional crossing of the A12 as there is already an adequate crossing for this type of traffic across the A12 just 400 meters or so further away, this forms part of the cycle way known as IM11. This crossing is more than adequate for the increased volume of traffic and it is currently under utilized. This will also provide an adequate route to the rest of the area from the proposed new development Brightwell Lakes. There is a large area of SSSI that needs protecting especially as it is currently being damaged by heavy pedestrian use.</p> <p>I am sure that the money that would be required to construct this non essential route could be better used elsewhere in the district. Please consider these comments as an outright objection to the proposal as it is not needed for the reasons specified.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Andrew Johnson	427	No	<p>Whilst accepting the need for improvements to walking and cycling networks, we do object to some of the proposals outlined in the ESC Draft.</p> <p>In particular we consider that, for the following reasons, the impacts of the proposed route (IM12)</p>	<p>Objection noted.</p>

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				<p>would be detrimental to the naturalised woodland between the A12 dual carriageway and Eagle Way (East) and through Birch Woods (which the Strategy document names “Martlesham Woods”), to Eagle Way (West).</p> <p>1 Visual intrusion – The impact associated with the proposed route through both areas of woodland would be visually intrusive, would ‘urbanise’ the present environment, and have a significant impact on the enjoyment of the woodland. The associated street lighting would be inappropriate within the existing woodland and would be visually intrusive to residents in Lancaster Drive, Coopers Road, Birch Drive, Avocet Lane, Forest Lane and Warren Lane and whose properties abut the woodland.</p> <p>2 Habitat – Birch Woods has developed over many years and has become a significant and established habitat to a wide variety of indigenous flora & fauna. Whilst the paving of a significant area for walking and cycling will have minimum impact on the daytime movement of wildlife, the street lighting is likely to have a significant detrimental impact on nocturnal wildlife.</p> <p>Additional comments:</p> <p>As owners of a property on Martlesham Heath, we note that most of the proposed route IM12 passes through assets presently owned and managed by Martlesham Heath Householders Limited (MHHL), of which we are part owners. Consequently we have a</p>	

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				<p>vested interest on all proposals which directly or indirectly affect this private land.</p> <p>Presumably if ESC decides to proceed with the Walking & Cycling proposals, and any or all the owners of the land do not wish to give up the land for that purpose, then ESC will be obliged to serve Compulsory Purchase Orders on each owner (Householder) on Martlesham Heath?</p> <p>We note that the new route would probably become a dedicated highway and consequently would be managed by Suffolk County Council (Roads and Transport) as the Highway Authority.</p>	
IM12	Andrew Larder	474	No	<p>Developing this route will destroy the beautiful woods that we currently have, in which my children and I have seen all manner of wildlife on our daily visits. This is an unspoilt area, and it is very unlikely to stay that way once there is a constant flow of people that have little interest in looking after it - just look at the fires constantly set alight during the summer in the other parts of the heath. This is even more disturbing when it is considered that there is a perfectly adequate and underused route over the existing A12 footbridge, which we ourselves use regularly.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Andrew Lewis	744	No	<p>Having viewed the above strategy document, I am very much against the proposed cycle path in Martlesham Heath shown on your plan as IM12 specifically the section between the west of the A12</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling</p>

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				<p>connecting to Eagle Way between Warren Lane and Forest Lane and I wish to make the following comments regarding this as follows:-</p> <p>The land in question is owned and managed by Martlesham Heath Householders Ltd of which all freeholders living on Martlesham Heath (approximately 1200 households) own a share in that company and as far as I am aware, as one of those 1200, have not been approached regarding the purchase of this land.</p> <p>This development was created with great vision and has been used in many professional situations to demonstrate specific principals of development throughout the world and to create this “scar” through the centre is not acceptable.</p> <p>This cycle path would have a detrimental effect on the well established Birch Woods (which you incorrectly refer to as Martlesham Woods) from a nature and wildlife perspective, aesthetics, as a leisure area and the light and noise pollution. The suggestion that only a small number of trees would be felled is quite unrealistic and the area would take a considerable time to re-establish itself.</p> <p>With the advent of electrically assisted and powered cycles and scooters which are often able to reach speeds in excess of 30 MPH as well as conventionally powered cycles, I am very concerned for the safety of the many members of the public who regularly would have to cross this cycle-way -</p>	<p>and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>there have already been many incidents on existing Martlesham Heath cycle-ways where pedestrians and cyclists share the same carriageway.</p> <p>The crossing of Eagle Way at both Coopers Road/Lancaster Drive and by the windsock (Warren Lane) would also introduce considerable unnecessary hazards to both traffic and pedestrians/cyclist alike.</p> <p>We have been lucky enough to maintain a low crime rate in Martlesham Heath but by introducing a quick and easy getaway route for criminals this cycleway would encourage this behaviour.</p> <p>I accept that we should be encouraging the use of cycles etc and the the benefits this needs to give to our environment but this must be balanced against the impact of the additional infrastructure planned when there is a more than adequate existing network through the Heath which utilises the existing footbridge adjacent to Martlesham Leisure quite happily.</p> <p>I also object to the likely cost of such a project which I'm certain would run into Millions of pounds which would ultimately be bourn by ourselves either directly in local taxes or nationally in countrywide taxes when those sums of money could be much better purposed.</p> <p>Please confirm receipt of this letter of objection.</p>	

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				<p>I have discussed this matter with neighbours in Warren Lane and they are in agreement with these sentiments.</p> <p>Should you wish their names and addresses these can be provided.</p>	
IM12	Andrew Mercer	558	No	<p>People from Brightwell Lakes can use the existing path and footbridge - over the footbridge by Play2Day, past Birchwood School, and then across Eagle Way to the existing path next to Gorseland school and onto Kesgrave. This is a well established walking / cycling route. Building an alternative route is unnecessary - it is expensive, disruptive and involves cutting down trees in existing woodlands (and also not a "joined-up" route)</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Andrew Peirson	161	No	<p>Whilst I have no major objection to the overall strategy, I do strongly object to the proposal identified in Map Reference IM12 and in particular the proposed route through Martlesham Woods (more correctly known as Birch Woods). The area is an important piece of amenity land primarily used by the residents of Martlesham Heath and also a natural habitat for various species of birds and mammals. Given that Eagle Way is a relatively quiet road with adequate footpaths, this should be utilised instead creating a new thoroughfare through Birch Woods. In addition, there is already an adequate</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide</p>

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				cycle/footpath from the Brightwell Lakes area that joins up with the existing footbridge across the A12. There seem very little sense in building a second footbridge when the existing bridge isn't excessively used at present. As for a crossing point along Eagle Way, this likely to create an increased risk of accidents between drivers and cyclist, especially as there is a general propensity for cyclists to think that they have the right of way / priority to cross at junctions.	a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Andy Hewett	74	No	The section dealing with the route through woodland on Martlesham Heath is just not sensible. Why disrupt a perfectly good stretch of woodland, which by the way is called Birch Woods not Martlesham Woods, to add a wide tarmac path when there is an adequate and quiet route along Eagle Way for cyclists. We should be conserving woodland, not digging it up.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Anna Thomason	102	No	At a time where we are being encouraged to protect the planet the plan to put a cycle path through a woods is wrong. Trees, plants and wildlife will be disrupted, cut down and killed. Not only would constructing the cycle path damage the natural woodland it would also encourage additional use of Birch Woods which is private	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch

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				<p>property belonging to Martlesham Heath Householders who pay a maintenance charge every year to look after their land. Adding a cycle path will make more people who are not residents aware of the woods which will increase footfall and therefore increase land maintenance and litter picking costs for the residents to have to pay.</p> <p>Eagle Way being such a quiet road can and is used by cyclists and is already in place and would not hugely increase the distance being cycled and as a road it will be better maintained than cycle paths in the future. This is based on the state of current paths and cycle paths that have been in place many years in the area and are not maintained.</p> <p>On a personal safety issue having a cycle path even if lit through a woods is not in the dark as safe as using the road where more traffic, walkers and houses are located.</p> <p>In terms of nature and animals living in the woods to add lighting for this cycle path will be intrusive and encourage animals to leave the area.</p> <p>In conclusion please consider firstly the negative impact on the environment this will have along with the lack of respect this will show you have for environmental conservation and secondly that this fundamentally goes against the principles of the way Martlesham Heath was designed and built with all householders belonging to Martlesham Heath</p>	<p>Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				Householders and that we pay to manage and maintain our privately owned land.	
IM12	Anne Greathead	723	No	I wish to object to the new proposals for the path cutting through Birch woods (Martlesham Woods). The felling of beautiful trees is a carnage and the disruption of wildlife habitat as well is deplorable. Martlesham Heath has managed all these years by using the existing safe roads for cycling around the village and between local villages. Designated cycle paths have been in place since the village began, and were part of the design process of the village, so changing this is not an option.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Ann Weidman	694		I feel that cutting through Birch Woods in Martlesham Heath with a road and lighting would spoil the woods and it could be dangerous if cyclists are cycling through the woods where walkers are walking.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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IM12	Anonymous	736	No	<p>i WISH TO RAISE MY CONCERNS REGARDING THE ABOVE AND IN PARTICULAR ITS DETRIMENTAL EFFECT TO MARTLESHAM HEATH.</p> <ol style="list-style-type: none"> 1. THE LAND IS OWNED JOINTLY BY MARTLESHAM HEATH HOUSEHOLDERS LTD. 2. MUCH OF THE LAND CONCERNED IS DESIGNATED AS SSSI 3. AREAS OF BIRCH WOODS/MARTLESHAM WOODS WILL BE DESTROYED TARMACED OVER. THIS IS TOTALLY UNACCEPTABLE. 4. THE HABITAT OF WILDLIFE AND FLORA WILL BE DESTROYED AND SERIOUSLY EFFECTED BY TARMAC AND THE MASSIVE PROPOSED INCREASE IN HUMAN TRAFFIC AND LIGHTS ETC. THIS IS UNACCEPTABLE 5. THE PLAN IS ILL THOUGHT OUT. 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Anouska Hadley	473	No	<p>I strongly object to a footbridge cutting through the woods between Lancaster Drive & Coopers Road. Having lived in Lancaster Drive for 8 years now we have already noticed a huge increase of traffic along the A12. Also being dog walkers we are worried about the effect this path, lights and footbridge will have on the wildlife. There is a large community of deers, foxs, badgers and hedgehogs etc to name a few. We lose these animals and it affects the whole Eco system, we will lose the birds, butterflys and bugs etc. This will be devastating for the Wildlife that live in the woods. Already us humans are disturbing their homes and pushing them to extinction. Government & councils need to seriously think about the impact building is having on our wildlife, in the next 30 years this will impact us as humans and it will be too late then.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				The siting of the bridge itself doesn't make sense, as you seem to be drafting walkers and cyclists to Kesgrave ominities and businesses instead of our own community benefiting from this. Shouldn't you look after your own first. There is already a footbridge further down the a12 near Martlesham Leisure, would you not be better widening this for increase numbers in walkers & cyclists. Makes perfect sense!!	
IM12	Anthony Dann	611	No	This proposal to create a new pathway across the field south of Martlesham Heath is not required as there is an existing public footpath from Dobbs Lane to the A12 through the field. Therefore building a new path would be a waste of money and resource, which could be devoted to upgrading the existing routes through Martlesham Heath, including the existing footbridge renovation.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Anthony Mahoney	419	No	The crossing of the A12 by a bridge landing in Woodland between Coopers Road and Lancaster Drive will use land designated as having Village Green status and is therefore illegal. Further the creation of a cycle way through the Birch Woods would do untold damage to wild life and would damage the fundamental nature of this	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch

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				largely unmanaged wood which is a very effective Carbon sink	Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Anthony Martin	128	No	<p>Strongly object to the creation of a paved footpath/cycleway through Martlesham woods (Birchwood). This will spoil the character of the woods which are currently for walking only. It is a great place for children to explore. Creating a cycleway would require extra vigilance for parents "Look out for Bikes"!!</p> <p>The construction would also require destruction of a considerable area of mature woodland. This is against the principles of COP26.</p> <p>If the proposed crossing of the A12 to Brightwell Lakes goes ahead, it would be possible to continue the cycle path on Eagle way east to join up to IM11 path without touching the woods. Eagle Way already has footpaths, It would be possible to create cycle lanes in the roadway.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	B.E Shaw	852	No	<p>I am contacting you with regard the proposed above development.</p> <p>I live in Martlesham and purchased my property over 6 years ago.</p> <p>One main reason I purchased the property was the lovely field view I have from the back of my property</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch</p>

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				<p>and the feeling of being closer to nature.</p> <p>I strongly object to any development of this area and am shocked that this is even being considered, especially in light of the fact mature trees will be sacrificed.</p> <p>Likewise I am not happy that my privacy will be affected with cyclists or walkers invading my tranquility by literally passing the bottom of my garden and am also concerned about security issues as I live on my own. I would have to install CCTV cameras and erect a high fence so I feel secure in my own home. I really think this is unacceptable and would respectfully ask that you reconsider and cancel any such application.</p>	<p>Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Barbara Thrower	717	No	<p>I wish to lodge my objection to this proposed Strategy, particularly the felling of at least 25 of our beautiful mature trees. This is not acceptable at all for any reason!</p> <p>Why is it necessary to spoil beautiful natural areas with tarmac and lampposts?</p> <p>The current bridge, foot/cycle paths and street lights are more than is necessary in this area.</p> <p>Could you advise if there has there been a survey of footfall and cyclists using the existing facilities?</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Barry Scowen	62	No	<p>I am somewhat concerned of the plan to construct a hard and illuminated path through Birch Woods</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from</p>

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				<p>following the route of a current footpath,when a safe and alternative route around Eagle Way is already available a route that is safe and already exists.</p> <p>A high percentage of walkers currently using this path are dog walkers following the current route or joining it from other paths.Should a hard path be constructed this will allow cycling at speed and In my experience many are reluctant to use a bell or expect pedestrians to stand aside.The odd accident will only deter elderly residents from walking and appreciate our wonderful wood.</p>	<p>urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Barry Scowen	210	No	<p>It seems a hard and illuminated cycle path through Martlesham woods is unnecessary,on leaving the proposed bridge an turning left onto a quiet road the average cyclist could reach what would be the far end of the proposed cycle paths exit in a very short period of time.</p> <p>Further more an illuminated footpath would surly disturb the wildlife which enjoy uninterrupted freedom through hours of darkness.The path I understand will be some 4m wide will leave an unnatural scar and remove a number of hedge type wild shrubs boardering the current path.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Beaumont, David	363	No	<p>Response to Public Consultation on Proposed Travel Routes IM9(part), IM10 & IM12</p> <p>I am totally OPPOSED to routes IM9 (part that links Gorseland School to Portal Avenue), IM10 & IM12 as</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close</p>

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				<p>proposed by East Suffolk Council (ESC) on the grounds that they:</p> <ul style="list-style-type: none"> a) endanger a protected Site of Special Scientific Interest (SSSI), known as Martlesham Heath's 'Western Corridor' b) endanger carefully conserved woodlands, known as Martlesham's Portal Woods, Birch Woods and the woodland between Coopers Road and Lancaster Drive Hamlets c) are contrary to ESC's objectives to protect the existing ecology and counter Global Warming d) have been proposed with no prior consultation with the landowners, Martlesham Heath Householders Ltd (MHHL), of which the residents of Martlesham Heath Village are shareholders e) are unsafe for both pedestrians and road users of Eagle Way, particularly near Birchwood School and the proposed crossings either end of Birch Woods. <p>Furthermore, locating the crossing of Dobbs Lane by IM10 on a 'blind corner' is totally irresponsible.</p> <p>I will now expand on the above issues.</p> <p>A) The SSSI and immediate vicinity As ESC planners well know, the 'green land' west of the Martlesham Heath's housing to the boundary of Dobbs Lane is a protected SSSI. Consequently, this SSSI is very sensitive to 'overuse' for recreational purposes and thus efforts are consciously made to not exploit easy access. This land is leased by MHHL, which has set up as a charity, SSSI Ltd, to manage the site with guidance from Natural England.</p>	<p>proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>Martlesham residents respect and regard it as a privilege to act as guardians of this SSSI which is one of the few remaining lowland heath sites in the country and the breeding ground for an endangered rare butterfly, the Silver Studded Blue.</p> <p>Whilst route IM10 does not seem to cross the SSSI it does run along its northern border abutting MHHL land which MHHL has purposely kept as natural as possible whilst accommodating access in order to respect the proximity of the SSSI. Reptiles and valuable flora are in abundance on this northern border of the SSSI; in fact it has been reported that there is a relatively large population of Adders located and breeding in this area.</p> <p>I am annoyed that planners regard ‘informal trails’ shown on various maps of the SSSI and nearby area as ‘paths’ which they ARE NOT, unless specifically designated as such. This includes the area of MHHL land between Eagle Way and the footpath/cycleway that run North-South on the eastern boundary of Gorseland School.</p> <p>Any ‘urbanisation’, with wide paved paths, cycleways and lighting between Dobbs Lane and Eagle Way will encourage ‘overuse’ and endanger the carefully controlled ecology of this area which MHHL and conservancy groups of volunteers have nurtured for decades, on occasion using they own money.</p>	

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				<p>B) The Woodlands</p> <p>Firstly, part of Route IM9 that connects Gorseland School and Portal Avenue is part of the old airfield perimeter track and has historical significance. This area is heavily wooded and is conserved by the Portal Conservation Group which has made significant effort to maintain this area as a wildlife sanctuary. This area is also used as an informal education area for school children to understand the importance of nature and the ‘quiet environment’ it bestows encourages flora and fauna like ‘Muntjac deer’, reptiles, ‘glow-worms’ and numerous bats.</p> <p>The transformation of this ‘track’ into a heavily used, paved and worse still, lit ‘travel route’ will damage this habitat.</p> <p>Secondly, part of IM12 that connects the western side of Eagle Way to IM13 scythes through one of Martlesham Heath’s prime ‘carbon sinks’, known as Birch Woods. This wood has been preserved for low impact recreational walking and has a multitude of mature birch trees amongst other varieties which will be destroyed by the proposed wide paved footpath/cycle route resulting in light pollution and possible flooding.</p> <p>IM12 then proceeds eastwards to further destroy a purposely created natural barrier separating Coopers Road and Lancaster Drive to cross the A12 and link up with IM13. This ‘barrier’ was conceived as a natural barrier between ‘Hamlets’ as part of the original approved plans for Martlesham Heath, as well as acting as a ‘sound barrier’ for the busy A12. A</p>	

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				<p>'travel route' through this 'barrier' would change the demarcation of the two Hamlets making them appear to be contiguous and worse still act a 'sound tunnel' concentrating the noise pollution of the A12 traffic into the heart of Martlesham Heath.</p> <p>Another bridge crossing the A12 is not necessary as one already exists (IM11). Another bridge would be expensive and offer little benefit but cause major disruption and destruction of a mature, natural, ecologically sound environment. Furthermore, the Brightwell Lakes development planning approval stipulated a need for a controlled crossing/junction on the A12 as the main access to the development, connecting it to the existing 'bridleway' on the western side of the A12.</p> <p>C) Ecology and Global Warming policy ESC is guilty of sending out mixed messages on policy. On the one hand it is lauding itself as encouraging the protection of the ecology to help fight Global Warming and on the other hand it then proposes 'sustainable travel solutions' which will damage the careful nurtured ecology of Martlesham Heath. Martlesham Heath was never designed to have an east-west urban cycle route running through it: 'throwing the baby out with the bath water' comes to mind.</p> <p>Is ESC serious about protecting the District's ecology and supporting and encouraging the public who are willing to offer their time and money in helping?</p>	

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				<p>D) Lack of consultation I believe that the publication of this “Strategy Proposal” was the first anyone in Martlesham was made aware of the major impact these “Travel Routes” would have on the Parish. I find it astonishing that ESC hasn’t contacted the affected landowners earlier to establish if there were any extenuating circumstances why these proposals should or should not be considered.</p> <p>E) Safety issues Is ESC seriously considering that the northern section of IM12 using Eagle Way is a sensible and safe option for pedestrian, cyclists and other road users? If so, then it is ill-conceived.</p> <p>Anyone who uses that section of Eagle Way could tell ESC that this proposal is a disaster waiting to happen. This section of road is heavily used by young school children, their parents, the elderly, and others. There is widespread street parking on this section of Eagle Way in school-time with parents ‘dropping off’ children, school events, etc. Furthermore, this is a bus route and a major arterial road for Martlesham Heath residents and their vehicles getting on to the A12. The road itself was not designed to accommodate parked traffic, parked buses, a commercial bus route and now a proposed cycle route.</p> <p>Another safety issue is the location of the crossing of Dobbs Lane by IM10 on a ‘blind corner’ which is</p>	

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				<p>totally irresponsible. This is a busy 'rat run' for commuters and anyone using the proposed crossing will be obscured from motorist's view right up until the last moment due to foliage and trees in the vicinity.</p> <p>Conclusion It seems obvious to me that these 'travel routes' have been conceived and proposed as a 'desk based' exercise to get from 'A to B' using any 'available' green space, probably viewed using Google maps' satellite view and without consulting the landowners concerned or environmental agencies like Natural England. I have come to this opinion based on previous suggestions for path routes by planners of ESC and Suffolk County Council (SCC) when trying to link Brightwell Lakes development to Grange Farm. ESC are rightly proud of their respect for protected environments (e.g. SSSIs), why do they insist on using Martlesham to 'mope up' previous strategic planning inadequacies with a 'spaghetti junction' of paved, lit footpaths/cycleways?</p> <p>It should be noted that the Brightwell Lakes planning approval was based on an 'A12 Access Crossing' as the main entrance to Brightwell Lakes which incorporated a pedestrian/horse/cycle facility to the existing bridleway opposite. No mention or agreement had been made to have another crossing across the A12 linking into Martlesham Heath as one already existed and was part of the National Cycle Network.</p>	

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				<p>The proposed crossing of the A12 via IM12 would have probably been adamantly opposed by residents and Martlesham Parish Council to the Brightwell Lakes Development if it had been proposed and now this 'Sustainable Travel' proposal could be seen as a means of the ESC getting a crossing by stealth. <u>Is this how ESC wants to be seen to be planning infrastructure?</u></p> <p>I hope the above comments are considered and are helpful in shaping ESC's Draft Cycling and Walking Strategy.</p>	
IM12	Brenda Shakespeare	567	No	<p>Spoiling the woods which are very well used by walkers and a green lung should be prohibited and the cost of lighting plus damage to the environment with light pollution is abhorrant. If this path went ahead it would encourage folk using a short cut over the sssi which is sometimes used by schoolchildren now and a further loss for wildlife</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Brian Hawkins	223	Yes	<p>I feel the integrity of Martlesham Heath needs to be protected. Therefore nothing should be developed outside the current boundaries of the development. Therefore the SSI (Western Corridor), the old Martlesham Heath runway area, Porters wood and</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close</p>

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				the agricultural land south of the development should not be included in any plans. With this in mind the route through the Birch Wood has some merit. However the needs of the habitat need to be protected and in particular any route should not be illuminated. Any provision or walking and cycle developed on the Brightwell Lakes should integrate with the current cycle path network. Eagle way is not a busy thoroughfare. It seems to me it is sensible to improve it as a road for both cyclists and motorists and therefore not touch the birch wood at all.	proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Bull Phil and Janet	677	No	<p>We recently had our attention drawn to your strategy document for which comments are to be submitted by 5pm tomorrow (10 January 2022).</p> <p>We have comments relating to the Martlesham Heath part of the plan as shown in the map on page 23 of the document and described in the subsequent pages. Specifically, our comments relate to a portion of the proposed route labelled IM12 that would link IM13 on the Adastral Park/Brightwell Lakes side of the A12 via a new bridge and then pass through Birch Woods (called Martlesham Woods in your document) to join Eagle Way near to Forest Lane.</p> <p>As keen walkers, we support the provision of cycling and walking routes in the area. However, we believe that the creation of a lit tarmac surface through the woodland would unnecessarily despoil the nature of the woodland, which is an important amenity for the residents. We also believe that it would encourage cyclists to cross Eagle Way and take a short cut</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>through the SSSI area of the Western corridor heathland to get to Dobbs Lane. We can see the logic behind creating a new bridge crossing the A12 at what will become the southern end of Brightwell Lakes to provide a more direct route through Martlesham Heath, but we would prefer that the route should be northwards along IM13 to join IM11 and then to pass over the existing bridge (or a widened one) and along the Valiant Road route. If the southern bridge is still deemed necessary, then consider taking the route northwards along Eagle Way on the east side of the village to join IM11 to pass along Valiant Road rather than creating IM12 through the woodland.</p> <p>We note that the proposed IM12 is marked as a medium priority, so deleting this section from the plan should not have a major impact on the plan objectives.</p>	
IM12	Burley Linda	67	No	<p>It is a lovely, peaceful space, enjoyed by the community. The proposed cycle pathway would interrupt this and there is a non-busy road which circumnavigates the road, and so therefore it is not necessary to disturb the wood</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and</p>

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					Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Caroline Bickers	397	No	This route will destroy a much used and enjoyed woodland in the centre of Martlesham Heath village. My opinion is that IM12 should be routed to the south of the village with no need to cross Eagle Way twice and any encroachment on the Woods. Getting people out of their cars is fine but not at the expense of local flora, fauna and amenity land.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Caroline Hawkes	704	No	<p>Absolutely no need for this. We already have a bridge over the A12. We need fewer cycle tracks and tarmac routes.</p> <p>Have you paid no attention to the current policy of PLANTING MORE TREES. Cyclists have become a nuisance to pedestrians around the Heath area. And can't you even get the name if the woods correct - indicative of your total indifference to environmental issues in the area.</p> <p>Get real and listen to local voices before Martlesham Heath is concreted over. We already live with the consequences of overdevelopment. Please leave us our trees - a paradise in Covid times and spare us an influx of cyclists.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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IM12	Caroline Miller	484	No	I would not be keen to see a wide path through Martlesham Woods which would result in felling trees. There is already a route over the existing footbridge so this seems a waste of money.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Carrie Fulcher	729	No	<p>I have viewed the proposed cycle track connecting a new development at Brightwell Lakes to Kesgrave which is intended to cross the A12 via a new bridge and thence cut a new track across Martlesham Heath and its woods before joining Eagle Way.</p> <p>I would like to object strenuously to this proposed route for a number of linked reasons.</p> <p>1) There is already a bridge crossing the A12 at a roughly 5minute cycle ride along a serviceable cycle path further up the main road. There is also an underpass with dedicated cycle and pedestrian sides another 5 minutes away. As a cyclist myself, I know these routes well.</p> <p>2) Eagle Way itself, which route IM12 is planned to merge into, is itself a relatively quiet road catering</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>for, in the main, local village residents. With no heavy traffic, few large vehicles and being pretty well flat, it is an easy and pleasurable route for a cyclist, and little further to cycle than the proposed new route.</p> <p>3) The damage to wildlife by the concreting of an up to 5metre path, and it's proposed lighting , would be irreversible.</p> <p>4) the woods are used regularly by residents for daily exercise, for a pleasant walk to school and by dog walkers; it seems to be irresponsible of the council to be failing to consider the well-being of current residents in favour of luring in potential new residents with an unnecessary plan.</p> <p>5) Given that there is already fairly decent provision for cyclists, the plan makes no economic sense, particularly in the face of the costs of COVID to businesses in the local area.</p>	
IM12	Charlotte Sach	101	No	<p>I wish to object to the proposed cycle route through martlesham heath birchwood.this is used by many residents of the heath for quiet relaxation and exercise.The construction of a tarmac path with lighting and the obvious loss of.hedgerows and tree's do to any widening , straightening required,will spoil the whole ambience of this valuable green space.please leave the wood and it's wild life alone.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide</p>

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					a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Cheryl Wilkes	629	No	The proposed IP12 route duplicates other existing routes. The introduction of a lit tarmac surface will cause extensive damage to Martlesham Woods which is a haven to many species of bird and animals. Several paths cross the proposed route and safety between pedestrians and cyclists will be compromised.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Chris Hardcastle	407	No	My primary concern is the proposal to route IM12 through Martlesham Woods. For the benefit of the environment and the wildlife the woods should remain as they are (no paving or lighting) and used for walking only. I'm not against the new IM12 bridge across the A12 but am concerned about its impact on the small wooded area through which it needs to connect to Eagle Way. If there is no alternative, the path/cycle way connection should be routed either to one side or the other of the wooded area between Coopers Road and Lancaster Drive rather than diagonally across it, and be of a design that would minimise the impact on the wooded area environment. From the connection point on Eagle	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				Way, IM12 should make use of the existing road either going north or south or both.	
IM12	Chris Rogers	257	No	<p>I have read the response by the Board of Martlesham Heath Householders Ltd (MHHL), which was submitted to you on 16 December 2021. I am in total agreement with all the comments made by the MHHL Directors and I interpret and summarise them as follows:</p> <ul style="list-style-type: none"> • We do not need any more footpaths in the Martlesham Heath area as we are well provisioned at present. • We are totally against the building of any new tarmac and lit cycle paths on our private MHHL land. • We are emphatic in our view that the proposed cycle route IM12 through two of our woodlands, which would involve the felling of many trees, should be discarded. • We do not agree to a new bridge being built over the A12 onto our land. The existing bridge should meet local needs for many years to come. • We are very happy for the continued use of our two permitted routes: round the perimeter track in Portal Woods and from Birchwood School to the Grove. These routes could possibly be incorporated into your strategy. • We recommend ESC consider changing the priorities of the proposed routes. <p>The Sandlings Long Distance Footpath</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p><i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p> <p>Route of National Cycle Route 1</p> <p>My personal proposal for cycle routes in the Martlesham Heath area is shown below.</p> <p>Red line: existing permitted routes Blue line: possible permitted routes subject to negotiation</p> <p>In the map I have shown changed route priorities together with the existing and possible permitted routes. My view is that any future agreed permitted routes should have an improved dirt/ stone surface but certainly not tarmac. Photographs of the two possible future permitted routes are shown below.</p> <p>Route already taken by cyclists and pedestrians across our land near Eagle Way and Parkers Place</p>	

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				 <p data-bbox="846 695 1420 754">Sandlings path as it enters our land from Dobbs Lane which some cyclists use</p>  <p data-bbox="846 1166 1368 1193">Part of Martlesham Heath Village Looking North</p>	

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				 <p data-bbox="846 863 1312 890">Martlesham Heath Control Tower Museum</p>  <p data-bbox="846 1302 1402 1361">I hope you find my suggestions useful and that you will consider incorporating them into your strategy.</p>	

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				<i>The maps have not been published due to potential copyright concerns, but these were still fully considered and assessed in forming the Strategy.</i>	
IM12	Chris Rogers	358	No	<p>Westbound Cycle and Pedestrian Connections from the New Brightwell Lakes Development</p> <p>An Invitation</p> <p>In 2022 work will commence on the new Brightwell Lakes Development. An outline of the development will be found in the link below.</p> <p>Redevelopment of Brightwell Lakes, Martlesham · Taylor Wimpey</p> <p>When completed, I estimate that Brightwell Lakes will have a population of around 6,000 which is higher than the current population of Martlesham Heath Village.</p> <p>In this response to the draft strategy, I would like to invite the Policy Planning Team of East Suffolk Council to consider 3 actions, bearing in mind the reasons and evidence, which are included later in this response:</p> <p>Firstly, I would like you to delete route IM12 from your draft strategy. This route is clearly shown in the image below.</p> <p>Secondly, I would like you to consider inserting a new bridleway from Brightwell Lakes to Rushmere Heath, as shown in purple in the map below.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>Thirdly, I would like you to use your influence with any contacts you may have in Suffolk County Council Transport Department to persuade them to withdraw any thoughts of a new bridge over the A12. Two already proposed sites are shown in red in the map below.</p> <p>Key to colours: Yellow: Larger patch – Ipswich Heaths site of special scientific interest Smaller patch – Martlesham Common Red: Alternative bridge sites proposed by SCC Blue: The proposed Toucan/Pegasus crossing to bridleway westbound Green: The existing bridge would fulfil all future needs from Brightwell Lakes</p> <p>The case that developing the existing bridleway route westbound from the Brightwell Lakes Development via a Toucan or Pegasus crossing across the A12 would be cost effective, beneficial and should be included in the Draft Strategy</p> <p>When driving to Felixstowe, I noticed there is a Pelican Crossing across the main A14 road as shown below. Therefore, there is no reason why there should not be a crossing across the A12 road (Toucan or Pegasus) from Brightwell Lakes to the existing Bridleway.</p> <p>Indeed, there are already plans submitted to ESC for such a crossing as shown below. Also I have given a</p>	

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				<p>link to this same diagram.</p> <p>Microsoft Word - 10391TA01Rv6 (eastsoffolk.gov.uk)</p> <p>My proposal is that ESC continues the existing bridleway to Rushmere Heath. This would mean negotiations with the landowner for the middle section of the route as shown in the image below in blue and also conversion of the existing footpath to a bridleway as shown in light blue in the image below.</p> <p>On some maps there is already a footpath shown alongside the Foxhall Road as indicated in blue. With the landowner’s agreement it would just need converting to a bridleway.</p> <p>This new bridleway route would be primarily for walkers but also as it is a bridleway, cyclists and equestrians would be able to use it. As to whether the bridleway would require a tarmac surface and lighting is a matter for ESC and whether that would be cost effective in view of its potential usage. My personal thought is that tarmac and lighting would be unnecessary as the soil is light and sandy and with a graded and maintained track, the surface would be adequate for cyclists.</p> <p>This direct route would be far more attractive for Brightwell Lakes residents wishing to travel in the direction of Ipswich rather than having to go in a dog-leg via route IM4. The traffic envisaged would not be huge, however, for the modest cost required</p>	

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				<p>to implement this route, the investment would certainly be worthwhile.</p> <p>The case that any additional bridge westbound over the A12 from the Brightwell Lakes development is without merit</p> <p>Route IM4 through 2 woodlands owned by MHHL, only makes sense if there is a new bridge. In MHHL's formal response to the consultation, emailed to ESC on 16 December 2021, they outline in detail their objections to route IM4. In summary, their view is that driving a new tarmac lit route through woodland and thereby felling over 25 mature trees would be environmental vandalism.</p> <p>Below is a quote from the Local to Martlesham Facebook page giving SCC's latest position as to its decision making. I am assuming this quote is authentic and recent.</p> <p><i>Graeme Mateer - Head of Transport Strategy - Suffolk County Council "However, I can confirm that a definite location for the new pedestrian/cycle bridge has not yet been decided. This will need to be subject to further design and consultation as part of any next steps, leading up to the submission of a final business case and planning permission, assuming the Outline Business Case is successful. We are considering options for a new bridge to be between the northern and southern edges of the Brightwell Lakes development."</i></p>	

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				<p>Below is an extract from your ESC Draft Strategy where you cast doubt on the need for another bridge. Therefore the evidence is not overwhelming that an additional bridge is required.</p> <p><i>Page191 Draft Strategy</i></p> <p><i>Connectivity and Growth - The A12 is a significant barrier creating a wall between the residential areas to the west and the services and employment opportunities to the east. However, without teaming a second bridge that's located to the south with an onward route that cuts through the Martlesham Heath woodland up to north west Eagle Way, the onward travel gain of using the southern bridge rather than the existing A12 foot/pedal bridge is lost; the cyclist/pedestrian journey time (and energy) cost to reach the Broomfields shared paths for onward access to Longstrops Bridleway (which is set to be the 'keystone' of the strategic route between Brightwell Lakes and Ipswich) is higher than simply using the existing A12 foot/pedal bridge.</i></p> <p><i>The difference is not huge, though. With high levels of modal shift, a second bridge - regardless of the advantage lost - would be worth installing just to manage the pedestrian/cyclist flows, which would largely be coming from the south post-delivery.</i></p> <p><i>As suggested by the respondent, replacing the existing bridge with a higher-capacity (wider) and more accessible (less steep) bridge may achieve similar benefits, though.</i></p>	

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				<p>Below is a link to the Cycle Infrastructure Design publication and I highlight the Department of Transport’s thoughts on “iconic items”. To my mind another bridge over the A12 would be a “white elephant” bridge and would not be supported by the DoT.</p> <p>Cycle Infrastructure Design (publishing.service.gov.uk)</p> <p><i>Cycle Infrastructure Design - Published July 2020</i></p> <p><i>1.6 Summary Principles: The following summary principles form an integral part of this guidance</i></p> <p><i>12) Major ‘iconic’ items, such as overbridges must form part of wider, properly thought-through schemes.</i></p> <p>There is sometimes a temptation to build costly showpiece structures in isolation without thinking enough about the purpose they truly serve and the roads and routes which lead to them. We will only support such things when they overcome a major barrier on a desire line which cannot safely be crossed in other ways, and where they form an essential, properly-connected part of a wider network of good, safe routes.</p> <p>Below is a link to the Consultant’s Transport Assessment for the Brightwell Lakes Development. This assessment was using the existing bridge only.</p>	

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				<p>Microsoft Word - 10391TA01Rv6 (eastssuffolk.gov.uk)</p> <p>When considering whether a new bridge would be cost effective, one must consider what journeys would be undertaken. Below from the assessment are extracts of typical journeys and probable destinations. An additional bridge would offer no extra benefit from what was envisaged previously by the consultants. My own view is that the existing bridge over the A12 on National Cycle Route 1 is perfectly adequate as is now to meet the future needs from Brightwell Lakes.</p> <p>My estimation is that at least 75% of future cycle and pedestrian journeys in any direction from Brightwell Lakes would be to the extensive Martlesham retail and industrial park on the east side of the A12. Crossing the A12 westbound would not be necessary. Indeed a new school is to be located at Brightwell Lakes.</p>  <p>The screenshot shows an accessibility assessment report with the following table:</p> <table border="1"> <thead> <tr> <th>Facility</th> <th>Accessibility Score (0-100)</th> <th>Score</th> <th>Target</th> <th>Score</th> <th>Score</th> <th>Score</th> </tr> </thead> <tbody> <tr> <td>Education</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>Brightwell Primary School</td> <td>85%</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> <tr> <td>Eastwood Primary School</td> <td>87</td> <td>87</td> <td>87</td> <td>87</td> <td>87</td> <td>87</td> </tr> <tr> <td>King's High School</td> <td>83</td> <td>83</td> <td>83</td> <td>83</td> <td>83</td> <td>83</td> </tr> <tr> <td>St. Andrew's Church</td> <td>81</td> <td>81</td> <td>81</td> <td>81</td> <td>81</td> <td>81</td> </tr> <tr> <td>Health</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>Eastwood Community Centre</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> <tr> <td>Eastwood Health Centre</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> <tr> <td>Eastwood Pharmacy</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> <tr> <td>Eastwood Surgery</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> <tr> <td>Work, Employment and Misc.</td> <td></td> <td></td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td>Eastwood</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> <tr> <td>Eastwood Community Centre</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> <tr> <td>Eastwood Health Centre</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> <tr> <td>Eastwood Pharmacy</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> <tr> <td>Eastwood Surgery</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> <td>85</td> </tr> </tbody> </table>	Facility	Accessibility Score (0-100)	Score	Target	Score	Score	Score	Education							Brightwell Primary School	85%	85	85	85	85	85	Eastwood Primary School	87	87	87	87	87	87	King's High School	83	83	83	83	83	83	St. Andrew's Church	81	81	81	81	81	81	Health							Eastwood Community Centre	85	85	85	85	85	85	Eastwood Health Centre	85	85	85	85	85	85	Eastwood Pharmacy	85	85	85	85	85	85	Eastwood Surgery	85	85	85	85	85	85	Work, Employment and Misc.							Eastwood	85	85	85	85	85	85	Eastwood Community Centre	85	85	85	85	85	85	Eastwood Health Centre	85	85	85	85	85	85	Eastwood Pharmacy	85	85	85	85	85	85	Eastwood Surgery	85	85	85	85	85	85	
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				<p>Summary</p> <p>If you at ESC believe that my thoughts above make sense, I would be grateful if you would share these thoughts with SCC, so they would withdraw any notion of an additional bridge and spend that money in developing the new cost effective bridleway route which I have outlined above.</p> <p>I am sure that residents to be of Brightwell Lakes, in years to come, would appreciate this new direct bridleway route to Ipswich, not only as a leisure route but also, perhaps in some instances, as a longer distance commuting route for those working in the Ipswich Hospital area. The Policy Planning Department of ESC would then be acknowledged for their vision and far-sightedness in providing such a route.</p> <p><i>Some of the images/maps have not been published due to potential copyright/data protection concerns, but these were still fully considered and assessed in forming the Strategy.</i></p>	
IM12	Chris Sach	58	No	<p>As a resident of Martlesham Heath and a member of mhhl, I am strongly opposed to any hard cycle route being built through the Birch Wood. Any widening, tarmacing or lighting will totally ruin that section of woodland for residents who enjoy the peaceful, ambience and natural beauty of this location. Speeding cyclists will be a never ending danger to people out walking with their family's and pets. Please don't ruin this lovely piece of woodland to save cyclists a few minutes journey time.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and</p>

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					walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Christine Plant	605	No	Whilst measures to support a healthy lifestyle and less dependence on a car is good, the routes planned which dissect well established natural woodland is not acceptable. I refer in particular to IM12, which will drastically reduce the amenity value, cover a vast section of the land in tarmac and ensure that people already using that area will be met and passed (at some speed) with cyclists using the route. This is popular dog walking land and has been used as such for as long as the Birch Woods have been a part of life at Martlesham Heath, it is also enjoyed by those getting valuable exercise on foot, free to wander without the need for a specific route to follow. Destruction of the natural habitat will affect not only people but wildlife as well, I would ask for further thought to be given to all these factors.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Christine Renfro	113	No	I am absolutely against these proposals, they will disturb our lovely dog walks, potential hazards for children enjoying our woods as we know and have witnessed cyclists in the past. I have lived and enjoyed these woods for 35 years. What time would be saved by this route? These woods are owned by the residents so please leave them alone!!!! What next 'A road through here' heaven forbid. Think again and spend the money on more worthy ideas.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new

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					recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Christopher Pinder	417	No	This is an act of public vandalism on precious areas of natural beauty in Martlesham Woods and Portal Woods. Cycle paths could instead be constructed on the very edges of Martlesham Woods. On the edges of the existing road Eagle Way. Although this is not an as-the-crow-flies route it would represent much less devastation of natural unspoilt habitat. East Suffolk Council has already approved other detrimental developments to the Martlesham Heath Village, notably the development of the Car Park into residential flats by McCarthy and Stone. Please can you leave all other parts of our village untouched!	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Chris Wilmot	693	No	<p>I am contacting you to object to the creation of the Cycle and Walk routes IM11 and IM12 over Martlesham Heath. The segments of IM11 and IM12 that are proposed to go over Martlesham Heath, could be avoided by implementing a short route between the intersection of IM13 and IM14 near Martlesham Community Hall and IM10. Which would then have a much lesser impact on the area.</p> <ul style="list-style-type: none"> The proposed IM12 route crosses Martlesham Woods. This is where people walk with their dogs. It would involve the cutting down of trees and splitting the wood in to two parts; one large and one 	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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				<p>small. It would have the effect of reducing the area of the wood for both human beings, dogs and wild animals, as they would tend to stay in the remaining larger part.</p> <ul style="list-style-type: none"> • IM12 crosses Eagle Way at two points. How will the Council address these two road crossings, will there be traffic lights? Or will there be the system that is used in Broomfield of extending the pathway in to the road, which is frankly dangerous. The exit points will have an impact on road traffic going round Eagle Way. • The exit of IM12 by Forest Lane could lead to cyclists choosing to cross the SSSI to make a short cut to IM4, IM7 and IM10. I note that although the title of the document is “Cycling and Walking Strategy”, it also mentions equestrian use, one would not want horses going on to the SSSI. • Although these proposals refer to Cycling and Walking strategy, they will undoubtedly lead to an increased use of illegal e-Scooters which will create other problems. There is nothing in the strategy to address this. • The document is a strategy document “The East Suffolk Cycling and Walking Strategy 	<p>Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>identifies potential cycling and walking infrastructure opportunities across the district. The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure. It provides context and information to support detailed infrastructure proposals and inform decision making to support cycling, walking, and equestrian use". A strategy document should describe the high level objectives that will be achieved by it, describe the benefits and then show how the proposed strategic solution complies with the objectives and benefits. This document doesn't do that it just assumes a solution, which means that it will be wrong.</p> <ul style="list-style-type: none"> • The document does not give any forecast volumes for cycling and pedestrian (and horse) traffic along each of the routes to justify the cost and environmental impact verses the benefits. How do you know that you will get enough users to justify the changes? • One would expect any Cycling and Walking strategy to be linked to a bus strategy. I would have expected Cycling and Walking routes to converge around the Martlesham Park & Ride and not cross Martlesham Heath in the way that they are doing. 	

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IM12	Claire Punter	198	No	There are more than enough cycle routes through the Heath and why would mass cyclists want to go from 'Brightwell Lakes to Kesgrave. Certainly not to school, as there is supposed to be a new school on the site. I think people need to be more worried about the traffic and workers trying to get to Ipswich etc for work	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Clive Roper	572	No		Objection noted.
IM12	Clive Roper	575	No	I am opposed to the cutting down of 25 mature trees in Birch (Martlesham) Woods, in a year when we are being encouraged to plant more trees. At 2 or 3 trees for every felled tree would have to be planted for every felled tree. The leaves falling from the remaining trees would create a hazard on the path and would require ESC to regularly sweep them up. The existing trees roots would damage the proposed footpat as seen by the damage to all the existing footpaths in Martlesham for which neither SCC or ESC will repair. Again the low energy lighting will have serious consequences for the nocturnal animals (foxes, monkjacks), birds and insects that live in these woods. The lands between Eagle Way and the A12 are	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				leased to MHHL, who are opposed to this footpath and will not be giving their permission.	
IM12	Colin Maunder	160	No	<p>I'm sorry, but this is totally inappropriate. The woodland concerned is a valued community-owned resource, well-used by walkers of all ages. Widening the path would destroy trees, brambles and other plant life, which provide food and homes not only for the many birds in the woods, but also the local population of muntjac deer. And lighting it? Why? Anyone who's worried about being out in the dark would still be well away from anywhere that felt safe. The track might be paved and lit, but it would still be isolated.</p> <p>And what would it all achieve? Eagle Way is hardly busy. A few bumps would slow the few cars that exceed the speed limit, making it even safer. And if you want to build a crossing for pedestrians and cyclists across the A12 to serve the new development, why not reconnect the bridle way that crosses south of Martlesham Heath? Do we really need a 15ft-plus bridge with access ramps between Coopers Road and Lancaster Drive that would destroy the environment both for those who live immediately next to it and others nearby? Lights? Litter? People now able to overlook previously secluded gardens?</p> <p>In short, this proposal is both misguided and pointless.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Colin Snowden	88	No	<p>Cyclists can use Eagle way as is, it's not a busy road. The road is already popular cyclists. If children are a consideration they are more likely to use a route</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is</p>

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				<p>going over the square to shop at McColls , also Greggs for savouries etc which lies on the same general route.</p> <p>Making a cycle path through the woods would damage the woods with all the implications thereof, would be costly, the money could be used on footpath repair. There will be conflicts between users, dogs being exercised and cyclists travelling far too fast, such incidents occur already. During winter and Autumn months the conditions, leaves, ice, will be dangerous for cyclists.</p> <p>This is in my opinion a poorly considered scheme spending money which could be better used elsewhere and this is a feeling, echoed by cyclists who have commented and other people in the area via social media.</p>	<p>recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Coralie Saunders	596	No	<p>This proposed strategy would result in unacceptable destruction of valuable mature woodlands which provide an increasingly vital carbon sink and barrier to noise and air pollution from the A12. What is the problem with cycling on Eagle Way which is quiet and well lit and avoids potential conflict between fast moving cyclists and walkers. Our Birch Woods which you call Martlesham Woods is very well used by people walking between home and shops, doctors surgery and School. Last year the peace and tranquility Birch Woods provides us with became a lifeline to many in lockdown and a tree had painted stones placed around and decorations hung from it which children had made. A nature trail was also created for children to follow teaching tree names</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>and encouraging them name what they saw on their walk. It is scientifically proven that access to natural, wild spaces reduces anxiety and depression and improves our health.</p> <p>These routes would be built on land owned by MHHL on our behalf as we pay an annual covenant fee to them so i must ask what claim do you have to this land? Are you going to use compulsory purchase orders to impose your plan on us?</p>	
IM12	Dave Parsons	247		<p>As a resident within the planned area I see many parts of it as "useful"</p> <p>to the general public - if it promotes activity and exercise in people.</p> <p>One part I feel would be a costly waste of investment is part of the Medium rated IM12 section which I assume includes a new method of crossing the A12 ? The section between the junction IM12 and IM13, along to the junction between IM12 and IM11 is not needed.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	David Carruthers	165	No	<p>I would like to object to the routing of cycleway IM12 through Martlesham Woods.</p> <p>Sadly, the cycling and walking strategy seems heavily biased towards cyclists and I am concerned that, in many of the areas affected by the new works, the environment for walkers may actually be made less attractive and more urban and risky, as they will be sharing space (perhaps just the other side of a white</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In</p>

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				<p>line) with fast moving cyclists from across a wide area who will be keen to get to their destinations as soon as possible. As part of the plan there must surely be a case for reserving some attractive and low speed walking environments exclusively for walkers alone, away from wheeled vehicles of any kind.</p> <p>The Birch Woods is a prime example. At the moment the woodland is beautifully peaceful and natural and there is a sense of slowing down and leaving the built environment behind as soon as you set foot in it. There is an attractive transition from the bottom of the Green into the woods and that northern margin of the woodland seems to attract more than its share of birds. I often hear nightingales there during their season. Very occasionally you meet a cyclist but they have to go slowly, and they are often accompanying children on small bikes. The pace in the woods is geared to walkers seeking quiet recreation and enjoying nature, rather than commuting cyclists on their way somewhere else. The proposed urban style cycleway would disrupt and degrade all of that, visually, ecologically and by changing the perception of the place as being away from it all. The urbanisation and pedestrian/cycle coexistence that is being applied to routes elsewhere as part of this plan makes it even more important that we should try and preserve an oasis of calm in the woods.</p> <p>It would be easy to re-route that section of IM12 around the southern loop of Eagle Way, and doing</p>	<p>its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>so would add little distance for cyclists and may actually involve fewer cyclists having to cross Eagle Way itself. In any case cyclists would have an alternative route as IM11 through Martlesham Heath centre is also being improved as part of the strategy.</p> <p>Alternatively, IM12 could be routed entirely to the south of Martlesham Heath village, perhaps along the route of PROW6 or adjacent to the Welham Plantation.</p> <p>Please preserve the Birch Woods as an area for quiet recreation for walkers.</p>	
IM12	David Carruthers	166	No	<p>I would like to object to the routing of cycleway IM12 through the Martlesham Woods.</p> <p>Sadly, the cycling and walking strategy seems heavily biased towards cyclists and I am concerned that, in many of the areas affected by the new works, the environment for walkers may actually be made less attractive and more urban and risky, as they will be sharing space (perhaps just the other side of a white line) with fast moving cyclists from across a wide area who will be keen to get to their destinations as soon as possible. As part of the plan there must surely be a case for reserving some attractive and low speed walking environments exclusively for walkers alone, away from wheeled vehicles of any kind.</p> <p>The Birch Woods is a prime example. At the moment the woodland is beautifully peaceful and natural and</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>there is a sense of slowing down and leaving the built environment behind as soon as you set foot in it. There is an attractive transition from the bottom of the Green into the woods and that northern margin of the woodland seems to attract more than its share of birds. I often hear nightingales there during their season. Very occasionally you meet a cyclist but they have to go slowly, and they are often accompanying children on small bikes. The pace in the woods is geared to walkers seeking quiet recreation and enjoying nature, rather than commuting cyclists on their way somewhere else. The proposed urban style cycleway would disrupt and degrade all of that, visually, ecologically and by changing the perception of the place as being away from it all. The urbanisation and pedestrian/cycle coexistence that is being applied to routes elsewhere as part of this plan makes it even more important that we should try and preserve an oasis of calm in the woods.</p> <p>It would be easy to re-route that section of IM12 around the southern loop of Eagle Way, and doing so would add little distance for cyclists and may actually involve fewer cyclists having to cross Eagle Way itself. In any case cyclists would have an alternative route as IM11 through Martlesham Heath centre is also being improved as part of the strategy.</p> <p>Alternatively, IM12 could be routed entirely to the south of Martlesham Heath village, perhaps along the route of PROW6 or adjacent to the Welham</p>	

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				<p>Plantation.</p> <p>Please preserve the Birch Woods as an area for quiet recreation for walkers.</p>	
IM12	David Foster	21	No	<p>The Woods you refer as Martlesham Woods are actually Birch Woods and are owned by Martlesham Heath Householders Ltd. As is the land bordering the A12 where you seem intent on landing a bridge. Both areas have village Green status.</p> <p>You should concentrate on the existing bridge making it fit for purpose. If you really want a Bridge that should be further south linking to the Bridal Path.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	David Green	270	No	<p>I support the overall strategy but strongly OBJECT to this route:</p> <p>1) It will damage what is a very precious and fragile resource for the village (Birch Woods called Martlesham Woods on the consultation) significantly disturbing the flora and fauna with a wide tarmac track and lighting</p> <p>2) There is a more suitable route (IM11) building on the existing bridge crossing and linking through to IM10 on Eagle Way</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and</p>

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					Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	David Robson	754	No	Having moved to Martlesham Heath over 20 years ago I am increasingly enraged by proposals to ruin our area for the benefit of others and to our detriment . We have some outstanding walks here which are already used by Lycra clad morons , I know this as I have been hit on numerous occasions by them , as has my dog . I understand you have to justify jobs there , but surely the most important thing in planning these things should be common sense and not the ruination of our surrounding area with more pathways and cycle routes resulting in a loss of trees and more damage to the area . Having shared our local area and local events with non residents and seen them taken away and misused , i.e. Music On The Green , and out of area runners defecating in the woods I am totally opposed to your plans and wish to lodge my dissatisfaction at them in the highest terms .	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Deborah Bother	665	No	<p>Comments on the cycling and pedestrian proposals for Martlesham Heath from a resident.</p> <p>Many aspects of the current proposals are of great concern due to their impact on the natural flora and fauna which has been allowed to flourish for decades, their impact on residents and in some aspects unnecessary financial costs.</p> <p>In particular:</p> <p>1: Why is it deemed necessary to build a second foot bridge over the A12?</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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				<p>There is no evidence of how much use it would get, the impact on woods both to the south of Coopers Road and Avocet Lane would be immeasurably damaging to residents and wildlife alike and taking cyclists and pedestrians through woodland for no good reason is unsafe. There is ample space to upgrade the present bridge over the A12 with minimal disruption and at less cost.</p> <p>2: Keeping all the pedestrian and cycling 'traffic' close to the new McCarthy and Stone flats will provide social benefits to the new residents and commercially benefit the Martlesham Heath shops and Square.</p> <p>3: Much more careful planning needs to be invested in improving the route on through the Broomfield and Whinfield area. This is acknowledged to be a complex situation where what has been available was problematic from the start. Using a route in close proximity to the Control Tower but then taking 'traffic' to either Grange Farm along the present cycle way (which was originally a farm track) is already adequate. Making both Deben Avenue and Dobbs Lane alternate routes to the improved A1214 cycling and walking route will allow for any increased use.</p>	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Dee Knights	742	No	I am writing to express strong objections to the proposed footpath through the woods on Martlesham Heath i.e.IM12 on your map. This goes against everything the Government is promoting to protect the environment and achieve net-zero	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close

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				carbon emissions. The voluntary organisations on Martlesham Heath are going all out to plant as many new trees as possible, the Queen is promoting the "Queen's Green Canopy" and East Suffolk Council has its own Treebilee Project of donating oak trees. So how does this equate with your plans to destroy many mature trees in this unspoilt area?. A lit pathway will desecrate the area and bring with it additional problems of litter and anti-social behavior. It is totally unnecessary just to save cyclists a few extra yards from using a perfectly good route along Eagle Way.	proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	D Emerson	705	No	<p>I must object to the proposed IM12 cycle way on martlesham heath</p> <p>Martlesham woods or birch woods as they are known is the last remaining part of martlesham that is still a haven for many wildlife species birds, foxes, monk jacks, badger's etc every time i walk through those woods i feel how lucky we are residents and members of the public to be able to do so in peace and tranquility.</p> <p>To desecrate them with tarmac cycle tracks and street lighting would be sacrilege.</p> <p>To what gain! the existing cycle path IM11 is perfectly useable and adequate to get to IM13. is it really worth all that cost and the loss of wildlife, habitat and the trees that will have be felled. I think not.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				In fact we need to be planting more trees not cutting them down to try and holt climate change.	
IM12	Denis Cooper	368	No	<p>The route is shown to pass through through Martlesham Woods (otherwise known as Birch Woods, Martlesham Heath). These woods are home to lots of wildlife including (in summer) several nesting Nightingales which are in serious decline in the UK. The proposal would involve loss of important wildlife habitat.The route would increase public access to the woods which together with any associated lighting would drive/scare wildlife away from a wider area than just the path.</p> <p>I have observed Jays, Bats, Lizards, Hedgehogs, various tits and Deer in the woods. Newts, frogs, toads and snakes are found in the vicicnty and are therfore probably present in the Woods</p> <p>There is no need for an enhanced pathway through the woods. The route could use the existing highway - Eagleway around the woods with no construction costs, additional lighting or habitat destruction.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Derek Riley	229	No	<p>The following is submitted as I strongly do NOT support the proposals as outlined in the Draft Cycling and Walking Strategy .</p> <p>I have lived continuously on Martlesham Heath for a period of approaching 38 years. I have worked close by and my children have been educated in local schools. For many years I have been able along with my family and friends to walk and cycle safely on Martlesham Heath and the excellent footways, cycle paths that are currently in existence provide an ideal</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new</p>

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				<p>safe environment to both walk and cycle in the immediate area. Martlesham Heath has massive appeal for families as the housing is interspersed with large open spaces and fantastic natural woodlands. I challenge any changes to this environment.</p> <p>The proposal to build a lit cycleway through the fantastic woodland, would in my opinion, along with my family and friends, erode the quality of life on Martlesham Heath. The safe cycling facilities are perfectly adequate already.</p> <p>It is suggested that in excess of 25 established trees in the natural woodland would need to be felled to create the cycle way. How does this fit in with the current important global need to plant and maintain trees? The lighting that would be associated with the proposed cycleway would be environmentally damaging and would just provide further light pollution.</p> <p>I recently along with several other people planted natural bluebell bulbs in the wood and there have been a number of other similar projects. The woods have been used by numerous local children and children's clubs and organisations for nature related activities in recent years. Why do you want to destroy this natural environment for the sake of a cycle path that is not needed and nobody wants.</p>	<p>recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Diane Watson	581	No	<p>As a householder on Martlesham Heath I strongly object to this proposal.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is</p>

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				<p>If the purpose of the proposed link is to connect the proposed new Brightwell Lakes development with Martlesham Heath then this should have been identified as part of the planning application process and adequately funded by the developer. However it would appear that the need was clearly not identified in the planning process and should not now be brought forward at what will be at the public's expense. If the link was not determined as part of the planning process why is it needed now.</p> <p>From looking at the National Design Guide the recommendations appear to be at odds with this document. The location of the bridge and pathway through Martlesham Woods appears to be a totally random decision:</p> <ul style="list-style-type: none"> • It carves up Martlesham Heath. • It takes cyclists and walkers away from the facilities offered on The Square when the existing route does exactly this. • The pathway through Martlesham Woods is isolated so would not be determined as safe. • There is wildlife in the Woods: foxes, muntjacs, bats, hedgehogs, birds to name a few and we all happily co-exist now. • This pathway would go against the existing character of Martlesham Heath. <p>Finally it must be remembered that we are all fighting a climate emergency and to build a bridge</p>	<p>recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				which would result in removing valuable trees between Coopers Road and Lancaster Drive which currently helps to negate the carbon emissions from vehicles on the A12 is outrageous.	
IM12	Dianne Carruthers	181	No	<p>I would like to object to the the proposed new cycleway running through the birch woods in the middle of Martlesham Heath. At the moment the birch woods are a quiet, slow paced haven for walkers, children and nature, and the proposed cycleway would introduce fast moving bicycles, tarmac and lighting, and would destroy its quiet ambience and recreational value.</p> <p>Please could you re-route that part of the proposed cycle route IM12 to run adjacent to the southern section of Eagle Way where it would add little to its overall length and would avoid intrusion into the woodland. Alternatively (and preferably) IM12 and its bridge could be routed to the south of Martlesham Heath altogether, where it would avoid funnelling cycle traffic from the new development at Adastral Park through the village.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Dr K D Odam	751	No	<p>I object to the proposed footpath IM12 through 'Martlesham Woods'.</p> <p>Any prospective user of IM12 could easily use the existing IM13 and IM11 route3. The map below shows that the IM12 route would save about half a kilometre. Say 90 seconds for the average cyclist and 5 minutes for a slow walker. Building a bridge is going to be expensive. Is it justified to save a few minutes?</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new</p>

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				<p>Who are the expected users of IM12?</p> <p>Anyone from the new development wanting to get to shops would reach Marks and Spencers or Boots within the same distance as the village square. Tesco is only a 300 metres further.</p> <p>How many people from the new development are expected to cycle to work in Ipswich? It is too far to expect many people to walk.</p> <p>The footpath between Forest Lane and Warren Lane crosses this route. It takes pedestrians and cyclists from the south end of Martlesham Heath to the village shops and pupils to Birchwood and Gorseland schools. Cyclists on your proposed IM12 would cross this path and could well cause problems for existing users.</p> <p>The 'Martlesham Woods' on your map are regularly used for recreation by the inhabitants of Martlesham Heath. They are owned by Martlesham Heath Householders for the enjoyment of their members - myself included. Any traffic which can justify the cost of IM12 will inevitably reduce the recreational enjoyment of the woods.</p> <p>I believe that the route of IM12 through the woods was opened up some years ago as a hunting ground to encourage bats and owls. A busy illuminated path is not likely to help in this aim.</p>	<p>recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
IM12	Eamonn O'Byrne	872	No	<p>Re: East Suffolk Council Draft Cycling and Walking Strategy (November 2021)</p> <p>Regarding your recommendations for the key corridor of Ipswich to Melton I make the following comments and objections as</p> <ul style="list-style-type: none"> • A resident of Martlesham Heath and • A shareholder in Martlesham Heath Householders Limited (MHHL) <p>Martlesham Heath – Martlesham Village</p> <p>Map Reference IM12 (Martlesham Woods/Brightwell Lakes)</p> <p><i>The recommendation is to introduce segregated cycling and walking track along Eagle Way (west), through Martlesham Woods, and along Eagle Way (east). Introduce a crossing point on Eagle Way (west) into Martlesham Woods, and on Eagle Way (east) from Martlesham Woods. Introduce a cycling and walking bridge over the A12. Connect segregated cycling and walking track along Eagle Way (east) to the bridge, into Brightwell Lakes and Barrack Square. Ensure cycling and walking track through Martlesham Woods is appropriately lit, with natural surveillance where possible.</i></p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>Appropriately lit, cycling and walking track through Martlesham Woods</p> <ul style="list-style-type: none"> - As depicted on the map and in the text of the proposal, IM12 runs through ‘Martlesham Woods’. To anyone familiar with the area these woods are known as and referred to locally as ‘Birch Woods’. This belies the statement in section 3.1 of the strategy that recommendations are based on the analysis of <i>available evidence and community representations</i>. - The woodland paths of Martlesham (Birch) Woods have been used by the local population for recreation, for more than thirty years. The existing woodland path (along the route of IM12) is used by walkers as a quiet retreat from the noise and hustle of daily life. The construction of a hard surface changes the character of the way, in that it introduces a contention with cycle traffic while destroying the unmanaged nature of the environment. - The creation of a hard path cannot in all cases be considered an improvement. <i>If the construction of a hard surface changes the character of a way, it goes beyond ‘improvement’</i> {Court of Appeal in the case of Cowen -v- Secretary of State for the Department of Environment Transport and the Regions}. - A cycle path bisecting the woods will tempt cyclists to use the rest of the woodland by providing multiple points of access to the remaining woodland footpaths. It is easy to envisage a new un-official route being created halfway along IM12 leading 	

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				<p>north and across the green to Martlesham Square.</p> <ul style="list-style-type: none"> - To cyclists the woodland will be an area that needs to be negotiated on the way to somewhere else. This is completely at odds with the current users of the woodland who see it as a destination in itself. This invariably leads to conflict. o Consider the well documented cases of Strava-man attempting a personal best while pedestrians his way. o Its reasonable to expect an introduction of litter to the areas given the differing view a transient user will have of the woods. It should be noted that these woodlands are almost free of litter today. - During the Autumn the paths of Birch Woods can be covered (foot deep) in leaves. To the current users this is a welcome and natural feature. To a cyclist, it will be a hazard. <p>How would this hazard be mitigated for in the long term without further destroying the natural features of the woodland?</p> <ul style="list-style-type: none"> - A shared cycle and walking track, curving through a woodland surrounded by green trees will lead to accidents, usually to the detriment of the pedestrian. <p>Who will be liable for such accidents (the cyclist, the pedestrian, the council, or the woodland owner)?</p> <ul style="list-style-type: none"> - This is a relatively unmanaged woodland which is home to a mixture of wildlife. Owls, deer, and fox are known to be seen and heard in these woods. <p>What impact will additional human traffic have on a habitat such as this?</p> <p>It is known that street lighting has a detrimental effect on birdlife in the cities of this country.</p>	

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				<p>What impact will additional lighting have on the birds that live in this wood?</p> <p>Crossing point on Eagle Way (west) into Martlesham Woods</p> <p>- An explicit crossing point on Eagle Way (west) for route IM12, the intention of which is to lead cyclists from the woods and north to pick up IM10(IM4), will have a detrimental effect on Martlesham Heath's SSSI.</p> <p>It is impossible to imagine that cyclists will not create an un-official route from Martlesham Woods and across the SSSI to reach IM4.</p> <p>General Comments</p> <p>I have the following general comments to make regarding the associated sections of the draft strategy:</p> <p>1.3 Initial map-based consultation (19 October 2020 to 7 December 2020)</p> <p>I am a cyclist, living and working in the centre of the East Suffolk for the last thirty years. I am a member of the cycle group based at my workplace (approximately 4000 employees). I do not recall an invitation to take part in a consultation between October and December of 2020.</p> <p>Neither am I aware of any landowners (such as MHH), who currently host cycle paths across their</p>	

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				<p>land, being consulted at this time.</p> <p>I would have expected MHHL as the owners of Birch (or Martlesham) Woods to have been consulted.</p> <p>1.3 Steering Group</p> <p>Given that a steering group comprised of ESC and SCC officers was established to inform and guide the development of this (the ESC Cycling and Walking) Strategy, why do the two appear to conflict each other in places?</p> <p>For example:</p> <ul style="list-style-type: none"> - SCC's A12 strategy proposes that Portal Avenue accommodate a bus route to avoid the lights at A12/A1214 junction. This conflicts with route IM9 on the ESC Cycling Strategy. - SCC proposes that the North Green/Valiant Road/Eagle way be a gated bus route. This conflicts with route IM11 on the ESC Cycling Strategy. - SCC is considering several options for additional crossing capacity over the A12. As of January 2022, no definitive option has been published. The ESC Cycling Strategy assumes that IM12 will use a new crossing at the southern boundary of the BT site. <p>2.3 Gear Change: A bold vision for Cycling and Walking (DfT July 2020)</p> <p>To understand the context of the wider policies that The Strategy seeks to add value to, it is suggested</p>	

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				<p>that The Strategy should be read in conjunction with this document (amongst others). One of the stated aims outlined in the 'Gear Change' document is - Healthier, happier, and greener communities I would suggest that the destruction of part of a privately owned woodland, used by the local community for recreation and the pursuit of health does not enhance the greenness of that community.</p> <p>Additional Comments</p> <p>The standard and safety of existing cycle routes in the Martlesham area is severely lacking and do not back the case for any new routes.</p> <p>For example:</p> <ul style="list-style-type: none"> - Cycle paths are encroached by vegetation (for example at Portal Avenue). - Cycle paths conflict with walking paths and in some cases merge (for example at the approach to the pedestrian bridge from the rear of the bowling alley) and cross each other (for example at the front of BT reception) with no indication as to the fact that walkers should have priority <p>A new hard surfaced and lit path does not fit in the nature and character of the Martlesham Woods.</p> <p>The new path through Martlesham Woods does not provide any additional benefit to cyclists:</p>	

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				<ul style="list-style-type: none"> - There is no definitive route towards the east from Martlesham Woods as an A12 crossing at that point is not confirmed. That is an eastern destination is not clear. - There are several existing routes towards the West which negates the need for a path through the woods. - It is difficult to understand how this route would save much in time or distance over existing routes. - It simply funnels cyclists north and towards the same local destinations of existing routes. 	
IM12	Ed Abbott	215	No	<p>Please use eagle way as the route around Martlesham heath - it will use the minimal amount of tax payers money, the road is wide, slow and could accomodate cycle lanes. Going through the woods is not ideal but is preferable to cutting through a field which would impact directly on a number of houses and stare right into gardens. Surely the northern bridge (option A) and then following the 0.3miles extra around eagle way before diverting to Kesgrave is the easiest, most financially savvy and least disruptive to the residents as possible.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Edwards	65	No	<p>In regard to IM12 - in particular the section that goes from Eagle Way east to Eagle Way west. This section is completely unnecessary. It is already possible for walkers to traverse the woods safely, and it is surely no hardship for cyclists to go an extra quarter of a mile and just follow Eagle Way round its southern section? This is not a busy road, and is already well</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the</p>

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				lit. It makes no sense to instead opt for destroying a wide section of the woodland as you propose, especially given the cost of installing a suitable surface and lighting. (In the autumn, that surface would be completely covered in leaves anyway, making it hazardous for cyclists). Cutting a wide path through a section of amenity woodland for no good purpose is tantamount to environmental vandalism - at a time when we are supposed to be more conscious than ever about protecting our planet. How an you possibly justify this to save cyclists a 5 minute ride round a perfectly safe road?	proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Elaine Everitt	137	No	<p>In 35 years of living here I have never heard of Martlesham Woods!</p> <p>The birch woods are privately owned by every householder in the "new" village of Martlesham Heath through Martlesham Heath Householders Limited. They have been very loosely managed and have been a green lung for residents and a haven for flora and fauna.</p> <p>The existing bridge over the A12 and cycle path around Eagle Way are perfectly adequate; we use them to shop at the stores and retail parks which have appeared on the other side of the A12. The developers and councils have allowed far too much to be built there, with insufficient car parking space. Most shoppers come from outside the area and none of them are likely to use the cycle paths.</p> <p>We are currently being told by government and many organisations that we must protect trees and</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>plant many more to avoid the drastic effects of climate change. It would be a crime to ruin our woods with tarmac paths and lights when roads exist and distances would not be shortened.</p> <p>There is no justification at all for this section of the plan.</p>	
IM12	Elizabeth Hackett	740	No	<p>I am writing to strongly oppose the planning of the Martlesham Heath cycle and walking strategy.</p> <p>I don't understand how we all pay the MHHL fees to protect the beautiful heath land and wildlife yet you abuse this by firstly allowing the block of flats and car park to be built by the Square and now by destroying the mature trees and heathland to put yet more concrete down and cause more light pollution. Don't you think that if this is the way forward we should stop paying to preserve it as you are going against the initial intentions of the heath.</p> <p>There are enough lovely paths for walking and cycling already on the heath. They are not over crowded at all so all this seems totally unnecessary.</p> <p>The children love playing in the woods and exploring nature. More bikes going through and people from other estates will make it more dangerous. This plan would deter parents of the local children to encourage them out to play in these areas.</p> <p>We enjoy living on the Heath feeling that we are in the countryside and do not wish to see bridges, concrete, lights that will make me feel I'm living in</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>more of an urban area.</p> <p>Please let us keep the Martlesham Heath area as natural and beautiful as possible. Allow nature in and not urban development.</p>	
IM12	Emma Noye	625	No	<p>We live next door to these woods and strongly oppose the introduction of a cycle path cutting through Birch Woods (incorrectly referred to as Martlesham Woods in the document). It is unnecessary and will destroy the natural ambience of the woods. I urge you to reconsider this part of the proposal.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Eric Cook	682		<p>Regarding the proposed route IM12 Through the Birch Woods on Martlesham Heath.</p> <p>I've lived on Martlesham Heath for the past 38 years. It is a delightful place to live. Martlesham Heath was planned from the very beginning, to have individual hamlets separated by open spaces of heathland, woods and village greens. The Birch woods are a precious amenity enjoyed by local residents, and supporting a wide variety of wildlife. To route IM12 through the middle of the woods would be an act of environmental vandalism. Adopting a route, on the open land, to the south of Martlesham Heath Village</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and</p>

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				<p>across would be more appropriate.</p> <p>From the age of 11 to 71 a bicycle has been my main form of transport, I only use my car if there are no other options. I regard Eagle Way as a very safe route for cyclists. In the last 38 years I know of no accidents involving motor vehicles and cyclists on Eagle Way.</p>	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Ernest John Geary	56	No	<p>The destruction of part of a Wood in the name of ' Sustainable Transport ' Climate Change and protection of the environment is ludicrous! Add the CO2 emissions from the manufacture of the concrete and other materials needed compounds the damage. Proposals IM11, 13 and 14 provide the links.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Ernest John Geary	225	No	<p>The suggestion of putting a hard surfaced lit path through the woods goes against all the biodiversity and wellbeing policies. As for ' natural surveillance ' this is a wood, trees have leaves! See ' community suggestion ' 347 for a better idea.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and</p>

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					walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Europa Translations (Bennett, John)	663	No	<p>We object to any part of our woods being paved and lit in order to provide a cycle path. These woods are enjoyed by walkers as they provide a peaceful natural environment which would be ruined by an illuminated cycle path.</p> <p>The existing bridge and paths provide ample facility for cyclists.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Fiona Carlin	604	No	<p>Sadly I feel this is further destruction of woodland where no benefit will be gained. Nightingales, deer, foxes, badgers are amongst many animals that will be impacted due to the reduction of their habitat and bringing urban values into a natural wooded area. The light pollution, further destroying our climate, is against Cop26 values signed by governments so surely should be followed by local councils.</p> <p>Furthermore during National Tree week, 27 November to 5th December, and the UK's largest tree celebration, Martlesham took delivery of hedgerows and trees from The Woodland Trust and</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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				<p>Suffolk County Council, planned to make our Parish more wildlife friendly and climate resilient. If this proposal goes ahead this will be in total contradiction of the powerful message and benefits gained and negating the investment from our Council.</p> <p>The expectation that this will link the new Brightwell Lakes development to Ipswich is a pie in the sky dream to tick a box for development approval. There is already a foot and cycle bridge over the A12 that could be improved, therefore saving massive amounts of council funding, which we are constantly told is reducing and should therefore be spent wisely.</p> <p>If you live in this area, you will see the increasing no. of cars not cyclists and purely throwing much needed council budget at a project that will give no benefit seems extremely wasteful.</p> <p>The routes already available and used are more than suitable and no time savings will be made so I can't see anything to be gained in proceeding with this.</p>	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Fosker	207	No	<p>The wood should be left as it is, so many animals and trees. The crossing and path would be better as described on the A12 consultation - results not yet shared? To go round the back of the heath straight across from if33?</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide</p>

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					a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Gavin Raby	24	No	The destruction of these woods for a new bridge is not acceptable. We back on to these woods and they provide a home for lots of wildlife including muntjac deers. We would lose privacy into our gardens and also lose the natural barrier these woods provide us from the A12 traffic and noise. In additional the further paths through the woods I wouldn't deem suitable for cycling due to the terrain. There is a perfectly suitable bridge just further up the A12 which provides a far better cycle route on tarmac and wouldn't involve the destruction of woodland and associated wildlife habitat.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Gavin Raby	25	No	Reference IM12 - The destruction of these woods for a new bridge is not acceptable. We back on to these woods and they provide a home for lots of wildlife including muntjac deers. We would lose privacy into our gardens and also lose the natural barrier these woods provide us from the A12 traffic and noise. In additional the further paths through the woods I wouldn't deem suitable for cycling due to the terrain. There is a perfectly suitable bridge just further up the A12 which provides a far better cycle route on tarmac and wouldn't involve the destruction of woodland and associated wildlife habitat.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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IM12	Gavin Raby	380	No	<p>I'm very opposed to this proposal, it would involve destruction of woodland which is home to a wide variety of wildlife, you would have a tarmaced and lit pathway put through a natural wooded area which would totally destroy it. A new footbridge would result in the removal of trees which gives the houses shielding from the pollution and noise of the road, it would mean complete loss of privacy for the residents of the houses it would overlook. Why is a new bridge being considered when there's an existing one only a few hundred metres further up linking up existing cycle lanes? The money should be spent on better playground and sports facilities in the area.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Gemma Thomas	654	No	<p>Introducing a lit cycling route to Martlesham Woods will completely alter this area for the worse. This is an area frequented by walkers, but the current paths are more than adequate for that purpose. Adding a tarmac path here would completely ruin the natural feel of the woods. I think this would also negatively impact the wildlife living in the woods.</p> <p>I would also question the safety of children and people with dogs trying to cross the cycle path from the remaining woodland paths to get to the Green and properties in the area if there are fast moving cycles coming along the path.</p> <p>i also think that creating a cycle path there would encourage people to cycle straight across Eagles Way onto the Heathland to cut onto longstrops,</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				rather than using the cycle paths, which would again affect the natural heathland there and could be dangerous given the number of dogs that are walked in that area.	
IM12	Gentry, Thomas H F	318	No	<p>I object to this proposal as I see no benefit to Brightwell Lakes or Martlesham Heath. It runs through privately owned land – land owned collectively by all the house owners of Martlesham Heath and managed on their behalf by Martlesham Heath Householders Ltd (MHHL) as is required by a legally enforced restrictive covenant.</p> <p>The proposal is environmentally damaging and disruptive to the wildlife in the area. The removal of trees is a retrograde step and not beneficial to the ecosystem. Any attempt to mitigate this by planting replacement trees elsewhere is pointless as the damage will be immediate but any benefit from the trees will not be apparent for several years, and may well be too late the way climate change is going. It will not, in any case, alter the damage done in the location from which the trees will have been removed.</p> <p>As the home owners are obliged to pay MHHL for the maintenance of the jointly-owned land I am concerned that any future maintenance of this unwanted path across their land will fall upon their shoulders, a totally unjustified imposition. I sincerely hope that if this proposal does unfortunately go ahead the East Suffolk Council will provide the necessary funds for any upkeep as and when required; it should not fall upon the</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>Martlesham Heath residents.</p> <p>If it really is necessary to give residents of Brightwell Lakes access to Eagle Way an alternative route should be planned which avoids damage to the environment and wildlife, and also avoids foisting extra expense on Martlesham Heath house owners.</p>	
IM12	Geoff Passmore	230	No	<p>This proposal, as currently formulated, should not go ahead for a number of reasons.</p> <p>The woods at Martlesham Heath are a valuable public amenity helping the mental well-being of a wide range of people. The proposal would totally destroy it's value and replace it with an eyesore and (based on what evidence shows has happened elsewhere in similar circumstances) encourage vandalism and anti-social behaviour. The land is privately owned, and it is highly unlikely that it would be made available to the council. It is also, I believe, subject to a section 52 agreement to maintain it's natural character and prevent wanton development.</p> <p>The current bridge over the A12 nearby which caters for cyclists is far from reaching it's capacity, so a further expensive bridge is not necessary. In the current climate, the council should be allocating funds to more pressing needs (such as social care) rather than to a scheme which seems to be driven more by a "tick the green credentials box" culture.</p> <p>The proposal to route a path through the woods would, in any case, turn out to be a white elephant</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				as, with such a long distance away from built-up areas, many potential users would feel frightened about using it after dark. A few street lights would not prevent this. If the plan for a new bridge did go ahead, then a much better approach would be to route the cycle way along Eagle Way in either (or both) directions. This approach is being proposed in 1M10 and 1M11, so there is absolutely no reason why this should not also happen in this case.	
IM12	Gill Harris	765		<p>The new bridge over the A12 and through Martlesham Woods passes through woodland managed by MHHL. All households in Martlesham Heath pay an annual maintenance charge for "community amenity" land. Kesgrave and Brightwell Lakes contribute nothing yet this proposed route will benefit them.</p> <p>Martlesham Woods are a little oasis well used by young families and dog walkers. Deer, foxes and nightingales, woodpeckers, robins, tits etc abound currently but an intrusive cycle path and light will negatively impact the wildlife. It will also impact all walkers as cyclists race through on their way to and from walk.</p> <p>There will also be the temptation for leisure cyclists to forge their own off road paths through other parts of the woods. Petty vandalism and anti-social behaviour is also more likely.</p> <p>There will also be the temptation on exiting the woods to cut across the SSI instead of using Eagle Way to link to existing path</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>If a new bridge has to be built, cyclists should be expected to use Eagle Way to join up with the existing cycle path through the centre of the village and recrossing EagleWay near the Valiant Road bus stop and use existing cycle path past control tower and Gorseland School.</p> <p>I am a keen walker and cyclist but I feel it is particularly important to maintain green, wildlife-friendly oases. A lot of volunteers have worked hard over many years, and continue to do so, to maintain the woods and SSI for the peaceful enjoyment of the people who live in the parish. We do not want these spaces turned into rat runs.</p>	
IM12	Gill Harris	766		<p>See comment below for IM12. Similar arguments exist for the perimeter track through Portal Woods. There is already a cycle path alongside the A1214 so no need for further damage to woodland.</p> <p>The new bridge over the A12 and through Martlesham Woods passes through woodland managed by MHHL. All households in Martlesham Heath pay an annual maintenance charge for "community amenity" land. Kesgrave and Brightwell Lakes contribute nothing yet this proposed route will benefit them.</p> <p>Martlesham Woods are a little oasis well used by young families and dog walkers. Deer, foxes and nightingales, woodpeckers, robins, tits etc abound currently but an intrusive cycle path and light will negatively impact the wildlife. It will also impact all</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>walkers as cyclists race through on their way to and from walk.</p> <p>There will also be the temptation for leisure cyclists to forge their own off road paths through other parts of the woods. Petty vandalism and anti-social behaviour is also more likely.</p> <p>There will also be the temptation on exiting the woods to cut across the SSI instead of using Eagle Way to link to existing path</p> <p>If a new bridge has to be built, cyclists should be expected to use Eagle Way to join up with the existing cycle path through the centre of the village and recrossing EagleWay near the Valiant Road bus stop and use existing cycle path past control tower and Gorseland School.</p> <p>I am a keen walker and cyclist but I feel it is particularly important to maintain green, wildlife-friendly oases. A lot of volunteers have worked hard over many years, and continue to do so, to maintain the woods and SSI for the peaceful enjoyment of the people who live in the parish. We do not want these spaces turned into rat runs.</p>	
IM12	Ginny Turner	85	No	<p>I strongly disagree with the proposal to turn the footpath through Martlesham woods into cycle route. My arguments are listed below:</p> <ol style="list-style-type: none"> 1. The UK hosted COP26 a few weeks ago. As a result of agreements, we should be trying to reduce our carbon footprint. Eagle way runs around Martlesham 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the</p>

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				<p>woods, it is well lit and quiet and as such would make a sensible bike route without any of the infrastructure energy costs needed to convert the footpath in the woods. Turning the footpath into a cycle path would result in loss of trees which absorb carbon dioxide from the atmosphere also these trees contain fixed carbon which, when cut down, would be returned to the atmosphere.</p> <p>2. A busy lit footpath would damage natural habitats in the woods. Both bikes and lights would effect local plants and animals. This summer I had grass snakes in my garden, they would have come from the woods. It is illegal to injure grass snakes in the UK and a lit, tarmacked cycle path would certainly effect their habitat. I feel strongly that you should not be damaging wild life areas when other areas would be available for use such as down the edge of the fields which abut Martlesham Heath or around Eagle way.</p> <p>3. Martlesham woods are a tranquil, beauty area for local people. I meet many people walking in the woods, some with very young children, some elderly, many with dogs. The advent of a busy cycle path would make it difficult for such people to walk along this footpath. National cycle route one through the centre of Kesgrave is not a good place for walkers because of all the fast cyclists.</p> <p>4. Martlesham parish council has pledged to reduce its carbon footprint. It devoted a whole day to engaging local people to reduce their carbon</p>	<p>proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>footprint. Part of that plan must be to preserve the trees and woods in Martlesham as this is an important way in which carbon dioxide is removed from the atmosphere. It seems inappropriate for the plans to be made which ride rough shod over the wishes of the local parish council.</p>	
IM12	Graham Emsden	233	No	<p>There is an existing pedestrian/cycle bridge crossing the A12 from Martlesham Heath to the Leisure Area of of the Industrial/commercial area of Martlesham which is just a short cycle ride from the Brightwell Lakes development and would therefore be in the right place to allow cyclists and pedestrians to cross the A12 without the need for an additional bridge.</p> <p>Further the proposed new bridge and associated cycle/pedestrian track would require the destruction of a considerable amount of established woodland which could only be described as vandalism. This destruction would also remove valuable carbon storage, something we are constantly being reminded is essential to us all.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Graham Ixer	770	No	<p>Broadly the proposals seem acceptable with one notable exception. The proposed route labelled IM12 on the draft plan seems both unnecessary and environmentally damaging where it goes through what you have labelled Martlesham Woods. It is surely not asking too much for cyclists to negotiate that part of the village using Eagle Way and while some improvement to the surface for walking might be desirable very few would wish to walk through that particular area after dark so lighting seems inappropriate.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide</p>

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				<p>I also have some concerns about where the route labelled IM10 crosses the existing footpath running from Broomfield to Gorseland School. This is a heavily used path at certain times and the potential for accidents seems quite high. I also feel the route labelled IM9 should be pedestrian only particularly the section between Gorseland School and Portal Avenue.</p> <p>In general while I would support attempts to improve both cycling and walking the way the cycle paths are used by too many cyclists make their joint use dangerous and if ways could be found to slow some of the more enthusiastic “fitness” cyclists the proposals would be more attractive. Sadly the whole cycling industry in this country seems to be aimed at either performance or family outings unlike in Europe where the bike is regarded much more as simply another means of transport.</p>	a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Graham Martin	756	No	<p>I wish to register my objection to IM12 of the ESC Cycling and Walking Strategy.</p> <p>This section is obviously aimed at serving the proposed new housing on the eastern side of the A12, I contend that they will be well enough served by exiting routes IM11 and IM13.</p> <p>I understand that this whole cycling strategy is aimed at encouraging people to cycle rather than use cars, however, I have lived on Martlesham heath for over thirty years and I can assure you that cyclists are not a common sight, certainly not common enough to warrant the kind of expenditure that this</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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				<p>route would require. It is my opinion that this route is not likely to encourage many people to start cycling, benefit those that already cycle very little and would offer a very poor cost to benefit ratio.</p> <p>The route will cause undesirable environmental disturbance to the much valued wood that forms, with the green, the core of Martlesham Heath "village" as it would involve surfacing and lighting some existing paths (there are those that are displeased even with the hogging that has been applied to some paths recently). Wild birds are already confused by the lighting covering large areas and a lit path will do nothing to help, and, as things stand, dogs can be allowed off the lead in safety, that will not be the case if IM12 is constructed.</p> <p>I feel that IM12 is an unnecessary, undesirable and expensive proposal.</p>	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Guy Hillyard	749	No	<p>We have recently been advised of the proposal to push a bridge across the A12, cutting through the woods between Coopers and Lancaster Drive, along with a new paved and lit cycling/pedestrian route from there through Birch Woods (which you refer to as Martlesham Woods).</p> <p>I would like to raise a significant objection to this proposal as long term resident of Martlesham Heath, as I see this as a major intrusion to the infrastructure and unnecessary construction, knowing that a bridge already exists for this purpose and has done so for considerable period of time. This currently provides a natural route across</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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				<p>the Heath and adequately caters for the cyclists and walkers from Adastral Park (BT) etc. through to Grange Farm and see no reason to cut across the existing land as you have proposed.</p> <p>You seem to have a total disregard for the quality of life that people enjoy for the continual and attritional encroachment on the fragile infrastructure that exists in preference to pandering to the unnecessary and ill planned developments.</p> <p>When is the East Suffolk Council actually going to be brave enough to preserve the countryside it is intended to manage.</p> <p>Please accept this email as my OBJECTION.</p>	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Hatch, Christopher	767		<p>I am writing to express some views on certain aspects of the East Suffolk Cycling and Walking Strategy, as currently being circulated for public consultation, and as specifically relating to the Martlesham Heath and Martlesham areas.</p> <p>Of particular concern is the proposed cycling route marked IM12 on the local plan. Its construction, involving the creation of a hard surfaced route with streetlights running through Birch Woods, would result in a direct loss of natural habitat for both mature trees and a range of wildlife. Surely it is a matter of both shame and unprofessionalism to have even considered building this route through designated County Wildlife site? Route IM12 would also tend to encourage inconsiderate cyclists to cut across the SSSI towards Dobbs Lane</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>(the local IM4 route) rather than take the planned option of using a short length of Eagle Way and transfer on to route IM7 towards Kesgrave and beyond.</p> <p>In further regard to route IM12, the proposed location of new bridge across the A12 is ill-considered. To address this concern, consideration should instead be given to upgrading (perhaps covering-in and certainly widening) the existing bridge over the A12 on route IM11. More generally, would it not be better to consider building a new cycle/footbridge (or subway) nearer to the Foxhall Road/A12 junction and aligning it with a re-routed PROW6 bridleway? This could then be connected with a new cycle route running adjacent to Foxhall Road and leading along to Bell Lane. Cycle route links could also be constructed to join this route to proposed route IM4 as well as provide the optimal route for cyclists leaving the new Brightwell Lakes dwellings and wishing to travel to Kesgrave or Ipswich. Of course, a narrow strip of farmland along Foxhall Road, would have to be procured for this purpose. However, there will be some costs and consequences, for any choice of new tarmac-route provision in this increasingly built-up area.</p>	
IM12	Hay, Fiona	769	No	<p>I wish to oppose the draft cycling and walking strategy in terms of the proposals which will have a significant impact on Martlesham Heath Householders Limited land.</p> <p>A new bridge over the A12: There is already an existing foot/cycling bridge over the A12 with safe</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the</p>

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				<p>access to this for cyclists and walkers. There is no need to build another bridge when people just need to walk or cycle up to the existing structure. There is good access to the existing bridge via roads with safe crossing (ie pedestrian crossing near Aldi) and approaches from beyond the Holeshot Cafe, past Barrack Square, onto Gloster Road and up to the bridge. There can surely be no reason why it is necessary to build another bridge (only a short distance up the road from the existing one) which will add to noise and light pollution for nearby residents. There can be no reason why this should even be considered if it means felling existing mature trees. We are supposed to be trying to save nature not destroy it. There is enough pressure on the flora, fora and wildlife as it is around Martlesham Heath with all the retail development that has gone on as well as building projects nearby. And, we have yet to experience what the full impact of 2,000 homes on the BT land will be. If the idea is to join the new housing development with Martlesham Heath it should be by way of a subway that comes out onto Eagle Way thus avoiding the destruction of trees and the wildlife that depend on these trees.</p> <p>Once people cross the A12 via the existing bridge, there are ample cycle and walkways on the Heath. There is no need to destroy trees and land to build any more. We have access to paths that take us to amenities on the Heath e.g. The Square and for people to go further on to Kesgrave and into Town. This path should especially not be considered as it</p>	<p>proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>will include a path through Birch Woods and may be a risk in terms of the SSSI - as people exit the proposed path near Forest Lane onto Eagle Way, it may be tempting to take a short cut across the SSSI.</p> <p>There is a proposal to have up to 8 sets of traffic lights up the A12 from where it heads north from the A14 up to Melton. This includes several sets of traffic lights running the length of the stretch of the A12 past Martlesham Heath. Therefore, there will by definition be the opportunity to build in yet more safe crossing for people wishing to get from one side of the A12 to the other.</p> <p>We do NOT need a lit tarmac path through Portal Woods! A wood should not be lit up and there is nothing wrong with the existing perimeter track. There are cycle paths that continue up to the A1214 as well as a path past the Control Tower and on to Kesgrave. A lit path will impact on nocturnal wildlife. Yet again, something is being considered when we are meant to be safeguarding the nature we have around us, not doing something that would directly impact upon it.</p> <p>To summarise, there are already existing safe places to cycle and walk around the Martlesham area including safe access to an existing bridge across the A12. There are moves afoot to put in umpteen sets of traffic lights along the A12 which could provide yet more safe crossing. We are meant to be trying to preserve the natural environment and this 'project' will have a direct impact on the natural environment</p>	

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				on Martlesham Heath. The ideas for Martlesham Heath included in this draft plan are ill conceived and totally unnecessary. I am sure if there is spare money to develop cycling and walking pathways that there are places far more in need of them than here.	
IM12	Heather Turner	741	No	<p>Along side MHHL I have concern about the proposal to push a bridge across the A12, cutting through the woods between Coopers and Lancaster Drive, and create a new paved and lit cycling / pedestrian route from there through Birch Woods (which East Suffolk Council refer to as Martlesham Woods).</p> <p>This action will affect the aesthetic appearance of the village which has been tirelessly upheld by the residents.</p> <p>The residents would be giving up over one mile of valued land consisting of natural woodland forming a pleasing opportunity to share the wildlife</p> <p>And natural beauty and also the possibly losing and also conserving the wild nature of Martlesham Householders land.</p> <p>Assuming the same width as the Kesgrave cycle path this would be approximately 4.5m wide. MHHL feel that this would damage the nature of the woods which is essentially 90% unmanaged and is a very effective Carbon sink as well as providing habitat for local wildlife.</p> <p>Thus impacting unfavourably on our carbon footprint.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>It would also bypass the village centre yet still join the same cycle path on Eagle Way. Saving in distance is minimal compared to using the existing bridge.</p> <p>Following the disruption to the area and the felling of over 25 mature trees which would be impossible to replace in the near future and the noise and general upheaval to this beautiful area is totally unexceptionable to me as a resident of over 22years standing. I feel I am not alone among my fellow villagers in voicing this feeling.</p> <p>I am concerned greatly about the future possible public impingement to the highly valued well established SSI causing impact to the SSI .</p> <p>Because of the increased use off this new facility it would cause a massive impact to this area and might lead to losing this most valued site.</p> <p>Alongside MHHL I also have concerns regarding the path alongside the Police HQ and the way the council wants to join it up to a new paved path through Long Strops by cutting the corner off the path near the Control Tower. Would also impact on the Village greatly by opening up possible Vandalism.</p> <p>My last thought is a positive one: We have in place a warren of footpaths</p> <p>and cycle ways including keep fit circuit, Please why don't we use this funding in a positive way and</p>	

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				<p>refurbish existing pathways to improve</p> <p>The local area and make it better for residents and visitors alike.</p> <p>Let's us not resort to our own vandalism in the name of improving and so called development to what is an already beautiful facility.</p>	
IM12	Helen Boxall	577	No	<p>I strongly object to a cycle path through Martlesham Woods. Martlesham Woods is a much loved haven of tranquillity, appreciated by every age group who live here. The woods are home to many animals including red-listed sparrowhawks, bats, foxes, deer, squirrels, buzzards. Slicing off a huge piece of the wood for an urban, 4.5m-wide, lit, tarmac road would destroy it, especially as many people are now racing along on electric bikes. Martlesham Heath was a carefully planned village, designed to avoid rat runs. The existing roads are quiet, so are safe for cyclists. In view of the above there is no justification for building yet more cycle paths which would only be to the detriment of the existing woodlands and endangered species.</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Helen Wootten	90	No	<p>I disagree with the path being changed to allow more cycling in the Woods instead of using Eagle Way. It would save little or no time for cyclists and is a potential hazard for children, the elderly, dog walkers (some have well trained dogs off lead) plus all the wildlife.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and</p>

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					walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	H J Bennett	737	No	<p>I would like to lodge the following objections re the above.</p> <p>1: There is already a footpath/cycle crossing over the A12 - from east to west giving safe access to Martlesham Heath village centre, Birchwood and Gorsland schools and onwards to Grange Farm etc. which is paved and lit</p> <p>2:The construction of the proposed second bridge would be disruptive to the already congested A12 . It would be higher and have more impact on the surroundings, overlooking homes and gardens in addition to the extra light pollution as I assume there will be lighting installed.</p> <p>3. The woodland between the A12 and Eagle Way is a non public natural haven and gives a safe route for wildlife. This sanctuary would be lost together with trees, flora and fauna habitat.</p> <p>4 The Martlesham Heath Birch Wood is used by many as a peaceful, relaxing and health restorative place of natural beauty, populated with a variety of tree species, flora and fauna. As well as an opportunity to catch a glimpse of different wildlife during the changing seasons.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>Clearing the area for the proposed route from Eagle Way through to the Forest Lane area would entail felling a number of trees and destroying the natural fabric of the wood. The increased footfall would impact on the whole wood, not just the proposed route, with the feeling of safety when walking on ones own (as many residents do) being lost, particularly mornings and evenings.</p> <p>5 My understanding is that the Western Corridor is a SSSI which has restrictions on certain developments which covers a cycle path (and lights) where the proposed woodland route emerges on the west side of Eagle Way. If that is so, it would seem that a new route would have to go alongside Eagle Way until Valiant Road where it would join the existing footpath/cycle route.</p> <p>6 This new proposed route will be expensive to put all the infrastructure in place; disruptive to residents and businesses, not to mention continuing costs of maintenance, lighting, etc. The loss of habitat and privacy, in addition to possible loss of property value and local trade. In addition the inconvenience and disruption of motorists during construction.</p> <p>Taking the points listed above, I strongly object to the proposed cycle and footpath crossing the A12 between Brightwell and Martlesham Heath called the 'North Option'.</p>	
IM12	Ian Dudley	375	No	<p>I wish to lodge my objections to the above proposals concerning Martlesham Heath / Brightwell Lakes development for the following reasons:</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is</p>

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				<p>There is already a footbridge bridge across the A12 which , in my opinion, is not the most used or popular way to cross!</p> <p>To build a new bridge across the A12 is, in my opinion, a waste of public money. Who would, be responsible for the upkeep?</p> <p>Council tax rates are high enough!.</p> <p>The proposed new foot/cycle path routes through the village, would create in the region of 1 mile of tarmac cycle/footpaths with them cutting through Land owned and managed by Martlesham Heath Householders Ltd, on behalf of the resident membership.</p> <p>The proposed bridge would necessitate the creation of another path to continue through Birch Woods (Martlesham Woods) and would require the felling of at least 25 mature trees, an action that is not very environmentally friendly!</p> <p>Where the proposed path would exit near Forest Lane on Eagle Way, cyclists and pedestrians would be tempted to take, (on their way to Dobbs Lane)</p> <p>a 'short cut' across our village's SSI (Site of Special Scientific Interest) which is maintained by MHHL and numerous volunteer residents.</p> <p>Why is there a need for a new lit tarmac pathway</p>	<p>recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>route around the perimeter of Portal Woods ?</p> <p>WHY is a new lit tarmac route from Dobbs Lane along the back of Whinfield /</p> <p>Broomfield leading to Eagle Way a necessity?</p> <p>I can think of no reasons that the above proposals should be accepted, indeed, the proposed exercise is another example of humans interfering with the environment when it is unnecessary.</p> <p>If people want to cycle or walk, they are able to do so within the currently available cycle/footpaths!</p>	
IM12	Ian Green	106	No	<p>During the lockdown I walked through the woods most days for my exercise. I enjoyed looking at the painted stones that were done by children and left for others to appreciate.</p> <p>I would object to the woodland paths being concreted over, firstly because it would harm the habitat and also cause damage and injuries to children should they fall off their bicycles. The other suggestion that it is to be lit is ridiculous. Aren't we supposed to be saving on electricity to make the planet greener. There is nothing wrong with the paths through the woods as they are.</p> <p>Eagle Way could not be classed as a busy road and it is amply wide enough for cyclists.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Ian Read	564		<p>My wife and I welcome ES Council's acknowledgement that cycle pathways in our neighbourhood are overdue for increase and improvement. Also that there is a need for a</p>	<p>While the support for the previously proposed cycling and walking bridge is welcomed, the importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the</p>

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				<p>strategic plan, to ensure that cycle paths are joined-up (in the literal sense!) to encourage more cycling.</p> <p>We are long-term residents of Martlesham Heath and cycle users, both for transport and leisure/fitness. Martlesham Heath cycle path provision has always been inadequate (e.g. compared to the newer Grange Farm, Kesgrave development), so in principle we welcome planning and investment to address this.</p> <p>IM12 - In principle, we believe the proposed second footbridge over the A12 is a good idea. Having a very busy trunk road bisecting current and planned conurbations is very much less than ideal. The existing footbridge and paths are barely wide enough to be shared between the current volume of cyclists and pedestrians, who also have to cross the busy Gloster Road to continue on the cycle path past along the front of Adastral Park, so a new wider footbridge where planned would serve residents of the new Brightwell Lakes development well. However the proposed cycle and pedestrian route to the bridge seems to have been perversely designed to maximise the destruction of the Martlesham Heath woodland, rather than the opposite! Providing a dedicated cycle path along Eagle Way - with its broad verges - between IM11 and IM12, would be much easier and cheaper, meaning only a small section of wood/scrub-land would need to be crossed to reach the main road. It would also encourage more cyclists to go past/use the Martlesham Heath shopping square.</p>	<p>proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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IM12	Ian Sargeant	260	No	If this is genuinely a consultation then surely you would also need to visit every resident of Lancaster drive to see what the visual impact would be to the residents whom have gardens, houses and bungalows that will all suffer being greatly overlooked and suffer from blocked natural light as a direct result of this construction of a cycle/walking bridge over this point of the A12 so close to Lancaster drive residents property. You would also be destroying with this proposed pathway cycle route a safe and quiet area of natural woodland populated by many animals often seen including deer, bats, butterflies and bees. I 100% object to this appalling proposal opposite lancaster drive.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Ian White	240		With respect to your Draft Cycling & Walking Strategy for Martlesham Heath. I have few objections to your proposal and like the fact that two of these routes, the one around the old perimeter track in the Portal Woods and the new one proposed through the Birch Woods via a new bridge over the A12, appear to be reasonable given the increase in cycle traffic likely to occur with the development of the so-called 'Brightwell Lakes' site. Having said that I wouldn't wish to walk along them at night! By the bye, where is the lake(s)? The loss of some 25 mature trees has to be set against this proposal. However, my only concern is who will be responsible for the maintenance of these cycle paths, particularly the lightning and the verges and what will be done about a proportion of the cyclists who ride in a reckless manner past/through	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				pedestrians. Are we perhaps allowed to knock them of their bicycles.	
IM12	Ian Wright	356	No	<p>I would like to object to the proposed route of the cycle track through Martlesham Woods shown as IM12 on the plan for the following reasons:-</p> <p>1) There is already a bridge across the A12 on the IM11 route which could be used via IM13, saving the cost of the additional bridge.</p> <p>2) The area known as Martlesham Woods is a very peaceful and safe haven for walkers. The introduction of cyclists will inevitably cause accidents between cyclists and walkers.</p> <p>3) If a cycle track bridge is definitely required, could the route of IM12 turn north where it meets Eagle Way just south of the Coopers Rd junction, then follow Eagle Way heading north until it meets and joins up with IM11?</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Janet & Clive Thornhill	242	No	<p>Martlesham Heath residents have received information from Martlesham Heath Householders Ltd about your proposal to build a new Bridge across the A12, followed by the creation of 1 mile of new lit tarmac/cycle footpaths on land owned and managed by MHHL on behalf of our members. Land for which every one of the residents of Martlesham Heath pay a substantial premium for its use. And for which is for the use and benefit of the new Brightwell Lakes Development .For which they would presumably pay nothing.</p> <p>I would like to register my objection to this scheme</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and</p>

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				<p>in the strongest possible terms. On grounds of the damage this would cause to the precious environment of our village as envisioned and created so many years ago by the founders and architects of our village.</p> <p>Our valued village amenity of Birch Woods (who decided to rename I this Martlesham woods?) would suffer the loss of 25 mature trees, which can only be of detriment to the environment and its wildlife, as well as the loss of recreation and dog walking amenities used for generations by local residents.</p> <p>Next I see that this new construction would entail carving a fully lit tarmac path through the woods some 4.5m wide. Huge in cycling terms – this is more like a cycling motorway than a path! With all the associated light pollution. The prospect of the massive number of cyclists who would have free access to our woods – not to mention that it would probably become the venue of choice for not just cycles but also mopeds, scooters and the new generation of electric e-scooters which are illegal for use on roads and pavements which would have a heaven-sent opportunity of taking the so-called path over for their recreational use. All 3 areas of Birch Woods, Portal Woods and our SSSI would be at risk of irreparable damage. Speaking of which, this scheme would entail regular repairs and ongoing maintenance to he Brightwell Lakes Cycle Path – would this ongoing expense be foisted upon the residents of Martlesham Heath by regular increases to our annual maintenance fee?</p>	<p>Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>In short I feel that in proposing this new Bridge and Cycle Scheme the Council have acted in an extremely high-handed manner without thought or care for our wildlife, amenity and any care whatsoever for the interests of Martlesham Heath with the intention of acquiring and mis-using land it does not even own, which has been faithfully husbanded and protected by the Volunteers who freely give huge amounts of time and effort for the benefit of this village. We would like the Council to carefully reconsider this strategy which is totally inappropriate for this area. The cyclists cannot deem it unreasonable to be asked to cycle round Eagle Way if they need to access the other side of the village. It is</p> <p>a quiet road, not at all busy, and will take them only minutes to do so.</p>	
IM12	Jan Hamill	308	No	<p>I believe this proposed cycle path is not necessary, a waste of tax payers money, and detrimental to the wildlife within Birch Woods. In todays challenging times re climate changes it is imperative to maintain the woods as they are. The area will not benefit a 4.5 metre path running through Birch Woods. Find another way.</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Jan Hamill	313	No	<p>Waste of tax payers money</p> <p>detrimental to the wildlife of Birch Wood</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling</p>

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				<p>Against climate change strategies - deforestation</p> <p>no major benefit.</p> <p>Other routes available</p>	<p>and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Janine Davey	338	No	<p>I would like to object to the planned cycle route through Birch Woods, Martlesham Heath (referred to in your document as Martlesham Woods). This is a precious amenity area for local residents and an area of natural woodland. The proposal of felling mature trees to make way for the cycle route along with construction of lighting will damage the woodland and disturb wildlife. Where the path would exit on Eagle Way, cyclists and pedestrians would be tempted to take a short cut across our SSSI on their way to Dobbs Lane.</p> <p>The same argument applies for the lit tarmac routes through Portal woods and near Dobbs Lane. Our natural areas and wildlife are constantly under threat from these types of schemes and would urge you to re-visit this decision.</p> <p>I see the proposed schemes as an unnecessary waste of public money seeing as there is a perfectly acceptable existing through-cycle route from the A12 to Grange Farm.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>No doubt you will be conducting the necessary surveys and producing data to justify this expense. The local residents would be very interested in your findings.</p> <p>Please read and carefully consider any objections you receive and not treat this consultation as a 'box-ticking' exercise.</p>	
IM12	Jason Burgess	220	No	<p>This route will result in harm to habitat and amenity through adverse impact and loss of woodland for little benefit to both cyclists and pedestrians compared with other routes identified. It would also result in the (perhaps unintended) consequence of increased pedestrians and possibly cyclists continuing westwards over Eagle Way and across the Martlesham Heath SSSI towards Dobbs Lane - increased trips along this obvious desire line will also result in harm to the SSSI.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Jean Rogers	428	No	<p>I totally oppose this proposal to create a cycle path through Birch Woods/Martlesham Woods. Cyclists who cross the A12 on the newly proposed footpath can join Eagle Way and continue to use the existing well lit road to cross over to the west side of Eagle way to continue their journey reusing existing infrastructure.</p> <p>I feel very strongly about this complete waste of</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and</p>

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				<p>money as this is an area used by locals for personal exercise, family walks and dog walking. The most likely cyclists using this proposed route are likely to be teenagers making their way to and from from Kesgrave High School. Unfortunately, teenagers are not likely to use their lights or bells or take much care if anyone else is using the path. I anticipate that there will be accidents particularly with the elderly trying to get out of the cyclists way. My experience is based on the behaviour of cyclists in Cambridge.</p> <p>Furthermore, the unnecessary destruction of trees to create this path is unbelievable when the local community has sought to carefully maintain this area over many decades. It is contrary to climate change policy.</p>	<p>walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Jean Turner	256		<p>Regarding the proposed link from the new Brightwell Lakes development through Martlesham Heath towards Ipswich (IM12), I have to say that your plan to put a paved and lit cycle path through our lovely Birch Woods (Martlesham Woods on your plan) is pure ecological vandalism. If you must put a fourth crossing over/under the A12 at Brightwell, and I don't see the need when another bridge crossing is quite close by, then cyclists can use the existing road network which is not that busy, or you could put your cycle path round Eagle Way which is already lit and only slightly longer, instead of spoiling one of the very few accessible natural spaces in the area. Currently pedestrians are able to enjoy a quiet walk through the wood along its natural pathways and certainly don't need a paved and, even worse, lit path to do so. Also, damaging the woodland in this</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				way is bound to have a detrimental effect on the local wildlife. I find it hard to believe you are even considering such a thing and request that this plan is revised.	
IM12	Jenny Edgerley	475	No	I fully support the need for a cycling strategy however, I believe the location of this route would have a huge impact on what is a very natural environment and for little benefit. The birch woods (you refer to them as Martlesham woods) are a haven for wildlife, is mostly unmanaged and is well used by walkers enjoying the peace and quiet which is one of the things so unique about the village. There are existing formal cycleways/footpaths from the footbridge leading across Martlesham Heath village to grange farm which are well lit and well used so there is no need to destroy the tranquility and beauty of this area of woodland.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Jeremy and Maggie Cook	683	No	<p>We Strongly Object to East Suffolk Council’s Draft Cycling and Walking Strategy</p> <ul style="list-style-type: none"> We have lived on Martlesham Heath for 43 years. One of the main reasons we chose to live here was because of the woodland and green areas within the village. The proposed bridge crossing the A12 is unnecessary when there is another bridge close by. We understand that the saving in distance of the proposed cycle route is minimal compared to using the existing bridge and cycle route. 	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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				<ul style="list-style-type: none"> • The proposed cycle path is to cut through Birch woods (Martlesham woods) which will require the felling of at least 25 mature trees and the clearing of considerable undergrowth. We have frequently walked in these woods with our children and now with our grand children. The loss of habitat will have a detrimental impact on our wild life. We have observed Deer, Foxes, Squirrels and nesting birds including Nightingales and Sparrow Hawks. There are also many different species of Butterflies living within the woods. • The totally ridiculous idea of lighting the pathway will add to light pollution and disturb our night wild life • This proposed new cycle track will most likely result in the cyclists taking a short cut across the Site of Special Scientific Interest to get to the Dobbs lane cycle track. • We already have a very well used and extensive cycle network which we have personally used. • This proposal does not add to the Eco positives. The affect on the wildlife will be devastating and will destroy our environment. 	<p>Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Jeremy Wilson	426	No	<p>I am writing to raise my strongest objections to the section of IM12 passing through Martlesham Woods and known locally as the Birch Woods.</p> <p>1) The Martlesham Woods are a priceless green reserve in the increasingly urbanised environment of</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For</p>

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				<p>this part of Suffolk. A tarmac and lit cycle path through the woods will severely and permanently damaged the woodland environment. At present most access to the Martlesham Woods is from the Martlesham Heath Green to the north. To have a potentially busy cycle path obstructing the path from the Green to the Woods will make accessing them potentially hazardous.</p> <p>2) The building in itself of such a path in Martlesham Woods will damage the environment. Trees will need to be felled and green areas cleared.</p> <p>3) During the pandemic, many people have discovered Martlesham Woods who were unaware of them before. This path would degrade the environment not just for Martlesham Heath residents such as myself but for anyone living within walking distance.</p> <p>4) This part of the path is quite unnecessary. On a bicycle, a detour round Eagle Way would only add a few minutes ont a journey if that. Eagle Way is not a busy road nor ever likely to be.</p>	<p>these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Jeremy Wilson	692		<p>I am writing to you concerning my comment 426 on the Draft Cycling and Walking Strategy Ipswich to Melton Key Corridor Martlesham Heath - Martlesham Village IM12.</p> <p>It has occurred to me since writing the comment that the path in question passes over land owned by MHHL Ltd of which I and my wife, in common with</p>	<p>In implementing the recommendations set out in the Strategy the intention is to work with landowners and relevant organisations, such as SCC, to ensure the best outcomes for all involved. As such, it will also be important to draw upon various funding and delivery opportunities. However, the importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling</p>

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				other householders, have a legal share in. Therefore I am assuming that you will compulsorily purchase the land for the cycle path. Is this your intention?	and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Jill McFarland	255	No	<p>The proposed route through Martlesham Woods will be very disruptive to the wildlife that live in the woods and the felling of trees is not acceptable, is also a very popular dog walking area and as the dogs are off the lead this could cause accidents with cycles travelling far too fast to avoid the dogs or people. Another route should be found that connects to the existing cycle paths, using the bridge that already exists, there would then be no need for another bridge across the A12, using the path along the side of the BT site could then link up with the bridge and the cycle path leading into Martlesham by the new development that is being being in 2022.</p> <p>The route near Forest Lane onto Eagle Way could encourage pedestrians and cyclists to use a short cut through the SSSI which could also cause significant disruption to wildlife which should be avoided at all costs.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	John and Jo Goodluck	738		Martlesham Heath - Does ESC intend to recompense members of Martlesham Heath Householders Ltd for land to be utilised in the Cycling Strategy which is	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is

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				<p>owned and managed by Martlesham Heath Householders Ltd. Martlesham Parish Council have declared the climate emergency and as a village much work is done to mitigate this. The Martlesham Woods area is a valuable carbon sink and a haven and food source for migrating birds especially Fieldfare, Redwing and Waxwing, as well as a home to common lizard. It is an incredible idea that to help reduce carbon ESC is proposing to cut down mature trees. The gap between Cooper's Road and Lancaster Drive has lain undisturbed by humans for many years, probably since Martlesham Heath was built. It is a haven and a corridor for wildlife. If any bridge is to be put here then it ought to be one especially for wildlife, as main roads contribute to isolating wildlife populations. The cycling route could easily pass closer to the village center.</p>	<p>recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	John Hovell	610	No	<p>I am making these comments as both a resident of Martlesham Heath and a keen walker and cyclist. I am a frequent user of existing local routes.</p> <p>I believe that the village of Martlesham would be better served if the traffic that may use IM 12 were to use IM11 through the centre of the commercial area of the village so both giving a purpose to the route and benefiting the local shops/businesses. This would have the same popularity and benefit that the existing IM7 route through Kesgrave that passes the Kesgrave commercial area.</p> <p>The existing A12 footbridge serves the Martlesham industrial/commercial area plus BT. This crossing is</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>currently inadequate and will be increasingly so in the future. That is, it needs improving now. The proposed new IM12 bridge will not ease the existing bridge traffic and hence is redundant.</p> <p>The IM12 route would encourage pedestrians/cyclists to take a shortcut to the west across Martlesham Heath to Dobbs Lane/IM4, a natural wildlife area that cannot support heavy traffic.</p> <p>IM12's passage through Martlesham Woods would be devastating for the wildlife in these woods. I have seen deer and foxes, grass snakes and adders, green and lesser spotted wood peckers, Jays, nuthatch, tree creeper, sparrowhawk, and many other creatures in these woods. A wide, lit, heavily used path through this area would be devastating for the local wild life.</p> <p>Overall, I see IM12 and the associated footbridge as unnecessary, and detrimental to the local environment. A better use of resources would be to route the traffic through Martlesham centre to support local business and over the existing footbridge (improved) to access the Martlesham industrial area and BT.</p>	
IM12	John Kelso	779	No	<p>I object to the provision of a new bridge across the A12 and the suggested provision of a cycle/pedestrian route being established between Coopers Road and Lancaster Drive.</p> <p>Also to the provision of a cycle/pedestrian route through the Martlesham Heath Birchwood which</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For</p>

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				<p>would be lit and hard surfaced which would prove detrimental to the existing wildlife. It should be noted that both of the afore mentioned areas are owned by the residents of Martlesham Heath via Martlesham Heath Householders limited.</p> <p>The section of cycle/pedestrian path along Eagle Way between the birchwood and Valiant Road should also be deleted.</p> <p>I would also suggest that just because funds are available from Developer contributions they should not be used for facilities that would prove detrimental to existing communities ,residents and much appreciated existing facilities.</p>	<p>these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	John McQuin	634	No	<p>I am unsure what segregated walking and cycling routes implies on this route. Is it completely separate routes ie the existing path is maintained for walkers or a shared path with a dividing line.</p> <p>Does the reference to natural surveillance imply that the council believes that introduction of this route may lead to increased anti-social activity or crime?</p> <p>The introduction of a new surfaced route through Martlesham Woods would involve the loss of trees and expenditure of energy for lighting. This will be detrimental to the wildlife in the woods and incur initial and ongoing costs for little benefit. The cost of a bridge crossing over the A12 seems hard to justify for a lower priority route.</p> <p>The strategy recognises that the route is not high priority and it can be seen that access from Brightwell Lakes to Martlesham Heath village centre</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>can be achieved from the junction of IF33 and IM13 via IM13 and IM11 at very little extra cost in time or distance compared with the route that goes from IM13, IM12 and IM11. Similarly the route through to IM10 is not significantly improved by the availability of IM12, and not at all for travel towards Woodbridge and Melton. The distances involved are short irrespective of route,</p> <p>It is difficult to see how the required expenditure, as well as adverse impacts on Martlesham Woods, can be justified for such a minimal improvement in access.</p>	
IM12	John Olby	383	No		Objection noted.
IM12	John Olby	386	No	I strongly object to any clearance through MARTLESHAM WOOD particularly felling trees. DO NOT FELL TREES.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Jonathan Clyne	398	No	By "Martlesham Woods" it is assumed Birch Woods are being referred to. This is private land (as is all of the land owned by Martlesham Heath Householders Ltd ("MHHL"), the shareholders of which are the	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling

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				<p>residents of Martlesham Heath) and so any changes would need the approval of MHHL.</p> <p>There is no need for any segregated cycling or walking tracks other than those that already exist. These were carefully planned into the design of Martlesham Heath and there is no need, or requirement, for any change. The existing crossings are perfectly adequate, well used and are safe.</p> <p>There are already 3 crossing points over / under the A12 and there is no need for any further crossing points, especially where the proposals would cause very significant damage to private property and the environment. Cyclists and walkers from the new Brightwell Lakes development can easily use the bridge over the A12 (or one of the other crossings).</p> <p>Dedicated crossing points over Eagle Way are not needed. The road is already quiet and safe to cross at all times of the day or night.</p> <p>The proposed new crossing of the A12 would destroy the natural woodland between the A12 and Eagle Way. The proposed lighting would be detrimental to the wildlife, and the quiet enjoyment of area by the residents.</p> <p>There is no need for a dedicated cycle / walking track through Birch Woods. The existing track is well used by walkers (primarily residents of Martlesham Heath) and the occasional cyclist. The woodland is maintained by MHHL and is deliberately kept as wild</p>	<p>and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				as possible. Wildlife flourishes, with many species of birds and other wildlife seen regularly. At the appropriate time Nightingales can be heard (and sometimes seen). Cutting a dedicated cycle / walking track through this woodland (and on up to the SSSI), and then lighting it would be hugely damaging to this special environment.	
IM12	Jonathan Dixon	589	No	<p>I do have a particular concern with the proposal "IM12" to build a corridor through the Birch Woods at Martlesham Heath. The Birch Woods (referred to in the consultation as "Martlesham Woods") is a popular asset owned by, and for the residents of Martlesham Heath, and its natural beauty provided a calming environment for many during the anxiety of the pandemic. It is home to many wild animals, include deer (muntjac) and foxes, which would be disturbed by any development work and artificial lighting, as well as its use as a public thoroughfare. This path is not currently a public right of way, and is owned by the people who live on Martlesham Heath, as members of MHHL.</p> <p>I understand that there may be a need for cycle and walking routes from Brightwell Lakes, however I believe that IM12 offers little advantage over the existing IM13/IM11 route using the existing pedestrian footbridge next to Martlesham Leisure. I also see that IM25 & IM26 have also proposed as routes to Ipswich from Brightwell Lakes. However if it is considered that a second footbridge over the A12 is required, then connecting to the existing Public right of way ("PROW6") just to the north of Welham Plantation would enable a more direct</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>route from Brightwell Lakes to Dobbs Lane, and onward to Long Strops (IM4).</p> <p>In conclusion I generally support the principles behind this initiative, and am generally supportive of the proposals to improve cycling and walking routes throughout East Suffolk, but object to IM12 through the Martlesham Birch Woods.</p>	
IM12	Jon Saunders	854	No	<p>I am writing to you to express my deep concern and objection to the proposed strategy to destroy more habitat in the area of Martlesham Heath, in the name of progress.....</p> <p>The East Suffolk Cycling and Walking Strategy Draft will have a significant impact on MHHL land (which I am a member of) and I feel the need to express my objection to this 'strategy' in very strong terms.</p> <p>As there are currently more than enough cycle paths around Martlesham Heath, most of which I do not see as being heavily used, as cyclists and joggers prefer to use the road as it is better maintained. I also do not see the necessity to push another cycle bridge across the A12, when there is a perfectly usable one a few 100 yards to the west of the proposed extra location. Is it going to get to the point where there is a bridge every yard so that poor pedestrians and cyclists don't have to take a slightly longer route. I used to work at BT and walk to work over this bridge and never found the it an inconvenience that I had to walk just that bit further than if I had gone in a dead straight line from my house.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>This proposed new bridge and the additional proposed cycle route cutting through the woods between Coopers and Lancaster Drive, and the creation of a new paved and lit cycling / pedestrian route from there through Birch Woods is a ridiculous idea, which may tick a few boxes for the sake of ESDC receiving funding from central government, but will never actually get any return on investment both financially and spiritually, and seems to be another one of these projects that ESDC will plough headlong into without actually taking into consideration any of the concerns and objections of the residents that currently live on the Heath (in the same way that the expansion of the Martlesham business park and the new development next to BT, the proposed development at Martlesham Police Headquarters and of course the new old peoples home on the Heath and the loss of parking (which impacts local business and residents) will just roll on).</p> <p>The proposed route would also be another nail in the coffin of the shopping area on the Heath, as it would encourage people to bypass this but then join up with Eagle to the south of these facilities. Again, the saving in distance is minimal compared to using the existing bridge and it would also encourage passing trade to use the shops on the Heath and help keep these businesses afloat, especially after the pains they have endured over the pandemic.</p> <p>The paved areas through the woodland (assuming</p>	

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				<p>the same width as the Kesgrave cycle path) would damage the nature of the woods which is essentially 90% unmanaged and is a very effective Carbon sink as well as providing habitat for local wildlife. It is also a great resource as far as getting away from it all is concerned, and as a Martlesham resident these woods are somewhere I walk on a daily basis to get away from daily life, and watch the local wildlife going about its business.</p> <p>I also have concerns regarding the path alongside the Police HQ and the way the council wants to join it up to a new paved path through Long Stropps by cutting the corner off the path near the Control Tower.</p> <p>As a lot of the land covered by the strategy is owned by MHHL, how do you propose to compensate Martlesham residents for the upheaval and more noise and intrusion caused by this work. We have already had to suffer the extra noise, pollution and traffic due to the expansion of the Martlesham retail park, and this noise, pollution and traffic is only going to be increased by the other proposed developments in the area.</p> <p>I seriously cant see people suddenly selling their cars and using these cycle ways as modern life doesn't work that way (well maybe only in the happy clappy minds of members of ESDC's cabinet and Planning , who do not live in areas that these plans will have any effect on). Also, If they wanted to encourage cycling, how about spending money to resurface the</p>	

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				<p>existing cycle routes as a) most cyclists don't use them as they are not nice to cycle on and b) change the law/highway code so that it states that where cycle paths are provided then cyclists must use them, leaving the road free for cars and c) have to have insurance and pay some form of road fund to use these facilities.</p> <p>While we're on the subject of cars, why do the council not invoke some form of enforcement relating to parking around the schools at pick up/drop off time. The standards of parking and driving around Birchwood, Gorselands and Kesgrave high schools at these time is appalling and parents should be encouraged to drive and park within the law, and maybe even encouraged to cycle or walk to the schools with their children.</p> <p>And just in case you are wondering, yes, I am also a cyclist, and regularly cycle into Ipswich and Woodbridge from the Heath without any problems.</p> <p>I have more to say, but I feel that none of the opposing views from Martlesham residents will be taken into consideration, and the strategy will become a plan and become reality, no matter what local residents think, as has been demonstrated numerous times before.</p>	
IM12	J T Hancock and Associates Ltd (Jeremy Hancock)	292	No	<p>Whilst I am in support of the concept of overall improvement to cycling facilities and the improvement of existing routes I wish on behalf of myself and my wife Jane Hancock to place an objection to the proposed route for IM12 on the</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close</p>

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				<p>following grounds.</p> <p>a) Taking this "Medium Priority Route" through Martlesham Woods seems unnecessary and damaging to to both the valuable habitat and solitude provided by the woodland perhaps creating conflict with the range of of wildlife that make use of the woodland. To create an illuminated cycleway would particularly degrade the dark conditions which are important to many species inhabiting or visiting this locality. The woodland has for many years been of great value to local residents for recreational purposes only requiring marginal management including consolidation of the most used pathways. The introduction of any lit routeway would in my view be detrimental to this woodland being an "urban" incursion into a very special woodland asset.</p> <p>b) The next point I wish to raise is that Eagle Way provides an excellent cycle route around the Martlesham Heath and there are many connecting roads enabling cycle access to virtually all positions within the heath and connecting roads to Route IM11 and beyond without passing through the Martlesham Woods. The need for the route seems only to arise because of the foot and cycle bridge mooted from the the proposed Adastral Park Development. My contention is that such a bridge is unnecessary as the IM13 route to the west of the A12 connects with the IM11 Route and its existing foot and cycle Bridge over the A12.</p>	<p>proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				c) Bearing in mind the likely cost of a footbridge link, a new illuminated consolidated cycle route through dense Woodland, to provide only a "Medium Priority Route". Would it not be better to redirect such finance to points where high quality cycle routes are required especially where they segregate cycle traffic from major vehicular highways.	
IM12	Judith Grayley	739	No	<p>I would like to comment on the above proposal.</p> <ol style="list-style-type: none"> 1. Why the expense and considerable traffic disruption of building a new bridge over the A12, are cyclists incapable of cycling the short distance from the Brightwell development to the existing bridge? 2. The felling of at least 25 mature trees I find inexcusable and shocking given the fact that trees are good for the air quality and the environment and in the knowledge that McCarthy and Stone will be decimating all the trees that surround the car park by the Doctors for development. <p>A lit tarmac path through our Birch Woods, which you erroneously call Martlesham Woods, would be a disaster for walkers as some, not all, cyclists are selfish and ride fast and with no warning. I think it would be only a matter of time before there would be a collision between cyclist and pedestrian or dog.</p> <ol style="list-style-type: none"> 3. Can you GUARANTEE that cyclists, if this ill thought out strategy's goes ahead, would not take a short cut over the Site of Special Scientific Interest. <p>I think not.</p> <p>Can I suggest the if East Suffolk Council has money to squander on plans like this that they use their funds to repair all the many potholes in the roads.</p> <p>Thank you for your courtesy in reading my objection to this plan.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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IM12	Julian Page	211	No	<p>There is currently a perfectly good walking and cycling route across Martlesham Heath using the the existing cycle/footbridge.</p> <p>It would be inappropriate and unnecessary to destroy the natural environment of the Martlesham Heath woods by putting in a surfaced and lit route through the middle of it.</p> <p>The money could be far better spent putting in crossings at IF14, and also on the section of the A12 West of Brightwell where there are 3 rights of way crossings which are virtually impossible on foot in normal traffic conditions.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Julie Jewhurst	472	No	<p>I object to Martlesham Woods and its wildlife being disturbed further. We have already had to suffer a so called fitness trail being put through the wood last year, which has spoilt some of the naturalness of it. There is no need for the cycle route to go through the woods!! Eagle Way is a wide road that can be safely used by cyclists. There is no need to destroy trees, put down tarmac and put up lighting, which are all against new government policies to be greener and are detrimental to the environment. The wood is a natural, quiet place where nature and gentle recreation work together. Putting this unnecessary cycle path through will greatly diminish an already small wood and upset the wildlife even further. The wood is loved by Martlesham Heath residents and is an important part of this special residential</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				development and never intended to be chopped up for cycle paths or fitness trails.	
IM12	Juri Keyter	66	No	<p>I would like to object against all proposed changes related to the Martlesham Woods. The pathways through the Martlesham Woods are perfectly suitable for cycling and walking as it is now, but there are also ample alternate routes bypassing the Martlesham Woods for those who prefer to walk / ride on paved and lit routes. The Martlesham Woods is a woodland area, totally unsuitable for cycling in winter due to leaves covering the pathways, but perfect for natural recreation.</p> <p>Before this project is implemented, please can we see how many people will actually benefit from this (if any)? The current roads, paths and bridge over the A12 is easily accessible from anywhere in Martlesham Heath and I am not sure what this proposal is intended to solve.</p> <p>The lay-by on the A12 in this area is a far more troublesome issue and available funds should rather be spent on this before it is wasted on something that destroys and has no benefit.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Justine Lewis	806	No	<p>The addition of a new pedestrian/cycle bridge over the A12, a short distance from the existing one (route IM11) would cause significant traffic disruption for very little gain.</p> <p>The felling of at least 25 mature trees to build the path through Birch Woods (referred to in your document as Martlesham Woods) is environmentally disastrous, especially when this is in addition to the</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In</p>

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				<p>trees to be lost as part of the McCarthy and Stone development next to The Square. Martlesham Heath is sited next to the heavily used A12 and the woods help to offset the traffic pollution.</p> <p>There is a strong likelihood that cyclists would continue their journey using IM12 and take a short cut over the Site of Special Scientific Interest.</p> <p>Lit tarmac paths through established woodland would change the character of the woods. Urbanisation would discourage wildlife. The majority of residents who enjoy the woodland and heathland areas for dog walking and leisure, without having to drive, would instead have to drive to other heathlands such as Sutton Heath and Rendlesham - more cars on the road.</p>	<p>its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Justin Mayhew	405	No	<p>I find it incredible in 2022 that anyone could propose constructing a new tarmacked and lit surface through undeveloped woodland. The damage to the environment and ecosystem of the woods will be irreparable. I cannot see the benefit of this section of the cycle route, at most it would save 50m in travel distance to the new Brightwell development. The cost and carbon footprint of a cyclepath/footpath through the woodlands and a new bridge over the A12 would be significant. Surely the money would be better spent upgrading the existing bridge over the A12 (IM11) and improving the links to brightwell development via IM13. The safety of cyclists and walkers at night would also be a concern in Martlesham woods, compared to routes IM11 and 13 alongside existing highways.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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IM12	Justin Watts	115	Yes	<p>I write as a homeowner on Martlesham Heath, a cyclist, a car driver, and a dog owner.</p> <p>Eagle Way is not a safe route for cyclists. It might have been if people drove within the speed limit, and with proper care, but they don't. I have on several occasions nearly been hit by speeding motorists on Eagle way. I am a very experienced cyclist, and can anticipate stupidity and inconsideration by drivers but the fact is that many people are put off cycling by the dangers posed by bad driving. And many drivers drive badly</p> <p>If we are to move away from our society's addiction to the motorcar, we need proper provision that encourages people onto bicycles. IM 12 is exactly the kind of provision that is needed. I strongly support it.</p> <p>Of course any development needs to be sympathetic to the very important Martlesham woods. It needs to make proper provision that guards the safety of pedestrians. I am not sympathetic to the voices I have heard that oppose a cycle route on the basis that it would interfere with dog walkers. I walk my dog, and where it is not safe for the dogs be off the lead, my dogs on the lead. Other people ought to take the same view. If they do, the cycle path to post them no difficulty.</p> <p>There is plenty of provision nearby for walking a dog on the lead (and, for well-trained dogs, off the lead as well, on the heath). It is not obvious to me that any dog should be off the lead in the woods, where</p>	<p>Support noted. The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				there are many other wildlife considerations to take into account, in particular bird life.	
IM12	Justin Watts	1058	Yes	<p>I strongly support the provision of a cycle route on the route IM12 through Martlesham Woods. I say this as the owner of a house on Martlesham Heath, and a supporter of the need to stop our societal addiction to the motor car. I have been nearly hit by cars cycling round Eagle way on more than one occasion. If cars stuck to the speed limit on Eagle way, it would be a safe route for cyclists. But they do not, and provision that keep cyclists safer is of vital importance. There is an enormous need to make cycling easier and more accessible, and with the growth of the electric bike industry, more older people are likely to find cycling a convenient mode of transport. We should be doing all we can to encourage it.</p> <p>The Woodlands are, of course, important and any development must be sympathetic and have proper regard for the safety of pedestrians. I am a dog owner and therefore very conscious of the need for safe places to walk the dog. I am, however, not sympathetic to the voices I have heard that suggest that dog walking provides a reason not to permit this cycling development. It is incumbent upon dog owners, and their responsibility, to walk their dogs with regard to the safety of others. That is why I walk my dog on a lead except in areas that are wholly suitable for a different approach, which in the case of my dogs means a fenced secure area.</p>	Support noted. The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				Consequently I strongly support the development of IM12.	
IM12	Karen Gould	38	No	I am shocked that route IM12 is being proposed. This would pass through a long stretch of very well established woodland and involve cutting down trees and destroying established habitats. Currently these trees form an effective carbon sink in line with international moves to protect trees which help to offset global warming. There are other tarmac roads and paths nearby which could be utilised and/or improved to encompass a dedicated cycle path without destroying important habitats and trees which have been in place for well over 40 years.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Kate Startin	239	No	For local residents Martlesham wood / the Birch Wood is not primarily a thoroughfare but a precious and loved resource. It is used by local walkers and visitors alike as a recreational space. Children and families play in it. Older residents use it to take shorter but safe walks. It has helped to get many of us through the pandemic. The wood has established itself naturally over the last 40 odd years. I have seen a herd of deer clattering across Eagle Way from the woods to the thicket by Coopers Road, and I frequently see muntjac deer when I am walking. It can be a place of calm and peace. Over the years we have seen the trees thicken and grow, and modern science tells us it is a good carbon sink. The prospect of someone driving a cycling highway	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				through our glorious wood fills me with horror - and this, to save cyclists a few minutes on Eagle Way. It just seems so crass. It's not on any main route and the future residents of Brightwell Lakes don't need to come through Martlesham Heath to get to either Ipswich or Woodbridge. Please do not do this.	
IM12	Kathleen Barchus	70	No	<p>Birch Woods has a diverse habitat and is a site of natural beauty. It is used and enjoyed by a lot of pedestrians and to have a cycle path running through it would ruin the enjoyment of people enjoying the woods. There are already a lot of cycle paths linking Martlesham Heath with the surrounding areas.</p> <p>Cyclists already use the path through the woods so another one is not necessary and to add lamps would add to the light pollution.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Kathleen Brierley	360	No	<p>I would like to make my views known about the proposed draft cycling and walking strategy.</p> <p>I strongly object to the felling of trees & disturbing the vegetation of Martlesham (Birch) woods by constructing a new path through that area. I feel that part of the plan is completely unnecessary.</p> <p>I hope you will take residents objections in to account when considering this proposal.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide

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					a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Kathy Passmore	231	No	The proposal to build a cycle path through Martlesham Heath woods would destroy the natural woodland and involve the removal of many trees and bushes. Also, it would be a potential hazard and an accident waiting to happen. Young children and dogs frequently use these woods, and this would not be safe with cyclists whizzing through at speed. Eagle Way is a road which is far more suitable for cyclists as is the underpass and footbridge which crosses the A12. One further point, the money spent in building a cycle path which is not needed, would be far better spent elsewhere.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Katy Bell	658	No	I write to object to the proposed East Suffolk Council draft cycling and walking proposal. My reasons for strongly objecting includes the felling of trees to facilitate such a path through Birch Woods. This part of the proposal is entirely unnecessary. I cycle this route on a very regular basis in daylight hours with my family and feel that there is absolutely no need to tarmac and light this pathway. It would harm wildlife, fell trees, create light pollution and encourage people to 'hang about' in the woods after dark which is not socially acceptable. During the dark I currently opt to cycle around Eagle Way and can assure you that it is a quiet and safe journey as it currently stands and	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>probably adds a minute or two at most to the route. In fact the whole of Martlesham heath is very cycle friendly already; one of the many plus points to living in this area.</p> <p>It is very possible to promote cycling across the villages without a formal tarmac path. Once covered with leaves they are slippery anyhow and who would have right of way when it comes to cyclists vs dog walkers?</p> <p>Absolutely not required. We are an active village with a significant amount of cyclists (just look at the cycle sheds at kesgrave high school) all of these pupils have safely commuted from the villages for decades. The proposed route would add nothing other than the destruction of MHHL land, which I thought as a resident of Martlesham heath I was part owner. I therefore do not give consent for such a poorly thought out project, obviously designed by people that have no idea how villagers travel and have not sought to seek out opinions.</p>	
IM12	Kay and David Ransby	244	No	<p>We are opposed to the proposed new bridge over the A.12 linking the new planned Brightwell Lakes development with Martlesham Heath and the proposed tarmac route from Dobbs Lane along the back of Bloomfield and Whitfield to Eagle Way.</p> <p>There is sufficient footpath and cycle tracks linking Dobbs Lane and Martlesham Heath. If Brightwell Lakes are linked to Martlesham Heath it will create an almost unbroken route from there into the Heath, Kesgrave, Grange Farm., Rushmere and</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new</p>

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				<p>eventually Ipswich.</p> <p>The East side of Ipswich is becoming one vast urban sprawl between Ipswich and Woodbridge.</p> <p>We want to retain the spaces around our development and the nature of being a separate village.</p> <p>Tarmac routes will encourage motor cyclists, and boy racers to abuse the access. There is already a problem with this around Eagle Way, the industrial area and Tesco's car park.</p> <p>Much money has already been spent on accommodating cyclists with cycle lanes which are hardly used along the A.1214, Kesgrave-Ipswich Road and Colchester Road and Valley Road, Ipswich. There are more worthy projects deserving of this expenditure.</p>	<p>recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Ken Davies	689	No	<p>Whilst accepting that the Cycling and Walking strategy is the way forward and is being imposed by national government, I am dismayed that the proposals are so short sighted in relation to the destruction of Martlesham Woods (Birch Woods?) In the current rush towards all things 'green', carbon capture, tree planting etc etc, why are you considering destroying a minimum of 25 mature trees and surrounding vegetation, for the minimum gain to a small minority of cyclists.</p> <p>As I understand the maps, a new crossing over the A12 meets the east side of Eagle Way, (IM12) and</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and</p>

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				<p>then inexplicitly cuts through and destroys the woods before meeting the west side of Eagle way. It then continues north to meet with the existing crossing, IM11, before doubling back on itself to link with IM10.</p> <p>So is the route through the woods necessary when continuing north on the east side of Eagle Way would bring you to the same point:- is there a time saving? If so, it would be minimal and does not justify the destruction planned.</p> <p>I hardly dare mention the loss of habitat to our wildlife, the physical attraction of the woods to the neighbourhood, the loss of carbon capture, all to the benefit of the residents, whose voice needs to be heard. You will not find many residents, if any, in Martlesham Heath in favour of this proposal, please give it a serious review.</p>	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Kevin Moore	103	No	<p>I absolutely oppose IM12. Would I be correct in suggesting that the person(s) who have proposed this route do not live on Martlesham Heath? There is already an established cycle route running along the A12 towards and past BT, then running alongside the bowling alley and Fitness club, over the A12, through Martlesham Heath and on into Kesgrave. IM12 proposes, one assumes, another overpass and then through some beautiful and established woodland for no benefit to the residents here and such little gain for cyclists (I am an avid cyclist). If you are desperate to create another overpass then the cyclists can arrive on Eagle Way and cycle either left or right along Eagle Way to join</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and</p>

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				the existing cycle-ways, without destroying the woods here that are enjoyed by so many residents. A further beauty of these woods is the total lack of light pollution at night. One would assume that a cycle-way through the woods would require some lighting during the darker nights. Do not do this. I am absolutely opposed.	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Lawrence Clark	111	No	<p>The woods are an important and unique characteristic of Martlesham Heath that allows families to enjoy walks and children to spend time recreationally gaining valuable exercise, not to mention dog walking. To build a cycle park through the middle of this would change the nature of this crown jewel of the heath. The high speed cycling that it would facilitate would pose a risk to children and pedestrians and an end to this green space.</p> <p>Currently it is 0.7 miles to cycle around Eagle Way and it would reduce to 0.4 miles if you build a cycle path through the woods. The average speed of a commuter cyclist is 14.5 miles per hour meaning that there is only a saving of 74 seconds. Considering the negative externalities this seems like a poor return.</p> <p><i>The images/maps have not been published due to potential copyright/data protection concerns, but these were still fully considered and assessed in forming the Strategy.</i></p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Leigh Cunningham	280	No	As a Martlesham Householder I object to building a second pedestrian bridge over the A12 to connect Martlesham Heath with the new Brightwell Lakes development as we already have a pedestrian bridge in place connecting Martlesham Heath at Eagle Way	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close

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				<p>which ends by Martlesham Leisure.</p> <p>Furthermore, I understand that you are proposing to create a path that cuts through the woods between Coopers Road and Lancaster Drive, which would then continue through Birch Woods (not Martlesham Woods) and this would require the felling of mature trees. Cyclists and pedestrians would then be tempted to take a short cut where the path exits near Forest Lane on Eagle Way and traverse the Martlesham Heath Site of Special Scientific Interest (SSSI) on their way to Dobbs Lane. I also object to this.</p> <p>Martlesham Heath is an attractive area to live, with beautiful woods and ample existing areas to cycle and walk freely. The land is owned and managed on behalf of its members by MHHL. The members are the people who live here who will be affected by East Suffolk Council's proposals which involve removing part of our woods, building a totally unnecessary bridge and spoiling our Heath by adding a total of 1 mile of tarmac cycle/footpaths on our land and I trust our comments will be taken into serious consideration during this consultation process.</p>	<p>proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Lesley Vince	327	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the</p>

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IM12	Lesley Vince	330	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch</p>

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				<p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this area are minimal in comparison to the loss of habitat/habitat changes.</p>	walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Lesley Vince	336	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this area are minimal in comparison to the loss of habitat/habitat changes.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Lesley Vince	342	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new

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IM12	Lesley Vince	349	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this area are minimal in comparison to the loss of habitat/habitat changes.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Linda Cowell	686	No	<p>I am writing to you to express my views on the above strategy with particular refence to the proposal to instal a lit, tarmac route thro Birch Woods - referred to as Martlesham Woods on your Plan.</p> <p>This woods is extremely well used by walkers and has an abundance of wild life , especially bird life. Each summer the Martlesham Heath Conservation Group conduct a survey of the bird songs heard. Even nightingales can be heard here in spring time when they are singing for a mate. The felling of at</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>least 25 mature trees, construction of a cycle path and the traffic it would bring would be extremely disruptive and it being lit during darkness could well be disastrous for wild life.</p> <p>The other concern about a cycle path thro Martlesham woods is where it is proposed to exit onto Eagle Way. This is almost opposite a footpath leading onto the Heath. This part of the Heath is a Site of Special Scientific Interest and our Conservation Work Party works hard to maintain the conditions demanded by English Nature for this catagorisation. The temptation may be for cyclists exiting this cyclepath onto Eagle Way to cross the road and use the footpath - cycling across the SSSI to get to Dobbs Lane.</p>	
IM12	Linda Ledgerwood	125	No	<p>I do not support the proposed route through birch woods and portal woods. These woods are used by us all for walking and for children playing. There are lots of dear and wildlife that live in these woods. If a 4.5 metre lit cycle path was put through the woods it would have a detrimental impact on the wildlife. It would also be dangerous for young children playing and the older generation walking if cyclists who can cycle up to high speeds are whizzing through these woods. This is a well maintained natural woodland which has recently had native bluebells planted as well. In this day and age with climate change so high on the agenda any loss of trees and natural vegetation will impact more. The existing cycle path from the industrial estate over the main road through the roads of Martlesham Heath gives a</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				completely viable route to Kesgrave without endangering anyone or any wildlife.	
IM12	Linda Ledgerwood	250		The south option will run along behind all the houses bordering the farmers fields. This would be another cycle tract that would have to be maintained when the cycle routes through Martlesham are already there and give a direct route to Kesgrave. Existing routes should be maintained rather than destroying natural woodlands or farms which are enjoyed by all the residents of Martlesham. Stop building outside of existing planning areas and maintain existing cycle tracts. If this is to support cyclists from BT then it is definitely not needed as they all cycle down the main roads as they do not use the cycle paths.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Lisa Calver	680		My comments on the Ipswich to Melton Corridor Proposal The purpose of your strategy is to. <i>...’identify potential cycling and walking infrastructure opportunities across the district. The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure. It provides context and information to support detailed infrastructure proposals and inform decision making to support cycling, walking, and equestrian use’.</i> You are focusing on new roads and infrastructure whilst ignoring existing routes near to those you are	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>proposing; namely the IM12 crossing with IM11. National Cycle Route 1 is already in existence using crossing IM11, but its route is not mentioned in the ESC draft strategy?</p> <p>The consented Brightwell Lakes development is clear that the existing bridge over the A12 should be used as the only pedestrian and cycle connection to Martlesham Heath Village with a Pegasus crossing further south to connect to an existing Bridlepath running along the south edge of the asparagus field, shown on your map below as Prow6. (see attached)</p> <p>You do also show on the map a Bridleway veering north from the same crossing leading nowhere? (Red circle) This does not exist. The actual bridleway on the map crosses through the middle of an Asparagus field. The route that is used and accepted runs further south along the wooded copse as shown on the picture below. (see attached)</p> <p>The north edge of this field backs onto residential properties. The field sits a metre higher than most of the properties so any 'unintentional' depositing of people to this location leading to an informal creation of a pathway along our boundaries is unacceptable. Any users will be overlooking our properties which have enjoyed uninterrupted views across the field since 1986.</p> <p>If IM12 is considered essential to the network at this location 'to make the A12 less of a barrier to people walking and cycling, and to enable more journeys to</p>	

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				<p>be made on foot or by cycle in future’ (Suffolk.gov.uk/a12 improvements) Those same proposals did state ‘underpasses’ were an option. This option is more palatable to me than another bridge as it is less of an eye sore, poses less of a risk to drivers by way of distractions or threatening behaviour from above. Underpasses are more user friendly to both people and wildlife. It will also be less impactful on the existing residents on Martlesham Heath.</p> <p>The proposal to put IM12 through Birch Woods, ironically announced on 31st October, Day 1 of COP26, is incomprehensible given the National & Local political emphasis on protecting the environment. Mature trees, lower canopy plants and wildlife habitats will be damaged or destroyed in the process to install essentially a cut through when the route around Eagle Way is a mile longer! You propose to light it which will certainly affect the local bat, owl and bird populations and add to the light pollution reducing our local enjoyment of the night sky.</p> <p>It is also naive if the council think cyclists would exit the proposed IM12 path on the west side of Eagle way and not continue straight across onto the SSSI as a shortcut to Dobbs Lane. Marked on the above map in Blue, when again the existing National Cycle network is only meters away!</p> <p>Since the Bus Route 66A was reduced to 1 an hour Eagle Way is already being used as the quiet cycle</p>	

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				network around the village. It is so quiet the school children aged 10/11years old from Birch Woods Primary do their Cycle Proficiency on it! Neither parents or teachers would permit that if it were considered unsafe to do so.	
IM12	L Miles	626	No	<p>Having considered the proposed plan I am dismayed to discover that your proposal includes plans to install a new paved and lit foot/cycle bridge from the new Brightwell Lakes development onto Martlesham Heath between Coopers Road & Lancaster Drive. The wooded land between these two roads is privately owned by MHHL and is something of a wildlife sanctuary, often deers & foxes can be seen in this area and the woodland opposite.</p> <p>This would cut directly through 'Birch Woods', which in your documents you refer to as Martlesham Woods. The woods are a central part of living on the Heath and I am extremely concerned at your strategy to destroy the natural environment for us residents.</p> <p>The nature and habitat of wildlife would be detrimentally affected by this proposal, apart from the fact that it is a well-used wood by local dog walkers, photographers, residents and community groups such as scouts, local nurseries and school groups. A new paved pathway would increase activity in this area resulting in raised noise as well as the intrusion of lighting to shatter the natural ambiance of the 90% unmanaged woods as they currently are.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>The woods also have the added benefit of being very effective on an ecological viewpoint by providing a carbon sink which is helping to keep CO2 at a manageable level in the atmosphere. Why destroy this to construct an unnecessary tarmac path through woodland?</p> <p>There is already an existing foot/cycle bridge across the A12. This could be widened and lit if that is the proposal as it is also near existing stores and not creating another cut through separating the village of Martlesham Heath. The existing bridge has the added benefit of routing people towards all of the retail units hence reducing traffic chaos on the roads and also increasing the number of people who would be going past the village shops on the Square which would have a beneficial effect on their businesses.</p> <p>If as a previous consultation proposed, the construction of a new footbridge south of the Brightwell Lakes development across farmland and around the outside edge of Martlesham Heath village seems not to be visible on this strategy plan. This would provide a direct link towards Dobbs Lane and would prevent cutting our village up across its heart and provide a safer and quicker route to Kesgrave and onward towards Ipswich. The narrow paths linking the closes and open spaces on Martlesham Heath are not sufficiently wide enough to accommodate your proposals. There is also a proposal for a newly</p>	

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				<p>constructed lit tarmac route from Dobbs Lane along the back of Whinfield/Broomfield leading to Eagle Way - this would impact on land which is SSSI protected.</p> <p>Another alternative route would be to construct the new footbridge nearer to Welham's Plantation with an ongoing path leading more directly to Dobbs Lane</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
IM12	M A Everitt	716	No	<p>I strongly object to the proposed tarmac cycle route through woodland owned by the residents of Martlesham Heath. Birch Woods (nothing to do with Martlesham) is a much loved resource and adds value to all our properties.</p> <p>It hard to believe that felling 25 mature trees and illuminating the woods will not have a significant impact on the resident wildlife.</p> <p>This natural resource has been much appreciated during the pandemic and should not be sacrificed to provide a short cut for residents of the yet to be developed BT housing estate. IM11 would provide a safe route for them without destroying our woods.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Margaret Hackett	750	No	<p>Re the Martlesham wood path,I think it would be wrong.Eagle way is a wide road and a cycle path could go along the side of the road.It is a lovely</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and</p>

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				quiet walk in that wood and children have freedom to walk in a little bit of nature.	walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Margaretha Field	724	No	We have enough footpaths and cycle lanes around our area. Can't see the point, does seem a same to cut down trees and giving more light pollution for the sake of apparently 1 mile.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Margaret Wilson	696	No	<p>Comments on IM12 Martlesham Heath – Brightwell Lakes.</p> <p>Route across Birch Woods (Martlesham Woods)</p> <ul style="list-style-type: none"> • From the perspective of a Martlesham Heath resident and member of Martlesham Heath Householders Ltd. (MHHL), Eagle Way should continue to provide the cycle route, with Birch Woods remaining undamaged. • This proposal directly impacts private land at Martlesham Heath. There has been no attempt to 	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new

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				<p>notify those who own and use the land or live along the proposed route, or to consult with local groups. I would make wider comments had there been more time, but I will restrict these comments to the immediate area - one that I have walked and cycled around for over 30 years.</p> <ul style="list-style-type: none"> • This suggested IM12 route is not strategic; it is pointless, costly and destructive. <ul style="list-style-type: none"> o It will not save any car journeys. o It does not facilitate cycling; it is of similar distance as Eagle Way. o It will compromise the enjoyment of walks in the natural woodland enjoyed by many local residents and families. o It involves the destruction of many trees. o The woodlands at Martlesham Heath are privately owned land that is managed locally. These woodland paths should remain natural in surface and layout. A straightened, tarmac or floodlit commuter route through Birch Woods is not appropriate, nor wanted. o A new route opens up the Birch Woods, to the use of the other paths by cycles or even mopeds or other vehicles. This will damage the woodland further. This is to the detriment of those out walking; currently they can do so without the fear of negotiating traffic. Do not underestimate the importance of being able to walk through the local natural woodland environment undisturbed on people’s health. Do not prioritise an unnecessary route for speeding Strava participants over leaving the woods as is. Leave it for those who want to continue to enjoy 	<p>recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>their local walks without having to drive to a quiet destination.</p> <ul style="list-style-type: none"> o Pedestrian routes have already suffered in preference to cycling. Over towards the Industrial Estate, pavements have been re-marked as cycle routes. You can be forced off the path or cut up at the crossings by cyclists. o When reaching the Forest Lane side of this route there will be degradation of the SSSI opposite, as people form other paths. You are setting a precedent of taking this ‘shortcut’ through private or protected land. o With the growth of housing in the area, it may needlessly lead thousands of passers-by into the middle of a residential estate, Birch Woods and the SSSI rather than taking the current route past the village centre. <p>• There is already a bridge across to Martlesham Heath near the village centre, which is the most appropriate location, and a cycle route:</p> <ul style="list-style-type: none"> o A new 3m cycle way is already proposed as part of the development of the new flats at Eagle Way. This continues to take people past the village facilities and shops. Alternate routes risk bypassing the village shops and businesses, which is not an advisable strategy. The Retail Park is also to the North, where there are additional crossing and subways. There are no amenities in the south so no crossing is required there. o A cycle route across Birch Woods suggests the use of Martlesham Heath merely as a future rat run for the new estate. 	

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				<p>o A pedestrian route along the Adastral side of the A12 (IM13) currently provides the strategic main route along the Adastral - Foxhall Roundabout areas. Planning should ensure more direct routes from the new housing developments to other main destinations such as the supermarkets Aldi and Tesco areas and Retail Estate without damaging Birch Woods or the SSSI.</p> <p>o For routes to the south, there are crossings south of the current bridleway, cut in two by the A12, that could be improved to provide a joined up route towards the new country park recreational area, proposed for land south of Foxhall Road, or towards Ipswich - this does not endanger the SSSI, Birch Woods or residential property and gardens.</p> <p>o The private woodland separating the hamlets at Coopers and Lancaster are part of the original village design and should not be damaged. They provide a wildlife haven, as does Birch woods.</p> <p>• Maintenance: Funds need to be directed to strategic routes and maintenance of the current well used cycle routes and pedestrian paths around the village rather than creation of new hard paths through woodland/side routes.</p> <p>o What are the plans for maintenance and repair and replacement over its lifetime so that it does not become a liability and increasing eyesore? Are lifetime costs considered when new paths with associated signs and lights and bins are added?</p> <p>o Who will cover the cost of inevitable litter, associated insurance, and any vandalism?</p> <p>o Who will cover damage adjacent to path and</p>	

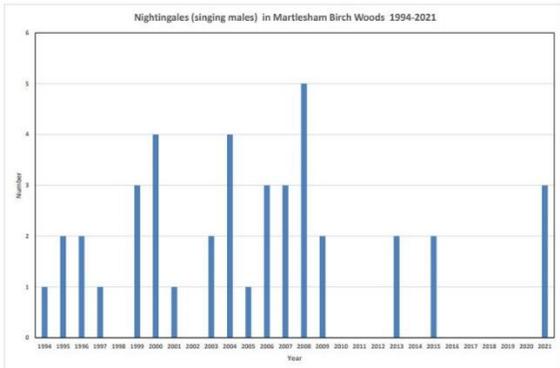
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				beyond? The woodlands and green spaces have suffered from increased use these past couple of years and some areas need to recover- turning it into a through route will be even more damaging. o What additional costs will land on the residents?	
IM12	Maria Dobbie	378	No	My husband and I think it would be a waste of money to build a new bridge over the A12, as there is already a route to the existing bridge which is only slightly longer. In addition, Martlesham woods are relatively small and well used by walkers. Cyclists are often inconsiderate of walkers and this makes the area less attractive particularly to the elderly walkers. The removal of mature trees required for a new wider path, would also make the woods less desirable in a time when we need more trees.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Marie Finbow	180	No	My husband and I strongly oppose this proposed scheme, which seeks to destroy large areas of existing established woodland, to make way for cyclists and walkers. These woodlands consists of a significant number of long-established silver birch trees. Each silver birch is capable of absorbing up to 3100 kilos of CO2 to clean the air. However, cutting down CO2 absorbing trees to make way for cycling and walking facilities will release extra carbon and greenhouse gasses into the atmosphere. When these trees decompose, they emit even more CO2! By removing woodland, we effectively remove the natural systems that already absorb and store	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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				<p>carbon.</p> <p>If the council is serious about reducing carbon emissions, it should not be looking to destroy established woodland areas. All the residents of Martlesham Heath financially contribute to these woodlands being maintained through the Martlesham Heath Householders Ltd - a scheme which is meant to protect our woodlands and green spaces from development.</p>	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Mark Kendall	699	No	<p>Please could you record my objection to the above proposals.</p> <p>Specifically, I am unhappy with the proposed path through Martlesham Woods - a charming patch of woodland and a much loved local amenity.</p> <p>I would understand the proposed route better if there were no alternatives. But cyclists and walkers already have the option of using the well lit and quiet Eagle Way to the south of the woods; or there is an existing cycle route through the centre of the village - possibly bringing additional revenue to the businesses there.</p> <p>The proposed path would offer very little if any time saving over these alternatives but would incur additional cost and would lead to the destruction of a part of the woodland - at a time when everyone else seems to be embracing re-wilding.</p> <p>I hope I've made my feelings clear and would be</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				happy to be contacted to discuss this proposal further.	
IM12	Martlesham Conservation Group (Phil Smith)	894	No	<p>IM12</p> <p>Martlesham Conservation Group is completely opposed to this route and ask that the Council withdraw this route completely.</p> <p>a) The route runs through the Martlesham Birch woods and would lead to a direct loss of habitat as mature trees would have to be felled and a hard surface path with streetlights constructed. The site is a County Wildlife site and so should have been offered a high level of protection from such developments.</p> <p>The woodland contains a wide range of wildlife: birds such as Nightingales, reptiles such as Viviparous Lizards, Slow Worms, Adders and Grass Snakes, and insects such as Glow Worms, have been observed or are likely to be present. The proposed path development will have a detrimental impact on the wildlife both directly due to habitat destruction and also through the lighting from the proposed cycleway.</p> <p>Policy SCLP10.1 states “Proposals that will have a direct or indirect adverse impact (alone or in-combination with other plans or projects) on locally designated sites of biodiversity or geodiversity importance, including County Wildlife Sites, priority habitats and species, will not be supported unless it can be demonstrated with comprehensive evidence</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>that the benefits of the proposal, in its particular location, outweighs the biodiversity loss.”</p> <p>The woodland is used for informal recreational activity. The encouragement of fast moving cycles through this area will seriously detract from this activity and make it more difficult for local residents and groups such as Cubs, Scouts, Brownies and Guides to make safe use of the area - another good reason why this route should be withdrawn.</p> <p>b) The route will lead to a degradation of the wildlife on the Martlesham Heath (Ipswich Heaths) SSSI.</p> <p>Users of route IM12 on Martlesham Heath travelling westwards through the Martlesham Woods path will be far more likely to carry on in a straight line after reaching Eagle Way and take a short cut across the SSSI towards IM4 on Dobbs Lane rather than turning right onto Eagle Way. The increased recreational pressure on this already well-used and fragile area will lead to a further degradation of the wildlife on the SSSI. The Martlesham Heath SSSI is one of the few remaining examples of lowland heath in the UK. This habitat is rare not only nationally but worldwide, as the UK is one of the few countries in the world with lowland heath. The Martlesham Heath SSSI is noted for a range of species, especially the Silver Studded Blue butterfly, a nationally rare species. It is also noted for its population of Nightingales, rare insects, reptiles (Adders, Viviparous Lizards, Slow Worms, Grass Snakes), and has also recorded very rare species such as the</p>	

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				<p>Natterjack toad. Over 30 species of butterfly have been recorded locally.</p> <p>We again draw the attention of the Council to Policy SCLP10.1.</p> <p>c) IM12 will also lead to the destruction of a natural wildlife corridor that was put in place during the planning of Martlesham Heath to separate the hamlets of Coopers Road and Lancaster Drive. It also acts as a barrier (for sound and pollution) from the adjacent busy A12. The wildlife present in this area is similar to the Martlesham Birch Woods as it forms an extension of the woodland across Eagle Way. The area is also a registered village green.</p> <p>d) The proposed new bridge across the A12 for IM12 is not necessary as there is an existing bridge on route IM11. The new bridge is an unnecessary extra expense. It offers little benefit as travellers to/from Brightwell Lakes could follow IM13 and IM11. There would be degradation of the ecology in the vicinity of the bridge as well as increased sound and air pollution.</p> <p>e) The whole of route IM12 is unnecessary as more viable alternatives are proposed via IM11 and IM13 that provide better use of existing infrastructure and therefore would be less ecologically damaging and of lower cost.</p>	

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				 <table border="1"> <caption>Nightingales (singing males) in Martlesham Birch Woods 1994-2021</caption> <thead> <tr> <th>Year</th> <th>Number</th> </tr> </thead> <tbody> <tr><td>1994</td><td>1</td></tr> <tr><td>1995</td><td>2</td></tr> <tr><td>1996</td><td>2</td></tr> <tr><td>1997</td><td>1</td></tr> <tr><td>1998</td><td>1</td></tr> <tr><td>1999</td><td>3</td></tr> <tr><td>2000</td><td>4</td></tr> <tr><td>2001</td><td>1</td></tr> <tr><td>2002</td><td>1</td></tr> <tr><td>2003</td><td>2</td></tr> <tr><td>2004</td><td>4</td></tr> <tr><td>2005</td><td>1</td></tr> <tr><td>2006</td><td>3</td></tr> <tr><td>2007</td><td>3</td></tr> <tr><td>2008</td><td>3</td></tr> <tr><td>2009</td><td>5</td></tr> <tr><td>2010</td><td>2</td></tr> <tr><td>2011</td><td>0</td></tr> <tr><td>2012</td><td>0</td></tr> <tr><td>2013</td><td>2</td></tr> <tr><td>2014</td><td>0</td></tr> <tr><td>2015</td><td>2</td></tr> <tr><td>2016</td><td>0</td></tr> <tr><td>2017</td><td>0</td></tr> <tr><td>2018</td><td>0</td></tr> <tr><td>2019</td><td>0</td></tr> <tr><td>2020</td><td>0</td></tr> <tr><td>2021</td><td>3</td></tr> </tbody> </table>	Year	Number	1994	1	1995	2	1996	2	1997	1	1998	1	1999	3	2000	4	2001	1	2002	1	2003	2	2004	4	2005	1	2006	3	2007	3	2008	3	2009	5	2010	2	2011	0	2012	0	2013	2	2014	0	2015	2	2016	0	2017	0	2018	0	2019	0	2020	0	2021	3	
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IM12	Martlesham Heath Householders Ltd (Sir/Madam)	289	No	<p>IM12</p> <p>23. Unfortunately all of the Directors were personally dismayed by the proposal to put IM12 through 2 of our woodlands: Birch Woods (shown as Martlesham Woods in the draft strategy) and the woods between Coopers Road and Lancaster Drive. We find this proposal quite bizarre and incomprehensible given the emphasis on protecting the environment. The Board totally rejects this proposal together with the siting of a new bridge across the A12. The IM12 route has been walked and approximately 25 mature trees would need to be felled. Damage would also be done by contractors' vehicles and plant.</p> <p>24. Notwithstanding our objections of driving a tarmac path through the woods, it is the Board's view that there is insufficient width of Council verge to put a cycle route northwards along the west side of Eagle Way.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>																																																										

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IM12	Martlesham Parish Council (Diane Linsley)	884		<p>This is a contentious element of this consultation. The proposed route, including the bridge landing area, would cross land which is part of MHHL's holding on behalf its shareholder residents, and which is maintained at the residents' expense. The woods act as an area of comparative tranquility for a</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For</p>

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				<p>community which has come under increasing pressure from development in the surrounding area - first from the west by the construction of Grange Farm, and now from the north end east by development of up to 2300 homes. It played a particularly important role during lock down.</p> <p>Construction of the bridge itself appears likely to result in the destruction of around half of the woodland between Lancaster Drive and Coopers Road. That land, which is home to deer, bats and other fauna, acts as a natural visual barrier from the noise and pollution from the A12. As it is impassable on foot, it also provides a level of security, preventing ingress from the A12. It has six gardens backing directly onto it. The land is at grade with or in some parts lower than the A12 carriageway, so to clear the carriageway, the construction of the abutment and the necessary ramp will create a dominant feature which would impact on the amenity of the adjoining houses.</p> <p>The current narrow path east <> west through Birch Woods is unsurfaced, and meanders through informal wildlife friendly woodland managed on the principle of natural regeneration. To create a straightened route, say 5 m wide, would potentially involve the loss of many trees and transform the character of the area, especially if illuminated.</p> <p>In summary, our considerations in opposing the construction of a bridge in this particular</p>	<p>these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>location, and a surfaced path through Birch Woods are based on the following:-</p> <ol style="list-style-type: none"> 1. It is not necessary - see comments elsewhere and our strategic view set out in our full comments. 2. Potential extensive loss of woodland and impact on remaining woodland. 3. It is strongly opposed by the community which in effect owns it. 4. Its environmental impact is contrary to our declared climate emergency and many initiatives which that has spawned (tree planting, wilding, wildlife corridors etc) 5. Due to the secluded nature of the woodland there is an increased risk of ASB - the major concern being fire-starting in woodland which abuts the rear gardens of about 44 houses. 6. Increased fear of crime. 7. Concern that people trying to get from IM12 at its junction with Eagle Way onto IM4 would not follow the route shown in the consultation. It is a near certainty that desire routes would become established across the SSSI to reach IM4, and lots of bikes would result in further damage. 8. There is insufficient land for an off road route for IM12 up the west side of Eagle Way, and cycling on-road at school times would be problematical as this is where parents park for drop off/collect at Birchwood School which gets very busy in that area. 9. Increased costs falling on MHHL and hence the residents. <p>With IM11/13/14 and a widened existing footbridge</p>	

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				<p>in place, IM12 is not necessary for local traffic within Martlesham. It is also unsuitable as part of a commuting route to BL, Adastral Park and the industrial/retail area as the links to IM 4/7/5 are, in places, tortuous through suburban roads and pathways which were not designed from the outset for high levels of through traffic, with side roads, and conflicts with school and local traffic.</p> <p>We note the response to submission 278 (page 191 of the PDF) which says:-</p> <p><i>Connectivity and Growth - The A12 is a significant barrier creating a wall between the residential areas to the west and the services and employment opportunities to the east. However, without teaming a second bridge that's located to the south with an onward route that cuts through the Martlesham Heath woodland up to north west Eagle Way, the onward travel gain of using the southern bridge rather than the existing A12 foot/pedal bridge is lost; the cyclist/pedestrian journey time (and energy) cost to reach the Broomfields shared paths for onward access to Longstrops Bridleway (which is set to be the 'keystone' of the strategic route between Brightwell Lakes and Ipswich) is higher than simply using the existing A12 foot/pedal bridge. <u>The difference is not huge, though.</u> With high levels of modal shift, a second bridge - regardless of the advantage lost - would be worth installing just to manage the pedestrian/cyclist flows, which would largely be coming from the south post-delivery. <u>As suggested by the respondent, replacing the existing</u></i></p>	

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				<p><u>bridge with a higher-capacity (wider)and more accessible (less steep) bridge may achieve similar benefits, though.</u></p> <p>We believe that the final sentence underlined above is true, and that the cost and environmental impact of the IM12 is not justified.</p> <p>We believe that the focus should be on a more direct route to connect to IM7 further west, utilising in part the existing bridleway way (see comment IDs 597 529 and 687) with the aim of providing a faster route into Ipswich without going all round the houses.</p> <p>We do not understand the reference in the previous sentence to “...flows which would be largely coming from the south post-delivery”. What flows are these? Traffic from the eastern part of the BL development heading for the Retail Park and Martlesham Heath would find the IM14/1M11 route more convenient. In addition, a crossing picking up the bridleway with an EXPRESS ROUTE toward Ipswich would divert traffic from BT and Brightwell Lakes away from using the existing bridge in favour of a more direct route as proposed. Thus, the case for a second bridge to manage extra flow is substantially weakened.</p> <p>There is already a planned bridleway crossing of the A12 where the BL Boulevard connects. In line with NP policy, MAR 13, MPC renews its request that the opportunity should be taken to create an EXPRESS cycle route using the bridleways north of Welham</p>	

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				<p>Plantation, which could then connect to a short cycleway along the north side of Foxhall Road, which could in turn connect to the network of PROWs near the speedway stadium to connect to IM4. This will give a fast, convenient link to and from Ipswich for BL, Adastral Park, and the commercial retail areas on the east side of the A12.</p> <p><i>The images/maps have not been published due to potential copyright/data protection concerns, but these were still fully considered and assessed in forming the Strategy.</i></p>	
IM12	Martlesham Parish Council (Paul Whitby)	700		<p>In response to the proposed Cycling and Walking strategy:</p> <ol style="list-style-type: none"> 1. I have no objection to an additional crossing over the A12, but I would strongly favour a tunnel which is also designed as a wildlife tunnel, in line with the East Suffolk rewilding initiative, which seeks to create a wildlife corridor from Rushmere St. Andrew out to the Deben. These things need to be joined up (both the green spaces and the thinking). 2. I object to felling ANY trees in the Birch Woods [IM12] (shown as Martlesham Woods on your map). If a track across the south of the Village Green is required, then take the track around the edge of the trees at the south of the green. 3. Instead of a path through the trees or across the green [IM12], what is wrong with taking the south route around Eagle Way once the path from the tunnel meets the road? 	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>4. Make Eagle Way 20mph - this will make the road safer for everyone, particularly cyclists [IM12].</p> <p>5. I object to lighting on the perimeter track through Portal Woods [IM9]. Cyclists should be encouraged to have lights on their bikes.</p>	
IM12	Martlesham Sea Wall Group (Thomas O'Brien)	875		<p>3. I agree a foot crossing of the A12 is necessary here. Brightwell Lakes is planned to have its own schools and shops and other amenities which Martlesham Heath residents would enjoy also. A method of easily walking across the A12, especially if a secondary school is built, would be frequently used. Also I am in favour of a made up path across the Birch Woods. But I can understand the annoyance local residents feel about the construction of a <u>lit</u> cycle way across the woods. I walk there often. I enjoy listening to the chiff chaffs, nightingales, and I've heard young sparrow hawks calling from their nest. A lit path would certainly be an intrusion. Also these woods are definitely a dog hot spot so the inclusion of racing cyclists doesn't mix and is unsafe.</p>	<p>While support for the previously proposed cycling and walking bridge is welcomed, the importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Martyn & Sue Cook	684	No	<p>Please include my reply as a definite "NO" to the above strategy. To continue please read on.</p> <p>Pardon me but I thought the idea of conservation and to our continued life on our planet was to plant more trees and not cut them down as would be the case of creating this totally unnecessary cycle and walk way particularly through Birch Woods Martlesham Heath.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and</p>

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				<p>As a regular user of the footpath network on Martlesham Common I am appalled that the idea has even been considered. Why! I hear you ask. You take your life in your hands when taking to those walkways. Cycles both singular and in groups career along them forcing the walker to jump out of the way lest they be mown down.</p> <p>Just because our government in it's ignorance decided to waste money on this strategy does not mean that local authorities should take up the idea. There are far more necessary causes that require monetary support in these troubled times.</p> <p>Our government should have known better in these times of hardship to waste money on this ludicrous and totally unnecessary plan.</p>	walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Martyn and Jacqueline Reed	746	No	<p>This email relates specifically to East Suffolk Council Draft Cycling and Walking Strategy. The plan will significantly impact on Martlesham Heath Householder Limited (MHHL) land, i.e. Birch Woods (sometimes referred to by Suffolk Council as Martlesham Woods). Birch Wood and other areas is land that is held by Share by each and every resident freeholder of MH; not common land, etc.,</p> <p>The plan aims to connect the new Brightwell Lakes development with a lit and Tarmac surfaced foot/cycle path, that path cutting through existing residential/wooded areas, across Birch Woods then exiting onto Eagle Way between Forest Lane and Warren Lane.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>As a long term residents of MH, 36 years, my wife and I want the unique environments and habitats we have in MH to be maintained; a development that is precious to humans and animals, one that has been carefully appreciated, managed, protected and maintained by its residents, by MHHL and its predecessor Martlesham Consultants, as laid out in the original MH development plan.</p> <p>Of particular concern are the potential plans for Birch Woods, a natural and largely unspoilt Wood where we can roam freely, where the precious animal life of the Wood co-exists without fear from any hussle and bussle within and from outside that area; the same must surely apply to the Western Corridor SSSI. Adding a Tarmac, lit foot/cycle path into these sensitive enviroments is completely unnecessary given that there are more than adequate foot/cycle access points via existing footpaths and a foot bridge adjacent to Adastral Park.</p> <p>The current and future planned development of the MH commercial and surrounding area is already causing air polution and significant traffic congestion, this can only get worse with the planned Brightwell Lakes build; one can only imagine the associated disruptions during construction of Brightwell Lakes over a number of years, perhaps a decade or more. Please leave Birch Woods and the Western Corridor alone.</p> <p>My wife and I strongly object to the East Suffolk</p>	

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				Council Draft Cycling and Walking Strategy - Martlesham Heath (MH)	
IM12	Martyn Shakespeare	496	No	Strongly disagree. The path through "Martlesham Woods" is a well-used walking route. Upgrading this to a cycle route is unnecessary, and will significantly damage the tranquility and feel of the woods, especially if lit. Also significant danger of cyclists continuing across SSSI to join IM4 which will cause additional damage. Not convinced that a new bridge over A12 is required just here, and damage to woodland.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Mary Gooding	92	No	I am objecting to the IM12 part of the cycling strategy due to the impact it will cause through Birch Wood. There is enough very good cycling paths between MARTLESHAM and Ipswich without having to go through a beautiful wood. It will destroy the wildlife and it will be dangerous for members of the public who enjoy just walking through this lovely area. I thought we were supposed to be looking after the environment	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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IM12	Mary Gooding	100	No	I would like to strongly object to the cycle path through a beautiful wood full of mature trees and wildlife. Why would the county council want to vandalise this area when there are already existing cycle paths which have been in existence for years all the way from martlesham industrial estate through to Ipswich. I thought we were supposed to be protecting the environment not the opposite	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Mary Odam	862	No	<p>“Martlesham Woods” are much used by families, dog-walkers, local Scout and Guide groups and people simply going about their normal business within the village. To place a “cycle highway” (IM12) through them with cyclists likely to be travelling at speed is likely to result in collisions and injury to all parties. The time of most risk will be in the morning rush-hour when children are walking to school and commuters are hastening to work.</p> <p>Many of the paths in “Martlesham Woods” have been opened up in the past few years, I believe to be more friendly to wildlife, in particular bats – of which there are many. The addition of lighting within the woods will deter these animals and the resident owls. We need fewer lights, not more.</p> <p>There are many muntjac living in and about the</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>woods. Anyone cycling through the woods as speed, particularly in the hours of darkness, is at risk of a collision with one of these.</p> <p>I understand that there are to be 25 mature trees felled to facilitate these “improvements”. We all understand the importance of trees in the combat against greenhouse gases. It cannot be responsible to fell healthy trees when there are reasonable alternatives (i.e. the local roads and existing cycleways).</p> <p>In particular I see little to be gained in building an extra bridge across the A12 (IM12) and carving a cycleway through the woods over improving the route across the existing bridge and through the centre of the village on existing roads (IM11) – all for a saving of some 500 yards.</p>	
IM12	Mary Trowse	701		<p>While a second crossing over the A12 might be useful, I think that the problems with route IM12 outweigh any benefits:</p> <ul style="list-style-type: none"> • a wide tarmac segregated cycling and walking path through Martlesham Woods(aka Birch Woods) would be very detrimental to the wildlife and biodiversity, particularly if it has lighting • Many cyclists following route IM12 to/ from Kesgrave would take a shortcut over the Martlesham Heath SSSI whether or not there is an official path. This would be extremely detrimental to wildlife and biodiversity in such a sensitive area 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<ul style="list-style-type: none"> People going between Brightwell Lakes and Martlesham Heath could use the existing bridge, especially if IM11 and IM13 are improved 	
IM12	Matthew Simpson	110	No	<p>The route proposed for IM12 passes straight through Birch Woods. This untouched woodland is home to a wide variety of animals and owned by Martlesham Heath Householders limited.</p> <p>The proposed lit cycle path would not only have a detrimental effect on the wildlife but the disturbance to the surrounding area would be irreversible. The current footpaths through the woods are used by a large number of local families and dog walkers as they provide a safe pedestrian route. The introduction of a cycle path with increased cycle traffic will stop this route from being a safe place for children to roam and dogs to walk off lead.</p> <p>Having grown up on the Heath and now a resident with a young family there is no need for a cycle path to cut across the woodland when a safe route already exists - Eagle way! The road is quiet and wide enough for cyclists with the footpath set away from the carriageway perfect for pedestrians. Both of the above are used daily by myself and my young family. A cut through via Birch Woods is completely unnecessary.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				Birch woods is a huge asset to the area and any development that affects this will negatively impact all local residents (who own this land under MHH limited). In addition the environmental impact needs to be considered as we should be protecting such areas rather than cutting through them simply to save 5 minutes on a bike when a perfectly suitable road / footpath based route exists.	
IM12	Mel Cunnell	688	No	Regarding the proposals to change and/or add cycling routes through Martlesham Heath Parish I would like to raise the following thoughts. I am extremely surprised and alarmed that any changes to the existing routes are lawful given the number of covenants governing rules and regulations placed on all aspects of living and future development within Martlesham Heath. The proposed additions of route IM12 also relate to land which is maintained via an annual levy paid by all households within Martlesham Heath. These areas were never envisaged as general access areas for non-residents. The proposal for the wooded area described as Martlesham Woods presents a high potential negative impact on existing wildlife habitats not to mention the unacceptable destruction of over twenty mature trees. The start and finish points of this route are currently served by existing routes with a similar travelling distance. This route would also create a further unnecessary obstacle over the A12, the cost of which could be better allocated to more deserving areas. This route would also create the potential for strangers inadvertently straying into sensitive areas such as the SSSI contained within the Heath and has potential for new unwanted	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>tracks being created as was demonstrated during the recent lockdown, the number of crossing points on Eagle Way would also increase. The impact of this route would present a considerable distraction from the character of the area and is not within the spirit envisaged when the original development was proposed.</p> <p>It is also worth noting that the objectives of the Brightwell Lakes development were to create a new community with links via cycling and walking to employment and current retail provision provided by the Industrial Estate together with existing links to public transport and the Park and Ride facility all of which are provided by current routes without the need for route IM12.</p> <p>In conclusion the case for IM12 is not a justifiable expenditure nor is its requirement proven. I therefore object to the provision of IM12 through Martlesham Woods and its junction with Eagle Way near Forest Lane.</p>	
IM12	Michael and Sheila Parker	753	No	<p>I strongly object to the proposed paths for cycling in this area.</p> <p>Particularly the proposed path through Martlesham Woods, which is known locally as Birch Woods. An annual bird survey always counts at least twenty different species in the woods. Lighting and general disturbance would have a detrimental effect on the bird life. Cyclists are often seen cycling on Eagle Way. They don't seem to complain.</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				If East Suffolk Council have excess money to spend why not assist the hard working group of helpers working on Martlesham Heath. We could do with some tools to help clear gorse, brambles and small saplings and satisfy the demands of Natural England.	
IM12	Michael Farahar	721		<p>With reference to East Suffolk Council's East Suffolk Cycling & Walking Strategy - Draft 2021 consultation, I would like to support the draft's recommendations contained within S 3.15, The Ipswich to Melton Key Corridor recommendations.</p> <p>I would like to suggest though that an innovative and respectful approach is adopted to minimise the impact on both local residents and the natural environment when planning for the implementation of recommendations IP4, IM9, IM10 & IM12, for example by the use of low level lighting rather than traditional full height lamp posts and/or lighting intensity controlled by movement sensors etc,</p>	Support for the Ipswich-Melton Key Corridor is noted. However, the importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Michael Rogers	479	No	<p>I strongly oppose this scheme.I feel very strongly about this complete waste of money as</p> <p>First off, I believe it entails the destruction of a number of mature trees, in this day and age of global warming / carbon sequestration this should be avoided at all costs, to say nothing of the loss of the enjoyment of the trees which the local community has sought to carefully maintain in this area over many decades! (Even if the scheme goes ahead in modified form, this is not a motorway, people / cyclist can go around the trees.)</p> <p>Secondly, cyclists who cross the A12 on the newly</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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				<p>proposed footpath can join Eagle Way and continue to use the existing well lit road to cross over to the west side of Eagle Way to continue their journey reusing existing infrastructure.</p> <p>Thirdly, this is an area used by locals for personal exercise, family walks and dog walking. The most likely cyclists using this proposed route are likely to be teenagers making their way to and from Kesgrave High School. Unfortunately, teenagers are not likely to use their lights or bells or take much care if anyone else is using the path. I anticipate that there will be accidents particularly with the elderly (me) trying to get out of the cyclists way. My experience is based on the behaviour of cyclists in Cambridge (see my comments re: IM10)</p>	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Mike Cowell	687	No	<p>I would like to comment on the above document, specifically in relation to Martlesham Heath.</p> <p>IM12 is shown coming across the A12 then passing thro Birch Woods (Martlesham Woods). It then exits on Eagle Way, opposite a footpath leading to the SSSI of the western corridor.</p> <p>I am a keen cyclist and cycle every day but I am against this proposal because,</p> <ol style="list-style-type: none"> 1. At the exit to Eagle Way, it will tempt people to try to cross the SSSI en-route west 2. Birch Woods is a peaceful quiet retreat. I don't want trees felled and a wide, lit tarmac road installed, which will destroy the atmosphere for wildlife and residents alike. 	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				A better approach would be to improve access to the heath (and further west) via the existing footbridge across the A12 (IM11). I regularly use IM13 and IM11 to do this and it is clear that the existing cycleways are inadequate and too narrow. Cycling past Barrack Square is easy but Gloster Road around Aldi is quite dangerous because of the narrow width of the cycle lane and the massive increase in traffic over the last few years.	
IM12	Mike Turner	71	No	<p>The proposed route through Martlesham Woods seems unnecessary, will have a high environmental impact and will be a waste of money.</p> <p>It is unnecessary because the current pathway is completely adequate for walkers (I walk on it many times a week with my dog), and (speaking as a cyclist) using Eagle Way is already an existing route suitable for cyclist, it is quite, well lit and with an existing good cycling surface.</p> <p>From an environmental perspective, it seems mad to do destroy an existing and well used woodland for this purpose and at the same time create additional light pollution. The path would also need lots of leaf clearing in the autumn to make this safe for cyclists - in fact, I can see cyclists avoiding it and using Eagle Way in the autumn and winter as the road is likely to be better maintained.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Mike Ward	216	No	I see absolutely no practicality in wasting time, cost and effort to create a new cycle path in woodland or field when there is a perfectly established path on Eagle Way. That would need the minimum of	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling

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				upgrade and this would not upset or offend any resident of Martlesham Heath.	and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Mike Ward	232	Yes	<p>I support this but only if Eagle Way is improved as a cycle route. The woods and fields around Eagle Way should be left alone as they are full of nature. We need to save our environment. I'm not a cyclist myself but I see the benefits and attraction, cycling is good for the environment, digging up countryside is not.</p> <p>We have a perfectly good path round Eagle Way, let's use it!</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	M Meyerstein	224	No	The proposed south option is a problem The path that would run behind the properties on Lancaster Drive, Mayfields, Digby Close, etc., would be high above the ground level of the houses (due to the slope of the land) and users of the path would be able to see directly into the windows of those properties. Residents would need to build 6-foot	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the

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				fences across the rear of their gardens, thus preventing them from seeing out over the open land. Also, the western end of the proposed path terminates at a SSSI, over which cycling is not permitted.	proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	M Meyerstein	410	No	<p>it is not necessary for IM12 to have a new footbridge and an illuminated path through Birch Woods (known to the Council as Martlesham Woods). The saving of distance compared to using the existing footbridge at IM11 is insignificant. The environmental impact of an illuminated path through the woods and across green land between Coopers Rd and Lancaster Drive would be significant.</p> <p>Also, the proposed route of IM12 would encourage cyclists to cut across SSSI land to get from Eagle Way to the IM4 part of the cycle path. This would bring the cyclists into conflict with walkers and anyway cycling is not permitted across that SSSI land.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Moira Weaver	107	No	<p>I am a cyclist and resident of Martlesham Heath. I absolutely object to a path being cut through our precious Birch Woods. It is clear from the maps and from my own experience that there are alternatives far less destructive to the environment. We should not be cutting down trees to make new paths when it is clear from previous comments on existing paths that these simply need to be maintained.</p> <p>With reference to a new crossing -there are already</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and

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				2 crossings over the A12 that allow links to existing cycle paths. I cannot see the need for a third.	walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Moira Weaver	241		<p>I am a frequent cyclist and live in Martlesham Heath. Whilst I absolutely support a revised and improved network as outlined on your site, I absolutely do NOT approve of plans to put your routes as proposed through the Heath.</p> <p>In particular I and my husband object to the planned route through Birch Woods (you call it Martlesham Woods). This is a precious area of natural woodland where you are proposing the felling of at least 25 mature trees, and lighting that will disturb wildlife. The exit would tempt people to take a short cut across our SSSI. I cannot see the logic of this proposal, particularly in view of the urgency to preserve our threatened wildlife on our planet.</p> <p>The same illogic and I would say, brutality, seems to apply to the suggestion to cut through Portal Woods.</p> <p>I appeal to your consciences to consider the future impact of your proposals. Neither of these paths are necessary for a good cycling experience in this area.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Mr & Mrs D Evans	905	No	Turning to the impact on Martlesham Heath itself. You appear to focus on new roads and infrastructure whilst ignoring existing routes near to those you are proposing; namely the IM12 crossing with IM11. The National Cycle Route 1 is already in existence and uses the same route in part as IM11, but its route is	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For

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				<p>not mentioned in the your draft strategy.</p> <p>The proposal to put IM12 through Birch Woods (referred to in the consultation document as Martlesham Woods) would seem to go against East Suffolk Council’s Environmental Policy. Mature trees, shrubs and wildlife habitats will be damaged or destroyed in the process of installing a tarmac route through the woods. We can see no logic in this when the route, assuming the North Option bridge is built, via Eagle Way, Sidecentre Gate and Valiant Road is only slightly longer! Eagle Way, whilst the spine road of the Heath, is not a busy road and is mainly used by residents. Buses are now infrequent since the removal of Route 66 except for the hourly 66A. Current footpaths are probably equidistant for pedestrians. You propose to tarmac and light the path through the wood which will certainly affect the local bat population together with nesting birds including nightingales and various mammalian and other species.</p> <p>It would seem more logical to use the current bridge over the A12, upgrading it if thought necessary, as it connects more easily with the current infrastructure. Better signage would help those not familiar with its position. For instance at the moment those approaching the mini roundabout on Eagle Way from the south have no signage to show there is a crossing bridge and have been known to cross the A12 at the roundabout!</p> <p>The use of the current infrastructure would result in</p>	<p>these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>no detriment to the environment and would protect and enhance the biodiversity of Martlesham Heath, an aim of the East Suffolk Environmental Policy whilst promoting the use of walking and cycling in line with this draft strategy.</p> <p>In summary we do not support the provision of a cycleway through the woods believing that the current infrastructure is more than sufficient.</p>	
IM12	Mr & Mrs M Calver	681		<p>We wish to register our objection to the construction of a new cycle way through Birch Woods (which East Suffolk Council refers to Martlesham Woods?) Cycle way ref IM12.</p> <p>Our concerns are that the cycle way is carving through a treasured amenity owned and used by Martlesham Heath residents. The existence of such a cycle way is bound to cause unwanted antisocial behaviour to nearby residents and in the woods itself. Passing through the woods it will be hard to police and therefore could well be a target for crime activities.</p> <p>This peaceful litter free amenity will be spoiled forever!</p> <p>Please reroute avoiding these lovely woods.</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Mr R and Mrs V Gadsdon	715	No	<p>We strongly object to the cycle path through Martlesham Woods</p> <ol style="list-style-type: none"> 1. The cutting down of mature trees. 2. Encroachment on the wildlife and their habitat. 	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision</p>

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				<p>3. Light pollution in the area.</p> <p>4. Introducing more visiting of people to a Site of Special Scientific Interest which could destroy the area.</p> <p>5. Increase of litter</p>	<p>along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Nicholas Kitchen	72	No	<p>I am a keen cyclist however I am dismayed at the proposal to carve a cycle path through mature woodland at Martlesham Heath or indeed anywhere else along the proposed route. The steady expansion of Ipswich is slowly destroying natural amenity and countryside. Already the area of Martlesham is being overdeveloped with unnecessary housing development, I find any destruction of natural woodland reprehensible.</p> <p>Eagle Way is well lit, easily navigable and perfectly suited as a cycle route. Any proposed cycle route should use this existing road to circumnavigate the woodland at Martlesham. The woodland is a natural haven within the housing estate, a rare natural amenity that residents enjoy. The proposed development of a lit concrete cycle path will destroy this amenity for residents who pay a premium on houses and their council tax to live in the area. The inevitable destruction of mature trees and woodland is not acceptable in today's climate especially given the obvious, safe and existing route that can be taken around Eagle Way.</p> <p>I strongly oppose this application and urge the council to reconsider</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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IM12	Nicki Finan	727		<p>I am writing with comments regarding the proposed new bridge, footpaths and cycleways through Martlesham Heath.</p> <p>Some of these proposals will have significant impact on our privacy, security and enjoyment of our property and surroundings.</p> <p>With a path backing directly onto gardens, security and privacy will be adversely affected and I would like to know what steps will be taken to mitigate these risks should the proposal go ahead.</p> <p>Additionally, the trees act as a buffer between properties and the A12 road noise and pollution and removing any of these beautiful mature trees seems to be an environmental contradiction. Foxes and muntjac deer have a path through these woods.</p> <p>The Birch Woods do not need a cycle path in my opinion. Individuals already cycle through the existing paths if they like a challenge. Why not add a cycle path to Eagle Way which is already lit? Again, in this era of environmental sustainability how can removing beautiful mature trees and their attendant benefits be justifiable? The nature of the woods which is 90% unmanaged and is a very effective Carbon sink as well as providing habitat for local wildlife.</p> <p>I question who will use these paths, the bridge already there that emerges near the village centre and shops is well used and an obvious route to Tesco and the other many retail outlets on the industrial</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>side. Saving in distance is minimal compared to using the existing bridge. The new community of Brightwell Lakes will presumably develop its own local infrastructure.</p> <p>In summary, I cannot see the justification for destroying trees to gain very little, if any benefit.</p> <p>Looking at the map with proposed routes, the existing distance from the Adastral Park side over the bridge is the same as the proposed new bridge which bypasses the village centre so footfall is diverted from village businesses and the local pub.</p>	
IM12	Nigel and Diane Bennett	662	No	<p>We are writing to comment and object to your above strategy document. In particular we (my wife and I) are concerned by your proposal called IM12 which cuts directly through Martlesham Heath.</p> <ul style="list-style-type: none"> • Firstly the proposed footbridge and cycle path across the A12 which apparently will land in the wooded area between Coopers Road and Lancaster Drive. This land is privately owned by Martlesham Heath Householders Limited (MHHL) who manage this and many other areas on the Heath on behalf of the residents. It is the subject of "Village Green Status" and our understanding is that it would require an Act of Parliament to reverse this and change its use, not simply a Compulsory Purchase Order. • Secondly the proposed cycle path then crosses Eagle Way and goes straight 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>through what your maps call “Martlesham Woods” which we in the village call “Birch Woods”. This is again private land owned by MHL who received this from the original developers of the village, Bradford Property Trust. These woods are a very natural environment which are 90% unmanaged to allow nature to take its course and provide a habitat in which foxes, deer, bats and numerous species of birds thrive. I suspect that if your proposal were ever allowed to go ahead then we would never hear a nightingale or owl from our house again. It has been suggested that putting a cycle / footpath through this area would result in the felling of at least 25 mature trees which would be totally irresponsible in this time of significant climate change when every tree should be cherished and not felled to allow for a cycle path (and I am a keen cyclist). In particular, there is a single poplar tree that is a dominant feature of the woods and greatly loved by my wife who may be tempted to physically protest if this tree were under threat of being felled.</p> <ul style="list-style-type: none"> On the map it suggests that IM12 will cross the A12 and proceed in an almost straight line across Eagle Way on the eastern side of Martlesham Heath before dissecting Birch Woods. However, if you actually look at the proposed route it almost certainly goes through a garden belonging to a household. The only way that the path 	

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				<p>could proceed is a zig zag way to cross Eagle Way see my comments on Health and Safety below.</p> <ul style="list-style-type: none"> • At the exit of the Birch Woods your document suggests that IM12 will take a sharp right turn onto Eagle Way. This is no doubt to avoid the SSSI area again managed by MHHL and its subsidiary MHH SSSI however I fear that for some cyclists and walkers the temptation of crossing onto the pathway that leads to the SSSI and then to Dobbs Lane will be too tempting and lead to the further destruction of this valuable resource and further damage the habitat of the “Silver Studded Blue Butterfly” which Natural England, alongside MHHL and its subsidiary are striving hard to protect. • Health and Safety – how are you going to help ensure the safety of cyclists crossing Eagle Way which loops round Martlesham Heath. The recent introduction of the speed sign notifying drivers of their speed is useful but still not an effective deterrent to those who think that Eagle Way is a race track and can race back to their homes at 50mph in a 30mph area. We are also interested in your intent to make walking and cycling safer, as the current footbridge over the A12 between Gloster Road and Burgess Place is not sufficiently wide for both walking and cycling and has been the scene of several near misses in our experience. My wife crosses Eagle Way every day to go over the 	

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				<p>footbridge and it is hard to see cars coming from the right on the sharp bend and there should be a zebra or similar crossing put in place. It is used by lots of people including the elderly who may not be able to run while crossing! Surely the safety and upgrading of the current infrastructure should be paramount before further development. The Industrial Estate, that has now been turned into a Retail Park by the Council's decisions to allow the growth in retail outlets and then the further significant development of merchant outlets (such as Travis Perkins) have only added to the dangers for both walkers and cyclists alike.</p> <p>In our opinion the Council would be better improving the Foxhall Road (more street lighting and potentially widening of the road) and crossing the A12 near the Foxhall roundabout and building a dedicated lit cycle path alongside Foxhall Road which could then either come up Dobbs Lane which is another road which could be improved to link with the path called IM4. As a cyclist I would certainly be in favour of an improved route along Foxhall Road. As an alternative the cycle path suggested by myself alongside Foxhall Road could be extended all the way to Bell Lane. Or use the existing PROW which exists to the south of Martlesham Heath before connecting to IM4 or the roads as suggested above.</p>	

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				Please note that we do NOT agree with your proposed strategy and want to underline that MHHL is a representative of all the householders (i.e. owners) approximately 1400 households and should NOT be dismissed as one voice when they levy a charge on each householder to manage and maintain the green spaces, yet have to waste their resources asking the householders such as ourselves to ensure that our views which would be similar to those voiced by MHHL are actually noted. Perhaps you should also consider consulting with the legal owners of land (i.e. MHHL) that you intend to cross with proposed paths before placing documents in the public domain and terming them strategy?	
IM12	Nigel Maxwell	813	No	<ul style="list-style-type: none"> — it is not a good idea to put a cycle path through the existing Birch Wood — think back to the campaign to save the Portal Wood from being made into a Park & Ride when common sense won and a better decision was made — please consider re-routing IM12 along the eastern side of Eagle Way (parallel to Coopers Road) up to Burgess Place in order to join with IM11 	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Nik Bestow	11	No	It is completely unnecessary to build a lit cycle path through Martlesham Woods. Eagle way is wide enough to support a cycle path and does not involve destroying woodland.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling

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				It is also disappointing that you have adopted the North option for the bridge. The Southern option would have been more sensible.	and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Nina Paradine	865	No	<p>I have read the above document and must completely say NO to this idea.</p> <p>The people living on Martlesham Heath had to pay a premium when buying their property. This is a beautiful place to live, with the way the road layout was set. A very safe place to drive, with beautiful walks through the forest in the middle and all around the housing. Absolutely amazing.</p> <p>You CANNOT and SHOULD not, put a plan like this destroying, THE WOODLAND and the whole environment of MARTLESHAM HEATH.</p> <p>Surely, this should have been thought about and sorted prior to building another housing estate the other side of BT Martlesham.</p> <p>ABSOLUTE MADNESS.</p> <p>REGARDS, A Resident of Martlesham Heath since the first stage of development.</p>	The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>Moving three times to different homes, but still staying and loving the Environment. You SHOULD NOT Destroy MARTLESHAM HEATH with this terrible, awful Plan.</p> <p>ABSOLUTE MADNESS!!!!!!!</p> <p>NO NO NO NO</p>	
IM12	Parsons, Pauline	246	No	<p>I object to the proposals of a new route from the new 2000 homes at Brightwell Lakes across Martlesham Heath.</p> <p>In particular, the section through Birch Woods. If a new footbridge is to bring cyclists across the A12 south of BT, then when it meets Eagle Way there is sufficient pavement for pedestrians and quietly-trafficed road on Eagle Way for cyclists to join up with existing walking and cycling routes.</p> <p>I do not think it is necessary to put a new tarmac route through a beautiful feature such as Birch Woods. Why are we proposing to fell trees when we need so many planted? It doesn't make sense.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Pat Lisseman	312	No	<p>With reference to your proposals I would like to lodge an objection to:</p> <p>1. IM12</p> <p>This is an area which includes Martlesham Woods</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For</p>

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				<p>where walkers, children and dogs can roam freely without worrying about cyclists. It is also an SSSI.</p> <p>Eagle Way has little traffic and there are very rarely cars parked at the kerbside and is perfectly safe and adequate for cyclists.</p> <p>Residents of Martlesham Heath love their environment and open spaces - please don't destroy by encouraging through traffic,</p>	<p>these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Patricia Embery	708	No	<p>I am writing to register my strong objection to the planning of a new footpath/cycle track through Birch Woods on Martlesham Heath. I understand that this is to connect up the new development at Brightwell Lakes through to Kesgrave but we have adequate facilities/footpath already in place from Eagle way without imposing damage to SSSI and disturbing wild life, apart from taking trees down which we are encouraged to preserve and plant more, and also incurring unnecessary cost. Martlesham Heath is an unusual development which most residents feel protective of. We love our open spaces with the natural footpaths already there which came into their own during the pandemic – Please do not ruin them,.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Patrick Carlin	556	No	<p>This is an unnecessary destruction of local woodland for the convenience and facilitation of the new development at Brightwell Lakes. The time/distance advantage gained over existing routes is minimal and is yet another erosion of our local community that is already under pressure from developments at Brightwell Lakes, Police Headquarters, BlackTiles Lane, McCarthy & Stone etc,etc.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch</p>

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					Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Pat Smith	370	No	I would like to register my objections to the proposals for new cycle routes in Martlesham. These seem to me to be in direct conflict with current aims. We should be aiming to reduce light pollution which is already high in Martlesham Heath. Also, there have been calls for people to plant more trees. They will not have any effect for several years, yet these proposals mention felling at least 25 mature trees. Surely this goes against the Government forestation policy? There is also a risk to our wild life in Portal Woods, Birch Woods and in particular the SSSI. Cyclists already pose a danger to pedestrians on the Heath, particularly the elderly and these additional routes are going to make life even more treacherous.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Paul Burke	679		I have concerns regarding the draft cycling and walking strategy. Specifically the proposal to push a bridge across the A12, cutting through the woods between Coopers and Lancaster Drive, and create a new paved and lit cycling / pedestrian route from there through Birch Woods (which East Suffolk Council refer to as Martlesham Woods). The existing footpath through Birch Woods is already perfect for pedestrians and light cycle traffic. The woods support a range of wildlife and I enjoy my	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new

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				<p>frequent encounters with deer and foxes along the path when walking the dog. It really doesn't need to be widened, tarmac'd and floodlit. Apart from the damage that would be caused to trees either side of the path and the overall ambience of the woods which are very natural, we really don't need fast moving cycle traffic mixing with slow moving dog walking pedestrian traffic. Cyclists should be encouraged to use the existing road and cycle infrastructure.</p> <p>I'm not convinced that a 2nd bridge crossing is required so close to the existing bridge, although I accept that the road / path infrastructure needs to be improved passed Adastral Park. Maybe an underpass would help to cross the busy road from the BT roundabout onto the industrial estate. However, if a 2nd bridge is to be built, it should simply be used as an efficient route to get cycle traffic onto Eagle Way.</p>	<p>recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Paul Davey	323		<p>As a keen walker and cyclist I am very much in favour of ESC having a strategy for my benefit.</p> <p>However as a resident of Martlesham Heath I considerable concerns regarding the draft proposals published this year.</p> <p>My objections and reasons are listed below.</p> <p>2. The existing cycle path between IM7 and IM11 is not shown. IM12 is not therefore required.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and</p>

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					Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Paul Finbow	612	No	<p>I strongly object to this proposal. There is no reasonable justification to create a cycle / walking path through our well-established woodland areas or build another bridge across the A12. This proposal is a complete waste of public money and has the potential to increase carbon emissions, rather than reduce them! My wife and I are both keen cyclists and regularly cycle from Martlesham Heath to Felixstowe, Brightwell and the Trimleys. We regularly use the bridge across the A12 and the time saving proposed by the new bridge between Coopers Road and Lark Rise is minimal compared with using the existing bridge and in no way justifies the destruction of well-established woodland, which already do an essential job of capturing carbon emissions from the busy A12.</p> <p>The proposal to develop a tarmac, lit walkway through our existing, established woodland is deplorable to say the least. Not only does the proposed new walkway go nowhere, the actual time saving from using Eagle Way is minimal, especially for cyclists and these established woodlands are a haven for wildlife, as well as including many large Silver Birch Trees, which do an essential job in capturing the carbon emissions from the A12.</p> <p>I urge you to remove these proposals from your strategy.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Paul Kitchen	75	No	<p>I consider it damaging to the woodland and the wildlife it sustains. The woodland already has</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from</p>

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				natural footpaths and offers a useful amenity to Martlesham Heath residents who pay a premium to enjoy the natural environment. It seems totally unnecessary as Eagle Way already offers perfectly acceptable cycling and walking route.	urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Paul Newman	515	No	<p>In particular two corridors IM10 & IM12 where they are constructed on open or wooded land owned by Martlesham Heath Householders Ltd (MHHL) whose shareholders are all residents of Martlesham Heath.</p> <p>I am fundamentally opposed to cutting any 4.5m tarmac artificially lit cycleway through Birch Woods (incorrectly titled Martlesham Woods), Portal Woods & the open land at the back of Whinfield/Broomfield. To cut such a tarmac cycleway would involve the destruction of many mature trees & would fundamentally change the whole concept of open sandy paths that the residents currently enjoy either exercising or walking dogs. The existing cycle/foot bridge over the A12 alongside Martlesham Leisure could be upgraded, if thought necessary, & the existing paths & roads on Martlesham Heath could be altered to accommodate more cyclists. This can be achieved by making the entire Martlesham Heath enclave a 20 MPH Zone</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>from the only two access junctions at the BT & Tesco roundabouts on the A12. The roads could be rebuilt to use the same traffic calming methods used on the Ravenswood development that has the same 20 MPH Zone so that cyclist, walkers & children would be safe.</p> <p>In any event none of the proposed construction will be allowed on any of the open or wooded land owned by MHHL as it is all covered by a Section 52 restriction placed on it by the original developer Bradford Property Trust. In fact Bradford Property Trust over 40 years ago thought to put tarmac paths through Birch Woods after they had put the Section 52 restrictions in place & found that they were not allowed to do so. Having been in contact with Bidwells Solicitors in Cambridge who drew up the original Section 52 restrictions I was informed only a full act of Parliament could lift the restrictions in the event that one of the parties to the agreement did not agree. As a shareholder in MHHL I have instructed them to oppose any construction, which I understand has the support of all the directors.</p>	
IM12	Peter Davies	55	No	<p>The plan is very good in parts, the section cutting down trees in an established wood is not good, lit and probably tarmac surface...does not bode well for the environment. A better suggestion would be fringing the outer section of the woods south around eagle way where there is space, so I am not in favour of this hardened surface way through woodlands...</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and</p>

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					walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Peter Davies	695	No	I understand that you have created a plan for a combined linked cycle/walkway covering a route through from Woodbridge and Melton towards Ipswich connecting villages and areas en-route. A commendable plan..... However , there is one section that is completely out of the current thinking and the recent climate issues, and that is the plan to drive a tarmac'd 4.5m wide route through Birch Woods Martlesham Heath (you have referred to them as Martlesham Woods which is incorrect). What an absolutely crazy idea!! So, you are planning on removing mature trees disturbing wildlife of which there are endangered species (ie Nightingales, Blackcaps, Great Spotted Woodpeckers etc etc...) taking away the rights of the residents (who own/lease this land) to peacefully walk these numerous footpaths (Natural footpaths) to enjoy the freedom and outdoor wildlife and scenery, for the sake of an ill thought out plan for cyclists in this area using a lit tarmac cycleway, thereby encouraging litter (people do throw things away on cycle routes just look around you) so there will be pollution and disturbance 24/7. Tarmac is a polluting substance which will leak into the surrounding natural areas. Removing trees is also adding to the carbon footprint not reducing it. You cannot replace these trees or the wildlife in and around them.	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>I have lived on Martlesham Heath for 42 years, it was designed to have space and outdoor areas around the hamlets, the wildlife has taken at least 35 years to recover from the building work and general disturbance, all for the wellbeing of residents and visitors alike. It seems as if our Countries Councils have no connection or respect for the local people, you are given some money by central Government and it burns a hole your pocket!! Perhaps you should have come and talked to the local residents first.</p> <p>I have heard in the last few years Tawny Owls in and around the Birch Woods, these declining species need dark nights and no disturbance from lights, noise and general human presence. The Nightingales have returned year after year from Africa to almost the place where they nested the year before, you are in complete disregard for any of the wildlife and environment on Martlesham Heath and specifically Birch Woods. You will remove what we have now in peace and tranquillity and the right to the wildlife etc in those areas in complete disregard for the latest issues with wildlife the environment etc. Contrary to all the current thinking, the Government only yesterday gave a signal to rewild areas of countryside, well we already have a wild area called Birch Woods. Yes, I am very angry and you have offended me and my family.</p> <p>Let me offer an alternative for you if you do want a cycle/pathway around that area....a simple plan meaning little or no disturbance to the area inside</p>	

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				<p>Birch Woods. Your plan includes a footbridge/ underpass over or under the A12 and then cut through the small area of woods beside the A12 to enter Eagle Way, to cross over and then lay a new widened path/cycleway all around the southern edge of the woods beside Eagle Way itself, the space is there, very little in the way of removing vegetation and trees (if any) and it will end up in the same position as the pathway through the woods, then crossing over the road by a proper crossing and then to continue on your planned route through to Grange Farm properly signposted, the verges are wide enough to take a widened route, it is a slightly longer route but hey you are on a bike, and walkers can take an elongated route if required as an alternative to through the woods, power is nearby for low level lights as the road lamps are in close proximity and a proper safe crossing can be installed somewhere safe (maybe nearer the school) to enable safe passage over Eagle Way...is this too logical!? It also means it serves the Southern half of Eagle Way residents' access to a safe route without cutting through unmade pathways to reach the planned tarmac cycleway in the woods and would encourage people to use it rather than existing pathways suitable only for pedestrians.</p> <p>Come what may I and many others will oppose these proposals through the woods by whatever means we have at our disposal....</p>	
IM12	Peter Kerridge	409	No	This proposal contradicts all the fundamental principals of the design of Martlesham Heath, a design that has proved to work exceptionally well	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is

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				<p>and the author's poor understanding of this threatens to ruin our valuable environment.</p> <p>Martlesham Heath was designed to have a central natural woodland linked by protected natural areas between hamlets to create wildlife corridors. In addition hamlets are linked by cycle/footpaths that mean there is no need to use the natural footpaths in the woodland or indeed Eagle Way. As it turns out the circular layout and low traffic numbers on Eagle Way provides a superb cycle track and so cyclists choose to use this road as a training facility which works incredibly well. Walkers on the other hand make use of the woods for recreational walks as well as walking through to the Green and Square facilities and have done so since day one. The use of the woods by walkers is currently symbiotic to any cyclists who use its natural pathways. This is because the natural paths slow down cyclists meaning that the numerous walkers in no way feel threatened by speeding cyclists. The vast majority of walkers in the woods do not want unnatural walking surfaces, this can be show by the fact that the Council two years ago installed a man-made surface purporting to be part of a fitness trail (something that is never used because the equipment is so basic and poor - what a waste of money). Since the installation of this artificial pathway, the residents of Martlesham Heath have literally voted with their feet and created a whole host of new natural paths throughout the woods. So, as the whole crux of this report is to encourage walking this can easily be done by leaving</p>	<p>recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>this area alone. Cycling, as anyone who know anything about the Heath can be done quite safely on the numerous paths or for more ardent cyclists by using Eagle Way a very quiet road with no cars parked on it and very good sight lines making it exceptionally safe. Indeed in the 35 years of my living here I can't recall a single accident involving a bike on Eagle Way. Presumably the author has investigated this prior to writing these recommendations.</p> <p>The proposal to put a tarmac lit cycle path through the woods is horrendous and will ruin this valuable natural resource, something that this report is fundamentally meant to protect. Martlesham Heath is blessed with numerous tarmac paths and quiet, safe roads which work perfectly well as is shown by the fact that Kesgrave School has the highest percentage of students going to school by bike, which all the parents on the Heath feel perfectly comfortable with. So the question should be asked, if it ain't broke, why fix it? What you will be breaking is the lovely natural pathways we have in our woods and actually discourage walking there because people don't always want to walk on tarmac and certainly don't want to be jumping out of the way of speeding bikes.</p> <p>This idea of having the tarmac cycle path running through the woods could only have been dreamt up by someone from outside of this area. These woods are privately owned and managed, to put any form of lighting in there would only encourage anti social</p>	

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				<p>behaviour late at night and leave youngsters vulnerable to any form of criminality from littering, to drug abuse and god forbid child molestation. And who will be left to pick up the pieces? Not the Council, they quickly wash their hands from any involvement as can be seen when we had people sleeping in the woods.</p> <p>These woods are one of the few remaining nesting sites for Nightingales (particularly since the Travis Perkins development was allowed decimated their local stronghold. This proposal will further jeopardise the survival of this declining species in Martlesham, what more could deter a night singing bird than street lights and habitat removal. As I've already commented on elsewhere, I thought the idea was to reduce carbon emissions not increase them by putting in street lights everywhere, that even your own Council turn off at midnight.</p> <p>This leads on to the proposal for another bridge linking the Heath with Adastral park. Even a basic understanding of the failures of estate design of the 1960s and 70s concludes that the more entries and exits from an Estate the more there is inclined to be anti social behaviour so this is something we can quite happily do without thank you. If I want to cycle to Adastral Park and the lake there I go over the existing bridge and down the side of Falcon Caravan Park its a lovely cycle and something the new residents at Brightwell lakes should also be encouraged to do. We certainly do not want a short cut through our woods by people who have</p>	

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				<p>absolutely no vested or financial responsibility for looking after them. And that's without commenting on the exit on the other side which will only lead to more bikes cutting through onto the SSSI and endangering that area.</p> <p>All in all these proposals are not needed and very much not wanted.</p>	
IM12	Peter Simmons	406	No	<p>A bridge to connect Brightwell Lakes development and Martlesham Heath may be of some benefit for those cyclists and walkers wishing to get to Brightwell, Newbourne, Waldringfield and surrounding areas however the new path through Martlesham woods should not be considered for the following reasons:</p> <ol style="list-style-type: none"> 1) Wildlife in Martlesham Wood would be affected whilst the work was being undertaken to complete the new route. 2) The addition of lighting to the proposed route as stated in the recommendations would also impact the wildlife. The addition of artificial light in a forest is not natural. Several species of deer live in and around this area and are likely to be affected. 3) A footpath through the forest already exists for walkers needing to take the short cut so a new one is not necessary. 4) Cyclists wishing to traverse East to West Martlesham Heath already have a route around Eagle Way, the road is seldom busy so it is quite safe. 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>Young Children not wanting to cycle in the road can cycle on the existing footpath around Eagle Way as the local children already do. Note: the existing footpaths could benefit from some repairs to surface areas that have been disrupted by tree roots.</p> <p>5) The government are encouraging people all over the country to plant trees to help the environment . I feel that the removal of established trees, required to make way for the new route, is detrimental to the environment. It's a big price to pay in order to save some cyclists and walkers making the additional effort to complete the additional few hundred metres around Eagle Way.</p>	
IM12	Peter Young	457	No	<p>This seems like a good way to spend a significant amount of money on a route which will provide virtually no benefit and cause maximum destruction and inconvenience to residents both human and animal.</p> <p>The only benefit that I can see from this proposal is that it is roughly 100m shorter than the existing route across the existing footbridge from Martlesham Leisure to Eagle Way.</p> <p>There are so many disbenefits it is difficult to list them all, but I can come up with;</p> <ul style="list-style-type: none"> • Loss of amenity of privacy and light pollution for those houses which are adjacent to the proposed footpath through the woods and between Coopers Road and Lancaster Drive 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<ul style="list-style-type: none"> The destruction of roughly 3/4 mile of natural woodland in Birch Woods (referred to as 'Martlesham Woods' on the map) including the removal of at least 25 mature trees Permanent damage to the effectiveness of the woods, which act as an effective carbon sink and provide habitation for natural wildlife The costs involved in erecting a new bridge over the A12, rather than improving the existing bridge The disruption caused to wildlife inhabiting Birch Woods (a.k.a Martlesham Woods) and loss of amenity to residents during the construction phase The inevitable pressure this route will put on the Site of Special Scientific Interest between where the proposed route ends at Forest Lane and the proposed new track described in IM4, leading to damage to the SSSI, because people will not bother to go up to the proposed new path in IM10, they will simply cut the corner. 	
IM12	Philip Gooding	91	No	I object to this, mainly because of destruction of environment, ie trees and wildlife habitats. This path through Birch Woods is totally unnecessary. It will be dangerous for walkers with cyclists tearing through the footpaths. I thought this land belonged to Martlesham Heath Householders. How can East Suffolk council just take this land ?	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In

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					its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Philip Wilmot	236	No	<p>I object to IM12 because I believe that it will have a number of adverse impacts on Martlesham Heath. These include the following:</p> <ol style="list-style-type: none"> 1. The construction of a bridge over the A12 in order to deliver pedestrian and cycling traffic to/from IM13 is likely to create privacy problems for at least some homes in Coopers Road and Lancaster Drive. In making this assertion I assume that for engineering reasons the profile of the bridge will be similar to the one that already exists, and which takes the form of an arch, so that users are elevated relative to the surrounding land and will be able to peer into people's gardens and even upstairs windows. I will not however be personally affected by this. 2. A new bridge over the A12 will lead to the destruction of woodland and open space on the western side of the A12/eastern side of Eagle Way. As far as I know this is not officially protected land but it does nevertheless provide a habitat for wildlife and birds and is used for informal leisure. It also provides a natural buffer between adjacent hamlets. The need for such natural buffers between hamlets was a principle that guided the creation of Martlesham Heath as a whole, and it has contributed significantly to the overall success of the 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>development.</p> <p>3. The construction of a lit, paved path, perhaps as much as 4.5 metres wide, through what is referred to as Martlesham Woods, but which is known locally as Birch Woods, is likely to involve the felling of mature trees, and a loss of habitat for wildlife and birds. It will also interfere with the use of the woods for informal leisure by both residents of Martlesham Heath and those living further afield.</p> <p>4. There is a risk that the construction of IM12 will lead to an increase in both pedestrian and cycle traffic using the Martlesham Heath SSSI (formally known by Natural England as Unit 2 of the Ipswich Heaths SSSI) as a shortcut to their ultimate destination, especially when one considers the provision of IM4 and IM10. This land is leased to Martlesham Heath Householders Limited and managed by its subsidiary Martlesham Heath SSSI Ltd. with the aid of grants from Natural England. Currently the area is effectively open to the public but this can only remain the case if the site does not deteriorate as a result. Significant through traffic might require action: this land is not a thoroughfare.</p> <p>5. It seems reasonable suppose that IM12 will require the introduction of traffic lights at the two points at which it crosses Eagle Way. This would not be acceptable to residents.</p> <p>6. IM12 appears to follow the western segment of Eagle Way for a considerable distance. I think it is</p>	

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				<p>reasonable to suppose that residents would object very strongly to the introduction of any cycle-only lanes on the existing metalled surface. We would not want Eagle Way to become another Old Felixstowe Road!</p> <p>7. There is a pre-existing and already very problematic combined cycle and pedestrian path that links the western segment of Eagle Way to the bottom of Deben Avenue, crossing Broomfield at a point where there is an absurd build-out that obstructs the road. If IM12 is constructed, what measures can and will be put in place to prevent a growth in traffic using this path? Safety is already at risk, especially at the beginning and end of a school day.</p>	
IM12	Phil Squirrell	420	No	<p>The proposal for a footbridge over the A12 IM12 is ill conceived as there is a existing bridge 400m away.</p> <p>Also the disruption to the woodland would be unacceptable. We have maintained these woods for the enjoyment of Martlesham Heath residents for over 40 years. They belong jointly to each householder on the village. The existing cycle route is more than sufficient for the village and the future development of the new village BrightwrlL Lakes.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Prue Denton	112	No	<p>I feel that the proposal severely damages the green space amenity that was given to the residents when</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from</p>

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				<p>Martlesham Heath was created. I love walking through Birch Woods (the document calls them Martlesham Woods for some reason). They have been a life saver during the pandemic as it's so calming to walk among the trees and it avoids needing to go elsewhere in a car, so it's environmentally sound. It is good for everyone's mental health to be in natural surroundings. I am appalled at the proposal to build a hard surface cycle route right across it. It will totally change the quiet ambiance, the crunch underfoot of leaves and twigs and the sunlight through the trees along winding paths. Indeed, how many trees must be cut down to create a wide route through? People of all ages need this valuable facility.</p> <p>When my children were young, they loved playing in the woods and now that we have young grandchildren, they are also safe to go and explore, imagining the Gruffalo popping out from behind a tree, finding a nice stick to carry, picking blackberries and all the other lovely things about being in a wood. During the pandemic, lots of local children and a local artist left painted stones for others to find and eventually, a beautiful pebble circle was created. New woodland treats have followed such as labels describing the different types of trees and recently, little pixie figures have appeared for exploring children to delight upon. A large man-made surface to create a fast cycle route with lighting is nothing short of criminal in these beautiful surroundings and to whose benefit? Commuters have perfectly good alternatives.</p>	<p>urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>As well as the loss of amenity, a fast cycle route will become a danger - older children commuting to/from Kesgrave High school and adults commuting to work will all go quite fast along a properly constructed cycle route, which is not sensible in an amenity where young children and elderly people are enjoying the natural environment. It is also a very popular dog walking route. These woodlands should not become a through-route, it is a green space destination for the benefit of residents and visitors.</p> <p>I feel as if planners are only considering one aspect at a time. When new developments are created, there is a need to identify or create green space. But now, having got some excellent green space for the residents of Martlesham, someone else is coming along with a different agenda for cycle routes and they are not concerned about the loss of green space amenity that will result.</p> <p>I live very near to where the new bridge is proposed to feed onto Eagle Way. I strongly object to this too - not because it's near where I live (I won't see it from my house), but because it will naturally lead people towards Birch Woods, either with a hard path, as proposed, or it would encourage people to use the unmade paths if left as they are now. Either way, it mixes cyclists with pedestrians, which is not safe and the experience of both types of user is compromised.</p>	

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				<p>I am also a cyclist - for years I have cycled from Lancaster Drive, Martlesham Heath to the Kesgrave sports hall to play badminton. I find it no problem at all to cycle round the southern side of Eagle Way to join up with the existing cycle route on the western side of the village or to head north on Eagle Way, past the mini roundabout (near the BT roundabout) and join the cycle path at the northern side of the shops. I am now in my 70s and if I can manage it, I'm quite sure younger fitter cyclists can manage this. I gather that, as part of the new McCarthy Stone development, they will be improving that northern cycle route, so it makes perfect sense to encourage cyclists to take that route to the western side of the Martlesham Heath. I wouldn't dream of cycling through the woods, even though that's the shortest route from where I live.</p> <p>The Kesgrave cycle/pedestrian path was properly designed to fit in with the housing scheme at Grange Farm. That route is also reasonably wide, with the aim of reducing accidents, although no doubt there will still be some on a shared cycle/pedestrian route. I take extra care when cycling there because pedestrians are not always careful about which lane they are walking in, especially when there is a group of people chatting or someone with young children or dogs, who may run out in front of you. When large numbers of school children are cycling together among pedestrians with pushchairs and young children, it's a problem - not all cyclists are as careful as I am.</p>	

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				<p>Birch Woods was not designed to have a similar wide cycle route built within it and it's just not a suitable location. Perfectly safe routes exist already so it's totally unnecessary. Eagle Way is a wide road and I've never felt unsafe cycling along it. If the existing route were to be improved, I would only suggest creating a cyclists' only path around the mini-roundabout for the benefit of younger cyclists, who may be less used to navigating roundabouts (although as a 10 year old living in a busy city at that time, I remember having to be very traffic aware).</p> <p>My arguments against this proposal have all related to the impact on humans. But having a hard lit path through Birch Woods must also have an adverse impact on the wildlife which lives within it e.g. the nightingales, deer and other animals.</p> <p>I feel that this scheme has been drawn up by someone who has no idea of the loss of amenity this would create for the existing community. Maybe they've never actually visited and walked the route for themselves and it's all been done on paper. It's been drawn up with an idea of creating a super highway for cyclists from Felixstowe to Ipswich with no care at all for the impact on those who need the natural woodland it will ruin.</p> <p>I most strongly object to this proposal. I hope the decision makers will take the time to read these comments and view the photos I am attaching, showing the woods throughout the seasons.</p>	

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				<p>Attachments:</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454647/PJP/-/11740405%201%20IMG%5F0352%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454648/PJP/-/11740405%202%20IMG%5F3048%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454649/PJP/-/11740405%203%20IMG%5F0289%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454650/PJP/-/11740405%204%20IMG%5F0358%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454651/PJP/-/11740405%205%20IMG%5F3423%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454652/PJP/-/11740405%206%20IMG%5F2751%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454653/PJP/-/11740405%207%20IMG%5F0355%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454654/PJP/-/11740405%208%20IMG%5F0357%2EJPG</p>	

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				<p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454655/PJP/-/11740405%209%20IMG%5F3050%2EJPG</p> <p>https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454656/PJP/-/11740405%2010%20IMG%5F6028%2EJPG</p>	
IM12	R.J.and R. Manning	309		<p>As the proposed footpaths 1M12 from the new A 12 bridge through the Birch Woods (Martlesham Woods) will involve felling at least 25 mature trees and widening the tract through the woods to accommodate both cyclists and pedestrians safely, in a time when we are all encouraged to plant trees? ?.</p> <p>Would it not be a more sensible and a better environmental decision to widen the footpath adjacent to Eagle way to accommodate cyclists, this would of course mean maintaining the width of the footpaths which must surely be simpler than maintaining a path through woodland.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Rachael Drouet	79	No	<p>This a beautiful, quiet and well preserved wood. To cut down trees seems to ride against every action most councils are doing to preserve woodlands. Wildlife will be affected by both the path and the proposed lighting. Plus this is owned by MHHL, so not sure how this can even happen.</p> <p>This should not go ahead on environmental grounds.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new

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					recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	R H Benton	664		<p>I object to the draft plans for a cycleway through Martlesham Woods (known locally as Birch Wood) for the following reasons:</p> <ol style="list-style-type: none"> 1. Destruction of a significant area of privately owned amenity woodland granted to householders of Martlesham Heath by virtue of their payment of a premium for their membership of Martlesham Householders Limited (MHHL). This woodland has taken over forty years to develop into a natural evolving wood. 2. The draft proposal requires the principal footpath access to the privately owned Martlesham Woods represented by its shareholding residents as MHH Limited to be turned from ecologically sustainable woodland into a paved and lit transit route for both cycle and pedestrian traffic. 3. The draft proposal would remove a significant area of wildlife habitat from Martlesham Woods that has taken decades to evolve and would also destroy the green corridors that are essential for maintenance of a thriving local wildlife community. 4. The draft proposal imposes on the landowner (MHHL) the additional liabilities in law of what may be considered (erroneously) by the public to be a right of way for cyclists and pedestrians and yet as privately owned land the restrictions normally applied by the highway authority for the safety of its users will not apply. This reason of legal liability is 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				regularly advanced by local authorities as an excuse for not adopting public amenities. 5. The draft proposal would pose a significant unnecessary risk to the safety of existing users of the wood - a mixed community of individuals, couples, families with young children and groups of dog walkers who are able to currently enjoy the freedom of the multiple woodland pathways without the danger of having to look out for speeding cyclists as they cross a cycle track through the wood. Eagle way provides a far safer existing alternative route that can safely distance pedestrians on a path from cyclists/other wheeled road users with adequate highway lighting.	
IM12	Richard Knights	743	No	<p>I fervently object to the proposed lit cycle/footpath IM12 across Birch Woods (not Martlesham Woods!). This is totally unnecessary as cyclists can quite easily turn right where they join Eagle Way (between Coopers Road and Lancaster Drive) and join up with IM11. Alternatively, they can go left round Eagle Way until they join IM10 thus avoiding the need for unnecessary and expensive lighting.</p> <p>It would be absolute sacrilege to decimate mature tree in the Woods which are currently capturing carbon. This goes against the Green Policy that we are all being urged to adhere to. This would destroy the natural habitats of wildlife, especially the deer.</p> <p>Also concerning is the potential for increased vandalism and litter in this beautiful area.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Richard Startin	399	No	A cycle/walking track through Martlesham Woods is absurd. This is a choice parcel of woodland with high	The importance of Birch Woods as a natural environment for wildlife and residents, free from

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				<p>bio-diversity, and a good carbon sink. There are a variety of "country-walk" routes through them which are a delightful resource to local residents and visitors. The proposed track would be destructive and intrusive.</p> <p>Cyclists on the IM12 route can use Eagle Way to bypass them. Walkers have a natural and attractive way through in any case.</p>	<p>urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Rita Starling	752		<p>I wish to place on record my strong objection to the above proposal. It is unnecessary and undesirable. These proposed cycling and walking routes will be of no benefit to residents of Martlesham Heath, and will cause untold damage to Martlesham Heath, the environment and the ecology at a time we are being encouraged to plant more trees and hedges to compensate for the disastrous results of climate change.</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Robert Bissell	227	Yes	<p>I support this in principle only.</p> <p>It would be better, cheaper and more conservation friendly, if the route skirted the Martlesham Woods (assuming by this you mean 'BirchWoods?') and followed Eagle Way from West to East. This route already exists, is already lit & avoids disturbing the flora & fauna in the woods, including the need to bring light pollution into the woods! I believe this route would only add around 100m max to the route</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and</p>

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				<p>and be the lowest cost & lowest impact on flora & fauna.</p> <p>I am pleased that the southern option along the edge of the field to the south of the heath, is no longer mentioned. If this was ever to be considered again, please look at the option of using the bridle path between the two fields between the Heath and Foxhall Road, I have not seen a horse there for many many years now!</p>	walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Robert Buckingham	675	No	<p>I am writing to register my formal objection to the proposals put forward in East Suffolk Cycling and Walking Strategy Draft.</p> <p>Of particular concern is the proposal to push a bridge across the A12, cutting through the woods between Coopers and Lancaster Drive, and create a new paved and lit cycling / pedestrian route from there through Birch Woods (which you refer to as Martlesham Woods).</p> <p>I feel that this would damage the nature of the woods which is essentially 90% unmanaged and is a very effective Carbon sink as well as providing habitat for local wildlife. Surely removing trees is no good thing given the challenges we are facing regarding climate change. How can this be a sensible or “green” course of action?</p> <p>It would also bypass the village centre yet still join the same cycle path on Eagle Way. Any saving in distance is minimal compared to using the existing bridge. Therefore in addition to the considerable</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				environmental impact, any benefits would be negligible versus the considerable implementation costs.	
IM12	Robert Fulcher	731	No	<p>Much as I applaud moves to improve cycling routes I must express my objections to two of your planned routes.</p> <p>Route IM 12 through Birch Wood is unnecessary and very damaging to wildlife and nature. The wood in spring and summer has a very high density of nest sites including many summer migrants. Many of these migrant birds are already suffering a decline in numbers. The removal of many trees and the subsequent construction of this footpath would ruin this particular part of the woods.</p> <p>The need for this path is quite unclear. Cyclists can easily use Eagle Way by adding a cycle lane. The existing paths are more than adequate for walkers to use.</p> <p>All the comments above also apply to the unnecessary part of route IM8 from near Gorseland Primary school and passing through Portal Wood.</p> <p>I hope your above proposals will be reconsidered to help safeguard our fragile environment.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Robert Power	1083	No	<p>- far less intrusive ways than the south option which would achieve the same goal without the detrimental effect to the wildlife</p> <p>- I am totally in support of walking and cycling strategies however the south option this just does</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For</p>

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				<p>not make environmental sense and also adversely effects the residents who's property's border onto the south route</p> <p>- the proposed path is against my garden and I would lose privacy and the view which was one of the main reasons for buying my property</p> <p>- please look at the north or other options that don't adversely effect the Martlesham Heath neighbourhood or environment in the way that the South option does</p>	<p>these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Robert Sadler	282	No	<p>EAST SUFFOLK COUNCIL (ESC) DRAFT CYCLING AND WALKING STRATEGY</p> <p>THE STRATEGY IS PROPOSED A SIGNIFICANT IMPACT TO MHHL LAND</p> <p>ESC PROPOSE TARMAC/CYCLE/FOOT PATHS,FELLING 25 MATURE TREES, GOING THROUGH (SSSI) LAND AND OTHER TARMAC ROUTES ON OUR LAND OWNED BY US??</p> <p>MARLESHAM PARISH COUNCIL ANNUAL REPORTS STATES WE SHOULD MODEL OUR VILLAGES AS MARTLESHIM VILLAGE (MV) IS?</p> <p>DID YOU READ THIS?</p> <p>WE CAME HERE IN YHE 80s</p> <p>TESCO WANTED TO BUILD HERE. WE VOTED AGAINST IT. SO YOU GAVE THEM PERMISSION ??</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>THEN YOU BUILD ANOTHER 50 BUILDING IN THE SAME AREA.</p> <p>NOW WE CANNOT GET OFF THE VILLAGE.</p> <p>A14 GRIDLOCKED.</p> <p>YOU HAVE NOW PASSED 41 FLATS TO BE BUILT IN THE CENTRE OF THE VILLAGE??</p> <p>WE ARE HAVING 2,500 HOUSES BUILD AT BRIGHTWELL?</p> <p>YOU ARE BUILDING AT THE POLICE HQ? NEVER TO BE BUILT ON LAND???</p> <p>PEOPLE STILL TRYING TO BUILD HOUSES ON MV LAND?</p> <p>YOU ARE DESTROYING MV WITH BUILDING AND CARS??</p> <p>A14 IS BLOCKED MOST OF THE TIME??</p> <p>YOU ARE KILLING MV??</p> <p>WHAT ABOUT THE CARBON FOOTPRINT OF MV???????</p>	
IM12	Robert Simpson	167	No	<p>I strongly object to the proposed route of IM12 through Martlesham Woods (Birch Woods)</p> <p>There is no need for this as IM11 (west of the A12)</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling</p>

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				<p>can connect with IM10 (Eagle Way) via 'The Square' using existing cycle paths.</p> <p>To carve through a woodland area with associated lighting and hard surface destroys a very special habitat and may encourage antisocial activity. The thought of this and children and dog walkers avoiding cyclists is of great concern. Please save money and the woods by adopting the above suggested route.</p>	<p>and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Robert Stallard	768		<p>Birch Woods, erroneously referred to as Martlesham Woods in the ESC strategy document, is a small woodland area enclosed by Eagle Way on the east, south and west sides and to the north opens up to the village green at the centre of Martlesham Heath Village allowing an uninterrupted access for the community to enter the woods, free of traffic. This was an integral part of Bradford Property Trust Ltd's original design concept for Martlesham Heath Village in the 1970s. Extensive tree planting took pace in the 1980s and the wood has established itself as a habitat for wild life and other eco systems which survive in a woodland environment. This is a source of pleasure and education for the community of Martlesham Heath Village who collectively own the land and subscribe to its maintenance, which is managed by Martlesham Heath Householders Ltd.</p> <p>I am not in favour of the ESC strategy document's proposed cycle/walking route IM12 cutting a swathe possibly 4 to 5 metres wide through the middle of the wood from the east side to the west side which</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>will inevitably incur the loss of countless trees, contrary to current government green policies encouraging the planting of trees. This will not only have an impact on the wild life but also create a physical barrier between the village green and the woodland. The route will introduce light and noise pollution, an element of danger which currently does not exist, and all the urban signage, furniture and discarded litter that follows. There are already cycle tracks IM13 and IM11 which are earmarked for improvement and arrive at the same location with a similar travel distance. Why is the M12 route required when it is so detrimental?</p> <p>I note in the ESC recommendation the term ‘with natural surveillance where possible’. Is the strategy suggesting the route through woods will not be a safe place and invite crime to the area? If natural surveillance is not possible is the recommendation suggesting the use of CCTV cameras on the route?</p> <p>The question of cost to build a new bridge over the A12 and a mile of illuminated track through private woodland, must also be taken into account at a time when the ESC tax payer is being asked to pay higher council tax due to underfunding existing services and infrastructure which ESC are responsible for providing.</p> <p>I appreciate the logic behind the planned route, but I believe it is unnecessary and shows very little respect to, or understanding of, the woodland and</p>	

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				its importance to Martlesham Heath Village and its community.	
IM12	Robin Guy	263	No	<p>As a martlesham heath resident I strongly oppose the proposed cycle way through Martlesham Woods (Birch Woods). The woods are owned by MHHL on behalf of residents and this route is already used for recreation by a wide number of users. Its appeal is that it is natural woodland and it must remain. The route only leads to Eagle way, takes cyclists away from the commercial hub of the village and will threaten to increase human activity on the SSSI western corridor as cyclists & walkers seek short cuts to Dobbs Lane, Kesgrave. It will only save a few minutes for cyclists and the loss of amenity does not justify support for the recommendation.</p> <p>I do however support the proposal for a bridge crossing at the south end as this will connect communities, but regret the impact on the wooded area between Coopers & Lancaster. Planning proposals must mitigate this impact.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Robin Johnson	771	No	<p>Can I register my strongest possible objections to proposal 529 for the route of IM12.</p> <p>The proposal shows an unnecessary and complete disregard for the wildlife and residents of Martlesham Heath. The tiny use of cycling on this route can easily be met by improvements to IM11 and IM13 at much lower financial and environmental costs.</p> <p>Birch woods has a good range of resident breeding birds, including Greater Spotted and Green</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide</p>

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				<p>Woodpeckers, Common Redpoll, Song Thrush, Coal Tit, Goldcrest and Greenfinch and more. It also supports migrant birds including Brambling, Redwings and Fieldfare. A number of these birds will be driven away by a wide illuminated pathway through the woods.</p> <p>Bats also feed in the woods and may be resident in the summer. Clearly an illuminated cycle path would disrupt these bats.</p> <p>Birch woods have a high amenity and health value to residents of Martlesham Heath. They are used extensively by dogwalkers and children playing. A cycle path would inevitably lead to accidents between cyclists and dog walkers, children and dogs.</p> <p>It's clear from these proposals that no analysis has been undertaken of the use of the existing A12 cycle/footbridge on route IM11. If you had, you would be aware that less than 50 cyclists a day use this bridge, with the majority pre pandemic going to BT. Very few of these cyclists travel from Ipswich. Given that there is no large employer in Kesgrave, few cyclists are going to travel westwards from Brightwell Lakes on route IM12.</p> <p>Much cheaper and less environmentally damaging options are readily available by improving routes IM11/IM13. Alternatively, a cycle route could easily be provided around Eagle Way. This would have higher benefit, as it would be of value to the residents of Martlesham Heath, whereas route IM12 is of no</p>	<p>a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>value at all.</p> <p>As a recreational cyclist, my number one ask of East Suffolk council is that you spend any available money on improving the road surfaces and drainage of rural roads. The large number of pots holes and very rough surface on many of these roads make them extremely dangerous for cyclists. Fix what you have before building new!</p> <p>In summary, I can only surmise the proposal for route IM12 is a very visible and expensive attempt by East Suffolk council to Greenwash the building of Brightwell Lakes. It has no merit and no value.</p>	
IM12	Robyn Gaffer	68	No	<p>I have spent approximately 30 years of my life living on Martlesham Heath. The area is cherished for its unspoilt heath and forest lands and this in an area frequently used by families with young children and dog walkers. Whilst I applaud the effort to make travel routes more practical and safer and the encouragement of cycling schemes, I think it is ludicrous that consideration is being given to tearing down part of the natural habitat of the area in order to use it as a cycle route. Eagle way is a very safe road, that is well lit and frequently used by cyclists without issue, I do not understand why for this small section of the route they cannot be diverted onto there rather than through the woodland. In a time when we should be trying to do all we can to save the planet, why are we considering creating a less practical route, at the cost of nature, when an existing more practical route already exists? During the winter months the path would likely be covered</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>in mud or wet leaves causing the route to be hazardous to cyclists, and as a parent of young children who frequently enjoys a leisurely walk in the area I want to continue doing so, without having to keep my children constantly at arms length for the fear of a speeding cyclist coming suddenly upon us. In cases where there are no other options but to remove nature to build paths or roads I do understand, but when there is a safer, cheaper, practical, more environmentally friendly option I feel it must be utilised. In all my years on the Heath I have never known of a cycling accident on Eagle way, I have never known of cyclists complaining it is unfit for purpose and I have never known the area to suffer traffic congestion with the exception on the two roundabout exists to the Heath at rush hour. I feel passionately this plan should not be given the go ahead</p>	
IM12	Sally Thompson	747	No	<p>I am writing to object to the new proposals to put cycle paths and walk ways on Martlesham Heath and Rushmere common.</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath. Recommendations IM1, IM2, and IM3 provide highly desirable cycling and walking routes between Ipswich and areas further east and therefore have been retained. Such routes will enable a greater number and diversity of individuals to access the common, convert</p>

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					trips to cycling and walking that might ordinarily be made by private car thereby reducing environmental impacts on people and wildlife, and help to support active and healthy lifestyles. The creation of bridleways through Rushmere Common would act to formalise cycling and walking and therefore, through careful design, reduce conflict between cyclists and pedestrians. The safety of users has been considered throughout the preparation of the Strategy, and IM1 would provide a much safer walking and cycling route for all users than currently provided for elsewhere on roads alongside vehicles. However, in order to reduce the impact of cycling and walking tracks through the Common IM3 has been amended to run along the eastern edge of the Common between IM4 and IM2.
IM12	Sally Wright	691	No		Objection noted.
IM12	Sandra Green	579	No	I object to the route through Martlesham Woods (Birch Woods) as this will significantly affect habitats in the wood and spoil this precious amenity used by many residents. Other routes are better.	The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Sandra Phillips	866	No	I would like to lodge my objection to the new plans which will put cycle paths and a connecting bridge through Martlesham Heath village for the following reasons: There are no advantages to the Martlesham Heath community to have a bridge and path that will	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the

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				<p>disturb and destroy animal life and habitat.</p> <p>The woods affected are areas that the Martlesham Heath residents use regularly there are often points of interest left by the residents for example through lockdown a painted stone trail and at Christmas a selfie with a naughty elfie point and Gruffalo Trail which the children loved. It is a concern that bringing more traffic from other areas through these woods would destroy the community feel.</p> <p>Bringing more people to the village via this route does not benefit the people of Martlesham Heath. the current bridge leads to a road access and encourages visitors to use our shopping area.</p> <p>We have no information about the amount of traffic the cycle path would attract which leaves the question as to whether it is actually required.</p> <p>If the proposed route is for the purpose of providing a shortcut to schools for young people there is a concern that they may shorten the route further by making their own pathways through other parts of the Heath Further destroying the habitat and increasing the need for Martlesham Heath residence Association to maintain and repair the environment at their cost.</p> <p>Overall I believe there is already an existing bridge which could be utilised better and provide a cheaper option and which encourages use of the village shop</p>	<p>proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				facilities without destroying the woods of Martlesham Heath.	
IM12	Scott Thomason	640	No	<p>1) In the Policy context 2.3 it states the aims as 'healthier, Happier, Greener communities' yet the route planned for IM12 will mean the felling of established trees and reduction of woodland in the area and addition of concrete / tarmac paths making the area significantly less green.</p> <p>2) It states in the recommendations that the path will be lit, Thus using more electricity and creating additional CO2.</p> <p>3) Under Recommendations 3.2 it states 'key routes between and through settlements where there are significant opportunities for modal shift to arise from improved cycling and walking infrastructure'. As of today there is no settlement in Brightwell that would register any improvement or modal shift.</p> <p>4) Even if / when a significant settlement does exist in Brightwell there is an alternative route from Brightwell through Martlesham Heath with IM13 / IM11 which will not require the expense of a new bridge nor destruction of established woodland. IM12 serves little purpose or advantage over these alternative routes.</p> <p>This would be needless expense for a pointless path which will destroy established woods currently providing a carbon sink and natural habitat for wildlife.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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IM12	S Desmond	690	No	<p>I object to the proposal of the extension of the cycling track on the Heath due to its effect on the environment I live in.</p> <p>The current footbridge provides adequate access to and from the Heath from the other side of the A12, there is little or no justification for a second.</p> <p>Felling of established trees that are not causing an issue isn't in line with current climate and environmental plans. A path through the woods would also affect the numerous dog walkers whose dogs may not be aware of cyclists in the area, like my own dog who is deaf, and could be injured when off-lead there.</p> <p>I was led to believe when I moved here that SSSI land was sacrosanct and would never be disturbed as I'm sure were other residents.</p> <p>Any lit paths on the non residential areas would also be an infringement of residents right to enjoy the current lack of light pollution we have.</p> <p>For these reasons I would not wish this proposal to go ahead.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Sexton, Alan	853		<p>I am writing this email as there is an error on the web site preventing comments.</p> <p>I am therefore having to restrict my comments to Martlesham Heath. Martlesham Heath is already served by a network of cycle paths and all that is needed is replacing of ambiguous signage and road</p>	<p>ESC was not aware of an issue with the submission of comments through the web portal (Inovem) throughout the consultation, and a large number of comments were received through the web portal throughout the consultation. The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is</p>

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				<p>markings to give cyclist correct direction.</p> <p>Even If there has been established a need for another bridge across the A12, to build a further made up path through established woodland ,wildlife and biodiversity of Martlesham Woods can only be described as vandalism. The path IM12 should make a right turn after crossing the bridge, follow Eagle Way north and join with IM11. This would achieve the same result without spoiling the existing amenity.</p>	<p>recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Simon Miles	848	No	<p>Having considered the proposed plan I am dismayed to discover that your proposal includes plans to install a new paved and lit foot/cycle bridge from the new Brightwell Lakes development onto Martlesham Heath between Coopers Road & Lancaster Drive. The wooded land between these two roads is privately owned by MHHL and is something of a wildlife sanctuary, often deers & foxes can be seen in this area and the woodland opposite.</p> <p>This would cut directly through 'Birch Woods', which in your documents you refer to as Martlesham Woods. The woods are a central part of living on the Heath and I am extremely concerned at your strategy to destroy the natural environment for us residents.</p> <p>The nature and habitat of wildlife would be detrimentally affected by this proposal, apart from the fact that it is a well-used wood by local dog walkers, photographers, residents and community</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>groups such as scouts, local nurseries and school groups. A new paved pathway would increase activity in this area resulting in raised noise as well as the intrusion of lighting to shatter the natural ambiance of the 90% unmanaged woods as they currently are.</p> <p>The woods also have the added benefit of being very effective on an ecological viewpoint by providing a carbon sink which is helping to keep CO2 at a manageable level in the atmosphere. Why destroy this to construct a man made</p> <p>There is already an existing foot/cycle bridge across the A12. This could be widened and lit if that is the proposal as it is also near existing stores and not creating another cut through separating the village of Martlesham Heath. The existing bridge has the added benefit of routing people towards all of the retail units hence reducing traffic chaos on the roads and also increasing the number of people who would be going past the village shops on the Square which would have a beneficial effect on their businesses.</p> <p>If as a previous consultation proposed the construction of a new footbridge south of the Brightwell Lakes development across farmland and around the outer edge of Martlesham Heath village would provide a direct link to this new path and would prevent cutting our village up across its heart and provide a safer and quicker route to Kesgrave and onward towards Ipswich. The narrow paths linking the closes and open spaces on the Heath are</p>	

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				not sufficiently wide enough to accommodate your proposals. There is also a proposal for a newly constructed lit tarmac route from Dobbs Lane along the back of Whinfield/Broomfield leading to Eagle Way - this would impact on land which is SSSI protected. Another alternative route would be to construct the footbridge nearer to Welham's Plantation with an ongoing path leading more directly to Dobbs Lane.	
IM12	Simon Shaw	590	No	<p>A path through Martlesham Woods has several disadvantages.</p> <p>It would increase light pollution in the woodland which has recently been demonstrated to have a negative impact on invertebrate life. It has been shown that LED lights attract almost 50% more invertebrates than sodium lighting.</p> <p>"Street lighting has detrimental impacts on local insect populations". Douglas Boyes. Science Advances. Vol 7 No 35</p> <p>"LED lighting increases the ecological impact of light pollution irrespective of color temperature" Pawson and Bader Ecological Soc. of America Vol 32 Iss.1</p> <p>It would impact on the nightingales which are present in the woods and which are on the UK red list for population decline.</p> <p>Source BTO.</p> <p>https://app.bto.org/birdfacts/results/bob11040.htm</p> <p>The cycle route would bypass the shops and doctors surgery at the The Square, Martlesham Heath.</p> <p>My own recommendation would be to improve cycle infrastructure along Eagle Way North Bound to join IM11 where it crosses Eagle way, north of the mini</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>roundabout as: It would encourage trips to the local shops to be made by bike there would be no or little increase in light pollution there is a wide verge already available there is little increase in distance to reach the west end of valiant way</p>	
IM12	Smith, Carolyn	783		<p>Thank you for consulting on your draft cycling and walking strategy; hopefully this will enable you to learn from local knowledge and prevent you from making expensive mistakes which will damage precious heathland and adversely impact well used green spaces in the Martlesham and Kesgrave areas.</p> <p>I am in favour of encouraging walking and cycling, but not at the expense of damaging scarce habitats and the loss of local amenity, all of which will occur if the routes IM4, IM9, IM12 and IM10 are constructed in their proposed form.</p> <p>Routes IM12, IM10, IM4</p> <p>Why are you proposing surfacing over and urbanising valued green spaces, important for recreation and wildlife?</p> <p>This is in direct contravention of the SC Local Plan Policy SCLP10.1 (Biodiversity and Geodiversity).</p> <p>Namely:</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<ul style="list-style-type: none"> • · the woodland to the west of Dobbs Lane, and Longstrops (IM4) • · Portal Woods (IM9) • · Martlesham Birch Woods (IM12), • · the heathland to the northern end of Martlesham Heath SSSI, and southern end of Martlesham Common (IM10) <ul style="list-style-type: none"> • Improve the existing A12 crossing to the north of the BT roundabout and improve IM13 to encourage the use of this route across the A12 instead of spending additional money on a crossing to the south. <p>Or:</p> <p>Extend IM13 southwards to a new bridge crossing over the A12 north of the Foxhall Road/Waldringfield Road roundabout, and continue on a new route through the edge of the field alongside Foxhall Road to Bell Lane, and continue along the field edge to connect with IM4 west of Bell Lane.</p> <p>Construct a bridge crossing the A12 to connect IM13 with footpath PROW6, re-route PROW6 alongside the wooded area to the south and then across to Foxhall Road, and continue eastwards along the field edge towards Bell Lane as above.</p> <p>Both of these options give an opportunity to</p>	

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				construct a wildlife corridor alongside this route, with screening trees and wildflower planting.	
IM12	Stephanie Miller	76	No	I am all in favor or improving cycling in the area - but not through a wood like this. Build a route around Eagle way, a slightly longer route, by all means. But this wood is too small for such a route without causing a huge issue to the current users and its abundance of wildlife, and impossible to do without taking down a large number of trees. Please please think carefully before proposing thi	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Stephen Buckingham	676	No	<p>I am writing to register my formal objection to the proposals put forward in East Suffolk Cycling and Walking Strategy Draft.</p> <p>Of particular concern is the proposal to push a bridge across the A12, cutting through the woods between Coopers and Lancaster Drive, and create a new paved and lit cycling / pedestrian route from there through Birch woods (which you refer to as Martlesham Woods).</p> <p>I feel that this would damage the nature of the woods which is essentially 90% unmanaged and is a very effective Carbon as well as providing habitat for local wildlife. Surely removing trees is no good thing given the challenges we are facing regarding climate</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>change. How can this be a sensible or "green" course of action?</p> <p>It would also bypass the village centre yet still join the same cycle yet path on Eagle Way. Any saving in distance is minimal compared to using the existing bridge. Therefore in addition to the considerable environmental impact, any benefits would be negligible versus the considerable implementation costs.</p>	
IM12	Stephen Denton	542	No	<p>The proposal to build a bridge landing on the woodland between Coopers Rd and Lancaster Drive and creating a route through what are known locally as Birch Woods is both unnecessary and very damaging to the environment with a significant loss of trees and habitat at a time when Martlesham has declared a Climate Emergency. Birch Woods are a valuable haven for local residents and children who can and do wander safely through the woods. It's been a godsend during lock downs. See photo of Birch Woods.</p> <p>It is a key feature of the original vision of Martlesham Heath - which incidentally has been cited as an exemplar development in two iterations of the SCDC local plan.</p> <p>The strava lines on the map show very low levels of cycle usage which are somehow used as part of a justification for creating a formal routes. The data is not consistent with the reality. I very regularly walk through the woods to get to the village green and shops and I can honestly say I very rarely see</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>cyclists in the woods and there is no tyre mark evidence of much cycling. The only regular cycling I think is on the metalled N - S route from Forest Lane to Warren Lane - probably one or two individuals living in Forest Lane cycling to work or the shops.</p> <p>Having passed through the woods IM12 would take cyclists literally "round the houses" up Eagle Way to the Control Tower and then back down to pick up IM4 which is a key strategic route being proposed for getting to Ipswich, and places in between. In reality people would cycle across the SSSI causing significant damage to this sensitive area by creating their own "desire" routes.</p> <p>This damage to the woods and SSSI, which are much valued by their "owners" ie we shareholders in MHHL (which manages the land for our benefit), will be very upset and resentful that our amenity should be so substantially changed in character. It's very dispiriting for those volunteers who have worked hard over the last 40 years to maintain, at no cost to the district council. the environment we enjoy.</p> <p>But there is an alternative - to create a fast cycle route into Ipswich using the Bridleway which runs on the north side of Welham Plantation and intercepting with IM4 further west. This fast route would service commuters to and from BT, the Business Park and Brightwell Lakes and would mean the the existing bridge (suitably widened as proposed) would have enough capacity without the</p>	

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				<p>need for IM12. Together with IM13, IM14 and IM11 residents of Brightwell Lakes would have good access to the retail areas and the existing bridge.</p> <p>Finally, it's a shame that this proposal may be causing some residents whose gardens back on to the field south of Martlesham Heath, to support IM12 because they, understandably, do not want a route built along their back fence - which was one of the options put forward in the A12 consultations document. It's understandable that people have been confused by disjointed separate consultations and very much doubt they would support IM12 if they were confident that any alternative did not run alongside their back fence.</p> <p>Attachments: https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454662/PJP/-/11768373%201%20P1050300%2Ejpg</p>	
IM12	Steve Branton	554	No	<p>Being residents of MH and regular walkers through Martlesham Woods we are strongly opposed to the proposed 'Urbanisation' of the path through the woods just to provide a short cut between the east and west sides of Eagle Way. Any construction of pathways / lighting would completely destroy the nature of the woods and just make it another rat run for cyclists travelling between Adastral Park / Brighwell Lakes and Kesgrave. If there is any need for cyclists to reach the proposed new bridge over the A12, the existing Eagle Way road should be suffice. If indeed the new bridge is build, it would be appropriate to prevent cyclists using the woodland</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and</p>

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				<p>path as a shortcut.</p> <p>If money is available to build a new bridge it might be better spent of improving the existing bridge, making it wider to cater for both cyclists and walkers (not to mention the current need for space in the pandemic).</p>	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Steven Moore	482	No	<p>I object to creating a wide path through Martlesham Woods when there is already an existing path along IM13 and IM11 which is not much longer and will avoid spoiling existing woodland. This existing route also connects with the existing cycling track through Kesgrave and Grange Farm.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Stuart Calver	345	No	<p>I wish to register my objection to the above proposed walkway/cyclepath.</p> <p>In my view the scheme is unnecessary, since a crossing already exists over the A12 and the proposed route offers only a limited benefit which is outweighed by the felling of mature trees within Martlesham Heath area.</p> <p>Furthermore, the proposal includes establishing a paved and lit route through the Birch Woods, however it overlooks the safety/security aspect of</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide</p>

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				<p>such a pathway through woodland during hours of dusk and darkness, resulting, I'm sure, in very little usage during those hours. That defeats the whole purpose of such a pathway.</p> <p>Also the proposal crosses a SSSI which should be protected at all costs.</p> <p>If the purpose is to access amenities such as schools, perhaps you should be looking at planning schools and other facilities in Brightwell Lakes.</p>	<p>a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Sue Sapsed	868		<p>Birch Woods (referred to in strategy document as Martlesham Woods) is a beautiful woodland enjoyed by many residents which, I have been told by many, contributes to their mental health.</p> <p>Any encroachment of this space by concrete paths and lighting would seriously detract from the wonderful peace to be had when walking in there.</p> <p>Another cycling and pedestrian bridge across the A12 (primarily to take account of potential residents on Brightwell lakes who will already have a beautiful environment) will increase visitor numbers in Birch Woods (already set to rise due to the unwelcome McCarthy and Stone development) and place these woods in a vulnerable position.</p> <p>Please protect Birch Woods and omit the bridge to them and the pathways through them.</p> <p>Re. maps:</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>The map for IM12 shows the footpath continuing from the woods across the SSSI and then on to join with IM10 and on to Dobbs lane.</p> <p>The SSSI is a much valued Site of Special Interest and ANY additional footfall across this area would make it even more difficult for the wildlife. In this time of climate change PLEASE avoid bringing additional people onto this area.</p>	
IM12	Sue Spencer	86	No	<p>I oppose this proposal it seems totally unnecessary and would cause wanton destruction of established woodland and scrub areas which are increasingly rare and an important habitat for birds including nightingales. A lit cycle way would change the character of the woods as a place loved by adults and children alike for safe and quiet enjoyment of nature and wildlife. Many people walk in the woods at all times of day and to destroy any part of this amenity woodland and put in a paved lit track so cyclists can speed through and save a few minutes is just not needed. The value of the woods to local residents was amplified during lockdown when so many people discovered the enjoyment of walking for the first time and the benefits to mental and physical health of being outdoors. Martlesham birch woods is a very small area so any loss of habitat would be significant.</p> <p>Eagle way is a quiet well lit road and to cycle or walk around it, even without the future improvements suggested, would be both practical and safe and only take a few more minutes.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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IM12	Susan Self	628		I am concerned that where the cycle path exits near Forest Lane on Eagle Way that this will tempt cyclists to take a short cut across a Site of Special Scientific Interest (SSSI) instead of continuing along the proposed route up Eagle Way.	The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Susan Simpson	816	No	<p>I would like to register my objection to the proposed cycle path in Birch woods at Martlesham Heath. I thought policy should be to preserve the local environment and plant more trees, this cycle route would mean trees would be destroyed and a valuable green space also destroyed. Lighting the path would also cause unnecessary light pollution which would in turn damage the habitat of local wildlife.</p> <p>This area is frequently used by local people as a route to the green and also by dog walkers, in my experience dog walkers and cyclists do not mix well.</p> <p>There is already a bridge over the A12 and this connects the village with areas outside it, it links up with Farriers Close and goes across the village to hamlets the other side and then on to Gorseland school which in turn allows access to Dobbs Lane, do we really need anymore cycle paths? I often walk the footpath next to the cycle path and very rarely see a cyclist, except for school children on the way to and from school.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				This policy does not seem to have been well thought out and I would like to strongly oppose it.	
IM12	Taylor Wimpey East Anglia (Sir/Madam)	760		<p>Cycling and Walking Strategy</p> <p>Brightwell Lakes is identified within the “Ipswich to Melton Key Corridor” and a number of the recommendations within the Cycling and Walking Strategy relate to existing connections in this part of East Suffolk. Of particular relevance is the following:</p> <ul style="list-style-type: none"> IM12 Martlesham Woods/Brightwell Lakes (Medium Priority): Linking into the improvements under IM13, the Strategy proposes another connection over the A12 into Martlesham Heath and beyond. 	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Terry Duffell	710	No	This village is characterized by its rural nature which will be largely ruined by the installation of lit and permanent cycling routes through the area: the SSSI could be endangered if people deviate from the proposed route and Birch Woods/Martlesham Woods would lose significant numbers of trees and its character if the plans are implemented.	The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Theresa Robson	755	No	<p>I wish to add my comments with regard to the proposed cycle/walking route through Martlesham Heath from the new Brightwell Lakes development.</p> <p>Whilst in agreement that a proposal regarding</p>	The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now

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				<p>improvements to cycling and walking facilities would indeed be beneficial to health and the environment, I am extremely disappointed that the planners feel that it would be appropriate to fell mature woodland and disturb areas of natural beauty and special scientific interest in order to do so! This strategy appears to be 'cutting it's nose off to spite it's face!'. Why destroy an area already naturally providing health and welfare benefits by felling trees, laying down tarmac and putting in artificial light? Are we not trying to minimise light pollution, save our natural green spaces and plant more trees to prevent more damage to our planet? The year 2022 is the Queens Platinum Jubilee and she has asked that everyone plants a tree to celebrate and to attempt to undo some of the appalling damage that has been done to our beautiful countryside.</p> <p>I have lived at Martlesham Heath for 24 years, the reason I moved here is because it was an area of beauty, of green space, fresh air and because the people who live here care about where they live and care about the habitat and wildlife that co-habit the area. Planners have a responsibility to create environmentally forward thinking developments for everyone, but not to take short cuts to do so. To go ahead with this proposal would not only be short sighted but also lazy and I implore the planners to think again and to create a more environmentally friendly proposal for everyone involved rather than use it as a box ticking exercise.</p>	<p>recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Thomas Burman	440	No	I consider this route to be unnecessary given that there is a perfectly adequate foot / cycle bridge less	The importance of Birch Woods as a natural environment for wildlife and residents, free from

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				<p>that half a mile further up, which links to foot and cycle paths all the way to Kesgrave.</p> <p>Construction of this route will have a negative impact on the local environment as it will be necessary to remove various trees and disturb nature (there are a family of foxes in this area as they visit our garden). This will impact on the properties which surround the wood between Lancaster Drive and Coopers, especially given that the proposal is to make the route diagonally through the woods (longest and most disruptive route), therefore apparently just missing the corner of my property and currently very private garden. It appears as though this bridge will provide people with a direct view into my garden and house.</p> <p>The route through Martlesham Woods appears to be carving a new walkway just a stones throw from the existing established walkways, which is a completely unnecessary destruction of woodland.</p> <p>The woodlands and nature in this area are one of the key considerations and benefits for persons wishing to live in this area.</p> <p>Additional to above, the proposed new bridge across the A12 will no doubt cause major disruption to traffic flow during construction, which is already severely built up especially during rush hours, to the point when leaving the village can be very time consuming. These proposals would no doubt result in this problem being escalated.</p>	<p>urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				A constant build-up of traffic along this stretch of the A12 will cause an increased level of pollution to the residents who's properties back on to this stretch of road, which could cause health concerns.	
IM12	Thomas Ware	706		<p>I am a resident of Martlesham Heath and was notified by post of East Suffolk's Draft Cycling and Walking Strategy on Wednesday 5th January 2021. The five day consultation period is interesting, but just enough time to pass comment on the impending changes to land owned by Martlesham Heath Householders Ltd.</p> <p>I am in principle in favour of <u>all</u> three changes of the proposed routes through the village and would welcome their adoption. I think it's vital to secure new safe, accessible, and well lit walking and cycling routes over the A12 to the proposed 2,000 new homes at Brightwell Lakes. The proposed route appears to connect well with existing walking and cycling routes and leads to the major amenities of the Heathlands Primary School. It will naturally lead a more direct route for others to cycle over the former airstrip land (now an SSSI), across Dobbs Lane, and on to route IM4 through Longstropes.</p> <p><u>Mitigation</u></p> <p>With any new development there are always going to be consequences to the existing wildlife and their habitats which is unfortunate and should be mitigated where possible. I would therefore like to propose planting enough trees to replace the 25</p>	Support for the Draft Strategy IM12 recommendation is welcomed. However, the importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				mature trees that will need felling. The location could be to the sides of the entrance of the new bridge to be built over the A12 as part of the Brightwell Lakes development. There is an existing 'green area' south of the Antares Building which could provide an ideal location.	
IM12	Tim Gill	583	No	<p>We are objecting to the IM12 proposal. We have a number of concerns re: IM12 which is proposing to create a new lit cycle route through the woods at the centre of Martlesham Heath village. Our property in Forest Lane currently backs onto Martlesham Woods and the proposed cycle route causing us many concerns.</p> <ul style="list-style-type: none"> • Security – The proposed cycle route will bring to the area a much higher volume of people which in turn will bring unwanted attention to ours and other properties which back onto the existing woodland. This is even more of a concern in light of recent burglaries that have taken place on Martlesham Heath, the cycle route will provide easier access for theft and potential damage to properties. • Privacy - Our property is currently in a very secluded and peaceful surrounding and we are currently not overlooked by anyone. Through the process of removing trees to accommodate the new path, we are hugely concerned that the proposed plans will make the view of our house very prominent to those using the cycle path 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>with direct line of sight to the back and side of our property.</p> <ul style="list-style-type: none"> Nature – The cycle path will create significant disruption to the natural beauty and nature within this area of woodland including wildlife such as birds, deer and bats. It is not necessary to create this disruption by demolishing many mature trees when there is an existing suitable route that already exists. This cycle route will bring an increase of unwanted pollution in the forms of light, noise and litter to what is currently an area of tranquility. <p>One of our key reasons for selecting our location on Forest Lane, Martlesham Heath is because of the natural beauty of the central woodland and the privacy it provides us at the back and side of our property. This proposal will have a detrimental effect on Martlesham Heath and therefore we object to the proposal of IM12.</p>	
IM12	Tim Morris	273	No	<p>I wish to register my objections to the proposed IM12 cycle and footpath crossing the A12 between Brightwell and Martlesham Heath, currently called “North Option”.</p> <p>My objections are:</p> <ol style="list-style-type: none"> There already exists an adequate crossing which directly connects the East side of the A12 with Martlesham Heath village centre; Birchwood and Gorseland schools and onward to Grange Farm etc. This route is fully paved and lit providing a safe and 	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide</p>

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				<p>secure passage. Very little would need to be done to connect the new Brightwell Lakes development with this existing route.</p> <p>2. Building a new “North Option” bridge crossing would disrupt traffic on the A12 during construction.</p> <p>3. The route would pass through woodland between Eagle Way and the A12. This woodland is left alone, nobody enters it, and is a sanctuary for wildlife and fauna. Putting a bridge and path through here would inevitably cause loss of trees and disruption to wildlife and local residents. Its protective environment would be lost for ever.</p> <p>4. The A12 road at this crossing point is level, if not higher than the surrounding land, unlike the existing crossing where the A12 is in a cutting, below the surrounding land. Therefore, a new bridge would stand out more and would have a greater impact on it’s surroundings, overlooking homes and gardens.</p> <p>5. One impact, particularly if the bridge is high, is to compromise the privacy and security of the properties backing onto the wood.</p> <p>6. Presumably the path would be lit, resulting in light pollution and discouraging wildlife.</p> <p>7. The proposed onward route through woodland across Martlesham Heath raises similar issues in terms of disruption to wildlife and fauna and light pollution (if lit). However, there are additional issues:</p> <p>8. The route bypasses the village centre and its shops. The opportunity to encourage greater footfall may well be missed and a convenient retail site for cyclists.</p>	<p>a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>9. The woods across Martlesham Heath provide a peaceful place to wander and enjoy nature at a slower pace. Putting a paved (and possibly lit) cycle path through here would destroy that tranquillity and further deplete wildlife.</p> <p>10. I don't believe the route passing through woodland, away from inhabited areas, would be a safe or comfortable place to be after dark.</p> <p>11. Where the wooded route emerges to meet the West side of Eagle Way, it is faced by the Western Corridor. The Western Corridor is an SSSI which I believe restricts development, including the construction of a cycle path (and lights). This being the case, the route would have to run alongside Eagle Way until it joined the existing cycle/footpath by Valiant Road.</p> <p>12. What would this new route achieve? It might gain 400 meters over the existing route, but at what cost? How much would it cost to build a bridge and paths? Cost of maintenance and lighting. Loss of habitat, tranquillity and privacy. Possible loss of property value and local trade. Inconvenience to motorists during construction.</p> <p>I really don't think there's an adequate justification for an additional cycle/pedestrian crossing.</p>	
IM12	Tom Boother	666		<p>Although I support more walking and cycling areas these should not be created on established woodlands or trails.</p>	<p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation</p>

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					(IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Tony Ellison	321		<p>Regarding the path through Birch Woods. I have the same concerns regarding impact to wildlife. I do agree a North link would be a benefit but I would like to see it on the either the boundary to Martlesham Heath or on the route of the existing public footpath which not very well maintained at this time (red routes below).</p> <p>I hope you would agree that my concerns are totally negative on any development and can be seen as constructive criticism on the proposed plans from the point of view of a local cyclist and concerned environmentalist.</p> <p><i>The map has not been published due to potential copyright concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Tony Greathead	725	No	I wish to register strong objections to the proposed strategy above as it relates to Martlesham Heath, particularly any felling of trees in Birch Woods/ Martlesham Woods and the creation of lit tarmac cycle/footpaths. Martlesham Heath was created as a special and unique environment with woodland to be enjoyed by walkers not turned into an urban highway. There is no need for additional cycle paths when we have a safe road in Eagle Way around the village, and as a resident since 1982 I value the care with which Martlesham Heath Householders Ltd (MHHL) manages the environment on my and other residents' behalf, and for which we all pay an annual	The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.

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				<p>subscription.</p> <p>I can understand the rationale for a new bridge over the A12 connecting the proposed Brightwell Lakes development to Martlesham Heath south of the BT roundabout, but anything which threatens to degrade Martlesham Woods (your term) or the site of special scientific interest further west from its current state is to be resisted.</p> <p>The creation of lit tarmac routes within Martlesham Heath village will only encourage further urban sprawl in this beautiful environment.</p>	
IM12	Trouse, David	707		<p>IM12 Martlesham Woods/Brightwell Lakes</p> <p>This is a bad idea due to the serious negative impact on biodiversity both in Birch Woods (called Martlesham Woods in the strategy) and on the Martlesham Heath SSSI.</p> <p>To introduce a new walking and cycling path would entail significant building work with disturbance to wildlife and destruction of habitat. Increased usage of this route and artificial lighting would also disturb wildlife in the area.</p> <p>Although the proposed route of IM12 is along Eagle Way (west) to join IM10 for onward travel towards Ipswich people would inevitably choose to use the paths across the SSSI because this would provide a shorter connection to IM10/IM4. Thus the unintended consequence of developing IM12 would be significant negative impact on the biodiversity of</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>the SSSI.</p> <p>Better alternatives to the proposed IM12 would be to improve the IM13/IM11 route, in particular improvements to the existing cycle/foot bridge across the A12. Alternatively, a route to Dobbs Lane via PROW6 and PROW46 (intensively farmed land) would avoid the SSSI and Woods hence have less impact on biodiversity.</p>	
IM12	Tsun Kan Ng	222	No	<p>I strongly object to the proposed South Option for new cycle lane and footpath (Map Reference IM12). I object strongly to having a cycle lane and footpath being laid right next to my back garden fence so passers by are able to look directly into my back garden and home. This would be a severe intrusion to my much valued privacy.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	Tsun Kan Ng	543	No	<p>I am expressing concerns with regards to the proposed new cycle and walking lane across Martlesham Heath village (IM12). I object to the proposed new path across the Martlesham Wood as this would damage the nature of the woods as a natural wild life habitats. The saving in distance that this short cut provides is minimal as compared to using the existing bridge. I believe the better option for this would be to improve walking and cycle path</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In</p>

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				alone the Eagle way and the existing bridge across A12.	its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Valerie Duffell	713	No	<p>I have NO IDEA why a route through Birch Woods has even been proposed. This would be highly damaging to our precious, green spaces which have proved so popular, especially during the recent pandemic. Birch Woods and the SSSI on the other side of Eagle Way are sought after, tranquil spaces for walkers who seek healthy exercise surrounded by trees and heathland.</p> <p>The thought of a tarmac and lit route through these beautiful areas of heathland and woods is utterly shocking and appalling.</p> <p>If anyone on the planning committee would like me to show them my regular cycling routes in this area, I will be very pleased to illustrate that the proposed route will be damaging to the environment and a waste of money.</p> <p>Upgrading & regular maintenance of existing routes will suffice. There is no need for new paths.</p>	The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Valerie Riley	155	No	<p>I am very much in favour of improved cycle and walking routes and we are very fortunate to have some good existing routes (my children cycled to Kesgrave school every day, including 6th form). But I am really concerned that this proposal seeks to cut through Martlesham Woods - an area that is highly</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For

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				valued for its safe and peaceful environment. Why is there a need to cut down trees and install lighting when we are all working towards lowering carbon emissions? What damage to the natural habitat? How can smaller children safely use the woods on foot or by bike when they will have to cross a cycle route where cyclists are likely to be travelling at speed? Have you seen the areas of the woods where children have contributed and appreciated the surroundings or are these routes based on looking at a map? Eagle Way is NOT a busy road especially with the decrease in bus service so it would make more sense to create a safe cycle route on this road. The proposed route will still join with the existing provision towards Kesgrave so better to improve the existing cycle provision from the bridge over the A12 - much less impact on the environment and preserving a much used local amenity which should be a high priority.	these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Vivienne Maunder	269	No	<p>Unnecessary, as viable alternatives are available. An existing footbridge exists.</p> <p>Unsound environmentally. The plan appears to be to rip out well loved trees from woods owned by residents at Martlesham. And to impact on the many walkers who enjoy these woods every day</p> <p>Unethical - this move would require - I assume - compulsory purchase. With no clear benefit to local residents, and all benefit to accrue to the developers of a greenfield site nearby (a plan not supported by the majority of local residents)</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and

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				Undemocratic - as no serious attempt has been made to directly communicate with those most affected (I live just yards away, yet no one at the council has seen fit to advise me of these plans)	Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Wes Carter	73	No	<p>I strongly object to the plan to build a cycle/walking path through the woods in Martlesham Heath. This is an unspoilt, natural environment that is much prized by locals as a place to walk in a peaceful setting. Building a path through these woods would destroy the tranquility and the natural environment. It is already possible for pedestrians to safely walk through these woods, although walking around them only adds a relatively short amount of time. Equally, Eagle Way already provides a suitable, safe route for cyclists and adds only a few minutes to their journey compared to a path through the woods.</p> <p>It is a waste of money to provide this route through the woods and the disadvantages of this proposal far outweigh the advantages.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	West	616	No	Adding tarmac and lit cycle ways through areas of outstanding beauty (woodland) for walkers is not acceptable. Not everyone wants to or can cycle on the proposed IM12 or IM9 cycle routes so they should not be changed. I myself am a keen cyclist but these proposals are not in the interest of Martlesham residents.	The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Whymark Nichola	697	No	I really hope that my voice is heard and that many have come to you with the same viewpoint to put an	The importance of Birch Woods as a natural environment for wildlife and residents, free from

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				<p>end to this damaging and unnecessary proposal.</p> <p>The woods were here first! Simple as that! Taking down at least 25 mature trees is disgusting. All in order to provide a bike route which is so not necessary. Once those trees are taken down they can never ever be replaced. The woodland will be ruined forever.</p> <p>Those that live in this area know all too well that there isn't a massive amount of cyclists. Why do you think that, even with the Brightwell Lakes development that this will increase to the point a flashy 4.5m wide fully lit cycle path is necessary? I think you are wishing for something that won't happen.</p> <p>Even if there is more cyclists why can't they use the roads and for kids the pavements like everyone else does. Martlesham Heath is quiet. It does not warrant this at all. The damage to natural habitat, the damage to wildlife, the unnecessary and vulgar use of money to build it. For what?</p> <p>What I find incredibly sad is that I bet most people making these decisions don't even live on the heath. In fact I am sure they don't because if they did they would not want to see the woodland disturbed and damaged.</p> <p>We own two dogs. Whilst they are always on a lead I know lots of other dog owners who let their dogs off the leads in those woods. There are also children</p>	<p>urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>very often on walks there. A bike path where people pick up speed is not going to be the best in those woods.</p> <p>I am also an avid cyclist. Very often my routes take me from Martlesham Heath through to the Brightwell area. There are underpasses under the A12 and there is already a bridge connecting the Heath to the opposite side of the A12. Why do you need to build another bridge for pedestrians/cyclists? As an avid cyclist, I urge you to reconsider this proposal. It is not needed! It is ludicrous to even suggest it.</p> <p>We, as owners of the woods should all be allowed to vote. Every single household on the Heath should be able to give their options in a voting system. If the vast majority think it's a good idea then it can be considered. If the masses say no then you should respect that.</p> <p>I am also concerned that this proposal has even being given consideration in the first place. The land is owned by us. Not the council. By the residents.</p> <p>I refer back to my original point. The trees were here first. Who are you to take that away from this area? Who are you to take away something so beautiful? To help a handful of cyclists?</p> <p>If you want to invest in something perhaps you can ensure the road infrastructure is the best it can be when the new development is built. For example the</p>	

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				<p>roads capable of taking the volume of extra traffic. It is already a nightmare trying to get off the Heath onto the A12. With added traffic it will only get worse. Despite what you think the vast majority of people living there will be driving, not cycling.</p> <p>This email is a 100% anti proposal. Please reconsider. One of the reasons we moved to the Heath was because of it's natural beauty and the woodlands play a major part in this.</p>	
IM12	William Buttigieg	376	No	<p>I would like to register my objection to the proposed cycle paths and uprooting of trees near our homes. We have been campaigning and replanting new saplings and then this happens.</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>
IM12	William Dobbie	234	No	<p>My wife and I think it would be a waste of money to build a new bridge over the A12 as there is already a route to the existing bridge which is only slightly longer. In addition, Martlesham Woods are relatively small and tend to be well used by walkers for exercise. Cyclists are often inconsiderate of walkers and this would make this amenity less desirable for them. The removal of mature trees required for a</p>	<p>The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch</p>

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				new wider path would also make the woods less desirable in a time when we need more trees.	Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM12	Zoe Abbitt	902	No	<p>Reference the new bridge over the A12(connecting the new Brightwell Lakes development with a path cutting through the woods between coopers Road and Lancaster drive) the route for the new path through Birch Woods would require felling of at least 25 mature trees. This flies in the face of government commitments to encourage the planting of trees – not the felling. Additionally the proposed path would surely damage the nature of the woods which is essentially unmanaged and is a very effective carbon sink as well as providing habitat for local wildlife.</p> <p>It would be helpful to know the reasons why the current bridge is not sufficient.</p> <p>Furthermore could there not be consideration of an eco friendly alternative to tarmac?. tarmac areas again goes against government commitments - this collides with current government proposals such as those to provide subsidies to farmers to allow the creation of wildlife areas not their destruction.</p>	The importance of Birch Woods as a natural environment for wildlife and residents, free from urbanising effects of a cycling and walking track is recognised. So too is the suggestion that a new cycling and walking bridge might not be necessary in close proximity to an existing cycling and walking bridge. For these reasons, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.
IM13	Martlesham Parish Council (Diane Linsley)	885		IM13 has been promised in the planning permission for Brightwell Lakes and the proposed improvements to the A12 corridor. As a key link from BL to the facilities of the industrial and retail parks, the rest of	In order to highlight the important role Brightwell Lakes will play in improving cycling and walking infrastructure in the area, IM31 has been incorporated into the Strategy. IM31 highlights the cycling and walking

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				<p>the community of Martlesham (via IM11 over the bridge), and through routes, it should be given the highest priority for completion before the first properties (which will be in the SW corner of BL) are occupied in 2023. Specific issues which need to be addressed include:</p> <ul style="list-style-type: none"> • A continuous footway along the eastern side of Gloster Rd - ideally as far as the bus stop next to the Mercedes van dealership, provided this can be achieved without the loss trees. • A crossing should be provided for access to Beardmore Park from the bus stop opposite the rear of Marks and Spencer. • A crossing arrangement past the BT main entrance. • A defined route through or around the “ribbon” car park abutting the BT security fence. <p>Pedestrians and disabled users would greatly benefit from a series of segregated walkways allowing them to pass from one shop to another without the need to cross a busy car park. More dropped kerbs allowing greater ease of use for those in wheelchairs or with pushchairs is required.</p> <p>More extensive secure cycle storage for users of shops and other facilities (eg bowling alley) would help encourage a modal shift.</p>	<p>infrastructure that will be delivered by Brightwell Lakes. IM13 has been amended to reflect the infrastructure improvements within IM31.</p>

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				<i>The image has not been published due to potential data protection concerns, but was still fully considered and assessed in forming the Strategy.</i>	
IM13	Martyn Shakespeare	500	Yes	Sounds generally positive. Widening and resurfacing of route parallel to A12 would be great. Please ensure this stays open during Brightwell Lakes development, there is little alternative for N/S cycle traffic.	Support noted.
IM13	Mary Trowse	702		I am pleased to see improvements proposed to Gloucester Road, but I am disappointed that the walking and cycling improvements to the Anson road/ Beardmore Park junction suggested in Community Recommendations 54, 263, 92 and 682 have been dropped. There needs to be a safe way for cyclists and walkers to get to Marks and Spencer's etc, Pets at Home and Next etc. from the underpass otherwise people like me will be discouraged from cycling or walking to the shops.	Anson Road has been incorporated into IM10, connecting the A12 underpass and Felixstowe Road. Consultation comments made to the initial consultation on the Strategy have been incorporated into the Strategy as Community Recommendations.
IM13	Moira Weaver	108	Yes	I am a cyclist and resident of Martlesham Heath. I do not support the extra crossing over the A12. If you make these improvements this will make it completely unnecessary. you need to make these improvements as the traffic round this area is appalling and rarely favourable to a cyclist. YOu also need to maintain the existing cycle paths	Support for IM13 is noted. In relation to IM12, amendments have been made to remove the recommended cycling and walking track through Birch Woods and a new cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a cycling and walking route between Kesgrave and Brightwell Lakes has been recommended, at IM32. This recommendation passes to the south of Martlesham Heath.
IM13	Nik Bestow	20	Yes		Support noted.
IM13	Prue Denton	124	No	I am in agreement with <i>some</i> of this proposal, but specifically not the second footbridge. Therefore, I must register this as an objection.	In order to provide for a more attractive cycling and walking environment throughout the retail park IM13 has been amended to ensure more cycling and walking crossing points are introduced along Gloster Road that

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				<p>I have sometimes cycled from the existing A12 footbridge to Barrack Square. The cycle lanes marked along Gloster Road aren't wide enough to feel safe. When I leave the footbridge, I usually cycle on the inner path along Gloster Road and then cross over and join the path which goes past the Adastral entrance. This is relatively easy.</p> <p>The return journey is more awkward, as you have to remember to cross the Barrack Sq road to pick up the path going past the Adastral entrance. If you don't turn off at that point, you reaching the junction with Gloster Road where it is less easy to cross with traffic coming from the A12 roundabout. I think it just needs good road markings to make the cycle route connections more obvious and encourage drivers to slow down.</p> <p>I don't think it's necessary to create a new bridge over the A12. The existing bridge would be fine if it was widened. It leads into a more sensible part of the residential side of Martlesham Heath, joining up with the cycle route north of the shops. To be clear, I specifically object to a southern bridge because the proposed route comes out opposite Birch Woods (you refer to them as Martlesham Woods) which would encouraging cycling through those woodlands, which I do not agree with. I have outlined my reasons under IM12.</p> <p>The retail park is a nightmare and very difficult to navigate on a bike and needs improvement. I have no problem cycling from Martlesham Heath village</p>	<p>reflect desire lines, and to reflect the need for more cycle parking throughout the retail park.</p>

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				<p>to Tesco via the A12 underpass, but getting around from there to the different retail areas is very difficult if not downright dangerous. Car drivers rarely give way and I usually end up having to dismount and walk my bike around rather than compete with cars and vans. This is an area which definitely needs more attention. The idea of a parallel crossing on Anson Road (presumably the one near Costa Coffee) is helpful, but this won't solve all the problems.</p> <p>Dedicated cycle paths linking each retail area are needed with safe crossings across the various roads. Cycle racks would be beneficial near each group of stores, so people can cycle from one area to another and lock their bikes up. For example, there's nowhere to lock my bike up in the M&S shopping area or the one near Next. I have often cycled over the existing bridge to the Bowling Alley (to play table tennis), but that also has no dedicated cycle racks.</p> <p>The whole retail area has not been well designed with cyclists in mind. Improving this would encourage local residents (existing and those moving into Brightwell Lakes) to use their bikes more and cut down on car traffic. I much prefer to use my bike for local journeys, but I don't like cycling there for the reasons explained.</p> <p>It would be much better to spend money sorting out cycle paths on that east side of the A12, rather than creating a new foot/cycle bridge and</p>	

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				laying an unnecessary additional cycle route through Birch Woods in Martlesham Heath.	
IM13	Smith, Carolyn	784		<p>Thank you for consulting on your draft cycling and walking strategy; hopefully this will enable you to learn from local knowledge and prevent you from making expensive mistakes which will damage precious heathland and adversely impact well used green spaces in the Martlesham and Kesgrave areas.</p> <p>I am in favour of encouraging walking and cycling, but not at the expense of damaging scarce habitats and the loss of local amenity, all of which will occur if the routes IM4, IM9, IM12 and IM10 are constructed in their proposed form.</p> <p>Routes IM12, IM10, IM4</p> <p>Why are you proposing surfacing over and urbanising valued green spaces, important for recreation and wildlife?</p> <p>This is in direct contravention of the SC Local Plan Policy SCLP10.1 (Biodiversity and Geodiversity).</p> <p>Namely:</p> <ul style="list-style-type: none"> • · the woodland to the west of Dobbs Lane, and Longstrops (IM4) • · Portal Woods (IM9) • · Martlesham Birch Woods (IM12), 	The need to avoid loss of or harm to the natural environment has been considered throughout the preparation of the Strategy. IM13 will require the widening of existing infrastructure along Gloster Road to accommodate cyclists and pedestrians, which will result in the loss of some grass verge.

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				<ul style="list-style-type: none"> the heathland to the northern end of Martlesham Heath SSSI, and southern end of Martlesham Common (IM10) Improve the existing A12 crossing to the north of the BT roundabout and improve IM13 to encourage the use of this route across the A12 instead of spending additional money on a crossing to the south. <p>Or:</p> <p>Extend IM13 southwards to a new bridge crossing over the A12 north of the Foxhall Road/Waldringfield Road roundabout, and continue on a new route through the edge of the field alongside Foxhall Road to Bell Lane, and continue along the field edge to connect with IM4 west of Bell Lane.</p> <p>Construct a bridge crossing the A12 to connect IM13 with footpath PROW6, re-route PROW6 alongside the wooded area to the south and then across to Foxhall Road, and continue eastwards along the field edge towards Bell Lane as above.</p> <p>Both of these options give an opportunity to construct a wildlife corridor alongside this route, with screening trees and wildflower planting.</p>	
IM13	Stephen Denton	552	Yes	This route must be in place before the first homes are built on Brightwell Lakes. Currently the developers have submitted plans the first phase in	It is not the role of the Strategy to dictate when cycling and walking infrastructure planned to be delivered as part of the Brightwell Lakes development should be

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				the south west corner of the site and I understand they hope first occupation will be in 2023.	implemented. IM31 has been incorporated into the Strategy to highlight the cycling and walking infrastructure that will be delivered by Brightwell Lakes. IM13 has been amended to reflect the infrastructure improvements within IM31.
IM13	Taylor Wimpey East Anglia (Sir/Madam)	759		<p>Cycling and Walking Strategy</p> <p>Brightwell Lakes is identified within the “Ipswich to Melton Key Corridor” and a number of the recommendations within the Cycling and Walking Strategy relate to existing connections in this part of East Suffolk. Of particular relevance is the following:</p> <ul style="list-style-type: none"> IM13 Barrack Square/Gloster Road (Very High Priority): this is described as the link to Brightwell Lakes through Martlesham Retail Park and to Felixstowe Road. It is a route that is already in place but the Council are looking for this to be improved through segregations and changing priorities for cyclists and pedestrians. 	In order to highlight the important role Brightwell Lakes will play in improving cycling and walking infrastructure in the area, IM31 has been incorporated into the Strategy. IM31 highlights the cycling and walking infrastructure that will be delivered by Brightwell Lakes. IM13 has been amended to reflect the infrastructure improvements within IM31.
IM14	Chris Adelson	456	Yes		Support noted.
IM14	Lesley Vince	341	Yes	Good idea to upgrade this route which does not impact on an area of woodland/heathland and has advantages to be gained.	Support noted.
IM14	Martlesham Conservation Group (Phil Smith)	899		<p>IM11 /IM14</p> <p>a) The bridge across the A12 should be upgraded as it is currently too narrow and would better support the route of IM11.</p> <p>b) The route of IM11 should join with IM14 and not cross over. Spratt’s Plantation woodland should not</p>	In order to limit harm to Spratt's Plantation IM14 has been amended to end at Betts Avenue. IM31 has been introduced to show the cycling and walking infrastructure that will be delivered through the Brightwell Lakes development, thereby linking into IF33 (now MF3).

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				<p>be totally surrounded with routes. It would be best to retain the west (industrial) side only.</p> <p>c) Why does IM11/IM14 not continue south along the east side of the BT site to link with IF33? There is a missed opportunity here.</p>	
IM14	Martlesham Parish Council (Diane Linsley)	887		<p>Similarly, IM14 in conjunction with IM11, provides important links within Martlesham; it will be the most convenient route to the retail and industrial areas and the A12 footbridge for residents in the eastern half of Brightwell Lakes. To improve these links, the existing footbridge needs to be widened, and Felixstowe Road restored to its status as a cycle priority route as mentioned elsewhere in this response.</p> <p>Having said the above, the Parish Council is surprised and disappointed that the so-called new northern quadrant route has not been proposed to carry the stretch of IM11 between IM14 and Gloster Rd. This would present the opportunity to create a purpose built route for all user types from scratch, and avoid the multiplicity of business access points along Betts Avenue, and would even be a viable alternative route to parts of the retail area and the footbridge for residents in parts of the western section of Brightwell Lakes.</p> <p>Consideration should be given to a part time pedestrian/cycle entrance into the north east corner of the Adastral Park complex to allow north bound commuters from Adastral Park to pick up IM14 thus</p>	<p>In relation to Felixstowe Road, IM10 (now IM28) recommends the introduction of a modal filter to prevent vehicle through traffic. IM11 represents an opportunity make improvements for cycling and walking, in addition to that provided for along the northern Brightwell Lakes access. In order to highlight the important role Brightwell Lakes will play in improving cycling and walking infrastructure in the area, IM31 has been incorporated into the Strategy. IM31 highlights the cycling and walking infrastructure that will be delivered by Brightwell Lakes. IM11 has been amended to reflect the infrastructure improvements within IM31. Betts Avenue and the northern Brightwell Lakes access represent similar cycling and walking routes and therefore it is not expected that they would both come forward.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				avoiding the rigours of Gloster Rd at rush hour and the staggered T junction at its northern end.	
IM14	Martyn Shakespeare	508	Yes		Support noted.
IM14	Paul Davey	328		<p>As a keen walker and cyclist I am very much in favour of ESC having a strategy for my benefit.</p> <p>However as a resident of Martlesham Heath I consider considerable concerns regarding the draft proposals published this year.</p> <p>My objections and reasons are listed below.</p> <p>5. The purpose of IM11 and IM14 (popular walking routes) is not clear when good alternatives are available.</p> <p>In conclusion I would prefer ESC to properly maintain existing footpaths and cycleways rather than spend large sums on unnecessary projects which are never used.</p>	Potential conflict between pedestrians and cyclists has been considered throughout the preparation of the Strategy, and particularly where recommendations propose upgrading footpaths to bridleways. The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.
IM14	Taylor Wimpey East Anglia (Sir/Madam)	762		<p>Cycling and Walking Strategy</p> <p>Brightwell Lakes is identified within the “Ipswich to Melton Key Corridor” and a number of the recommendations within the Cycling and Walking Strategy relate to existing connections in this part of East Suffolk. Of particular relevance is the following:</p> <ul style="list-style-type: none"> IM14 Footpaths/Felixstowe Road (Very High Priority): this route is to provide an off-road route from Brightwell Lakes to Felixstowe Road by improving the existing connections 	In order to highlight the important role Brightwell Lakes will play in improving cycling and walking infrastructure in the area, IM31 has been incorporated into the Strategy. IM31 highlights the cycling and walking infrastructure that will be delivered by Brightwell Lakes. IM14 has been amended to reflect the infrastructure improvements within IM31.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				to allow for cycling and walking on a wider, resurfaced and well lit route.	
IM15	Chris Adelson	458		The road surface on the bridge area is very lumpy.	The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot. However, the introduction of cycling infrastructure would provide an improved surface for cyclists.
IM15	David Adelson	464	Yes		Support noted.
IM15	Martlesham Parish Council (Diane Linsley)	888		<p>IM15 stretches from Crown Point to the start of IM17 which is at the junction with Sandy Lane (IM16).</p> <p>We agree fully with the assessment that the cycling infrastructure here is poor or non- existent, but the scope for improvement seems very limited and this is made worse by the sheer volume of traffic* using it. For example, cyclists using the Red Lion Hill are not safely segregated from vehicles which frequently pass too closely. For those who push their bike up the hill, the pavement in this location, is very narrow. Cyclists going downhill risk cars pulling out into their path at the Red Lion junction.</p> <p>Turning right into Sandy Lane can be a challenge at busy times for cyclists. A 20mph limit on Top Street would be worth considering if it was enforced in some way.</p> <p>We are therefore open to discussion with the authorities regarding the ideas put forward in the recommendations.</p>	IM15, in combination with IM8, IM10, IM16 and IM17, look to improve the cycling and walking environment through a number of interventions. There are narrow stretches which may limit the ability to introduce high quality cycling and walking infrastructure along Main Road and The Street, and the most appropriate detailed design solutions will need to be considered as the recommendation is taken forward.

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				<p>The only real solution would stop this road being a rat-run, which in turns depends on traffic calming measures in the Old Felixstowe Road, or making it so unattractive to drivers, that the A12 route is preferable.</p> <p>If it has already been decided that the A12 improvements scheme will work well enough to allow closure of the Old Felixstowe Road to through traffic it would have been very helpful if that information had been made available at the time of this consultation.</p> <p><i>* A survey in Feb 2019 measured 12200 vehicles per day along the Street (both directions combined) .</i></p>	
IM15	Martyn Shakespeare	510	No	Turning lane into School Lane/Bealings Road from Main Road should not be removed, provides safety for cyclists turning right. Narrow road does make it difficult to do much, but traffic now not so heavy because of bypass.	Whilst it is appreciated that turning lanes provide a refuge for cyclists, the width of the highway may require turning lanes to be removed in order to introduce cycling and walking infrastructure along the road. This will have a narrowing effect on the road and therefore likely reduce vehicle speeds, creating a safer cycling and walking environment.
IM15	Woodbridge Town Council (Greg Diaper)	433		<p>Route IM 15 – Crown Point to Sandy Lane</p> <p>Whilst this route is not within Woodbridge, it is the single proposed access route to Woodbridge along the strategy’s Ipswich to Melton corridor. It is fundamental to increasing cycle traffic between Kesgrave/Martlesham and Woodbridge/Melton. Woodbridge Town Council (‘WTC’) thus consider the route should be recategorized as very high priority.</p>	As set out in the Draft Strategy, and carried forward into the final Strategy, IM15 is a very high priority recommendation. The most appropriate detailed design solutions will need to be considered as the recommendation is taken forward to delivery.

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				<p>WTC consider it is crucial to make this section more attractive for cycle use as it is a part of longer distance inter community access.</p> <p>The highway has a significant volume of vehicular traffic. The current southbound only, non-segregated, road painted cycle lane from Sandy Lane to Crown Point works satisfactorily but there is no similar northbound lane. WTC recommend that a similar lane is added northbound and that a local 20mph limit be introduced from just west of Crown Point to just east of the Bealings Road with signs also indicating cycle lane presence to make this section safer for use both along it, and at the junctions which have significant vehicular junction movements. WTC recognises that further improvements would be difficult to incorporate.</p> <p>North of Bealings Road to Sandy Lane WTC believe that a segregated cycle path and footway should be incorporated on the eastern side of the highway as available land and minimal vehicle access off that side of the highway occur. This section has higher foot traffic due to circular walks associated with Martlesham Creek. WTC consider the development of IM16 as a walking route will increase this foot traffic markedly.</p>	
IM15	Zac Barnes	601		<p>The strategy was absolutely right to identify Ipswich to Melton as a key route – it seems like exactly the sort of route where cycling makes a lot of sense as a transport option given the distance between Ipswich and Kesgrave and Woodbridge is too long to be a reasonable walk but easily doable by bike. The route</p>	<p>Taken together IM8 and IM15 propose to offer segregated cycling and walking infrastructure between Portal Avenue and Sandy Lane, with other recommendations continuing the route west and north. There are narrow stretches which may limit the ability to introduce high quality cycling and walking</p>

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				<p>would be almost entirely contained by residential areas so would reach a large number of people.</p> <p>Despite this National Cycle Route 1 is rather disappointing north of Kesgrave and some of the proposals lack ambition. IM8 for instance has a lot of good bicycle infrastructure but switches between the north and south sides twice. This being immediately after the subway and Valley Farm Road (though there isn't any infrastructure past 52.070952°N, 1.276235°E – the majority of the route) for the route out of Ipswich; and at 52.070293°N, 1.274108°E and immediately before the subway for the route into Ipswich.</p> <p>The on-road bike lane in IM8 and IM15 remains a weak link. It is too small to allow bicycles to travel in both directions (only going towards Ipswich) and offers little protection to cyclists.</p> <p>Widening the pavement on the north side of the road to pedestrian and cyclist standard appears to be possible up to Nunn Close and may be possible on the south side between Crown Close and Felixstowe Road but due to the shortness of this section, it may not be worthwhile.</p> <p>Additionally, consideration should be given to upgrading the route to a protected bicycle lane through the installation of plastic bollards etc. on the dividing line, though the narrowness of the bike lane may make this impossible. At the very least it should be elevated from advisory to mandatory everywhere</p>	<p>infrastructure, and the most appropriate detailed design solutions will need to be considered as the recommendation is taken forward.</p>

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				<p>this is possible (white dash to white solid line).</p> <p>I have, for my own amusement, drawn up a fully off-road Kesgrave – Woodbridge cycle path using paths on the east of the A12 but this would require some kind of crossing over the B1438 at some point, ideally a bridge over the section that is in a cutting.</p>	
IM16	Alan Porter	126		<p>Sandy Lane is a steep hill as is Ipswich Road into Woodbridge. If the new path went to Woodbridge via the path by the sewage works in Sandy Lane and followed the river there would be no hills to negotiate. Cyclists hate hills. This would be a safer route. I live in Woodbridge and used to cycle to Martlesham, I hated those hills!</p>	<p>The river walk has been retained as a walking only route due to the limited space with which to introduce cycling and the significant cost that would be required to make the river walk suitable for cycling.</p>
IM16	Chris Adelson	459	Yes		Support noted.
IM16	David Adelson	465	Yes	<p>Add segregated footway alongside existing road West of railway bridge, otherwise this is not an attractive place to walk owing to short sight-lines and poor drainage.</p> <p>Add crossing on B1438 Ipswich Road close to the junction with Sandy Lane.</p> <p>Install low-level lighting, or at least "cats-eyes" on Sandy Lane to assist with walking and cycling after dark.</p>	<p>The most appropriate lighting and detailed design solutions will need to be considered as the recommendation is taken forward to delivery.</p>
IM16	Hayley Liddell	42	No	<p>I am a resident of Dukes Park (off Sandy Lane). Blocking Sandy Lane at the bridge seems like a daft move to me because:</p> <ul style="list-style-type: none"> Traffic through Woodbridge (which is already very congested) will be increased, and journey times/distances to most places 	<p>The recommendation would result in a minor detour which would have a minor effect on vehicle emissions, but would provide a higher quality cycling and walking environment which would have a positive impact on modal shift and therefore emissions. The proposal would reduce vehicle through traffic and therefore provide a much safer cycling and walking environment.</p>

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				<p>local residents are likely to go to (i.e. the A12, Martlesham Tesco, Ipswich etc.) will be extended (this isn't very environmentally friendly).</p> <ul style="list-style-type: none"> Local residents will no longer have easy access to local supermarkets (Martlesham Tesco etc.) Leaving/returning home will now be to drive down California. Both ends of this road are very dangerous - there is limited visibility, and I have witnessed several crashes, and a lot of near misses on both of these junctions (a mix of vehicles, cyclists and pedestrians), the latter on a several times a week basis. The number of cyclists/walkers using Sandy Lane is really quite minimal, especially during the rush hour (it's rare for me to see a single walker/cyclist on my daily commute). Have studies on numbers actually been done? The rat run is very much a thing, almost all of it being local taxi drivers. Would it not be better address this and perhaps implement some sort of "residents only" restrictions (which are quite successful on Cumberland Street in Woodbridge)? 	
IM16	Kirk Weir	170	No	Punitive to local residents	The recommendation would result in a minor detour.
IM16	Martlesham Parish Council (Diane Linsley)	889		The safety and utility of the corridor will be greatly improved by the closure of Sandy Lane to through vehicular traffic. The parish council has been pressing for a 30mph limit on Sandy Lane for at least six years. However, even with its closure, failure to	Felixstowe Road and Main Road are addressed by other recommendations (IM28, IM8 and IM15). IM28 proposes the same solution to that at Sandy Lane, a modal filter.

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				address the long-standing problems on Old Felixstowe Rd and traffic volumes on the A1214, mean that cycle connectivity between Martlesham and Woodbridge would continue to be hazardous and inhibit modal shift.	
IM16	Martyn Shakespeare	512	Yes	Always choose this route to get into Woodbridge from Martlesham, and reducing vehicle through traffic may be helpful. However, does a bus route not run via here (174?)	There is currently no bus route along Sandy Lane, however a modal filter would not preclude buses from using Sandy Lane in the future. A camera enforced modal filter could be introduced rather than a traditional bollard modal filter.
IM16	Moray MacPhail	178	Yes	<p>I live in Melton, and have a unit on the Martlesham Creek Estate, so I have walked, cycled, ridden a moped and driven up and down Sandy Lane for more than a decade. It is a nightmare to walk down, not too bad to ride down, and actually slower to drive down than going to Woodbridge via the Street and Ipswich Road.</p> <p>So it would make very good sense to put bollards under the railway bridge. This would allow vehicular access along all of Sandy Lane for those who need it, but reduce through traffic, so improving the road for walkers and riders.</p>	Support noted.
IM16	Paul Jordan	7	Yes	I strongly support this proposal	Support noted.
IM16	Peter Hammond	195	Yes	This is an easy win. There is no reason not to do this.	Support noted.
IM16	Robin Sanders	258	Yes	The route south and west of the railway bridge is used by a wide range of commercial and private vehicles going to the numerous businesses along this stretch. It is effectively a single lane carriageway with passing places. I find it unsuitable/unsafe at present for walking (particularly in the wet as you get splashed by vehicle passage) and at times intimidating when cycling as vehicles will try to	Two wheeled motorist vehicles cannot be prevented from using a traditional bollard modal filter as they are broadly the same width as bicycles and can be narrower than some cargo bikes. However, the modal filter could be designed as a camera enforced modal filter to allow cyclists and pedestrians as well emergency vehicles and refuse vehicles, whilst preventing all other motor vehicles. The most appropriate lighting and detailed

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				<p>pass. Whilst 4 wheel vehicle rat run traffic to/from Woodbridge will be stopped by the modal filter the remaining traffic and narrow carriageway will, in my view, still cause issues. I consider a footway as a minimum, and preferable a cycleway and footway, should be provided on the north side, raised above the road on the verge, to provide safe and splash free walking. The north side is best as this would preclude most vehicle movements across the line of travel.</p> <p>North of the railway bridge there will be little traffic until Dukes Park/California but thereafter an increasing amount. There is only a footway very local to Dukes Park. The narrow road between Dukes Park and Broomheath has poor sightlines on the bends and a near blind crest (at Broomheath) is challenging from a safety perspective. A segregated cycle/pedestrian section of the road would assist.</p> <p>Low level lighting of the whole stretch of IM 16 would be advantageous for night-time use.</p> <p>Finally can the modal filter be designed to preclude passage of 2 wheeled motorised vehicles i.e. motorbikes?</p> <p>I agree this is a good route for greater walking and cycle use.</p>	design solutions will need to be considered as the recommendation is taken forward to delivery.
IM16	Steve Liddell	105	No	<p>IM16 should not be encouraged as a cycle route. IM17 is should be the preferred route for the corridor, with IM19 and similar routes being used to take cyclists from the Outer Woodbridge route to</p>	Sandy Lane provides a more direct and desirable route into Woodbridge than other routes and is therefore very high priority. The recommendation would result in a minor detour which would have a minor effect on

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				<p>the town centre. The reasons are:</p> <ul style="list-style-type: none"> - IM16 leads into Ipswich Road. This is extremely busy and is not suitable for cyclists, as it is busy with little room for vehicles to pass cyclists. Roads in the vicinity of Old Barrack road are much quieter and should be used to access the town centre from the IM17 route - The proposed modal filter will result in residents living the vicinity of Sandy Lane, and those working/visiting the industrial estate having to drive further resulting in more emissions and therefore does not improve overall sustainability - Sandy Lane is a rat run to some extent, but as a result of the higher volumes, traffic moves quite slowly. A modal filter is likely to make the road more dangerous to cyclists as cars will not be expecting traffic to be coming from the railway bridge and are likely to be less careful watching for bikes when they leave the industrial estate - Sandy Lane is considerably more hilly and longer than the route offered by IM17. A well designed cycle route running along Top Street, to the Football Club and alongside the A12 would be a useful addition to the local facilities. Cyclists should be encouraged to use this route with segregated lanes where possible. IM16 should not be encouraged as a corridor route, as it will increase the number of cyclists using Ipswich Road which is not desirable. 	<p>vehicle emissions, but would provide a higher quality cycling and walking environment which would have a positive impact on modal shift and therefore emissions. The proposal would reduce vehicle through traffic and therefore provide a much safer cycling and walking environment.</p>

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IM16	Woodbridge Town Council (Greg Diaper)	434	Yes	<p>Route IM 16 – Sandy Lane</p> <p>Woodbridge Town Council ('WTC') agrees this is a very high priority route and agree a modal filter at the railway is appropriate. WTC support this route.</p> <p>The section west of the railway bridge is a single-track highway with passing places and will continue to have frequent commercial vehicular traffic including lorries as there are a numerous businesses along it. The steeply sloping banks, flat gradient and highway unevenness leads to drainage issues i.e., puddles during and after rain. It is thus unattractive as a walking route. WTC believe it will remain so unless a segregated footway is added. WTC recommend this is on the northbound side as there are minimal vehicles accessing across that side of the highway.</p> <p>North of the railway there will remain limited commercial traffic along the route to service the horticultural premises near the railway with increasing residential traffic north of Dukes Park. North of Dukes Park the narrow highway has short sightlines and lack of footways up to Broomheath. WTC believe this could be improved with a white-lined unsegregated combined cycle and walking lane.</p> <p>The junction with the Ipswich Road is unattractive for Woodbridge bound cyclists from a safety perspective. Further, for walkers along Ipswich Road to/from the west it is necessary to traverse, between</p>	<p>The most appropriate design, surfacing and lighting solutions will need to be considered as the recommendation is taken forward to delivery.</p>

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				<p>this position and 20 Ipswich Road, from the north side footway to the south side footway to walk on a footway into Woodbridge. For these reasons WTC recommend a crossing arrangement for cyclists and walkers be added at the Sandy Lane/Ipswich Road junction to encourage use.</p> <p>Finally, WTC recommend this route be lit with low level lighting where there is no current streetlighting.</p>	
IM17	Anonymous	276	Yes	<p>Add cycle priority boxes and cycle route priority leadign up to them at the level crossing on Wilford Bridge Road .cars try to squeeze past cyclists on this road from the level crossing to the roundabout . it should be a cyclist priority route, similar to that in Martlesham,</p> <p>widen river wall footpath for cyclists to then go direct to Woodbridge off road</p>	<p>The detailed design of recommended improvements to Wilford Bridge Road will be considered as the recommendation is taken forward to delivery. The river walk has been retained as a walking only route due to the limited space with which to introduce cycling and the significant cost that would be required to make the river walk suitable for cycling.</p>
IM17	Chris Adelson	460	No	<p>Ok up to the football club if a bridge is in place. At Grundisburgh Road come into the town and follow up Catherine Road and either back out to the A12 or use Warwick Avenue onto Bredfield Road and down into Melton Hill. Keep cyclists away from the A12 whenever possible as it's noisy and polluted. Cyclists can make use of the toilet facilities in town as these haven't been mentioned in the strategy.</p>	<p>Consideration has been given to the difficulties in introducing cycling and walking infrastructure alongside the A12 between Grundisburgh Road and Hasketon Road, and for this reason IM17 has been amended to re-route part of the route along Grundisburgh Road and Hasketon Road by making these roads one way and reallocating road space to segregated cycling and walking infrastructure. In order to increase the segregation between cyclists, pedestrians and the A12, IM17 has also been amended to make use of the existing track to the rear of Peterhouse Crescent.</p>
IM17	David Adelson	467	No	<p>Woodbridge Town Council has some very good suggestions about this.</p>	<p>There are challenges in introducing cycling and walking infrastructure along Wilford Bridge Road whilst avoiding harm to the natural environment. In order to connect</p>

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				<p>Also, Wilford Bridge Road has some narrow sections where it will be difficult to introduce off-road cycleway or footway. One idea would be to put a track through Melton Picnic Site.</p> <p>It's disappointing that the strategy doesn't consider beyond the Bromeswell Roundabout. Cycling routes to Sutton Hoo, Hollesley village and common, and Eyke are very important to getting people out of cars.</p>	the end of IM17 for onward cycling and walking it has been extended along a short stretch of Orford Road to Common Lane, which is a designated Quiet Lane.
IM17	Kevin Algar	176	Yes	This is a good idea.	Support noted.
IM17	Martlesham Parish Council (Diane Linsley)	890		This would be welcome assuming a safe arrangement where it crosses the B1438.	The B1438 is a heavily trafficked road and thus a crossing point over the B1438 will need to be safe for all users. The most appropriate crossing will need to be considered as the recommendation is taken forward to delivery.
IM17	Martyn Shakespeare	511	No	Not convinced of the need for most of route alongside A12. If I want to get from Sandy Lane to Melton I go via Woodbridge, more direct and more pleasant.	The routes through Woodbridge are likely more desirable for cyclists and pedestrians, which is reflected in their priority status. However, IM17 offers an alternative route if needed.
IM17	Peter Kerridge	603	No	<p>Nobody is going to use this protracted route to get from Martlesham to Melton they will simply continue to use Ipswich Road and Melton Hill.</p> <p>By far the best route if cycling from Martlesham to Woodbridge and Melton is along the river wall, now that really would make a difference. If done sympathetically with the environment.</p>	IM17 represents an alternative route for cycling and walking than going through Woodbridge, however more direct routes through Woodbridge will be more desirable and this is reflected in their priority status. The river walk has been retained as a walking only route due to the limited space with which to introduce cycling and the significant cost that would be required to make the river walk suitable for cycling.
IM17	Stephen Mayhew	39	Yes	Additionally the route from on the Seckford Hall side from the Seckford Road Junction with the A12 along the A12 to the second Bredfield turning is in poor condition and the hedges and undergrowth poorly maintained. It is becoming difficult to use in places	The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.

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IM17	Tracy Rogers	63		<p>I wish to make the following comments:</p> <p>The cycling strategy for Ipswich to Melton stops at the end of more urban areas and therefore disregards opportunities for increased leisure cycling.</p> <p>Although Sutton Hoo is cited as a major factor influencing the cycling strategy the plan does not include the road from the roundabout to Sutton Hoo. (B1083) This road requires widening of the pedestrian pavement and provision of a cycle path in order to encourage both pedestrian and cyclists. At the moment the pavement is not wide enough for more than one person and there is no cycle path. This oversight will keep visitors from walking and cycling to Sutton Hoo.</p> <p>How will the strategy cope with the narrow width of the Wilford Bridge Road at Melton Train station and up to the roundabout?</p> <p>I look forward to hearing your plans.</p>	<p>Introducing cycling and walking infrastructure along Wilford Bridge Road to the desired standards whilst avoiding harm to the natural environment will be challenging. A route between Willford Bridge Road has been added to the Key Corridor, recommendation IM30, connecting to Sutton Hoo.</p>
IM17	Woodbridge Town Council (Greg Diaper)	435		<p>Route IM 17 – Outer Woodbridge</p> <p>Woodbridge Town Council ('WTC') consider IM 17 should be the primary strategic route for cyclists travelling to/from Ipswich Kesgrave/Martlesham to/from Woodbridge and Melton. As such it should be recategorized as very high priority and is supported by WTC.</p> <p>WTC recommend ESC examine further new links to</p>	<p>Consideration has been given to the difficulties in introducing cycling and walking infrastructure alongside the A12 between Grundisburgh Road and Hasketon Road, and for this reason IM17 has been amended to re-route part of the route along Grundisburgh Road and Hasketon Road by making these roads one way and reallocating road space to segregated cycling and walking infrastructure. In order to increase the segregation between cyclists, pedestrians and the A12, IM17 has also been amended to make use of the</p>

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				<p>this route from Woodbridge to aid access. Specifically, WTC’s view is a new route via Clare Avenue and Warren Hill Road and Ipswich Road/Cumberland Street and Cherry Tree Road would be particularly attractive to walkers and cyclists. (see attachment). It would have less issues than the proposed route IM 18.. The section of Ipswich Road/Cumberland Street that is the B1438 is currently being assessed with SCC to be a 20mph zone.</p> <p>IM 17 between Seckford Hall Road and B1079 roundabout could usefully be diverted for much of its length onto the access track behind Peterhouse Crescent which extends all the way to Bilney Road rather than being immediately alongside the A12. It would be more attractive and safer being more remote from the A12. There would be minimal impact on the mature trees than the ESC proposed route beside the A12 would have. This current access track is little used and unattractive with some fly tipping. It also has a track linking it to Peterhouse Crescent directly in line Clare Avenue.</p> <p>WTC consider the section of IM 17 from the B1079 to Hasketon Road will be particularly difficult to improve to make it safer. There are numerous vehicular crossings to properties, a parking area and the entrance/exit from the Shell filling station along this stretch. Driver’s attention will remain more on joining/leaving the A12 than on cyclists or walkers. It would be an unattractive section of IM 17 which may impact use of this route. WTC recommend that this</p>	<p>existing track to the rear of Peterhouse Crescent. In addition, IM19 has been removed from the Strategy and replaced with IM29 which introduces a route from IM17 to Ipswich Road along Peterhouse Crescent, Clare Avenue and Warren Hill Road.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>section be removed and the bulk of proposed IM 21 forms part of the IM 17 route.</p> <p>WTC request consideration is given at the B1079, or close by, for IM 17 to include access for cyclist and walkers to the west of the A12 particularly to the garden centre and associated businesses. WTC suggest ESC incorporate a crossing at this location in its strategy. WTC note a crossing at this location may form part of SCC's A12 improvements proposals.</p> <p>Whilst outside the boundary of Woodbridge, WTC consider ESC will have considerable difficulty with incorporating a cycle lane down Woods Lane given the recent new footway is sub-standard in width, due to land constraints and the highway is sub-standard in width. An alternative route linking to IM 22 via Warwick Avenue and Bredfield Road should be considered.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
IM18	Chris Adelson	461	No	<p>Yes to secure storage at the station.</p> <p>No to using the Kingston field route because it conflicts with existing use of the area and is indirect. With the proposed 20mph speed limits in the town, the direct route to the station would be via Ipswich Road. Extra lighting in a countryside setting. Loss of trees from Porter's Wood. Extra traffic across the</p>	<p>Consideration has been given to amending IM18 to avoid Kingston Fields and the associated community facilities by re-routing IM18 along Beech Way, Cherry Tree Road, Kingston Farm Road and Jetty Lane. However, in order to avoid harm to Porter's Wood as well as Kingston Fields community facilities IM18 has been amended to run along Ipswich Road, Cherry Tree Road, Kingston Farm Road, and Jetty Lane before</p>

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				already busy car park area near the skate park. To name a few...	entering the car parks and arriving at Woodbridge Railway Station.
IM18	Kirk Weir	171	No	Strong objection. This is a cycling route through existing quiet woodland/rural/agricultural/park/play areas. Creating a through route to Woodbridge Station is of benefit to virtually no-one at the expense of severe disruption to the amenity value of all of the above.	Consideration has been given to amending IM18 to avoid Kingston Fields and the associated community facilities by re-routing IM18 along Beech Way, Cherry Tree Road, Kingston Farm Road and Jetty Lane. However, in order to avoid harm to Porter's Wood as well as Kingston Fields community facilities IM18 has been amended to run along Ipswich Road, Cherry Tree Road, Kingston Farm Road, and Jetty Lane before entering the car parks and arriving at Woodbridge Railway Station.
IM18	Martyn Shakespeare	514	Yes	Like this idea. Pity it has to start at the top of a nasty hill climb! Any chance of routing it along railway from Sandy Land rail bridge?	Consideration has been given to amending IM18 to avoid Kingston Fields and the associated community facilities by re-routing IM18 along Beech Way, Cherry Tree Road, Kingston Farm Road and Jetty Lane. However, in order to avoid harm to Porter's Wood as well as Kingston Fields community facilities IM18 has been amended to run along Ipswich Road, Cherry Tree Road, Kingston Farm Road, and Jetty Lane before entering the car parks and arriving at Woodbridge Railway Station.
IM18	Michael Farahar	722		Although included as "very high priority" I have doubts as to the practicalities of IM18, however desirable it may be.	Consideration has been given to amending IM18 to avoid Kingston Fields and the associated community facilities by re-routing IM18 along Beech Way, Cherry Tree Road, Kingston Farm Road and Jetty Lane. However, in order to avoid harm to Porter's Wood as well as Kingston Fields community facilities IM18 has been amended to run along Ipswich Road, Cherry Tree Road, Kingston Farm Road, and Jetty Lane before entering the car parks and arriving at Woodbridge Railway Station.

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IM18	Woodbridge Town Council (Greg Diaper)	436		<p>Route IM 18 – Sandy Lane to Woodbridge Station</p> <p>Woodbridge Town Council ('WTC') has numerous concerns with this route, that the strategy designates as very high priority. WTC does not support this route. The route is proposed as a lit segregated track which, if compliant with Department of Transport IT1/20 design guidelines would be a minimum of 4m wide. WTC see the following specific issues.</p> <ul style="list-style-type: none"> • The route traverses the southern edge of Porters Wood over a significant length. It will require removal of numerous trees in an area of woodland used and valued by a large number of Woodbridge residents. • The route has severe gradients at the top of the valley slope which would be unattractive to both cycle riders and walkers unless slackened by significant earthworks. • The route from halfway down the valley slope and along the western edge of the railway is prone to flooding. The government surface flooding map (see attached) show a high risk of flooding. Additional maps at https://check-long-term-flood-risk.service.gov.uk/map also show this section of the route has high risk flood water velocities and depths of floodwater 	<p>Consideration has been given to amending IM18 to avoid Kingston Fields and the associated community facilities by re-routing IM18 along Beech Way, Cherry Tree Road, Kingston Farm Road and Jetty Lane. However, in order to avoid harm to Porter's Wood as well as Kingston Fields community facilities IM18 has been amended to run along Ipswich Road, Cherry Tree Road, Kingston Farm Road, and Jetty Lane before entering the car parks and arriving at Woodbridge Railway Station.</p> <p>In relation to connecting IM17 into Woodbridge, a new recommendation (IM29) has been introduced to the Strategy, following Peterhouse Crescent, Clare Avenue, and Warren Hill Road.</p>

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				<p>between 300 and locally over 900mm, also deemed high risk. WTC consider substantial earthworks and drainage works would be essential to make this a safe route.</p> <ul style="list-style-type: none"> • The fields are grazed by cattle; fencing, gates and crossings would be essential. • The routes traverses Kingston Field. The Field is constrained by covenant to recreational use only within the land transfer to WTC. Further the alignment of the route conflicts with the current use of the area specifically <ul style="list-style-type: none"> ○ WTC’s developing proposals for a community garden, ○ the new basketball area ○ the mini football area ○ cuts across the entrance to the highly used children’s playground ○ cuts across the entrance to the Pavilion, ○ would require a number of mature trees to be removed close to Jetty Lane, ○ fails to incorporate use of the new crossing to Jetty Lane taking a technically difficult and unacceptable alignment across the entrance to the allotments. • WTC have concerns about the safety of a cycle track that traverse the heavily used 	

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				<p>Station car park where there is considerable foot traffic crossing the proposed track alignment.</p> <p>Further the development of this proposed route would be costly in terms of land purchase and engineering works required.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
IM19	Chris Adelson	466	No	<p>There is already a crossing point on B1438 at the junction with California.</p> <p>Old Barrack Road - Cycling and walking track needs to be continuous throughout the length of the road otherwise it's not worth doing. This is the usual way into town for people who have not come in on Ipswich Road. You would not naturally head down Warren Hill Road and Portland Crescent. While Warren Hill Road has plenty of room the suggested route makes no sense. Adding a footway to Portland Crescent would only be good if the grass areas are untouched. Meadow Walk, as you call it, is not suitable for cycling. The pathway is narrow and sloping, is enjoyed as a dog walk and is a wildlife corridor. There is a very sharp blind bend where the path narrows into a long alleyway which is suitable for walkers and has private properties on both sides.</p> <p>The shop on Old Barrack Road attracts a lot of</p>	<p>Due to safety concerns in relation to school children and the limited space along the route with which to incorporate high quality cycling and walking infrastructure IM19 has been removed. However, the recommendations associated with Quay Street have been retained within IM20.</p>

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				<p>parking. Cycle racks outside the store and limited parking might encourage people to shop on foot or bike. I know for a fact people drive very short distances to shop here.</p> <p>The alternative route of Seckford Street is preferable especially when the speed is limited to 20mph.</p> <p>Wider pavements on Quay Street are essential.</p>	
IM19	David Adelson	468	No	<p>The proposed route is not useful. Instead the strategy should look to improve the route from Old Barrack Road to Market Hill along Seckford Street, and from Market Hill to the station.</p> <p>The proposed 20mph limit, if enforced along Seckford Street, will make this route much better to use.</p> <p>The location of the Co-op store on a narrow section of Old Barrack Road near a very wide section can cause problems with poor sight-lines and cyclists getting hit by car doors opening. I suggest using the wide part of the road to provide parking for the shop, and not allowing parking immediately outside the shop.</p> <p>Church Street is one-way (uphill), but this is ignored by some cyclists. As it provides a direct route between Market Hill and Quay Street - and then to the station - I suggest bringing in an official cycling lane in the downhill direction, which would have to Give Way to traffic coming uphill at the narrowest point, and widening the footways. This probably</p>	<p>Due to safety concerns in relation to school children and the limited space along the route with which to incorporate high quality cycling and walking infrastructure IM19 has been removed. However, the recommendations associated with Quay Street have been retained within IM20.</p>

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				<p>means losing some parking bays - it might be possible to compensate on Market Hill as people frequently park on double-yellow lines there, seemingly causing no problem.</p> <p>The suggestion to have Quay Street one-way (uphill), with wider footways and cycling allowed downhill, is very welcome however.</p>	
IM19	Kirk Weir	172	No	There is no reason why cyclists using the existing footpaths across Fen Meadow and Fen Walk should not dismount and walk. Are you really proposing new ROW across the grounds of an Infant/Junior School? I can see the EADT Headline "Council proposes Paedos Paradise".	Due to safety concerns in relation to school children and the limited space along the route with which to incorporate high quality cycling and walking infrastructure IM19 has been removed. However, the recommendations associated with Quay Street have been retained within IM20.
IM19	Martyn Shakespeare	516	Yes		Support noted.
IM19	Peter Hammond	192	Yes	Quay street should have one way system same as Church street	Due to safety concerns in relation to school children and the limited space along the route with which to incorporate high quality cycling and walking infrastructure IM19 has been removed from the Strategy. However, the recommendations associated with Quay Street have been retained within IM20.
IM19	The Seckford Foundation (Richard Stone)	177	No	<p>This recommendation is totally unsuitable for the following reasons:</p> <ul style="list-style-type: none"> This proposed alternative route presents us with a major safeguarding concern. The Prep School is a secure site during the school day (The gates at Cumberland Street and Market Hill are routinely locked and the only way to enter the site is via reception, which itself is locked). We have a very strict policy regarding this. This proposed 	Due to safety concerns in relation to school children and the limited space along the route with which to incorporate high quality cycling and walking infrastructure IM19 has been removed. However, the recommendations associated with Quay Street have been retained within IM20.

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				<p>alternative route would require us to open our site. To be clear – this means that members of the public could walk onto school grounds where there are children as young as 4. The proposed alternative route goes through areas regularly used by the children. As I am sure you can understand, there are potentially catastrophic consequences of having unfettered access to the Prep School grounds. Should a safeguarding incident occur, there would be serious ramifications for both the council and the school – it would not be a stretch to say that a serious safeguarding incident by a member of the public legitimately being on school grounds could be enough to cause parents to vote with their feet and a school to close.</p> <ul style="list-style-type: none"> <li data-bbox="896 925 1411 1181">• We do not believe that IM19 provides a “significant opportunity for modal shift to arise from improved cycling and walking infrastructure”. At best it provides an alternative. There are other options – for example through Fen Meadow and along Seckford St which would be broadly the same distance. <p>I note that a number of primary schools have recently successfully applied to have established rights of way through their grounds re-routed for safeguarding concerns, therefore the suggestion that one could be created through a primary school</p>	

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				which has never had one previously, goes against common sense and recent trends.	
IM19	Woodbridge Town Council (Greg Diaper)	437		<p>Route IM 19 – California to Quay Street</p> <p>Woodbridge Town Council ('WTC') is of the opinion that this route is unlikely to be successful in attracting cyclists and walkers. It is far from direct and has the following other unattractive features</p> <ul style="list-style-type: none"> • The crossing of Ipswich Road is at location with considerable vehicular traffic movements both along, and on and off, Ipswich Road posing risks to cycling and walking users. • The section along the east side of Fen Meadow is too steep for cyclists and ends in a blind corner with Fen Walk. • To have adequate width the current attractive tree and hedge lined path along the east side of Fen Meadow would be severely impacted • The section along Fen Walk cannot be widened to provide adequate width. • The ESC alternative proposal to using Fen Walk, taking the route through the Abbey junior school grounds and entrance, has safeguarding issues 	Due to safety concerns in relation to school children and the limited space along the route with which to incorporate high quality cycling and walking infrastructure IM19 has been removed. However, the recommendations associated with Quay Street have been retained within IM20.

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				<p>WTC however consider the proposals for Quay Street and Cumberland Street have considerable merit. If combined with the alternative route we propose in our comments on IM 17 these aspects of the route would be welcomed.</p> <p>If ESC wishes to continue to develop this route WTC recommend that the section from Warren Hill Road to Cumberland Street is amended to use the full length of Warren Hill Road and then Ipswich Road to Cumberland Street.</p>	
IM20	Anonymous	274	Yes	<p>I would support cycling both ways through the thoroughfare and much more limited vehicular access to the thoroughfare .</p> <p>I would support cycling boxes at the junction with Lime Kiln Quay cyclists going straight on or turnign right from Melton are very vulnerable .</p>	<p>Consideration has been given to cycle access to the Thoroughfare, however IM20 has been amended to introduce cycle parking at either end of the Thoroughfare to encourage pedestrian use of the Thoroughfare. IM20 has also been amended to incorporate the IM19 recommendations to Quay Street, and incorporate an additional route between Melton Road, the former Council offices site, and Hamblin Road car park. The priority attributed to IM20 has been increased from 'high' to 'very high'.</p>
IM20	Chris Adelson	495		<p>Access to the Thoroughfare is already restricted but signage and past habits mean that people are still very unclear on what is allowed.</p> <p>More cycle storage is to be welcomed.</p> <p>The junction at the north end is very difficult for cyclists and there is not enough room for advanced stop lines if the left turn on the lights is to stay. Melton Hill is narrow but could use be made of other routes including the land where the old council offices are currently unoccupied.</p>	<p>Due to the constraints along Melton Hill and the northern end of the Thoroughfare it will be challenging to introduce cycling and walking infrastructure. Consideration has therefore been given to alternative routes, and a route between Melton Hill, the former Council offices side, and Hamblin Road car park has been incorporated to IM20. IM20 has also incorporated the IM19 recommendations to Quay Street, as well as cycle parking at either end of the Thoroughfare. The priority attributed to IM20 has been increased from 'high' to 'very high'.</p>

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				Where the road widens the road surface is very bumpy which means cyclists take the line more to the middle of the road. A route that is only partial down this road is a waste of time. It needs to be a through route. Look into alternatives behind Old Maltings Approach.	
IM20	David Adelson	469	No	<p>The shopping "high street" section of Thoroughfare is not suitable for through cycling traffic owing to the heavy pedestrian use. It would be best to keep it as it is, with cyclists allowed to weave through the pedestrians in the south-westerly direction only.</p> <p>Instead, cycling traffic should be on a segregated track alongside Quayside as commented elsewhere.</p>	Due to the constraints along Melton Hill and the northern end of the Thoroughfare it will be challenging to introduce cycling and walking infrastructure. Consideration has therefore been given to alternative routes, and a route between Melton Hill, the former Council offices side, and Hamblin Road car park has been incorporated to IM20. IM20 has also incorporated the IM19 recommendations to Quay Street, as well as cycle parking at either end of the Thoroughfare. The priority attributed to IM20 has been increased from 'high' to 'very high'.
IM20	Kevin Algar	175	Yes	Though i support this in principle, a lot of residents would have nowhere to park along Melton Road.	Consideration has been given to parking along Melton Road, and while this does provide a challenge to the introduction of cycling and walking infrastructure, it could be feasible to meet the needs of all users of Melton Road. Due to the constraints along Melton Hill and the northern end of the Thoroughfare it will be challenging to introduce cycling and walking infrastructure. Consideration has therefore been given to alternative routes, and a route between Melton Hill, the former Council offices side, and Hamblin Road car park has been incorporated to IM20. IM20 has also incorporated the IM19 recommendations to Quay Street, as well as cycle parking at either end of the Thoroughfare. The priority attributed to IM20 has been increased from 'high' to 'very high'.

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IM20	Kirk Weir	173	No	Thoroughfare is already, and successfully, closed to all non-pedestrian traffic including cyclists. There is an existing and well used footpath from Sun Lane and across Pytches Road and down to Turnpike Lane and hence across Melton Playing Field for those wishing to avoid walking on the busy Melton Road. The problem is for cyclists on Melton Road on which parking on both sides limits the width of the highway and is a real danger to cyclists. Residents (especially at the Woodbridge end) and their visitors have little alternative to parking on this road. Could you envisage a low speed cycle track along Turnpike Lane and through the playing fields	Due to the constraints along Melton Hill and the northern end of the Thoroughfare it will be challenging to introduce cycling and walking infrastructure. Consideration has therefore been given to alternative routes, and a route between Melton Hill, the former Council offices side, and Hamblin Road car park has been incorporated to IM20. IM20 has also incorporated the IM19 recommendations to Quay Street, as well as cycle parking at either end of the Thoroughfare. The priority attributed to IM20 has been increased from 'high' to 'very high'.
IM20	Martyn Shakespeare	517	Yes	Make Thoroughfare (and Quay Street) bi-directional for cyclists if possible (not clear if that's included); would require modification to traffic lights.	Consideration has been given to cycle access to the Thoroughfare, however IM20 has been amended to introduce cycle parking at either end of the Thoroughfare to encourage pedestrian use of the Thoroughfare. IM20 has also been amended to incorporate the IM19 recommendations to Quay Street, and incorporate an additional route between Melton Road, the former Council offices site, and Hamblin Road car park. The priority attributed to IM20 has been increased from 'high' to 'very high'.
IM20	Peter Hammond	193	No	On street park is essential on Melton Hill, properties have no off-street parking. Do not remove parking for properties. Traffic calming is required on Melton Road , the road is long and wide and there are regular issues with severe speeding and overtaking. Two pedestrian crossings (electronic signal crossings) should be available for crossing Melton	Consideration has been given to parking along Melton Road, and while this does provide a challenge to the introduction of cycling and walking infrastructure, it could be feasible to meet the needs of all users of Melton Road. Due to the constraints along Melton Hill and the northern end of the Thoroughfare it will be challenging to introduce cycling and walking infrastructure. Consideration has therefore been given to alternative routes, and a route between Melton Hill, the former Council offices side, and Hamblin Road car

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				Road near to Spar shop (garage) and from Melton playing field to shop opposite (formerly Springside). Next best solution would be crossing 'islands' in the middle of the road.	park has been incorporated to IM20. IM20 has also incorporated the IM19 recommendations to Quay Street, as well as cycle parking at either end of the Thoroughfare. The priority attributed to IM20 has been increased from 'high' to 'very high'.
IM20	Peter Hammond	194	Yes	<p>Walking from Melton to Woodbridge can be improved:</p> <p>Clear trees and bushes that are causing obstructions on pavements on both sides of Melton Hill and Melton Road.</p> <p>Improve / repair pavements along Melton Hill and Melton Road.</p> <p>Add pedestrian crossings (zebra crossings) to allow safe crossing of roads at Old Maltings Approach, Deben Meadows (new development), and Dock Lane.</p>	The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance cannot.
IM20	Rachel Smith-Lyte	641	Yes	<p>Advance stop lines are essential at both ends of Melton Road. As a cyclist, many times I have sat at the traffic lights at the Woodbridge end, heading south and waiting to enter the Thoroughfare feeling extremely vulnerable at the head of a car queue, with oncoming traffic heading towards Melton and therefore me! I wonder whether a cycle lane should be inserted down the middle of Melton Road rather than to one side? Otherwise cyclists may find it difficult, on approaching Melton crossroads, to manoeuvre easily into the middle of the junction if we want to turn left towards Wilford Bridge. This should also mean that there doesn't need to be two</p>	Cycling infrastructure is generally best avoided in the middle of streets owing to the difficulty in accessing the infrastructure and the potential conflict with vehicles on both sides. Due to the constraints along Melton Hill and the northern end of the Thoroughfare it will be challenging to introduce cycling and walking infrastructure. Consideration has therefore been given to alternative routes, and a route between Melton Hill, the former Council offices site, and Hamblin Road car park has been incorporated to IM20. IM20 has also incorporated the IM19 recommendations to Quay Street, as well as cycle parking at either end of the Thoroughfare. The priority attributed to IM20 has been increased from 'high' to 'very high'.

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				cycle lanes on either side - i.e. one heading north and one heading south.	
IM20	Woodbridge Town Council (Greg Diaper)	438		<p>Route IM 20 – Thoroughfare/Melton Road</p> <p>Woodbridge Town Council’s (‘WTC’) view is that a route between the centre of Woodbridge and Melton should be very high priority and not high priority as the draft strategy proposes.</p> <p>WTC would wish to clarify that the road between the traffic lights and Pytches Road is the northern part of the Thoroughfare and thus the draft strategy recommendation incorrectly refers to the traffic lights being “north of the Thoroughfare”.</p> <p>WTC, whilst highly supportive of more cycling, do not wish to see the Thoroughfare between the traffic lights and Cross Corner as a busy cycle route due to the considerable foot fall and cross pedestrian use. WTC see this area as a destination with cycle parking adjacent to it. To that end WTC is about to add cycle racks at Selwyn Gardens, adjacent to the traffic lights. WTC welcomes the Strategy’s intention for further cycle storage, but this should be adjacent to, but not along, this part of the Thoroughfare.</p> <p>WTC’s view is that the proposed route is also compromised by</p>	<p>Due to the constraints along Melton Hill and the northern end of the Thoroughfare it will be challenging to introduce cycling and walking infrastructure. Consideration has therefore been given to alternative routes, and a route between Melton Hill, the former Council offices side, and Hamblin Road car park has been incorporated to IM20. IM20 has also incorporated the IM19 recommendations to Quay Street, as well as cycle parking at either end of the Thoroughfare. The priority attributed to IM20 has been increased from ‘high’ to ‘very high’.</p>

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				<ul style="list-style-type: none"> <li data-bbox="891 316 1413 475">• The narrow highway between Old Maltings Approach and Pytches Road which is unattractive to cyclists given the considerable vehicular traffic and, in particular, lorry traffic. <li data-bbox="891 517 1413 836">• The traffic lights and the adjacent highway to the north is not a safe environment for cyclists. The proposed advance stop lines would place cyclists in an exposed position regarding traffic turning into the Thoroughfare towards Melton from Lime Kiln Quay Road and would encourage cyclists to weave through traffic or divert onto the eastern footway in this narrow section to get to the advance stop lines. <p data-bbox="846 884 1413 1398">WTC recommend that at Old Maltings Approach or preferably New Quay Terrace and New Quay Lane, the route should divert off Melton Road to cross onto ESC's land to reach Deben Road and then onto Lime Kiln Quay Road with a dedicated crossing at that junction. A segregated cycle track could be formed along the current footpath of Quayside to the Hamblin Road lights with foot traffic directed up to Selwyn Gardens to either go via Elmhurst Park to Hamblin Road or the Thoroughfare south of the traffic lights. Current foot traffic on the Quayside footway is minimal. Cycle traffic to/from Woodbridge Station from/to Hamblin Road could use the current pedestrian crossings and then the B1438. WTC's is seeking, with SCC, to make this stretch of the B1438 a 20mph zone.</p>	

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				<p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	
IM21	David Adelson	470	Yes		Support noted.
IM21	Woodbridge Town Council (Greg Diaper)	439	Yes	<p>Route IM 21 – Farlingaye High School</p> <p>Woodbridge Town Council ('WTC') support this route and consider that, from a local perspective, this route is of very high priority. WTC would wish to see its implementation as soon as possible to address the safety issues with school children along the Hasketon and Grundisburgh Roads part of the route.</p> <p>WTC in its comments on IM 17 consider this route would be a preferable link between the B1069 and Hasketon Road for that stretch of the IM 17 Outer Woodbridge route due to safety concerns.</p> <p>WTC suggest that to ameliorate traffic queuing at peak school times and issues with traffic filtering in from Bilney Road, Grundisburgh Road be made one way eastbound and Hasketon Road one way westbound. The latter could be aided by linking the pedestrian crossing lights on the A12 just to the north of Hasketon Road, to new traffic monitors on Hasketon Road, to allow traffic to filter out safely onto the A12 southbound.</p>	There is limited space to introduce cycling and walking infrastructure alongside the A12 between Grundisburgh Road and Hasketon Road, and for this reason the recommendations for Grundisburgh Road and Hasketon Road have been incorporated into IM17.
IM22	Chris Adelson	501		<p>Trees should be retained and new planting added. Lighting should be timed so that the wood remains</p>	In order to avoid harm to the woods IM22 has been amended to remove cycling and walking route through the woods and instead introduce cycling and walking

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				dark over night so that birds etc. are not disturbed by light pollution.	infrastructure along Saxon Way, Melton Grange Road, Green Man Way, Turnpike Lane and Hackney Road.
IM22	David Adelson	471	Yes		Support noted.
IM22	Melton Parish Council (Pip Alder)	1084		<p>At the meeting of Melton Parish Council's Planning and Transport meeting last night concern was raised about one of the proposed routes in the cycling and walking strategy. There is a key route that takes people down Bredfield Road and through Burkes Wood and across the playing field onto Melton Road. Councillors are very concerned about the creation of a cycle route through the woods. The area is a conservation area and we already discourage cyclists from using that route as they cause damage to flora and fauna and the route is not suitable for cyclists. We have had reports of cyclists coming through the woods and colliding/having near collisions with walkers and dogs. The route also continues across the playing field which is in fact two football pitches. The Council is very supportive of safer routes for pedestrians and cyclists but do not feel this route is suitable for a cycle route.</p> <p>We appreciate the consultation has closed but would like our concerns to be considered and added to the responses.</p>	In order to avoid harm to the woods IM22 has been amended to remove cycling and walking route through the woods and instead introduce cycling and walking infrastructure along Saxon Way, Melton Grange Road, Green Man Way, Turnpike Lane and Hackney Road.
IM22	Rachel Smith-Lyte	650	No	<p>Hmmm. When you say "into the green space south of Bury Hill" that basically means Burkes Wood! The Parish Council and tree wardens have and are working really hard to preserve whats left of this special remnant ancient woodland that hasn't been built on by Bury Hill Estate. I'm also rather concerned about the route being as you say "appropriately lit". Light disturbance will affect bats</p>	In order to avoid harm to the woods IM22 has been amended to remove cycling and walking route through the woods and instead introduce cycling and walking infrastructure along Saxon Way, Melton Grange Road, Green Man Way, Turnpike Lane and Hackney Road.

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				<p>and other mammals and birds living in the wood (light pollution also affects human health as it happens). So whilst a cyclist and certainly a cycling advocate, I would think very carefully before laying tarmac in Burkes Wood. Rather hoggin would be a much better medium in keeping with the natural surroundings. I believe there is no need for tree felling - any path doesn't have to be straight, right? In fact its more interesting if its not. It would also slow cyclists - no bad thing especially from a pedestrian's perspective! Also avoiding the need for loss of trees.</p> <p>Attachments: https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455282/PJP/-/11775797%201%20Hoggin%20path%2Ejpg</p>	
IM22	Rachel Smith-Lyte	651	No	<p>In the interests of dark skies and animals, birds and locals restful sleep, please do not light this, should the path through this precious and extremely rare remnant ancient woodland go ahead. We humans need to accept not everything can or should be lit and either not use it at night or wear a head torch etc. No tree loss is necessary - a winding path is also more natural and slows cyclists down, no bad thing in this case. Please also do not concrete or tarmac it. Having previously worked in countryside conservation, I know Hoggin works well as a natural medium for paths.</p>	<p>In order to avoid harm to the woods IM22 has been amended to remove cycling and walking route through the woods and instead introduce cycling and walking infrastructure along Saxon Way, Melton Grange Road, Green Man Way, Turnpike Lane and Hackney Road.</p>
IM22	Zac Barnes	599	Yes	<p>With regards IM22, it is good to see a proposal to open up the woods to cycling, but I have a few concerns. Firstly, I am unclear as to what is meant by "ensure the route is appropriately lit, particularly through the wooded area that lacks natural</p>	<p>In order to avoid harm to the woods IM22 has been amended to remove cycling and walking route through the woods and instead introduce cycling and walking infrastructure along Saxon Way, Melton Grange Road, Green Man Way, Turnpike Lane and Hackney Road.</p>

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				<p>surveillance.”</p> <p>Adding streetlights to Melton Wood would lead to a massive disruption of the character of the area which is a largely natural woodland, waste electricity which is still generated partially through fossil fuels, not to mention posing a danger of disrupting the foxes, badgers, and bats that live in the wood.</p> <p>Forgive me if I have misunderstood the technical term, but I believe that increased lighting may not actually accomplish what you intend to as there is some suggestion that increased lighting can actually attract criminals (my mother has an ongoing project to reduce street lighting with Suffolk County Council at the University of Suffolk, if you would like more details on the impact of street lighting on nature you may wish to contact Dr Hannah Steventon Barnes there). Regardless Woodbridge (especially this area) is very safe, I walk through it regularly at night and have never had any problems.</p> <p>The only criminal activity that I know of taking part in the wood is littering. Additionally, I have heard that people use to ride motorbikes down PROW 19 leading to anti-motorbike bars being installed on either end of the path. Designating it as a bridleway would require the removal of these bars and so measures would have to be considered to stop motorbikes from using it.</p> <p>If only the part between Leeks Hill Road and Melton Park is designated as such, the cameras installed</p>	

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				<p>there would probably allow any infringing motorbikes to be caught. I just think that consideration should be given to this.</p> <p>I am grateful for the introduction of a crossing point onto Bury Hill, the existing bridleway (PROW 20) provides a very useful off-road route but it is hard to get to as Bredfield Road can be busy. It would also be useful to have a zebra or toucan crossing at the other end over the A1152 to link it to the bridleway to Valley Farm Road (PROW 21).</p> <p>While you are amending rights of way in the area, I feel I should bring it to your attention what appears to be an error - that on the definitive map PROW 24 reaches a dead end about 150 meters from the A12. The track continues on to the A12 and while it is not necessarily properly maintained, there seems to be no reason the bridleway should extend to the A12.</p> <p>Finally, I wish to again stress in very strong terms that I believe this would be an incredibly poor choice that would severely damage the character of the location and the enjoyment that walkers and potential cyclists would get from experiencing nature. Additionally, I think that it would have a strongly negative impact upon the Wood that is on the boundary of an AONB.</p>	
IM23	Rachel Smith-Lyte	653	Yes	Yes but please do not light the path. Users (including me) can wear a head torch or carry an actual torch or not use it at night. Already too much light pollution and disturbance of mammals including rare bats, birds (and people) in a climate and linked	The most appropriate surfacing and lighting solutions will need to be considered as the recommendation is taken forward to delivery. IM23 has been amended to avoid cycling and walking along Yarmouth Road and

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				<p>ecological emergency.</p> <p>Please also surface the path sensitively - e.g. Hoggin instead of tarmac.</p> <p>Really pleased about having a safe crossing on Yarmouth Road.</p>	<p>provide a more desirable route along St Audrys Road and Lodge Farm Lane.</p>
IM23	Ufford Parish Council (Judi Hallett)	730		<p>Ufford is deemed to be just outside this Corridor but there are routes which could be added to link our Parish more effectively to the proposed Ipswich to Melton cycling infrastructure. Specifically, the IM23 route from Wilford Bridge Road across to Melton Road could be extended to Lower Road , Ufford via Decoy Farm, using PROW 6 and 10 to provide a safer connection for cyclists and walkers who wish to access the countryside on the north eastern side of the River Deben.</p> <p>That said, the proposed route for IM23 appears to follow narrow footpaths which are, at present, inappropriate for use by cyclists - for example the section of IM23 from the B1438 across to IM24 is an extremely narrow PROW with overhanging trees and hedges.</p>	<p>In order to facilitate greater cycling and walking connections between Ufford and Melton IM23 has been amended to upgrade Footpath 6 to a bridleway, and widen and resurface to accommodate cycling and walking.</p>
IM24	Chris Adelson	503	Yes		Support noted.
IM24	Rachel Smith-Lyte	655	Yes	<p>Great idea. As a regular user of Lodge Farm Lane I can confirm there is at present (and presuming the awful idea of the Yarmouth Road care home doesn't go ahead) a low traffic situation on Lodge Farm Lane.</p>	<p>While Lodge Farm Lane provides a low traffic attractive cycling and walking route, the areas of the route further south cut across well developed wildlife areas and for this reason IM24 has been removed from the Strategy. IM23 remains in the Strategy, thereby providing a connection between Melton Park and Melton Railway Station.</p>

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IM25	Chris Adelson	504	Yes	Plant new trees to compensate for any removals. Ensure sensitive, timed, lighting.	The most appropriate landscaping and lighting solutions will need to be considered as the recommendation is taken forward to delivery.
IM25	Martlesham Parish Council (Diane Linsley)	891		<p>IM25 appears unsuitable as a link towards Ipswich. It arrives in a part of Ipswich that has no leisure, entertainment or community facilities to serve as a destination. The retail park there, specialises in goods or a scale of shopping which requires a car. It is also still distant from the town centre.</p> <p>There is already a planned bridleway crossing of the A12 where the BL Boulevard connects. MPC renews its request that the opportunity should be taken to create a cycle route using the bridleways north of Welham Plantation, which could then connect to a short cycleway along the north side of Foxhall Road, which could in turn connect to the network of PROWs near the speedway stadium to connect to IM4. This will give a fast, convenient link to Ipswich for Brightwell Lakes, Adastral Park, and the commercial/retail areas on the east side of the A12.</p>	IM25 seeks to provide an alternative route into Ipswich than that set out through Martlesham and Kesgrave, as well as connecting to the Ipswich-Felixstowe Key Corridor. The Ipswich-Melton Key Corridor has been amended to incorporate a new recommendation (IM32), which seeks to introduce a traffic free cycling and walking route between Brightwell Lakes and Kesgrave via Bridleway 6 along Welham's Plantation to the south of Martlesham Heath.
IM26	Julian Page	214	Yes	<p>In addition to this route (IM26) a useful and very pleasant quiet route to Ipswich could be made using PROW25, Kennels Road, Purdis Road, Purdis Farm Lane, PROW4, and PROW1.</p> <p>Clearly to make this route viable a new crossing would be required across the A12. This one crossing would service 3 existing rights of way all close by that have been effectively severed by the building of the A12.</p>	The suggested route would have a similar effect of linking Brightwell Lakes and south east Ipswich along Public Rights of Way and on quiet roads. However, due to the need for a cycling and walking bridge over the A12 and the support for IM26 the recommendation has not been amended.

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IM26	Martyn Shakespeare	533	Yes	Could be interesting. Probably nicer route (albeit not quite so direct) than section adjacent A12 of IM25	Support noted.
IM26	Mary Trowse	703		My view on this depends on what you mean by 'widening and resurfacing' existing footpaths. Although a safe cycle route to Felixstowe is desirable, I feel strongly that cycle and footpaths through rural countryside should be sensitively designed to blend in with the surroundings and cause minimal disruption to wildlife during construction and use. A wide grey tarmac path would not be suitable.	The route would need to be widened and resurfaced in places to ensure safe use by pedestrians and cyclists. However, the most appropriate surfacing solution will need to be considered as the recommendation is taken forward to delivery.
3.20 - 3.24	Andy Bird	120	Yes	<ul style="list-style-type: none"> Note - A14 original work cut through local routes with no crossings added. Not showing - off-road route to south Ipswich - via Nacton Woods. Avoids busy Nacton road. IF14 bridge - required. IF33 via Hemley/Waldringfield would be better route and pick up with NCN 	<p>Comment noted. It is agreed that a route directly between Bucklesham and Felixstowe Road 'east' (and ergo the Ipswich to Felixstowe Key Corridor) via Levington Lane and a new bridge over the A14 would be ideal, and has been included as a 'desirable' recommendation. This has not been given higher priority due to the high cost of such a bridge that would benefit a small population. Bucklesham benefits from a couple of relatively low traffic on-carriageway routes towards Ipswich or Felixstowe, though these are not likely to be acceptable for all users due to the lack of segregation from motor vehicles.</p> <p>Regarding the use of 'Nacton Wood' to avoid Nacton Road - it is unclear where this relates to; Decoy Wood does not appear to be appropriate for cycling through or necessary when the various sections of Felixstowe Road that make up the Ipswich to Felixstowe Key Corridor are recommended for improvement.</p> <p>Regarding the use of a route through Waldringfield and</p>

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					<p>Hemley, this was considered early on in the process and decided against due to the potential for increased recreational pressure on the SAC/SPA designated sites, and not a reasonable option when an alternative was identified (MF3).</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
3.20 - 3.24	Felixstowe Town Council (Ash Tadjrishi)	914		<p>Recognition of the concept of the Ipswich to Felixstowe Key Corridor is greatly welcomed as a long-standing aspiration of local communities. However, we believe that the leg from Felixstowe / Trimley to Martlesham, and hence beyond, would be better treated as a Key Corridor in its own right. Furthermore, there are certain elements of the detailed proposals which are of concern, see below, without prejudice to the concept.</p>	<p>Comment noted. The previous two route arms of the Ipswich to Felixstowe Key Corridor have now been split into two, with the Martlesham to Felixstowe (via Brightwell Lakes) route now recognised as a Key Corridor in its own right.</p>
3.20 - 3.24	Felixstowe Town Council (Ash Tadjrishi)	923		<p>The examination of detailed potential routes within the Felixstowe and Trimleys area is greatly welcomed, as are the recognised categories. However, it does appear that the existence and further potential of some of the leisure opportunities, particularly to the north-east parts of the town, require further recognition and emphasis.</p>	<p>Comment noted. The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel</p>

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				<p>Many of the routes proposed are welcomed and supported. However, there are concerns on certain specific proposals as below, without prejudice to the overall concepts, which are supported.</p> <p>The concepts of good routes and permeability within the North Felixstowe Garden Village and other associated developments is greatly welcomed, but we assume that details in this area will be subject to further consultation regarding the proposed Master Plan Community engagement as per SCDC Local Plan Policy SCLP12.3 and para. 12.62</p>	<p>infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. In some locations, the existing physical constraints may mean retrofitting active travel infrastructure is either not possible to be delivered to as high a standard as we would aspire to or not possible at all. This may mean some schemes are unable to be brought forward.</p> <p>As stated in Policy SCLP12.3 North Felixstowe Garden Neighbourhood, the development will be delivered through a masterplan approach brought forward through landowner collaboration and community engagement.</p>
3.20 - 3.24	Felixstowe Town Council (Ash Tadjrishi)	960		<p>NOTE C</p> <p>The Felixstowe Definitive Map (and The Trimleys)</p> <p>The Strategy geography is frequently defined in terms of the PROWs as defined on the Definitive Map, in this case for the parishes of Felixstowe, Trimley St. Mary and Trimley St. Martin. It will be fundamental to the implementation of the eventual</p>	<p>Comment noted. This comment relates to updating Suffolk County Council's Definitive Map with the correct boundary lines and with the changes to PROW routes that have occurred since the Map's last revision. However, East Suffolk Council does not have any control over the frequency of updates to the Definitive Map.</p>

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				<p>Strategy that these underlying documents are correct and up to date.</p> <p>However, unfortunately, the Felixstowe map is grossly out of date and in some instances directly misleading.</p> <p>We recognise that this issue is not directly the subject of this consultation, and FTC will be submitting the details below, with further evidence, to Suffolk County Council with a strong request that the map be fully updated. Nevertheless, it is relevant to list some of the issues here as they directly relate to certain of the proposals, and the eventual documents will certainly need to be consistent. Hence what follows is an initial but incomplete assessment of this issue.</p> <p>The Felixstowe Definitive Map. From SCC website Nov. 2021</p> <p>Title block: FELIXSTOWE 244 WORKING COPY OF RECORDED RIGHTS OF WAY DERIVED FROM THE DEFINITIVE MAP FOR THE FORMER RURAL DISTRICT OF DEBEN Relevant date: 26 October 2015 Working copy revision date: 24 July 2020</p> <p>Fundamental errors: Map shows Felixstowe parish as part of former Deben RDC, incorrect – was of course Felixstowe UDC from 1893 until 1974, then Suffolk Coastal DC,</p>	

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				<p>now East Suffolk DC. Felixstowe and the Trimleys were never part of the Deben RDC. Even the old district boundary in the Deben is not shown – only as a parish boundary.</p> <p>Parish boundary Felixstowe / Trimley St. Mary incorrect</p> <p>This boundary was revised c. 1985 very sensibly in connection with the building of the A45 (now A14) Dock Spur Road and Candlet Road. However, the old boundary, further west is shown on the map.</p> <p>For information as to parish PROWs and Town / Parish Council areas of interest the correct boundary, as shown on OS 1:25K scale mapping is thus:</p> <p>a) Felixstowe CP Western boundary: Shows old boundary with Trimley St. Mary, changed c.1990??? Post creation of A45, now A14 Dock Spur Rd. (Port of Felixstowe Rd) Incorrect from map reference TM293372 Junction of Trimley FPs 6 & 19 To map reference TM261342 (junction of Essex / Suffolk CC border in estuary. See current OS 1:25000 map Boundary follows: Trimley FP6 to map ref 292366 (junction of Trimley FPs 6,7,8,20); Trimley FP20; Felixstowe FP28 to junction with Candlet Rd; centre line of A154 Candlet Rd to A14 J58; Centre Line A14 Dock Spur Rd.; A14 J59 Northbound on-slip; Blofield Rd to junction with Parker Avenue; SE edge Parker Avenue to junction with Fagbury Rd; SW edge of</p>	

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				<p>Fagbury Rd to railway level crossing; straight line across Port of Felixstowe Trinity Terminal to quayside at map ref. TM266342; quayside to map ref TM264343; straight line to County boundary in estuary.</p> <p>b) Potential confusion of FP numbers due to change of parish boundary. Renumbering needed for Trimley FPs 7 & 20? Felixstowe FP32B now in Trimley.</p> <p>c) Quintons Lane PROW15: southern section, developed for residential use in the 1930s and now public highway is still shown as Bridleway.</p> <p>We understand the HMG have set a deadline of 2026 for new PROWs to be mapped, so it is essential that these issues are clarified fully in advance of that, but corrections are need on the same timescale as the publication of the C&W Strategy.</p> <p>FP47, on the northern seafront is shown as Bridleway on the OS map, but not the Definitive Map, and its precise location is not clear – see F7 FP31 south, FP32 no longer exist.</p> <p>FPs 67, 68 at new railway bridge</p> <p>Critically FP32B, now in Trimley St. Mary need to be extended in PROW terms across the A14 footbridge. It is correctly an aspiration of the Strategy to establish a link from there to the rail bridge, although that is challenging. But if a route should be defined other than on current highway, a desirable and potentially possible solution, it will be essential</p>	

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				<p>for that to be correctly defined in PROW terms. There are several other instances, particularly in the marsh land area in North East Felixstowe with a similar issue, if the proposed PROWs turn out to be feasible.</p> <p>Proposals F7 F43 F96 F95 F64 F104</p>	
3.20 - 3.24	Gray, Roy	317		<p>THE REPORT LISTS ROUTES MOST USED BETWEEN SEPT 2020 AND AUGUST 2021 I NOTE THAT HIGH ROAD WEST/HIGH ROAD FROM WALTON TO TRIMLEY BORDER NOT SHOWN. THIS I DO NOT BELIEVE IS CORRECT, THE TRAFFIC FLOW BOTH WAYS IS ALMOST CONTINUOUS, AND INCREASINGLY EUTH THE TWO ESTATES BEING/PLANNED TO BE BUILT ALONGSIDE ACADEMY AND OPPOSITE, ROUNDABOUTS AT BOTH ENDS OF ROAD PLANNED TO LINK HIGH ROAD AND CANDLET ARE ESSENTIAL. WITH THE ESTATES BEING BUILT AT THE TRIMLEYS THE TRAFFIC VOLUME WILL CREATE BIG PROBLEMS PARTICULARLY BY WALTON CURCHES AND TRIMLEY SCHOOL A TRAFFIC DIVERTING SYSTEM SO TRAFFIC USES CANDLET ROAD AND BETTER FOOTPATHS/CYCLING LANES CREATED.</p>	<p>Comment noted. The Cycling and Walking Strategy map does not indicate traffic flows but instead indicates the location of recommended improvements to cycling and walking (and where appropriate, horse riding) infrastructure.</p> <p>There are a number of cycling and walking infrastructure recommendations for the Walton and emerging North Felixstowe Garden Neighbourhood areas to help improve the safety and convenience of active travel users in this area, and to therefore support modal shift away from motor vehicles to active travel for shorter journeys.</p> <p>The route following recommendations F69/F91, F51, F30 and F44 will in particular serve to open up a safe and convenient cycle and pedestrian route that allows for safer and more convenient transfer between central Felixstowe, Walton, the Trimleys and Kirton than the High Road/High Street/High Road West/High Road East</p>

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					route. Notwithstanding this, recommendations F1-F6 are intended to make cycling along the High Road route safer for those still wishing to use this route.
3.20 - 3.24	Julian Page	213		On the A12 West of Brightwell there are 3 rights of way that cross the A12 at surface level and thus are very dangerous or impossible. A single bridge or tunnel here would open up a number of useful walking and cycling routes that have been severed by the building of the A12.	The draft Strategy does not include a recommendation for a bridge or tunnel over/under the A12 to the west of Bucklesham, though recommendation IM12 includes a bridge to the west of Brightwell Lakes. Bucklesham Road already serves as a bridge over the A12 from Bucklesham, and recommendations IM25/26 (which are 'either, or' options) serve to increase the accessibility to Bucklesham Road from Brightwell Lakes/Martlesham so that it can be better utilised. Foxhall Road is highly unsuitable for cycling due to its bends and high speeds, and is made even less accessible by its steep hill in the Bixley Farm area; for these reasons further A12 crossing infrastructure between the Bucklesham Road bridge and IM12 was not taken forward as a recommendation.
IF3	Chris Petty	33		The section referred to is a national speed limit (60MPH) section in an otherwise 40MPH limit road. Could you add taking the speed limit down to 40MPH for this section.	Changes to vehicle speed limits on roads within the district are outside of the scope of the East Suffolk Cycling and Walking Strategy, and cannot be directly actioned by East Suffolk Council as we are not the Highways Authority. However, as many consultation responses have requested speed limit changes to roads within the district, a list of them has been collated separately to be sent directly to the Highways Authority, Suffolk County Council.
IF5	Chris Petty	34		The barriers to the entrance of the park from both Bucklesham Road and Murrills Road need to be removed or made easier to cycle around	Comment noted. The need for removal of these barriers to the park has now been explicitly included in the recommendations listed under IF5.
IF12	Chris Petty	32	Yes	You will need to consult with Suffolk Show organisers as during the Suffolk show this road is converted into three lanes to cope with the large	All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment

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				amount of vehicle traffic attending the show at the Suffolk Showground	for potential impacts and optimisation of routing and design by qualified persons.
IF13	Andy Smith	790		<p>It is highly undesirable to eliminate the existing carriageway. Neither is it necessary for successful implementation of this route.</p> <p>There is ample highway land here, as on the rest of Felixstowe Rd (east) to provide a 3m track with good separation from the highway.</p> <p>But the proposal would require elimination of both right lane filters to and from Bridge Road, Nacton, creating direct T-junctions to a stretch with fast moving traffic. Also this area remains a vital space on occasion when "Operation Stack" is invoked for holding HGV traffic unable to enter the Port of Felixstowe, less used than previously but remaining an important facility when needed.</p> <p>We recommend that the route IF13 should be a new path to the north of Felixstowe Road for its entire length</p>	<p>Comment noted. This recommendation (IF13) was reviewed, and two alternative solutions have been included in the Strategy for the Highways Authority to consider and take forward to the further assessment and design stage, as appropriate. In short, one of these options include repurposing the dualled section of carriageway and the other does not.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
IF13	Felixstowe Town Council (Ash Tadjrishi)	915		<p>It is highly undesirable to eliminate the existing carriageway. Neither is it necessary for successful implementation of this route.</p> <p>There is ample highway land here, as on the rest of Felixstowe Road (east) to provide a 3m track with good separation from the highway.</p> <p>But the proposal would require elimination of both right lane filters to and from Bridge Road, Nacton, creating direct T-junctions to a stretch with fast moving traffic.</p> <p>Also this area remains a vital space on occasion when "Operation Stack" is invoked for holding HGV</p>	<p>Comment noted. This recommendation (IF13) was reviewed, and two alternative solutions have been included in the Strategy for the Highways Authority to consider and take forward to the further assessment and design stage, as appropriate. In short, one of these options include repurposing the dualled section of carriageway and the other does not.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment</p>

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				<p>traffic unable to enter the Port of Felixstowe, less used than previously but remaining an important facility when needed.</p> <p>We recommend that the route IF13 should be a new path to the north of Felixstowe Road for its entire length</p> <p>NB it is also very relevant that contained in the current NSIPS DCO submission on the Sizewell C Power Station, a major Freight Management Facility.is proposed to be created in the 2 fields to the north of the A14, at the western end of the dualled section, with a major new entrance to that section.</p> <p>Any proposals for this area should be developed taking account of this.</p> <p>It seems likely that the station will be approved, with a build period of some 12 years possibly commencing in 2024, and the Freight Management Facility will be in build or operational for much of that time.</p>	<p>for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
IF14	Isobel Fleming	83	No	<p>Should any provision be made for a bridge across the road for pedestrians and cyclists it should also include horse riders to give access to any off road network being proposed.</p>	<p>Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. It is</p>

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					intended that in locations that are appropriate and useful for horse-riding, the infrastructure design will reflect this through all-user design.
IF14	Julian Page	212	Yes	<p>I used to use this route when cycling, but no longer do as the traffic levels now make it too dangerous.</p> <p>This would be a very useful extra link and should have its priority raised.</p>	Support noted.
IF16	Andy Smith	791		<p>The effective closure of Morston Hall Road to all traffic except buses is not acceptable. (Despite retention of access via the south east half for access to adjacent properties.)</p> <p>While this can and should be a prime new route for cyclists (probably not many- walkers) that should not be at the expense of eliminating its core role as a local road. It can easily be provided by a new path on the south verge, as per comment below on IF18</p> <p>The new single carriageway road was provided, obviously at quite a large public expense, in the late 90s, as part of the scheme eliminating the previous very unsafe right turn onto the “Woodbridge turn”, previously the A1093. But that had unfortunately created the isolation of the farm in particular (which had been reduced to a tee junction directly to only the westbound carriage way of the A45 / A14), but also other local “desire line” local routes after the dualling of the A45 / A14. c.1988. These must be preserved.</p> <p>Additionally, since then, as the A14 has seen a rapidly increasing national importance and hence</p>	Comment noted. IF16 was removed from the Strategy and replaced with a re-worked IF18. IF18 now provides two options for the Highways Authority to consider, one with and one without use of the carriageway (and a modal filter).

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				traffic, the inevitable occasional major traffic incidents are becoming an increasingly frequent issue. Morston Hall Road can provide a degree of relief on some of those occasions – far from perfect but infinitely better than nothing. Given that we are seeing delays of many hours on such occasions, highways policies need to be to seek better means of managing the events - certainly not to eliminate the only alternative access to Felixstowe and Trimley other than through Bucklesham / Kirton etc lanes.	
IF16	Felixstowe Town Council (Ash Tadjrishi)	916		<p>The effective closure of Morston Hall Road to all traffic except buses is not acceptable. (Despite retention of access via the south east half for access to adjacent properties.)</p> <p>While this can and should be a prime new route for cyclists (though perhaps not very many walkers) that should not be at the expense of eliminating its core role as a local road. It can easily be provided by a new path on the south verge, as per comment below on IF18</p> <p>The new single carriageway road was provided, obviously at quite a large public expense, in the late 1990s, as part of the scheme eliminating the previous very unsafe right turn onto the “Woodbridge turn”, previously the A1093. But that had unfortunately created the isolation of the farm (which had been reduced to a T-junction directly to only the westbound carriage way of the A45 / A14), but also other local “desire line” local routes after the dualling of the A45 / A14. c.1988. These must be preserved.</p>	Comment noted. IF16 was removed from the Strategy and replaced with a re-worked IF18. IF18 now provides two options for the Highways Authority to consider, one with and one without use of the carriageway (and a modal filter).

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				<p>Additionally, since then, as the A14 has seen a rapidly increasing national importance and hence traffic, the inevitable occasional major traffic incidents are becoming an increasingly frequent issue. Morston Hall Road can provide a degree of relief on some of those occasions – far from perfect but infinitely better than nothing. Given that we are seeing delays of many hours on such occasions, highways policies need to be to seek better means of managing the events - certainly not to eliminate the only alternative access to Felixstowe and Trimley other than through Bucklesham / Kirton etc lanes.</p>	
IF17	Andy Smith	792		<p>The effective closure of Morston Hall Road to all traffic except buses is not acceptable. (Despite retention of access via the south east half for access to adjacent properties.)</p> <p>While this can and should be a prime new route for cyclists (probably not many- walkers) that should not be at the expense of eliminating its core role as a local road. It can easily be provided by a new path on the south verge, as per comment below on IF18</p> <p>The new single carriageway road was provided, obviously at quite a large public expense, in the late 90s, as part of the scheme eliminating the previous very unsafe right turn onto the “Woodbridge turn”, previously the A1093. But that had unfortunately created the isolation of the farm in particular (which had been reduced to a tee junction directly to only the westbound carriage way of the A45 / A14), but also other local “desire line” local routes after the</p>	Comment noted.

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				<p>dualling of the A45 / A14. c.1988. These must be preserved.</p> <p>Additionally, since then, as the A14 has seen a rapidly increasing national importance and hence traffic, the inevitable occasional major traffic incidents are becoming an increasingly frequent issue. Morston Hall Road can provide a degree of relief on some of those occasions – far from perfect but infinitely better than nothing. Given that we are seeing delays of many hours on such occasions, highways policies need to be to seek better means of managing the events - certainly not to eliminate the only alternative access to Felixstowe and Trimley other than through Bucklesham / Kirton etc lanes.</p>	
IF17	Felixstowe Town Council (Ash Tadjrishi)	917		<p>The effective closure of Morston Hall Road to all traffic except buses is not acceptable. (Despite retention of access via the south east half for access to adjacent properties.)</p> <p>While this can and should be a prime new route for cyclists (though perhaps not very many walkers) that should not be at the expense of eliminating its core role as a local road. It can easily be provided by a new path on the south verge, as per comment below on IF18</p> <p>The new single carriageway road was provided, obviously at quite a large public expense, in the late 1990s, as part of the scheme eliminating the previous very unsafe right turn onto the “Woodbridge turn”, previously the A1093. But that had unfortunately created the isolation of the farm</p>	Comment noted.

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				<p>(which had been reduced to a T-junction directly to only the westbound carriage way of the A45 / A14), but also other local “desire line” local routes after the dualling of the A45 / A14. c.1988. These must be preserved.</p> <p>Additionally, since then, as the A14 has seen a rapidly increasing national importance and hence traffic, the inevitable occasional major traffic incidents are becoming an increasingly frequent issue. Morston Hall Road can provide a degree of relief on some of those occasions – far from perfect but infinitely better than nothing. Given that we are seeing delays of many hours on such occasions, highways policies need to be to seek better means of managing the events - certainly not to eliminate the only alternative access to Felixstowe and Trimley other than through Bucklesham / Kirton etc lanes.</p>	
IF17	Trimley St Martin Parish Council, (Caroline Ley)	306	No	<p>IF17 and IF18 Morston Hall Road</p> <p><i>Trimley St Martin Parish Council disagrees with the installation of bus gate filters for the following reasons:-</i></p> <p><i>The single track road with passing places was provided at considerable expense to facilitate the needs of local traffic after the dualling of the A45 / A14. This purpose should continue to be borne in mind.</i></p> <p><i>Morston Hall Road is used as an emergency diversion for A14 traffic. It is pressed into service when work takes place on the A14 and when the A14</i></p>	Comment noted.

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				<p><i>is closed by a road accident or vehicle breakdown. Use of the emergency route is becoming increasingly common, but it would not sit comfortably with an APNR bus gate system.</i></p> <p><i>Farm traffic and residents emerging from Morston Hall Lane would not be permitted to proceed along Morston Hall Road toward Ipswich which makes no sense as there is no evidence of dangerous incidents having occurred</i></p> <p><i>All traffic but in particular farm traffic, i.e. Home Farm Nacton, would be required to travel to the High Road to access the A14 which we know, when added to the projected increase in traffic due to additional housing on the peninsula, would cause congestion and be unnecessarily hazardous.</i></p> <p><i>Traffic wishing to proceed to Felixstowe from Suffolk Yacht Harbour etc. would need to go via Seven Hills junction, which as we know is a very busy junction.</i></p> <p><i>It would be unfair to local residents to remove their right to use Morston Hall Road as a link to routes to Ipswich. Without the option to use Morston Hall Rd, all Trimley St Martin drivers heading to Ipswich would need to make a lengthy detour, or negotiate an acceleration lane. This is something with which many local drivers would be most uncomfortable.</i></p> <p><i>Possible solutions to this</i></p> <p><i>1. a) Create a cycle path on field side of Morston Hall</i></p>	

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				<p>Road which already has a wide grassed verge and could be upgraded. (This has always been the thought of many a local cyclist);</p> <p>2. b) Use existing cycle path and create protection from A14 traffic (esp. back draught from HGVs) by erecting fencing with hedging planted behind. Hedging, once high enough, would in turn protect, and be more pleasing to the eye in the long term.</p>	
IF18	Andy Smith	793		<p>This section should be located away from the A14, south of the vegetated bund, on the southern verge of Morston Hall Rd, for which there is space on highways land. This verge is poorly vegetated for the most part, little loss of habitat.</p> <p>In certain places, minor changes may be required to the parallel drainage ditch.</p>	Comment noted.
IF18	Felixstowe Town Council (Ash Tadjrishi)	918		<p>This section should be located away from the A14, south of the vegetated bund, on the southern verge of Morston Hall Road, for which there is space on highways land. This verge is poorly vegetated for the most part, little loss of habitat.</p> <p>In certain places, minor changes may be required to the parallel drainage ditch.</p>	Comment noted.
IF18	Simon Shaw	591	Yes	<p>Move the cycle route well away from the dual carriageway, alongside the existing bus way and single track road.</p>	Comment noted.
IF18	Trimley St Martin Parish Council, (Caroline Ley)	305	No	<p>IF17 and IF18 Morston Hall Road</p> <p>Trimley St Martin Parish Council support improvements to the cycle path, as explained in the final paragraph of this comment, but disagree with the installation of bus gate filters for the following reasons:-</p>	Comment noted.

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				<p><i>The single track road with passing places was provided at considerable expense to facilitate the needs of local traffic after the dualling of the A45 / A14. This purpose should continue to be borne in mind.</i></p> <p><i>Morston Hall Road is used as an emergency diversion for A14 traffic. It is pressed into service when work takes place on the A14 and when the A14 is closed by a road accident or vehicle breakdown. Use of the emergency route is becoming increasingly common, but it would not sit comfortably with an APNR bus gate system.</i></p> <p><i>Farm traffic and residents emerging from Morston Hall Lane would not be permitted to proceed along Morston Hall Road toward Ipswich which makes no sense as there is no evidence of dangerous incidents having occurred</i></p> <p><i>All traffic but in particular farm traffic, i.e. Home Farm Nacton, would be required to travel to the High Road to access the A14 which we know, when added to the projected increase in traffic due to additional housing on the peninsula, would cause congestion and be unnecessarily hazardous.</i></p> <p><i>Traffic wishing to proceed to Felixstowe from Suffolk Yacht Harbour etc. would need to go via Seven Hills junction, which as we know is a very busy junction.</i></p> <p><i>It would be unfair to local residents to remove their</i></p>	

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				<p><i>right to use Morston Hall Road as a link to routes to Ipswich. Without the option to use Morston Hall Rd, all Trimley St Martin drivers heading to Ipswich would need to make a lengthy detour, or negotiate an acceleration lane. This is something with which many local drivers would be most uncomfortable.</i></p> <p><i>Possible solutions to this</i></p> <ol style="list-style-type: none"> <i>1. a) Create a cycle path on field side of Morston Hall Road which already has a wide grassed verge and could be upgraded. (This has always been the thought of many a local cyclist);</i> <i>2. b) Use existing cycle path and create protection from A14 traffic (esp. back draught from HGVs) by erecting fencing with hedging planted behind. Hedging, once high enough, would in turn protect, and be more pleasing to the eye in the long term.</i> 	
IF22	Felixstowe Town Council (Ash Tadjrishi)	919		<p>This route would be very welcome, notably in connecting not only to Kirton, etc, but also in connection with IF23 & F44 a connection from Ipswich to the whole northern and eastern parts of rural and to-be-developed Felixstowe. See other comments on this topic. We propose that this should also become High Priority as part of a wholly off-road link from Ipswich into Felixstowe.</p>	Support noted.
IF22	S Browns	31	Yes	<p>This could easily be continued in to Felixstowe with a cycle only track, which can use the present bridle paths and low volume roads.</p>	Comment noted.
IF22	Trimley St Martin Parish	307	Yes	<p><i>This route has been marked as high priority. Trimley St Martin Parish Council considers that, like F20, it should be very high priority because of the link to F26</i></p>	Comment noted.

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	Council, (Caroline Ley)			<p><i>and underpass to Candlet Track.</i></p> <p><i>IF22 should not be shared with any motorised traffic.</i></p>	
IF22	Trinity College, Cambridge (Sir/Madam)	1071		<p>Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College's landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to the wider Trimley Estate, owned and controlled by Trinity College Cambridge (including recommendations F9, IF22, IF24, F10, F36, F113, F38)</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals affecting the Trimley Estate, referenced above, subject to detailed</p>	<p>Support noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>

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				<p>exploration of feasibility, viability and agreement of details as part of future masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across the Estate in more detail. The delivery and funding of proposals requiring significant new infrastructure, such as hard surfaced routes and lighting and signage, would need to be subject to further discussion.</p> <p>We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.</p>	
IF23	Felixstowe Town Council (Ash Tadjrishi)	920		<p>Proposal welcomed see comments ref IF22</p> <p>This route would be very welcome, notably in connecting not only to Kirton, etc, but also in connection with IF23 & F44 a connection from Ipswich to the whole northern and eastern parts of rural and to-be-developed Felixstowe. See other comments on this topic. We propose that this should also become High Priority as part of a wholly off-road link from Ipswich into Felixstowe.</p>	Support noted.

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IF23	Martyn Shakespeare	532	Yes		Support noted.
IF23	Susan Harvey	614	Yes	The pedestrian/cycle bridge over the A14 as existing is not suitable for 2 way cycling and pedestrians. At present you have to dismount and push bikes. This crossing over the A14 needs to be replaced so that it is fit for the future. This is a very well used route.	Comment noted.
IF24	Alison Vickers	617	Yes	An excellent idea and will be essential if a new primary school is built at land around Reeve Lodge, Trimley St Martin to enable safe cycling from Kirton to this school.	Support noted.
IF24	Felixstowe Town Council (Ash Tadjrishi)	921		This proposal, together with the linked elements towards Martlesham is greatly welcome, providing a much-needed route from Felixstowe to Martlesham and beyond. As mentioned above in the section on Key Corridors, we feel this important route would be better presented as a Key Corridor in its own right.	Support noted.
IF24	Martyn Shakespeare	519	No	In general I find this road quiet enough, but I can see that this might be helpful for users of the primary school from Kirton.	Comment noted.
IF24	Susan Harvey	657	Yes	IF24 Kirton Road. Very good idea. Helps children from Kirton to get to the new Trimley St Martin school behind Reeve Lodge.	Support noted.
IF24	Trinity College, Cambridge (Sir/Madam)	1072		Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments	Support noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of

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				<p>through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College’s landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to the wider Trimley Estate, owned and controlled by Trinity College Cambridge (including recommendations F9, IF22, IF24, F10, F36, F113, F38)</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals affecting the Trimley Estate, referenced above, subject to detailed exploration of feasibility, viability and agreement of details as part of future masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across the Estate in more detail. The delivery and funding of proposals requiring significant new infrastructure, such as hard surfaced routes and lighting and signage, would need to be subject to further discussion.</p>	<p>implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>

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				We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.	
IF25	Lesley Vince	344	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this area are minimal in comparison to the loss of habitat/habitat changes.</p>	Comment is not relevant to this recommendation; comment relates to recommendation IM12, and has been responded to via Comment ID 331.
IF26	Susan Harvey	613	Yes	IF26 Very Good improves connectivity 'off road'.	Support noted.
IF30	Susan Harvey	615	Yes	Bearing in mind that this is the heavy lorry route into the village and used by local farm vehicles at present I support a good cycling and walking crossing point at Innocence Lane	Support noted.

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IF33	Eamonn O'Byrne	871		A completely new route that carried cyclists' westwards and directly towards Ipswich would be of significant benefit. Planned correctly, a westerly extension of IF33 would promote cycling to the residents of Waldringfield, Newbourne and the new Brightwell Lakes development.	Support for the principle of recommendation IF33 (now MF3) is noted. The option to extend the recommendations for the peninsula area eastward to serve Waldringfield, Newbourne and Hemley was considered but was ultimately decided against due to the sensitivity of habitat sites in the area to recreational pressure.
IF33	Lesley Vince	346	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this area are minimal in comparison to the loss of habitat/habitat changes.</p>	Comment is not relevant to this recommendation; comment relates to recommendation IM12, and has been responded to via Comment ID 331.
IF33	Martlesham Parish Council (Diane Linsley)	892		We welcome this link to Felixstowe and district, which is already used as a leisure route.	Support noted.
IF33	Martyn Shakespeare	518	Yes	Interesting (and avoids the narrow Brightwell hills where I've had near misses with vehicles on several occasions).	Support noted.
IF33	Nigel Maxwell	809		— please consider a cycle path connecting IF33 and IM4 using the southern perimeter of Martlesham Heath, i.e. without entering Martlesham Heath	Comment noted.

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				— this would be a very quick route from Brightwell Lakes to the start of the Kesgrave Park Run	
IF33	Nigel Maxwell	811		<p>IF33 & PROW46</p> <p>— please ensure that walkers on PROW46 will be able to easily use to the A12 walking bridge to cross over to IF33 Brightwell Lakes</p> <p>— it would be better to re-route the proposed IM12 cycle path around the southern perimeter of Martlesham Heath, using some of the farmer’s field</p>	<p>Comment noted. IM12 has been re-routed around Eagle Way and IM32 has been added to create a connection between IM4 (Long Strods bridleway) and the Brightwell Lakes site. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
IF33	Simon Shaw	585	Yes	<p>Make IF33 emerge at the A12 immediately opposite the proposed bridge to avoid cycling beside the A12. The traffic noise is very high on the existing footpath and even with some separation the noise and pollution will be high.</p>	<p>Comment noted. IF33 has been recorded as MF3 and amended to terminate on Ipswich Road at a key access point into the Brightwell Lakes site. The existing cycling and walking track alongside the A12 has now been absorbed into the recommendations included under IM31, which will be designed, planned and delivered directly through the phased build out of the Brightwell Lakes site.</p>
IF33	Simon Shaw	593	Yes	<p>I have commuted from Felixstowe to Martlesham by cycle in the past and the distance involved is approx 10miles each way. I believe that the cycle route needs to be a fast route if cyclists are to be encouraged to commute. I believe that this can only be achieved if cyclists use the existing rural road system via Brightwell dip, Innocence Lane and Trimley Road to get to Trimley. Consideration should be given to making this route safer for cycling.</p>	<p>Comment noted. The intended benefits of delivering an off-carriageway route between Martlesham and Felixstowe extend beyond the needs of commuter cyclists that are comfortable with the risks associated with on-carriageway, rural road cycling. This route is intended to open up longer distance leisure cycling, reaching groups (such as adults cycling with children) that may not currently cycle or that do not currently cycle longer distances due to the risks posed to them by</p>

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				<p>Additional upgrading of footpaths would I believe encourage leisure cyclists to make the journey to Felixstowe.</p> <p>IF32 seems too wiggly and offers little advantage over the road.</p>	<p>motor vehicles when cycling on-carriageway. The delivery of IF33 (now MF3) would not preclude on-carriageway cycling for those that wished to continue. The rural roads along the most popular on-carriageway route between Martlesham and Felixstowe are difficult to improve on a continuous enough basis to make substantive improvements to safety and convenience due to constraints to widening to create appropriate segregation for cyclists and pedestrians.</p>
IF33	Simon Shaw	594	No	<p>I do not support the upgrade of IF33 where it runs parallel with Newbourne road. Newbourne road at this point is single track and ideally suited for cycling whereas IF33 would pass through woodland and heathland close to the Newbourne Springs reserve.</p> <p>I feel that making Newbourne road safer for cyclists would be a better option with less impact on the countryside.</p>	Objection noted.
IF33	Taylor Wimpey East Anglia (Sir/Madam)	758		<p>Cycling and Walking Strategy</p> <p>Brightwell Lakes is identified within the “Ipswich to Melton Key Corridor” and a number of the recommendations within the Cycling and Walking Strategy relate to existing connections in this part of East Suffolk. Of particular relevance is the following:</p> <ul style="list-style-type: none"> • IF33 Ipswich Road (High Priority): this is described as an opportunity to establish a route between Felixstowe and Martlesham via the permitted Brightwell Lakes development. The route broadly follows the orientation of the Brightwell Lakes Spine 	Comment noted.

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				Road and the connection to Ipswich Road through the SANG valley.	
IF34	Lesley Vince	347	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this area are minimal in comparison to the loss of habitat/habitat changes.</p>	Comment is not relevant to this recommendation; comment relates to recommendation IM12, and has been responded to via Comment ID 331.
IF35	Lesley Vince	348	No	<p>Martlesham woods (Birch Woods) I believe to be the property of MHHL and should not be subject to radical change by another organisation.</p> <p>the woods have an important habitat for wildlife.</p> <p>a cycle/pedestrian route would dramatically change this habitat. We should be preserving not losing such woodland.</p> <p>the woodland is important from a climate point of view in particular as a carbon sink</p> <p>The advantages of a slightly shorter cycle ride in this</p>	Comment is not relevant to this recommendation; comment relates to recommendation IM12, and has been responded to via Comment ID 331.

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				area are minimal in comparison to the loss of habitat/habitat changes.	
3.25 - 3.27	Andy Bird	121	Yes	<ul style="list-style-type: none"> F115 high value route 	Support noted.
3.25 - 3.27	Andy Smith	794		<p>Many of the routes proposed are welcome.</p> <p>However, much greater attention should be paid to the issues of potential new conflicts, as per comment to</p>	Comment noted.
3.25 - 3.27	Felixstowe Town Council (Ash Tadjrishi)	936		<p>Separate note re access to golf course wall from Clifflands</p> <p>Approximately 15 years ago, there were negotiations between the Golf Club, SCC PROW and FTC around the Club's safety concerns on the 2 historic footpaths crossing the course. (FP1 & FP11 Northern section) It was envisaged that they be re-routed via a new ramp provided by the club, on their land northward from the top of the steps to the sea wall path, an excellent new facility. However, those negotiations sadly ended without agreement. It was raised again during the debate on the recent Golf Club planning application, but with a similar result. There is clearly both a logical need and an opportunity currently to re-visit this - if dealt with urgently with the backing of the Cycling & Walking group.</p>	Comment noted.
3.25 - 3.27	Trimley St Mary Parish Council (Debra Cooper)	874		In general, it would make sense to review the scoring associated with all requests made along the Trimley High road as one consolidated request and consider a suitable range of interventions to address various issues as one integrated solution. The Key Corridor Recommendations provided above do seem to have done this, so perhaps the Community Comments Assessments could reference where they	Comment noted.

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				have contributed to the Key Corridor Recommendation?	
F1	Andy Smith	795		All of these elements affecting the High Road West / High Street should be re-examined carefully in the light of feasibility reference conflicts, as above	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
F1	Felixstowe Town Council (Ash Tadjrishi)	924		<p>For the section within the Felixstowe Parish boundary, we strongly support this principle, subject to recognition of the conflicts identified - in particular, parking for shops.</p> <p>We agree with Trimley St. Mary Parish Council that this recommendation should not stop at Church Lane but continue to the Howlett Way roundabout.</p>	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried</p>

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					<p>forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
F1	Martyn Shakespeare	522	Yes	<p>Unfortunately car drivers often park in the existing cycle markings on this road (especially outside the shops). Not sure what can be done about this.</p>	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>In some locations, the existing physical constraints may mean retrofitting active travel infrastructure is either is not possible to be delivered to as high a standard as we</p>

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					would aspire to or not possible at all. This may mean some schemes are unable to be brought forward.
F1	Trimley St Mary Parish Council (Debra Cooper)	298	Yes	This has to be the highest priority given its proximity to the High School, new development nearby and the obvious current dangers. This recommendation should not stop at Church Lane but continue to the Howlett Way roundabout.	<p>Support noted. The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
F2	Anonymous	279	Yes	I agree this is a difficult problem . However we need to prioritise cycling as I am not sure how many will go on the much longer "off road" routes suggested. There is spaces for some cycle ways ,on the main road even if intermittent. Consideration need to be given so that this is not the default route when the A14 is closed .	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried</p>

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					forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.
F2	Felixstowe Town Council (Ash Tadjrishi)	928		NB This is incorrectly titled - it refers to High Road (Trimley) should be corrected. No other comment from FTC	Comment has been noted.
F3	Andy Smith	796		All of these elements affecting the High Road West / High Street should be re-examined carefully in the light of feasibility reference conflicts, as above	Comment noted. This recommendation has been removed.
F3	Felixstowe Town Council (Ash Tadjrishi)	929		Support these plans for shared paths, which are a key element in giving cyclists space, safety and clarity – therefore attracting more people to cycle (instead of driving). Vehicle access retained. There is some confusion between "High Road", "High Road West" and "High Street". F2 is actually in Trimley, despite being called "High Road West", which is a road in Felixstowe.	Comment noted. This recommendation has been removed.
F4	Andy Smith	797		All of these elements affecting the High Road West / High Street should be re-examined carefully in the light of feasibility reference conflicts, as above	The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district. All of the recommended improvements included in the

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					<p>Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>In some locations, the existing physical constraints may mean retrofitting active travel infrastructure is either not possible to be delivered to as high a standard as we would aspire to or not possible at all. This may mean some schemes are unable to be brought forward.</p>
F4	Felixstowe Town Council (Ash Tadjrishi)	930		<p>Support these plans for shared paths, which are a key element in giving cyclists space, safety and clarity – therefore attracting more people to cycle (instead of driving). Vehicle access retained.</p> <p>While desirable from a Cycling perspective, it is doubtful whether it is deliverable, considering both pedestrian and traffic safety.</p> <p>Between Recreation Lane and opposite Seaton Road, the street as a whole is narrow, both footpaths and the road itself. There would appear to be little opportunity to provide a shared path safe while retaining safety for pedestrians and road traffic, even with realignments.</p>	<p>Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. In some locations, the existing physical constraints may mean retrofitting active travel infrastructure is either not possible to be delivered to as high a standard as we would aspire to or not possible at all. This may mean some schemes are unable to be brought forward.</p>

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				See also comment in F124 ref duplicate path south side Seaton Rd to lights.	
F4	Seamus Bennett	200	Yes	Fully support this. Difficult but advantages are significant.	Support noted.
F5	Andy Smith	798		All of these elements affecting the High Road West / High Street should be re-examined carefully in the light of feasibility reference conflicts, as above	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>In some locations, the existing physical constraints may mean retrofitting active travel infrastructure is either not possible to be delivered to as high a standard as we would aspire to or not possible at all. This may mean some schemes are unable to be brought forward.</p>
F5	Felixstowe Town Council (Ash Tadjrishi)	931		This section between important traffic junctions at Garrison Lane and Beatrice Avenue is by far the most heavily trafficked in the town. The need for provision	The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East

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				<p>of safe right turn lanes at Garrison Lane, Springfield Road and Railway approach, in addition to a very busy access to the petrol station, and the protected emergency access to the fire and police stations should not be prejudiced.</p> <p>A narrowing of the Springfield Road junction could only increase potential safety issues with inbound and outbound traffic in conflict on this busy stretch, with several distractions for drivers at that location, which interacts with, but is not controlled by, the traffic signals at Station Approach.</p> <p>It cannot benefit the community if aspirations for safe cycling routes create other safety issues and likely resentment by large sections of the public.</p> <p>Any detailed proposal for this area should demonstrate clearly that the above can be resolved and be subject to wide consultation when a detailed proposal is available for scrutiny.</p> <p>A safe cycle and walking route is also desperately needed between Hamilton Road/Town Centre, the railway station and the new North Felixstowe Garden Neighbourhood development and proposed new leisure centre. This is possible by using the route: Hamilton Road (Great Eastern Square) to the Railway Station, thence Station Approach, across High Road West into Glenfield Avenue, left into Fairfield Avenue. At the northern end of Fairfield Avenue, open up existing pedestrian access onto a segregated cycle route alongside the A154 Garrison Lane northbound to the Grove Road roundabout,</p>	<p>Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>In some locations, the existing physical constraints may mean retrofitting active travel infrastructure is either not possible to be delivered to as high a standard as we would aspire to or not possible at all. This may mean some schemes are unable to be brought forward.</p>

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				linking in with the existing signalled pedestrian crossing to Taunton Road/Candlet Road (with proposed segregated cycle lane as far as Gulpher Road overbridge) - also linking in with the crossing to the Cowpasture Allotments and existing Grove Road segregated cycle lane to the medical centre, Eastward Ho and Abbey Grove.	
F5	Seamus Bennett	201	Yes	again fully support this - vital change with big advantages.	Support noted.
F6	Felixstowe Town Council (Ash Tadjrishi)	932		Agree this is very high priority and despite the high vehicle traffic levels is already much used by (the braver!) cyclists. Imagine how much use it would get, and pollution, noise, road wear and tear it would save if properly segregated along its whole length. Therefore, we would request a 'ribbed' delineation of the cycle lane, plus a coloured surface treatment, to discourage encroachment by vehicles.	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
F6	Seamus Bennett	143	Yes	agree this is very high priority and despite the high vehicle traffic levels is already much used by (the braver!) cyclists. Imagine how much use it would get, and pollution, noise, road wear and tear it would	The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward

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				<p>save if properly segregated along its whole length. Therefore 'bollards' (or similar) as used along parts of the bypass in Ipswich should be introduced, possibly on a trial basis. this would still comfortably allow two lines of vehicle traffic. Yes, it might be 'challenging' in the face of some HRE residents (I live on HRE and would welcome it) but the potential gains are great. This is the kind of move we need to make significant modal shift, with all its advantages, a reality.</p>	<p>improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
F7	Felixstowe Town Council (Ash Tadjrishi)	933		<p>The precise alignment of FP47 is not clear on several maps. Our understanding is that FP47 is along the promenade above the sea wall. This needs to be made clear.</p> <p>This is welcomed in principle. FP47 (on sea wall Brackenbury to Dip) is already classified Bridleway on the Ordnance Survey 1:25K map, but not on the SCC Definitive Map, and should be signed also for cycling, including the existing ramped access down to the sea wall promenade opposite Cliff Court. FP11 is also mentioned and shown as F115. However, as per the map the northern fragment from the sea wall to Ferry Road below the Club House is not appropriate for enhancement, which should be made clear.</p>	<p>Comment has been noted.</p>

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				N B the map is incorrectly annotated at Brackenbury as "Clifflands Park". That area is known as Brackenbury, but "Clifflands " refers to the northern area adjacent to the Golf Club	
F8	Andy Smith	801		<p>This is on the line of the original old path continuing Elmcroft Way straight through Cliff Estate to Ferry Road (north). Currently "No Cycling" ! it is app 2m all the way.</p> <p>I support the proposal for improvement, but strongly request that it be re-classified for shared cycling use, despite beign slightly narrower than the "preferred" style.</p>	This recommendation has been removed from the Strategy.
F8	Felixstowe Town Council (Ash Tadjrishi)	938		<p>This is part of FTC submission ref W6B in our original submission</p> <p>It is on the line of the original old path continuing Elmcroft Way straight through Cliff Estate to Ferry Road (north). Currently "No Cycling" it is approximately 2m width along its length.</p> <p>We support the proposal for improvement, but request that it be re-classified for shared cycling use, as per Note B to this submission</p>	This recommendation has been removed from the Strategy.
F8	Felixstowe Town Council (Ash Tadjrishi)	944		<p>F8</p> <p>National and Local Coastal pedestrian Paths.</p> <p>Designated and signed long distance walking paths are an increasingly important element in encouraging walking for promoting tourism, for leisure and for health.</p>	F8 has been removed from the Strategy.

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				<p>It follows therefore that any Cycling and Walking Strategy, at County, District or local level should have as a core element the recognition, promotion and mapping of relevant paths, and a Strategy Objective to expedite their use both in their own right and for local routes to link to and use them wherever that can be achieved.</p> <p>In Felixstowe (and the whole Orwell North bank) we have:</p> <ol style="list-style-type: none"> 1) The long standing Stour and Orwell Estuaries Walk. 2) The Suffolk Coast Path 3) And in the final stages of Parliamentary approval, 2 sections of the National Coast Path Shotley Gate to Felixstowe Ferry Felixstowe Ferry to Bawdsey <p>Further details of the National Coast Path can be obtained from Natural England</p> <p>We strongly request that these are classified and mapped as part of the ESC Strategy, and that the final Strategy expedites and overtly recognises those, including local signage.</p> <p>Relevant proposals include, but certainly not limited to those below, however we have not individually detailed the many seafront links to the Suffolk Coast Path and the National Coast Path</p> <p>Proposals F8</p>	

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				F9 F40 F95 F113 Objectives 2 Policy Context	
F8	Felixstowe Town Council (Ash Tadjrishi)	956		<p>NOTE B</p> <p>Rigid adherence to 3m minimum width for shared cycling / pedestrian paths.</p> <p>FTC suggest that there are a number of instances where long standing off-highway footways in urban areas, usually designed to be c. 2m in width are a resource suitable in certain cases for shared use. Specifically, these are typically relatively short individual lengths, used almost entirely by local people as everyday desire lines, and hence familiar to the great majority of users, with relatively low usage volumes either by existing pedestrian users or, importantly, potential cycling users. Hence the shared use would appear to present a low risk to shared uses.</p> <p>We believe these are a wasted resource, capable of providing “quick wins” at low cost to create Improved cycling opportunities. We would request this criterion be relaxed in appropriate locations. We suggest this approach is substantiated in the following paragraph from DfT Local Transport Note 1/12, particularly the phrase “preferred” minimum, thus: Paragraph 7.34</p>	F8 has been removed from the Strategy.

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				<p>A width of 3 metres should generally be regarded as the preferred minimum on an unsegregated route, although in areas with few cyclists or pedestrians a narrower route might suffice. Where a significant amount of two-way cycling is expected, additional width could be required. However, the need here for additional width is not clear cut, because the absence of segregation gives cyclists greater freedom to pass other cyclists. It might therefore depend on user flows.</p> <p>Proposals include F8 F104 F108</p>	
F9	Andy Smith	802		<p>F9 from Christmas Yard Woods, and then onwards to Nacton Shore and beyond forms part of the National Coastal Path and the AONB Stour & Orwell Walk. This should be recognised and the route designed accordingly for pedestrians only.</p>	<p>Comment noted. This route includes parallel bridleways and footpaths, with improvements relating to the bridleway, which already permits cycling and walking. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p>

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F9	Felixstowe Town Council (Ash Tadjrishi)	939		F9 from Christmas Yard Woods, and then onwards to Nacton Shore and beyond forms part of the National Coastal Path and the AONB Stour & Orwell Walk. This should be recognised, and the route designed accordingly for pedestrians only. See Note A The additional loop for a local Circular Walk is supported, subject to the conservation caveats quoted, which also apply to F113	This route includes parallel footpaths and bridleways; the recommended improvement relates to the bridleway, where cycling and horse riding are already permitted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.
F9	Felixstowe Town Council (Ash Tadjrishi)	946		<p>National and Local Coastal pedestrian Paths.</p> <p>Designated and signed long distance walking paths are an increasingly important element in encouraging walking for promoting tourism, for leisure and for health.</p> <p>It follows therefore that any Cycling and Walking Strategy, at County, District or local level should have as a core element the recognition, promotion and mapping of relevant paths, and a Strategy Objective to expedite their use both in their own right and for local routes to link to and use them wherever that can be achieved.</p> <p>In Felixstowe (and the whole Orwell North bank) we have:</p>	<p>Comment noted. The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the</p>

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				<p>1) The long standing Stour and Orwell Estuaries Walk.</p> <p>2) The Suffolk Coast Path</p> <p>3) And in the final stages of Parliamentary approval, 2 sections of the National Coast Path</p> <p>Shotley Gate to Felixstowe Ferry</p> <p>Felixstowe Ferry to Bawdsey</p> <p>Further details of the National Coast Path can be obtained from Natural England</p> <p>We strongly request that these are classified and mapped as part of the ESC Strategy, and that the final Strategy expedites and overtly recognises those, including local signage.</p> <p>Relevant proposals include, but certainly not limited to those below, however we have not individually detailed the many seafront links to the Suffolk Coast Path and the National Coast Path</p> <p>Proposals</p> <p>F8</p> <p>F9</p> <p>F40</p> <p>F95</p> <p>F113</p> <p>Objectives</p> <p>2 Policy Context</p>	<p>local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p>
F9	Isobel Fleming	82	No	<p>There is already a bridleway through the Trimley Marshes Nature Reserve which is well used by horseriders and cyclists, in addition to the footpath</p>	<p>Comment noted. As covered in the recommendation, F9 is intended as a leisure route (not intended for commuting purposes) and therefore facilitating faster</p>

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				<p>only along the sea wall. The surfacing is suited to the area and is perfectly adequate for both cyclists and horseriders and should not be replaced to make it suitable for faster cycling - in many places the surface requires riders to merely walk through the Reserve, and this should be kept as the status quo.. There is no impact from horseriding on the nesting birds.</p>	<p>cycling speeds is not the intention of improvements. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p>
F9	Peter Franklin	163	No	<p>There should be cycling access all the way from Trimley Marshes all the way along the River Orwell riverside footpath. This is a fantastic leisure cycle route, providing access to the beautiful and varied areas along the river that just wouldn't be possible to see in one trip if walking for say a couple of hours.</p> <p>Whilst in some places the track is narrow, it is perfectly possible for cyclists and pedestrians to pass each other if care is taken and cyclists stop. In my experience this has always worked well.</p> <p>I cannot believe that cycling would be any more disruptive to ground nesting birds than walking, if using the track. In fact cyclists are more likely to stick to the track than walkers.</p> <p>This is one of my favourite cycling routes in the area precisely because it's nowhere near roads and</p>	Support noted.

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				I therefore feel safer; plus benefitting from the great views and a good couple of hours of exercise.	
F9	Trinity College, Cambridge (Sir/Madam)	1070		<p>Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College’s landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to the wider Trimley Estate, owned and controlled by Trinity College Cambridge (including recommendations F9, IF22, IF24, F10, F36, F113, F38)</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals affecting the Trimley Estate, referenced above, subject to detailed exploration of feasibility, viability and agreement of</p>	<p>Support noted. The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p>

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				<p>details as part of future masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across the Estate in more detail. The delivery and funding of proposals requiring significant new infrastructure, such as hard surfaced routes and lighting and signage, would need to be subject to further discussion.</p> <p>We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.</p>	
F10	Isobel Fleming	84	No	<p>Any modifications to allow cycling and walking along this proposed route must also provide for the needs of horseriders who use this area extensively owing to the location of a livery yard. Any upgrading should be made to a bridleway or Byeway with adequate provision of surface and safe access. It seems the needs of horseriders has been totally disregarded throughout this whole plan, even down to the document's name " Cycling and Walking". This is a rural area at present, although being decimated by huge building schemes, which</p>	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they</p>

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				makes it even more important that the safety of horses and their riders is paramount in any planning.	<p>will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p> <p>It is intended that in locations that are appropriate and of relevant routing for horse-riding, the infrastructure design will reflect this through all-user design.</p>
F10	Pigeon Investment Management Ltd (Fillmore, Andrew)	1062		We generally support the proposals in relation F32. In particular, the section between Goslings Farm track and Grimston Lane. We also support the section of F10 that would provide a link between F32 and F11.	Support for this recommendation is noted.
F10	Trinity College, Cambridge (Sir/Madam)	1073		<p>Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College's landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p>	Support noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.

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				<p>Proposals relevant to the wider Trimley Estate, owned and controlled by Trinity College Cambridge (including recommendations F9, IF22, IF24, F10, F36, F113, F38)</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals affecting the Trimley Estate, referenced above, subject to detailed exploration of feasibility, viability and agreement of details as part of future masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across the Estate in more detail. The delivery and funding of proposals requiring significant new infrastructure, such as hard surfaced routes and lighting and signage, would need to be subject to further discussion.</p> <p>We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided</p>	

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				where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.	
F11	Pigeon Investment Management Ltd (Fillmore, Andrew)	1069		We generally support the Council's aspiration to create a route along the western edge of the Trimley villages that will encourage walking and cycling. The Pigeon scheme has been designed to allow for the retention and continued use of Footpath 31/Footpath 4, which will be retained as part of a green corridor through the Pigeon scheme, thereby providing for connectivity with F11.	Support for this recommendation has been noted.
F12	Pigeon Investment Management Ltd (Fillmore, Andrew)	1077		This recommendation is noted.	This recommendation has been removed.
F13	Pigeon Investment Management Ltd (Fillmore, Andrew)	1078		<p>FP13 includes two parts: an east-west connection and north-south connection. The east-west section could be accommodated within the Pigeon scheme.</p> <p>However, any improvements to the surface of the east-west section of F13 should be carried out by the Council as part of the wider improvements to Footpath 31 (F11).</p> <p>In respect of the north-south connection, this appears to follow the alignment of the primary street that will be created as part of the Pigeon scheme. Whilst we support the creation of this link, which will provide pedestrian and cycle connectivity with the new school, the alignment of the primary</p>	Support noted. Recommendations relating to this site allocation have been amended.

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				<p>street is indicative at this stage so the alignment of the north-south element of F13 will need to follow the approved road alignment.</p> <p>F13 refers to the creation of a link with Gun Lane. However, the Council’s mapping shows this section as F14 - please see our comments to F14 below in respect of this part of the corridor recommendation.</p>	
F14	Alison Vickers	578	Yes	<p>We agree that this route should be given a very high priority because of the reasons listed. However, please could the improvements continue along Gun Lane to the High Road. This is only a few extra feet of track and would be so useful to people living around the Gun Lane area.</p>	<p>Support noted. Recommendations relating to this site allocation have been amended.</p>
F14	Pigeon Investment Management Ltd (Fillmore, Andrew)	1079		<p>F14 includes two elements: a section along Gun Lane and a section through the Pigeon scheme. We generally support the proposed section through the Pigeon scheme, which makes provision for cycle and pedestrian links between Gun Lane and the primary street (F13) and proposed primary school site. The Pigeon scheme also provides for a financial contribution towards improvements to Gun Lane between the site and High Road, which we note is omitted from the draft walking and cycling strategy map.</p> <p>However, in respect of the section of F14 along Gun Lane, we note that any benefits associated with this improvement are largely dependent on improvements to the existing PROWs to the south of the railway line. As such, we would suggest that any improvements to Gun Lane (southwest of the Pigeon</p>	<p>The recommendations relating to this site allocation have been amended.</p>

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				scheme) would need to be brought forward as part of a wider set of improvements, including those south of the railway line.	
F16	Pigeon Investment Management Ltd (Fillmore, Andrew)	1059		As per our response to the community recommendations above, the Pigeon scheme includes the provision of a series of off-site highway improvements that will be secured via planning condition. These works will include the provision of new cycle/footway infrastructure and improvements to the existing refuge islands at the roundabout that forms the junction between High Road and Howlett Way.	Comment noted.
F18	Pigeon Investment Management Ltd (Fillmore, Andrew)	1060		The Pigeon scheme makes provision for this route to be retained as part of the scheme. It should be noted, that the western part of F18 is proposed to be retained as within an area of green amenity space. As such, the walking and cycling strategy should allow for appropriate surface treatments between these two respective parts. i.e. it should allow for the western section to be retained as an informal path, rather than a sealed surface.	Comment noted.
F19	Trimley St Martin Parish Council, (Caroline Ley)	311	Yes	<i>This route has been marked as high priority. Trimley St Martin Parish Council considers that, like F20, it should be very high priority because of the link to F26 and underpass to Candlet Track.</i>	This recommendation has been removed from the Strategy.
F22	Trimley St Mary Parish Council (Debra Cooper)	873	Yes	F22 AGREE with Recommendation (Church Lane) This road is even narrower than Thurmans Lane and with steep banks either side offers no refuge at all along a significant proportion of its length for pedestrians or cyclists should they meet any traffic. This route is only viable in conjunction with the planned development on Howlet Way, and even then offers limited scope and opportunity.	Support noted.

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F24	Trimley St Martin Parish Council, (Caroline Ley)	310	Yes	<i>F24 – This is described in the text as remaining as it follows a route through trees. Trimley St Martin Parish Council comment that this would not be suitable for cyclists due to soil erosion and subsequent protrusion of tree roots and generally uneven ground.</i>	Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme.
F25	British Horse Society (Lynda Warth)	670		<p>The information above is to support the principle and some of the detail of the need to include equestrians in the LCWIP project and to protect the amenity of the bridleway network for equestrians since that is the only network available to them.</p> <p>I am conscious that I have not addressed the question of the actual routes included in the consultation. I cannot provide this information as I do not know the area well enough. This information needs to be provided by local horse riders however, due to the lack of BHS Access Officers in Suffolk, it may well take time to collate. Caroline and I would appreciate guidance on which information you require as a priority.</p> <p>However, the following boxes in the consultation do raise concern because they all refer to bridleways and potential ‘improvements’ or ‘resurfacing’ to the path surface, ostensibly for the benefit of cyclists. Care must be taken to ensure that any changes do not discriminate against the intended</p>	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment. It is intended that in locations that are</p>

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				<p>bridleway user – equestrians - and that shared use does not result in the displacement or reduction in amenity of existing users – this includes the allocation of path width for a hard top path to enable cyclists to ride side by side whilst reducing the same social amenity for horse riders.</p> <p>The term ‘improvement’ has been challenged in Court.</p> <p>Section 62 (2) of the Highways Act 1980 gives Local Highway Authorities a power to carry out works to improve highways.</p> <p>The Court of Appeal in the case of Cowen –v- Secretary of State for the Department of Environment Transport and the Regions (1999) 3PLR108 concluded that if the construction of a hard surface changes the character of a way it goes beyond ‘improvement’.</p> <p>It will always be a matter of fact that tarmacing a bridleway will change its character. When deciding to tarmac a bridleway, the highway authority should take safety implications and the enjoyment of current users, including riders, into consideration.</p> <p>Improvements to a bridleway must make the way easier, safer, or more enjoyable its users, enhancing its suitability for use. Any works to a bridleway which detract from its suitability might not be a proper exercise of the highway authority’s power to improve a highway.</p>	<p>appropriate and of relevant routing for horse-riding, the infrastructure design will reflect this through all-user design.</p>

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				<p>Any changes to crossings which are proposed to link new routes must be multi user including equestrians. Any barrier along a route renders the whole route useless. All routes need to be RSA assessed from the outset to ensure they are not delivered and then discovered not to be suitable for equestrians.</p> <p>Care needs to be taken in the use of the Strava information – this is not a valid NMU representative group. Also, as we mentioned, the Strava purportedly cycling information, may well be distorted by equestrians who use the app but without the opportunity to register as horse riders rather than cyclists.</p>	
F25	Jayne Smith-Howell	162		<p>When considering the surface for Bridleways 5 and 10 leading to the Candlet Track, and the Candlet Track which is also a bridleway it is suitable for equestrian use.</p>	<p>Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment. It is intended that in locations that are appropriate and of relevant routing for horse-riding, the infrastructure design will reflect this through all-user design.</p>

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F26	Martyn Shakespeare	521	Yes	Useful	Support noted.
F26	Trimley St Mary Parish Council (Debra Cooper)	301	No	There is a recognised need to provide a facility for pedestrians along the length of Thurmans Lane. This need will only grow with additional housing on the Mill Farm development and also Howlett Way. As a minimum, pedestrian shelters should be provided to allow safe passing of pedestrians and vehicles.	Comment noted.
F27	Trimley St Mary Parish Council (Debra Cooper)	300	Yes	There should be a footpath/cycle path across St Marys Green and parallel to the fence where an existing footpath is visible by wear. Passive footpath provision should be made for a pedestrian/cycling pathway to be available along part of the village green where it fringes or is part of nearby development land. It would be a good idea to link with the bridleway on to 'Mill Farm new estate.'	Comment noted.
F29	Trimley St Mary Parish Council (Debra Cooper)	299	Yes	AGREE with Recommendation (Faulkeners Way)	Support noted.
F30	Felixstowe Town Council (Ash Tadjrishi)	941	Yes	Support.	Support noted.
F30	Trinity College, Cambridge (Sir/Madam)	1067		<p>Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College's landholdings</p>	Support noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. The

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				<p>at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to North Felixstowe Garden Neighbourhood (recommendations F49, F45, F46, F30, F51)</p> <p>Trinity College owns and controls land at the western end of the North Felixstowe Garden Neighbourhood allocation and is working with the Council and other stakeholders to bring it forward for development in due course.</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals associated with the North Felixstowe Garden Neighbourhood allocation, referenced above, subject to detailed exploration of feasibility, viability and agreement of details as part of the masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across North Felixstowe Garden Neighbourhood site and the College's land holdings more generally in detail.</p>	<p>assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment. It is intended that in locations that are appropriate and of relevant routing for horse-riding, the infrastructure design will reflect this through all-user design.</p>

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				We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes developed in East Suffolk, and in Felixstowe and the Trimley villages in particular, is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.	
F31	Andy Smith	812		<p>This path as already been implemented and in use as part of the “Laureate Fields” development, as a footpath.</p> <p>The eastern end could be reclassified for cycling use, but the western part is now largely adoptable as highway on completion of the remainder of the development.</p> <p>Hence the proposal as written is superfluous in the context of the Strategy, and should updated as above.</p>	Comment noted.
F31	Felixstowe Town Council (Ash Tadjrishi)	945		Support the aim of facilitating easy shared use cycle/walking route across Laureate Fields development to Ferry Road.	Support noted.
F32	Pigeon Investment Management Ltd (Fillmore, Andrew)	1061		We generally support the proposals in relation F32. In particular, the section between Goslings Farm track and Grimston Lane. We also support the section of F10 that would provide a link between F32 and F11.	Support noted.

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F33	Andy Smith	814		Welcome this proposal	Support noted.
F33	Felixstowe Town Council (Ash Tadjrishi)	948		Welcome this proposal	Support noted.
F34	Andy Smith	815		Welcome this proposal	Support noted.
F34	Felixstowe Town Council (Ash Tadjrishi)	934		<p>This is the access road from ESC car park and will shortly also become the access to the repositioned Golf Club car park. Therefore, a Bridleway is not appropriate in this location. It is not currently a PROW footpath.</p> <p>The land is owned by ESC, with public access, the details of which should remain to be determined by them, not via PROW.</p> <p>It has planning permission for significant changes in connection with the Club's proposed new club house. It is currently in practice fully suitable for Cycling & Walking, except for the "out only" rising steels, but they can be bypassed. Again, there should be early discussions via ESC Planning and Asset Management to ensure Cycling & Walking is overtly incorporated into the scheme when built.</p>	Comment noted.
F34	Felixstowe Town Council (Ash Tadjrishi)	950		Welcome this proposal	Support noted.
F34	Seamus Bennett	138	Yes	I feel this should be a high priority to upgrade existing footpath to bridleway as the Dip road is a fast moving stretch. In order to encourage families / children to cycle to the ferry rather than drive. This could be an attractive link to the 'prom' (especially if upgraded as planned) to allow traffic-free cycle route to the Ferry avoiding the fast moving road through golf course. This is also part of the national cycle route incorporating the Bawdsey ferry.	Support noted.

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				Currently this route is attractive and well used by experienced cyclists but has great potential to be accessed by less confident cyclists and children, if the traffic-free opportunities are implemented.	
F34	Seamus Bennett	139	Yes	please see my comment on F34 - changing the status should be a HIGH priority as relatively easily and cheaply done, but contributes to traffic-free route to Fx Ferry	Comment noted.
F36	Trimley St Mary Parish Council (Debra Cooper)	295	No	Trimley St Mary Parish Council disagree with this recommendation as Keepers Lane is currently an earth farm track and should stay this way. This is a serene woodland walk that would lose its character if it was to be tarmacked over. It is also very narrow in places and not suitable for significant numbers of cyclists. Cycling traffic (for the Port) joins the Blofield Track (F37) via Cordys Lane. The Blofield Track route (F37) needs to be properly surfaced for cycling traffic and enhanced for walkers.	<p>Comment noted. The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
F36	Trinity College, Cambridge (Sir/Madam)	1074		Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the	Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and

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				<p>Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College’s landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to the wider Trimley Estate, owned and controlled by Trinity College Cambridge (including recommendations F9, IF22, IF24, F10, F36, F113, F38)</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals affecting the Trimley Estate, referenced above, subject to detailed exploration of feasibility, viability and agreement of details as part of future masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across the Estate in more detail. The delivery and funding of proposals requiring significant new infrastructure, such as hard</p>	<p>optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p>

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				<p>surfaced routes and lighting and signage, would need to be subject to further discussion.</p> <p>We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.</p>	
F37	Andy Smith	817	Yes	<p>Strongly supported - part of essential long distance options Felixstowe -Ipswich See also section on SCLP12.7 PoF Recommendation,as referred to on map,</p>	Support noted.
F37	Felixstowe Town Council (Ash Tadjrishi)	959		<p>Strongly supported as part of essential long-distance options between Felixstowe and Ipswich See also section on SCLP12.7 PoF recommendation, as referred to on map.</p>	Support noted.
F37	Trimley St Mary Parish Council (Debra Cooper)	296	Yes	<p>Blofield Track (F37) needs to be properly surfaced for cyclists and enhanced for walkers.</p>	Support noted.
F38	Andy Smith	818	Yes	<p>Strongly supported as a core link to Walton N and NFGV.</p>	Support noted.
F38	Felixstowe Town Council (Ash Tadjrishi)	961	Yes	<p>Strongly supported as a core link to between Walton and the Felixstowe North Garden Neighbourhood.</p>	Support noted.
F38	Trimley St Mary Parish Council (Debra Cooper)	297	Yes	<p>Both accessibility to this footpath and the footpath itself would need to be addressing. Once both of these issues are addressed it's hard not to see this</p>	Support noted.

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				becoming a route for cyclists commuting to the Port so cycling infrastructure would be needed.	
F38	Trinity College, Cambridge (Sir/Madam)	1076		<p>Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College’s landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to the wider Trimley Estate, owned and controlled by Trinity College Cambridge (including recommendations F9, IF22, IF24, F10, F36, F113, F38)</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals affecting the Trimley Estate, referenced above, subject to detailed exploration of feasibility, viability and agreement of</p>	<p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment. It is intended that in locations that are appropriate and useful for horse-riding, the infrastructure design will reflect this through all-user design.</p>

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				<p>details as part of future masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across the Estate in more detail. The delivery and funding of proposals requiring significant new infrastructure, such as hard surfaced routes and lighting and signage, would need to be subject to further discussion.</p> <p>We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.</p>	
F39	Andy Smith	819	Yes	<p>I strongly support this in principle, especially on Nicholas Rd.</p> <p>However, issues of adjacent land ownership (leases) will be challenging at Blofield junction and most of Parker Avenue.</p> <p>Early engagement with Trinity Estates via Bidwells is recommended.</p> <p>Ref SCLP12.7</p>	Support noted.
F39	Felixstowe Town Council (Ash Tadjrishi)	962		<p>The principle is strongly supported, especially on Nicholas Road. However, issues of adjacent land ownership (leases) will be challenging at Blofield junction and most of Parker Avenue. Early</p>	Support noted.

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				engagement with Trinity Estates via Bidwells is recommended. Ref SCLP12.7	
F40	Andy Smith	820		<p>I support the proposal - as far as it goes. However there is a major omission in not extending the full length of Fagbury Rd to the railway level crossing - at least as pedestrian route. This is to be part of the national Coastal Path, and is already the recognised long distance Stour and Orwell Walk See Note A to this submission</p> <p>It is unacceptable, as mentioned elsewhere, that the proposed C&W Strategy does not recognise and expedite the existence of the National Coastal Path. This was FTC proposal W1A (mapped as 690)</p> <p>I am disappointed at the dismissive response to that: "No foreseen connectivity and growth benefits" , etc. I strongly object to this omission and the inappropriate comment to 690.</p>	Comment noted.
F40	Felixstowe Town Council (Ash Tadjrishi)	947		<p>National and Local Coastal pedestrian Paths.</p> <p>Designated and signed long distance walking paths are an increasingly important element in encouraging walking for promoting tourism, for leisure and for health.</p> <p>It follows therefore that any Cycling and Walking Strategy, at County. District or local level should have as a core element the recognition, promotion and mapping of relevant paths, and a Strategy Objective to expedite their use both in their own right and for local routes to link to and use them wherever that can be achieved.</p>	Comment noted.

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				<p>In Felixstowe (and the whole Orwell North bank) we have:</p> <ol style="list-style-type: none"> 1) The long standing Stour and Orwell Estuaries Walk. 2) The Suffolk Coast Path 3) And in the final stages of Parliamentary approval, 2 sections of the National Coast Path Shotley Gate to Felixstowe Ferry Felixstowe Ferry to Bawdsey <p>Further details of the National Coast Path can be obtained from Natural England</p> <p>We strongly request that these are classified and mapped as part of the ESC Strategy, and that the final Strategy expedites and overtly recognises those, including local signage.</p> <p>Relevant proposals include, but certainly not limited to those below, however we have not individually detailed the many seafront links to the Suffolk Coast Path and the National Coast Path</p> <p>Proposals F8 F9 F40 F95 F113</p> <p>Objectives 2 Policy Context</p>	

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F40	Felixstowe Town Council (Ash Tadjrishi)	963		<p>We support the proposal - as far as it goes. However there is a major omission in not extending the full length of Fagbury Road to the railway level crossing - at least as pedestrian route. This is to be part of the national Coastal Path and is already within the recognised long-distance Stour and Orwell Walk (see Note A to this submission).</p> <p>It is unacceptable, as mentioned elsewhere, that the proposed Cycling & Walking Strategy does not recognise and expedite the existence of the National Coastal Path. This was FTC proposal W1A (mapped as 690)</p> <p>We are disappointed at the dismissive response to that: "No foreseen connectivity and growth benefits" etc. FTC strongly objects to this omission and the inappropriate comment to 690. See also comments on F9 and F113 above reference Coastal Path</p>	Comment noted.
F40	Seamus Bennett	151	Yes	<p>why not a link through to the Trimley marshes area to make this a complete route from Trimley into western Felixstowe. This could be very attractive to many cyclists</p>	Comment noted.
F41	Felixstowe Town Council (Ash Tadjrishi)	975	Yes	Support	Support noted.
F42	Felixstowe Town Council (Ash Tadjrishi)	976	Yes	Support	Support noted.
F43	Andy Smith	821	Yes	<p>Strongly support.</p> <p>This is one of a number of issues related to the Permissive map being grossly out of date,</p> <p>In this case the building of Dock Spur Road, including</p>	Comment noted.

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				changes to the Parish Boundary and several rights of way. Clearly the A14 footbridge should be a PROW. This and the other related issues require urgent attention by SCC ROW Dept.	
F43	Felixstowe Town Council (Ash Tadjrishi)	980	Yes	<p>Strongly support.</p> <p>This is one of a number of issues related to the Permissive map being grossly out of date, See Note C</p> <p>In this case the building of Dock Spur Road, including changes to the Parish Boundary and several rights of way. Clearly the A14 footbridge should be a PROW. This and the other related issues require urgent attention by SCC ROW Dept.</p>	Comment noted.
F44	Alison Vickers	576	Yes	I think that this new track would be a great idea. It would make it much easier for people from Kirton and Trimley St Martin to access the Capel Hall picnic site and the footpaths off Capel Hall Lane	Support noted.
F44	Felixstowe Town Council (Ash Tadjrishi)	1001	Yes	Strongly support. ESC should ensure that this is delivered via the NFGN Master Plan, and available at an early stage in development of the NW area	Comment noted.
F44	Martyn Shakespeare	520	Yes	Nice	Support noted.
F45	Andy Smith	822		<p>I support the proposal to upgrade and consolidate this existing bridleway from Kirton and consider this to be a very high priority to provide an attractive, relatively low-cost green corridor in and out of Felixstowe (including the NFGN) taking pressure off and providing alternative to overused Walton High Street.</p> <p>However, i object to the proposal to close Gulpher</p>	This recommendation has been removed from the Strategy.

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				<p>Road from Hill House corner to Treetops.</p> <p>This proposal is not overtly indicated on the map. This is wholly inappropriate, as many consultees will depend on the map to drive their comments. The map simply indicates F44 as affecting Candlet track.</p> <p>It is important to recognize:</p> <ul style="list-style-type: none"> • the Persimmon permission, notably as it relates to the northern boundary. • the NFGN allocation policy, even the in the limited detail currently available - notably NO vehicular access across Grove or from any section of Gulpher Road (See extract below from SCLP para.12.49*), the map at para. 12.62, clearly indicating the areas for development separated along the whole northern side by a green buffer south of Gulpher Road. • The presence of other dwellings, businesses and farms along the whole of Gulpher Road, all of whom would be "re-routed" to travel for their access to " Ferry Road/Colneis Road/Candlet Road". <p>This also appears to be contrary to the policy relating to NFGN as per SCLP12.3**</p> <ul style="list-style-type: none"> • Specifically, the existence, for good or ill, of significant business units at Gulpher Business Park, all of whose traffic would 	

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				<p>have to go all along the main narrow length of Gulpher Rd. to Ferry Rd.</p> <p>*SCLP2020 para. 12.49:</p> <p><i>Currently Candlet Road is the northern boundary of Felixstowe and as such is anticipated to provide the primary vehicular access points into the areas to be masterplanned. It is anticipated that a site of this size will require multiple access point for vehicular traffic as well as further links provided solely for walking and cycling. Opportunities to create a network of vehicular and non-vehicular (pedestrian and cycling) links throughout the Garden Neighbourhood are to be explored to provide interconnectivity and free movement through the site and beyond. New vehicular junctions will need to be established to provide access from Candlet Road and ensure That Gulpher Road which is designated "Quiet Lane" is not used for vehicular access. The Council will work closely with landowners, Suffolk County Council and Highways England to establish the most appropriate highway access points.</i></p> <p>**SCLP Policy 12.3:</p> <p><i>1. m) A network of pedestrian, cycling and vehicular routes that provide connectivity and movement across the Garden Neighbourhood and with adjacent areas whilst protecting and enhancing local Quiet Lanes;</i></p> <p><i>n) Provision of new vehicular access points off</i></p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<i>Candlet Road and/or improvements to existing accesses supported by further access for pedestrian and cycle traffic in other locations;</i>	
F45	Felixstowe Town Council (Ash Tadjrishi)	1003		<p>We support the proposal to upgrade and consolidate this existing bridleway from Kirton and consider this to be a very high priority to provide an attractive, relatively low-cost green corridor in and out of Felixstowe (including the NFGN) taking pressure off and providing alternative to overused Walton High Street.</p> <p>However, we object to the proposal to close Candlet Road from Hill House corner to Treetops. This proposal is not overtly indicated on the map. This is wholly inappropriate, as many consultees will depend on the map to drive their comments. The map simply indicates F44 as affecting Candlet track.</p> <p>It is important to recognize:</p> <ul style="list-style-type: none"> • the Persimmon permission, notably as it relates to the northern boundary. • the NFGN allocation policy, even the in the limited detail currently available - notably NO vehicular access across Grove or from any section of Gulpher Road (See extract below from SCLP para.12.49*), the map at para. 12.62, clearly indicating the areas for development separated along the whole northern side by a green buffer south of Gulpher Road. • The presence of other dwellings, businesses and farms along the whole of Gulpher Road, all of whom would be "re-routed" to travel for their access to " 	This recommendation has been removed from the Strategy.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>Ferry Road/Colneis Road/Candlet Road".</p> <p>This also appears to be contrary to the policy relating to NFGN as per SCLP12.3**</p> <ul style="list-style-type: none"> Specifically, the existence, for good or ill, of significant business units at Gulpher Business Park, all of whose traffic would have to go all along the main narrow length of Gulpher Road. to Ferry Road. Any measures that can be taken to consolidate the Quiet Lane status of Gulpher Road would be welcomed – for example, speed restrictions and other traffic calming measures from the urban part of the road along the more rural elements. <p><i>*SCLP2020 para. 12.49:</i></p> <p><i>Currently Candlet Road is the northern boundary of Felixstowe and as such is anticipated to provide the primary vehicular access points into the areas to be masterplanned. It is anticipated that a site of this size will require multiple access point for vehicular traffic as well as further links provided solely for walking and cycling. Opportunities to create a network of vehicular and non-vehicular (pedestrian and cycling) links throughout the Garden Neighbourhood are to be explored to provide interconnectivity and free movement through the site and beyond. New vehicular junctions will need to be established to provide access from Candlet Road and ensure That Gulpher Road which is designated "Quiet Lane" is not used for vehicular access. The Council will work closely with landowners, Suffolk</i></p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>County Council and Highways England to establish the most appropriate highway access points.</p> <p>**SCLP Policy 12.3:</p> <p><i>m) A network of pedestrian, cycling and vehicular routes that provide connectivity and movement across the Garden Neighbourhood and with adjacent areas whilst protecting and enhancing local Quiet Lanes.</i></p> <p><i>n) Provision of new vehicular access points off Candlet Road and/or improvements to existing accesses supported by further access for pedestrian and cycle traffic in other locations;</i></p>	
F45	Trinity College, Cambridge (Sir/Madam)	1065		<p>Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College's landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to North Felixstowe Garden Neighbourhood (recommendations F49, F45, F46, F30, F51)</p>	This recommendation has been removed from the Strategy.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>Trinity College owns and controls land at the western end of the North Felixstowe Garden Neighbourhood allocation and is working with the Council and other stakeholders to bring it forward for development in due course.</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals associated with the North Felixstowe Garden Neighbourhood allocation, referenced above, subject to detailed exploration of feasibility, viability and agreement of details as part of the masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across North Felixstowe Garden Neighbourhood site and the College's land holdings more generally in detail.</p> <p>We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes developed in East Suffolk, and in Felixstowe and the Trimley villages in particular, is designed in a manner that is sensitive to the rural or semi-rural environment of</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.	
F46	Felixstowe Town Council (Ash Tadjrishi)	1008	Yes	Strongly supported, and see F47 below	Support noted.
F46	Trinity College, Cambridge (Sir/Madam)	1066		<p>Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College's landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to North Felixstowe Garden Neighbourhood (recommendations F49, F45, F46, F30, F51)</p> <p>Trinity College owns and controls land at the western end of the North Felixstowe Garden Neighbourhood allocation and is working with the Council and other stakeholders to bring it forward for development in due course.</p>	<p>Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment. It is intended that in locations that are appropriate and useful for horse-riding, the infrastructure design will reflect this through all-user design.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals associated with the North Felixstowe Garden Neighbourhood allocation, referenced above, subject to detailed exploration of feasibility, viability and agreement of details as part of the masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across North Felixstowe Garden Neighbourhood site and the College's land holdings more generally in detail.</p> <p>We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes developed in East Suffolk, and in Felixstowe and the Trimley villages in particular, is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.</p>	

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F47	Andy Smith	823		<p>These issues are at an advanced stage of discussion reference the current Planning Application DC/16/2778/OUT and recent associated DC/21/1322/ARM, DC/21/3662/ARM , all commented on by FTC, and others. See all FTC responses on each. The current proposals from Bloor Homes cover some of the issues referred to here under F47, and by FTC, but not all.</p> <p>Specifically:</p> <p>1)New roundabout entrances are to be constructed on High Road and Candlet Road, with a linking "boulevard" creating a core north / South route, but there are some concerns around its detail design in relation to the adjacent highway.</p> <p>2) It is essential that high quality C&W links are provided in each case, especially to C&W routes along both sides of Candlet Road, and to the NFGN. The detail of that is still uncertain, not least as the Candlet roundabout has been designed for a 60 MPH Road, but consultation is now complete regarding reduction to 40MPH, which would require a different specification for the roundabout.</p> <p>3) A western link to Treetops is to be provided, not via the old "dumb end" in Treetops, which is unfortunately subject to a ransom strip, but via a new cul-de sac and 3m path connection to the land between Treetops and Ash Tree Close, owned by SCC and we understand SCC are likely to seek a contribution for that footpath to be linked through. This replaces an existing permissive path from FP28 on Candlet Road along the north and eastern perimeters to an "informal" link to the above. (through the hedge!)</p>	Comment noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				4) In the context of F38 and others for a primary strategic route from Walton N, and the whole NFGN complex, to the Port, Western Felixstowe and to Ipswich, a high quality shared link to the SW corner of this site adjacent to the A14 overbridge will be essential. However, the entrance to the High Road is split between the boundaries of the current residential application and the future business units allocation on the High Road frontage, hence Bloors are currently proposing only a 2m path within the current site boundary. I hope that SCC Highways and ESC Planning will be able to reconcile this temporary conflict in the context of both sites.	
F47	Felixstowe Town Council (Ash Tadjrishi)	1019		<p>These issues are at an advanced stage of discussion reference the current Planning Application DC/16/2778/OUT and recent associated DC/21/1322/ARM, DC/21/3662/ARM, all commented on by FTC, and others. See all FTC responses on each. The current proposals from Bloor Homes cover some of the issues referred to here under F47, and by FTC, but not all.</p> <p>Specifically:</p> <p>1) New roundabout entrances are to be constructed on High Road and Candlet Road, with a linking "boulevard" creating a core north / South route, but there are some concerns around its detail design in relation to the adjacent highway.</p> <p>2) It is essential that high quality Cycling & Walking links are provided in each case, especially to Cycling & Walking routes along both sides of Candlet Road,</p>	Comment noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>and to the NFGN. The detail of that is still uncertain, not least as the Candlet roundabout has been designed for a 60 MPH Road, but consultation is now complete regarding reduction to 40MPH, which would require a different specification for the roundabout.</p> <p>3) A western link to Treetops is to be provided, not via the old "dumb end" in Treetops, which is unfortunately subject to a ransom strip, but via a new cul-de sac and 3m path connection to the land between Treetops and Ash Tree Close, owned by SCC and we understand SCC are likely to seek a contribution for that footpath to be linked through. This replaces an existing permissive path from FP28 on Candlet Road along the north and eastern perimeters to an "informal" link to the above. (Through the hedge!)</p> <p>4) In the context of F38 and others for a primary strategic route from Walton N, and the whole NFGN complex, to the Port, Western Felixstowe and to Ipswich, a high-quality shared link to the SW corner of this site adjacent to the A14 overbridge will be essential. However, the entrance to the High Road is split between the boundaries of the current residential application and the future business units allocation on the High Road frontage, hence Bloors are currently proposing only a 2m path within the current site boundary. We hope that SCC Highways and ESC Planning will be able to reconcile this temporary conflict in the context of both sites.</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
F48	Andy Smith	824		<p>Supported in principle. However, the conflict on FP31 "not wide enough for cycling", and the benefits to be gained from shared use are unlikely to be resolved if the full width is insisted upon, as the adjacent land is residences in the newly completed Walton Hall development to the west and Felixstowe School to the east. This should not be allowed to prejudice its early provision via improved maintenance, particularly given the likely user base, who will be familiar with that situation.</p> <p>Additionally, unfortunately there is no connection between several available points in the Walton Hall estate and FP31, most notably in the SE corner, where suitable space exists. We would request that SCC Highways and ESC Planning seek this by negotiation with relevant parties. This would yield a significant benefit to the estate residents in access via the bridge to west Felixstowe and the Port.</p>	Comment noted.
F48	Felixstowe Town Council (Ash Tadjrishi)	1024		<p>Supported in principle. However, the conflict on FP31 "not wide enough for cycling", and the benefits to be gained from shared use are unlikely to be resolved if the full width is insisted upon, as the adjacent land is residences in the newly completed Walton Hall development to the west and Felixstowe School to the east. This should not be allowed to prejudice its early provision via improved maintenance, particularly given the likely user base, who will be familiar with that situation.</p> <p>Additionally, unfortunately there is no connection between several available points in the Walton Hall estate and FP31, most notably in the SE corner,</p>	Comment noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				where suitable space exists. We would request that SCC Highways and ESC Planning seek this by negotiation with relevant parties. This would yield a significant benefit to the estate residents in access via the bridge to west Felixstowe and the Port.	
F49	Felixstowe Town Council (Ash Tadjrishi)	1029	Yes	Strongly support	Support noted.
F49	Trinity College, Cambridge (Sir/Madam)	1064		<p>Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p> <p>We would welcome a meeting to discuss the detail of the proposals affecting the College's landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to North Felixstowe Garden Neighbourhood (recommendations F49, F45, F46, F30, F51)</p> <p>Trinity College owns and controls land at the western end of the North Felixstowe Garden Neighbourhood allocation and is working with the Council and other stakeholders to bring it forward for development in due course.</p>	<p>Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment. It is intended that in locations that are appropriate and useful for horse-riding, the infrastructure design will reflect this through all-user design.</p>

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				<p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals associated with the North Felixstowe Garden Neighbourhood allocation, referenced above, subject to detailed exploration of feasibility, viability and agreement of details as part of the masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across North Felixstowe Garden Neighbourhood site and the College's land holdings more generally in detail.</p> <p>We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes developed in East Suffolk, and in Felixstowe and the Trimley villages in particular, is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
F50	Andy Smith	825	Yes	I strongly support this, and would request upgrading to a higher priority due to ease of upgrading existing ROW with good gains, as Cliff Road is a high speed and dangerous route for less confident cyclists / children.	Comment noted.
F50	Felixstowe Town Council (Ash Tadjrishi)	1033	Yes	Strongly Support, and we request upgrading to a higher priority due to ease of upgrading existing ROW with good gains, as Cliff Road is a high speed and dangerous route for less confident cyclists / children.	Comment noted.
F50	Seamus Bennett	202	Yes	consider a higher priority due to ease of upgrading existing ROW with good gains, as Cliff Road is a high speed and dangerous route for less confident cyclists / children.	Comment noted.
F51	Andy Smith	805	Yes	Support	Support noted.
F51	Felixstowe Town Council (Ash Tadjrishi)	942	Yes	Support	Support noted.
F51	Felixstowe Town Council (Ash Tadjrishi)	1035		<p>Strongly support in principle.</p> <p>However, all faculties and routes in the NFGN will be consulted on as part of the Master plan and detail locations and lines considered in that integrated context</p> <p>Discussion to date have focussed on securing use of the existing highway land along Candlet Road, including the Gulpher Road overbridge.</p> <p>The section from the Trelawny Place development to Garrison Lane roundabout is nearing completion on the north side of Candlet Road. However, the section from the overbridge to the Trelawny Place side entrance has become difficult, if not impossible due to layout of the current construction.</p> <p>An early decision is needed as to the principle of</p>	Comment noted.

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				<p>whether all or parts of F51 are best delivered adjacent to Candlet Road or within the NFGN sites, not least as the design of the roundabout serving both the Bloors development at Walton North and the western part of NFGV is currently in detail discussion under the Walton N application.</p> <p>We would seek assurances that cyclists and walkers from NFGN should, as a priority be easily and safely able to cross southwards onto Garrison Lane and use proposal F59 in order to access town/rail station etc most directly, without have to tackle the incline up to Beatrice Ave roundabout. Enhanced crossing facilities at the western exit of the roundabout should be included in this proposal.</p>	
F51	Seamus Bennett	203	Yes	<p>My concern here is that cyclists and walkers from NFGN should, as a priority be easily and safely able to cross southwards onto Garrison Lane in order to access town/rail station etc most directly, without have to tackle the incline up to Beatrice Ave roundabout. It's not clear at present if/how they will be able to do this. Yes, a wide shared and segregated path along Candlet road is definitely needed, but it is its linkages into town which will determine how many people choose to walk/cycle rather than driving.</p>	Comment noted.
F51	Trinity College, Cambridge (Sir/Madam)	1068		<p>Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.</p>	<p>Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and</p>

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				<p>We would welcome a meeting to discuss the detail of the proposals affecting the College’s landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to North Felixstowe Garden Neighbourhood (recommendations F49, F45, F46, F30, F51)</p> <p>Trinity College owns and controls land at the western end of the North Felixstowe Garden Neighbourhood allocation and is working with the Council and other stakeholders to bring it forward for development in due course.</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals associated with the North Felixstowe Garden Neighbourhood allocation, referenced above, subject to detailed exploration of feasibility, viability and agreement of details as part of the masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss</p>	<p>functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment. It is intended that in locations that are appropriate and useful for horse-riding, the infrastructure design will reflect this through all-user design.</p>

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				<p>and explore the opportunities across North Felixstowe Garden Neighbourhood site and the College's land holdings more generally in detail.</p> <p>We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes developed in East Suffolk, and in Felixstowe and the Trimley villages in particular, is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.</p>	
F52	Felixstowe Town Council (Ash Tadjrishi)	1038	Yes	Strongly support	Support noted.
F53	Andy Smith	826		This should be re-examined carefully in the light of feasibility reference conflicts, as above	Comment noted. F53 has been removed from the Strategy.
F53	Felixstowe Town Council (Ash Tadjrishi)	1040	Yes	Support	Comment noted. F53 has been removed from the Strategy.
F54	Andy Smith	827		This should be re-examined carefully in the light of feasibility reference conflicts, as above	Comment noted.
F54	Felixstowe Town Council (Ash Tadjrishi)	1043		<p>Support.</p> <p>However regarding "Play Bollards, there may be concern about encouraging children to "play" at the very edge of a still busy (but narrower) road? Colneis Road is a key link between at least 3 schools, in addition to that just confirmed on the Trelawny</p>	Comment noted.

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				Place development, as well as through Elmcroft lane through to the Cliff Estate, Golf Club and on to Felixstowe Ferry. This route should also take some pressure off High Road East.	
F54	Felixstowe Town Council (Ash Tadjrishi)	1056	Yes	Support. However regarding "Play Bollards, there may be concern about encouraging children to "play" at the very edge of a still busy (but narrower) road? Colneis Road is a key link between at least 3 schools, in addition to that just confirmed on the Trelawny Place development, as well as through Elmcroft lane through to the Cliff Estate, Golf Club and on to Felixstowe Ferry. This route should also take some pressure off High Road East.	Comment noted.
F54	Seamus Bennett	140	Yes	fully support this - Colneis Road is very wide, with ample room for cycling and key link between schools, as well through Elmcroft lane through to Golf Club. Also takes some pressure off High Road East.	Support for a shared path along Colneis Road noted.
F55	Felixstowe Town Council (Ash Tadjrishi)	1049		Support in principle However, all facilities and routes in the NFGN will be consulted on as part of the Master Plan and detail locations and lines considered in that integrated context, hence significant expenditure should be postponed until detail development plans for NFGV come forward, with potential re-routing. Any bollards at Links Avenue to be north of maintenance entrance to Eastward Ho. At Park Farm, agricultural use will continue until NFGN development, so only necessary at Links end.	Comment noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
F55	Seamus Bennett	145	Yes	Fully agree with this. Be aware that house(s) on Rosemary avenue may be interested in accessing Hyems lane near Colneis school, from the rear of their properties (recent planning application for a garage in a rear garden which would almost certainly seek to gain vehicular access along the current bridleway). This should be guarded against as it would seriously compromise the development of this as a traffic-free route, especially for children going to/from the school.	Support noted.
F56	Felixstowe Town Council (Ash Tadjrishi)	1051		Support in principle However, all facilities and routes in the NFGN will be consulted on as part of the Master Plan and detail locations and lines considered in that integrated context. The detail of bollards etc here may not be relevant in that context.	Comment noted.
F58	Andy Smith	828		I strongly support this in in principle. However, there may be concerns about the practicality of FP19 in the Grove and FP57 through to Gulpher Road in regard to cycling, given the drainage and environmental issues there: these would need to be fully explored. Also I would reemphasise here that the suggestions in F45 regarding potential vehicular uses to Gulpher Road are inconsistent with both the LP and this F58 paragraph. NB the text on the interactive map under F58 is much wider than listed here. It is not apparent how some of those other elements are addressed in the text document.	Comment noted. F45 has been removed from the Strategy.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
F58	Felixstowe Town Council (Ash Tadjrishi)	1052		<p>Strongly supported in principle.</p> <p>However, there may be concerns about the practicality of FP19 in the Grove and FP57 through to Gulpher Road in regard to cycling, given the drainage and environmental issues there: these would need to be fully explored.</p> <p>Also, we would reemphasise here that the suggestions in F45 regarding potential vehicular uses to Gulpher Road are inconsistent with both the LP and this F58 paragraph. NB the text on the definitive map under F58 is much wider than listed here. It is not apparent how some of those other elements are addressed in the text document.</p>	Comment noted. F45 has been removed from the Strategy.
F58	Martyn Shakespeare	528	Yes		Support noted.
F59	Andy Smith	829		This should be re-examined carefully in the light of feasibility reference conflicts, as above	Comment noted.
F59	Felixstowe Town Council (Ash Tadjrishi)	1054	Yes	Support	Support noted.
F60	Felixstowe Town Council (Ash Tadjrishi)	1057	Yes	Strongly supported.	Support noted.
F61	Felixstowe Town Council (Ash Tadjrishi)	1046	Yes	Support.	Support noted.
F63	Andy Smith	830		<p>F63 Mill Road (Mill LANE!)</p> <p>This should be re-examined carefully in the light of feasibility reference conflicts, as above</p>	Comment noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
F63	Felixstowe Town Council (Ash Tadjrishi)	1045	Yes	Support NB the correct title is Mill Lane.	Support noted.
F64	Andy Smith	831	Yes	<p>Support</p> <p>It is very important to take opportunity to make much better uses of both Rail bridge and A14 pedestrian bridge..</p> <p>However the land at the southern end of the Downs is owned by the adjacent residential properties.</p> <p>The situation in this area is complex, again not helped by outdated elements in the Definitive map,</p> <p>There is in principle an ability to create a path on the unused land between the estate western boundary and the highway land on Dock Spur Road, but the issue is complex, believed to date back to the original planning permission for Orwell Green c. 1900.</p>	Comment noted. F64 has been removed from the Strategy.
F64	Felixstowe Town Council (Ash Tadjrishi)	1044	Yes	<p>Support</p> <p>It is very important to take opportunity to make much better uses of both Rail bridge and A14 pedestrian bridge.</p> <p>However, the land at the southern end of the Downs is owned by the adjacent residential properties.</p> <p>The situation in this area is complex, again not helped by outdated elements in the Definitive map,</p> <p>There is in principle an ability to create a path on the unused land between the estate western boundary and the highway land on Dock Spur Road, but the issue is complex, believed to date back to the</p>	Comment noted. F64 has been removed from the Strategy.

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				original planning permission for Orwell Green c. 1900. A separate paper is currently in preparation and will follow.	
F65	Felixstowe Town Council (Ash Tadjrishi)	1042	Yes	Support FTC Suggest making these upgrades to existing paths a HIGHER priority (currently Medium) due to low cost versus potential gains in access to skatepark etc for young people in particular.	Comment noted.
F65	Seamus Bennett	199	Yes	would make this a HIGHER priority. Should be low cost but with significant benefits, for young people especially.	Comment noted.
F66	Andy Smith	832		Support this and would regard this to be of a higher priority than currently recommended. The issue is whether "providing sufficient width" can be made available throughout". Acquisition of additional railway land is unlikely. (The footpath itself is on railway owned land) Acquisition of land from FTC's Cemetery would impinge on the vegetation border provided specifically in former years to preserve the quiet environment of the We suggest again, reference Note B that FP42 is in fact suitable for shred use, with a width generally app.2.3m. However, we note the positive comments about Langley Avenue, but with the reservation as to the Northern end being currently also a vehicular route (c. 110m). This is public highway.	Comment noted. F66 has been removed from the Strategy.

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F66	Felixstowe Town Council (Ash Tadjrishi)	1041	Yes	Support this and would regard this to be of a higher priority than currently recommended. The issue is whether "providing sufficient width" can be made available throughout". Acquisition of additional railway land is unlikely. We suggest again, reference Note B that FP42 is in fact suitable for shared use, with a width generally app.2.3m. However, we note the positive comments about Langley Avenue, but with the reservation as to the Northern end being currently also a vehicular route (c. 110m). This is public highway.	Comment noted. F66 has been removed from the Strategy.
F67	Andy Smith	833		This should be re-examined carefully in the light of feasibility reference conflicts, as above	Comment noted. F67 has been removed from the Strategy.
F67	Felixstowe Town Council (Ash Tadjrishi)	1039		We note that the cemetery path and (potentially) Langley Ave provide parallel routes in this area and also that Wadgate and Vicarage roads are densely populated residential areas with on-road parking requirements. However, we also note that Wadgate road also potentially serves local school(s) and therefore on balance do not object to this proposal but agree that it should remain medium priority.	Comment noted. F67 has been removed from the Strategy.
F68	Andy Smith	834		This should be re-examined carefully in the light of feasibility reference conflicts, as above	Comment noted.
F68	Felixstowe Town Council (Ash Tadjrishi)	1037	Yes	Support.	Support noted.
F69	Andy Smith	835	Yes	Strongly support – with a very high priority as the Trelawny Place development rapidly proceeds.	Support noted.
F69	Felixstowe Town Council (Ash Tadjrishi)	1036	Yes	Strongly support – with a very high priority as the Trelawny Place development rapidly proceeds. And we would repeat our comments under F51	Support noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				reference the need for improved crossing facilities at the Garrison / Candlet roundabout.	
F69	Seamus Bennett	147	Yes	Yes, VERY high priority, yes, east side better. But key issue is connectivity at its northern end - specifically to the (planned) candlet road cycle path and all the new housing in new Trewalny Place and upcoming NFGN. This aspect MUST be addressed - how will cycling be a main route from NFGN without fully connected safe route through to the town without breaks - which the plan currently appears to indicate. Could a route through the Glenfield Ave estate be a possibility?	Comment noted. F69 connects to F51 for access to/from the North Felixstowe Garden Neighbourhood, and both F91 and F5 for onward travel towards the centre of Felixstowe or eastward towards the coastline. Appropriate crossing infrastructure over both roundabouts along Grove Road to Garrison Lane 'north' and Beatrice Avenue are included in the recommendations for F69 and F54.
F70	Andy Smith	836	Yes	Strongly support, but upgraded to Very High Priority – a simple “quick win”	Support noted.
F70	Felixstowe Town Council (Ash Tadjrishi)	1034	Yes	Strongly support, but upgraded to Very High Priority – a simple “quick win”	Support noted.
F70	Seamus Bennett	205	Yes	absolutely, make it VERY high priority should have been done years ago - and perhaps Fairfield Ave would then be a good candidate to be a 'cycle street' - it's a natural car cul-de-sac, has a school on it and provides a logical route into town and old felixstowe.	Comment noted.
F71	Andy Smith	837		<p>We support the intention to improve safety for all road users and maintain traffic flow.</p> <p>These roads are probably the most heavily trafficked in Felixstowe and congested at peak times. We would be concerned if proposals to introduce two +15 second cycle filter lights might increase congestion, worsen air quality, and cause vehicle traffic to seek other less appropriate routes.</p> <p>Therefore, a roundabout may be preferable but</p>	<p>Comment noted. The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried</p>

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				<p>space available is limited by comparison with existing roundabouts in the town and elsewhere (e.g. Tescos Martlesham). And, as stated, inevitably on occasion large numbers of HGVs must be able to negotiate the junction without risk of damage to street furniture etc. Possibly a "humped" design could allow a solution for the HGV issue.</p> <p>Detailed design work on all options should be provided for consultation before any decision is made on any potential option, or none.</p>	<p>forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>Any changes to the High Road/Garrison Lane junction are likely to be delivered through the North Felixstowe Garden Neighbourhood development to support increased capacity, flow and safety on the key transport routes through Felixstowe to and from the site. The recommendations under F71 are therefore only a starting point for consideration as part of the more holistic assessment anticipated for bringing wider improvements to the network forward.</p>
F71	Felixstowe Town Council (Ash Tadjrishi)	1032		<p>We support the intention to improve safety for all road users and maintain traffic flow. These roads are probably the most heavily trafficked in Felixstowe and congested at peak times. We would be concerned if proposals to introduce two +15 second cycle filter lights might increase congestion, worsen air quality, and cause vehicle traffic to seek other less appropriate routes. Therefore, a roundabout may be preferable but space available is limited by comparison with existing roundabouts in the town and elsewhere (e.g., Tescos Martlesham). And, as stated, inevitably on occasion large numbers of HGVs must be able to negotiate the junction without risk of damage to</p>	<p>Comment noted. The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and</p>

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				street furniture etc. Possibly a "humped" design could allow a solution for the HGV issue. Detailed design work on all options should be provided for consultation before any decision is made on any potential option, or none.	<p>design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>Any changes to the High Road/Garrison Lane junction are likely to be delivered through the North Felixstowe Garden Neighbourhood development to support increased capacity, flow and safety on the key transport routes through Felixstowe to and from the site. The recommendations under F71 are therefore only a starting point for consideration as part of the more holistic assessment anticipated for bringing wider improvements to the network forward.</p>
F71	Seamus Bennett	206	Yes	the suggested upgrades to this junction for C&W should be VERY high rather than 'medium' priority. This intimidating, vehicle-dominated intersection needs to be improved in order to ensure the attractiveness of the (new) C&W routes which arrive here is not compromised by being faced with a dangerous high-speed and almost unavoidable junction.	Comment noted.
F72	Felixstowe Town Council (Ash Tadjrishi)	1031		This is a new and interesting idea, which could provide a major improvement for this area. However, the main corner site has recently received planning permission for a veterinary surgery and 3 houses. (Application number DC /21/2139/FUL), up to the existing fence line to the ex-railway land. However, in principle access from that boundary with Garrison on the railway land, descending the embankment may well be feasible. FTC would	Support noted.

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				support further investigation on this. Some members may be able to assist with information on the history of the site.	
F72	Martyn Shakespeare	525	Yes		Support noted.
F73	Andy Smith	838	No	We object to this proposal as it appears to be impractical and low priority.	Objection noted. F73 has been removed.
F73	Felixstowe Town Council (Ash Tadjrishi)	1030	No	We object to this proposal as it appears to be impractical and low priority.	Objection noted. F73 has been removed.
F74	Andy Smith	839		<p>This should be re-examined carefully in the light of feasibility reference conflicts, as above</p> <p>The central issue is that Garrison Lane is a core route through the town, accessing whole of south sea front area large areas of housing, connecting through to A14 via Walton Avenue, significant industrial area at Carr Road, etc. Links together the western ends of St, Andrews, Cobbold, Mill Lane and Orwell Road. Any works which would significantly impede its free flow should not be accepted. It is significantly narrower than for example High Rd East (app.12m vs 15m).</p> <p>Encouraging cyclists to use Chaucer / Surrey / Newry could go some way to ameliorating this issue.</p>	Comment noted. F74 has been removed.
F74	Felixstowe Town Council (Ash Tadjrishi)	1028		NB Road names. "Walley Lane" is presumably meant to signify Valley Walk Mill Road should read Mill Lane. We would support the provision of a safe cycling route along the whole of Garrison Lane, not just to the St. Andrews Road junction. However, it is not clear how the space for a shared path would be created. The road space is used for a range of safety	Comment noted. F74 has been removed from the Strategy as, due to the constraints along this route, it was considered unlikely that sufficiently high enough quality active travel infrastructure would be achievable to make significant improvements for cyclist's safety and convenience. In this area of Felixstowe the Strategy has

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				<p>features along its length. There are right turn lanes at Orwell Road, Mill Lane lights and St. Andrews Road, Valley Walk and new needed for Deben School site, including pedestrian islands. Also it should be noted that a major new junction is to be created accessing housing and a sports hub on the site of the old Deben school, also likely to need traffic management measures. These issues should be clarified before a detailed proposal is adopted. The central issue is that Garrison Lane is a core route through the town, accessing whole of south sea front area large areas of housing, connecting through to A14 via Walton Avenue, significant industrial area at Carr Road, etc. Links together the western ends of St, Andrews, Cobbold, Mill Lane and Orwell Road. Any works which would significantly impede its free flow should not be accepted. It is significantly narrower than for example High Road East (app.12m vs 15m). Encouraging cyclists to use Chaucer / Surrey / Newry could go some way to ameliorating this issue.</p>	<p>instead focused on providing reasonable alternative routes to use of the southern section of Garrison Lane.</p>
F74	Seamus Bennett	148	Yes	<p>the WHOLE of Garrison Lane needs to be a safe cycling route! Unless I'm missing something, the current map looks like a classic case of a great route which suddenly stops and leaves the cyclist in dangerous limbo on a busy road.</p>	<p>Comment noted. The section of Garrison Lane to the north of High Road (Garrison Lane 'north') was identified as having sufficient space and potential to be improved, with recommendations added to the Strategy under recommendation references F69 and F70 for fully segregated cycle/pedestrian tracks along the eastern side, connecting Garrison Lane 'north' with High Road, Grove Road and Fairfield Avenue. However, the section of Garrison Lane to the south of High Road (Garrison Lane 'south') is constrained by limited width and limited potential to absorb existing carriageway space to a sufficient degree of consistency to make improvements</p>

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					to the route worthwhile in terms of improving the safety and convenience of cyclists (poor quality infrastructure may even have a net negative impact on safety and convenience). It is for this reason that recommendation F74 was removed from the final Strategy, and no recommendation for the southern most section of Garrison Lane 'south' was included.
F75	Felixstowe Town Council (Ash Tadjrishi)	1027	Yes	Support. NB the road name is Mill Lane.	Support noted.
F75	Seamus Bennett	149	Yes	good but what about Garrison lane from here to the Lidl roundabout, onto Langer Road - this should be a very high priority.	Comment noted. Due to the width restrictions along most of the stretch of Garrison Lane to the south of the crossroads with Mill Lane, it was considered unlikely that a high enough quality scheme to make the works worthwhile would be able to be achieved. Consequently no recommendations were made in the Strategy for the southern section of Garrison Lane.
F76	Felixstowe Town Council (Ash Tadjrishi)	1026	Yes	Support. This was FTC submission no C2B We suggest that Chaucer Road would be a good candidate for a Cycle Street, subject to linking to improved cycling provision on Garrison Lane, Surrey Road and Newry Avenue – also potential “Cycle Streets.	Support noted.
F77	Felixstowe Town Council (Ash Tadjrishi)	1025	Yes	Supported.	Support noted.
F78	Andy Smith	840		This appears to be a complex and expensive method to effectively just get Northbound Cyclists past the Lidl frontage – one part of which there is already an unused block paved area of carriageway.. It would also introduce conflicts a) with northbound cyclists having to cross Langer Road, and then b)	Comment noted.

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				<p>Cavendish Road.</p> <p>Surely simpler just to continue cycle lane on western side to link with crossing over Garrison Lane proposed elsewhere?</p>	
F78	Felixstowe Town Council (Ash Tadjrishi)	1023	No	<p>This appears to be a complex and expensive method, to effectively just get Northbound Cyclists past the Lidl frontage – on part of which there is already an unused block paved area of carriageway. It would also introduce conflicts a) with northbound cyclists having to cross Langer Road, and then b) Cavendish Road.</p> <p>Surely simpler just to continue cycle lane on western side to link with crossing over Garrison Lane proposed elsewhere.</p> <p>Object See comment on F79</p>	Comment noted.
F79	Andy Smith	841		<p>I am surprised to see 3 separate proposals for the main length Langer Road, this F79, F80, and F133.</p> <p>This would result in 3 parallel routes, taking up more road space than is available. After considerable thought, we suggest that the optimum solution would be to have simple uninterrupted cycle lanes on each side of the carriageway. This is particularly suitable at this location due to the presence of yellow lines throughout, removing the common concerns about parked cars in these circumstances.</p> <p>See also concerns about the various elements in adjacent sections.</p>	Comment noted. F79 and F80 have been amended to include the recommendation of three different options for further consideration at design and implementation phase and consultation with the community to identify the design solution that best meets the different needs of users of Langer Road.

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F79	Felixstowe Town Council (Ash Tadjrishi)	1022		We are surprised to see 3 separate proposals for the main length Langer Road, this F79, F80, and F133. This would result in 3 parallel routes, taking up more road space than is available. After considerable thought, we suggest that the optimum solution would be to have simple uninterrupted cycle lanes on each side of the carriageway. This is particularly suitable at this location due to the presence of yellow lines throughout, removing the common concerns about parked cars in these circumstances. See also concerns about the various elements in adjacent sections.	Comment noted. F79 and F80 have been amended to include the recommendation of three different options for further consideration at design and implementation phase and consultation with the community to identify the design solution that best meets the different needs of users of Langer Road.
F80	Andy Smith	842		This would introduce a number of conflicts – most significantly at the gates to Langer Road School – with cyclists passing potentially at speed past the gates and at school times through the crowd of small children and mothers. Similarly on a smaller scale outside St. Edmunds Church This must surely outweigh the desire to use a shared path for cyclists rather than a cycle lane, for which there is space? South of Beach Station Road, where traffic volumes are significantly lower, the justification to avoid a cycle lane is also less.	Comment noted. F79 and F80 have been amended to include the recommendation of three different options for further consideration at design and implementation phase and consultation with the community to identify the design solution that best meets the different needs of users of Langer Road.
F80	Felixstowe Town Council (Ash Tadjrishi)	1021	No	Object – see comment on F79.	Comment noted. F79 and F80 have been amended to include the recommendation of three different options for further consideration at design and implementation phase and consultation with the community to identify the design solution that best meets the different needs of users of Langer Road.
F81	Felixstowe Town Council (Ash Tadjrishi)	1020	Yes	Strongly support proposal to allow cycling in Langer Park. Suggest this should be a HIGH priority initiative as ideal for the many young families and (school)	Comment noted.

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				children using this area. Potential issues in the areas mentioned by AJS don't make the basic idea of this proposal invalid. It wouldn't obviate need for improved cycle infrastructure on Langer Road, which would partly serve a different purpose (e.g., commuting) and directly serve school.	
F82	Andy Smith	843		On the assumption that one or other form of dedicated cycle facilities are provided on Langer Road, there can no justification for "pinching" the carriageway as suggested, adding further to delays and congestion. And as suggested in this response at the Garrison Lane / High Road lights, adding 2 x 15 second delays to the lights cycle, in addition to the existing pedestrian phase, so 30 seconds per cycle of some 1m 30s, would reduce the traffic capacity of the junction by some 1/3rd, again with all the issues of congestion and air quality etc associated with that.	All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
F82	Felixstowe Town Council (Ash Tadjrishi)	1018	Yes	Support. Perhaps a trial of this in one location, with a head start for cyclists of say 10 sec could be manageable. Also, if the current lights indeed don't detect cyclists, then this does need updating, so would support that proposal.	Support noted.
F83	Felixstowe Town Council (Ash Tadjrishi)	1017	Yes	Support. Support high priority, this route has many disparate uses both port related and tourist, reference caravan parks to and from seafront.	Support noted.
F84	Andy Smith	845	No	This is neither feasible nor necessary if a full shared path is to be provided on the northern side. This part of Walton Avenue is very narrow and can barely handle 2 passing HGVs, let alone the current mix of traffic when HGVs are turning into and out of the adjacent premises, utilising the full width of the road	Objection noted. F84 has been modified to no longer include a recommendation for infrastructure to be delivered that runs adjacent to the Walton Avenue carriageway on the south side; F84 now only covers the transition (from F83) around the south side of the Dock Gate 1 roundabout.

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				<p>for the turn, and in the case of the very narrow access to the water treatment works and the adjacent V KVP haulage business, often needing to do a partial reverse manoeuvre on the highway.</p> <p>By what mechanism can “excess business forecourt space” be acquired? Also from the last premises on the south side to the diverging path approaching Dock Gate 1 roundabout, the adjoining land (currently scrub) is hard on the carriageway.</p> <p>Object</p>	
F84	Felixstowe Town Council (Ash Tadjrishi)	1016	No	<p>Object. Whilst we consider that a continuous path from Dock Gate 1 to Langer Road is necessary, given the recommendation under F83, we do not support an additional share path on this side of the road. However, to provide access to Port Number 1 Gate via the “cut-off” on the line of the old road, suggest a crossing from F83 to South side immediately west of the entrance to the Kingdom Hall.</p>	<p>Objection noted. The recommendation under F84 for new infrastructure adjacent to the Walton Avenue carriageway on the south side has been removed; F84 now relates only to the transition (from F83) around the Dock Gate 1 roundabout's south side.</p>
F85	Felixstowe Town Council (Ash Tadjrishi)	1015	Yes	<p>Support in principle This a potentially superb new pedestrian route, linking through to the Kingsfleet & Deben river banks. However, the bridleway access is only to, not along the Kingsfleet bank. FP6 is a footpath only, for good reason: the challenges of providing, and even more subsequently maintaining, cycling on clay flood banks are substantial, whether owned by a landowner (as I believe here) or the EA as the destination is dependent on cycling being introduced on the Deben banks, a significant issue. See comments B & C in item F95.</p>	<p>Comment noted. F85 has been removed from the Strategy.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
F85	Martyn Shakespeare	531	Yes	Could be nice, provided widening the path doesn't detract from the feel of the area.	Support noted. F85 has been removed from the Strategy.
F86	Felixstowe Town Council (Ash Tadjrishi)	1014	Yes	Support.	Support noted.
F87	Felixstowe BID (Sheline Gledhill)	154	Yes	Felixstowe BID is pleased to see new cycle parking provision in the plan and would support the installation of extra bike racks for the entire seafront from Cobbold's Point to Manor Terrace so cyclists can alight, lock their bikes, and make use of the cafes and restaurants, rides and amusements along the front. At the moment there is very little provision except for outside of the Leisure Centre. The BID would like to be engaged on where to place the new bikes racks and have some input on their design, which we feel could go beyond the standard 'Sheffield stand' to something more aesthetically pleasing which would add to the attraction of the front - some we have seen are almost art installations in their own right. At the moment, bikes are routinely chained to lamp posts and road signs which is unsightly for all and unsatisfactory for the cyclists. The BID would like to take this opportunity to contribute to the debate and find a solution which would enhance the area as well as encouraging cyclists to dismount and visit our local businesses.	Support for the inclusion of cycling parking locations in the Felixstowe area of the Strategy has been noted. Guidance on the design of cycle parking solutions that are attractive, interesting, relevant to their location and add to the overall quality of the public realm is anticipated to come forward through supplementary planning guidance currently being prepared by the Council that focuses on supporting the creation of healthy environments.
F87	Felixstowe Town Council (Ash Tadjrishi)	1013	Yes	Support.	Support noted.
F88	Felixstowe Town Council (Ash Tadjrishi)	1012	Yes	Support.	Support noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
F89	Felixstowe Town Council (Ash Tadjrishi)	1011	Yes	Support in principle, However, is there space? Paths are packed on good days. Any stands should not impede pedestrians unduly.	All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity, and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
F90	Felixstowe Town Council (Ash Tadjrishi)	1010	Yes	Support.	Support noted.
F91	Felixstowe Town Council (Ash Tadjrishi)	1009		The intention here is laudable as Beatrice Ave should not be used as the main route out of Felixstowe from the town centre or Old Felixstowe (traffic is signposted towards High Road West and Garrison Lane). Currently, there is free flowing traffic along this straight road, which is crossed every day by hundreds of residents and school children. Rather than being seen as an essential artery, Beatrice Ave could be described as an over-used residential street. Therefore, some traffic calming measures on Beatrice Ave would be welcome, or perhaps a 20mph speed limit. Another way to potentially achieve this would be by making the traffic priority at this point from Dellwood Avenue to Fairfield Avenue, with 'Give Way' signs at Beatrice Avenue.	Support and further comments noted.
F91	Martyn Shakespeare	527	No	Understand logic, but would put more traffic onto High Road West. Also currently a bus route (albeit not may of them) serving the medical centre. Also	Objection and further comments noted. The proposal has been adjusted to allow through traffic,

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				an inconvenience to residents of Colneis Road and Links Road who need to drive into town. Better to leave this as a through route. (I am slightly biased, it's my preferred route into/out of Felixstowe if driving in a car, rather than cycling, avoiding the delays and congestion on High Road West.)	however traffic calming measures and implementation of a 'cycle street' has been added in replacement in order to create a safe route for cyclists and pedestrians.
F92	Felixstowe Town Council (Ash Tadjrishi)	1007	Yes	Support, but with 5-10 sec head start, not 15sec. Pedestrians are rightly given extra time to cross junctions such as this, and cyclists also should have the opportunity to negotiate them safely. Junctions such as this are said to account for over 80% of urban cycle deaths/serious injury. Other locations in UK have been using these systems since 2013/15. Often the amount of time required is only 5 sec head start, e.g., to get ahead before vehicles start to turn.	The recommendations under F92 did not include signalling the Hamilton Road/High Road roundabout but instead improving the existing pedestrian crossings over each of the roundabout's arms to parallel crossings (for use by cyclists and pedestrians).
F92	Martyn Shakespeare	526	Yes	Also need to consider how to cross High Road into town centre	Comment noted. Recommendation F92 is for the delivery of parallel crossings over each of the Hamilton Road/High Road/Beatrice Avenue roundabout's arms, therefore improving the safety of the transition over High Road to/from the town centre (Hamilton Road area).
F93	Andy Smith	846	No	I do not believe it appropriate to add a further 30 sec to each light cycle, likely to reduce capacity and increase congestion and its side effects at this busy junction. Object	Objection is noted. The crossing head-start time outlined within F93 has been reduced to five seconds.
F93	Felixstowe Town Council (Ash Tadjrishi)	1006	Yes	Support, but with 5-10 sec head start, not 15sec. Pedestrians are rightly given extra time to cross junctions such as this, and cyclists also should have the opportunity to negotiate them safely. Junctions such as this are said to account for over 80% of urban cycle deaths/serious injury. Other locations in	Support noted. The crossing head-start time outlined within F93 has been reduced to five seconds.

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				UK have been using these systems since 2013/15. Often the amount of time required is only 5 sec head start, e.g., to get ahead before vehicles start to turn.	
F94	Felixstowe Town Council (Ash Tadjrishi)	1005	Yes	Support in principle as long-term potential However, this raises many questions with unknown answers. The core concept of a link to St. Andrews Rd. is attractive – but only deliverable by negotiation with landowner ref any potential planning application. This car park is owned by the East of England Cooperative, not deliverable by LA. Multistorey likely to raise many objections.	Comment noted. Recommendation F94 has been removed from the Strategy.
F95	Felixstowe Town Council (Ash Tadjrishi)	949		National and Local Coastal pedestrian Paths. Designated and signed long distance walking paths are an increasingly important element in encouraging walking for promoting tourism, for leisure and for health. It follows therefore that any Cycling and Walking Strategy, at County. District or local level should have as a core element the recognition, promotion and mapping of relevant paths, and a Strategy Objective to expedite their use both in their own right and for local routes to link to and use them wherever that can be achieved. In Felixstowe (and the whole Orwell North bank) we have: 1) The long standing Stour and Orwell Estuaries Walk . 2) The Suffolk Coast Path	Comment noted. Recommendation F95 has been removed from the Strategy.

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				<p>3) And in the final stages of Parliamentary approval, 2 sections of the National Coast Path Shotley Gate to Felixstowe Ferry Felixstowe Ferry to Bawdsey Further details of the National Coast Path can be obtained from Natural England</p> <p>We strongly request that these are classified and mapped as part of the ESC Strategy, and that the final Strategy expedites and overtly recognises those, including local signage.</p> <p>Relevant proposals include, but certainly not limited to those below, however we have not individually detailed the many seafront links to the Suffolk Coast Path and the National Coast Path</p> <p>Proposals F8 F9 F40 F95 F113 Objectives 2 Policy Context</p>	
F95	Felixstowe Town Council (Ash Tadjrishi)	1004		<p>If achievable, this route would be significant and welcome. However: A) It is believed to be public highway only northwards as far as the mapped gate at map ref. 318374, beyond which it is privately owned, hence would require negotiation with the landowner both in principle and reference future maintenance. B) FP53 and FP91 are on the Flood</p>	<p>Comments noted. F95 has been removed from the strategy.</p>

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				banks owned by the EA. It is questionable whether widening/surfacing for bicycles would be viable on the existing structure: at the very least permission from the EA would be required C) The 2 FPs are also part of the National Coastal Path Route; Natural England may take a view on whether mixed use is appropriate given the increased need for maintenance.	
F95	Martyn Shakespeare	530	Yes	Could make an interesting leisure route for cycling. Possibly crushed stone surfacing preferable to tarmac.	Support and further comments noted.
F96	Felixstowe Town Council (Ash Tadjrishi)	1002		<p>St. Andrews Road, both sides of Hamilton Road is another main access to and through the town, also prime route in and out for all the side streets Penfold to Princes, as well as business premises on street and to Bridge Road. It is therefore not “very low traffic”, which is a stated aim for Cycle Streets.</p> <p>While we perceive St Andrews Road to be a more likely candidate for a ‘cycle street’ than Cobbold Road (a key route into town) we believe the concept of ‘cycle streets’ for Felixstowe should be carefully considered and consulted upon in order to achieve broad consensus and understanding of the idea. We do however, agree with the need and scope to make St Andrews Road (and Cobbold) as cycle-friendly as possible.</p>	Comment noted. Recommendation F96 has been removed from the Strategy.
F97	Felixstowe Town Council (Ash Tadjrishi)	1000	No	<p>Object</p> <p>Whilst we would support an improved provision for cycling, we do not believe that Cobbold Road is suitable as a ‘Cycle Street’ given its use as a main vehicle route through the town centre.</p>	Objection noted. Recommendation F97 has been removed from the Strategy.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
F98	Felixstowe Town Council (Ash Tadjrishi)	999	No	<p>Object</p> <p>We do not agree with the closure of Cobbold Road to vehicle traffic.</p> <p>Cobbold Road is the core central link north-south right across the town. Closure would force much traffic onto much less suitable roads - Ranelagh, York, Victoria Street, etc. Given that Orwell Road is also 1-way at Lloyds, next South to North route is Hamilton and Wolsey Gardens - hardly suitable for main flows. NB traffic approaching town centre on Cobbold Road from Harvest House would have 3 "No entry" options. So would need "No through road" at Ranelagh Road.</p>	<p>Objection noted. The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>Recommendation F98 has been amended to include two potential options to take forward for consideration at design and implementation stage.</p>
F99	Felixstowe Town Council (Ash Tadjrishi)	998	No	<p>Object</p> <p>This does not appear to be a cycling or walking enhancement.</p> <p>Further redesign or enhancements to Triangle and shared space area should be considered as an integrated whole.</p>	<p>Objection noted. Recommendation F99 has been removed from the Strategy.</p>
F100	Felixstowe Town Council (Ash Tadjrishi)	997	Yes	<p>Support.</p> <p>Name error – Mill Lane not Road</p> <p>Support. Subject to:</p>	<p>Comment noted. Correction to street name has been made.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				Right turn lane into Highfield Road must be retained as is heavily used accessing car park and interacts with traffic queuing for lights. Mill Lane significantly narrower south of Hamilton Road. – is there space?	All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. Consideration of the need to retain the right-turn lane for Highfield Road would be assessed as part of the design and implementation phase of bringing this recommendation forward for delivery.
F100	Seamus Bennett	150	Yes	yes good but what without connectivity in all directions from garrison lane traffic lights (especially garrison lane south) cyclists are more likely to use the smaller side roads for travel southwards	Comment noted. F100 is primarily intended to provide for westward travel towards western Felixstowe via Mill Lane and Grange Road, due to the difficulty with providing infrastructure of a high enough quality along Garrison Lane 'south' to make it worthwhile (due to constraints).
F101	Felixstowe Town Council (Ash Tadjrishi)	996	No	This has been extensively consulted on, with the balance of user interests now decided. In any case, any further change must be done in an integrated way, see comment to F99. Object in this context.	Objection noted. Recommendation F101 has been removed from the Strategy.
F102	Felixstowe Town Council (Ash Tadjrishi)	995		Support in principle We suggest making this a Very High Priority route. It is an important route for Colneis and some Fairfield School pedestrian and cycling traffic. It has important potential to reduce car use for the school run. and could transform the journey to school experience for children and parents.	Support noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				Also, Rosemary Avenue has a significant role outside of school times is as preferred main route to and from the central part of Old Felixstowe. The proposal would entail the removal, or moving of lamp posts, but also potential removal of a small number of mature street trees, which should be avoided if possible.	
F102	Seamus Bennett	144	Yes	higher priority route - important route for Colneis and (some) Fairfield schools pedestrian and cycling traffic. Important potential to reduce car use for the school run. Rosemary Ave is wide enough but currently (I know from experience) it is actually quite treacherous for parents to cycle with kids to/from school. A shared use path could transform this experience and get many more children cycling/walking to school. Would consider this VERY high priority for these reasons.	Support noted.
F103	Felixstowe Town Council (Ash Tadjrishi)	994	Yes	Support. This would give this a higher priority as efficient to use existing PROW and gives good linkage off high road east. And/or consider enabling cycles along the short footpath at the end of Park Avenue to provide similar / alternative link off high road.	Comment noted. Recommendation F103 has been removed from the Strategy.
F103	Seamus Bennett	142	Yes	would give this a higher priority as efficient to use existing PROW and gives good linkage off high road east. And/or consider enabling cycles along the short footpath at the end of Park Avenue to provide similar / alternative link off high road.	Comment noted. Recommendation F103 has been removed from the Strategy.
F104	Felixstowe Town Council (Ash Tadjrishi)	957		NOTE B Rigid adherence to 3m minimum width for shared cycling / pedestrian paths.	Comment noted. Recommendation F104 has been removed from the Strategy.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>FTC suggest that there are a number of instances where long standing off-highway footways in urban areas, usually designed to be c. 2m in width are a resource suitable in certain cases for shared use. Specifically, these are typically relatively short individual lengths, used almost entirely by local people as everyday desire lines, and hence familiar to the great majority of users, with relatively low usage volumes either by existing pedestrian users or, importantly, potential cycling users. Hence the shared use would appear to present a low risk to shared uses.</p> <p>We believe these are a wasted resource, capable of providing “quick wins” at low cost to create Improved cycling opportunities. We would request this criterion be relaxed in appropriate locations. We suggest this approach is substantiated in the following paragraph from DfT Local Transport Note 1/12, particularly the phrase “preferred” minimum, thus: Paragraph 7.34</p> <p>A width of 3 metres should generally be regarded as the preferred minimum on an unsegregated route, although in areas with few cyclists or pedestrians a narrower route might suffice. Where a significant amount of two-way cycling is expected, additional width could be required. However, the need here for additional width is not clear cut, because the absence of segregation gives cyclists greater freedom to pass other cyclists. It might therefore depend on user flows.</p> <p>Proposals include</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				F8 F104 F108	
F104	Felixstowe Town Council (Ash Tadjrishi)	993		<p>NB PROW15 on the Definitive Map is the whole length of Quintons Lane, all Bridleway, including:</p> <ul style="list-style-type: none"> a) High Rd to Looe Road: clearly now highway, but still Bridleway on Definitive map. Clearly inappropriate since full development and highway adoption in the 1930s. See Note C. b) Diagonal Alley (!) Looe Rd to Sunray Ave c) Rear of Sunray / side of Colneis school to Colneis Rd d) Alley Colneis to Upperfield e) Lane adjacent Eastward Ho to meet Hyem's Lane NE corner (site of previous Quinton's Farm) <p>Land around Brackenbury to High Row Field is ESC owned, assumed access will be included in development plan (not a PROW).</p> <p>This proposal is duplicated at F136</p> <p>FTC Recommend: Southern Quintons Lane (a above) should be removed from Definitive Map.as Bridleway We support enhancement of all northern sections, but vehicle access is required for ESC at Eastward Ho and possibly farmland beyond, at least until the NFGN development is commenced.</p> <p>We support inclusion as a shared route in any Brackenbury Sport Centre redevelopment application, again accepting that the existing</p>	Comment noted. Recommendation F104 has been removed from the Strategy.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				footpath to High Row Filed is only c. 2m wide. See note B	
F105	Andy Smith	800		I strongly support this – but it is critical that the route is continued, as below as the core natural (and historic) route through the Cliff Estet to Cliff Road and beyond.	Support and further comments noted.
F105	Felixstowe Town Council (Ash Tadjrishi)	937	Yes	Strongly support. Colneis to Western Ave. This is part of FTC submission ref W6B in our original submission.	Support and further comments noted.
F105	Felixstowe Town Council (Ash Tadjrishi)	992		See Elmcroft Lane etc F8	Comment noted.
F106	Felixstowe Town Council (Ash Tadjrishi)	991		<p>The map and the description do not match here. F106 refers just to Westmorland Road, but the map shows also the whole length of Western Ave to Cliff Road. However, that only has the wide grass verges north and west of the junction with Keswick Close, Rather than new shared-use paths, we would propose a full continuation of Elmcroft lane F8 and make this a Very High Priority route, linking with F105.</p> <p>The highly desirable facility proposed here could then be achieved much sooner and at greatly less expense.</p> <p>However, if that were regrettably not accepted, we would reluctantly support the proposed shared path, but only for the stretch from the junction with F105, a core route in the wider scene.</p>	<p>Comments noted. The priority of F106 has been changed from medium to high and the text has been adjusted and now clearly refers to Westmorland Road, Western Avenue, and Cliff Road.</p> <p>F8 has been removed from the strategy.</p>
F108	Felixstowe Town Council (Ash Tadjrishi)	958		<p>NOTE B</p> <p>Rigid adherence to 3m minimum width for shared cycling / pedestrian paths.</p>	Comment noted. Recommendation F108 has been removed from the Strategy.

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				<p>FTC suggest that there are a number of instances where long standing off-highway footways in urban areas, usually designed to be c. 2m in width are a resource suitable in certain cases for shared use. Specifically, these are typically relatively short individual lengths, used almost entirely by local people as everyday desire lines, and hence familiar to the great majority of users, with relatively low usage volumes either by existing pedestrian users or, importantly, potential cycling users. Hence the shared use would appear to present a low risk to shared uses.</p> <p>We believe these are a wasted resource, capable of providing “quick wins” at low cost to create Improved cycling opportunities. We would request this criterion be relaxed in appropriate locations. We suggest this approach is substantiated in the following paragraph from DfT Local Transport Note 1/12, particularly the phrase “preferred” minimum, thus: Paragraph 7.34</p> <p>A width of 3 metres should generally be regarded as the preferred minimum on an unsegregated route, although in areas with few cyclists or pedestrians a narrower route might suffice. Where a significant amount of two-way cycling is expected, additional width could be required. However, the need here for additional width is not clear cut, because the absence of segregation gives cyclists greater freedom to pass other cyclists. It might therefore depend on user flows.</p>	

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				Proposals include F8 F104 F108	
F108	Felixstowe Town Council (Ash Tadjrishi)	990	Yes	Strongly support We welcome acceptance that path less than 3m can be acceptable share. as in a considerable number of other cases. See Note B	Comment noted. Recommendation F108 has been removed from the Strategy.
F109	Felixstowe Town Council (Ash Tadjrishi)	989	Yes	Support.	Support noted.
F110	Felixstowe Town Council (Ash Tadjrishi)	988	No	Object. Priory Road has no clear routes dependent on it, hence creating a "Cycle Street" delivers little new facility. Examples exist elsewhere in the town where this concept could be used to greater advantage. Priory Rd is a quiet street for cycling as it is.	Objection noted. Recommendation F110 has been removed from the Strategy.
F110	Seamus Bennett	141	Yes	putting in a 'cycle street' sounds great but I can imagine it could be unpopular with some residents - what will be the link at the lower end of Priory Road? to embark on this new idea it would seem better to do it somewhere with greater connectivity and potential gains - and Priory Rd is a quiet street for cycling on as it is.	Agreed. In reflection of the strategic value of the proposal, F110 has been removed.
F111	Felixstowe Town Council (Ash Tadjrishi)	987	Yes	Support.	Support noted.
F112	Felixstowe Town Council (Ash Tadjrishi)	986	Yes	Support.	Support noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
F113	Andy Smith	804	No	<p>I object to the proposed use as a “Leisure Loop” implying use by mountain bikes etc. It is intrinsically unsuited by location and geography fo this use.</p> <p>Bridleways 18,25,26 form part of the AONB's Stour and Orwell Walk, and soon the National Coastal Path: these should be recognised and the route designed accordingly - but for pedestrians only. See comment on F9.</p> <p>Hence enhanced surfacing for leisure and mountain bikes is not appropriate, also due to the ground condition quoted.</p> <p>The link across the railway level crossing (FP17) to Fagbury Road must be provided with enhanced signage and facilities to communicate with Network Rail - but this will never be straightforward. However it must be dealt with as now part of major designated pedestrian routes.</p>	<p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p>
F113	Felixstowe Town Council (Ash Tadjrishi)	940	Yes	Support.	Support noted.
F113	Felixstowe Town Council (Ash Tadjrishi)	951		<p>National and Local Coastal pedestrian Paths.</p> <p>Designated and signed long distance walking paths are an increasingly important element in encouraging walking for promoting tourism, for leisure and for health.</p> <p>It follows therefore that any Cycling and Walking Strategy, at County, District or local level should have as a core element the recognition, promotion and mapping of relevant paths, and a Strategy Objective to expedite their use both in their own</p>	<p>Comment noted. The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving the safety, convenience, connectivity, and completeness of the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they</p>

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				<p>right and for local routes to link to and use them wherever that can be achieved.</p> <p>In Felixstowe (and the whole Orwell North bank) we have:</p> <ol style="list-style-type: none"> 1) The long standing Stour and Orwell Estuaries Walk. 2) The Suffolk Coast Path 3) And in the final stages of Parliamentary approval, 2 sections of the National Coast Path Shotley Gate to Felixstowe Ferry Felixstowe Ferry to Bawdsey <p>Further details of the National Coast Path can be obtained from Natural England</p> <p>We strongly request that these are classified and mapped as part of the ESC Strategy, and that the final Strategy expedites and overtly recognises those, including local signage.</p> <p>Relevant proposals include, but certainly not limited to those below, however we have not individually detailed the many seafront links to the Suffolk Coast Path and the National Coast Path</p> <p>Proposals F8 F9 F40 F95 F113</p>	<p>will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety and functionality of the local movement networks and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p>

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				Objectives 2 Policy Context	
F113	Felixstowe Town Council (Ash Tadjrishi)	985		See section F9 etc reference National Coastal Path	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving safety, convenience, connectivity, and completeness if the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p>
F113	Trinity College, Cambridge (Sir/Madam)	1075		Please include the following comments as a response to your consultation on the draft Walking and Cycling Strategy, on behalf of Trinity College Cambridge. I made comments through the consultation portal earlier today regarding the Howlett Way site allocation in Trimley St Martin but was not able to submit these additional comments through the Portal in time.	Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and

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				<p>We would welcome a meeting to discuss the detail of the proposals affecting the College’s landholdings at the Trimley Estate before the Walking and Cycling Strategy is published, as highlighted in the comments below.</p> <p>Proposals relevant to the wider Trimley Estate, owned and controlled by Trinity College Cambridge (including recommendations F9, IF22, IF24, F10, F36, F113, F38)</p> <p>The College commends the efforts made by the Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with it to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects.</p> <p>We broadly support the proposals affecting the Trimley Estate, referenced above, subject to detailed exploration of feasibility, viability and agreement of details as part of future masterplanning and planning application processes. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across the Estate in more detail. The delivery and funding of proposals requiring significant new infrastructure, such as hard surfaced routes and lighting and signage, would need to be subject to further discussion.</p>	<p>functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment. It is intended that in locations that are appropriate and useful for horse-riding, the infrastructure design will reflect this through all-user design.</p>

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				We would highlight the importance of ensuring that lighting, signage and other infrastructure associated with new walking and cycling routes is designed in a manner that is sensitive to the rural or semi-rural environment of the many of the locations proposed. Overly engineered solutions should be avoided where possible to protect the special character of the area. This will require creative thought by developers and the District and County Council in fulfilling their roles as planning and highways authorities respectively.	
F114	Andy Smith	807		I do not think it appropriate to encourage any more use of the pedestrian crossing across the neck of the Dock Spur Roundabout. Other better routes are made available by other proposals on this document	Comment noted. Recommendation F114 has now been absorbed into F30.
F114	Felixstowe Town Council (Ash Tadjrishi)	943	Yes	Support.	Support noted.
F114	Felixstowe Town Council (Ash Tadjrishi)	984		See F30 etc, p.27 ref A14 crossing	Comment noted.
F115	Andy Smith	799		I support this in principle However, the reference to PROW11 should overtly exclude the section across the Golf Course. Also, the northern section of PROW62 from Mariners Lodge to the Sailing club, a flood bank and wall owned by the EA, is of a number of different older constructions and has recently partially failed in 2 places. The EA have imminent work scheduled, hence we suggest that urgent contact should be made with them to maximise this opportunity Approximately 15 years ago, there were negotiations	Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.

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				<p>between the Golf Club, SCC ROW and FTC around the Club's safety concerns on the 2 historic footpaths crossing the course. (FP1 & FP11 Northern section) It was envisaged that they be re-routed via a new ramp provided by the club, on their land northward from the top of the steps to the sea wall path, an excellent new facility. However those negotiations sadly ended without agreement. It was raised again during the debate on the recent Golf Club planning application, but with a similar result. There is clearly both a logical need and an opportunity currently to re-visit this - if dealt with urgently with the backing of the C&W group. Possibly best raised separately, out with the current consultation, inevitably a slow-moving process.</p>	<p>In some locations, the existing physical constraints may mean retrofitting active travel infrastructure is either not possible to be delivered to as high a standard as we would aspire to or not possible at all. This may mean that some schemes are unable to be brought forward.</p>
F115	Felixstowe Town Council (Ash Tadjrishi)	935		<p>Strongly supported. However, the reference to PROW11 should overtly exclude the section across the Golf Course. Also, the northern section of PROW62 from Mariners Lodge to the Sailing club, a flood bank and wall owned by the EA, is of a number of different older constructions and has recently partially failed in 2 places. The EA have imminent work scheduled; hence we suggest that urgent contact should be made with them to maximise this opportunity.</p>	<p>Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>In some locations, the existing physical constraints may mean retrofitting active travel infrastructure is either not possible to be delivered to as high a standard as we would aspire to or not possible at all. This may mean that some schemes are unable to be brought forward.</p>

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F115	Felixstowe Town Council (Ash Tadjrishi)	983		See section on Clifflands F7 etc.	Comment noted.
F115	Martyn Shakespeare	529	Yes	Will make for a nice leisure route plus linking with the Bawdsey Ferry.	Support noted.
F116	Felixstowe Town Council (Ash Tadjrishi)	982		Support in principle if needed in the longer term However, this would only work if additional coastal defence were needed to support the Shoreline Management Plan Policy to "Hold The Line", i.e., to protect the coast from erosion. This is not currently the case; the cliff appears stable with the current modest privately owned defences. There is no current indication of the situation changing. (Some minor works were publicly funded app 2012 below 2 properties on Golf Road).	Support and further comments noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of costs and benefits of delivering such a scheme.
F116	Martello Place Management Limited (Keith Phair)	117	Yes	<p>We responded on this topic in March 2020 to the Natural England consultation process on the Shotley Gate to Felixstowe Ferry section of the proposed Coastal Path.</p> <p>As the freeholder of a wide sea-facing frontage in the gap between the two sections of the Promenade, we concur with the comments above in F116 and think a continuous coastal path along the beach would have material amenity value to local residents and visitors. We repeat below the key section from our response to the March 2020 consultation:</p> <p>QUOTE</p>	Support noted.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>For the avoidance of any doubt on the part of the authorities and local residents, we wish to make it clear that Martello Place Management Limited is entirely supportive of the project to establish a coastal path and is happy to work with the relevant authorities to identify a suitable solution for bridging this 220m gap in the promenade and providing a continuous seaside public promenade for the whole of the Felixstowe peninsula.</p> <p>We believe that a continuous 7-kilometre seaside promenade for Felixstowe would add substantially to the attractions of the town, provide a significant public amenity and encourage tourism, helping the economy and people of Felixstowe to thrive. Our support for bridging the gap in the promenade is conditional upon the following points, which preserve the existing rights of Martello Place residents:</p> <ol style="list-style-type: none"> 1) Preservation of private access between Martello Place and the beach/prom 2) Equitable terms (relative to the neighbouring properties) in the event that any works may be required that would require the use of a strip of our land 3) Adequate protection and maintenance arrangements for the sea wall and secure fencing for our property 4) Agreeing a protocol to minimise disruption to 	

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				<p>Martello Place residents during any construction process</p> <p>We would be happy to discuss this further if you should wish to explore the creation of a true coastal path for Felixstowe.</p> <p>UNQUOTE</p>	
F116	Nik Bestow	98		<ul style="list-style-type: none"> Coastal Footpath Similarly the proposed Coastal Footpath should at least be included on the reference maps (the proposed path will, for example, change the walking and cycling route from Martlesham to Felixstowe). 	<p>The views, advice and local information provided by the communities of East Suffolk have been invaluable in being able to refine the recommendations of the East Suffolk Cycling and Walking Strategy and bring forward improvements likely to be effective in improving safety, convenience, connectivity, and completeness if the active travel infrastructure offer in key locations of the district.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. The assessment of improvements in sensitive locations for wildlife impacts will include appropriate assessment.</p>
F117	Chris Adelson	505	Yes		Support noted.

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F117	Felixstowe Town Council (Ash Tadjrishi)	981	Yes	Support. However the land is not "promenade", not in ESC ownership and ownership is unknown. We would request ESC to take steps to establish possession and enhance this small area, which could support limited car parking as well as the necessary Cycling & Walking route. But access to the flood gate must be preserved at all times.	Comments noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts of implementation and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. As such, ownership of land will be a consideration as the project progresses.
F118	Felixstowe Town Council (Ash Tadjrishi)	979	Yes	Support.	Support noted.
F119	Felixstowe Town Council (Ash Tadjrishi)	978	Yes	Support.	Support noted.
F119	Martyn Shakespeare	523	Yes		Support noted.
F120	Felixstowe Town Council (Ash Tadjrishi)	977		<p>Support this route if it can be shown that sufficient width for a fully separated cycle path can be achieved entirely separate from the roadway.</p> <p>This route is largely paralleled by the routes F118 and F119, but is more direct, serving tourism routes and businesses.</p> <p>It is of varying width and constricted at certain points. The speed bumps are essential to retain (indeed required by the PoF Planning permission).</p>	Comment noted. Recommendation F120 has been removed from the Strategy.

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				<p>It is well used by motor traffic, heavily so on good days. Cars are barely able to pass in both directions on certain lengths.</p> <p>FTC suggest that also a proposal be considered to sign the route from Langer Road via Manor Road, Manor Terrace and Landguard common. The short Manor Rd, and to a lesser extent Manor Terrace stretches are less than ideal, but still a useable short stretch</p> <p>A longer-term alternative could be to create a route using Old Fort Road and past the Martello Tower to join the high-quality route through Martello Park. The stretch from Old Fort Rd to the park road would need careful thought and investigation however, (Listed Building curtilage).</p>	
F120	Martyn Shakespeare	524	Yes		Support noted.
F121	Felixstowe Town Council (Ash Tadjrishi)	974		<p>The vision of a more open, calmer, greener, less polluted space in this area, which is highly frequented by children and families from four nearby schools (Felixstowe School, Causton, Maidstone and Grange), should be applauded and supported. Focusing on improving cycling and walking routes to and from schools should be at the core of the aspirations for the Cycling and Walking Strategy. This justifies some traffic calming/restriction of Maidstone Road to through-traffic but does not require complete closure. Consideration of residents is also paramount. Significant reconfiguration options for the local road</p>	Comments noted. Recommendation F121 has been adjusted and Maidstone Road is no longer recommended for road closure.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				network in this location should be given separate and wide-ranging consideration.	
F122	Felixstowe Town Council (Ash Tadjrishi)	973		<p>The vision of a more open, calmer, greener, less polluted space in this area, which is highly frequented by children and families from four nearby schools (Felixstowe School, Causton, Maidstone and Grange), should be applauded and supported. Focusing on improving cycling and walking routes to and from schools should be at the core of the aspirations for the Cycling and Walking Strategy. This justifies some traffic calming/restriction of Maidstone Road to through-traffic but does not require complete closure. Consideration of residents is also paramount. Significant reconfiguration options for the local road network in this location should be given separate and wide-ranging consideration.</p> <p>Ref. F122 – we do not believe that the proposal is feasible given that this privately owned land used as a car park for the Suffolk GP Federation.</p>	Comment noted. Recommendation F122 has been removed from the Strategy.
F123	Felixstowe Town Council (Ash Tadjrishi)	972		<p>The vision of a more open, calmer, greener, less polluted space in this area, which is highly frequented by children and families from four nearby schools (Felixstowe School, Causton, Maidstone and Grange), should be applauded and supported. Focusing on improving cycling and walking routes to and from schools should be at the core of the aspirations for the Cycling and Walking Strategy. This justifies some traffic calming/restriction of Maidstone Road to through-traffic but does not require complete closure. Consideration of residents is also paramount.</p>	Comment noted. Recommendation F123 has been adjusted and Maidstone Road is no longer recommended for a road closure.

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				Significant reconfiguration options for the local road network in this location should be given separate and wide-ranging consideration.	
F124	Felixstowe Town Council (Ash Tadjrishi)	971		<p>This is titled and described “Seaton Road” but is mapped also to extend along the south side of High Road West to the traffic lights, thereby duplicating the provision in F4 for a shared path also on the north side..</p> <p>For the Seaton Road section:</p> <p>Approve in principle.</p> <p>However, road space appears limited towards the western end, specifically at and past the projection no. 103, and again approaching and beyond the junction with King St. It would need to be demonstrated that it can be provided, or if necessary interrupted, along the whole length of this quite busy traffic route.</p> <p>For the additional second shared path on High Road West, we would object due to the limitations on space, especially approaching the Garrison Lane junction, certainly as a duplicated route here.</p> <p>However, in view of the difficulty of the F4 route from Seaton Rd to Recreation Lane, with the possibility that may not be feasible, it may make sense for a shared path from the lights to Seaton Road to serve both purposes on the south side, where the pavement is broad along most of that length.</p>	<p>Comments noted. The recommendation for F124 has been adjusted to just cover Seaton Road.</p> <p>All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts and optimisation of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme. In some locations, the existing physical constraints may mean retrofitting active travel infrastructure is either not possible to be delivered to as a high standard as we would aspire to or not possible at all. This may mean some schemes are unable to be brought forward.</p>

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F125	Felixstowe Town Council (Ash Tadjrishi)	970		This is virtually all covered by the planning permission and now in build.FTC however supports enhancement of FP24 to the south.	Comment noted. Recommendation F125 has been removed from the Strategy.
F126	Felixstowe Town Council (Ash Tadjrishi)	969	Yes	Support .	Support noted.
F127	Felixstowe Town Council (Ash Tadjrishi)	968	Yes	Support .	Support noted.
F129	Pigeon Investment Management Ltd (Fillmore, Andrew)	1063		A pedestrian refuge crossing facility is included as part of the Pigeon scheme.	Comment noted.
F133	Felixstowe Town Council (Ash Tadjrishi)	967		<p>This appears to largely duplicate F80, but on the other side of the road. There is not the space to accommodate both, let alone a need.</p> <p>NB The access to the Port at the end of Carr Road is indeed useful – but not a formal one and could be subject to closure by the Port for security.</p> <p>Assurance of access should be sought but would be relevant to F80.</p> <p>We object to the concept of two-shared paths along this road.</p>	Objection noted. Recommendation F133 has been removed from the Strategy.
F134	Felixstowe Town Council (Ash Tadjrishi)	966	Yes	Support.	Support noted.
F135	Felixstowe Town Council (Ash Tadjrishi)	965	Yes	Name error – Mill Lane. Support.	<p>Support noted.</p> <p>The name error has been amended.</p>

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F136	Felixstowe Town Council (Ash Tadjrishi)	964	Yes	Support. See our comments to F104.	Support noted.
F136	Seamus Bennett	146	Yes	I cycle this route regularly and feel the main concern here is that access is maintained even if the site is redeveloped in next few years. The existing surface and route is adequate. Would consider reducing priority of this section from High to MEDIUM. (I assume the split gates near the houses are to prevent motorcycle access? If not, then they could be removed to facilitate continuous cycling). This route through for cyclists and walkers should be signposted on High Road East as a safe route to the beach/Fludyers etc.	Comments noted. The recommendation to remove the existing cycle barriers has been added under F136.
3.28 - 3.31	David George	48	Yes	I definitely agree the Bascule bridge is a nightmare area to cycle through. Coming from Halesworth I often cycle into Lowestoft from Carlton Colville which is by far the safest way from either Beccles or Stoven and Sotterly side (which links to Southwold and Brampton/Halesworth). Once beyond Carlton Colville the routes (whichever you take) are far too complicated and have multiple junctions which are inherently dangerous. The cycle path along Tom Crisp Way is very good, but one you reach as far north as the roundabout with Waveney Road, it becomes very dangerous. I similarly agree that Denmark Road, as a natural cycling key route, is quite dangerous, even as far up as the roundabout at the top of Normaston Drive. If the section between Normaston Drive and Waveney Rod could be improved, this would really encourage cyclists from north, south and west into town.	Support noted.

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3.28 - 3.31	Ricky Andrews	4	Yes	<p>I am very surprised that a cycle route is not being considered from the Beccles Rd 146 at the junction with the Dutchmans pub along the A1117 going towards Pakefield.</p> <p>Looking at the map, I think there is a need to have a better cycle path along the A1117. There are many cyclists that use this route</p> <p>and they use the pavements. Speaking to some of them they tell me that they use the paths because they do not feel safe on the road.</p> <p>I would like to see a cycle path added to this route.</p>	<p>Agreed. Elm Tree Road/Cotmer Road is well used according to Strava and shows good potential for modal shift improvement according to PCT. The potential to incorporate this road into the key corridors has been explored further and added to the Strategy.</p>
3.28 - 3.31	Sue Kershaw	1081	Yes	<p>In past months, I commented on the huge need for the extension and improvement of the cycle path for Pakefield schools,the promenadeonce again the public consultation continues and closes next year!</p> <p>It's difficult for some, including me to attach comments on this very important council project, which will benefit the safety of cyclists, pupils at both schools,especially with climate change in the forefront of everybody's mind,cycling over cars, is such a worth while improvement needed ASAP.</p> <p>I understand our government has allocated funds to our council for these cycle paths, it appears as hard to do, as making comments on this public consultation website.</p>	<p>The support is noted. The consultation was designed to be innovative and user friendly, if this doesn't reflect your experience we welcome feedback.</p> <p>Key Corridor L45 has been extended to Pakefield high school.</p>
3.28 - 3.31	Woodland Trust (Oliver Newham)	633	No	<p>We have identified the following potential threats to ancient woods and ancient/veteran trees along this stretch of proposed cycleway.</p>	<p>This list of important ecological assets has been noted and full consideration will be given to the protection of these assets.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>Lowestoft Key Corridor</p> <p>Notable oak TM53149109, ID 60064 - L24 on A12</p> <p>Notable oak TM53129107, ID 60065 – L24 on A12</p> <p>Notable oak TM53119105, ID 60066 – L24 on A12</p> <p>Notable oak TM53119105, ID 60066 – L24 on A12</p> <p>Mutford Big Wood ASNW, TM49618914 – LB4 adjacent</p> <p>Unnamed ASNW, TM49378878 – LB4 adjacent</p> <p>Unnamed ASNW, TM49268871 – LB4 adjacent</p> <p>Veteran oak TM43928980, ID 48956 – LB12 adjacent</p> <p>Veteran oak TM43568977, ID 48957 – LB12 adjacent</p> <p>Veteran oak TM41698796, ID 48852 – junction of LB29 and LB27</p> <p>Veteran oak TM35428913, ID 48493 – LB36</p>	<p>It should be noted that limited improvements that require expanding the infrastructure are suggested along L24 so there is no likely impact here.</p> <p>Similar for LB4 the suggestion is to utilise the rural lane with limited hard infrastructure expansion.</p> <p>LB12 would look to utilise existing road/path space and the managed grass verge if expansion is required, but reference to these trees will be included.</p> <p>Junction of LB27/LB29 recommends the improvement of the path surface to allow more users. Reference to this tree will be included.</p> <p>LB36 states that a new cycle track should be the ambition, but recognises this may not be viable and instead notes that other improvements may be necessary instead. Reference to the tree will be included.</p>
3.28 - 3.31	Woodland Trust (Oliver Newham)	635	No	<p>We have identified the following ancient wood which would be threatened by the proposed cycleway in this area.</p> <p>Lowestoft to Hopton Key Corridor</p> <p>Foxburrow ASNW, TM53579553 - L5 adjacent</p>	<p>Full consideration has been given to the adjacent ancient woodland and the recommendation altered to state that no damage to the ancient woodland should occur and the recommendation relates to work to the A47.</p>

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L1	Chris Adelson	507	Yes		Noted.
L5	Chris Adelson	544		A path through the ancient woodland is not appropriate. The first option must be achieved.	Full consideration has been given to the adjacent ancient woodland and the recommendation altered to state that no damage to the ancient woodland should occur and the recommendation relates to work to the A47.
L6	Chris Adelson	545	Yes		Support noted.
L7	Chris Adelson	546	Yes	Not everyone feels safe travelling on this section. More use might help people to feel safer.	Support Noted.
L9	Chris Adelson	547	Yes		Support Noted.
L10	Chris Adelson	548	Yes	Keep grass verges where possible. Consider planting to create a barrier between path and road.	Noted. The path should be widened to the size required to meet best practice, but no further if a sufficient grass verge can be retained.
L11	Anthony Bucknole	174	No	<p>The last thing Lowestoft needs is the removal of one of the lanes on the bascule bridge to cater for cyclists.</p> <p>The third crossing is will alleviate the current high traffic density moving across the river Waveney but that's all. As Lowestoft continues to expand there will be an increase in traffic moving north and south. To shut a lane off will be counter productive and the extra capacity provided by the third crossing will soon be negated.</p>	<p>The suggested remodelling of the bridge is dependant upon sufficient reduction of traffic levels from the third river crossing and is not being recommended should the anticipated traffic levels on the Bascule Bridge as a result of the third river crossing not materialise.</p> <p>It should be noted that the Bascule Bridge represents a significant pinch point not only for vehicular traffic but also for cyclists. Its redevelopment not only improves the wider cycling network with its health and environmental benefit but could also improve access into the town centre resulting in more footfall.</p>
L11	Chris Adelson	549	Yes		Support Noted.
L11	Councillor Paul Ashdown	59	No	Sorry I cannot support any changes to vehicle traffic using the bascule bridge. This is the the comment I have made on several occasions we have fought for a third crossing not to make it a second crossing in a different place. Should this be brought before both Full Council and The Place Board I could not support it.	<p>The suggested remodelling of the bridge is dependant upon sufficient reduction of traffic levels from the third river crossing and is not being recommended should the anticipated traffic levels on the Bascule Bridge as a result of the third river crossing not materialise.</p> <p>It should be noted that the Bascule Bridge represents a</p>

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					significant pinch point not only for vehicular traffic but also for cyclists. Its redevelopment not only improves the wider cycling network with its health and environmental benefit but could also improve access into the town centre resulting in more footfall.
L11	Norman Brooks	60	No	<p>I read with dismay the proposal to reduce the lanes on the Bascule Bridge from 3 to 2.</p> <p>There are wide footpaths on both sides of the bridge and one of these could be repurposed and combined.</p> <p>For eleven years I have spent hundreds of hours lobbying and pushing for a THIRD crossing not a TWO AND A HALF CROSSING,</p> <p>the bridge is costing £117 million to improve connectivity in the North of the District and any reduction will have a devastating effect on trade in Lowestoft High Street.</p> <p>I have asked the question numerous times “are there any proposals to shut one lane as shown on the first draft of the proposal” and was given a 100% guarantee by officers this would not happen!!</p> <p>Also as Cabinet member for Transport I think I should have been consulted but I have not, this is not acceptable.</p>	<p>The suggested remodelling of the bridge is dependant upon sufficient reduction of traffic levels from the third river crossing and is not being recommended should the anticipated traffic levels on the Bascule Bridge as a result of the third river crossing not materialise.</p> <p>It should be noted that the Bascule Bridge represents a significant pinch point not only for vehicular traffic but also for cyclists. Its redevelopment not only improves the wider cycling network with its health and environmental benefit but could also improve access into the town centre resulting in more footfall.</p>

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				<p>I was at the first meeting of the place board but then told there was not a place for me strange.</p> <p>I cannot support any proposal to reduce the existing bridge in this way</p>	
L11	Rod Serling	359	No	<p>Just as the town has something given to it, you take it away. Its hard enough running a business without you making it harder</p>	<p>The suggested remodelling of the bridge is dependant upon sufficient reduction of traffic levels from the third river crossing and is not being recommended should the anticipated traffic levels on the Bascule Bridge as a result of the third river crossing not materialise.</p> <p>It should be noted that the Bascule Bridge represents a significant pinch point not only for vehicular traffic but also for cyclists. Its redevelopment not only improves the wider cycling network with its health and environmental benefit but could also improve access into the town centre resulting in more footfall.</p>
L11	Steel & Co (Danny Steel)	43	No	<p>As Chairman of Lowestoft Vision I represent the business and traders in Lowestoft. I can't support any proposal that will restrict the traffic flow into the town centre. Lowestoft like many towns is still trying to recover from the challenges of COVID and adapting to the shift in buying habits any restriction will be another hammer blow to Lowestoft's business. The Gullwing bridge is Lowestoft's third crossing not a replacement crossing.</p>	<p>The suggested remodelling of the bridge is dependant upon sufficient reduction of traffic levels from the third river crossing and is not being recommended should the anticipated traffic levels on the Bascule Bridge as a result of the third river crossing not materialise.</p> <p>It should be noted that the Bascule Bridge represents a significant pinch point not only for vehicular traffic but also for cyclists. Its redevelopment not only improves the wider cycling network with its health and environmental benefit but could also improve access into the town centre resulting in more footfall.</p>
L31	Cycling UK (John Thompson)	304	No	<p>Sharing with pedestrians needs to be avoided and confident cyclists just won't use it and will get abuse from drivers. Reduce the speed limit to 30 mph with speed calming. I accept it could somewhat reduce</p>	<p>Noted. Segregated infrastructure is the ambition where possible to achieve.</p>

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				<p>the cycling potential but it's the classic case of where, in my view, there need to be a trade-off.</p> <p>If cyclists and pedestrians can be segregated, fair enough.</p>	
L32	Cycling UK (John Thompson)	354	No	I would like clarification of why a segregated cycle lane is considered necessary to comply with current guidance.	Where possible lanes segregated from both traffic and pedestrians (either as a shared, but segregated path or dedicated cycle track) should be sought in accordance with the LTN1/20 principles. The Suffolk Design Streets Guide also notes it is best practice to design to this standard on secondary cycle routes. Gunton Cliffs appear wide enough to meet the higher quality, but if this approach is not viable a lower quality infrastructure may then be considered.
L33	Chris Adelson	551	Yes	Also public conveniences. It's a well used space.	Noted. Addition of public conveniences can be of benefit, but their provision is outside the scope of the strategy.
L42	Chris Adelson	553		The lower prom is very busy during the summer and people choose to sit out here. There could well be a conflict between riders and relaxed holiday-makers.	The comment is noted and the proposals should aim to support the visitor economy not to damage it. Accordingly a caveat has been added that the option to use the lower promenade should be considered against impact to leisure/tourism.
L44	Cycling UK (John Thompson)	293	Yes	<p>I am especially pleased to learn of this proposal.</p> <p>I have already reported to SCC that in my Cycling UK capacity I have had reports of near misses by those using the toucan crossing because drivers didn't notice the lights were red.</p>	Support Noted.
L56	Chris Adelson	555		Improved cycle ways should also include wheelchair routes from the care home. It is currently very difficult to take a resident in a wheelchair to the Community Centre/park or supermarket at Aldi due to narrow pavements and high kerbs. Union Lane is	Noted. The intention is to provide infrastructure to the best current standard where possible ensuring appropriate width allowing all users.

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				quiet but there is not pavement along the whole length and a blind corner.	
LH5	Mr Newsome	355	Yes	This makes perfect sense reopening the old railway line as a cycle way as has been done along the linear park railway line. It would allow cyclists a safe off road facility using the old railway line. I hope it will also be shared cycle way and pedestrian along with new lighting. The Stirrups Lane bridge made safe with an access ramp. If a garden village is to be built this cycle way is a must as it will provide a safe route for people travelling to school or work.	Comment noted.
LH8	Cycling UK (John Thompson)	352	No	Sharing with pedestrians should be avoided. Current provision runs broadly parallel to the A47. That said, what is the likelihood of people wanting to walk that distance?	Noted and it is agreed that segregation between cyclists and pedestrians should be sought first. There is potential for some pedestrians using this route as the distance between the allocated North of Lowestoft Garden Village and Hopton is small enough to allow walking particularly with another allocation south of Hopton narrowing the gap further.
3.36 - 3.39	David Beavan (East Suffolk Councillor)	46	Yes	It is important that this is extended to Southwold. Not easy I know with coastal erosion but could the coastal path be upgraded to a cycle path in this stretch? There are many people who commute from Lowestoft to Southwold for work. The bus connections are poor in the evenings and early mornings. The Wrentham road is too dangerous for cyclists, and the cycle path goes a long way inland.	Creating a route between Southwold and Lowestoft has been explored as part of either a southern extension to the Lowestoft-Kessingland Key Corridor or a Leisure Route. However, the constraints associated with waterways, the coast and the A12 have resulted in no feasible route being identified between Kessingland and Southwold.
3.36 - 3.39	Patricia Garrod	197	Yes	The cycle path which is on the pavement alongside the A12 between the Pizza Hut roundabout and the Kessingland turnoff roundabout is very very dangerous and need to be made much safer.	Comment noted.
LK1	David George	49	Yes	Absolutely agreed, the path between the Pakefield water tower roundabout and Kessingland is really	Comment noted.

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				dangerous. It would also be helpful if there were a safer (perhaps island-based) crossing over from the east side of the A12 to the Gisleham Road near the Morrisons/south Industrial Estate access road. At present, the shared pathway in this section is very, very dangerous and pedestrians are often not aware this is shared.	
LK3	Cycling UK (John Thompson)	291	Yes	I support this in principle, but with some reservations. While an off-road facility is necessary between Pakefield and Kessingland because of the perception of danger on the A12, they should not be necessary in Kessingland itself. The residential roads, including the main street could simply be 20 mph. Also, although advisory cycle lanes are very limited in their usefulness, combined with 20 mph limits they could be of some help in encouraging drivers to overtake cyclists at the correct width. The lanes would need to be the widths stipulated in Local Transport Note 1/20	The support is noted, the Strategy aims to remove cyclists off-road where possible to reduce potential for conflict. 20mph zones may be appropriate in places, but the Strategy does not recommend settlement wide 20mph zones.
3.40 - 3.44	David George	50	Yes	As a Halesworth Cyclist I often use Bungay and Beccles routes, and usually access Lowestoft either via Ellough/Beccles, or via the back road from Stoven through Sotterley, Hulver and Mutford - this is a lovely safe route. If the strategy wishes to avoid the centre of Beccles (which I agree is quite dangerous other than the section through Worlingham along the old main road), then how about routing from Carlton Colville, Mutford, Hulver, Ellough, Weston, Ringsfield, St Andrews, and then Mettingham into Bungay? OK it's not a particularly straight route but it's nice back roads.	Noted. The roads/paths around Carlton Colville, Mutford, Hulver, Ellough, Weston, Ringsfield, St Andrews, and Mettingham have been considered and in some cases included within the strategy.

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3.40 - 3.44	Worlingham Parish Council (Lesley Beevor)	357		<p>The following are the comments from Worlingham Parish Council</p> <p>Add a safer cycle lane onto Marsh Lane roundabout from Lowestoft Road</p> <p>Sandpit Lane to be added as part of the area under walking/cycling strategy</p> <p>Change the side of the cycle path on Hillside Ave</p> <p>Cycle path to be included from Cedar Drive to Ellough Ind Estate</p> <p>Cycle Path to be added from Cedar Drive to College Lane</p>	<p>Marsh Lane - Agreed. Consideration has been given to the cycling connection onto Marsh Lane and the Strategy adjusted.</p> <p>Sandpit Lane - This represents a possible cycling/walking route between Worlingham and Ellough Industrial Park. It is unlikely significant infrastructure could be added here, however some modest improvements could be considered and the Strategy adjusted.</p> <p>Hillside Avenue - The side of the proposed cycle lane shown in the draft strategy is indicative only and can be moved to whichever side yields the best results.</p> <p>Cedar Drive - Ellough Industrial Estate - A cycle/walking route has been included to meet this ambition. It is less direct than utilising Ellough Road itself, but allows progress through the allocated garden neighbourhood giving a connection to implementation.</p> <p>Cedar Drive - College Lane - See above. To continue the route onto College Road the existing infrastructure on the Ellough Road roundabout could be continued northwards.</p>
3.40 - 3.44	Worlingham Parish Council (Lesley Beevor)	1082	No	<p><i>First comment:</i></p> <p>worlingham parish council make the following comments regarding the consultation and requirements in the village. Comments as follows;</p> <p>add a safer cycle lane onto Marsh Lane roundabout from Lowestoft Road</p>	<p>Marsh Lane - Agreed. Consideration has been given to the cycling connection onto Marsh Lane and the Strategy adjusted.</p> <p>Sandpit Lane - This represents a possible cycling/walking route between Worlingham and Ellough Industrial Park. It is unlikely significant infrastructure could be added here, however some modest improvements could be considered and the Strategy adjusted.</p>

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				<p>add Sandpit Lane onto the cycling/ waking strategy</p> <p>the proposed cycle land on Hillside Ave needs to be on the opposite side of the road</p> <p>cycle path from Cedar Drive to Ellough Ind Est to be added</p> <p>Cycle path from Cedar Drive to College Lane to be added</p> <p><i>Second comment:</i></p> <p>add a safer cycle lane onto Marsh Lane from Lowestoft road</p> <p>Add Sandpit Lane onto the cycling / walking strategy</p> <p>The proposed cycle Lane on Hillside Ave needs to be on the opposite side of the road</p> <p>New cycle lane to be added from Cedar Drive to Ellough Ind Est</p> <p>New cycle Lane to be added from Cedar Drive to Cucumber Lane</p>	<p>Hillside Avenue - The side of the proposed cycle lane shown in the draft strategy is indicative only and can be moved to whichever side yields the best results.</p> <p>Cedar Drive - Ellough Industrial Estate - A cycle/walking route has been included to meet this ambition. It is less direct than utilising Ellough Road itself, but allows progress through the allocated garden neighbourhood giving a connection to implementation.</p> <p>Cedar Drive - College Lane - See above. To continue the route onto College Road the existing infrastructure on the Ellough Road roundabout could be continued northwards.</p>
LB6	Cycling UK (John Thompson)	302	Yes	<p>I would be interested to know if you've had an comments about the crossing. I accept it could be off-putting to novices and those with children. Even I as a case-hardened cyclists find being on the central reservation with fast moving traffic coming past unpleasant. However, while traffic calming and a lower speed limit would be steps in the right</p>	<p>The road is a well used A-road with a non-urban character that perhaps limits the crossing options. A Toucan crossing represents the best option for cyclists and can be explored further at a design stage, but it is likely that other options will be more viable.</p>

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				direction would it impede motor traffic so much to have a Toucan crossing?	
LB16	Ingleton Wood LLP and DLP Planning Ltd for and on behalf of Chenery's Farm Partnerships, Beccles Townlands Trust and Allison Homes	925		<p>2.6 Our clients agree with the principle of Route LB16 to provide a link through the allocation site, alongside Ellough Road. The representatives for the eastern parcel of land anticipate the delivery of a connection from a point at the site's boundary with Ellough Road to the existing southern bypass infrastructure. However, the exact route suggested by LB16 should at this stage should be indicative, as is consistent with other routes located within allocation sites within the Draft Strategy. It is expected that the LB16 route can be largely achieved through the aforementioned circular walking route for SANG / Country Park provision.</p> <p>2.7 As part of the Ellough Road access improvements, in addition to the circular route, a new footway is proposed along the length of Ellough Road providing opportunities for residents to walk both within the site or on the site edge.</p> <p>2.8 In respect of the recommended crossing point at Cedar Drive, this is not an explicit requirement of the allocation policy. The provision of a crossing on Ellough Road will be subject to Suffolk County Council finding such an arrangement both necessary and acceptable and being willing to enter into a Section 278 Agreement. It is also noted that the Draft Strategy recommends consideration of a lighted crossing for route LB15, which runs along Cedar Road. However, this Highways land is outside of any of our clients' landownership boundaries and</p>	<p>The support is noted and it is confirmed that the line on the map is only indicative and the exact route will be determined at a later stage.</p> <p>The Cycling and Walking Strategy considers the crossing point important, but it is agreed that the requirement will be guided by Suffolk County Council. The method of delivery will be determined at a later date subject to Suffolk County Council's guidance and consent.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				direct control. Accordingly, such a proposal would need to be guided by the County Council and supported by an evidence base that identifies the need for this provision and our client will provide all reasonable endeavours to provide a crossing point in this location should it be required.	
LB17	Ingleton Wood LLP and DLP Planning Ltd for and on behalf of Chenery's Farm Partnerships, Beccles Townlands Trust and Allison Homes	926		<p>2.9 The Beccles Southern Relief Road was opened in September 2018 and has been constructed with a substantial foot and cycle path running parallel to the carriageway, separated by a grass verge. The path is well signposted and already well-used. Any such duplicate path running parallel within the site would render one of the paths redundant. Notwithstanding the links expected within the site boundary, it is our clients' view that the Garden Neighbourhood permissions are not required to provide any further walking or cycling infrastructure along or parallel to the southern bypass itself.</p> <p>2.10 Our clients will also be providing a circular walking route within the site whereby this can connect to the existing route along the southern bypass. LB19 Rigbourne Hill</p> <p>2.11 The LB19 route is an existing green corridor that runs from the southern bypass to the town centre. It dissects the Garden Neighbourhood site via the track known as Oak Lane. Oak Lane is not within the ownership of any of our clients. Whilst we assume that the land is owned by Suffolk County Council Highways, we do not have control over this land. Our clients are not therefore anticipating the planning</p>	<p>The central key corridor (LB20) shown on the Cycling and Walking Strategy through the Garden Neighbourhood allocation is indicative only. The strategy attempts to show that a cohesive well connected cycle network through the site is required supporting the policy requirement for a permeable and legible layout.</p> <p>LB19 recognises the importance of this green corridor and the potential improvements that could increase its use. The method of delivery would be fully determined at a later stage.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>permissions for the Garden Neighbourhood to deliver the recommended improvements to the surface width of this path.</p> <p>2.12 Notwithstanding landownership matters, the eastern parcel is adjacent to the existing path at Oak Lane. The representatives for the eastern parcel have identified in their masterplan for the site that there will be a route of similar demarcation running along boundary of the site with Oak Lane due to the provision of the circular SANG / Country Park route.</p>	
LB20	Ingleton Wood LLP and DLP Planning Ltd for and on behalf of Chenery's Farm Partnerships, Beccles Townlands Trust and Allison Homes	927		<p>2.13 In accordance with the recommendation for LB20, our clients agree with the principle of providing high quality walking and cycling infrastructure throughout the Garden Neighbourhood allocation site.</p> <p>2.14 We also agree that the lines shown on the Cycling and Walking Strategy map should be considered indicative only. The permissions within the Garden Neighbourhood site are expected to deliver a complex series of cycling and walking routes which will be dictated by a range of material planning considerations, not least the circular route within the site-wide SANG and Country Park provision infrastructure. In the view of our clients, the exact routes should not be dictated any further by the Strategy to allow sufficient flexibility at planning application stage.</p>	The lines provided are indicative and the exact route would be determined at a later stage. The strategy attempts to show that a cohesive well connected cycle network through the site is required supporting the policy requirement for a permeable and legible layout.
LB23	George Blair	561	No	<p>Northgate, Beccles forms part of the Angles Way.</p> <p>Currently Northgate is used by overweight (including buses) and speeding vehicles, which creates a</p>	The role of Northgate Street has been considered in the formation of the Strategy. Accordingly a recommendation relating to Northgate has been included.

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>dangerous situation for walkers and cyclists. These facts are supported by a recent traffic speed survey carried out by Suffolk CC. See attached.</p> <p>Buses and heavy vehicles should be diverted from this road and traffic calming measures should be implemented, so that the road/route can be used safely.</p> <p>Please reconsider the North/South National cycle route (no 30) through Beccles a priority for improving cycling and walking in East Suffolk.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	<p>The National Cycle Network was considered in the formation of the Strategy.</p>
LB23	Michelle Golding	606		<p>in addition to comment 565, and the photo attached. I would like to add: https://www.norfolk.gov.uk/out-and-about-in-norfolk/norfolk-trails/long-distance-trails/angles-way/about-angles-way-and-points-of-interest: taken from the norfolk.gov.uk website.</p> <p>17% of the video promoting the Angles Way as an important walking and Cycling route, centres on Northgate in Beccles. It was filmed pre pandemic and at a time when there was (unbelievably) no heavy traffic in evidence.</p> <p>Since Norfolk CC give the route such a high priority for walkers and cyclists, they should be included in</p>	<p>The role of Northgate Street has been considered in the formation of the Strategy. Accordingly a recommendation relating to Northgate has been included.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				consultations (along with Suffolk County Council, and other stake holders) concerning the re routing of the 8 buses per hour (which encourage the use of other heavy traffic and the hundreds of vehicles that currently use it as a popular 'Rat Run') and enable the route to be safely used by walkers and cyclists	
LB25	Janet Holden	3	No	the majority of the strategy is aimed at cyclists, I'm concerned that by making public footpaths into cycle paths walkers will be put at risk, and this will encourage cyclists to commandeer other footpaths in the area. It would be better to designate the roads into the Ilketshalls as quiet roads and divert cyclists through these roads.	The strategy considers both cycling and walking and account for the needs of both in its recommendations. The key corridors attempts to utilise the optimum route and using rural roads was a key facet alongside that of PROW routes.
LB35	Andrew Saul	620	No	<p>The proposed route from Bungay along Low Road and then to Beccles via improved footpaths would provide a charming leisure route but would do little or nothing to serve the communities of Barsham, Shipmeadow and Mettingham which are mostly scattered along the B1062. There were once footpaths along the verge for part of the route but these have not been maintained and the verge is now completely impassable even on foot in many places.</p> <p>Few but the bravest in these villages will attempt to cycle or walk from one village to the other or on to Beccles or Bungay along the B1062, let alone allow their children to do so. But the leisure route along Low Road/ footpath is not a viable route for most people for work/school/domestic purposes. If cycling or walking are really to be put at the heart of transport decision making, then this must be addressed.</p>	<p>The proposed recommendation does provide an attractive route that helps connect Barsham, Shipmeadow and Mettingham via Lock's Lane and Callender Houses.</p> <p>It is recognised that additional connectivity could be provided by a route alongside the B1062 particularly to properties/businesses south of this road. A new route here would require significant new infrastructure. A community recommendation for a cycle path along the B1062 was received and scored.</p>

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				It can be addressed first by introducing easily and at minimal cost) the 30 mph limits promised by the local authority in the 1980's for each of Barsham, Shipmeadow and Mettingham, but withdrawn at the last minute. Second, a combined footpath/cyclepath should be created/recreated alongside the B1062.	
3.45 - 3.48	Alison Andrews	152	No	<p>By all means promote the walking routes but neither the Sailors Path nor the Aldeburgh railway line routes are suitable as well used cycle routes. There are occasional users at present but the paths are not suitable for a heavy cycling as well as walking route because</p> <ol style="list-style-type: none"> 1. they are not wide enough and walkers would have to stand in the edges or against the fences to let bicyclists pass (bicyclists do not often give way to walkers and that is a behaviour which will not be easily changed) 2. the ground forming both paths would not sustain the cutting effect of cycle wheels. Even with just walkers the paths can get quite muddy in places in winter and ground cut up by bicycles would make walking difficult. The ground consists of hilly sand (start of Sailors Path near Aldeburgh) beaten track of earth , leafy woodland, and on the Sailors Path duckboards where walkers have to go single file and wait for others coming the other way at wider points such as gates. Putting in a harder surface to accomodate bicycles would mean walkers would have a less giving surface to walk on, the whole experience would be a lot noisier with audible footfall and bicycles and take away the relaxation of walking on softer ground past fields and through 	Cycling, walking and wheeling can coexist along the Aldeburgh disused railway line and the Sailors Path if appropriate and sensitive infrastructure improvements are made. However, the Aldeburgh-Snape Leisure Route includes an alternative cycling and walking route to the Sailor's Path if it is considered more appropriate for the Sailor's Path to remain a walking only route.

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				woods. 3. both paths are well used by walkers, many with dogs which are free to run off the lead. The railway one has also many regular dog walkers doing circuits from nearby homes and also children at foot and in prams/buggies and that traffic does not fit well with bicycles. The Sailor's Path is already a well known circuit for many walkers- do promote that as it is great- but beware of the parking problems at either end.	
3.45 - 3.48	Andy Bird	123	Yes	Missing Tourist destination leisure routes opportunities: <ul style="list-style-type: none"> • Cycle path along woodbridge river, Martlesham, Woodbridge, Melton. Could also be a safe route from Martlesham. • Trimley Marshes circular route/with Felixstowe • Nacton beach and paths from Ipswich • Stour and Orwell walk from Trimley to Levington footpath - can be bridleway 	Leisure routes have been added to the Strategy, including between Nacton beach and Ipswich. Other suggestions are incorporated into the Key Corridor recommendations.
3.45 - 3.48	Benhall and Sternfield Parish Council (Melanie Thurston)	661		We would like to see the 3 Communities Link Project (Benhall/Kelsale/Saxmundham) reinstated to ensure that there is a full, linked, circular route (this was first mooted in 2016).	The 3 Communities Link project has been incorporated into the Strategy as the Benhall-Saxmundham-Kelsale Leisure Route.
3.45 - 3.48	Chris Adelson	559		Ensure that there is public access to toilets along the route for walkers and cyclists.	It will be important that services and facilities, such as toilets, are planned for along the Leisure Routes through detailed design stages.
3.45 - 3.48	Chris Adelson	560		Sailor's Path route - is a walking route. Fencing off the cows and widening the board walk would make it better for cyclists.	Cycling, walking and wheeling can coexist along the Aldeburgh disused railway line and the Sailors Path if appropriate and sensitive infrastructure improvements are made. However, the Aldeburgh-Snape Leisure Route

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				The route from the end of the path into Aldeburgh is along a very busy road. Provide a designated cycle path and reduce speed of traffic.	includes an alternative cycling and walking route to the Sailor's Path if it is considered more appropriate for the Sailor's Path to remain a walking only route. The proposed Aldeburgh-Snape Leisure Route seeks to provide an alternative to the current stretch along the A1094, by introducing cycling and walking route across the A1094 and along upgraded Footpath 16 through the golf course.
3.45 - 3.48	Christopher Lewis	40	Yes	I am an Aldeburgh Town Councillor and on the Services Committee which is responsible for comments on the environment. We have a proposal to make, which is for a dual-use path pedestrian/cyclist between Thorpeness and Aldeburgh. I sent in a copy of the proposal last year and it would link well with the Sailors Path and other proposed routes. Councillor Russ Rainger is aware of the proposal and supports it.	A Leisure Route between Thorpeness and Aldeburgh has been added to the Strategy.
3.45 - 3.48	Dave Fryer	408	Yes	There are many footpaths in the Leiston, Saxmundum, Aldeburgh and Thorpeness area but very few official cycle paths this causes friction between walkers and cyclists, To have paths up graded to cater for both would be a great advantage.	A number of Leisure Routes have been added to the Strategy, including between Saxmundham, Leiston, Aldeburgh and Thorpeness. Detailed design of the Leisure Routes will need to ensure walking, cycling and wheeling can coexist across the routes to ensure all users are catered for.
3.45 - 3.48	David Adelson	462	No	Due to the sensitivity of the RSPB nature reserve at North Warren and Church Farm Marshes, the existing footpath across the reserve should not be widened. Surfacing (to allow wheelchair access) should be done with sensitivity to existing habitat (bushes immediately adjacent the path). As FP6 and 20 lie along existing surfaced tracks between Saxmundham to Leiston Roads, upgrade to Bridleway is a paper exercise only.	Surfacing and lighting solutions must be sensitive to the natural and historic context of the Leisure Routes, which is often the very reason the route may be popular. The most appropriate surfacing and lighting solutions will need to be considered through the detailed design stages. A number of Leisure Routes have been added to the Strategy, including between Benhall, Snape, and Aldeburgh. The Aldeburgh-Snape Leisure Route proposes to remove cyclists and pedestrians from the

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				<p>This upgrade will make next to no difference, since there is no safe way to reach Aldeburgh by bike from either the Snape or Leiston directions. Also the section of the pedestrian route from the Sailor's Path car park is rough underfoot and barely suitable for able-bodied walkers, let alone wheelchairs or buggies. It should be the top priority for the leisure route to address this problem. Action in three areas would make a big positive difference-</p> <ol style="list-style-type: none"> 1. Upgrade the Sailor's Path from Snape to near Aldeburgh to allow cycling. Fence off livestock from the path. 2. Provide segregated cycleway and footway from Aldeburgh end of Sailor's Path towards Aldeburgh, up to existing footways and a point far enough into town that traffic is mostly sticking to 30mph. 3. Segregated pedestrian/cycle route FROM jct A1094/Priory Road (Snape) approx TM404593 TO jct A1094/Mill Road (Friston) approx TM409594, plus crossing point to allow safe cycling and walking between Priory Road and Mill Lane. A crucial 500m section of a leisure route between Snape and Leiston or Minsmere. 	A1094 between the Sailor's Path and Aldeburgh by creating a cycling and walking route along upgraded Footpath 16 through the golf course.
3.45 - 3.48	David Beavan (East Suffolk Councillor)	44	Yes	Dunwich to Southwold and Walberswick is also an important leisure route.	A number of Leisure routes have been added to the Strategy across East Suffolk, including between Minsmere, Walberswick and Southwold.
3.45 - 3.48	East Suffolk (Rachel Smith-lyte)	773		<p>Concerns around the Aldeburgh – Leiston</p> <p>The old railway track because some of it at least is</p>	Surfacing and lighting solutions must be sensitive to the natural and historic context of the Leisure Routes, which is often the very reason the route may be popular. The

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				<p>on sensitive nature areas and certainly RSPB North Warren - so I'm pro as long as it doesn't tarmac path (with lighting) and therefore disturb the wildlife further.</p> <p>Whilst mainly pleased with Melton proposals, I do feel that the Council's plans for rural areas lack ambition. The Council does not seem to recognise that people are deterred from cycling between villages or into their local town by understandable fears for their safety. The ideal standard for cycling is physical separation from motor vehicles and pedestrians.</p>	<p>most appropriate surfacing and lighting solutions will need to be considered through the detailed design stages.</p> <p>Throughout the preparation of the Strategy an appropriate balance has been sought between identifying realistic and deliverable cycling and walking infrastructure whilst also being ambitious.</p>
3.45 - 3.48	EDF (Jack Raven)	351	Yes	A route within this area links up the key tourism areas with rail links and Leiston. This helps to support Net Zero ambitions of Leiston (Net Zero Leiston) by providing active travel solutions as an alternative to vehicular transport, but also provides tourists an alternative to vehicular transport, which creates emissions and congestion. These active travel initiatives link well to sustainable transport solutions being explored, such as demand responsive transport.	Support noted. The transport plans for Leiston have been considered throughout the preparation of the Leisure Routes affecting Leiston to ensure cycling and walking infrastructure is aligned within and adjacent to Leiston.
3.45 - 3.48	Gallagher, Anthony	608	Yes	This is a welcome suggestion which builds on an established route. It is a sensible route encompassing the features of the area well	Support noted.
3.45 - 3.48	Henry Anderson	254	Yes	An excellent idea. A key feature for the success of the onward travel locations needs to be secure bike storage.	Support noted. It will be important that services and facilities, such as secure bike storage, are planned for along the Leisure Routes through detailed design stages.
3.45 - 3.48	Iain Johnston	772		I am very pro the strategy, really requesting some further study of the leisure side of road cycling. From reading the document, it looks like this has fallen through the data cracks somehow. I was	In responding to the number of consultation comments seeking further leisure cycling and walking routes, a number of Leisure routes have been added to the

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				<p>surprised to see the leisure zone triangle up by Aldeburgh/Yoxford/Sax, with no reference to the Sutton / Bawdsey peninsula, which in my experience is just as popular and many local and further afield cycle and triathlon clubs use as training routes. I'm sure you'll have seen some of the groups meeting at Honey and Harvey by your offices in Melton.</p> <p>I note Strava data was mentioned in your data. The global heatmap will certainly back up my point above. https://www.strava.com/heatmap#10.42/1.43133/52.12582/hot/ride</p> <p>Have you been in contact with any of these clubs, or may I pass details on?</p>	Strategy across East Suffolk, including between Melton and Bawdsey.
3.45 - 3.48	Julian Cusack	266	Yes		Support noted.
3.45 - 3.48	Leiston Together (Helen Greengrass)	844	Yes	<p>Leiston Together supports</p> <p>3.45 a Leisure Route in the Leiston area</p> <p>3.46 supports the opportunities as outlined however, would like the strategy to address some sort of direct connectivity between Saxmundham and Leiston to promote sustainable travel choices between the two neighbouring towns and a through route to Sizewell Beach.</p> <p>3.47 To take into account an emerging Transport Strategy of Leiston Town Council</p>	Support noted. An additional Leisure Route between Leiston and Sxmundham has been added to the Strategy. The Transport Strategy for Leiston Town Council has been considered throughout the preparation of Leisure Routes adjoining Leiston.
3.45 - 3.48	Leiston Town Clerk (Caroline Rinder)	377	Yes	Leiston-cum-Sizewell Town Council strongly supports the leisure route for the key opportunities that have been identified and are listed in the consultation. Our own emerging cycling and walking	Support noted. An additional Leisure Route between Leiston and Sxmundham has been added to the Strategy. The Transport Strategy for Leiston Town

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				<p>plans have been based around health and wellbeing, improved tourism access and boosting the local economy. The addition of the leisure route would very much compliment our own town plans and we are excited about the many benefits and opportunities that this would bring.</p> <p>An additional leisure route which benefits further the proposed plans is a more direct route from Leiston to Saxmundham. We believe this would be a popular route for residents especially travelling to and from Saxmundham, importantly for access to the train station.</p>	Council has been considered throughout the preparation of Leisure Routes adjoining Leiston.
3.45 - 3.48	Middleton cum Fordley Parish Council (Sharon Smith)	609	Yes	<p>1. Middleton cum Fordley Parish Council (MPC) strongly supports improvements in provision for Cycling and Walking in the area suggested for the 'leisure route'. However, it is disappointing that there is much less detail set out here than in other parts of the strategy. This makes constructive comment difficult.</p> <p>2. The strategy will need to respond to the approval of the SZC DCO if this is forthcoming. We are surprised that the draft strategy barely mentions this risk because if it does go ahead, it will cause significant disruption to many existing cycling and walking routes. Within our local area we particularly object to the loss of connectivity south to Kelsale and Saxmundham under the current proposals for the Sizewell Link Road (SLR). This includes the closure of Hawthorn Road, Fordley Road and Littlemoor Road each of which have recently seen increased use by walkers and cyclists and have been</p>	Support noted. The development consent order for Sizewell C has been granted, and therefore the Leisure Route recommendations reflect the infrastructure provisions that will accompany Sizewell C, including the Link Road. Leisure routes have been added to the Strategy across East Suffolk, including between Saxmundham, Yoxford, Darsham Railway Station, and Leiston. These routes reflect the importance of connections to other settlements such as Kelsale, Middleton, Theberton and Eastbridge. There are a variety of ways in which safe cycling and walking environments can be created, and the Leisure Routes seek to make best use of all available infrastructure opportunities depending on the specific local context. Community Recommendations have been assessed against a number of criteria including biodiversity and safety. Throughout the preparation of the Strategy an appropriate balance has been sought between

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>designated Quiet Lanes. There are also a number of Public Rights of Way that will be disrupted by the SLR with diversions and a requirement for walkers to cross the SLR unaided.</p> <p>3. The strategy for the leisure route as drafted assumes that the major benefits are to tourism and leisure. The untapped potential for leisure cycling was strongly evident during the first 2020 lockdown when a significant increased usage of rural roads by cyclists was observed in and around the village.</p> <p>However, MPC considers that the draft strategy understates the opportunity for modal shift (= replacing car journeys by cycling or walking) in this and other rural areas albeit that this shift is likely to take longer to establish as improvements in the infrastructure persuade more people that it is safe to cycle.</p> <p>Examples of potential journeys which could be made by cycle instead of car include:</p> <ul style="list-style-type: none"> • Westleton, Darsham, Eastbridge, Theberton, Middleton, Kelsale, Benhall and Sternfield villages into Saxmundham to access services including the health centre, shops and the train station. • Middleton, Yoxford and Darsham to the station at Darsham. 	<p>identifying realistic and deliverable cycling and walking infrastructure whilst also being ambitious.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>4. Creating a segregated and continuous cycle way along the A12 from Darsham station north to (at least) Blythburgh and south to Marlesford would provide an option for modal shift over longer journeys as well as supporting leisure cycling.</p> <p>5. Cycling and walking for access to services and for leisure in rural areas will always likely require passage along single track country lanes. The establishment of the Quiet Lanes network is helpful but needs to be supplemented by measures to further deter and slow down motor vehicles such as traffic calming measures, speed limits and (if supported locally) 'Access Only' traffic orders. Many passing places on these roads are in poor repair which can make them hazardous for cyclists trying to use them as refuges or to allow vehicles to pass.</p> <p>6. We note that in many country lanes there is limited space within the highways boundary to allow for the provision of footpaths or cycle paths. This is particularly the case when the road is bounded by hedges and or banks. We note that many of the community suggestions for footpaths along these roads have scored badly on biodiversity on the basis that hedges or other vegetation would need to be removed. This negative scoring often negates the acknowledged gains from safety in arriving at the total score.</p> <p>An example of this can be found in the assessment of Community Response Reference 97 where the comment reads ' Walkers wishing to link between Footpath Westleton 25 (Reckford Bridge) and Bridleway Westleton 26 (Black Slough) have to</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>walk along a dangerous stretch of the B1125 where there is no space for pedestrians around a tight bend'</p> <p>Comment 27 makes a similar point but seeks to extend the provision of a footpath from Reckford Bridge to Rectory Road, Middleton.</p> <p>Both these proposals score 3 for safety on the grounds that <i>'a narrow road at national speed limit with visibility constraints means the suggestion is considered to offer safety benefit'</i>. But this is negated by a score of -3 for biodiversity because of the <i>'Potential impact on existing hedge'</i>.</p> <p>The photo shows a section of the hedge taken in Winter. This is typical of such hedges with a high proportion of non-thriving elm and limited height, width and density. While other things being equal, we would always wish to preserve (and improve) existing hedges it would be quite possible to remodel the highway boundary in such a way as to incorporate a footpath while replacing the existing hedge further from the road boundary. A good quality replacement hedge using a good mix of native hedging plants can be established in a few years resulting in biodiversity gain.</p> <p>Both these proposals are supported by MPC but they score only 1 and 2 respectively. There are over 200 proposals that score 6 or more so it seems unlikely that these safety improvements will be undertaken. We suggest that the scoring system is</p>	

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				<p>currently over-weighting minor short-term negative biodiversity impacts relative to safety gains.</p> <p>We acknowledge that solutions which score well on safety may require the acquisition or licensing of land outside the existing highway boundary. It is not clear whether this is seen as part of the implementation strategy proposed by ESC or whether this approach would be supported by SCC in cases where there is significant positive scoring on safety.</p> <p>7. <u>Conclusion</u></p> <p>The draft strategy suggests some exciting opportunities for enhancing cycling and walking opportunities in rural East Suffolk, including the proposed development of the Aldeburgh-Leiston disused railway line.</p> <p>But overall, we consider that the strategy for our area lacks ambition by under-estimating the long-term opportunity for modal shift which could be encouraged by investment in safer cycling and walking routes.</p> <p>We also consider that locally important safety gains are likely to be over-looked as a result of a scoring system which does not give them sufficient weight.</p> <p>Attachments: https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/454765/PJP/-/11772437%201%20Hedges%2Ejpg</p>	

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
3.45 - 3.48	Peter Franklin	164	Yes	<p>NB. The marked route on the map disappears when zooming in, so it's not possible to see where the proposed route would be.</p> <p>I support any leisure cycling/walking routes, particularly if circular (ie not a to b). Try and replicate the great cycling routes found in Rendlesham Forest. We need more of these all over the district. Bear in mind many people will drive to the cycle routes so provide adequate facilities for parking and, ideally, toilets. Also, try to keep off the roads. Off-road cycling is such a pleasure, with no worries for safety and being able to take your time, stop and look around and really enjoy the experience, which is just not possible on roads.</p>	It will be important that services and facilities, such as toilets, are planned for along the Leisure Routes through detailed design stages. Cycling and walking infrastructure must be safe and therefore cycling and walking on roads will generally not be appropriate. As such, the Leisure Routes follow PROW network and low trafficked roads.
3.45 - 3.48	Philip Levermore	430	Yes	I am very much in favour of developing these cycle and walking routes around the local area. I also strongly feel any paths/routes provided are for all - cyclists and walkers together and not exclusively for either.	It is important that conflict between users is avoided wherever possible. Cycling, walking and wheeling can coexist along the Leisure Routes if appropriate and sensitive infrastructure improvements are made.
3.45 - 3.48	Phil Lines	41	No	How can you limit comments to starting with a yes/no choice on whether the plan is supported? I cannot answer yes or no because it is completely unclear what the 'leisure route' involves. Currently the map you have included offers no more information than a large blue line which seems to run right through my property. Until you provide proper, detailed information I therefore have to say no	The purpose of the 'yes/no' option at the Draft Strategy consultation was to enable greater understanding as to whether the general principle of more leisure cycling and walking routes would be supported in the area. A comments box was also available to use to provide more detailed comments. The Leisure Routes will not run through private properties, and will follow indicative routes where detailed infrastructure improvements are not set out.
3.45 - 3.48	Saxmundham Town Council (Jennifer Morcom)	228		Saxmundham Town Council has no objection to the leisure route but of far greater importance to us is safe cycling around and within Saxmundham itself and between Benhall, Saxmundham and	A number of Leisure Routes have been added to the Strategy, including between Benhall, Saxmundham and Kelsale. While the primary purpose of the Leisure Routes is to facilitate cycling and walking for leisure,

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				Kelsale. We feel that the safety and encouragement of cycling to school, work or local shops and services should be a priority.	recreation and tourism, they also provide safe and convenient infrastructure for commuting to work and school, as well as to access services and facilities. Cycling and walking infrastructure recommendations have also been made in respect of the South Saxmundham Garden Neighbourhood, as site allocation recommendations.
3.45 - 3.48	Steve Liddell	104	Yes	I support the provision of a leisure route, but would strongly suggest that the A1094 is not part of this route. This is a fast road with limited visibility in places. Ideally a leisure route would provide an attractive alternative to the A1094, with the aim of reducing or eliminated cycling traffic on the A1094	Support noted. A number of Leisure Routes have been added to the Strategy which cross the A1094, although none follow it. These Leisure Routes connect Benhall, Snape, and Aldeburgh. The Aldeburgh-Snape Leisure Route proposes to remove cyclists and pedestrians from the A1094 between the Sailor's Path and Aldeburgh by creating a cycling and walking route along upgraded Footpath 16 through the golf course.
3.45 - 3.48	Steven Falvey	189	Yes	I strongly support the creation of cycling and walking routes connecting these local rural communities. Doing so would enable tourists and local residents to use alternatives to motor vehicles when wanting to access local services.	Support noted. While the proposed Leisure Routes are primarily intended for leisure, recreation, and tourism use, they also function to enable residents to access services and facilities.
3.45 - 3.48	Sudbourne Parish Council (Bill Parker)	764		Thank you for the opportunity to respond to the cycling and walking strategy consultation. As a Parish Council we welcome and support the development and delivery of this strategy and have the following comments: 1. The development of leisure routes – We welcome this and seek clarification as to whether this is for cyclists or walking. It is our view that people either walk or cycle and are less likely to do both activities in the same trip. It is our view that in view of	The purpose of the Leisure Routes is to provide traffic free and low traffic routes suitable for all users, including cycling, walking, wheeling, and equestrian users. Roads such as the A1094 and B1122 have not been recommended to carry cyclists or pedestrians without significant segregated cycling and walking infrastructure improvements. While the Sailor's Path is currently solely a walking route, the Aldeburgh-Snape Leisure Route sets out recommendation options to either make the Sailor's Path a cycling and walking route, or to retain the Sailor's Path as a walking route and create a new cycling and walking route between Aldeburgh and Snape north of

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				<p>the growth and interest in cycling this needs to be developed further:</p> <ol style="list-style-type: none"> 1. We note that the proposal as it stands appears to use the A1094 Aldeburgh to Snape road. Whilst it is outside of our parish we have concerns over the safety of cyclists on this narrow and busy road and question whether this is a sensible route to promote. This route is also vulnerable to a potential significant increase in heavy vehicle usage with the proposed energy infrastructure developments in the Sizewell / Snape areas this also applies to the B1122. <p>In addition one of the benefits highlighted in document for this route includes: Links to the proposed Established leisure walking routes (e.g. the Sailors' Path route; between Aldeburgh and Snape, and the wider Sandlings Walk. We note that Sailors path is a walking route and therefore inappropriate for cyclists.</p> <ol style="list-style-type: none"> 2. We are surprised that the Woodbridge / Snape / Orford route (which passes through Sudbourne) is not proposed as a leisure route. We note that you 	<p>the A1094. A number of Leisure Routes have been added to the Strategy, including between Snape, Orford and Woodbridge, passing through Sudbourne.</p>

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				<p>have used Strava data and question as to whether this has been interpreted accurately. Especially in summer and at weekends this is a very popular circuit. It also benefits from:</p> <ol style="list-style-type: none"> 1. Access to onward travel opportunities, particularly Woodbridge train station; 2. Access to Ipswich / Woodbridge etc for those seeking longer rides 3. Tourism destinations such as Snape Maltings and Orford; <p>We suggest that this should be reconsidered and either the leisure route is extended or an additional route as described.</p>	
3.45 - 3.48	Suffolk Coast & Heaths and Dedham Vale AONBs (Simon Amstutz)	388		<p>There appears to be little reference in this section to the existing Suffolk Coast Path or the emerging England Coast Path or the Suffolk Cycle Route or existing walking and cycling routes promoted by the AONB, the Suffolk Coast Ltd or Discover Suffolk</p> <p>There are many other tourist destinations (beyond Snape Maltings) the towns on the coast, Sutton Hoo, Minsmere are all internationally recognised.</p> <p>east Suffolk has an opportunity to develop a leading role in more sustainable traveling from contributions from NSIPs</p>	A number of information sources have been used in the creation of the Strategy, and the Leisure Routes, including but not limited to the Suffolk Coast Path and emerging England Coast Path, Suffolk Cycle Route, and the AONB walking and cycling guides. A number of Leisure Routes have been added to the Strategy, including between Sutton Hoo and Bawdsey, as well as through Minsmere.
3.45 - 3.48	The Long Shop Museum (Fraser Hale)	196	Yes	Would it be possible to bisect the proposed route to introduce a link between Saxmundham (the closest railway station) and Leiston. This would create a	A Leisure Route has been added to the Strategy between Saxmundham and Leiston.

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				greater variety of ways to follow the route, while providing safe and attractive access allowing walkers and cyclists who have arrived in East Suffolk by train to more easily reach the visitor attractions in and around Leiston-cum-Sizewell. The Long Shop museum encourages its audiences to visit the town using public transport wherever possible. Providing a safe, low-carbon access option from a rail halt would allow us to step up our advocacy of this policy.	
3.45 - 3.48	Tony Pick	586	Yes	As a keen local cyclist I am very much in support of this scheme. Our local area would certainly benefit from the proposed leisure cycling routes and I would welcome this.	Support noted.
3.45 - 3.48	Tony Pick	587	Yes	As a keen local cyclist I am very much in support of this scheme. Our local area would certainly benefit from the proposed leisure cycling and walking routes and I would welcome this.	Support noted.
3.45 - 3.48	Ufford Parish Council (Judi Hallett)	732		<p>Section 3.45 onwards - Leisure Corridors</p> <p>The only “leisure route” proposed is in the Aldeburgh, Snape and Yoxford triangle. There should be more attention given to other popular cycle routes for example Woodbridge/ Orford/ Snape and Woodbridge/ Bawdsey/ Orford - both popular with their proximity or Rendlesham and Tunstall Forest. The latter would also facilitate linkage to the Felixstowe Corridor via the Bawdsey to Felixstowe Ferry.</p>	In order to reflect the need for leisure cycling and walking more widely across East Suffolk a number of Leisure Routes have been added to the Strategy, including between Woodbridge, Orford, and Snape, as well as Woodbridge, Bawdsey, and Orford.
3.49 - 3.52	James Winterbotham	498		Again Southwold and Reydon completely absent - was this because of perceived lack of need or because it is just too difficult?	Recommendations were made in different formats reflecting different needs. Southwold and Reydon are not part of a key corridor which took an evidence-based approach to create cohesive connections to (and through) the larger settlements. However,

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					<p>recommendations were made through the community consultation relating to Southwold and Reydon which were analysed and scored to then form part of the strategy. Furthermore, reflecting the desire to improve cycling throughout the council area and recognising the importance of leisure cycling and walking the leisure routes section has been expanded now connecting to Southwold.</p> <p>Finally, the allocations section includes opportunities in and around allocated site which includes one allocation in the Southwold and Reydon area.</p>
SCLP12.19	Chris Taylor	364	Yes	<p>Whilst the recommendation refers specifically to the "Brightwell Development"...</p> <p>Any improvements to the cycling / walking provision in relation to this development will also benefit those actively travelling to/from the surrounding villages Waldringfield, Newbourne and Bucklesham and onwards to Felixstowe.</p> <p>Recommendations for specific developments should also mention the broader area served. So as to inform the reader that there are benefits to others rather than just those who may live or work on the proposed development site.</p>	<p>The title used is taken directly from the allocation policy so does not refer to its broader benefits. Whilst the submitted point is noted a consistent approach has been used in all allocations.</p>
SCLP12.19	Taylor Wimpey East Anglia (Sir/Madam)	757		<p>Re: East Suffolk Cycling and Walking Strategy Consultation Response on behalf of Taylor Wimpey East Anglia</p> <p>On behalf of Taylor Wimpey East Anglia we are pleased to provide comments in response to the Cycling and Walking Strategy that East Suffolk</p>	<p>Comments noted. The SCLP12.19 site allocation recommendations have been amended to align with the routes to be delivered by the site via the planning application process.</p>

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				<p>Council have prepared. Taylor Wimpey East Anglia are actively involved with a number of sites across East Suffolk and are working closely with the Council and the local community to bring forward development at Brightwell Lakes.</p> <p>The Cycling and Walking Strategy covers the entirety of East Suffolk, but for the purposes of this consultation response we have focused on Brightwell Lakes and how this strategy influences the outline planning permission (approved by the Council in 2018) and the site allocation Policy SCLP12.19 contained within the East Suffolk (Suffolk Coastal) Local Plan (adopted in 2020).</p> <p>In August 2021, four planning applications were submitted to East Suffolk by Taylor Wimpey East Anglia which sought approval of reserved matters for the first phases of development at Brightwell Lakes. Applications DC/21/4002, DC/21/4003, DC/21/4004 and DC/21/4005 will deliver residential dwellings alongside new access routes and connections to improve Cycling and Walking infrastructure at this site.</p> <p>Overall the site at Brightwell Lakes will see the creation of up to 2,000 dwellings along with education provision, community facilities, open space, areas of recreation, heritage park and SANG areas alongside new access routes connecting the site to the A12 and Ipswich Road. Brightwell Lakes will be connected through a variety of cycling and walking routes throughout to encourage movement</p>	

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				<p>via sustainable modes of transport providing connections to services and facilities, as well as linking in with the existing Public Rights of Way Network.</p> <p>Alongside the activities to develop the site, Taylor Wimpey East Anglia are actively engaging with the local community to help inform and influence development as it comes forward. In accordance with the outline planning permission, the Brightwell Lakes Community Forum met on two occasions in 2021 with a programme of future meetings to take place. The engagement with the Community Forum provides an opportunity for the developers and the local community to share information and understanding about the site and emerging proposals. A key discussion point has been how Brightwell Lakes integrates with the existing communities (such as Martlesham and Waldringfield) through convenient and safe access points and linkages to the existing Public Rights of Way Network.</p>	
SCLP12.19	Taylor Wimpey East Anglia (Sir/Madam)	763		<p>The proposals identified within the Cycling and Walking Strategy relating to Brightwell Lakes are noted and Taylor Wimpey East Anglia are keen to see that the strategy better reflects the outline planning permission and the site allocation policy for Brightwell Lakes. As currently written, the strategy fails to address the wide range of connections into and through Brightwell Lakes.</p> <p>The intention behind the strategy is welcomed and seeking opportunities to improve opportunities for</p>	<p>Comments noted. The SCLP12.19 site allocation recommendations have been amended to align with the routes to be delivered by the site via the planning application process.</p>

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				<p>Cycling and Walking across East Suffolk is supported as a principal, however there needs to be more clarity as to how proposals relate to existing planning permissions and site allocations and the implementation of routes alongside development that may often include third party land.</p> <p>Taylor Wimpey East Anglia are keen to continue working with the Council, Suffolk County Council and the local community to enable Cycling and Walking opportunities to come forward at Brightwell Lakes in a timely manner but these will need to be funded through various means and not rely solely on developers.</p>	
SCLP12.24	Ipswich Borough Council (Anna Roe)	64		<p>The Council only wishes to comment on the 'Draft Cycling and Walking Strategy' at this time.</p> <p>The Council welcomes the opportunity to comment and is pleased to see that there are links included from the Ipswich boundary to connect with Martlesham. There is an aim to connect to Ipswich Hospital and this requires some additional work to ensure this is achievable from an Ipswich perspective.</p> <p>Map 1 of the Ipswich Cycling Strategy SPD 2016 shows the East Ipswich and Ipswich Hospital Corridors which the draft Strategy appears to align well with. This can be found at the following link: https://www.ipswich.gov.uk/sites/www.ipswich.gov.uk/files/cycling_strategy_spd.pdf</p>	The comments have been noted and the information supplied appreciated. The Strategy has attempted to create suitable connections with Ipswich following meetings directly with Ipswich Borough Council.

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				<p>The emerging Ipswich Local Plan includes a ‘green trail’ which is a <i>“walking and cycling route around the edge of Ipswich which also contributes positively to the enhancement of strategic green infrastructure to deliver benefits to both people and biodiversity and to help new developments deliver biodiversity net gain.”</i></p> <p>Policy DM10 of the emerging Local Plan states that the Council <i>“will seek to establish and extend a publicly accessible green trail around the edge of the Borough as illustrated on Plan 6 in order to address the need within the Borough for access to Natural and Semi Natural Greenspace. The green trail will provide an ecological corridor and a recreational resource for people to use. Development at the edge of the built up area will be required to provide links within the green trail as part of their on-site open space provision.”</i></p> <p>The key site allocations on the boundary of IBC and East Suffolk which will link into the Green Trail include the ISPA4.1 allocation at Humber Doucy Lane (adjacent to the land allocated through Policy SCLP12.24 of the recently adopted Suffolk Coastal Local Plan), the Ipswich Garden Suburb allocations and the land allocated for sports use north of the Millennium Cemetery along Tuddenham Road. There is an opportunity to link the East Suffolk cycling and walking network with these principal key allocations and the wider Green Trail. (This is in early stages of development, but we would not wish to lose these opportunities.)</p>	

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SCLP12.29	Benhall and Sternfield Parish Council (Melanie Thurston)	659		<p>We agree with all the recommendations relating to, and adjacent to our Parish, but would like to add the following:</p> <p>1. Saxmundham / Benhall SCLP12.29 South Saxmundham Garden Neighbourhood</p> <p>a) We note the proposal to make School Lane one-way with 20 mph. We feel this is likely to encourage speeding irrespective of speed limit – which could be more dangerous for pedestrians etc. Would two way to be allowed for cyclists and equestrians, with some careful signing?. We recommend that ES consider the Goldings Lane, Leiston solution ie (physical restriction half way along to minimise motorised use). We have raised this before – during the Local Plan consultation process.</p> <p>b) The map shows that in the past, three separate comments have been made re the footpath behind the hedge alongside South Entrance - Nos 39, 411 and 422. These all have been scored at 6, which includes minus 3 for loss of biodiversity resulting from removal of the hedge. At our last Parish Council Meeting, the Saxmundham environment consultant was present. When asked whether she favoured removal of the hedge to widen the roadside path, or whether she preferred to retain the hedge and widen the footpath behind, She chose the later. Benhall & Sternfield Parish Council agree with this. Therefore we wish to challenge the minus 3, and request that the score be amended to 9. This would have the effect of elevating this path in the priority listing.</p>	<p>It is possible that a one way system could allow contraflow for pedestrians, cyclists and equestrians. The alternative modal filter suggested has been incorporated into the recommendations.</p>

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SCLP12.29	Pigeon Investment Management Ltd (Fillmore, Andrew)	869		<p>EAST SUFFOLK COUNCIL DRAFT CYCLING AND WALKING STRATEGY RESPONSE ON BEHALF OF PIGEON INVESTMENT MANAGEMENT LTD</p> <p>Thank you for consulting Pigeon Investment Management Ltd ('Pigeon') on the East Suffolk Draft Cycling and Walking Strategy. We very much welcome the opportunity to participate in the current consultation. The following comments are submitted in the interests of facilitating the delivery of sustainable cycling and walking infrastructure in the district, and ongoing collaborative working between Pigeon and East Suffolk Council.</p> <p>Pigeon supports the general approach set out in the draft Strategy of promoting and facilitating new cycling and walking routes across East Suffolk, as well as the overarching themes for sustainable green travel and mental and physical wellbeing.</p> <p>Our site interests within the District include:</p> <ul style="list-style-type: none"> • Saxmundham Garden Community, that is the subject of an allocation in the Suffolk Coastal Local Plan and ongoing masterplan; • Land adjacent to Reeve Lodge in Trimley St Martin that is the subject of an outline planning application (ref. for a high quality and landscape and design-led sustainable scheme for up to 139 new homes (including provision of up to 46 affordable homes), land for a two-form entry primary school with pre-school, open space, SUDS, meadow and informal path on land south of Gun Lane, and all associated 	<p>The identification of the cycling and walking infrastructure recommendations in the draft Strategy as high-level opportunities highlighted the importance of engagement in the preparation of the Strategy. This wording has therefore been updated in the final Strategy to highlight the importance of delivering the recommendations as set out in the Strategy. The Strategy does not add new policy requirements, but expands on existing policy requirements in respect of how they are expected to be delivered, as set out in criteria g) and p) of the policy and the Infrastructure Delivery Framework (page 466 of the Local Plan). The Strategy represents much of the analysis that would need to take place as part of a Transport Assessment and effective masterplanning in order to recognise the important cycling and walking infrastructure both on and off site. Such detailed analysis is often not included in allocation policies but is expected at application stage. The Strategy therefore provides the framework to aid developers in designing cycling and walking infrastructure into development proposals. If supporting text to a policy is silent on a particular policy criterion it does not necessarily mean that it should be assumed that the policy criterion is narrow in its scope. The recommendations set out for this site are consistent with the policies contained in the Suffolk Coastal Local Plan.</p>

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				<p>infrastructure provision; and</p> <ul style="list-style-type: none"> • Land at High Road, Trimley St Martin. <p>To assist the Council in its ongoing preparation of the Cycling and Walking Strategy we have provided a detailed response to the Community Recommendations, Key Corridor Recommendations and Site Allocations Recommendations set out within the consultation document. Our comments in respect of the recommendations that are relevant to the Saxmundham Garden Community and the Trimley Villages are included as Appendix 1 (Saxmundham) and Appendix 2 (the Trimley Villages) respectively.</p> <p>Site Allocation Recommendations</p> <p>SCLP12.29 South Saxmundham Garden Neighbourhood</p> <p>Paragraph 1.5 of the draft Cycling and Walking Strategy states that ‘infrastructure opportunities’ identified should not be read as prescriptive proposals, or as the only way in which infrastructure improvements can be delivered, but as ‘high-level opportunities’. Paragraph 3.50 of the Site Allocation Recommendations advises these recommendations ‘add value’ to adopted planning policies and aid the delivery of sustainable development. Pigeon wishes to take this opportunity to emphasise that requirements relating to Cycling and Walking provision for South Saxmundham Garden Neighbourhood are set out under adopted policy</p>	

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				<p>SCLP12.29, and it is not the role of this Strategy to introduce new or amend existing requirements. The Strategy should clearly articulate this. Many of the recommendations being put forward are not identified in the adopted Local Plan, and are not set out in the Infrastructure Delivery Plan which formed part of the Local Plan evidence base.</p> <p>Whilst we acknowledge the councils’ aspirations regarding new provisions. As recognised in paragraph 1.7 there are a variety of funding and delivery opportunities available beyond improvements secured via planning permissions, and the Strategy should look to make use of these to facilitate Cycling and Walking improvements beyond those set out under adopted policy.</p> <table border="1" data-bbox="846 847 1391 1378"> <thead> <tr> <th data-bbox="846 847 1111 903">Recommendation</th> <th data-bbox="1111 847 1391 903">Pigeon Comments</th> </tr> </thead> <tbody> <tr> <td data-bbox="846 903 1111 1378">1. Introduce a cycling and walking track along the B1121, segregated by the road from the existing hedgerow.</td> <td data-bbox="1111 903 1391 1378"> Recommendations 1 – 4 collectively seek to improve connectivity for pedestrian/cyclists between Benhall and Saxmundham, through both new off-road segregated provision and improvements on-road. Pigeon fully supports enhanced active transport connectivity </td> </tr> </tbody> </table>	Recommendation	Pigeon Comments	1. Introduce a cycling and walking track along the B1121, segregated by the road from the existing hedgerow.	Recommendations 1 – 4 collectively seek to improve connectivity for pedestrian/cyclists between Benhall and Saxmundham, through both new off-road segregated provision and improvements on-road. Pigeon fully supports enhanced active transport connectivity	
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1. Introduce a cycling and walking track along the B1121, segregated by the road from the existing hedgerow.	Recommendations 1 – 4 collectively seek to improve connectivity for pedestrian/cyclists between Benhall and Saxmundham, through both new off-road segregated provision and improvements on-road. Pigeon fully supports enhanced active transport connectivity								

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					<p>along this corridor, which will improve access to services and facilities in Saxmundham for the benefit of Benhall residents.</p> <p>We however wish to highlight the requirements relating to off-site transport connectivity associated with the new Garden Neighbourhood are established under adopted policy, and it is not a requirement for such provisions to be provided as part of delivering the garden neighbourhood.</p> <p>The policy requirements are for <i>'Significant pedestrian and cycle accessibility throughout the site, with connections and improvements to networks beyond the site, including to the station and town</i></p>	

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					<p><i>centre</i>'. Paragraph 12.295 of the supporting text seeks to 'retain' the links between Benhall and the rest of the Garden Neighbourhood through existing footpaths, but does not seek enhancements of the route between Saxmundham and Benhall. The Cycling and Walking Strategy should acknowledge this, and make it clear that an opportunity to deliver improved connectivity between Benhall and Saxmundham is not liked to bringing forward the Garden Neighbourhood .</p>	
				<p>2. Upgrade Footpath 19 to a bridleway, widen and resurface accordingly.</p>	<p>Please see response to Recommendation 1.</p>	

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				<p>3. Introduce a signalised cycling and pedestrian crossing point on the B1121 to access School Lane.</p>	<p>Please see response to Recommendation 1.</p>	
				<p>4. Make School Lane one way for vehicles and extend the School Safety Zone along the entire length of School Lane to reduce the speed limit from the current 60mph to 20 mph.</p>	<p>Please see response to Recommendation 1.</p>	
				<p>5. Introduce a cycling and walking connection from the site onto Kiln Lane.</p>	<p>Pigeon support opportunities to create appropriate pedestrian and cycle connections between the new homes and SANG provision east of the railway. Whilst Kiln Lane could provide such an opportunity we would caution against</p>	

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					<p>the inclusion of this recommendation until further discussions with NetworkRail have been undertaken which fully explore the appropriate means of securing access over the railway.</p>	
				<p>6. Upgrade Footpaths 16 and 17 to bridleways, widen and resurface accordingly.</p>	<p>Pigeon supports the upgrade of Footpaths 16 and 17 to deliver improved connectivity between the Garden Neighbourhood and Saxmundham town centre. Any improvements should be secured through payment of S106 contributions with work undertaken by the County Council as statutory authority.</p> <p>These improvements should align with the existing PROW routes as per adopted policy, with any incursion off secured using council</p>	

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					<p>powers and delivered by the council.</p> <p>7. Upgrade the existing railway bridge to accommodate cyclists and pedestrians.</p> <p>8. Introduce a cycling and walking track from the existing railway bridge, north along the western side of the railway, the eastern edge of the cemetery, Park Lane, Alma Place,</p>	<p>Pigeon supports upgrading to the surfacing over the railway bridge in order to enhance cycling and walking connectivity between the Garden Neighbourhood and town centre, but wish to make the point that the bridge itself does not need to be upgraded. This recommendation should be amended to refer solely to improvements to the surfacing of the bridge.</p> <p>Criteria p of adopted policy SCLP12.29 requires the Garden Neighbourhood to connect to and improve networks beyond the site, including to the station. There is no requirement for</p>

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				Station Approach, and into Saxmundham Train Station.	additional connections beyond which already exist, and introducing a recommendation which is not set out in established policy is both unreasonable and counter-productive to bringing forward sustainable development. Pigeon do not support this recommendation.	
				9. Introduce pedestrian and cycle connections onto widened Footpaths 13 and 15.	Pigeon support improved connectivity to the north of the Garden Neighbourhood via upgrades to the existing footpath connections or through the creation of alternative routes as part of the redevelopment of the Fromus Centre. We suggest this recommendation is amended to include reference to creating a new link through the Fromus Centre as	

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					<p>an alternative to enhancements to Footpaths 13 and 15.</p>	
				10. Introduce cycling and walking connection onto Lincoln Avenue.	<p>As per the response to recommendation 8, it is not the role of the Cycling and Walking Strategy to introduce new off-site connections relating to the allocated Garden Neighbourhood beyond that set out in adopted policy.</p> <p>This recommendation should be removed.</p>	
				11. Introduce cycling and walking connection between the employment and residential development on either side of the A12.	<p>Pigeon fully support enhanced cycling and pedestrian connectivity over the A12, which will facilitate active transport links between the residential and employment areas. It should be noted there is an existing PROW connection over the A12 to the south of the</p>	

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					<p>employment allocation, and the recommendation should be amended to encourage the new crossing to better relate to this connection. Such an approach would ensure enhanced connectivity to not only the employment land, but wider footpath network west of the A12 which offers substantial recreation benefit to residents.</p> <p>Extensive discussions have been held with SCC Highways on the most suitable access arrangement and pedestrian/cycling crossing, with the agreed approach being the construction of a new roundabout on the A12 with Toucan crossing. Such an approach will provide a safe and</p>	

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					<p>accessible means of crossing the A12.</p> <p>12. Introduce a segregated cycling and walking track from the railway bridge to the required on-site community hub.</p> <p>Pigeon supports the delivery of a community hub at the Garden Neighbourhood which will help to meet the day-to-day needs of local residents of the new neighbourhood. It is important to recognise that such a hub should not compete/undermine the role of the established town centre.</p> <p>As such it is important to ensure good cycling and walking connectivity to the hub from across the site, and that an appropriate connection between the railway bridge and community hub does not necessary involve a segregated walking and cycling link. This recommendation</p>	

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				<p>should be amended to refer to a good connection between the railway bridge and community-hub.</p> <p>There should be a consistent approach to cycling/walking provision across the site, and it is unclear why this connection should not be signalled out.</p>	
SCLP12.32	-	278	Yes	This is a wide road and a major cycle way and is flat . .It should support a segregated cycle way where possible , and where not nearer the town priority cycle ways	The road referenced is assumed to be the Thoroughfare/Melton Hill. This road is recommended for improvement in both the key corridors and allocation recommendations. The road appears to be sufficiently constrained to restrict segregated infrastructure and the proposed recommendations reflect this, however this would be fully determined during the design stage.
SCLP12.32	David Adelson	421	No	There are some good ideas in this proposal, however it would be better to route cyclists through the low-lying rear open area of the former council offices, retaining land to do this when selling the site for development. Cyclists could then be routed from Melton Hill to Quayside/Lime Kiln Quay Road via New Quay Lane (or Old Maltings Approach), into the former council offices site, and from there to Deben Road. This avoids quite a bit of the hill for cyclists. A crossing on Quayside at the Deben Road junction (as	The suggested cycling and walking route through the allocation, alongside the railway line, Deben Road, Quayside, to Hamblin Road Car Park has been incorporated into the site allocation recommendations.

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				<p>suggested already in the site allocation?) would give access to the North side of Quayside, where a cycleway could be built on the existing footway, with pedestrians routed through Elmhurst Park or Thoroughfare.</p> <p>The suggestions already made in the site allocation could also be done as a pedestrian route, separating pedestrians from cyclists as well as separating both from motor traffic.</p>	
SCLP12.33	David Adelson	422	Yes	<p>The crossing of the B1438 mentioned, appears to be at the top of California by the Duke of York (going by the proposed routes in Woodbridge). There is already an island here of use to pedestrians. The proposed route IM16 isn't useful to cyclists or able-bodied walkers owing to several problems. However a crossing of the B1438 at Sandy Lane would be very useful indeed to walkers, improving walking from Sandy Lane towards the residential areas around Old Barrack Road and also along the B1438 East/West by connecting the footway sections together. It may also help to calm motorised traffic and assist in keeping speeds down to the legal limit.</p>	<p>The proposed B1438 bridge is located along the southern site boundary. IM16 proposes to improve cycling and walking infrastructure along Sandy Lane and Ipswich Road.</p>
SCLP12.40	Andrew McDonald	568	Yes	<p>Good proposals, with significant 'modal shift' and safety potential.</p>	<p>Support Noted.</p>
SCLP12.40	Chris Taylor	366	Yes	<p>3) ...track should be extended to Ash Road Junction where it can connect directly with Ash Road (as a proposed Quiet lane)</p>	<p>The site allocation recommendations have been amended to extend the route to Rendlesham Mews to the Ash Road/A1152 junction.</p>
SCLP12.40	David Adelson	423	Yes		<p>Support noted.</p>
SCLP12.40	john o'connor	47		<p>Yes! Tjis is an extremley important area for leisure cycling and the A1152 is a dangerous road at the moment and the lack of safe access to the roads east of this area is a limitation at present.</p>	<p>Support noted.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
SCLP12.41	David Adelson	424	Yes		Support noted.
SCLP12.42	David Adelson	425	Yes		Support noted.
SCLP12.45	Campsea Ashe PC (Richard Fernley)	191	Yes	This is certainly needed and along other footpaths in the area; along Ashe Road to the shop. The PC have been in discussion with landowners in this respect.	Support noted.
SCLP12.47	David Adelson	441	Yes		Support noted.
SCLP12.47	East Suffolk (Rachel Smith-lyte)	774		<p>Darsham area A12</p> <p>The A12 north and south from Darsham Station is lethal for cyclists and pedestrians – there clearly needs to be some sort of safe crossing at the very least from the station towards the villages of Westleton and Middleton etc.</p>	The comments are noted. The issues of crossing the A12 and accessing Darsham train station are recognised and the allocation recommendation recommends a crossing point. The exact location of which will be determined the design stage, but consideration of the onward connection to the other villages is important.
SCLP12.47	John O'connor	45	Yes	<p>Excellent plan on Yoxford to Darsham Station. However extending cycle/pedestrian path north beyond the station allows connection to existing cycle route at A12/Darsham and A12/Willow marsh Lane junctions. Current path is non-existent on northern side of level crossing and needs widening near these junctions.</p> <p>At Yoxford/A12/B1122 junction. In regard to Sizewell C the development needs to be managed for cyclists and pedestrians in the road re-configuration.</p>	The site allocation recommendations have been amended to extend the recommended cycling and walking track alongside the A12 to Darsham Service Station.
SCLP12.50	Anonymous	277	Yes	The whole route from Rendlesham to Woodbridge should be prioritised. A cycle priority route like that in Martlesham would work here. It is a major housing area and there is now significant employment at Bentwaters Air Base. Traffic moves very fast along this long straight road. The traffic lights in Eyke are ineffective. I would support cycle lanes within	Support noted. The route between Woodbridge and Rendlesham was not deemed to be a key corridor, however there is a leisure route between Melton to Orford which passes through Eyke and south of Rendlesham which can be accessed from Rendlesham through existing PROW routes. Furthermore suggestions along this route were made as part of the consultation comments which have been analysed and scored.

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				the village of Eyke and one or two more zebra crossings at both ends of the village			
SCLP12.50	David Adelson	442			Support noted.		
SCLP12.51	Herries, Rupert	57	No	<p>Please can you explain how on earth you expect the residents of Grundisburgh to take any of this seriously when you are in the process of approving 70 houses on Chapel Field which is surrounded by narrow unclassified lanes, one of which has a blind double bend with dangerous visibility?</p> <p>Planning Inspectorate appeal ref: APP/X3540/W/21/3280171</p> <table border="1" data-bbox="846 719 1384 788"> <tr> <td>Planning application reference:</td> <td>DC/20/3362/FUL</td> </tr> </table> <p>It has been pointed out to you repeatedly that passing places along Lower Road and many of the other surrounding lanes cannot be provided, and vehicles are already using residents' private driveways to make way for passing traffic.</p> <p>70 more houses on this site will inevitably increase traffic levels to an unsafe and unacceptable level.</p> <p>50% of the members of your own planning committee agree with this and yet you have used the chairman's vote to swing the decision against the people you are supposed to be representing. This is unforgivable and unethical.</p> <p>The chairman of your planning committee has</p>	Planning application reference:	DC/20/3362/FUL	The planning decision does not relate to the East Suffolk Cycling and Walking Strategy.
Planning application reference:	DC/20/3362/FUL						

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				<p>apologised to the people of Grundisburgh for the decision that was concluded last week.</p> <p>I would ask her what she is apologising for, and strongly express my rejection of her apology.</p> <p>If you continue to support the approval of this inappropriately large and located development by Hopkins Homes, you will be savagely misrepresenting our community.</p> <p>I would also stress that if you are refusing to listen to the considerations of the residents of our community, you should not be sending out emails of this kind, it is deeply insulting!</p>	
SCLP12.57	Andrew McDonald	569		<p>1 The footway should be within the site and behind the remaining hedgerow, to avoid suburbanising the entrance to Orford, with further damage beyond what will already be caused by this unnecessary development;</p> <p>2 The emphasis must be on ensuring that the rural character is retained, and the surfacing should be permeable/unsealed.</p> <p>3 Excellent recommendation, and there is ample opportunity within the village area, especially within the existing car park.</p>	The recommendation for a footway along the site frontage has been removed as the improvements to Footpath 3 should provide a traffic free cycling and walking connection into Orford whilst avoiding the potential harmful urbanising effect of hard infrastructure along the Ipswich Road site frontage.
SCLP12.57	David Adelson	447	Yes	Footpath 3 would make a good cycling and walking route, as long as it continues beyond the Orford parish boundary all the way to Chillesford.	Support noted. Footpath 3 extends to the Orford parish boundary. The Woodbridge-Snape Leisure Route recommends the creation of a cycling and walking route between Woodbridge, Orford and Snape, which follows Footpath 3 (Orford) and Footpaths to Chillesford.

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SCLP12.57	Janet Harber	153		<p>Re the upgrading of FP3 to surfaced bridleway:</p> <p>This Footpath currently crosses The Broom (Newton Broadway), which is itself a Footpath (not a Bridleway) and continues in a roughly westerly direction past Sudbourne Hall, crosses the Chillesford Lodge private road and comes out on the Orford Road just outside Chillesford. NB it is only FP3 as far as Sudbourne Hall after which it has other numbers.</p> <p>Is it proposed that this Footpath will be upgraded to Bridleway along all of its route? It might be more useful if the Footpath along The Broom track, which already has a hard surface, were also to be upgraded to Bridleway status.</p> <p>Before any hard surfacing is done it would be useful if local horse riders could be consulted because what is suited for cyclists is not always a suitable surface for horses.</p>	<p>Recommended improvements to Footpath 3 are set out to be delivered by the development of SCLP12.57. The extended footpath network beyond to Chillesford is included within the Woodbridge-Orford-Snape Leisure Route. Surfacing and lighting solutions must be sensitive to the needs of all users, including equestrian users, as well as the natural and historic context, which is often the very reason the route may be popular. The most appropriate surfacing and lighting solutions will need to be considered through the detailed design stages.</p>
SCLP12.58	Chris Taylor	367	Yes	<p>3) Otley Footpath 56 should also be upgraded to bridleway status, so as to give a complete bridleway from Chapel Road to Highhouse Road via existing bridleway 28 and also create a complete bridleway loop back on to Chapel road via bridleway 72.</p> <p>By creating Bridleways with an improved surface, this will create a significant 'car free' area for pedestrians, cyclists and horse riders to use, particularly the more inexperienced (inc children and the elderly) right in the middle of the village, at the</p>	<p>Recommendation 3 has been amended to align with the suggested infrastructure improvements.</p>

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				point where there will be a significant increase in vehicles and residents when the development happens. This should prove to be a positive benefit to the village, in some way countering the negative effects of the increased residential development.	
SCLP12.61	Andrew McDonald	570	Yes		Support noted.
SCLP12.62	Andrew McDonald	571	Yes	1 Strong safety implications and should be a priority 2 & 3 similarly	Support noted.
SCLP12.62	Chris Taylor	365	Yes	Any cycling and walking track along the A1152 to Rendlesham Mews should be extended as far as the Ash Road junction. So as to allow walkers and cyclists to access Ash Road directly. (Ash Road is to become a 'Quiet Lane' and an active travel route to Wickham Market & Campsey Ashe)	The site allocation recommendations have been amended to extend the proposed route along the A1152 to Ash Road.
SCLP12.62	David Adelson	448	Yes		Support noted.
SCLP12.64	Trinity College Cambridge (Sam Metson)	638	Yes	Trinity College Cambridge commends the efforts made by East Suffolk Council to enhance walking and cycling infrastructure provision across the District and looks forward to working with the Council to deliver shared aspirations in this regard where possible on land owned by the College, especially as part of emerging development projects. We would welcome direct engagement with the Council before the final Walking and Cycling Strategy is published to discuss and explore the opportunities across the College's land holdings in full detail. The College comments on the draft strategy for the	The Council recognises the challenges in retrofitting an application to meet the recommendations of the Strategy and is supportive of efforts made to incorporate the recommendations into the application. Recommendations 1, 2 and 3 of the draft Strategy have been incorporated into a single recommendation. A new recommendation has been added to introduce a cycling and walking connection between the Howlett Way crossing point and Ash Ground Close to enable cycling and walking between the site and area north of the site.

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				<p>Howlett Way Local Plan allocation site at this stage as follows:</p> <ul style="list-style-type: none"> • The College submitted a planning application for the development of this site in May 2020 (DC/20/1860/OUT), prepared to address the adopted and emerging policy requirements of the Felixstowe Area Action Plan (adopted January 2017) and the Suffolk Coastal Local Plan (adopted September 2020) at that time. The site is already constrained by the need to deliver as close as possible to the 360 dwellings and a new pre-school that the site is allocated for in the Local Plan, alongside demanding drainage attenuation and ecological mitigation requirements and the other policy expectations of the Local Plan. The challenge of 'retrofitting' the new requirements of the Walking and Cycling Strategy to the current application must be recognised by the Council. The College is nevertheless willing to try to do so where possible through a forthcoming update the application. • We believe that the delivery of a central north-south segregated cycleway and footway broadly aligned with recommended route F131 may be deliverable, and we are currently exploring this with our consultant team. Providing this route would have the significant benefit of providing a segregated cycle link 	

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				<p>between Howlett Way and the wider cycle network to the north and west, and Church Lane and the bridleway network further south - extending towards the North Felixstowe Garden Neighbourhood Local Plan allocation.</p> <ul style="list-style-type: none"> • Providing this link would negate the need for a segregated cycle link along the eastern boundary of the site, protecting the countryside character of the existing public footpath which passes the wooded embankment of the A14 slip road, partly on land that is not controlled by the College and which falls outside of the planning application boundary. We would highlight, however, that the new perimeter road running adjacent to the north-eastern boundary and the other roads and streets to be provided within the scheme will provide useful alternative cycle routes because they will not carry through traffic, and they will therefore be very lightly trafficked by vehicles. • Our planning application already includes a segregated 3m wide pedestrian and cycle route running along the majority of the Howlett Way frontage which links the enhanced 3m wide pedestrian and cycle crossing points on the Howlett Way/High Road roundabout junction with the new access junction into the scheme and the internal pedestrian, cycle and vehicular access network. This is shown in detail on 	

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				<p>drawings 67006-TA-002 and 004 submitted with the planning application (appended to the Transport Assessment, and attached for reference). These proposals have been subject to extensive discussion with the Highway Authority and are now agreed with them. They broadly equate to proposal F16 and F17 of the walking and cycling strategy.</p> <ul style="list-style-type: none"> • These proposals include a segregated pedestrian link adjacent to the eastern side of the Howlett Way frontage, connecting to an enhanced pedestrian crossing on the alignment of public footpath 26. A cycle link is not provided to this location because there is no cycling network further north or east and we are not aware of any proposals to address this in the foreseeable future. Again, this part of the application proposals has already been discussed and agreed by the Highway Authority. • The illustrative masterplan submitted with our planning application already provides other pedestrian connections through the site that are broadly in line with proposals F21, F23 and F130. <p>We believe that our proposals (with the forthcoming updates) will provide a significant enhancement to walking and cycling infrastructure available in this part of the Felixstowe peninsula and that they broadly accord with the emerging proposals of the Council's Walking and Cycling Strategy. We trust that the Council will be able to take these comments on</p>	

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				<p>board and amend the WCS's proposals for the site accordingly before the final document is published. If the Council would like to discuss any aspects of these comments before then, we would be happy to do so directly.</p> <p>Attachments: https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455236/PDF/-/11775413%201%20Access%20Drawing%2067006%2DTA%2D004%2Epdf https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455237/PDF/-/11775413%202%20Access%20Drawing%2067006%2DTA%2D003%2Epdf https://eastsuffolk.inconsult.uk/gf2.ti/af/1322978/455238/PDF/-/11775413%203%20Access%20Drawing%2067006%2DTA%2D005%2Epdf</p>	
SCLP12.65	Pigeon Investment Management Ltd (Fillmore, Andrew)	870		<p>1. – We generally support this recommendation. As per our response to F13, the east-west section of F13 could be accommodated within the Pigeon However, any improvements to the surface of the east-west section of F13 should be carried out by the Council as part of the wider improvements to Footpath 31 (F11).</p> <p>2. – These measures will be provided for by improvements to the existing footway along Howlett Way (which is proposed to be widened to a 3m shared foot/cycleway as part of the Howlett Way scheme) and the provision of a refuge island on High</p>	Recommendations 1, 2, and 3 in the draft Strategy have been incorporated into a single recommendation, and recommendation 4 has been retained in the Strategy.

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				<p>Road (that is proposed to be delivered as part of the Pigeon scheme).</p> <p>3. - The Pigeon scheme includes the provision of a series of off-site highway improvements that will be secured via planning condition. These works will include the provision of new cycle/footway infrastructure and improvements to the existing refuge islands at the roundabout that forms the junction between High Road and Howlett Way. These will provide safe and convenient cycling and walking crossing points over High Road.</p> <p>4. – As per our response to F14, the Pigeon scheme provides for a financial contribution towards improvements to the section of Gun Lane between the Pigeon scheme and High Road. Whilst we do not object to the Council’s proposals to improve Gun Lane southwest of 65, up to the railway bridge, these improvements are not directly related to the Pigeon scheme. Any benefits associated with improvement to Gun Lane (southwest of SCLP12.65) are largely dependent on improvements to the existing PROWs to the south of the railway line. As such, we would suggest that any improvements to Gun Lane (southwest of the Pigeon scheme) would need to be brought forward as part of a wider set of improvements, including those south of the railway line. As these wider improvements are not directly linked to site SCLP.65 they should either be funded via CIL contributions or other funding sources and should not be the subject of a request for a commuted sum as part of SCLP12.65.</p>	
SCLP12.68	David Adelson	453	Yes		Support noted.
WLP2.2	David Adelson	454	Yes		Support noted.

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WLP2.4	David George	52	Yes	Waveney Drive and Victoria Road could be an excellent link for safe cycling between the Kirkley side of Lowestoft and Oulton Broad, a great improvement. This would avoid in some cases having to cross the Bascule Bridge and navigate dangerous sections through the Station Square area and then along Denmark Road.	Support noted.
WLP2.7	Chris Adelson	557	Yes		Support noted.
WLP2.7	Cycling UK (John Thompson)	353	Yes	Dutch style roundabouts should be a priority.	Support noted.
WLP3.1	Ingleton Wood LLP and DLP Planning Ltd for and on behalf of Chenery's Farm Partnerships, Beccles Townlands Trust and Allison Homes	922		<p>1.0 Introduction</p> <p>1.1 These representations have been prepared by Ingleton Wood LLP and DLP Planning Ltd (DLP) on behalf of their respective clients, Chenery's Farm Partnerships and the Beccles Townlands Trust, and Allison Homes (formerly Larkfleet Homes) in response to the East Suffolk Draft Cycling and Walking Strategy. The consultation period commenced on 1st November 2021 and ends on 10th January 2022.</p> <p>1.2 This response is made jointly on behalf of the individual client interests in land comprising the Beccles and Worlingham Garden Neighbourhood. For clarity, the Chenery's Farm Partnerships and the Beccles Townlands Trust jointly represent the interests of the 'western parcel' and Allison Homes have an option on land known as the 'eastern parcel'.</p> <p>1.3 These representations respond specifically to the Draft Cycling and Walking Strategy (November 2021,</p>	<p>Recommendation 1 - The support for this proposal is noted.</p> <p>Recommendation 2 - The Strategy outlines key infrastructure improvements that benefit the allocation/policy and provide key sustainable connections. The provision of the extended cycling/walking track provides benefits to both allocations 3.1 and 3.3 by connecting a significant number of new residential properties to employment opportunities. The exact method of delivery would be determined at an application stage in consultation with Suffolk County Council.</p> <p>The Strategy does not add new policy requirements, but simply expands on existing policy requirements in respect of how they are expected to be delivered. The recommendations set out for this site are in accordance with the Waveney Local Plan, notable policy WLP8.21 that states that development will be supported where: 'It is well integrated into and enhances the existing cycle network including the safe design and layout of new routes and provision of covered, secure cycle parking'.</p>

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				<p>updated 8th December 2021). It is acknowledged that the consultation documents also comprise the Draft Strategic Environmental Assessment Screening Opinion (October 2021) and the Draft Habitats Regulations Assessment Screening Statement (October 2021), both relating only to the Draft Cycling and Walking Strategy. Our clients have no specific comment on either of these documents. However, we consider that the district-wide SEA and HRA that formed part of the Local Plan adoption should continue to be respected in full.</p> <p>1.4 These representations are made insofar as they relate to the development and delivery of the Beccles and Worlingham Garden Neighbourhood, which is an allocation that was adopted under Policy WLP3.1 of the Waveney Local Plan (March 2019). The delivery of the Garden Neighbourhood is subject to site specific requirements through the allocation policy, which were tested and examined through the evidence base that supported the production of the Local Plan.</p> <p>2.0 Representations</p> <p>Draft Cycling and Walking Strategy</p> <p>General Comments</p> <p>2.1 It is accepted by all parties that the Beccles and Worlingham Garden Neighbourhood will be required to provide adequate cycling and walking routes within the respective landowners’ site sufficient to</p>	<p>The Cycling and Walking Strategy provides recommendations in how this criterion could be met.</p> <p>Recommendation 3 - The Strategy outlines opportunities but the method of delivery would be determined at an application/design stage in consultation with SCC. Consideration to ensuring this route is as cycling/walking friendly as possible should be given when designing the site should it be bisected by roads.</p> <p>Recommendation 4 – This recommendation is a policy requirement.</p> <p>The Strategy recommendation has been altered to note Cedar Drive as the potential crossing point.</p> <p>Recommendation 5 – Response noted.</p> <p>Recommendation 6 - The Strategy outlines key infrastructure improvements that benefit the allocation/policy and provide key sustainable connections. The upgrading of the PROW routes will provide connectivity benefits, but the nature of the improvements and the method of delivery will be considered during a design/application stage.</p> <p>It should be noted that the Strategy does not add new policy requirements, but simply expands on existing policy requirements in respect of how they are expected to be delivered. The recommendations set out for this site are in accordance with the Waveney Local Plan, notable policy WLP8.21 that states that development</p>

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				<p>make delivery of the site acceptable in planning terms and in accordance with the adopted allocation Policy WLP3.1.</p> <p>2.2 Further to adoption, the need for this site to deliver a significant provision of Suitable Alternative (or Accessible) Natural Green Space (SANG) has arisen by virtue of updated guidance from Natural England. Due to the size of the site, Natural England will expect the provision of a circular walking route of 2.3-2.5km within the SANG and it is envisaged that this will be met within the site and also incorporate the Country Park requirement. This requirement has been factored into the masterplans currently being prepared by our clients but is not explicitly detailed in Policy WLP3.1, nor in the Draft Cycling and Walking Strategy. Our clients consider that the circular SANG/Country Park route will become a key strategic walking route of the allocation site and provide many of the suggested on-site links.</p> <p>2.3 The Beccles and Worlingham Garden Neighbourhood sits within the area identified as the 'Lowestoft to Bungay Key Corridor' and specifically contributes to the 'Beccles Route'. Within this area, LB16, LB17, LB19 and LB20 are identified as 'Very High Priority Routes' and are considered most relevant to the Garden Neighbourhood.</p> <p>2.4 In addition, the Draft Cycling and Walking Strategy identifies recommendations for existing site allocations, including for WLP3.1 – Beccles and</p>	<p>will be supported where: 'It is well integrated into and enhances the existing cycle network including the safe design and layout of new routes and provision of covered, secure cycle parking'. The Cycling and Walking Strategy provides recommendations in how this criterion could be met.</p>

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				<p>Worlingham Garden Neighbourhood. However, it is noted in Paragraph 3.50 that these recommendations should be understood as high level opportunities at this consultation stage and further at Paragraph 3.51 that the recommendations may be delivered via other funding and delivery opportunities in the area where they cannot be secured through a planning permission.</p> <p>2.5 These representations consider the relevant routes and site allocation recommendations, along with implementation considerations under the sub-headings below.</p> <p>WLP3.1 Site Allocation Recommendations</p> <p>2.15 The Draft Strategy outlines six specific recommendations for the Beccles and Worlingham Garden Neighbourhood allocation, which are considered individually as follows.</p> <p>1. The southern relief road cycling and walking track should be connected into the cycling and walking routes within the allocation and beyond into Beccles and Worlingham.</p> <p>2.16 Our clients agree with the principle of Recommendation 1 and have factored appropriate cycling and walking connections to the southern relief road into the masterplans that have been produced.</p> <p>2. The Benacre Road cycling and walking track should be continued east to the Benacre Road/Copland Way roundabout.</p>	

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				<p>2.17 We disagree with the inclusion of Recommendation 2 as a suggested item to be delivered through the Garden Neighbourhood allocation. The existing southern bypass walking and cycling track runs along the entirety of the southern site boundary of the Garden Neighbourhood. The extension suggested in Recommendation 2 is some way off-site and actually runs along the frontage of allocation site WLP3.3. Indeed, this is the first recommendation for WLP3.3 in the Draft Strategy. This requirement is not therefore considered directly related or relevant to WLP3.1.</p> <p>2.18 Furthermore, we do not consider this extension to be necessary to make the WLP3.1 allocation site acceptable in planning terms. There is no reference to this requirement in the site-specific criteria of Policy WLP3.1, nor any other site allocation policy in the adopted Local Plan or the Council’s Infrastructure Funding Statement (IFS), and thereby no policy requirement for it to be provided through the delivery of the Garden Neighbourhood. If this extension was considered a priority route for the District, it should have been considered and evidenced through the Local Plan and as part of the latest IFS.</p> <p>2.19 From a practical perspective, the provision of an extension to the footpath in this location will require the use of third-party land that is not in the control of our clients. It is not entirely clear whether there is sufficient land available within the Highway</p>	

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				<p>Authority's control to achieve a wide enough cycling and walking track from Church Road to the Copland Way roundabout.</p> <p>2.20 Additional works such as this are also an unknown in respect of their costs and if the Council choose that they are necessary they may compromise the delivery of the more essential scheme specific S106 related infrastructure required to mitigate the development at the delivery/early stage. If this is the case, there is likely to be a need for the relevant associated submissions to be supported by viability cases and reductions in S106 obligations.</p> <p>2.21 The Planning Practice Guidance (PPG) and Regulation 122 of The Community Infrastructure Regulations 2010 are clear that planning obligations must be necessary to make the development acceptable in planning terms, directly related to the development, and fairly and reasonably related in scale and kind to the development (Paragraph: 002 Reference ID: 23b-002-20190901). In the view of our clients, this recommendation would not meet the tests set out in the PPG or Regulation 112.</p> <p>3. A traffic free cycling and walking track connecting Oak Lane and the southern relief road should be introduced.</p> <p>2.22 As outlined at Paragraph 2.11, the track that continues south from Oak Lane is not within the ownership of any of our clients. We therefore expect that any improvements and upgrades to this track</p>	

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				<p>will be led by the Highways Authority.</p> <p>2.23 Our clients have confirmed that they have no intention of using the existing track referenced in Recommendation 3 as a vehicular access, nor the proposed circular route that will run closely parallel to this route within the eastern site boundary. The proposed circular route within the site will effectively provide a traffic free cycling and walking route from Oak Lane to the southern bypass, albeit not as direct as the existing track.</p> <p>2.24 However, there is a policy requirement to link the eastern and western parcels of the Garden Neighbourhood to achieve a comprehensive approach to the development. Accordingly, the existing track cannot be entirely traffic free as the spine road between the two parcels will need to cross this existing track. Linkage of the two sides of the site across Oak Lane is a critical requirement of the masterplan.</p> <p>4. A segregated cycling and walking track should be introduced along Ellough Road, as required by Policy WLP3.1, and connect into the existing cycling and walking infrastructure north of Rowan Way.</p> <p>2.25 As explained in Paragraph 2.6, the representatives for the eastern parcel agree with the principle of providing a link through the allocation site alongside Ellough Road in accordance with the allocation policy. However, the suggestion in Recommendation 4 that this track should connect into existing infrastructure north of Rowan Way</p>	

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				<p>conflicts with the recommendations for LB15 and LB16 which seek connections across to Cedar Drive. We expect that any such connection to Rowan Way would be via Cedar Drive using the offsite routes indicated under references LB15 and LB14. For the avoidance of doubt, our clients do not support a connection to Rowan Way directly from Ellough Road through this allocation, due in part to the potential conflicts with the root protection areas of existing boundary trees.</p> <p>5. As required by Policy WLP3.1, cycling and walking connections should be made between the allocation and Bluebell Way, Cucumber Lane, Darby Road, Nicholson Drive, Oak Lane, Field View Gardens/Foxglove Close and Cedar Drive.</p> <p>2.26 Our clients remain committed to providing a series of cycling and walking paths around the site with adequate connections, wherever lawfully achievable, to the existing surrounding street network. Due to the requirement for a circular route within the SANG, there will be a series of opportunities to connect the existing streets particularly along the northern site boundary. However, our clients can only provide tracks to the extent of their ownership boundaries. The final connection into the referenced streets will be dependent on the sites being contiguous.</p> <p>6. Footpath 18 should be upgraded to a bridleway, widened and resurfaced accordingly. Bridleways 12 and 15 should be widened and resurfaced and connect onto the southern relief road south of Marlborough Cottage. Footpath 3 should be</p>	

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				<p>upgraded to a bridleway, widened and resurfaced accordingly and connect the southern relief road to Bridleway 15.</p> <p>2.27 In accordance with Policy WLP3.1, our clients will respect the existing PROW network that runs within the allocation site boundary. These existing routes will be preserved and incorporated into the proposed series of additional footpaths within the SANG provision.</p> <p>2.28 Whilst we recognise and agree that some enhancement of the existing PROWs within the site is expected in accordance with Policy WLP3.1, the nature of the enhancements are not explicit in the allocation policy. Our clients consider that the priority regime for any upgrades should have full consideration to the ecological enhancements that will be proposed around the site to ensure a comprehensively planned network of paths across the Garden Neighbourhood. We therefore do not agree with the specific upgrades referenced in Recommendation 6 at this stage. We consider that the ability for the Local Planning Authority to secure necessary on-site upgrades should be reserved for the application stage when all material planning considerations can be taken into account. As such, the Strategy should not be as explicit in this recommendation.</p> <p>2.29 Furthermore, Recommendation 6 references Footpath 3, which is a PROW that runs largely offsite. There is no policy requirement to enhance</p>	

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				<p>any offsite PROWs and we do not consider this will be required to make the Garden Neighbourhood development acceptable.</p> <p>Implementation</p> <p>2.30 Alongside these representations, we have submitted a response on behalf of our clients to the Draft CIL Charging Schedule Consultation 2021. Within those representations, we expressed concerns in relation to the level of CIL being requested on the site. Whilst detailed costs are not available at this stage, we identified in those representations notable costs anticipated with the delivery of this scheme. The proposals suggested in this Draft Strategy have the potential to introduce additional unexpected costs that have not been accounted for by our clients, nor considered through the Local Plan viability appraisal or the latest IFS.</p> <p>2.31 Our clients consider that to ensure the scheme remains deliverable, there needs to be opportunity for flexibility in the mechanisms through which any anticipated requirements are secured.</p> <p>2.32 In this regard, we would expect most of the on-site footpath delivery to be delivered through the SANG requirement and secured via S106. As highlighted in our CIL representation, our clients' preference is for as much onsite infrastructure as reasonably required to deliver the scheme to be secured under S106 which guarantees any enhancements and benefits will be local.</p>	

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				<p>2.33 Similarly, there is the expectation that some identified works would be absorbed under S278. However, the S278 costs identified at present are only considered insofar as they are presented in the CIL consultation funding.</p> <p>2.34 We expect that any offsite works not directly related to the Garden Neighbourhood and outside of our clients' landownership which the Council envisages through the Cycling and Walking Strategy would be secured via District-wide CIL funding.</p> <p>3.0 Conclusions</p> <p>3.1 Our clients agree that a series of on-site cycling and walking paths should be provided within the Beccles and Worlingham Garden Neighbourhood in accordance with the adopted requirements of Policy WLP3.1 of the Waveney Local Plan and shown indicatively in Route LB20 of the Draft Cycling and Walking Strategy.</p> <p>3.2 Overall, our clients intend on delivering a development that provides significant walking and cycling provision, most notably through the new Natural England requirement to provide a 2.3km-2.5km circular route within the SANG that will be secured on the site. The SANG principles will achieve attractive, natural circulation routes within and around the site.</p> <p>3.3 It is our clients' view that the paths within the</p>	

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				<p>site and subsequent connections and from the site need to have full consideration of the proposed SANG arrangements and Country Park delivery. In this regard, we consider it would be unreasonable and premature for the Cycling and Walking Strategy to make specific recommendations outside of Policy WLP3.1 for the on-site provision without first considering the SANG requirements. Accordingly, the Strategy should allow flexibility for all relevant material planning considerations to be considered at application stage.</p> <p>3.4 It is also considered that the Draft Cycling and Walking Strategy introduces new proposed requirements relevant to the delivery of the Beccles and Worlingham Garden Neighbourhood including the Benacre Road cycling and walking track extension and significant enhancements to existing PROWs both on and off-site. These proposals were not fully considered at Local Plan preparation stage. Accordingly, they have not formed part of the Plan-level viability appraisal and do not accord with adopted Policy WLP3.1 of the Waveney Local Plan. Our clients do not support these additional recommendations being delivered through the Beccles and Worlingham Garden Neighbourhood.</p>	
WLP4.1	Halesworth Neighbourhood Plan Steering Group (Joyce Moseley)	131	Yes	<p>No 2. Excellent suggestion. Will this be the whole of Quay Street to the roundabout with Norwich Road? Will it mean no parking on Quay Street? With such little on street parking this might be of concern to some but it would make that stretch of road much safer. Could 20 mph be introduced?</p>	<p>2 - The proposal intends to connect Loan Pit Lane to the roundabout at Norwich Road to ensure that Healthy Neighbourhood is connected into the town centre. Parking should be retained where possible as a number of properties lack driveways.</p> <p>7. Support noted.</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>No 7. - good addition. see below</p> <p>No 8. Please note the owners of Footpath 21 have been contacted by the Neighbourhood Plan Group and have indicated a willingness to transfer ownership to the Millennium Green Trust. Discussions are taking place as to how best to do this. The Trustees are keen to create a cycle way down footpath 21 and then to take a route across their land to join up with footpath 11, 10 and 9. I cannot speak for them but the route shown to us does not go along footpath 22. The trustees (and walkers!) are keen for that to remain a pleasant country walk along side the old railway workings. This route connecting to 9 and 10 gives an alternative route into Halesworth Town Centre from Holton and onto the national cycle route running through the Green. The White Bridge would need to be maintained to cycle route standards.</p>	8. The additional information is noted.
WLP4.1	Jamie MacDonald	217		<p><u>1.</u> I welcome the suggestion of upgrading Halesworth FP7 to bridleway status which, in essence, would extend Loam Pit Lane through to Harrisons Lane.</p> <p>While I would also welcome the upgrading of Halesworth FP16 and Holton FP7, FP8, and FP9 to bridleway status, I would be far less inclined to do so if that meant they would be resurfaced and altered to any significant degree. This especially for Holton FP8 (known as 'Lover's Lane'), whose charm and pleasant character as a double-hedged ancient lane would be lost if any significant alterations were made to it. It may have formed, originally, as an occasional watercourse, and to this day can get a</p>	<p>1 - The support for the PROW upgrades are noted. It is agreed that all improvements in terms of width and surfacing should be mindful of the character of the area and no hard surfaces introduced if they would be inappropriate. The strategy will not recommend the loss of any established hedgerows due to their character and biodiversity value.</p> <p>The suggested improvements to footpath 6 has merits and has been included in the Strategy.</p> <p>4-5 - Support for this recommendation is noted.</p> <p>6 - Support noted.</p>

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				<p>bit 'stodgy' underfoot. So if it were to be surfaced, this may well cause an issue on Bungay Road. There has also been a suggestion in the plans for the proposed housing development along Harrisons Lane for this old lane to be link to the development, which again could also harm its character.</p> <p>I can see upgrading Holton FP7 and FP8 would make for a more useful link to Orchard Valley. However, a simpler alternative would be to upgrade Holton FP6 from Orchard Valley and to link to the new housing development off Hill Farm Road (as I understand it, this link is in the plans for that development). Linking the development to Holton Road as well, would make for a very useful link (especially as the development is set to link to Loam Pit Lane, north of the cemetery).</p> <p>4. and 5. If by 'shared cycle/footway' you mean the section linking Bungay Road, Holton (at the north end of Holton FP8) and Harrisons Lane, then that would be in preference to using Holton FP8 – in my opinion. Extending this along the remainder of Harrisons Lane, joining Halesworth FP7 as it does so, would also seem to make sense. Extending it in the other direction, along Bungay Road to Holton village, should also be explored. Although this road is a little on the narrow side, already.</p> <p>6. This link is vital in linking the new Hill Farm Road development with Loam Pit Lane, in the same way as the link on the opposite corner of the site linking to Holton FP6 – both forming a part of the plans for the</p>	<p>7 - Support noted.</p> <p>8 - It is agreed that the exit of footpath 11 onto Holton Road is problematic and addition of a recommendation improving this crossing point will be added. If this cannot be achieved then improvements to footpath 11 can be removed.</p> <p>The issues raised with footpath 9 is understood, but its improvement is deemed to still have value in providing connections to the town centre.</p> <p>The issues relating to footpath 10 are also understood and with the off-road cycle route running parallel means this is less of a priority.</p>

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				<p>development.</p> <p>7. Yes, a crossing point of some kind here would be crucial to any upgrade to Halesworth FP21, as exiting this path is currently a little dangerous – especially as the concreted entrance slopes quite steeply into the road at present.</p> <p>8. Similarly, the exit from Halesworth FP11 on to Holton Road is dangerous, even more so than for FP21 (above). It's difficult to see how any improvements could be made here, and is something that would need very careful consideration. But a cycle/bridleway link between Holton Road, via the Folly, to join the Millennium Green cycleway and the Town Park should be considered a priority, as it would make for a very useful link for many. However, it is unclear how this could be achieved. With its dangerous exit on to Holton Road (as above), then to my mind FP11 should be ruled out – even though it would otherwise be the simplest. FP22 brings its own problems, one being the Southwold Railway Trust's workings and future plans in this area. In case you are unaware, a little to the east from the exit of FP21 there is another well used path that leads down from Holton Road, and then westward along the old route of the Southwold Railway, joining FP21 and FP22 where they meet. It is not on the Definitive Map, but has been used for many decades. However, it is unclear to me whether this path (for its entirety) is a permissive path or one that was 'dedicated' by one of the landowners. I understand</p>	

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				<p>that the Millennium Green Trust will have this information. If not, then it seems to me a likely candidate for an application to modify the Definitive Map based on long user evidence. It certainly needs to be explored as an alternative, as its entrance/exit on to Holton Road is far safer than the two other options. (Let me know if you want me to investigate this further). As for Halesworth FP9 and FP10, then these would appear to be unlikely candidates for any upgrades. For FP9, given the Millennium Green cycleway already passes along this route and through the Town Park, it would seem unnecessary to alter the towpath along the New Reach (as long as the south end of FP11 joins the said cycleway after crossing the bridge). Even though (rightly or wrongly) it is on the Definitive Map, FP10 does not appear to be in usage today – and it would seem to be a problem area.</p> <p><u>9.</u> See comments for '1' and '6', above.</p>	
WLP4.2	Halesworth Neighbourhood Plan Steering Group (Joyce Moseley)	135	No	<p>The cycling and walking infrastructure should go into the new estate as well as around it. It needs to connect into Allington and then into Dukes Drive and the bus stop</p> <p>Please see proposals in the NP that suggests a new footpath running from this development around the backs of the houses in Dukes Drive to Walpole Road. There are no walking routes in the south of the town and this would create a pleasing connection between the edge of the present settlement and the countryside.</p>	The response provides recommendations that appear to offer significant benefits both to the site and wider town. Connections within the site are important and the strategy will be updated to reflect this.

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				A crossing over Roman Way to connect to the cycle route in Holmere Drive is welcome.	
WLP4.3	Jamie MacDonald	218		Yes, I wholeheartedly welcome any 'future proofing' of cycling and walking infrastructure.	Support noted.
WLP4.5	Halesworth Neighbourhood Plan Steering Group (Joyce Moseley)	133	Yes	<p>Introducing a cycle/walking track along the whole of Saxons Way was a key proposal by the NPSG. It should be on the eastern side of the road not on the side of site WLP4.5.</p> <p>If footpath 3 is to become a bridleway then should the crossing over Saxons Way be a Toucan rather than a zebra although the latter would be welcome.</p> <p>Mention is made of routing Footpath 3 through the White Swan car park! This is now a pub garden where considerable investment has been made by the new publicans.</p> <p>The area of the town centring on Swan Lane is one of the focus areas for the We Made That report commissioned by the town council to look at connectivity in the town. halesworthtowncouncil.or.uk/town-centre-connectivity-report/</p> <p>Footpath 9 is unregistered so contact with owners to discuss upgrade to bridleway has not been possible</p>	<p>The support for the improvements on Saxon's Way is noted and it is important that, where possible, neighbourhood and East Suffolk strategies align. The exact position may be dependant on highway factors or funding opportunities however reference to the opposite side of Saxon's Way can be made.</p> <p>This is noted and the strategy will be updated.</p> <p>The re-routing of the footpath was pending on whether sufficient width could be achieved on the existing footpath with the knowledge that there is less potential to widen the path adjacent the pub. However should the re-routing adversely impact a vibrant local public house this should be avoided so reference can be removed.</p> <p>The remaining comments have been noted.</p>
WLP4.5	Jamie MacDonald	219		<p>1. Yes - there is ample width along Saxons Way to add a dedicated cycleway, so should be explored further.</p> <p>2. A high priority, and has been so since the Relief Road was constructed.</p>	<p>1 - This comment of support has been noted</p> <p>2 - This comment of support has been noted</p> <p>3 - This comment of support has been noted, the reference to re-routing the pathway through the pub</p>

Document Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p><u>3.</u> Yes - 'Swan Lane', as its name suggests, once had a highway status higher than the status of 'footpath' it has today. So by some means or other, these higher rights (at least to bridleway status) should be 'reclaimed'. There being ample width, diverting the route through the car park of the 'The Swan' would seem very unnecessary.</p> <p><u>5.</u> Yes – as stated, this would help in linking up the wider 'proposed' cycle network by linking Saxons Way with the Millennium Green cycleway and to Holton Road. Higher highway rights, for what remains of 'River Lane', may subsist from the time before the Relief Road was constructed?</p>	<p>car park will be removed.</p> <p>5 - The definitive map shows that river lane is (partially) a footpath, but this point will be explored further with the PROW team.</p>
WLP4.6	David George	51	Yes	<p>This is a key one for Halesworth connecting north to nearby villages and supporting existing Sustrans route NCN1. It would help avoid busy sections of Sparrowhawk Way if cyclists could join it from the Norwich Road side of Halesworth rather than having to navigate through Holton Village and up Lodge Road, which comes out near the busy Bernard Matthews site entrances, and shares the route with large HGVs.</p>	<p>Support noted.</p>
WLP4.6	Halesworth Neighbourhood Plan Steering Group (Joyce Moseley)	134	Yes	<p>is the intention to try and get a segregated cycle and walking track along the whole of Norwich Road from Quay Street roundabout up to the Sparrowhawk roundabout? If so excellent. The NPSG would have preferred the western side of the road but more important is a continuous route without the present gaps</p> <p>There also needs to be cycle and pedestrian</p>	<p>While a segregated cycling and walking track along Norwich Road between the site and the Quay Street roundabout would no doubt be of great value, it cannot reasonably be expected to be delivered through the site allocation. For this reason, the recommendation seeks to introduce cycling and walking infrastructure between the site and the existing cycling and walking infrastructure south of Zemke Way. Cycling and walking crossing points of Norwich Road side streets will need to</p>

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				priorities at all the junctions up Norwich Road (Copenhagen crossings?)	be designed to convey the priority of users as set out in the Highway Code, which could involve the use of Copenhagen crossings.
WLP5.2	David George	53	Yes	Not related to the two Bungay proposals, but I'm really surprised that no suggestions are made for the current difficult cycle path section against traffic up Bridge Street towards the central Buttercross roundabout? As a rider you have a medium climb then have to give way at the top to a very busy and blind roundabout. This makes it particularly difficult from a standing start getting onto the roundabout. This is a particularly dangerous section and roundabout for cyclists to use.	As noted the suggestion doesn't adequately relate to the allocations to include in this section. However it should be noted that a community recommendation 336 was submitted and assessed. The community recommendations (depending on their score and viability) form part of the strategy.
WLP6.1	Reydon Parish Council (John Roger Cracknell)	188	Yes	Very supportive of these proposals.	Support noted.
3.53 - 3.55	Andy Bird	122	Yes	<p>On missing items</p> <p>Additional PROW upgrades can happen now - ie. Ipswich gardens Westerfield</p> <p>Missing cycle destinations:</p> <ul style="list-style-type: none"> • Twisted Oaks - bike park required safe cycle route - upgrade footpath that is already used • Rendlesham - road to rendlesham very fast and dangerous - either provide cycle path or make clear route I can provide using bridleways. 	<p>The site allocation recommendations for policies SCLP12.62 and SCLP12.40 seek a segregated cycling and walking track along the A1152 between the allocations and Ash Road.</p> <p>A number of Leisure Routes have been added to the Strategy, including between Woodbridge and Tunstall Forest, Nacton Beach and Ipswich.</p>

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				<ul style="list-style-type: none"> Tunstall - requires off-road routes for Viking Trail users from Woodbridge area. I can provide. <p>Missing Tourist destination leisure routes opportunities:</p> <ul style="list-style-type: none"> Cycle path along woodbridge river, Martlesham, Woodbridge, Melton. Could also be a safe route from Martlesham. Trimley Marshes circular route/with Felixstowe Nacton beach and paths from Ipswich Stour and Orwell walk from Trimley to Levington footpath - can be bridleway 	
3.53 - 3.55	East Suffolk (Rachel Smith-lyte)	775		<p>Quiet Lanes (Comm Rec – 3.55)</p> <p>Cycling and walking on our country lanes can be made safer by speed limits on the designated Quiet Lanes and simple measures such as repairing passing places on single track roads many of which are severely pot-holed. If we need to work with County Highways more closely to achieve this then lets do it!</p> <p>Summary</p> <p>I want to see much more ambition around safe walking and cycling and where traffic separation may not be possible, at the very least we should be reducing rural speed limits to 40 mph maximum between villages and towns. There is also way too much road kill and horse riders as well as cyclists and</p>	<p>Segregation from traffic should be sought wherever possible, but as you note this is not always achievable. In such cases options like improved traffic management features and modal filters have been suggested.</p> <p>Reducing speed limits is also a method that can be considered, but it would need to be assessed on a case-by-case basis by Suffolk County Council.</p>

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				pedestrians take their lives in their hands every time they venture out.	
3.53 - 3.55	Lesley Vince	350			Comment noted.
3.53 - 3.55	Marlesford Parish Council (Richard Cooper)	907		<p>3. Comments under Recommendations</p> <p>We support the creation of corridors which provide longer connections between and within urban areas and the creation of imaginative recreational routes such as that proposed for the Yoxford/Aldeburgh/Benhall areas. Provision of active travel infrastructure is welcomed, but we ask ESC to ensure that rural areas do not “miss out” when funding is allocated. We particularly urge the Council to consider the safety requirements for pedestrians and cyclists in rural areas and recommend the Marlesford to Wickham Market improvements as part of an effort to address safety issues.</p> <p>Attachments: <i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	Support noted. Post adoption it is intended that a priority table will be completed in conjunction with Suffolk County to ensure funding efforts are prioritised correctly.
3.53 - 3.55	Ufford Parish Council (Judi Hallett)	734		<p>Section 3.53 onwards - Community Recommendations</p> <p>These aim to link existing infrastructure, for example, where there are poor cyclist provision or where there are missing or abandoned/ difficult to access footpaths. Ufford comments on the need to improve infrastructure (from highways/ footpath upgrades to simple regular clearing of footpaths alongside the public highway) in the initial consultation do not appear to found favour. Looking</p>	The Strategy focuses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure, as new infrastructure can be delivered through the planning system while maintenance of existing infrastructure cannot.

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				<p>at the specific comments made, several of the Ufford suggestions are deemed to be “SCC highways maintenance issues” and have been discounted.</p> <p>This is disappointing given the use of these roads for leisure cycling, which has seen a significant increase during recent times. In addition, East Suffolk Council’s decision to “rewild” grass verges as well as to stop regular cutting and maintenance of footpaths goes against the intention to encourage walkers.</p>	
3.56 - 3.60	James Winterbotham	502		<p>I find the document very difficult to navigate. Why not bunch comments on specific locations together to get a better feel for the level of comment/need? This might show that the Reydon triangle (between the Blyth Estuary, the A12, Wrentham and Southwold) scores highly on overall added points yet the outcome of the process seems to have almost no proposals for this area. Equally other places might emerge as key areas of needed action.</p> <p>I question the balance between "modal shift" and biodiversity - particularly outside the towns and villages the roads are narrow, which dis-incentivises walking or cycling. But the suggestion that hedgerows are removed to provide pedestrian space is obviously negative for biodiversity. These often negate each other points wise.</p> <p>I was hoping for a more ambitious strategy that might set broader goals in terms of engaging landowners to make field edges more accessible. There are lots of reasons for them not to (encouragement to trespass, land use, legal</p>	<p>We have attempted to make the Strategy as easy as possible to use where possible. To look at all recommendations within a specific geographic area the overall recommendation map can be used.</p> <p>The biodiversity criteria can result in negative scores which will then be balanced against positive scores in other criteria. However, the addition of Biodiversity to the MCAF scoring system reflects the importance of retaining key biodiversity assets.</p> <p>Post adoption it is intended that a priority table will be completed in conjunction with Suffolk County to ensure funding efforts are prioritised correctly.</p>

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				liabilities) but an initiative from the top down might help create a different and less adversarial climate for such a debate. Landowner benefit when planning permission is granted for development(plenty of examples) but the "planning gain" is often limited to the specific development rather than the broader area.	
3.56 - 3.60	Margaret Shaw	851		<p>MCAF- Biodiversity</p> <p>Looking at the biodiversity assessments it would appear that the recognised endangered species and habitat have been considered but what about other impacts which are not bound by legal protection to the same extent as the silver studied blue butterfly?Some weight should be given to these and mitigation action required to lessen impact or replace habitat.</p> <p>1. There is no mention of the impact of lighting on insect, bird and mammal populations.</p> <p>Current research is suggesting a significant impact. I am aware of the following references</p> <p>"Street lighting has detrimental impacts on local insect populations".</p> <p>Douglas Boyes. Science Advances. Vol 7 No 35 "LED lighting increases the ecological impact of light pollution irrespective of color temperature" Pawson and Bader Ecological Soc. of America Vol 32 Iss.1.</p> <p>I understand that Norfolk and Devon County</p>	<p>The addition of Biodiversity to the MCAF scoring system reflects the importance of retaining key biodiversity assets.</p> <p>The MCAF is an initial assessment only and each proposal should they be advanced to a design stage would require a more detailed appraisal of the local, natural conditions.</p> <p>The most appropriate lighting solutions will need to be considered as the recommendation are taken forward.</p> <p>The importance of avoiding harm to our natural environment is recognised. For this reason, IM12 has been amended to remove the proposed cycling and walking track through Birch Woods and a cycling and walking bridge over the A12. In its place, IM12 now recommends improving cycling and walking provision along Eagle Way. Moreover, a new recommendation (IM32) has been introduced to provide a cycling and walking route between Kesgrave and Brightwell Lakes, passing to the south of Martlesham Heath.</p>

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				<p>Councils have used this work to change their street lighting-- has there been work on the impact of lighting on cycle paths on insects and birds when assessing the impact of lit cycle ways on insects and birds?</p> <p>2. The nightingales and night jars which inhabit the woods between Coopers Road and Lancaster Drive and through Martlesham Woods are on the red list of endangered species according to the British Ornithological Trust. This area of woodland is part of a corridor of woodland which stretches as far as Rushmere Heath . I think this woodland is more vulnerable to loss of biodiversity than an alternative cycleway area along the edge of the field at the back of the existing housing development and then skirting the SS1 to link up crossing dobbs lane.</p> <p>2. East Suffolk Strategic Plan aims to</p> <p>"mitigate human impact on the environment and reduce contributions to climate change by conserving natural resources.</p> <p>Not only would the proposed cycle way cause the removal of 25 mature</p> <p>trees-- which would contribute the carbon capture created by increased traffic flow but also the loss of the next generation of trees which are already growing in the woodland.</p> <p>MCAf assessments for biodiversity should include</p>	

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				some values for these issues and plans identify measures which must be taken to mitigate the these impacts.	
3.56 - 3.60	Trimley St Mary Parish Council (Debra Cooper)	1080	Yes	<p>Connectivity and Growth: This category does not discriminate on the quality of existing infrastructure and by doing so this adds an unfair weighting to new infrastructure developments ahead of much-needed improvement to existing (insufficient) infrastructure. It's often the case the infrastructure exists but is of such poor provision that it is unused, thereby rendering locations not well connected.</p> <p>Modal Shift: This category needs to consider the variety of user needs and demonstrate this in the scoring. When analysing school travel, unfortunately, the data within the Propensity to Cycle map is from 2011. This is before the High School moved to it's current location and may contain details on journeys to both Orwell and Deben High Schools. Up to date analysis of children cycling to school needs to be undertaken.</p> <p>Optimisation: Related community comments should be considered collectively to perhaps develop a workable solution that could cover multiple points raised. Individually, scores for suggested points solutions may not be high, but cumulatively as part of a wider solution, they might.</p> <p>Safety: This scoring seems quite subjective?</p> <p>Biodiversity: Whilst this should be encouraged, this category cannot be relevant to all community</p>	<p>The Connectivity and Growth assesses the potential connectivity uplift provided by the improvement factoring in the services and population being connected. The category optimisation assesses the extent to which the existing infrastructure provision is optimised.</p> <p>It is understood that the data for PCT uses the 2011 census, but PCT provides a consistent and objective methodology to applied across the council area.</p> <p>The community comments were used as an important evidence base in the creation of the key corridors where areas of interest and concern could be appropriately linked.</p> <p>The scoring methodology has attempted to make each category as objective as possible, but planning judgement has been used where appropriate. The roads speed limit, its width and use were considered.</p> <p>This category considers where planting can be made available, but at this stage this information is largely unknown. Accordingly the category largely relates to the loss of biodiversity meaning projects where the loss of biodiversity assets are high inversely scores poorly in this category. In some cases such as built up areas there may not be a biodiversity loss so a neutral score is provided.</p>

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				<p>comments. Again this weights in favour of new infrastructure developments that have more scope for additional planting because it is green field and less constrained. Also, does this category deserve the same weighting as Safety, for example?</p> <p>Leisure: Again, this cannot be relevant to all community comments. It also provides a weighting towards tourist attractions over some of the more functional routes. This should be counter-balanced by a 'functional enhancement' category that values traffic/congestion on core routes.</p>	<p>It is agreed that the leisure category doesn't apply equally to every recommendation, but it was included to recognise the importance of leisure and the visiting economy to East Suffolk. It should be noted that this category doesn't proportionally favour leisure opportunities as the category 'Connectivity and Growth' focuses more in everyday and commuter trips.</p>

Appendix 5 Formal Consultation Responses - Community Recommendations

Community Recommendation Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
8	Cycling UK (John Thompson)	40	Yes	I disagree with your scoring summary. Although it might not achieve significant modal shift, it does put those using disability tricycles at an unfair disadvantage and it is arguably discriminatory to say they are other routes, albeit slightly longer. They are also difficult for those with trailer and perhaps loaded panniers wanting to do shopping by bike, which should be encouraged.	The scoring for modal shift has been taken from the Propensity to Cycle Tool which showed limited potential growth in this area. Whilst the point is recognised that the barriers removal will encourage use of larger cycling vehicles it would not be a significant detour Bodian Way nor would the numbers encouraged likely to be high.
20, 234, 462	Mike Sherwen	97	Yes		Support noted.
22	George Redpath	4	Yes	<p>ESC have shared this improvement with SCC as the Highways Authority which is just walking away from the problem.</p> <p>I have it writing from SCC and the Highways Authority after pursuing this from 18 months, they will only be replacing white lines for cycle lanes when existing road surfaces are replaced, therefore no cycling lanes with white lines which are degraded to being non existent i.e. Marine Parade etc. will be under the auspice of planned maintenance. At a stroke this means that +/-80% of the ESC cycling strategy routes which are classed as On-Road signed cycle routes on the latest ESC cycling map no longer effectively exist as they've disappeared into the tarmac because SCC will not maintain?</p> <p>If ESC are not going to fight there corner with SCC as the highways authority this whole process is flawed, is compromised and means ESC is effectively devolving all responsibility for 80% of</p>	Maintenance issues fall outside the scope of the strategy and the control of East Suffolk Council. The land is control by Suffolk County Council and will be subject to their own maintenance policies.

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				our on-road cycle routes to SCC, therefore nothing will change?	
23	George Redpath	3	Yes	<p>Under Optimisation, I disagree 'the pedestrian aspect is unlikely to be significantly improved'? This path is extremely popular with pedestrians and cyclists alike and should be to a shared path standard. This could be the optimisation of a key corridor which is already a pseudo cycling route on an existing so on a cliff top path and will offer huge improvements to both pedestrians and cyclists. The Optimisation score should be min 1.</p> <p>Under Safety: It is stated 'no significant safety benefit', which of course is just nonsense and the criteria flawed if safety is only concerned with cyclists interacting with vehicular traffic? There is daily conflict on this path with cyclists and pedestrians due to its popularity as a coastal cliff top route from Pakefield into the town centre used by locals, tourists from the various caravan parks in Pakefield and cyclists. This should be changed to a shared cycle and pedestrian path and widened to suit.</p> <p>Biodiveristy is scored as -1. which if anyone had actually visited this site you would see the biodiveristy aspect consists of a muddy bank that would be cut back to widen the path and is not over a significant area as stated in comparison to the total grassed area available and probably around 1%.</p>	<p>Optimisation - The route is of a reasonable quality for walking already. Whilst a new surface would provide a modest benefit it is not significant to warrant a score under optimisation for walking purposes.</p> <p>Safety - The criteria relates predominantly to conflict between pedestrians and/or cyclist conflict with vehicles. Cyclists using routes not designated for their use is an enforcement matter.</p> <p>Biodiversity - A site visit has been undertaken. Whilst much of the route is flanked by managed grass there are narrower sections with denser growth. This is deemed to represent a small loss as the path will likely require widening to meet the LTN1/20 standard across the length of the route.</p>
24.	John Clark	15	Yes	While I agree that pedestrian safety on sections of roads without footpaths is a strateguc issue, the	Comment noted. All of the recommended improvements included in the Strategy are

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				<p>need to walk from Walpole to Halesworth in safety is an important local matter, and I attach a map with photos (from Google Maps) to show where I think there is a considerable safety risk. I suggest that someone walks along here sometime soon !</p> <p><i>The attachment has not been published due to potential copyright/data protection concerns, but was still fully considered and assessed in forming the Strategy.</i></p>	<p>done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
31	Cycling UK (John Thompson)	48	No	<p>The alternative runs more or less parallel with the A47 and, I suspect, would get EXTREMELY limited use. Money would be better spent on something else.</p>	<p>Providing an off-road cycle/walking route between Lowestoft and Hopton is a key ambition of the strategy. In accordance with the key corridors section ideally this would be between the A47 and Coast Road utilising the North of Lowestoft Garden Village. It is recognised that a path alongside the A47 is less valuable due to the less attractive environment so isn't the leading priority, but it does still have value.</p>
36	Cycling UK (John Thompson)	37	Yes	<p>Support in principle but, as the response states, there could be practical difficulties widening the path alongside the A12 so the 'beach' idea might well be the better option.</p>	<p>Support noted and improvements here are being further explored in the key corridors section.</p>
36	George Redpath	10	Yes	<p>I struggle with a score of Zero for Leisure when this route is directly outside the popular PONTINS Pakefield holiday village which is very busy throughout the year. I agree it is an unattractive route but this does not negate the fact it is a key route between Pakefield and Kessingland.</p>	<p>The comment is noted and it is agreed that the score for leisure should be raised reflecting that there are a number of holiday camps on this road.</p> <p>It should be noted that Suffolk County</p>

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				<p>A coastal path between Kessingland to Lowestoft is not going to happen in my lifetime because of the lack of a can-do culture in local government and the seemingly completely different agendas that exist between SCC & ESC, therefore makes it even more important from a Leisure perspective that the score is increased from 0 to 2 giving a total score of 10.</p>	<p>Council sit on the steering group and have had strong involvement so far,</p>
39	Benhall and Sternfield Parish Council (Melanie Thurston)	144		<p>We agree with all the recommendations relating to, and adjacent to our Parish, but would like to add the following: The map shows that in the past, three separate comments have been made re the footpath behind the hedge alongside South Entrance - Nos 39, 411 and 422. These all have been scored at 6, which includes minus 3 for loss of biodiversity resulting from removal of the hedge. At our last Parish Council Meeting, the Saxmundham environment consultant was present. When asked whether she favoured removal of the hedge to widen the roadside path, or whether she preferred to retain the hedge and widen the footpath behind, She chose the later. Benhall & Sternfield Parish Council agree with this. Therefore we wish to challenge the minus 3, and request that the score be amended to 9. This would have the effect of elevating this path in the priority listing.</p>	<p>Comment noted. This proposal has been identified as part of the Leisure Route recommendations. See recommendation LR8.1 in the strategy.</p>
41b	Paul Jordan	2	Yes	<p>I feel that the allocation of 0 for safety on this evaluation is incorrect and unjustified. There are reasonable cycle facilities either both east and west of this link alongside the A1214 at Rushmere but at this point cyclists must choose to either</p>	<p>Agreed. It is unclear whether the route south of the road is available for cyclists so it is assumed any improvement will take cyclists off of the road. A score of 2 has been provided.</p>

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				<p>illegally use the narrow footpath on the south side of the road or join the main traffic flow with no segregation. This makes this link a significantly less safe than the routes either side.</p> <p>I suspect that changing the safety rating and hence the modal shift rating may result in upgrading the resultant proposed works from High to Very High.</p>	
50	Jamie MacDonald	28	Yes	A vital link – as per my initial suggestion.	Support noted.
51	Tom Daly	99	Yes	<p>This would be a major new route, providing options for cyclists and walkers in this busy tourist area. A major advance for health and safety.</p> <p>Please think beyond having to put the cycle route directly beside the road and ripping through the existing trees and herbage. Place it behind, even if this means an arrangement with the farmer(s). the benefits are not just biodiversity there are benefits in landscape, amenity and rural experience. This would make it a new green route which would attract users and create a pleasant experience separated from the busy road.</p> <p>Also if the cycleway is not distinct and separated from a busy road the tractors and HGVs, particularly with SZC etc, will be wondering into the cycleway destroying edges etc.</p>	Comment noted. The score provided gives a value to the public comment, but design and implementation will be considered further as the project progresses. Further consideration will be given to potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
104	Christopher Makey	30	No	It is unfortunate that the document setting out the plan has been poorly drafted and poorly thought out using language that clearly shows a love of managementspeak that does very little to enhance what should be a thoroughly commendable action plan for the future. I will assume the following:	The on-line document has designed to be an innovative and easy to use approach for the public. We have attempted to use plain English where possible and further revisions will be undertaken to make it easier to understand.

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				<p>Under Part 2 'Policy Content' the intention is to provide safer access to both cyclists and pedestrians and that, as stated, "cycling and walking being put at the heart of the transport decision."</p> <p>Under 2.9: Suffolk Local Cycling and Walking infrastructure Plan it was stated that the intention is to encourage people to walk and cycle. The assumption being that routes would be made safe for such activities and that the wishes of those walking and cycling would be taken into account. For reasons set out below in respect of just one street namely Northgate in Beccles (see Plan 104) the wishes of those not using vehicles and the property owners of Northgate have been rejected. The assumption is that this failure is not limited to one specific Street but is endemic in the proposals (or lack of them) throughout the plan.</p> <p>Under 'Benefits of Cycling and Walking' (page 6 of the document) it is stated that an increase in cycling and walking will "Reduce road danger and noise" and will enable people "to enjoy being outdoors". It might be thought that these statements are self-evident but clearly when putting them into place requires some effort on the part of the County Council the lack of actual will becomes only too evident.</p> <p>Under 'Prioritising Improvements' at page 11 it is noted that the optimum solutions to achieve more</p>	<p>The section assumed to be referred to is 'Policy context' which outlines nationally and locally important documents relating to cycling and walking.</p> <p>Consideration for Northgate was given and the Strategy adjusted.</p>

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				<p>cycling and walking is to include 'Temporary and Experimental' traffic orders...to provide solutions'. Sadly when use could have been made of such orders it was clearly considered far too difficult and the idea was rejected (see Plan 104).</p> <p>At Parts 3.42-3.45 it appeared to be accepted that there is 'strong potential' for cycling (and presumably pedestrian) routes through Beccles but there is no reference to Northgate.</p> <p>Why is Northgate important. It is the route from the Quay to the town. It is currently a 'one-way' street used by over 100 buses a day (most of which then go over the bridge to Gillingham with a weight limit of 7.5 Tons despite the fact that the minimum weight of the buses is over 11 tons). There is a speed limit of 20mph which is ignored by over 90% of the vehicles using Northgate and given the narrowness of the pavements it is dangerous for pedestrians to use. The police and Highway Authority have been approached and my understanding is that both have said that they are unable to enforce the speed limit until somebody has been killed or seriously injured. I would be delighted to learn that that is not their view in which case they can explain why the speed limit is not enforced. The road travels over old cellars and is in a poor state of repair. Were it to be limited to vehicular use by those living in the road it would enable both pedestrians and cyclists to use it in safety and would mean that cyclists no longer use the pavements to cycle both up and down the road</p>	

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				<p>using the pavements to cycle on and putting pedestrians and themselves at considerable risk. According to 'The Guardian' Northgate is one of the loveliest streets in Suffolk and so it should be but the traffic usage means it is a 'rat run' and, currently, unsafe for both pedestrians and cyclists.</p> <p>It will, no doubt, be said that the above is 'special pleading' by somebody who lives in Northgate. However if the Plan is to be of relevance to both towns such as Beccles and to the safety and encouragement of cyclists and walkers in Suffolk the issues set out above should be addressed and not skirted round (and once again I remind you of what is said about Northgate in Plan 104).</p>	
110	Geoff Farrell	117	Yes	<p>Has to be seen in conjunction with 113 to achieve a continuous route between Snape and Aldeburgh. I use this route reasonably frequently and come up Priory Road and turn off to Friston to get to Aldeburgh via Thorpeness, although do cycle along the main road into Aldeburgh on occasion. The road is a daunting prospect even for the determined such as me. As with all routes, this has to be seen a part of a network and not simply stop.</p>	Support and further comments noted.
110	Tom Daly	100	Yes	<p>Very good development for cyclists and walkers. again, not beside the carriageway. Multiplier benefits to putting it behind the trees and hedges or it will just be a widening of the road and a wasted opportunity. (see comments to 51 above) if this requires arrangements with landowners so</p>	<p>Comment noted. The score provided gives a value to the public comment, but design and implementation will be considered further as the project progresses. Further consideration will be given to potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate</p>

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				be it, please invest in the future with foresight and creativity.	design, and the value for money in terms of the costs and benefits of delivering such a scheme.
112a	Cycling UK (John Thompson)	34	Yes	Of the two, if it is possible I support the idea of a route between Kessingland beach and Benacre. I am not against a cycleway alongside the A12 in principle, but the nature of the land alongside the A12 would not make it easy to construct and thus probably expensive, especially as it is surely necessary to be realistic about the number of people like to want to cycle specifically between Kessingland and Benacre. It could also be dual purpose serving as part of the leisure route that has also been suggested. More serious cyclists are likely to be comfortable riding on the A12 and not using the off-road facility would make them vulnerable to abuse from drivers.	Comment noted.
112b	Cycling UK (John Thompson)	36	Yes	<p>I think the point about the longer distance needs keeping in perspective. Cycling from Wrentham/Benacre to Lowestoft is not so much further than using the A12, especially as it will predominantly be fitter/more dedicated cyclists doing such rides. Indeed, for much of Carlton Colville it is probably shorter.</p> <p>Nonetheless, the idea is still good for the reasons I stated in my responses to recommendations 638 and 112a. Also, for leisure cycling it could be a continuation for those coming from/going to the Suffolk Coastal and Suffolk Sandlings AONB's via Walberswick and Southwold. Possibly also an extension of the Suffolk Coastal cycle route. There is already a minor road route between Wrentham</p>	The suggestion scored well under the MCAF methodology and the ambition will form part of the strategy as a community recommendation.

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				and Benacre Street, probalby no longer than the A12 route.	
113	Tom Daly	101	Yes	a good addition for Snape major attraction for rural leisure	Support and further comments noted.
Ref 119, Parking outside schools on Trimley High Road.	Chris Taylor	18	Yes	I fully support the comments regarding vehicles parking in the cycle lanes outside both the Primary School and Felixstowe Academy. Inconsiderate parking of cars (particularly by School Run Mums) in the cycle lanes effectively blocks them at the key time of day when they could be used by a large number of young cyclists. Its disappointing to see the cycle lanes marked out, but then blocked almost continuously along their entire length through Trimley by inconsiderate car drivers. Its a missed opportunity not to create a 'full cycle path' along this stretch of road... which in addition to local cycling is the only route out of Felixstowe available to cyclists.	Support and further comments noted.
142	Andrew McDonald	84	Yes		Support noted.
158	Andrew McDonald	79	Yes	This is a significant problem that needs a solution similar to this proposal, but with less potential ecological damage.	Support and further comments have been noted.
194	Clive Eastwood	56	Yes	New Road, Framlingham is much used by walkers and cyclists. The current 60mph speed limit is completely inappropriate.	Support noted. The speed of a road falls outside the remit of this project and should be passed to SCC.
197	Cycling UK (John Thompson)	41	No	The fact is the whole of the A146 between Lowestoft and Beccles is perceived as dangerous for cycling. so I doubt just having an off-road	Noted. This is a reasonable point to consider how any new potential infrastructure connects into Barnby is key as it is possible

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				<p>facility for Barnby bends would improve things. I also don't accept that the current alternative routes are so much longer if one is capable of riding the Beccles - Lowestoft distance. If one's only fear of cycling on the A146 is the Barnby bends, it is at least possible to avoid them in the easterly direction by using the one-way minor road out of Barnby village, which junctions with the A146 just east of the start of the bends.</p> <p>An off-road facility JUST for the bends could increase danger for cyclists when rejoining the carriageway because some drivers would not be expecting it.</p>	<p>to find a safer route through to Worlingham using the old Lowestoft Road once through Barnby. Any new infrastructure would need to clearly show a route entering Barnby as opposed to leaving an option to continue travelling along the carriageway.</p>
198	Kevin Moores	16	Yes	<p>The community recommendation 198 has been given a biodiversity score of -3 and a safety score of 0. These two scores need to be challenged. The biodiversity score appears to have been given with the assumption that the only way of achieving the recommendation would be a cycle route from Halesworth to Southwold alongside the River Blyth. However, the spirit of the recommendation could also be achieved by proper maintenance of the existing footpath alongside the river for foot traffic and the designation of a safe cycle route along the motor roads connecting Halesworth and Blythborough, and from Blythborough utilising the B1387 as a designated cycle route to reach Southwold via Walberswick for cycle traffic. Maintaining the existing footpath and upgrading existing roads to designated cycle routes would make cycle journeys considerably safer and more attractive to local people and</p>	<p>Comment noted. A walking and cycling connection between Halesworth and Southwold has been identified as part of the Leisure Route recommendations.</p>

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				tourists/holiday-makers, with no impact on biodiversity.	
203	Andrew McDonald	73	Yes	Significant onward travel benefits via Melton Stan as well as safety aspect.	Support and further comments noted.
207	Tom Daly	102	Yes	would be a most attractive route in this busy leisure area.	Support noted.
214	Mike Sherwen	64	Yes		Support noted.
221	Cycling UK (John Thompson)	50	No	<p>I understand the thinking but what about when there are larger/wider vehicles parked?</p> <p>While it is the case cyclists are advised to ride toward the middle of the carriageway to be properly seen, I suspect white lines toward the middle of the carriageway would tempt drivers to overtake on the inside. It is not exaggerating to say many would find that terrifying and I doubt parents of child cyclists would be happy about it. I realise there are cycle lanes in the middle of carriageways, such as approaching traffic lights, where there is more than on lane option. However, I don't consider that is comparing like with like.</p> <p>Taken to recommendation a logical conclusion, if accepted it should be the case for every residential road to have advisory cycle lanes in the middle of the carriageways.</p>	Noted. The response is reasonable that movement of a cycle lane to the centre of the road would need to be a wider district policy to avoid a piecemeal approach. The comment itself scores modestly so is not currently deemed a priority, however these scores can change.
222	Cycling UK (John Thompson)	47	Yes	While I don't believe it's as dangerous as stated, I accept there is a perception of danger. However, cyclists should be segregated from pedestrians.	Support Noted.
245	Cycling UK (John Thompson)	51	Yes	This makes sense, especially as it also helps leisure cycling for the North Sea Cycle Route.	Support is noted.

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247	Andrew McDonald	68	Yes		Support noted.
276	Mike Sherwen	90	Yes	It is astonishing that the National Trust , who own this track , have not done this already	Support noted.
276	Valerie Sherwen	111	Yes	I agree completed with this comment. The track should be defined as a public bridleway.	Support and further comments noted.
278	Alasdair MacLeod	59	No	I am opposed to (1) "provide an independent cycle / pedestrian bridge over the A12 connecting with Lancaster Drive." for the same reasons I am opposed to IM12. The IM12 route would be very damaging and so I am opposed to a bridge that would support that route. The existing footbridge bridge across the A12 in Martlesham Heath should be improved as suggested in the document.	The comments have been assessed against a standard methodology to give the recommendations value, but design and implementation will considered further as the project progresses. The recommendation will be subject to further assessment, which will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
305	Marlesford Parish Council (Richard Cooper)	142		<p>Marlesford Parish Council Comments on Community Recommendations</p> <p>In the initial consultation on a cycling and walking strategy at the end of 2020, Cllr Dr. Roger Waterfall made comments relating to Marlesford. These comments and the assessments of them have helpfully been presented in the current consultation document. The original representations are shown below, together with the Council's assessment <i>and in bold italics, we have added our comments on those assessments.</i></p> <p>Draft Cycling and Walking Strategy ASSESSMENTS OF COMMENTS RECEIVED AS PART</p>	<p>Agreed. With consideration to EDF/Sizewell C, 'Connectivity and Growth' should be awarded a 3.</p> <p>The scoring of 'Modal Shift' has been taken from the Propensity to Cycle Tool, which showed limited growth in this area.</p> <p>The proposal is for new infrastructure and does not, therefore, optimise existing infrastructure. Proposals relating to maintenance have not been assessed and have been passed onto SCC. Scores are awarded under the 'Optimisation' category</p>

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				<p>OF INITIAL CYCLING AND WALKING STRATEGY CONSULTATION OCTOBER TO DECEMBER 2020</p> <p>Community Recommendation 305 Marlesford A12 from Marlesford Road to B1116 (NW side of A12). To walk to Wickham Market from Marlesford requires several crossings of the A12. The path is often narrow and obstructed. A safe pedestrian and cycle way is required between Marlesford and Wickham Market. I'm sure many of the Council will have driven through Marlesford on the A12. Has anyone tried to walk from Bell Lane to the Framlingham Road (B1116)?</p> <p>A combined cycle/pedestrian track is required from Marlesford Road to the B1116 roundabout. This should be away from the highway, on the NW side of the hedge.</p> <p>2 1 0 3 -1 0 5 The commenter proposes a footway with a segregated cycleway between Marlesford Road junction and the B1116 roundabout on the NW side of the A12 behind the hedgerow. <i>Please note that the pedestrian and cycleway should be part of the SZC A12 mitigation measures in Marlesford village where improvements are proposed between Bell Lane and Marlesford Road.</i></p> <p>Connectivity and Growth – Wickham Market has a number of key services, which are not available in Marlesford, therefore the infrastructure will likely</p>	<p>where existing routes have been optimised physically or legally .e.g. a shared path has been optimised to a segregated bi-directional cycle track.</p> <p>Agreed. It is unlikely that farmland on the NW side of the hedgerow will have significant biodiversity value. A neutral score will be awarded to reflect this.</p> <p>Wickham Market has limited 'Leisure' value. Also, the proposal will likely have more 'Connectivity and Growth' value than that of 'Leisure'.</p>

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				<p>have significant connectivity benefit. However, as the proposed infrastructure does not cover the whole route into the centre of Wickham Market, a score of 2 is deemed acceptable.</p> <p><i>We argue that in the event of the SZC project going ahead, there is already a commitment to deliver a pedestrian and cycleway from Wickham Market to the Southern Park and Ride. This means that a new route from Marlesford would be able to connect at the Park and Ride and provide onward access to Wickham Market. The scheme should therefore attract a higher score.</i></p> <p>Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, the proposal would result in a small modal shift hence a score of 1.</p> <p><i>We believe that this is subjective and anecdotal evidence would suggest a higher usage rate if a new, safe, pedestrian and cycleway was provided. We believe that this score should be revised upwards.</i></p> <p>Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing.</p> <p><i>We draw your attention to Para 1.1 at the start of the East Suffolk Cycling and Walking Strategy which states that “the East Suffolk Cycling and Walking Strategy identifies potential cycling and walking infrastructure opportunities across the district. The Strategy focusses on the identification of new infrastructure opportunities</i></p>	

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				<p><i>rather than the maintenance of existing infrastructure. It provides context and information to support detailed infrastructure proposals and inform decision making to support cycling, walking, and equestrian use.” This statement seems to be at odds with the optimisation evaluation comment which appears to require optimisation of existing facilities. At the moment, the existing path is immediately adjacent to the A12 carriageway, is narrow and not fit for purpose. The only solution is a <u>new</u> path.</i></p> <p>Safety – This section of the A12 is a busy ‘A’ type road with a national speed limit and appears to be often utilised by HGVs. Despite this section of the A12 having an existing small footway adjoining the NW side, removing cyclists and pedestrians away from the road will have considerable safety benefit. A score of 3 is deemed reasonable. <i>We completely agree with this assessment, and it should be noted that the safety and fear and intimidation issues become greater if SZC and Scottish Power projects go ahead.</i></p> <p>Biodiversity – The commenter proposes implementing the cycleway and footway behind the existing hedgerow; therefore, the proposal will only result in the removal of managed grass hence a small negative score. <i>We believe that the land on the NW side of the existing hedge is in arable cultivation and because</i></p>	

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				<p><i>of this there should be no, or negligible adverse impact.</i></p> <p>Leisure – No leisure impact. <i>We would argue that the provision of any safe and serviceable path will, apart from its functional use of getting from Marlesford to Wickham Market, be used by local people for leisure. It is not correct to say that there is “No leisure impact”.</i></p>	
305 and 459	Marlesford Parish Council (Richard Cooper)	55	Yes	Yes - they will be submitted by separate e mail.	Support noted.
346	Tom Daly	103	Yes		Support noted.
351	Andrew McDonald	82	Yes		Support noted.
374	Geoff Farrell	115	Yes	It needs to be noted that the landowner permits horse riding along the verge of his field. I realise that he gets paid for that, but the point is that he is not averse to the principle of it being used as an alternative to using the verge of the road. If the concern is that permission for all and sundry to use that verge will result in a public right (quite understandable), then it could become a permissive path.	Support and further comments noted.
378	Pigeon Investment Management Ltd (Fillmore, Andrew)	132		As detailed above a series of improvements are proposed to the High Road/Howlett Way roundabout as part of the Pigeon scheme. In addition, it should be noted that the proposals for land off Howlett Way (SCLP12.54, application ref. DC/20/1860/OUT) include proposals for the existing footway on the southern side of Howlett	Comment noted.

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				Way to be widened to 3m to create a shared footway/cycleway, which will provide high levels of connectivity between the Howlett Way site and the new primary school, which forms part of the Pigeon scheme.	
379	Pigeon Investment Management Ltd (Fillmore, Andrew)	129		Opportunities to create cycling and walking routes have been a key consideration in the design of the Pigeon scheme. Our scheme proposes new on- site cycling and walking infrastructure, including 3m shared foot and cycleways linking with the proposed new school site, as well as off-site improvements to encourage walking and cycling.	Comment noted.
382	Cycling UK (John Thompson)	42	No	I indicated "no" with some hesitation. If a cycle track alongside the A146 between Worlingham and Oulton Broad is practical, I could support it. However, I have strong reservations about whether it is practical and the current off-road (off-A146 between Worlingham roundabout and North Cove) runs very much parallel to the A146. I also don't agree that the minor roads are dangerous in the dark and at commuter times. It's getting into the perception cycling on ANY roads is dangerous.	The score provided gives a value to the public comment, but viability will be considered further as the project progresses. The key corridors section notes that as a high priority new cycle infrastructure should be provided should a bypass to the Barnby Bends be considered. However improvements to the rural roads south of the A146 are considered for improvement as an alternative.
386	George Redpath	8	Yes	The Safety criteria is flawed if it generally relates only to conflict with vehicles, which is far too generic i.e. a cyclist riding down the upper prom slope on the cycle lane reaches speeds up to 20 mph and can be a danger to pedestrians at the pinch point adjacent to the Claremont Pier where pedestrians are crossing the cycle lane to reach the car park and/or London Road South. This is an important leisure route along the cliff top and coast but pedestrians can be frequently in conflict, it is therefore imperative that the cycle lane is	The impact to pedestrians will be considered in all recommendations. The ambition for any improvements is to conform to national best practice for shared surfaces.

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				highlighted and maintained, but equally signage is erected that highlights to pedestrians they are about to cross over a cycle route as they exit the lower prom and across the upper prom and cycle path outside the Claremont Pier, additionally signage should be erected making it clear to cyclists they must slow down along this route.	
390	Simon Shaw	87	Yes	The priority where the cycle path crosses side roads such as bracken Avenue, Deben Ave, Dobbs Lane needs to be changed to make it clear that cyclists have priority. It is difficult for a cyclist to look forward, behind and up the side road to see if it is safe to cross. Some drivers give way, but others don't and also queue on the cycleway.	Support and further comments have been noted.
391	Cycling UK (John Thompson)	45	Yes	<p>I support this but with reservations.</p> <p>That is because I do not accept the coast road between Corton and Hopton is dangerous for cycling. On the contrary, I suggest it should be used to encourage those staying at the holiday camps aligning it to go on leisure cycle rides. Certainly the speed limit for the road should be reviewed with speed-calming. I don't know why Sustrans has not progressed the idea of the former rail line but could it be because they concluded the coast road was okay so money would be better spent elsewhere? Similarly, I accept a path along the former rail line would help those who would perceive the coast road as dangerous, but I would argue priorities would need to be carefully considered. It actually strikes me the best idea would be a leisure cycling and walking route. I think that would make the spending worthwhile.</p>	<p>Support is noted. An off-road route between Hopton and Lowestoft is a key ambition of the strategy as shown in the key corridor section. However if this is not achievable then improvements to Coast Road are also of value.</p> <p>A score of 3 was given contrary to this comment, but the score was a reflection of getting cyclists and walkers off-road not only from Coast Road but from the more hazardous A47.</p>

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408	Yoxford Parish Council (Sharon Smith)	119	No	<p>Re. community comment 408 - Darsham Station. The modal shift score has been assessed as zero. We would like to challenge this. From Yoxford the footpath is so narrow (effectively 0.5m or less) and dangerous (immediately adjacent to the A12 in a 40 mph zone) so as to make it unusable for all but the brave hearted. The proposed improvements would make walking and cycling to Darsham Station much more feasible for people in Yoxford. We cannot see how the modal shift score has been calculated but a score of zero seems low. Also, we cannot tell if the difficulty of parking at Darsham Station has been taken into account. The car park is frequently full so, for many people, walking or cycling to the station are not viable options and driving to the station isn't either. Whole journeys to (Ipswich, Woodbridge, Lowestoft etc.) have to be made by car because the station can't be reliably accessed. An improved walking and cycling option would ensure reliable access to Darsham Station and enable more journeys to be made by train, and facilitate a much more environmentally impactful modal shift from car to train. Can this be included in the assessment?</p>	<p>The scoring for 'Modal Shift' has been taken from the Propensity to Cycle Tool, which showed limited growth in this area.</p> <p>Connection to the train station has been reflected within the 'Connectivity and Growth' scoring category.</p>
411	Benhall and Sternfield Parish Council (Melanie Thurston)	145		<p>We agree with all the recommendations relating to, and adjacent to our Parish, but would like to add the following: The map shows that in the past, three separate comments have been made re the footpath behind the hedge alongside South Entrance - Nos 39, 411 and 422. These all have been scored at 6, which includes minus 3 for loss of biodiversity resulting from removal of the hedge. At our last Parish Council Meeting, the</p>	<p>Comment noted. This proposal has been identified as part of the Leisure Route recommendations. See recommendation LR8.1 in the strategy.</p>

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				Saxmundham environment consultant was present. When asked whether she favoured removal of the hedge to widen the roadside path, or whether she preferred to retain the hedge and widen the footpath behind, She chose the later. Benhall & Sternfield Parish Council agree with this. Therefore we wish to challenge the minus 3, and request that the score be amended to 9. This would have the effect of elevating this path in the priority listing.	
412	Tom Daly	104	Yes	a safe crossing is essential	Support noted.
415	Andrew McDonald	72	Yes	In principle this is a very good recommendation, and the 8k cycling 'limit' may well be falling to the electrification of commuter bicycles, which would result in a more substantial score. Again the negative biodiversity score may overestimate the damage done after potential mitigation and compensation.	<p>This is a reasonable point and increased use of electric bikes will extend the average range of cyclists. However, it is felt that at this stage electric bikes are still the minority so 8k remains a viable limit as works to routes that exceed 8k would not achieve an optimum use. In future updates to this strategy this may change to better reflect changing travel patterns.</p> <p>The proposal may potentially result in the loss of the established hedgerow, trees and other foliage, which will have a significant adverse impact on biodiversity hence the awarded score of -3.</p>
416	Andrew McDonald	80	Yes		Support noted.
417	Caroline Price	58	No	To allow cyclists to cycle along Castle St against the one-way flow could mislead cars into doing the same - which already happens from time to time with drivers who don't know the town. Castle St is very narrow in places, with extremely narrow	Comment noted. The score provided gives a value to the public comment, but viability will be considered further as the project progresses. Further consideration will be given to potential impacts of

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				pavements, and 2-way traffic of any kind could jeopardise the safety of everyone. The distance being considered is very short; I don't see that it would harm cyclists to walk the short distance to Double St where they can cycle again - and then to walk the 50 yards or so into the town centre if they are travelling that way.	implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
417	Clive Eastwood	57	No	Castle Street, Framlingham is too narrow for a cycle lane against the traffic flow. A cycle lane at the wider, east end would take away several car parking spaces, particularly for residents.	Comment noted. The score provided gives a value to the public comment, but viability will be considered further as the project progresses. Further consideration will be given to potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
417	Geoff Farrell	116	No	The thought is admirable but the layout of the streets make this unwise - simply not safe. A score of +2 to take cycles off Fore Street must surely be overwhelmingly negated by a score of -3 (at least) by having cycles separated only by a line of paint from on-coming traffic down the top part of Castle Street - trucks as well as cars/motor bikes. And Double Street is unsuitable in my view - sight lines are poor, parking is both sides. Where do you put the paint? The only safe place is right down the middle - ie - create a two way cycle lane. A safety score of -3 surely. And where do cycles go at the bottom end of Double Street?	Agreed. Although the infrastructure will likely discourage cycling along the busy B1119, a on-road cycle lane is not ideal. The safety score will be reduced to a score of 1 to reflect this.
418	Cycling UK (John Thompson)	53	Yes	Makes perfect sense.	Support is noted.

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422	Benhall and Sternfield Parish Council (Melanie Thurston)	146		We agree with all the recommendations relating to, and adjacent to our Parish, but would like to add the following: The map shows that in the past, three separate comments have been made re the footpath behind the hedge alongside South Entrance - Nos 39, 411 and 422. These all have been scored at 6, which includes minus 3 for loss of biodiversity resulting from removal of the hedge. At our last Parish Council Meeting, the Saxmundham environment consultant was present. When asked whether she favoured removal of the hedge to widen the roadside path, or whether she preferred to retain the hedge and widen the footpath behind, She chose the later. Benhall & Sternfield Parish Council agree with this. Therefore we wish to challenge the minus 3, and request that the score be amended to 9. This would have the effect of elevating this path in the priority listing.	Comment noted. This proposal has been identified as part of the Leisure Route recommendations. See recommendation LR8.1 in the strategy.
422	Christopher Burslem	1	Yes	The project group organised a survey of the hedge from South Entrance to Benhall and it was felt that it could be replanted with selected hedging species and have significant biodiversity benefits, the present hedge is not in good condition and offers a ragtag border. We also feel that the benefits of a safe cycle way from both Kelsale and Benhall will have significant benefits for residents wishing to travel into Saxmundham for education, employment, shopping, leisure purposes and to reach the railway station and these benefits have been under recognised in your document.	Support and further comments have been noted. These are recommendations submitted to the Council as part of the initial consultation. The comments have been assessed against a standard methodology to give recommendations value; the removal of a hedgerow is considered to have significant adverse impact under biodiversity hence the score of -3.
423	Andrew McDonald	81	Yes		Support noted.

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434, The old railway line between Aldeburgh and leiston	Tom Daly	54	Yes	As indicated this has great potential of opening up countryside leisure and recreation for whole community. not car based. development of the old stations, coffee shop, local produce etc. a lasting community asset with multiple benefits. Maintain as a green route, nature enhancement, no chemicals.	Support noted.
445, 450, 452, 105, 324	Tom Daly	109	Yes		Support noted.
446, 474, 476	Tom Daly	106	Yes		Support noted.
447	Tom Daly	95	Yes		Support noted.
448	Tom Daly	94	Yes	be creative in minimising vegetation loss	Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
449 Leiston cum Sizewell	Tom Daly	93	Yes	an easy win	Support noted.
453	Tom Daly	96	Yes		Support noted.

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453	Tom Daly	98	Yes		Support noted.
455, 485, 444, 508, 473, 368, 470	Tom Daly	110	Yes		Support noted.
456 Abbey Lane	Tom Daly	92	Yes	To minimise biodiversity impact by locating cycleway behind existing vegetation. This will also increase the amenity experience for users.	Comment noted. All of the recommended improvements included in the Strategy are done so on the basis that when carried forward to the design and implementation phase they will be subject to further consideration and assessment for potential impacts and optimisation of routing and design by qualified persons. Assessment will include consideration of the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
457	Andrew McDonald	83	Yes	This and comment #158 both make the case for improving safety and connectivity for cyclists from A1152 into Rendlesham and across the A1152, connecting to Bentwaters and Friday Street. It's clear there needs to be an overall solution covering the stretch from an updated roundabout with B1069 at least as far as Rendlesham Mews, with traffic claiming, a dedicated cycling route and safer crossing.	Support and further comments have been noted.
459	Marlesford Parish Council (Richard Cooper)	143		Marlesford Parish Council Comments on Community Recommendations In the initial consultation on a cycling and walking strategy at the end of 2020, Cllr Dr. Roger Waterfall made comments relating to Marlesford. These comments and the assessments of them	Agreed. With consideration to EDF/Sizewell C, 'Connectivity and Growth' should be awarded a 3. The scoring of 'Modal Shift' has been taken from the Propensity to Cycle Tool, which

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				<p>have helpfully been presented in the current consultation document. The original representations are shown below, together with the Council's assessment <i>and in bold italics, we have added our comments on those assessments.</i></p> <p>Draft Cycling and Walking Strategy ASSESSMENTS OF COMMENTS RECEIVED AS PART OF INITIAL CYCLING AND WALKING STRATEGY CONSULTATION OCTOBER TO DECEMBER 2020</p> <p><u>Community Recommendation 459</u> Marlesford A12 north of Wickham Market. As already commented regarding walking, there is not a safe way of cycling from the Wickham Market roundabout to Marlesford and beyond. By providing a short section of Cycle/footpath this will allow cyclists direct access to Bell Lane which in turn leads towards villages around Parham Airfield, which is a designated industrial development area. This would allow somebody to cycle to work from Wickham to Parham without using the B1116 which is a busy route to/from Framlingham. Children from the villages could also safely cycle to school or access shops in Wickham.</p> <p>2 1 0 3 -3 0 3 The commenter proposes a footway and cycleway between Bell Lane and the B1116 roundabout.</p> <p>Connectivity and Growth – Wickham Market has a number of key services, which are not available in Marlesford, therefore the</p>	<p>showed limited growth in this area.</p> <p>The proposal is for new infrastructure and does not, therefore, optimise existing infrastructure. Proposals relating to maintenance have not been assessed and have been passed onto SCC. Scores are awarded under the 'Optimisation' category where existing routes have been optimised physically or legally .e.g. a shared path has been optimised to a segregated bi-directional cycle track.</p> <p>Agreed. It is unlikely that farmland on the NW side of the hedgerow will have significant biodiversity value. A neutral score will be awarded to reflect this.</p> <p>Wickham Market has limited 'Leisure' value. Also, the proposal will likely have more 'Connectivity and Growth' value than that of 'Leisure'.</p>

Community Recommendation Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>infrastructure will likely have significant connectivity benefit. However, as the proposed infrastructure does not connect into the centre of Wickham Market, a score of 2 is deemed reasonable.</p> <p><i>We argue that in the event of the SZC project going ahead, there is already a commitment to deliver a pedestrian and cycleway from Wickham Market to the Southern Park and Ride. This means that a new route from Marlesford would be able to connect at the Park and Ride and provide onward access to Wickham Market. The scheme should therefore attract a higher score.</i></p> <p>Modal Shift – According to PCT, if infrastructure is delivered to the highest standard, the proposal would result in a small modal shift hence a score of 1.</p> <p><i>We believe that this is subjective and anecdotal evidence would suggest a higher usage rate if a new, safe, pedestrian and cycle way was provided. We believe that this score should be revised upwards.</i></p> <p>Optimisation – The proposal is for new infrastructure and does not, therefore, optimise the existing.</p> <p><i>We draw your attention to Para 1.1 at the start of the East Suffolk Cycling and Walking Strategy which states that “the East Suffolk Cycling and Walking Strategy identifies potential cycling and</i></p>	

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				<p><i>walking infrastructure opportunities across the district. The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure. It provides context and information to support detailed infrastructure proposals and inform decision making to support cycling, walking, and equestrian use.” This statement seems to be at odds with the optimisation evaluation comment which appears to require optimisation of existing facilities. At the moment, the existing path is immediately adjacent to the A12 carriageway, is narrow and not fit for purpose. The only solution is a <u>new</u> path.</i></p> <p>Safety – This section of the A12 is a busy ‘A’ type road with a national speed limit and appears to be often utilised by HGVs. Despite this section of the A12 having an existing small footway adjoining the NW side, removing cyclists and pedestrians away from the road will have considerable safety benefit. A score of 3 is deemed reasonable. <i>We completely agree with this assessment, and it should be noted that the safety and fear and intimidation issues become greater if SZC and Scottish Power projects go ahead.</i></p> <p>Biodiversity – The proposal could result in the loss of the established hedgerow adjoining the NW side of the A12 which warrants a significant negative score. <i>Our proposal for a new pedestrian and cycleway</i></p>	

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				<p><i>would use land immediately to the NW of the existing established hedge (subject to obtaining private landowner agreement). It is therefore wrong to say that there would be loss of hedgerow and we ask that the score be revised accordingly. We believe that the land on the NW side of the existing hedge is in arable cultivation and because of this there should be no, or negligible adverse impact.</i></p> <p>Leisure – No leisure impact. <i>We would argue that the provision of any safe and serviceable path will, apart from its functional use of getting from Marlesford to Wickham Market, be used by local people for leisure. It is not correct to say that there is “No leisure impact”.</i></p>	
460	Valerie Sherwen	113	Yes	It is time to promote the river path as a place for walkers and cyclists, it just needs a will to do this, there is enough space to create shared or separated paths all along the river from Wilford bridge to Martlesham. it would be great for locals and visitors.	Support and further comments noted.
468	Andrew McDonald	67	Yes	It would be sensible to review the reason for the present lack of access before making any application ofr an Agreement.	The score gives value to the public comment, but the implementation of any measures are likely to need to be informed by further evidence gathering and detailed discussions with SCC as the Local Highways Authority.
471	Andrew McDonald	66	Yes	Funding for an Agreement would be sensible.	Comment noted.
472	Andrew McDonald	71	Yes		Support noted.

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486	Andrew McDonald	77	Yes		Support noted.
ref 489.	Chris Taylor	13		I would strongly challenge the score of "1..the route has limited leisure benefit" . the Wilford Bridge route is one of only two bridges that cross the river Deben in the Area (the other smaller one being at Ufford) and the one that most cyclists travelling from the Woodbridge & Ipswich direction will use. There has been a noticeable increase in "leisure cycling" seen across the county both by residents and visitors in recent times, even before COVID. Leisure cycling is abroad brush, essentially anything that is not cycling to work or school. The AOND makesfor easy cycling and given the popularity of Sutton Hoo, Rendlesham Forest, the Riverside Pubs and Coastal Villages as 'tourist' destinations and places which are within easy cycling distance for ESC residents it would make sense to up this score I think.	Agreed. The leisure score will be increased to 2 recognising the value of the bridge over the river and the connected PROWs. It is noted that, due to the location of the proposed infrastructure, the proposal will likely have more connectivity value than leisure value, therefore will not score the full 3.
493	Carlton Colville Town Council (Alison Ayers)	19	No	It had been suggested that a separate bridge for cycle/pedestrians be constructed. This would be a far better solution that closing one lane. The cycle path currently finishes at the bridge and has no link up.	The creation of a separate bridge (as opposed to re-purposing a lane) was considered in the creation of the strategy through submitted comment 784. Both scored well recognising that the bridge is a significant pinch point and obstacle to cyclists. Re-purposing a lane score slightly higher as a new bridge would represent a significant expenditure which doesn't optimise the existing infrastructure.
493	George Redpath	11	Yes	Under Safety it is stated that cyclists are forced to dismount which is total fantasy, with virtually no cyclists dismounting while crossing the bridge on this pavement. It's a misnomer to classify this as a	It is unclear when the response states, 'It's a misnomer to classify this as a cycle lane,' which cycle lane it refers to as the comment is regards to creating a cycle lane.

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				<p>cycle lane, it is actually a pavement for pedestrians and been classified as a cycle lane because a sign states its a shared route for cyclists and pedestrians, which ticked the box back in the Waveney Council days. On the ESC cycle map it is identified as Traffic-Free cycle route, which technically is correct, cyclists only clash with thousands of pedestrians who use this pavement each day.</p> <p>The Safety criteria is flawed and needs reviewing if it's only considering cyclists clashing with vehicles, it is chaos in peak periods when large numbers of pedestrians are crossing the bridge especially after it's been raised and a build up of both cars, pedestrians and cyclists attempt to cross all at the same time as the bridge is lowered. The clash between pedestrians and cyclist at this point needs to witnessed, and because the pavement has so much 'street furniture' cyclists are trying to avoid pedestrians and the furniture with some impatient cyclists jumping down onto the A12 in front of oncoming cars.</p> <p>The Safety score should be upgraded to a max score of 3.</p>	<p>As the response notes the scoring category refers to conflict between pedestrians/cyclists with vehicles as this has the greatest potential for serious harm should a conflict occur. The comment is noted and will be considered further.</p>
496	Arthur Stansfield	85	Yes	<p>Have the officers passed this issue on to the appropriate authority?</p> <p>I have had to turn round and use the B1078 when cycling because of this flooding</p>	<p>Maintenance issues fall outside the scope of the strategy and the control of East Suffolk Council. The land is control by Suffolk County Council and will be subject to their own maintenance policies.</p>
497	Arthur Stansfield	86		<p>This lane often floods towards Marlesford Lane with field run-off and makes it difficult for cyclists</p>	<p>Comment noted.</p>

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				<p>and pedestrians. Often have to walk through the field.</p> <p>There is also flooding from run-off near the crossing.</p> <p>This route can also be used to gain access to Blaxhall Common and Tunstall forest giving an off-road route through the forest through to Sudbourne and then Orford. I use it fairly often and have been impacted by the flooding on several occasions, resulting in having to use the B1078.</p> <p>This is a wonderful way to cycle to Orford with little interaction with traffic.</p>	
526	Cycling UK (John Thompson)	49	Yes	This could help the local tourism economy, especially as cyclists spend more than other tourists.	Support Noted. A score wasn't provided due to the significant length of the proposed infrastructure, but the lack of score does not mean it doesn't have value.
528	Alison Vickers	69	Yes	This suggestion gained a score of 8 which seems a good score, so hopefully the council will consider this seriously. An alternative suggestion of crossing the Trimley Flyover on the western side was made as part of the scoring response. However we think this would be equally dangerous without traffic lights.	Support and further comments noted.
529	Alasdair MacLeod	88	No	<p>I am opposed to a crossing in support of route IM12 (and am opposed to IM12 as I commented in the appropriate section).</p> <p>I am also opposed to running a route along the southern border of Martlesham Heath as this would end up damaging the SSSI.</p>	These are recommendations submitted to the Council as part of the initial consultation. The comments have been assessed against a standard methodology to give the recommendations value, but design and implementation will be considered further as the project progresses. Further

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				<p>If a new bridge/crossing must be built (I am not in favour but am told some councillors are keen) then I think it should be built at the southern edge of the Brightwell development with the PROW6 bridleway moved to run along Welham Plantation. I.e. moving PROW6 to align with Welham Plantation and then crossing the A12 to meet PROW12. This would keep the cycle traffic away from the SSSI, avoiding damage to it and allow a connection to Foxhall rd at the Dobbs lane junction allowing either onward travel to Ipswich along Foxhall Rd or more circular routes locally.</p> <p>The further south a bridge the more scope for traveling a more circular route, rather than an hour-glass route with Martlesham Heath as the pinch-point. This would also stand us in good stead when (I think inevitably) the agricultural land south of Martlesham Heath (either side of Welham Plantation) gets built on for housing.</p>	<p>consideration will be given to the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
529 & 278	Bowers	65	No	<p>Long term resident in Martlesham Heath</p> <p>Have concerns that adding an additional A12 bridge and paths.</p> <p>While these will not be much more efficient in terms of distance connecting Brightwell Lakes to Kesgrave and beyond than already existing routes, they will involve the destruction of part of Birch Woods ('Martlesham woods') with felling of mature trees, laying of tarmac and associated lighting infrastructure.</p>	<p>These are recommendations submitted to the Council as part of the initial consultation. The comments have been assessed against a standard methodology to give recommendations value, but design and implementation will be considered further as the project progresses. Further consideration will be given to the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of</p>

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				<p>I have a young family and we regularly use Birch Woods. I think this is an area that should be protected rather than destroyed. We also cycle and use the existing cycle paths which in no way seem to be at capacity and I would think be able to handle the additional usage from Brightwell Lakes.</p> <p>Thank you for your kind consideration of mine and my families views.</p>	<p>the costs and benefits of delivering such a scheme.</p>
529 and 278	Maria Jimenez	61	No	<p>As a Martlesham Heath resident (Avocet Lane), I oppose to the building of a bridge over the A12 connecting the new Brightwell Lakes development mainly due to the proposal of creating a path cutting through Birch Woods (to which you refer as Martlesham Woods). The creation of such path (labelled as IM12 on the proposal map) will not save a considerable amount of distance compared to using the existing path labelled IM13 on the map. However, what it would create is a considerable disturbance on the nature of Birth Woods which provides a safe habitat to a wide range of local animal species (such as muntjac deer, slow worm, sparrow hawks, hedgehogs, etc), potential of noise and littering in the area as well as requiring the felling of a number of mature trees.</p>	<p>These are recommendations submitted to the Council as part of the initial consultation. The comments have been assessed against a standard methodology to give the recommendations value, but design and implementation will be considered further as the project progresses. Further consideration will be given to the potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p>
544	Allan Cole	24	No	<p>I have walked this route twice a day for the last 2 years and before that once a day since 1988.</p> <p>I am also a cyclist , but do not cycle on unsafe routes and footpaths.</p> <p>This proposed route for cyclists as well as</p>	<p>The impact to pedestrians will be considered in all recommendations. The ambition for any improvements is to conform to national best practice for shared surfaces.</p>

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				<p>pedestrians is dangerous . There are numerous blind bends and narrow pinch points along it where cycling is dangerous to pedestrians.</p> <p>During the last 2 years I have been abused by cyclists who appear to be trying to achieve a “personal best “ time , this has happened in excess of 10 occasions whilst out walking with 2 dogs . My wife has been verbally abused to the point of being reduced tears twice by speeding cyclists on off road cycles, this was witnessed by others.</p> <p>This abuse and selfish attitude has apparently been exasperated by the off road cycling vogue , the abusive cyclists are both male and female and generally apparently over 35 years of age.</p> <p>This route has become extremely popular with the promoting of Sutton Hoo and is understandably a route visitors wish to enjoy SAFELY.</p> <p>THERE ARE ALREADY SIGNS INDICATING CYCLING IS PROHIBITED YET THESE ARE IGNORED.</p> <p>Please do not make this jewel in the crown pedestrian route a “Russian Roulette” as far as safety is concerned, for all ages of pedestrians ,able bodied and disabled by allowing / encouraging cyclists to use it , the nature of the paths and fauna do not permit a safe separation of pedestrians and cyclists as well as the blind corners and narrow sections at various points.</p>	

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				<p>This “problem” with cyclists with “attitude” has only become evident as a point of issue in the last 2 to 3 years , prior to that the occasional cyclist was encountered however I never experienced the abuse and vitriol that now seems to be the fall back option when politeness is offered , usually the abuse is discharged before one is aware a cyclist is behind you or having come round a blind corner almost into you.</p> <p>I do not like to use the expression , however I do fear a nasty accident will result if this proposal is granted , unless the 2 users can be safely separated along the entire length ... from Wilford Bridge to Martlesham (the users being Pedestrians and cyclists).</p> <p>Thank you for your time and consideration this is a footpath that should be globally recognized for its beauty and one we / you should be proud to be the custodians of , let’s not abuse that trust that we currently have to this and future generations.</p> <p>Thank you in anticipation of considering the aforementioned in any decision process on this proposed route.</p>	
544	Mike Sherwen	91	Yes		Support noted.
544	Valerie Sherwen	112	Yes	I totally agree with this comment. The road to Woodbridge is unsafe for cyclists due to the number of parked cars. The path along the river is a safe and pleasant route which should be possible	Support and further comments noted.

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				to share for both walkers and cyclists enjoying the route.	
547a Mill Lane Railway Bridge	Daniel Wareing	22		<p>I do not agree with the suggestion made by ESC in 547a to convert part of the pavement on the north side as a shared pedestrian/cycle way as I feel there may be intended negative consequences of creating conflicts between cyclists and pedestrians and users of mobility scooters, as well as forcing cyclists back onto the carriageway, after a very short distance, at a point where motorists travelling the same way will be suddenly swerving to their left.</p> <p>I support the suggestion made in community comment ref. X43 Mill Lane to reduce the visual running lane by the addition of white lines and hatching, and in addition to that I suggest that prominent signage facing traffic oncoming from the east to remind them about cyclists.</p>	Noted. The comments have been assessed against a standard methodology to give the recommendations value, but design and implementation will be considered further as the project progresses.
551	Caroline Cavill	27	No	<p>This route is bridleway and permissive footpath which a number of horse riders have specific land owner permission to use</p> <p>Should the surface be made into a hard surface, it will not be suitable for horse riders, and not optimal for walkers either</p> <p>It is a stable surface which can be easily walked, and ridden (horse and bike) all year round. To surface it in any other fashion would in effect make it a 'narrow tarmac road' which would just result in speeding cyclists and be a danger to all and unsuitable for this environmental setting</p>	This strategy aims to create an improved and inclusive network where the needs of all users, including equestrian users, will be considered.

Community Recommendation Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
551	Tom Daly	107	Yes		Support noted.
573	Alison Vickers	120	Yes	I think the need for a cycle track between the proposed new school site on land near Reeve Lodge, Trimley St Martin and Kirton village will be essential if the school is to be built. Many children's and adult's lives will be put in danger if they have to cycle the full length of Kirton Road at busy times, as I believe it is unsafe. It will be too far for children to walk from the village (about 2 and a half miles). It should have been given a higher score and very serious consideration.	Support and further comments noted.
573	Pigeon Investment Management Ltd (Fillmore, Andrew)	128		<p>Pigeon has submitted an outline planning application (ref. DC/20/5279/OUT) for land adjacent to Reeve Lodge, High Road, Trimley St Martin (site reference SCLP12.65) (the "Pigeon scheme"). The Outline planning application with some matters reserved (access to be considered) is for a phased scheme of up to 139 new homes (including provision of up to 46 affordable homes), land for a two-form entry primary school with pre-school, open space, SUDS, meadow and informal path on land south of Gun Lane, and all associated infrastructure provision.</p> <p>The Pigeon scheme includes the provision of a series of off-site highway improvements that will be secured via planning condition. These works will include the provision of new cycle/footway infrastructure and improvements to the existing refuge islands at the roundabout that forms the junction between High Road and Howlett Way. These works will provide a continuous 3m cycle link between the Pigeon scheme and the existing</p>	Comment noted.

Community Recommendation Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>foot and cycleway provision on High Road North. From High Road North, St Martins Green and Old Kirton Road provide a safe and convenient walking and cycling route between Kirton and Trimley St Martin (via the existing A14 footbridge). This provides for a safe and convenient walking and cycling route between Kirton and the Pigeon scheme.</p>	
575	Cycling UK (John Thompson)	52	No	<p>Current facilities away from the A47 is adequate and parallel.</p> <p>It is quite wrong to criticise cyclists who are comfortable using busier roads.</p>	<p>When a key corridor is identified where possible viable off-road routes should be explored. These provide additional safety and comfort to all users particularly those that are less confident.</p> <p>The response does not intend to criticise cyclists who use the busier road instead it intends to provide safer and more inclusive alternatives.</p>
576	George Redpath	6	Yes	<p>Under Optimisation, I disagree 'the pedestrian aspect is unlikely to be significantly improved'? This path is extremely popular with pedestrians and cyclists alike and should be to 'a shared path standard'. This could be the optimisation of a key corridor which includes the pinch point at the Jolly Sailors pub/restaurant where diners are sitting almost directly on the cycle lane. A dedicated cycle lane on the cliff top path and will offer improvements to both pedestrians and cyclists. The Optimisation score should be a min +1.</p> <p>Under Safety: It is stated 'no significant safety benefit', which of course is just nonsense and the criteria flawed if safety is only concerned with</p>	<p>Optimisation - Much of the path along the cliff is not a bridleway/cycle lane. Accordingly provide a new shared cycle lane will have only modest benefit for pedestrians (notably around the Jolly Sailor as stated) but the majority of the route the benefit is not significant enough to warrant a score.</p> <p>Safety - As the response notes the scoring category refers to conflict between pedestrians/cyclists with vehicles as this has the greatest potential for serious harm should a conflict occur. The comment is noted and will be considered further.</p>

Community Recommendation Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>cyclists interacting with vehicular traffic? There is daily conflict on this cliff top path with cyclists and pedestrians due to its popularity as a coastal cliff top route from Pakefield into the town centre used by locals, tourists from the various caravan parks in Pakefield and cyclists. This should be changed to a 'shared cycle and pedestrian path' and widened to suit.</p> <p>Biodiveristy is scored as -1. which if anyone had actually visited this site you would see the biodiveristy aspect consists of 1 metre of unmanaged scrub at the top of the banking toward the East that could be cut back to widen the path, there is no requirement to cut into the grassed area which is managed by NORSE. I suggest biodiversity should be 0.</p>	<p>Biodiversity - The foliage either side of the path varies at different sections with scrub land and managed grass. The scrub foliage is of a higher biodiversity value and the scoring comments does not describe this adequately.</p>
578, 497, and 678	Zac Barnes	33	Yes	<p>I beg to differ with your response to comment 578. An off-road cycle route from Wickham Market to the train station would significantly improve connectivity. A score of 2 was given to comments 497 and 678 for creating the possibility of a Wickham Market - Campsea Ash and train station link (I would suggest giving this proposal the same, ie 2 points).</p> <p>While I understand that PCT shows the current shortest route on the B1078 to be relatively quiet in terms of cycling, this is almost certainly impacted by the dangers cycling along the route poses to cyclists. Strava data for pedestrians shows heavy use of the path which suggests there would be at least some use by cyclists if they were</p>	<p>Agreed. The Connectivity and Growth score will be increased to 2 recognising the value of a new cycle route connecting Wickham Market to Campsea Ashe and, therefore, the Wickham Market train station. Furthermore, the safety score will be increased to 2 as the route does provide a safer alternative to the B1078. It is noted, due to the location of the existing PROWs, that the cycle route will not be entirely traffic free, so does not score the full 3 points for 'Safety'.</p> <p>The scoring for 'Modal Shift' has been taken from the Propensity to Cycle Tool, which showed limited potential growth in this area.</p>

Community Recommendation Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>allowed on it (I suggest at least 1 point).</p> <p>Comment 678 was given 3 points on safety for removing cyclists from the busy, narrow, and fast B1078. Comment 497 was given 2 points but this seems to be in relation to removing some traffic from the nearby C road by designating it as a quiet lane (I would suggest 3 points).</p> <p>Comment 497's suggestion of a quiet road is reasonable but would lengthen the journey from Wickham Market to Wickham Market Station by ~2km whereas comment 578 would shorten it by ~0.5km.</p> <p>Comment 678's suggestion may be worth considering but as you point out, it would cause a major impact to the hedgerow habitat along the route.</p> <p>I would additionally like the mention that if Wickham Market train station isn't actually going to be in Wickham Market it probably ought to have a high-quality cycling and path link.</p> <p>Taken with the existing points this would give a score of 8 and make it a high scoring recommendation.</p>	
579	Mike Sherwen	63	Yes	As much of the path from Martlesham to Wilford Bridge should be classified as "Share the Path" as possible	Support and further comments have been noted.
587	Peter Franklin	21	Yes	The track from Searson's Farm and then along the track to Trimley Nature Reserve does indeed need	Support noted.

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				improvement for cycling. The loose stone surface is not only difficult to ride but can also be off-balancing and risk falling off the bike. Ironically, the left-alone track that goes past the Nature Reserve track and up to Fagbury cliff is much easier to cycle as a dirt track - and much more fun when wet!	
594	Fiona Powell	25	Yes	<p>I am extremely concerned that no mention of horses, ridden or driven, is included in any of the comments I have read so far.</p> <p>The usage of routes on and off-road is apparently based on Strava heat maps, which exclude all horse activities.</p> <p>I'd like to see all users and potential users included, even when they exist in small numbers. Cyclists are 75% male; horse riders and carriage drivers are 80-85% female, so excluding equestrians in planned changes to routes is directly discriminatory against women - and users of disability trikes and scooters, quadricycles, recumbents and other less common legal users of safer off-road routes.</p> <p>Please consider all non-motorised users! We all need safe off-road routes,</p>	<p>Whilst the Strategy focuses on cycling and walking infrastructure, equestrian users have been considered throughout the preparation of the Strategy, with particular regard in respect of the PROW network.</p>
599C	Anthony Webb	17			N/A
613	George Redpath	7	Yes	<p>The Safety criteria is flawed if it generally relates only to conflict with vehicles, which is far too generic i.e. a cyclist riding down the upper prom slope on the cycle lane reaches speeds up to 20 mph and can be a danger to pedestrians at the pinch point adjacent to the Claremont Pier where pedestrians are crossing the cycle lane to reach the car park and/or London Road South. This is an</p>	<p>The impact to pedestrians will be considered in all recommendations. The ambition for any improvements is to conform to national best practice for shared surfaces.</p>

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				important leisure route along the cliff top and coast but pedestrians can be frequently in conflict, it is therefore imperative that the cycle lane is highlighted and maintained, but equally signage is erected that highlights to pedestrians they are about to cross over a cycle route as they exit the lower prom and across the upper prom and cycle path outside the Claremont Pier, additionally signage should be erected making it clear to cyclists they must slow down along this route.	
614	Cycling UK (John Thompson)	38	Yes	<p>It strikes me this is about perceived danger rather than actual danger. While a child stumbling into the road is dangerous, the fact is such things happen with children and I'm not clear how what is suggested would eliminate that danger.</p> <p>However, perceived danger is what dissuades many parents from allowing their children to cycle the school run, so if the proposal is practical I support it. It is worth bearing in mind, however, there are residential roads in the school's vicinity that could be made 20 mph.</p>	Support and further comments are noted.
614	George Redpath	12	Yes	<p>The Safety score is designated as 1.</p> <p>This does not support the reality on the ground, and states 'this road is 20 mph and relatively busy'. Let me state the facts as no PCT data is available and as a local who walks/cycles this route almost daily at all times of the day. There is only a short 200m section of this road classified as 20 mph, the remainder is national speed limit which pupils must negotiate if they are to use London Rd, and to state it is relatively busy is just wrong this is a</p>	The scoring could be clarified as the improvements were suggested between Pakefield School to Arbor Lane which then leads to the proposed route along the coast (key corridor L42 and L43 and comment 23) which offers a bypass to London Road. However it is recognised that the improvement is not dependant on a coastal path and without it Pupils would require the use of London Road and the scoring for safety could be increased.

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				<p>main arterial route into Pakefield, Kirkley & Lowestoft serving all local amenities in these settlements and onto Lowestoft North.</p> <p>May I suggest the Safety Score should be increased to the max 3.</p>	<p>The safety score will be increased to 2 recognising the issues on London Road. It is noted that the bypassing of a 30mph road would not normally score the full 3.</p>
616	George Redpath	5	Yes	<p>Under Modal Shift it has been stated this improvement 'would attract some cyclists who currently use London Road South', but if anyone involved in this strategy and had any local knowledge they would know the use of London Road South by cyclists is negligible due to the density of traffic, narrow road and the degradation of any cycling lanes/white lines, especially when when the scenic seafront/cliff-top route is a few metres to the East.</p>	<p>Support is noted. It is recognised that the constraints on London Road South limit its use, but some use is still likely particularly given the presence of on-road infrastructure so the suggestion will have an impact.</p>
618	Cycling UK (John Thompson)	39	Yes	<p>I support the development of any off-road routes that encourage more leisure cycling and possibly more utility cycling. that is, even though I regard "cycling on the A12 would be no less than life threatening" to be a big exaggeration</p>	<p>Support noted.</p>
618	George Redpath	9	Yes	<p>Under Connectivity & Growth it is stated that there is significant distance between the settlements means some cyclists will be dissuaded? The only reason the car is king between Southwold & Pakefield is because no established cycle lane exists between these settlements therefore exactly the reason we need to get our heads out the collective cycling sand and get serious about laying down cycle routes post COVID, and get people out of cars and onto cycles ala the Dutch model. This criteria should be increased to a score of 3.</p>	<p>The distance between Southwold and Lowestoft is approximately 9 miles to the edges of each settlement. Completing this route would not be possible for all/most cyclists particularly for regular trips and if a return journey is required. This means the connectivity benefit would be limited and the score is deemed viable.</p>

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619	Geoff Farrell	114	Yes	<p>619 should be seen in tandem with 374. My comment is that if 374 fails, then it looks like 619 would not be a contender. Result - no progress. That a safety score of +3 can be completely negated by a biodiversity score of -3 isn't fair. If that were to be replicated elsewhere then no development on a green field site would ever be permitted. In other words - in this case - the benefit of hugely increased safety should massively outweigh the moderate reduction in biodiversity. I would have the safety as +3 and the biodiversity as -1 (at most). This is a missing link between the paths in Letheringham / Easton and those around the south and south west of Wickham Market. If this link is restored then it is part of a long distance path running north - south.</p>	<p>Comment noted. The score provided gives a value to the public comment, but design and implementation will be considered further as the project progresses. Further consideration will be given to potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>The biodiversity criterion assesses whether the improvement can result in biodiversity gains or losses. Developing infrastructure through the woodland and hedgerow south of the B1078 will likely have a significant adverse impact.</p>
638	Cycling UK (John Thompson)	35	Yes	<p>I support it in principle but with reservations. I am not against cycleways alongside the A12 in principle but the suggestion is vague. The proposer does not indicate where a cycleway alongside the A12 should start/end. I suspect it would be difficult to make cycleways alongside the A12 continuous. I fear they would be stop/start affairs making it necessary to ride stretches on-road or, at best, crossing to the other side requiring cycle crossing, which would have the knock-on effect of moans from drivers.</p> <p>Specifically between Benacre and Lowestoft, I support the 'beach' idea as per recommendation 112a</p>	<p>The suggestion related to a route between Lowestoft and Kessingland scored well under the MCAF methodology and the ambition will form part of the strategy. IA potential of a route between Lowestoft to Southwold (encompassing Wrentham/Benacre utilising the A12 and encompassing Benacre/Wrentham was explored, but a clear and viable route was not identified.</p>

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				<p>The issue of comfortable routes between villages is a tricky one. Although it is predominantly about leisure cycling, local(ish) trips between villages (and between Benacre and Lowestoft) by bike rather than car should, of course, be encouraged. By all means, let's have quality cycle paths alongside the A12 where possible. However, I think some realism is required and perfect directness for journeys for those less traffic confident is unachievable.</p> <p>With respect to the proposers, I think they are arguing it's surely logical to have cycle paths alongside the A12 without considering the practical difficulties.</p>	
642, 464, 214, 213	Mike Sherwen	62		<p>I, and my family, have walked and cycled along this track along the river wall (its not a towpath !) between Melton and Woodbridge for the last 35 years It can be congested , requiring only courtesy, but is far preferable to using the B1438 for Safety and leisure reasons These four community recommendations would be best addressed by classifying it as "Share the Path" , which I have seen successfully used all around this country , and deals with etiquette</p>	Comment noted.
643	Andrew McDonald	78	Yes	Support in principle on account of onward travel from WMkt station.	Support is noted.
651	Peter Franklin	20	Yes	Routing cyclists off of Ferry Road and onto the prom / track from the Dip to Fx ferry is an absolute no-brainer. It's not just the fast vehicles on Ferry Road that cause concern, I also worry about getting hit by a golf ball!	Support is noted.

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654	Tom Daly	108	Yes	B1353 could be enjoined at point of least vegetation loss. Go Green.	<p>Comment noted. The score provided gives a value to the public comment, but design and implementation will be considered further as the project progresses. Further consideration will be given to potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>The biodiversity criterion assesses whether a recommendation could potentially result in any biodiversity gain or loss.</p>
655	Andrew McDonald	70	Yes	Very good recommendation; the negative 2 biodiversity score is harsh, given that mitigation/compensatory steps could quite easily be taken. Safety implications are paramount.	<p>Comment noted. The score provided gives a value to the public comment, but design and implementation will be considered further as the project progresses. Further consideration will be given to potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>The biodiversity criterion assesses whether a recommendation could result in a biodiversity loss or gain.</p>
655	Sudbourne Parish Council (Bill Parker)	127		2. Community Recommendations We note that you have used a Multi-Criteria Framework methodology and scoring system in	Agreed. Connecting the existing infrastructure within the village to PROW 4 will likely provide improved connection into

Community Recommendation Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				<p>your assessment. I would of been helpful if this had been explained in the initial consultation document as we could of provided more targeted information to help with your assessment.</p> <p>As a Parish we submitted 5 suggestions that have been allocated reference numbers:</p> <p>Reference 655 - New walkway to connect the pavement to the footpath at the northern end of the village We note that this proposal does not score in any of your first three categories which we believe is an error. The benefits of this proposal will improve the connectedness of the area and is a relatively simple solution providing great benefit. In addition the negative scoring for biodiversity is incorrect as there would be no need to remove foliage. We suggest the assessor should familiarise themselves with the detail of the locality. As a Parish Council we would be happy to discuss this in more detail with ESC. This is suggestion is the area we have most concern about and believe is most in need of solving.</p> <p>Thank you for consulting with us on this strategy we would welcome further discussion with you on how to make progress on resolving these issues.</p>	<p>Iken. The score under this criterion will be increased to reflect this.</p> <p>The scoring for 'Modal Shift' has been taken from the Propensity to Cycle Tool, which showed limited growth in this area.</p> <p>The optimisation criterion assesses whether a proposal provides significant upgrades to existing infrastructure. The proposal is for new infrastructure and does not, therefore, score under this category.</p> <p>The biodiversity criterion assesses whether the proposal can result in biodiversity gains or losses. In order to implement the infrastructure, there is a need to remove the wild green verge and small shrubbery adjoining the road.</p>
656	Andrew McDonald	76	Yes	(2) is a simple and sensible suggestion that could be implemented without biodiversity loss; (3) might be dealt with by on-road signage.	Comment noted. The score provided gives a value to the public comment, but design and implementation will be considered further as the project progresses. Further consideration will be given to potential

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					<p>impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.</p> <p>The biodiversity criterion assesses whether a recommendation could result in a biodiversity loss or gain.</p>
656	Sudbourne Parish Council (Bill Parker)	124		<p>2. Community Recommendations We note that you have used a Multi-Criteria Framework methodology and scoring system in your assessment. I would of been helpful if this had been explained in the initial consultation document as we could of provided more targeted information to help with your assessment.</p> <p>As a Parish we submitted 5 suggestions that have been allocated reference numbers:</p> <p>Reference 656 – New walkway to connect the pavement to the footpath at the southern end of the village.</p> <p>We note that this proposal does not score in any of your first three categories which we believe is an error. The benefits of this proposal will improve the connectedness of the area and is a relatively simple solution providing great benefit. In addition the negative scoring for biodiversity is incorrect as there would be no need to remove foliage. We suggest the assessor should familiarise themselves with the detail of the locality. As a</p>	<p>Agreed. Connecting the existing infrastructure within the village to PROWs 42 and 30 will likely provide improved connection to Orford. The score under this criterion will be increased to reflect this.</p> <p>The scoring for ‘Modal Shift’ has been taken from the Propensity to Cycle Tool, which showed limited growth in this area.</p> <p>The optimisation criterion assesses whether a proposal provides significant upgrades to existing infrastructure. The proposal is for new infrastructure and does not, therefore, score under this category.</p> <p>The biodiversity criterion assesses whether the proposal can result in biodiversity gains or losses. In order to implement the infrastructure, there is a need to remove the green verge adjacent and a small hedge adjacent the road.</p>

Community Recommendation Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				Parish Council we would be happy to discuss this in more detail with ESC.	
657	Sudbourne Parish Council (Bill Parker)	126		<p>2. Community Recommendations</p> <p>We note that you have used a Multi-Criteria Framework methodology and scoring system in your assessment. I would of been helpful if this had been explained in the initial consultation document as we could of provided more targeted information to help with your assessment.</p> <p>As a Parish we submitted 5 suggestions that have been allocated reference numbers:</p> <p>Reference 657 – Promotion of cycling events</p> <p>We are surprised that improving promoted events is outside of the strategy however we thank you for passing our comments are passed on to the relevant team.</p>	Comment noted.
659	Sudbourne Parish Council (Bill Parker)	125		<p>2. Community Recommendations</p> <p>We note that you have used a Multi-Criteria Framework methodology and scoring system in your assessment. I would of been helpful if this had been explained in the initial consultation document as we could of provided more targeted information to help with your assessment.</p> <p>As a Parish we submitted 5 suggestions that have been allocated reference numbers:</p> <p>Reference 659 – Condition of National Cycle Route 41</p> <p>We believe that improvements as suggested to this route would increase user confidence in this route</p>	Comment noted. Although the route itself has leisure value, the implementation of a sign is unlikely going to have significant leisure benefit.

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				and therefore increase leisure use. Therefore the scoring for Leisure should be reassessed.	
660	Sudbourne Parish Council (Bill Parker)	123		<p>2. Community Recommendations</p> <p>We note that you have used a Multi-Criteria Framework methodology and scoring system in your assessment. I would of been helpful if this had been explained in the initial consultation document as we could of provided more targeted information to help with your assessment.</p> <p>As a Parish we submitted 5 suggestions that have been allocated reference numbers:</p> <p>Reference 660 – Cycle route Snape to Orford through Tunstall Forest</p> <p>Whilst this suggestion is above your points threshold of 6 scoring 7, we suggest that there would be some additional benefit by the promotion of a safe clear route between Snape and Orford and this would create a positive modal shift or optimisation (the definitions between the two are not clear) and therefore should have some additional score.</p>	<p>The scoring for 'Modal Shift' has been taken from the Propensity to Cycle Tool, which showed limited growth in this area.</p> <p>The optimisation criterion assesses whether a proposal provides significant upgrades to existing infrastructure. Although the proposal upgrades footpaths to bridleways, it is creating a new cycleway and does not, therefore, improving an existing cycleway.</p>
667	Cycling UK (John Thompson)	43	Yes	<p>I support this in principle but some points need keeping in perspective</p> <p>I acknowledge the route includes the A143 between Haddiscoe and St Olaves, which is an accident blackspot and the B1074 between St Olaves and Somerleyton, which some drive too fast on. Overall, however, it needs keeping in perspective. I disagree about "car fast lanes." Again, it's leaning to the misperception</p>	Support and additional comments noted.

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				cycling on ANY type of road is dangerous. That said, I support the principle of cyclist/pedestrian priority on country lanes. There is perhaps an argument for many of them between Reedham and Lowestoft to be included in the Green/Quiet lanes network	
667	Cycling UK (John Thompson)	46	Yes	I meant to add that I suggest the B1074 should be 40 mph in its entirety	Comment noted. Changes to vehicle speed limits on roads within the district are outside of the scope of the East Suffolk Cycling and Walking Strategy, and cannot be directly actioned by East Suffolk Council as we are not the Highways Authority. However, as many consultation responses have requested speed limit changes to roads within the district, a list of them has been collated separately to be sent directly to the Highways Authority, Suffolk County Council.
687	Alasdair MacLeod	89	No	A new route along the south of Martlesham Heath would lead people into the SSSI. This would bring significant footfall and cycle traffic harm to an area that does not get a lot today, this would harm the SSSI - damage to vegetation and especially tend to drive animals away from what today is a very quiet refuge for them. It would lead people to cycle on the SSSI where (as far as I know) cycling is not permitted.	Comment noted. The score provided gives a value to the public comment, but design and implementation will be considered further as the project progresses. Further consideration will be given to potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
719	Andrew McDonald	75	No	'High quality infrastructure' that requires wholesale removal of trees etc would be inappropriate on this walking route; but a less drastic improvement should be possible.	Comment noted. The score provided gives a value to the public comment, but design and implementation will be considered further as the project progresses. Further consideration will be given to potential

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					impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
720	Andrew McDonald	74	Yes	Note that everyday cycling limit of 8k may underestimate range of modern electrically assisted bikes.	This is a reasonable point and increased use of electric bikes will extend the average range of cyclists. However it is felt that at this stage electric bikes are still the minority so 8k remains a viable limit as works to routes that exceed 8k would not achieve an optimum use. In future updates to this strategy this may change to better reflect changing travel patterns.
721	Tom Daly	105	Yes	plan for minimising vegetation loss. It does not have to be up against the road. Consult with landowners if necessary. parish councils could help. i am a ward member I can help.	Comment noted. The score provided gives a value to the public comment, but design and implementation will be considered further as the project progresses. Further consideration will be given to potential impacts of implementation on the safety, inclusivity and functionality of the local movement networks, location-appropriate design, and the value for money in terms of the costs and benefits of delivering such a scheme.
774	David Carne	26	Yes	I have exactly the same wish that Lowestoft trains should stop at Westerfield Station. A couple of years ago I wrote to my local MP Dr Dan Poulter who after a bit of chasing up I might add, wrote to Greater Anglia and eventually I had a full of excuses reply.	Train stops are outside the remit of the project.

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				<p>One of the reasons from memory was that as the service frequency was increased to hourly there wasn't time for them to stop!</p> <p>Even if Westerfield was made a Halt stop on demand would be a solution.</p> <p>I cannot really understand how Felixstowe can stop but Lowestoft trains un unable to.</p> <p>I feel you may have far more clout in dealing with this issue.</p> <p>It seems rather silly if you did want to use the train for walking or cycling to Lowestoft or intermediate stops from Westerfield at present you would have to change at Ipswich.</p>	
X26	Pigeon Investment Management Ltd (Fillmore, Andrew)	131		We note the Council's scoring summary for this matter and generally agree with the Council's summary. In respect of Optimisation, it should be noted that the Pigeon scheme includes a series of improvements to facilitate cycle and pedestrian improvements at the High Road/Howlett Way roundabout to provide for better cycle and pedestrian connectivity.	Support and further comments have been noted.
X27	Pigeon Investment Management Ltd (Fillmore, Andrew)	130		We note the Council's scoring summary for this matter and generally agree with the Council's summary. In respect of Optimisation, it should be noted that the Pigeon scheme includes a series of improvements to facilitate cycle and pedestrian improvements at the High Road/Howlett Way	Support and further comments noted.

Community Recommendation Ref	Name/ Organisation	Comment ID /Ref	Do you support this?	Comment	Council Response
				roundabout to provide for better cycle and pedestrian connectivity.	
X43 Mill Lane	Daniel Wareing	23	Yes	I fully support this suggestion particularly with regard to reducing the visual running line over the railway bridge.	Support noted.

Write to us



East Suffolk Council
Planning Policy and Delivery Team
Riverside, 4 Canning Road,
Lowestoft, NR33 0EQ

Call us



Planning Policy and Delivery Team (Local Plans)
01502 523029 / 01394 444557

Development Management (Planning Applications)
01502 523100

Email us



Planning Policy and Delivery Team (Local Plans)
planningpolicy@eastsoffolk.gov.uk

Development Management (Planning Applications)
planning@eastsoffolk.gov.uk

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Strategic Environmental
Assessment
Screening Opinion
Cycling and Walking Strategy
September 2022

Contents

1. Introduction	1
2. Legislative Background	1
3. Criteria for determining the likely significance of effects referred to in Article 3(5) of Directive 2001/42/EC	2
4. Assessment	3
5. Conclusion	7
Appendix 1: Responses from Statutory Consultees	8

1. Introduction

The draft Cycling and Walking Strategy (the Strategy) has been produced by East Suffolk Council. The Strategy will apply to the whole of the East Suffolk Council area, and will be endorsed by the Broads Authority which is the local planning authority for the Broads area.

In some circumstances a planning strategy could have significant environmental effects and may fall within the scope of the Environmental Assessment of Plans and Programmes Regulations 2004 and so require Strategic Environmental Assessment.

This screening report is designed to test whether or not the contents of the draft Cycling and Walking Strategy require a full Strategic Environmental Assessment (SEA). The legislative background below outlines the regulations that require the use of this screening exercise. Section 4 provides a screening assessment of the likely significant effects of the Strategy and the need for a full SEA. This SEA report reviews the consultation draft Cycling and Walking Strategy (published for consultation in November 2021).

2. Legislative Background

The basis for Strategic Environmental Assessment legislation is European Directive 2001/42/EC 'on the assessment of the effects of certain plans and programmes on the Environment'. This document is also known as the Strategic Environmental Assessment (or SEA) Directive. European Directive 2001/42/EC was transposed into English law by the Environmental Assessment of Plans and Programmes Regulations 2004, or SEA Regulations.

The SEA Regulations include a definition of 'plans and programmes' to which the regulations apply. SEA requirements relate to plans or programmes which are subject to preparation or adoption by an authority at national, regional or local level, which includes those prepared for town and country planning and land use. SEA is required where the plan or programme is likely to have significant environmental effects. It is therefore necessary to screen the Strategy to identify whether significant environmental effects are likely.

Where screening identifies significant environmental effects, a full Strategic Environmental Assessment is required.

3. Criteria for determining the likely significance of effects referred to in Article 3(5) of Directive 2001/42/EC

The preparation of the Strategy triggers a requirement to determine whether it is likely to have a significant environmental effect. This requirement is discharged by the 'responsible authority' being the authority by which or on whose behalf the plan is prepared. Before making a determination, the responsible authority shall: -

- a) Take into account the criteria specified in Schedule 1 to the Regulations; and
- b) Consult the consultation bodies.

The consultation bodies are defined in section 4 of the SEA Regulations. The opinions from the statutory consultation bodies: Historic England, the Environment Agency and Natural England, are therefore to be taken into account.

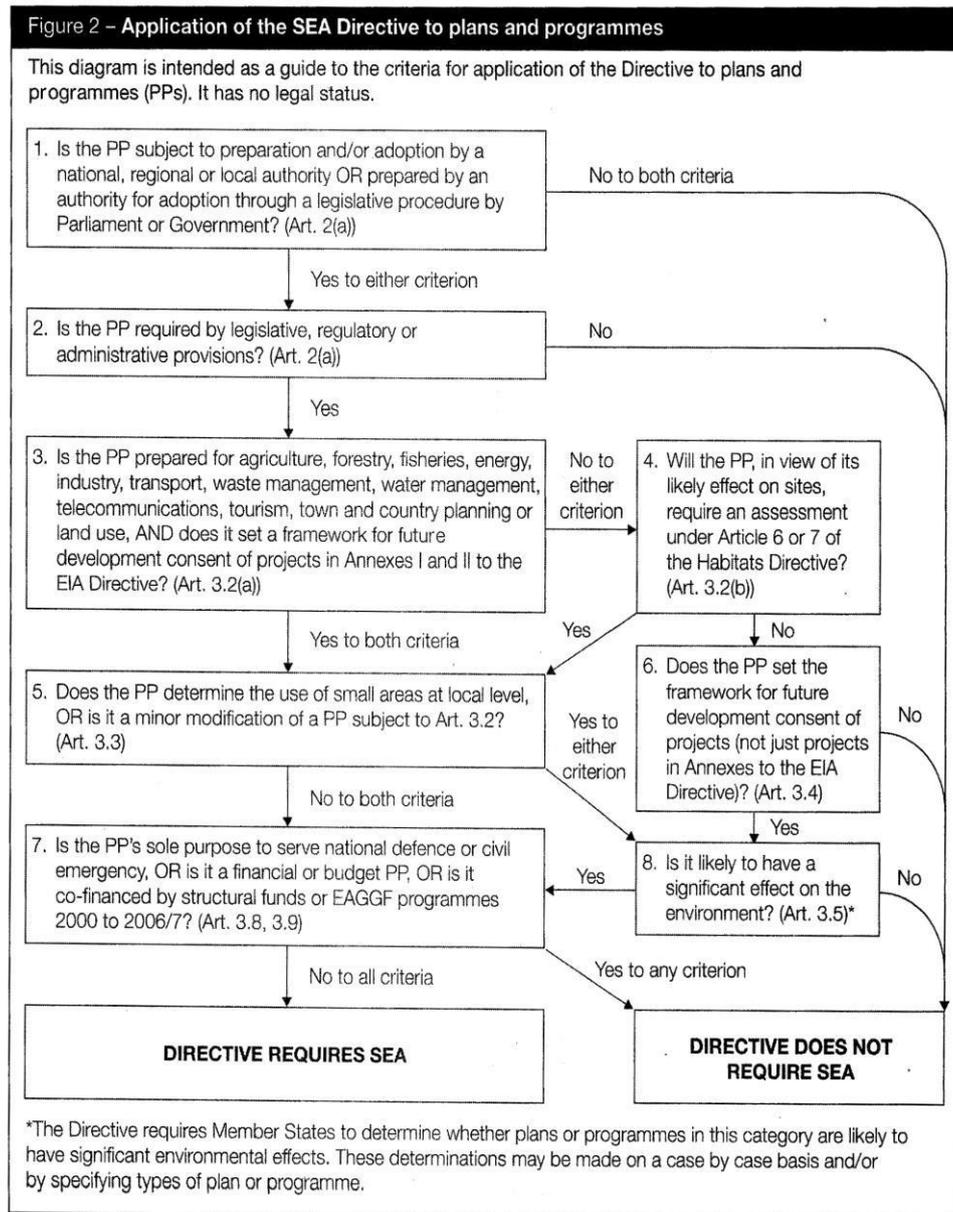
Schedule 1 of the SEA Regulations sets out the criteria for determining likely significant effects as follows:

1. The characteristics of plans and programmes, having regards, in particular to:
 - a. The degree to which the plan or programme sets a framework for projects and other activities, either with regard to the location, nature, size and operating conditions or by allocating resources.
 - b. The degree to which the plan or programme influences other plans and programmes including those in a hierarchy.
 - c. The relevance of the plan or programme for the integration of environmental considerations in particular with a view to promoting sustainable development.
 - d. Environmental problems relevant to the plan or programme.

- e. The relevance of the plan or programme for the implementation of community legislation on the environment (e.g. plans and programmes linked to waste-management or water protection).
2. Characteristics of the effects and of the area likely to be affected, having regard, in particular, to:
- a. The probability, duration, frequency and reversibility of the effects.
 - b. The cumulative nature of the effects.
 - c. The trans boundary nature of the effects.
 - d. The risks to human health or the environment (e.g. due to accidents).
 - e. The magnitude and spatial extent of the effects (geographical area and size of the population likely to be affected),
 - f. the value and vulnerability of the area likely to be affected due to:
 - i. special natural characteristics or cultural heritage;
 - ii. exceeded environmental quality standards or limit values;
 - iii. intensive land-use; and
 - g. the effects on areas or landscapes which have a recognised national, community or international protection status.

4. Assessment

The diagram below illustrates the process for screening a planning document to ascertain whether a full SEA is required.



Source: A Practical Guide to the Strategic Environmental Assessment Directive (2005)

The following assessment applies the questions from the preceding diagram. The answers determine whether the Strategy will require a full Strategic Environmental Assessment.

- 1. Is the PP subject to preparation and/or adoption by a national, regional or local authority OR prepared by an authority for adoption through a legislative procedure by Parliament or Government? (Art. 2(a))**

Yes. The preparation and adoption of the draft Cycling and Walking Strategy is being carried out by East Suffolk Council.

**2. Is the PP required by legislative, regulatory or administrative provisions?
(Art. 2(a))**

No. The Strategy is not a Supplementary Planning Document and will not form part of the statutory Development Plan.

3. Is the PP prepared for agriculture, forestry, fisheries, energy, industry, transport, waste management, water management, telecommunications, tourism, town and country planning or land use, AND does it set a framework for future development consent of projects in Annexes I and II to the EIA Directive? (Art 3.2(a))

The Strategy is being prepared in support of the delivery of town and country planning and land use policies.

The Strategy will not set a framework for the future consent of projects listed in Annexes I and II of the EIA Directive.

4. Will the PP, in view of its likely effect on sites, require an assessment for future development under Article 6 or 7 of the Habitats Directive? (Art. 3.2 (b))

A separate screening exercise has been carried out under the Habitats Directive (92/43/EEC) and Conservation of Habitats and Species Regulations (2017) (as amended). This has determined that a full Appropriate Assessment is not required.

5. Does the PP determine the use of small areas at local level, OR is it a minor modification of a PP subject to Art. 3.2? (Art. 3.3)

Not applicable (based on the responses to questions 3 and 4 above).

6. Does the PP set the framework for future development consent of projects (not just projects in annexes to the EIA Directive)? (Art 3(4))

No.

- 7. Is the PP's sole purpose to serve the national defence or civil emergency, OR is it a financial or budget PP, OR is it co-financed by structural funds or EAGGF programmes 2000 to 2006/7? (Art 3.8, 3.9)**

No. Not applicable.

- 8. Is it likely to have a significant effect on the environment? (Art. 3(5))**

No. The draft Cycling and Walking Strategy identifies potential cycling and walking infrastructure opportunities across East Suffolk. It provides context and information to support future detailed infrastructure proposals, and inform decision making to support cycling, walking, and equestrian use. The strategy builds on policies within both the Suffolk Coastal Local Plan and the Waveney Local Plan, specifically those relating to sustainable transport:

- Suffolk Coastal Local Plan Policy SCLP7.1: Sustainable Transport
- Waveney Local Plan Policy WLP8.21: Sustainable Transport

Part of East Suffolk falls within the Broads Authority area and is therefore covered by the Broads Authority Local Plan which was adopted in May 2019. In the north of the district some recommendations within the Strategy extend into the Broads Authority area.

All policies within the relevant Local Plans have been subject to a full Sustainability Appraisal, incorporating the requirements for Strategic Environmental Assessment.

It is unlikely that the Strategy will have a significant impact upon the environment.

5. Conclusion

The draft East Suffolk Cycling and Walking Strategy will support the implementation of policies in the East Suffolk Council - Suffolk Coastal Local Plan (adopted September 2020) and the East Suffolk Council - Waveney Local Plan (adopted March 2019) which were both subject to Sustainability Appraisal including Strategic Environmental Assessment.

It is considered by East Suffolk Council that it is not necessary for a Strategic Environmental Assessment to be undertaken of the draft Cycling and Walking Strategy to ensure compliance with SEA legislation.

Signed:



Dated: 22nd September 2022

Andrea McMillan
Planning Manager (Policy, Delivery and Specialist Services)
East Suffolk Council

Appendix 1: Responses from Statutory Consultees

Natural England Response

Date: 12 November 2021
Our ref: 372722

Anthony.Taylor@eastsoffolk.gov.uk
BY EMAIL ONLY



Hombeam House
Crewe Business Park
Electra Way
Crewe
Cheshire
CW1 6GJ

T 0300 060 3900

Dear Mr. Taylor

Planning Consultation: Screening for the Strategic Environmental Assessment (SEA) and Habitats Regulation Assessment (HRA) for the Draft East Suffolk Cycling & Walking Strategy

Thank you for your consultation on the above dated 01 November 2021 which was received by Natural England on the same date.

Natural England is a non-departmental public body. Our statutory purpose is to ensure that the natural environment is conserved, enhanced, and managed for the benefit of present and future generations, thereby contributing to sustainable development.

Screening Opinion Request: Strategic Environmental Assessment and Habitats Regulation Assessment

Habitats Regulation Assessment

Natural England notes that your authority, as competent authority under the provisions of the Habitats Regulations, has screened the proposal to check for the likelihood of significant effects. Natural England is a statutory consultee on the appropriate assessment stage of the Habitats Regulations Assessment process, and a competent authority should have regard to Natural England's advice.

Your assessment concludes that the proposal can be screened out from further stages of assessment because significant effects are unlikely to occur, either alone or in combination. On the basis of the information provided, Natural England concurs with this view.

Strategic Environmental Assessment

Where plans could have significant environmental effects, they may require a Strategic Environmental Assessment (SEA) under the Environment Assessment of Plans and Programmes Regulations 2004 (as amended). Further guidance on deciding whether the proposals are likely to have significant environmental effects and the requirements for consulting Natural England on SEA are set out in the [planning practice guidance](#).

Planning practice guidance also outlines that if an appropriate assessment is required for your neighbourhood plan this will also engage the need for a SEA. One of the basic conditions that will be tested by the independent examiner is whether the neighbourhood plan is compatible with European obligations, including those under the SEA Directive. Where a SEA is required it should be prepared in accordance with [regulation 12](#) of the SEA Regulations.

It is our advice, on the basis of the material supplied with the consultation, that, in so far as our

strategic environmental interests (including but not limited to statutory designated sites, landscapes and protected species, geology and soils) are concerned, that there are unlikely to be significant environmental effects from the proposed plan and therefore we concur with your conclusion that an SEA is not required.

Natural England advises that where opportunities for cycling and walking enhancement have been identified in proximity to European sites particularly which would lead to increased recreation, careful consideration is given to potential implications of increased recreational pressures on these sites.

Natural England would encourage close collaboration with the Suffolk RAMs strategy to ensure that where increased recreational usage of these sites may take place there is sufficient mitigation to ensure vulnerable features are protected. For example where a new or improved cycle path intersects an entrance into a designated area, there could signage which highlights the designations and the relevant vulnerable features.

This advice has been provided on a draft plan and Natural England recognises that there may be further iterations within which new or altered proposals may feature. Consequently this advice constitutes the best advice that can be given based on the information provided so far. Its quality and detail is dependent upon the quality and depth of the information which has been provided. It does not constitute a statutory response or decision, which will be made by Natural England acting corporately in its role as statutory consultee to the competent authority after an application has been submitted. The advice given is therefore not binding in any way and is provided without prejudice to the consideration of any statutory consultation response or decision which may be made by Natural England in due course. The final judgement on any proposals by Natural England is reserved until an application is made and will be made on the information then available, including any modifications to the proposal made after receipt of discretionary advice. All pre-application advice is subject to review and revision in the light of changes in relevant considerations, including changes in relation to the facts, scientific knowledge/evidence, policy, guidance or law. Natural England will not accept any liability for the accuracy, adequacy or completeness of, nor will any express or implied warranty be given for, the advice. This exclusion does not extend to any fraudulent misrepresentation made by or on behalf of Natural England.

Natural England are happy to discuss any of the advice provided in this email.

Further general advice on neighbourhood planning and the natural environment, including natural environment information sources is provided in Annex I.

For any further consultations on your plan, please contact: consultations@naturalengland.org.uk.

Yours Faithfully

Niall Walkden
Senior Adviser | Sustainable Development | Norfolk and Suffolk Team

Annex 1 - Neighbourhood planning and the natural environment: information, issues and opportunities

Natural environment information sources

The [Magic](http://magic.defra.gov.uk/)¹ website will provide you with much of the nationally held natural environment data for your plan area. The most relevant layers for you to consider are: Agricultural Land Classification, Ancient Woodland, Areas of Outstanding Natural Beauty, Local Nature Reserves, National Parks (England), National Trails, Priority Habitat Inventory, public rights of way (on the Ordnance Survey base map) and Sites of Special Scientific Interest (including their impact risk zones). Local environmental record centres may hold a range of additional information on the natural environment. A list of local record centres is available [here](#)².

Priority habitats are those habitats of particular importance for nature conservation, and the list of them can be found [here](#)³. Most of these will be mapped either as Sites of Special Scientific Interest, on the Magic website or as Local Wildlife Sites. Your local planning authority should be able to supply you with the locations of Local Wildlife Sites.

National Character Areas (NCAs) divide England into 159 distinct natural areas. Each character area is defined by a unique combination of landscape, biodiversity, geodiversity and cultural and economic activity. NCA profiles contain descriptions of the area and statements of environmental opportunity, which may be useful to inform proposals in your plan. NCA information can be found [here](#)⁴.

There may also be a local landscape character assessment covering your area. This is a tool to help understand the character and local distinctiveness of the landscape and identify the features that give it a sense of place. It can help to inform, plan and manage change in the area. Your local planning authority should be able to help you access these if you can't find them online.

If your neighbourhood planning area is within or adjacent to a National Park or Area of Outstanding Natural Beauty (AONB), the relevant National Park/AONB Management Plan for the area will set out useful information about the protected landscape. You can access the plans on from the relevant National Park Authority or Area of Outstanding Natural Beauty website.

General mapped information on soil types and Agricultural Land Classification is available (under 'landscape') on the [Magic](http://magic.defra.gov.uk/)⁵ website and also from the [LandIS website](http://www.landis.org.uk/)⁶, which contains more information about obtaining soil data.

Natural environment issues to consider

The [National Planning Policy Framework](#)⁷ sets out national planning policy on protecting and enhancing the natural environment. [Planning Practice Guidance](#)⁸ sets out supporting guidance.

Your local planning authority should be able to provide you with further advice on the potential impacts of your plan or order on the natural environment and the need for any environmental assessments.

¹ <http://magic.defra.gov.uk/>

² <http://www.nbn-nfor.org.uk/nfor.php>

³ <http://web.archive.nationalarchives.gov.uk/20140711133551/http://www.naturalengland.org.uk/ourwork/conservation/biodiversity/protectandmanage/habsandspeciesimportance.aspx>

⁴ <https://www.gov.uk/government/publications/national-character-area-profiles-data-for-local-decision-making>

⁵ <http://magic.defra.gov.uk/>

⁶ <http://www.landis.org.uk/index.cfm>

⁷ https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/807247/NPPF_Feb_2019_revised.pdf

⁸ <http://planningguidance.planningportal.gov.uk/blog/guidance/natural-environment/>

Landscape

Your plans or orders may present opportunities to protect and enhance locally valued landscapes. You may want to consider identifying distinctive local landscape features or characteristics such as ponds, woodland or dry stone walls and think about how any new development proposals can respect and enhance local landscape character and distinctiveness.

If you are proposing development within or close to a protected landscape (National Park or Area of Outstanding Natural Beauty) or other sensitive location, we recommend that you carry out a landscape assessment of the proposal. Landscape assessments can help you to choose the most appropriate sites for development and help to avoid or minimise impacts of development on the landscape through careful siting, design and landscaping.

Wildlife habitats

Some proposals can have adverse impacts on designated wildlife sites or other priority habitats (listed [here⁹](#)), such as Sites of Special Scientific Interest or [Ancient woodland¹⁰](#). If there are likely to be any adverse impacts you'll need to think about how such impacts can be avoided, mitigated or, as a last resort, compensated for.

Priority and protected species

You'll also want to consider whether any proposals might affect priority species (listed [here¹¹](#)) or protected species. To help you do this, Natural England has produced advice [here¹²](#) to help understand the impact of particular developments on protected species.

Best and Most Versatile Agricultural Land

Soil is a finite resource that fulfils many important functions and services for society. It is a growing medium for food, timber and other crops, a store for carbon and water, a reservoir of biodiversity and a buffer against pollution. If you are proposing development, you should seek to use areas of poorer quality agricultural land in preference to that of a higher quality in line with National Planning Policy Framework para 171. For more information, see our publication [Agricultural Land Classification: protecting the best and most versatile agricultural land¹³](#).

Improving your natural environment

Your plan or order can offer exciting opportunities to enhance your local environment. If you are setting out policies on new development or proposing sites for development, you may wish to consider identifying what environmental features you want to be retained or enhanced or new features you would like to see created as part of any new development. Examples might include:

- Providing a new footpath through the new development to link into existing rights of way.
- Restoring a neglected hedgerow.
- Creating a new pond as an attractive feature on the site.
- Planting trees characteristic to the local area to make a positive contribution to the local landscape.
- Using native plants in landscaping schemes for better nectar and seed sources for bees and birds.
- Incorporating swift boxes or bat boxes into the design of new buildings.
- Think about how lighting can be best managed to encourage wildlife.
- Adding a green roof to new buildings.

⁹<http://webarchive.nationalarchives.gov.uk/20140711133551/http://www.naturalengland.org.uk/ourwork/conservation/biodiversity/protectandmanage/habsandspeciesimportance.aspx>

¹⁰<https://www.gov.uk/guidance/ancient-woodland-and-veteran-trees-protection-surveys-licences>

¹¹<http://webarchive.nationalarchives.gov.uk/20140711133551/http://www.naturalengland.org.uk/ourwork/conservation/biodiversity/protectandmanage/habsandspeciesimportance.aspx>

¹²<https://www.gov.uk/protected-species-and-sites-how-to-review-planning-proposals>

¹³<http://publications.naturalengland.org.uk/publication/35012>

You may also want to consider enhancing your local area in other ways, for example by:

- Setting out in your plan how you would like to implement elements of a wider Green Infrastructure Strategy (if one exists) in your community.
- Assessing needs for accessible greenspace and setting out proposals to address any deficiencies or enhance provision.
- Identifying green areas of particular importance for special protection through Local Green Space designation (see [Planning Practice Guidance on this](#)¹⁴).
- Managing existing (and new) public spaces to be more wildlife friendly (e.g. by sowing wild flower strips in less used parts of parks, changing hedge cutting timings and frequency).
- Planting additional street trees.
- Identifying any improvements to the existing public right of way network, e.g. cutting back hedges, improving the surface, clearing litter or installing kissing gates) or extending the network to create missing links.
- Restoring neglected environmental features (e.g. coppicing a prominent hedge that is in poor condition, or clearing away an eyesore).

¹⁴ <http://planningguidance.planningportal.gov.uk/blog/guidance/open-space-sports-and-recreation-facilities-public-rights-of-way-and-local-green-space/local-green-space-designation/>

Environment Agency Response

From:Ipswich, Planning <[REDACTED]>
Sent on: Friday, November 12, 2021 2:21:57 PM
To:Anthony Taylor <[REDACTED]>
Subject:RE: Draft East Suffolk Cycling and Walking Strategy - SEA screening opinion

Good Afternoon

Thank you for your email.

I can confirm the Environment Agency have no comments to make in relation to these documents.

Kind Regards

Liam

Liam Robson
Sustainable Places Planning Advisor – East Anglia Area (East)
Environment Agency | Icen House, Cobham Road, Ipswich, Suffolk, IP3 9JD

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[REDACTED]

Historic England Response

From: Marsh, Andrew <[REDACTED]>
Sent on: Monday, December 6, 2021 9:19:05 AM
To: Anthony Taylor <[REDACTED]>
CC: Marsh, Andrew <[REDACTED]>
Subject: RE: Draft East Suffolk Cycling and Walking Strategy - SEA screening opinion

Dear Anthony

Thank you for consulting Historic England on the SEA screening opinion relating to the East Suffolk Cycling and Walking Strategy. Having reviewed the report I can confirm that we support the conclusion that it is not necessary for a Strategic Environmental Assessment to be undertaken of draft Cycling and Walking Strategy to ensure compliance with SEA legislation. I would be grateful if you would confirm receipt of this email.

Best wishes

Andrew Marsh BSc MA MRTPI
Historic Environment Planning Adviser
Development Advice | East of England
Historic England



Historic England
Brooklands | 24 Brooklands Avenue | Cambridge | CB2 8BU
www.historicengland.org.uk



Habitats Regulations Assessment Screening Statement

Cycling and Walking Strategy

September 2022

Contents

1. Introduction	3
2. Protected sites covered by this report	6
3. Cycling and Walking Strategy	7
4. Other Plans and Projects	1
5. Assessment of likely significant effects of the draft Cycling and Walking Strategy on Habitat sites.....	3
6. Summary and conclusions	23
Appendix 1: Sources of background information.....	24
Appendix 2: Locations of Habitat sites	25
Habitat sites within 20km of the East Suffolk Council- Waveney Local Plan Area.....	25
Habitat sites within 20km of the East Suffolk Council- Suffolk Coastal Local Plan Area.....	29
Appendix 3: Relevant Habitat sites.....	34
Appendix 4: Natural England Consultation Response	45

1. Introduction

1.1 The Conservation of Habitats and Species Regulations (2017) (as amended) provide protection for sites that are of exceptional importance in respect of rare, endangered or vulnerable natural habitats and species. The network consists of Special Areas of Conservation (SACs) and Special Protection Areas (SPAs). Both types can also be referred to as European Sites. The National Planning Policy Framework (NPPF) states that Ramsar sites should be afforded the same level of protection and refers to SACs, SPAs and Ramsar sites as 'Habitat Sites'.

1.2 The requirement to undertake Habitats Regulation Assessment (HRA) of plans and projects is set out in the Conservation of Habitats and Species Regulations (2017) (as amended).

1.3 Regulation 105 of the Conservation of Habitats and Species Regulations (2017) states:

'Where a land use plan:

(a) Is likely to have a significant effect on a European site or a European offshore marine site (either alone or in combination with other plans or projects), and

(b) Is not directly connected with or necessary to the management of the site, The plan-making authority for that plan must, before the plan is given effect, make an appropriate assessment of the implications for the site in view of that site's conservation objectives.'

1.4 The HRA is therefore undertaken in stages and should conclude whether or not a proposal or policy would adversely affect the integrity of any sites.

Stage 1: Determining whether a plan is likely to have a significant effect on a European site. This needs to take account of the likely impacts in combination with other relevant plans and projects. This assessment should be made using the precautionary principle. The screening assessment must reflect the outcomes of the 2018 judgement of the Court

of Justice of the European Union¹, which has ruled that where mitigation is necessary this must be identified through an Appropriate Assessment.

Stage 2: Carrying out Appropriate Assessment and ascertaining the effect on site integrity. The effects of the plan on the conservation objectives of sites should be assessed, to ascertain whether the plan has an adverse effect on the integrity of a European site.

Stage 3: Identifying mitigation measures and alternative solutions. The aim of this stage is to find ways of avoiding or significantly reducing adverse impacts, so that site integrity is no longer at risk. If there are still likely to be negative impacts, the option should be dropped, unless exceptionally it can be justified by imperative reasons of overriding public interest.

- 1.5 The draft Cycling and Walking Strategy (the Strategy) is being produced by East Suffolk Council. The Strategy will apply to the whole of the East Suffolk Council area. In the north of the district some recommendations extend into the Broads Authority area and therefore it's anticipated that the Strategy will be endorsed by the Broads Authority. This report considers whether there are likely to be significant effects on protected Habitat Sites and where a full Appropriate Assessment may be required.
- 1.6 East Suffolk Council (outside the Broads) is covered by two Local Plans, the East Suffolk Council - Suffolk Coastal Local Plan adopted September 2020 and the Waveney Local Plan adopted March 2019. Part of East Suffolk falls within the Broads Authority area and is therefore covered by the Broads Authority Local Plan which was adopted in May 2019.
- 1.7 Both East Suffolk Local Plans and the Broads Local Plan were subject to Habitats Regulations Assessment as part of their production. Where screening identified a likely significant effect, Appropriate Assessment was undertaken and the mitigation measures identified were incorporated within the Plans, resulting in conclusions that the plans will not lead to any adverse effects. Both

¹ C-323/17 – People over Wind, Peter Sweetman v Coillte Teoranta

Appropriate Assessments of the East Suffolk Local plans identified recreational disturbance particularly from dog walkers as a significant effect. The Council has subsequently produced a Recreational Avoidance and Mitigation Strategy and requires payment towards mitigation from residential developments within 13km of the protected Habitat sites. This approach also applies to the East Suffolk part of the Broads Authority area.

2. Protected sites covered by this report

2.1 Sites included in this assessment are listed in Table 1. This includes all sites that are within 20km of East Suffolk Council. The locations of the sites are shown on the maps in Appendix 2 and the Qualifying Features and Conservation Objectives of the sites are contained in Appendix 3, along with a summary of the pressures and threats as documented in the Appropriate Assessments for the Local Plans.

Table 1: Relevant Habitat sites

Name
Alde-Ore and Butley Estuaries SAC,
Alde-Ore Estuary SPA, Ramsar
Benacre to Easton Barents SPA
Benacre to Easton Barents Lagoons SAC
Breydon Water SPA, Ramsar
Broadland SPA, Ramsar
Deben Estuary SPA, Ramsar
Dew's Ponds SAC
Great Yarmouth North Denes SPA
Haisborough, Hammond and Winterton SAC
Minsmere to Walberswick Heaths & Marshes SAC
Minsmere – Walberswick SPA, Ramsar
Norfolk Valley Fens SAC
Outer Thames Estuary SPA
Orfordness – Shingle Street SAC
Sandlings SPA
Southern North Sea SAC
Staverton Park and The Thicks, Wantisden SAC
Stour and Orwell Estuaries SPA, Ramsar
The Broads SAC
Winterton-Horsey Dunes SAC

3. Cycling and Walking Strategy

- 3.1 This HRA report reviews the draft East Suffolk Cycling and Walking Strategy (published for consultation in November 2021). Since November 2021 the Strategy has been updated and refined in response to consultation comments and small amendments made to some recommendations, but these have not significantly changed the approach within the Strategy.
- 3.2 The Cycling and Walking Strategy identifies potential cycling and walking infrastructure opportunities across East Suffolk. The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure. It provides context and information to support future detailed infrastructure proposals, and inform decision making in order to support cycling, walking, and equestrian use.
- 3.3 Chapters 1 and 2 of the strategy are primarily descriptive, explaining the purpose of the strategy, the preparation process, and the policy context.
- 3.4 The key section of the strategy is the recommendations chapter (chapter 3). The recommendations fall into four categories:
- **Key Corridors** - Key routes between, and through, settlements where there are significant opportunities for modal shift to arise from improved cycling and walking infrastructure.
 - **Leisure route** - A potential Leisure Route has been identified, connecting a number of more rural settlements and projects.
 - **Local Plan Site Allocation Recommendations** - Recommendations for Local Plan site allocations, covering both on and off-site opportunities.
 - **Community Recommendations** - Recommendations submitted to the Council as part of the initial consultation which was held October - December 2020.
- 3.5 The Strategy is accompanied by a supplementary document setting out the assessment of all 800+ community recommendations that were received as part of the initial consultation. These are supplementary to the main body of the Strategy have not therefore been included in the screening table in section 5 of this report.

3.6 Once adopted, the Strategy will replace the Waveney Cycle Strategy (2016).

4. Other Plans and Projects

- 4.1 Regulation 105 of the Habitats Regulations requires consideration to be given to whether a Plan will have an effect either alone or in combination with other plans or projects.
- 4.2 As noted in the introduction, the other key plans are the Local Plans. The Local Plans set out the broad scale and distribution of development across the area of East Suffolk formerly covered by Suffolk Coastal District and Waveney District.
- 4.3 Both Local Plans seek to encourage walking and cycling, and both include specific policies relating to sustainable transport.
- Suffolk Coastal Local Plan Policy SCLP7.1: Sustainable Transport
 - Waveney Local Plan Policy WLP8.21: Sustainable Transport
- 4.4 The Local Plans also allocate land for development, in the form of site allocation policies. Where specific cycling and walking measures were identified through the local plan process these are detailed as policy criteria in the specific site allocation policies. In addition to these policy requirements, the Strategy has, where appropriate, identified a number of additional opportunities for cycling and walking associated with these site allocations.
- 4.5 The Suffolk Coastal Local Plan, as part of the Ipswich Strategic Planning Area (ISPA), sets out a further commitment to improvements to support sustainable transport to address the impacts of planned growth on the road network in and around Ipswich. An ISPA Transport Mitigation Strategy was prepared by Suffolk County Council in 2019. The Council is working with other authorities in the ISPA to develop an approach for funding and delivery of key improvements.
- 4.6 The East Suffolk Cycling and Walking Strategy will support the strategic priorities/objectives outlined in both Local Plans.

4.7 A screening process considered each policy in the Suffolk Coastal and Waveney Local Plans and concluded whether significant effects were likely and if Appropriate Assessment was therefore needed. The Appropriate Assessments of the Suffolk Coastal and Waveney Local Plans considered the following themes:

- Recreation pressure,
- Air quality and traffic emissions,
- Biodiversity net gain,
- Urbanisation,
- Water quality, resources and treatment, and
- Flood risk and coastal erosion.

4.8 Mitigation measures were identified within the Appropriate Assessments and incorporated within both Local Plans, resulting in a conclusion that neither plan would lead to any adverse effects on Habitat Sites within and in the vicinity of the (then) Suffolk Coastal and Waveney Districts.

5. Assessment of likely significant effects of the draft Cycling and Walking Strategy on Habitat sites

- 5.1 Table 3 below considers each relevant section of the draft Cycling and Walking Strategy in relation to whether there is potential for a likely significant effect on protected Habitat Sites. This constitutes Stage 1 as set out under paragraph 1.4 above. Consideration is given to the characteristics and location of the protected sites. The relevant sections are considered within the context of the Local Plan policies which, have themselves, been subject to Habitats Regulations Assessment, as set out in section 4 above.

Table 3: Likely significant effects of the Draft Cycling and Walking Strategy

Chapter	Assessment of potential impact on Habitat sites	Habitat sites that could possibly be affected	Likely significant effect identified	AA needed?
1.Introduction Purpose of the Strategy and Preparation of the Strategy	This section sets out the overall purpose of the Strategy and confirms that the Strategy will replace the 2016 Waveney Cycle Strategy. This sections also outline the process that has been undertaken to produce the consultation draft. This section is purely descriptive and does not in itself promote new infrastructure or development. This chapter will therefore not lead to likely significant effects on Habitat Sites.	None	None	No
1.Introduction Implementation of the Strategy	This section outlines how the Strategy will be implemented including reference to funding and delivery opportunities. This section explains that the opportunities identified within the Strategy should not be read as prescriptive proposals, or as the only way in which the infrastructure improvements can be delivered, but as high-level opportunities. Paragraph 1.6 of this section, clarifies that the implementation of the draft measures is likely to need to be informed by further evidence gathering and detailed discussions with the Local Highways Authority and that further assessments may also be required including (where necessary) Habitat Regulation Assessment.	None	None	No

	This section provides clarification on implementation and delivery requirements and processes (importantly highlighting potential HRA obligations). This chapter will therefore not lead to likely significant effects on Habitat Sites.			
2.Policy Context	This chapter outlines the national, regional and local policy context of relevance to cycling and walking in East Suffolk and signposts the key guidance that should be considered alongside the Strategy. This chapter is purely descriptive and does not in itself promote new infrastructure or development. This chapter will therefore not lead to likely significant effects on Habitat Sites.	None	None	No
3.Recommendations- Key Corridors	<p>This section outlines Key Corridors that have been identified as part of the Strategy. The Key Corridors are defined as the “key routes between, and through, settlements where there are significant opportunities for modal shift to arise from improved cycling and walking infrastructure”. This chapter explains that the “Key Corridors have been informed by an analysis of the responses to the initial map-based consultation, and the identification of patterns in those responses”. For each Key Corridor a number of recommendations have been identified which have been ranked as either medium, high or very high priority.</p> <p>Melton to Ipswich Key Corridor</p> <p>This route encompasses the parishes of Rushmere St Andrew, Kesgrave, Martlesham, Woodbridge, Melton, Brightwell, Bucklesham and Foxhall.</p> <p>Draft recommendations IM17, IM18, IM19 IM20, come within close proximity of the Deben SPA/ RAMSAR site, but</p>	Deben SPA/ Ramsar Stour and Orwell Estuaries SPA/Ramsar The Broads SAC Broadland SPA/ Ramsar	None	No

	<p>the recommendations primarily relate to improvements on existing pedestrian/ cycle routes and no new direct access points to the habitat sites are proposed. This section will therefore not lead to likely significant effects on Habitat Sites.</p> <p>Ipswich to Felixstowe Key Corridor</p> <p>This main route between the two settlements is made up of a combination of segregated cycle lanes, shared paths, modal filtered carriageway, and repurposed ex-dual carriageway. Draft recommendations F9, F52, F85, F95, F115 IM20, come within close proximity of the Stour and Orwell Estuaries and Deben SPA/ RAMSAR sites, but the recommendations primarily relate to improvements on existing pedestrian/ cycle routes and no new direct access points to the Habitat Sites are proposed. This section will therefore not lead to likely significant effects on Habitat Sites.</p> <p>Lowestoft Key Corridor</p> <p>This route encompasses the parishes of Oulton, Oulton Broad, Carlton Colville, Gisleham and Lowestoft. Parts of this Key Corridor fall within the Broads Authority area (as explained in paragraph 3.28).</p> <p>Draft recommendations L19, L20, L46 and L47, come within close proximity of The Broads SAC and Broadland SPA/Ramsar site at the Suffolk Wildlife Trust Carlton Marshes site but the recommendations primarily relate to improvements on existing pedestrian/ cycle routes and no new direct access points to the habitat sites are proposed.</p>			
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	<p>This section will therefore not lead to likely significant effects on Habitat Sites.</p> <p>Lowestoft to Hopton Key Corridor</p> <p>This route encompasses the parish of Corton and Lowestoft Town Council. None of the recommendations in this section come within close proximity of any of the identified habitat sites. This section will therefore not lead to likely significant effects on Habitat Sites.</p> <p>Lowestoft to Kessingland</p> <p>This route encompasses the parishes of Gisleham, Kessingland and Lowestoft Town Council. None of the recommendations in this section come within close proximity of any of the identified habitat sites. This section will therefore not lead to likely significant effects on Habitat Sites.</p> <p>Lowestoft to Bungay Key Corridor</p> <p>The route encompasses the parishes of Carlton Colville, Barnby, Mutford, North Cove, Ellough, Worlingham, Beccles, Weston, Barsham, Ringsfield, Shipmeadow, Ilketshall St Andrew, Mettingham, Ilketshall St John and Bungay. Parts of this Key Corridor fall within the Broads Authority area (as explained in paragraph 3.40).</p> <p>Draft recommendation LB23 comes within close proximity of the Broads Authority area and recommendations LB25 and LB35 run into the Broads Authority area. These recommendations do not, however, infringe on any the relevant SPAs, SACs or Ramsar sites. This section will</p>			
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	therefore not lead to likely significant effects on Habitat Sites.			
3.Recommendations- Leisure Route	This section outlines a potential leisure route that has been identified for further consideration in the future. The nature of this area differs from the Key Corridors, with more of a focus on recreational cycling and walking. The exact location of any potential leisure route is not specified in the Strategy. The Strategy makes reference to potential opportunities in the general area but does not in itself promote new infrastructure or development. This section will therefore not lead to likely significant effects on Habitat Sites.	None	None	No
3.Recommendations- Site Allocation Recommendations	This section provides additional detail on Cycling and Walking opportunities associated with the site allocation policies in both the Waveney Local Plan and the Suffolk Coastal Local Plan. The high level opportunities identified in this section address both on and off-site opportunities to inform the determination of future planning applications. A screening process considered all allocation policies in both Local Plans and concluded whether significant effects were likely and if Appropriate Assessment was needed. Where likely significant effects were identified appropriate assessment was carried out and mitigation incorporated into the relevant Local Plans. The relevant HRA reports are available here: Suffolk Coastal Local Plan: https://www.eastsuffolk.gov.uk/assets/Planning/Suffolk-Coastal-Local-Plan/Final-Draft-Local-Plan/Habitats-	None	None	No

	<p>Regulations-Assessment.pdfLocal-Plan/Final-Draft-Local-Plan/Habitats-Regulations-Assessment.pdf Waveney Local Plan: https://www.eastsuffolk.gov.uk/assets/Planning/Waveneyhttps://www.eastsuffolk.gov.uk/assets/Planning/Waveney-Local-Plan/Habitats-Regulations-Assessment.pdfLocal-Plan/Habitats-Regulations-Assessment.pdf</p> <p>This section will therefore not lead to likely significant effects on Habitat Sites.</p>			
3.Recommendations-Community Recommendations	<p>This section sets out the highest scoring community recommendations received as part of the initial consultation on the Strategy. All comments received were assessed against a Multi-Criteria Assessment Framework (MCAF) which is explained in detail in this section.</p> <p>Each recommendation (which may be made up of more than one consultation response) is assessed below:</p>	-	-	-
Brightwell (529)	<p>This recommendation suggests a safe crossing under the A12, upgrades to an existing bridleway, linking with cycleways to the hospital and Ipswich and the national cycle network.</p> <p>This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.</p>	None	None	No
Bucklesham (599b)	<p>This recommendation suggests 3 improvements</p> <p>1: To provide a safe way across the A12/A14 junction at Seven Hills.</p>	None	None	No

	<p>2: Provide a pedestrian/cycle bridge.</p> <p>3: Negotiate with the owner of Walk Farm for ATs' to use the tunnel at GR 252396.</p> <p>These recommendations do not come within close proximity of any of the identified habitat sites. These recommendations will not therefore lead to likely significant effects on Habitat Sites.</p>			
Darsham (338)	<p>This recommendation suggests 6 improvements to the area around Darsham station for improved access to coastal routes:</p> <ol style="list-style-type: none"> 1. A signalised crossing for pedestrians and cyclists. 2. Lower speed limit on the road at this point. 3. Advance signs warning of cyclists and/or pedestrians crossing. 4. Painting SLOW PEDESTRIANS/CYCLISTS CROSSING on the road in each direction. 5. Install a speed camera at this location. 6. Install a central refuge to allow the road to be crossed in two stages. <p>These recommendations do not come within close proximity of any of the identified habitat sites. These recommendations will not therefore lead to likely significant effects on Habitat Sites.</p>	None	None	No
Darsham (408)	<p>This recommendation suggests the inclusion of a dedicated cycle/foot path connecting Darsham Station with Westleton Road through the allocated site south of Darsham station by widening the A12 footpath to accommodate cyclists.</p> <p>This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation</p>	None	None	No

	will not therefore lead to likely significant effects on Habitat Sites.			
Felixstowe (174)	This recommendation suggests improvements to the bridleway surface and new signage to divert cyclists onto the bridleway which passes Hill House Cottages and Candle Farm between Gulpher Road and Thurmans Lane. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Felixstowe (258)	This recommendation suggests a new cycle connection between Gulpher Road and connections to the Garison Road and Colneis Road roundabout. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Felixstowe (259)	This recommendation suggests a segregated cycle route alongside the southbound side of the A154 Garrison Lane, between the Grove Road roundabout and the pedestrian entrance to Fairfield Avenue. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Felixstowe (260)	This recommendation suggests a safe cycle and walking route between Hamilton Road/Town Centre, the railway station and the new North Felixstowe Garden Village development and new leisure centre.	None	None	No

	This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.			
Felixstowe (389)	This recommendation suggests that clearer signage is needed along the walkway/promenade from Cobbolds Pt to Felixstowe Ferry. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Felixstowe (426)	This recommendation suggests linking up the two sections of cycle route on Walton Avenue (A154). This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Felixstowe (717)	This recommendation suggests upgrading the Hawkes Lane / footpath to Maidstone Rd & Runnacles Way via railway foot bridge for cycling use. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Felixstowe (FTC6)	This recommendation suggests converting both east bound footpaths on High Road East, lengthening the pedestrian island and the extending drop kerbs. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation	None	None	No

	will not therefore lead to likely significant effects on Habitat Sites.			
Felixstowe (FTC60)	This recommendation suggests constructing a new off-road facility from Undercliffe Road to Sea Road. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Foxhall (571)	This recommendation suggests the provision of a cycleway along the A1156 and additional safety features to enable cyclists to be able to turn right into Straight Road. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Halesworth (294)	This recommendation suggests improvements along Saxons Way to provide shared cycle and pedestrian paths. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Leiston (449)	This recommendation suggests improvements to connect the south of Leiston into the Town Centre via a combination of offroad and on-road cycle infrastructure. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No

Levington (199)	This recommendation suggests improvements along 'Old Felixstowe Road' to introduce segregated cycle lanes &/or reduce speed limits &/or prohibit through traffic. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Lowestoft (189)	This recommendation suggests improving the exit at the top of the High Street onto the A47. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Lowestoft (256)	This recommendation suggests creating a shared path through to Blundeston Roundabout, with a purpose built cycle track either side of the Yarmouth Rd through to Great Yarmouth. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Lowestoft (493)	This recommendation suggests improvements to Bascule Bridge by reducing it to two lanes for vehicles with cycling capacity on either side. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No

Lowestoft (522)	This recommendation suggests restricting the nearside lane of Battery Green Road to buses, taxis and cyclists only. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Lowestoft (524)	This recommendation suggests restricting the inside lane of the A12 dual carriageway as it approaches the Bascule Bridge to buses, taxis and cyclists only. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Lowestoft (252)	This recommendation suggests connecting the Norfolk Coastal Path at Hopton to the Suffolk Coastal Path at Lowestoft. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Lowestoft (618)	This recommendation suggests connecting the three settlements of Pakefield, Kessingland and Southwold for cycling. The exact route that this recommendation might follow has not yet been established however the assessment highlights the availability of PROW routes in the vicinity and the minor infrastructure required to implement this recommendation will not lead to likely significant effects on Habitat Sites.	Benacre to Easton Barents SPA	None	No
Martlesham (162)	This recommendation suggests a speed limit to Felixstowe Road and the widening of the footpath and cycle lanes. This	None	None	No

	<p>recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.</p>			
Martlesham (278)	<p>This recommendation suggests 3 improvements as part of the Brightwell Lakes development at Martlesham:</p> <ol style="list-style-type: none"> 1) provide an independent cycle / pedestrian bridge over the A12 connecting with Lancaster Drive. 2) provide some form of safe route to NCN 1 connection at the Gloster Road / Betts Avenue junction and upgrade (widen) the current pedestrian bridge across the A12. 3) Ensure that all roads within the development have combined cycle / footpaths <p>These recommendations do not come within close proximity of any of the identified habitat sites. These recommendations will not therefore lead to likely significant effects on Habitat Sites.</p>	None	None	No
Martlesham (329)	<p>This recommendation suggests improvements to Sandy Lane, including closing it to through traffic.</p> <p>This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.</p>	None	None	No
Martlesham (602)	<p>This recommendation suggests that there is a need to ensure that the Brightwell Lakes development delivers good cycleways to the A12 crossings. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.</p>	None	None	No

Martlesham (681a)	<p>This recommendation suggests safety improvements to Felixstowe Road, Main Road and Sandy Lane including safer crossing points.</p> <p>This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.</p>	None	None	No
Martlesham (681b)	<p>This recommendation suggests safety improvements to Felixstowe Road, Main Road and Sandy Lane including safer crossing points.</p> <p>This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.</p>	None	None	No
Melton (489)	<p>This recommendation suggests widening the footpaths along Wilford Bridge Road and providing a cycle lane into Woodbridge.</p> <p>This recommendation does come within close proximity of the Deben SPA/ Ramsar site, but the recommendations primarily relate to improvements on existing pedestrian/ cycle routes and no new direct access points to the habitat sites are proposed. This section will therefore not lead to likely significant effects on Habitat Sites.</p>	Deben SPA/ Ramsar	None	No
Melton (574)	<p>This recommendation suggests improvements along the B1438, including providing a purpose-built cycle path and a speed limit. This recommendation does come within close proximity of the Deben SPA/ RAMSAR site, but the recommendations primarily relate to improvements on existing pedestrian/ cycle routes and no new direct access</p>	Deben SPA/ Ramsar	None	No

	points to the habitat sites are proposed. This section will therefore not lead to likely significant effects on Habitat Sites.			
Nacton (251)	This recommendation suggests providing a full cycle/footpath from Nacton towards Warren Heath (past the Show Ground). This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Oulton Broad (191)	This recommendation suggests improved access to the Suffolk Wildlife Trust Carlton Marshes site from Oulton Broad South railway station. This recommendation falls within the Broads Authority area and comes within close proximity of The Broads SAC and Broadland SPA/Ramsar site at the Suffolk Wildlife Trust Carlton Marshes site, but the recommendations primarily relate to improvements on existing pedestrian/ cycle routes and no new direct access points to the habitat sites are proposed. This section will therefore not lead to likely significant effects on Habitat Sites.	The Broads SAC and Broadland SPA/Ramsar	None	No
Oulton Broad (671)	This recommendation suggests upgrading to existing footpath to accommodate cycles. This recommendation falls just outside the Broads Authority area and comes within close proximity of The Broads SAC and Broadland SPA/Ramsar site at the Suffolk Wildlife Trust Carlton Marshes site, but the recommendations primarily relate to improvements on existing pedestrian/ cycle routes and no new direct access points to the habitat sites are	The Broads SAC and Broadland SPA/Ramsar	None	No

	proposed. This section will therefore not lead to likely significant effects on Habitat Sites.			
Rushmere St Andrew (631)	This recommendation suggests widening the A1214 at Rushmere common to create dedicated cycle lanes on either side of the road, segregated from the pedestrian footway. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Stratton Hall (47)	This recommendation suggests using half of the Felixstowe Road as a cycle track and walkway. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Theberton (434)	This recommendation suggests using the old railway line between Aldeburgh and Leiston as cycle route. Part of this route would cut through the Sandling SPA, but would follow the route of existing footpath through the site with minimal upgrading required. The recommendation will not therefore lead to likely significant effects on the Habitat Site.	Sandlings SPA	None	No
Trimley St Martin (117)	This recommendation suggests restricting Morston Hall Road to residents and bus and cycle lane only. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Trimley St Martin (139)	This recommendation suggests creating a dedicated cycle lane alongside Morston Hall Road.	None	None	No

	This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.			
Trimley St Martin (378)	This recommendation suggests installing a separate, kerbed cycleway along Howlett Way. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Trimley St Martin (380)	This recommendation suggests widening the footbridge across the A14 and creating a cycleway which would join both sides of the A14. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Trimley St Martin (573)	This recommendation suggests providing new footbridge across the A14 and segregated path to provide safe access to the new primary school. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Trimley St Mary (265)	This recommendation suggests resurfacing the Blofield Track existing bridleway. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No

Trimley St Mary (727)	This recommendation suggests upgrading the Candle Track to enable cyclists to leave North Felixstowe and reach Trimley St Martin and Kirton on a traffic free route. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Waldringfield (601)	This recommendation suggests upgrading the existing permissive path across Waldringfield Golf Course to a PROW. This recommendation does not come within close proximity of any of the identified habitat sites. This recommendation will not therefore lead to likely significant effects on Habitat Sites.	None	None	No
Woodbridge (80)	This recommendation suggests providing a designated cycling space on main road from Melton, though Woodbridge, meeting up with the cycling section in Martlesham. This recommendation does come within close proximity of the Deben SPA/ RAMSAR site, but the recommendations relate to improvements on existing routes and no new direct access points to the habitat sites are proposed. This section will therefore not lead to likely significant effects on Habitat Sites.	Deben SPA/ Ramsar	None	No
Woodbridge (254)	This recommendation suggests closing Sandy Lane to vehicular traffic and making it access only. This recommendation does come within close proximity of the Deben SPA/ RAMSAR site, but the recommendations relate to improvements on existing routes and no new direct access points to the habitat sites are proposed. This section	Deben SPA/ Ramsar	None	No

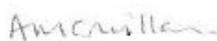
	will therefore not lead to likely significant effects on Habitat Sites.			
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6. Summary and conclusions

- 6.1 The draft East Suffolk Cycling and Walking Strategy identifies potential cycling and walking infrastructure opportunities across East Suffolk. The Strategy focusses on the identification of new infrastructure opportunities rather than the maintenance of existing infrastructure. It provides context and information to support future detailed infrastructure proposals and inform decision making. The recommendations in the Strategy are not prescriptive proposals, but high-level opportunities.
- 6.2 The focus of the majority of the recommendations is on the improvement of existing routes, through the introduction of new infrastructure such as modal filters, signage, and cycle lanes and/or resurfacing. The Strategy is clear that the implementation of any of the draft measures is likely to need to be informed by further evidence gathering and detailed discussions with the Local Highways Authority and that further assessments may also be required including (where necessary) Habitat Regulation Assessment.
- 6.3 Implementation of the East Suffolk Cycling and Walking Strategy Document will not lead to likely significant effects on protected Habitat sites.
- 6.4 Natural England were consulted on a draft of this Screening Statement as statutory nature conservation body. Their response is contained in Appendix 4.

Signed:

Dated: 16th September 2022



Andrea McMillan
Planning Manager (Policy, Delivery and Specialist Services)
East Suffolk Council

Appendix 1: Sources of background information

East Suffolk Council – Suffolk Coastal Local Plan (September 2020)

East Suffolk Council – Waveney Local Plan (March 2019)

Local Plan for the Broads 2015- 2036 (May 2019)

Habitats Regulations Assessment for the Suffolk Coastal Local Plan at Final Draft Plan stage (incorporating Main Modifications) (May 2020)

The Habitats Regulations Assessment of the Waveney Local Plan (December 2018)

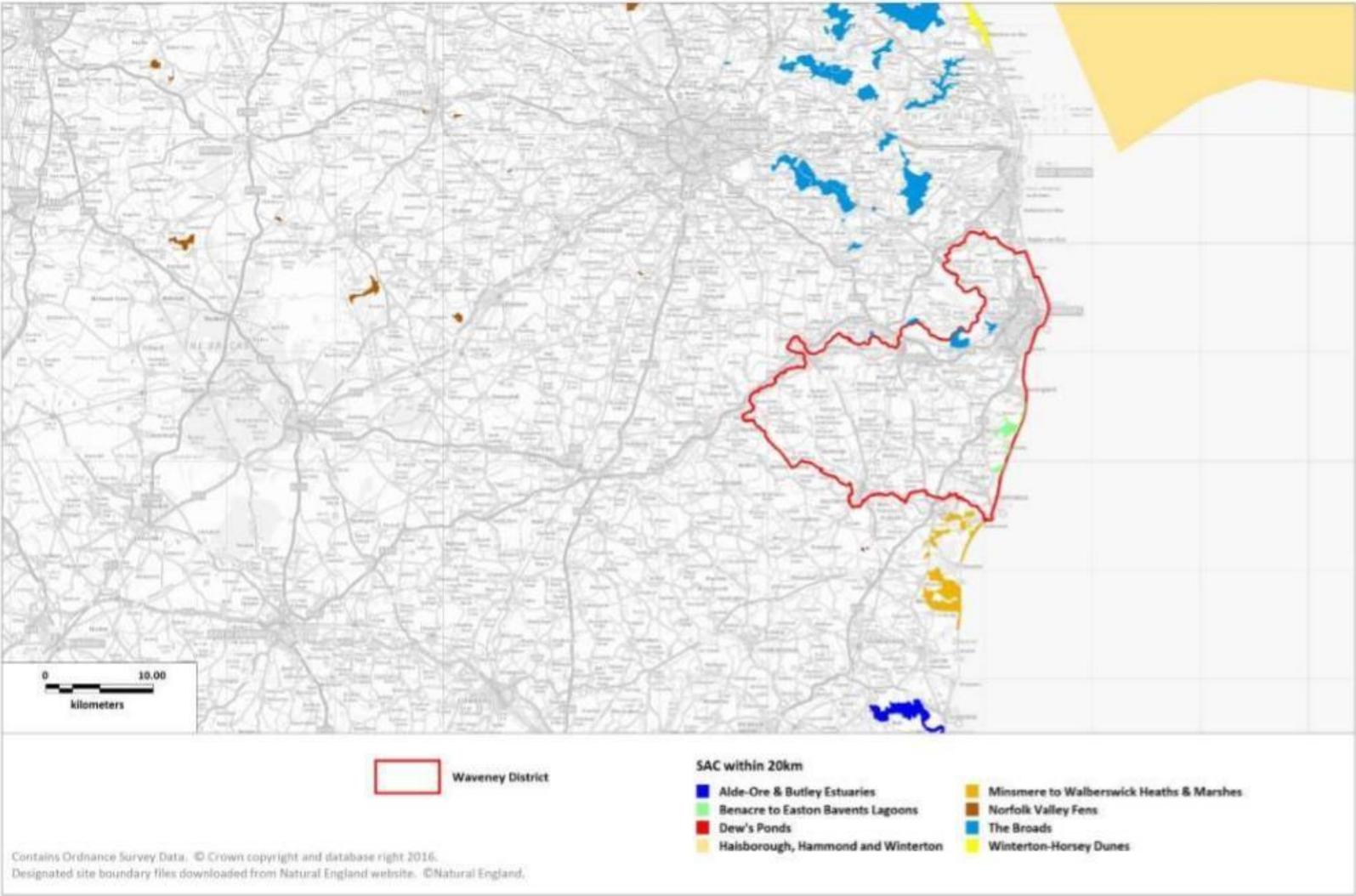
Habitat Regulations Assess of the Local Plan for the Broads (January 2019)

Habitats Regulations Assessment Recreational Disturbance Avoidance and Mitigation Strategy for Ipswich Borough, Babergh District, Mid Suffolk District and East Suffolk Councils (May 2019)

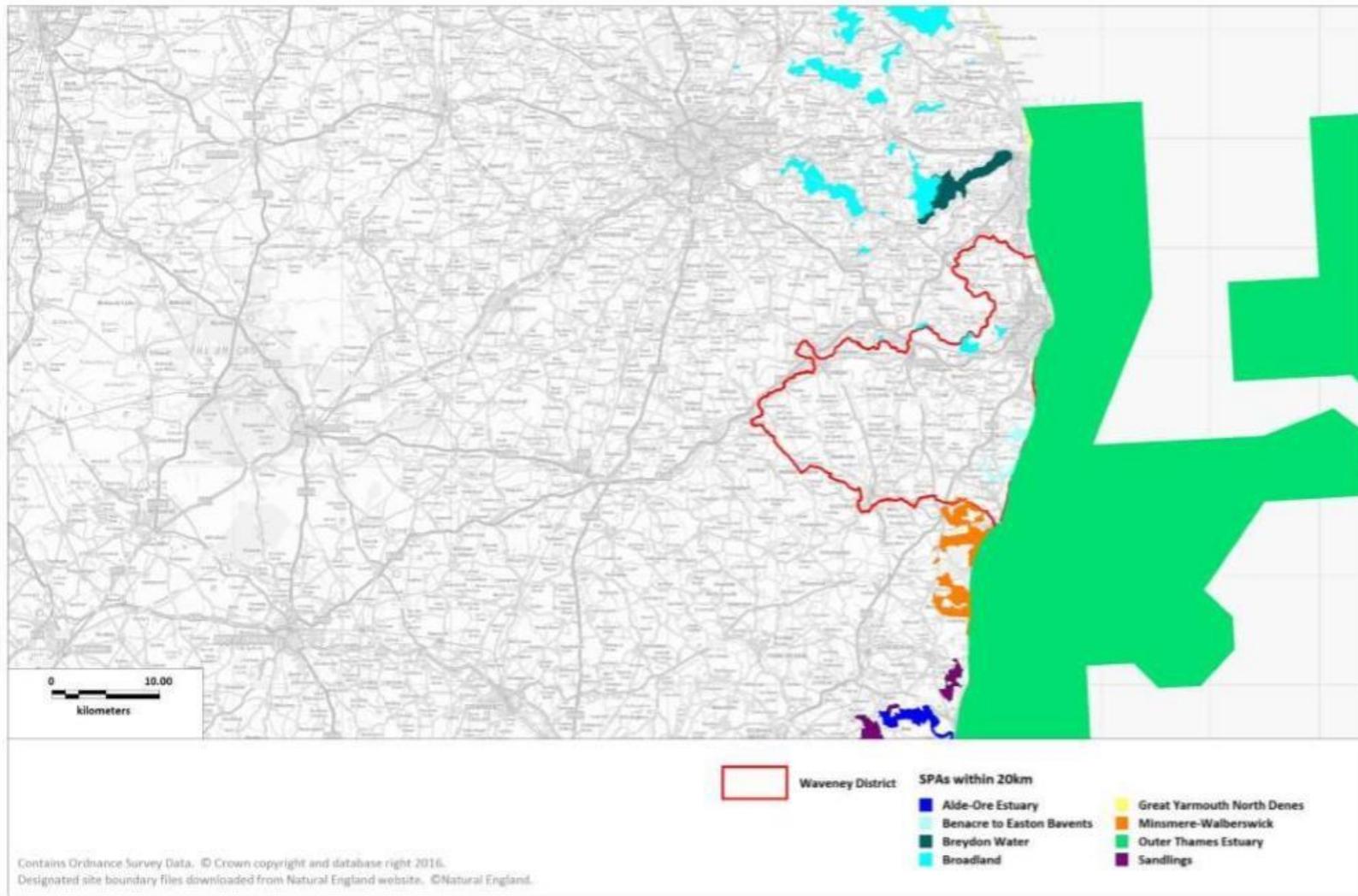
Appendix 2: Locations of Habitat sites

Habitat sites within 20km of the East Suffolk Council-
Waveney Local Plan Area

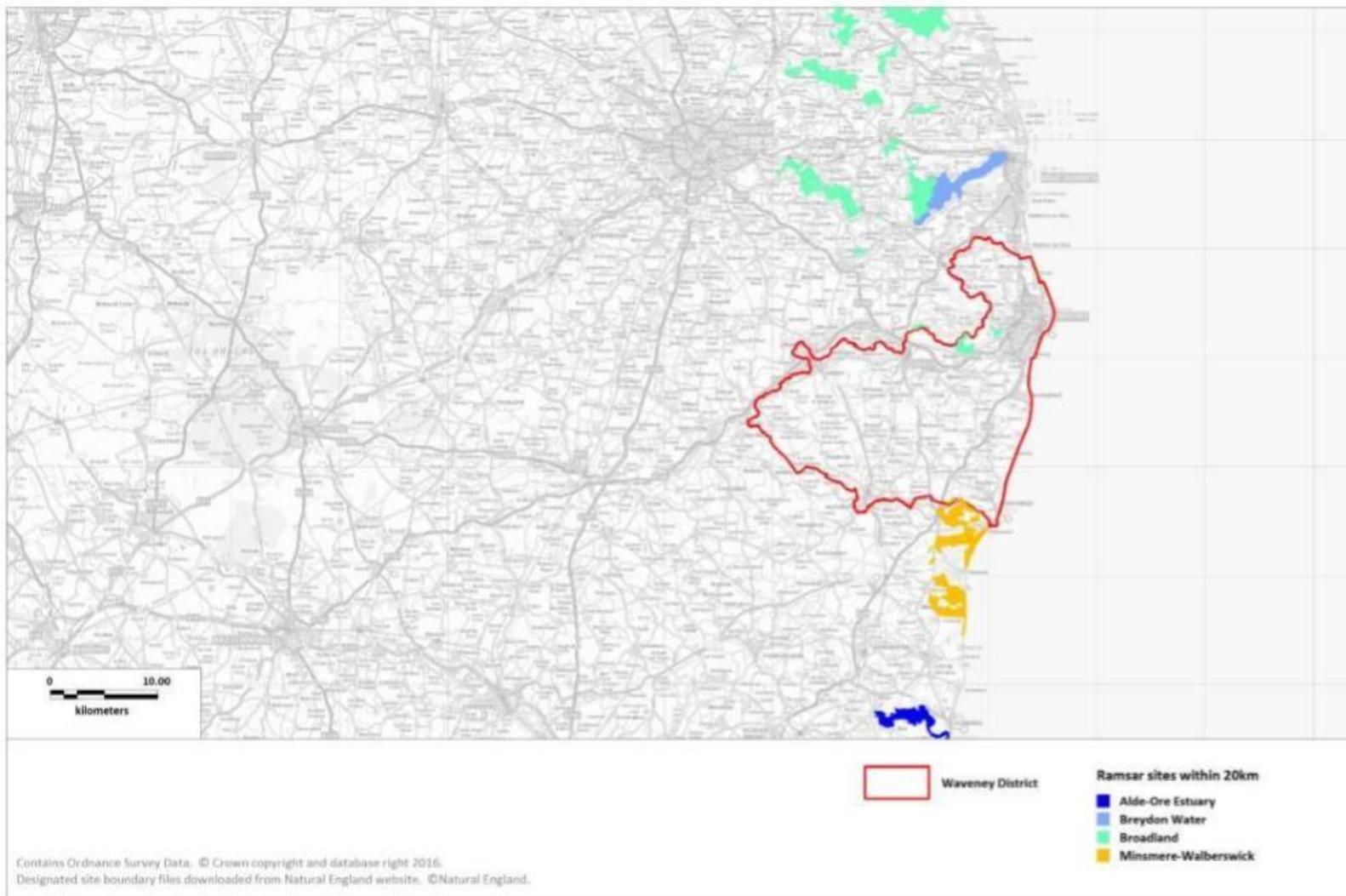
Map 1: SACs



Map 2: SPAs

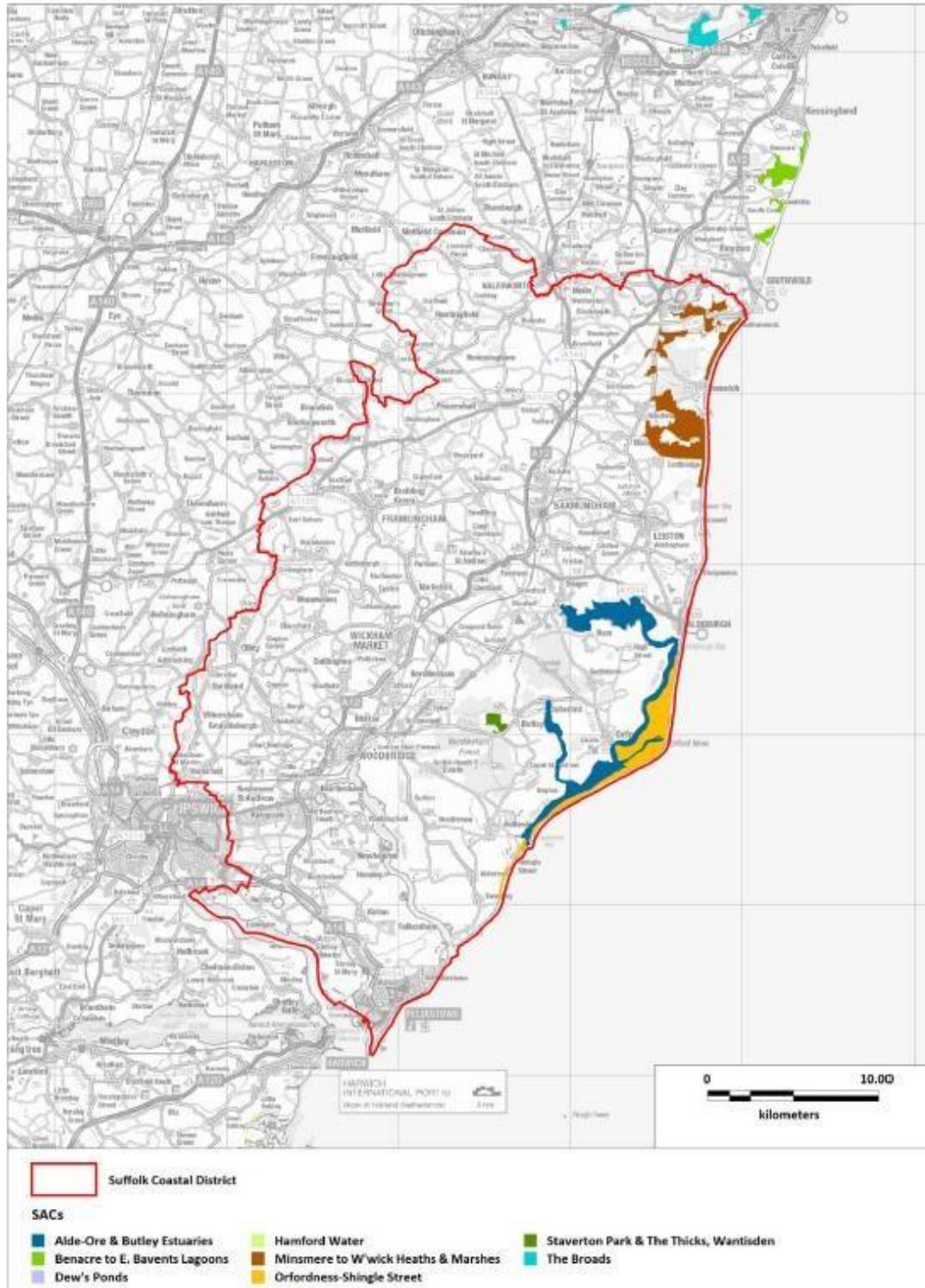


Map 3: Ramsar Sites



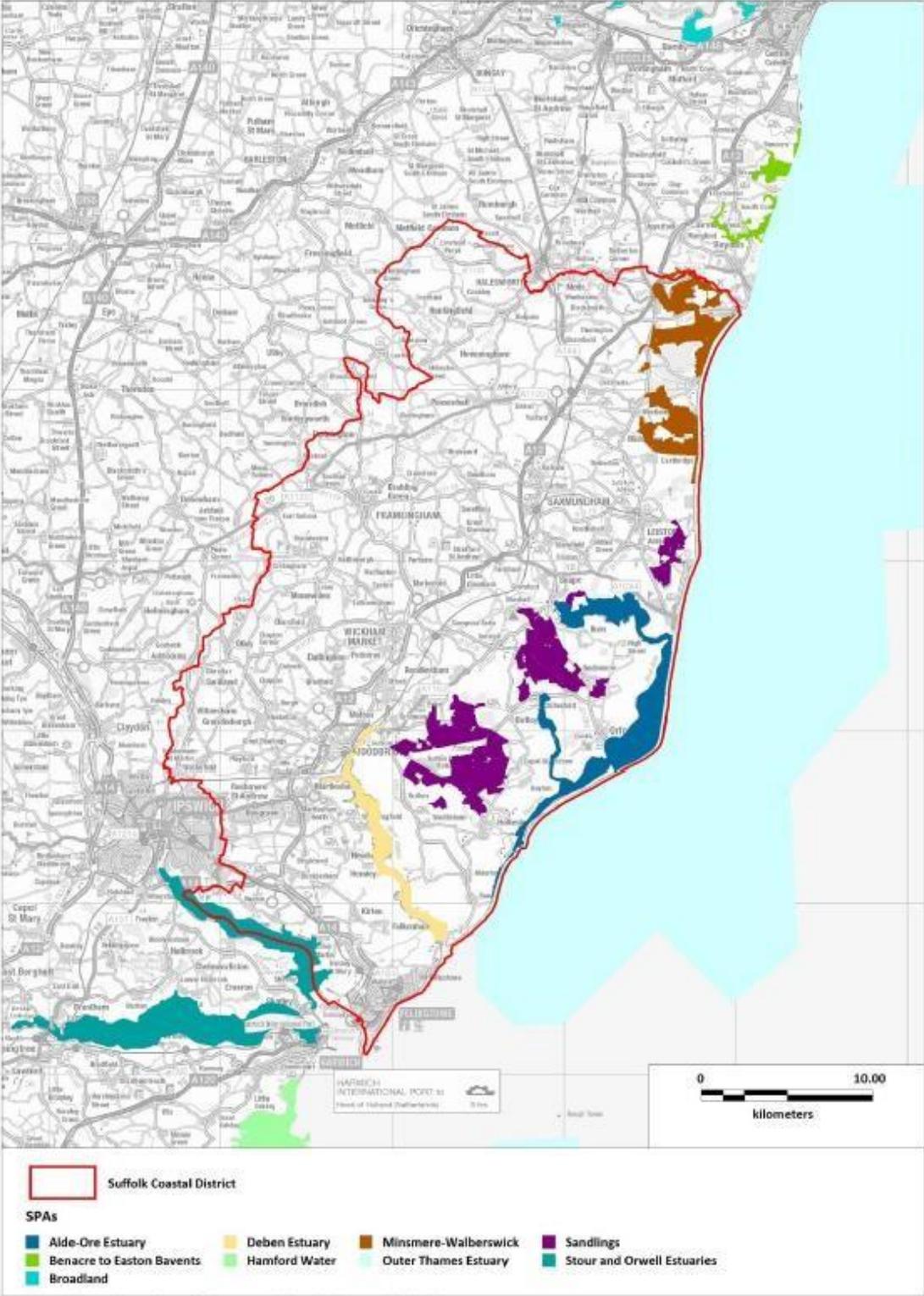
Habitat sites within 20km of the East Suffolk Council- Suffolk Coastal Local Plan Area

Map 1: SAC sites where boundary within 20km of Suffolk Coastal District



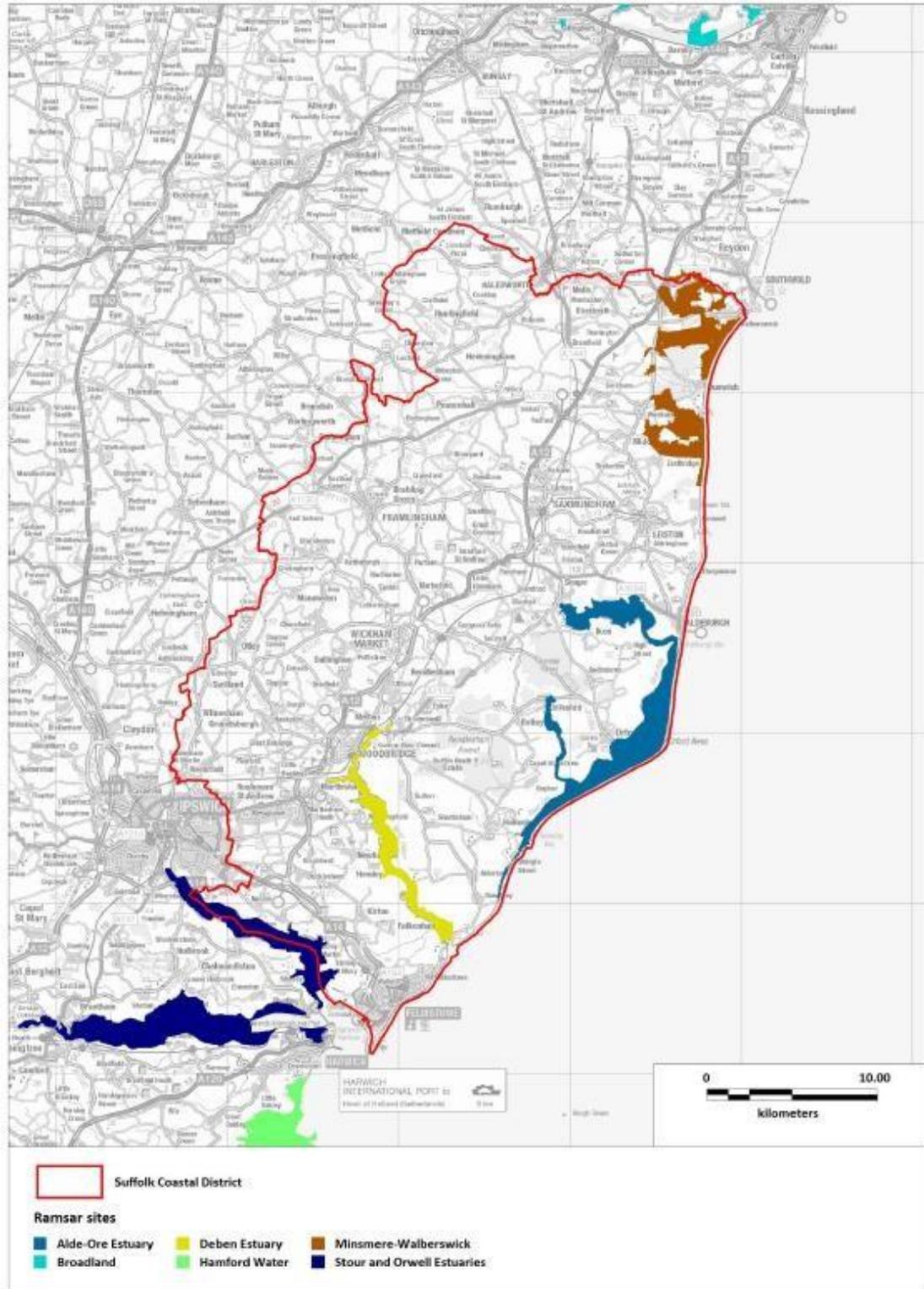
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Map 2: SPA sites where boundary within 20km of Suffolk Coastal District



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 Contains Natural England Information © Natural England and Database Right. All rights Reserved.

Map 3: Ramsar sites where boundary within 20km of Suffolk Coastal District



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Appendix 3: Relevant Habitat sites

Name	Qualifying features	Conservation Objectives	Pressure and threats (as summarised in the Habitats Regulations Assessment for the Suffolk Coastal Local Plan at Final Draft Plan Stage (December 2018))
Special Areas of Conservation			
Alde-Ore and Butley Estuaries	H1130:Estuaries H1140: Mudflats and sandflats not covered by seawater at low tide; Intertidal mudflats and sandflats H1330: Atlantic salt meadows	Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the Favourable Conservation Status of its Qualifying Features by maintaining or restoring: The extent and distribution of qualifying natural habitats; The structure and function (including typical species) of qualifying natural habitats; The supporting processes on which qualifying natural habitats rely.	Hydrological changes, public access/disturbance, inappropriate coastal management, coastal squeeze, inappropriate pest control, changes in species distributions, invasive species, air pollution, fisheries (commercial marine and estuarine) (Alde-Ore and Butley Estuaries SAC and Alde-Ore SPA)
Benacre to Easton Bavents Lagoons	H1150# Coastal lagoons, A195(B) <i>Sterna albifrons</i> : Little tern A021(B) <i>Botaurus stellaris</i> : Great bittern A081(B) <i>Circus aeruginosus</i> :	Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the Favourable Conservation Status of its Qualifying Features, by maintaining or restoring;	Public access/disturbance, water pollution, physical modification, changes in species distributions, fisheries (marine and estuarine).

	Eurasian marsh harrier	The extent and distribution of qualifying natural habitats, and the structure and function (including typical species) of qualifying natural habitats, and, the supporting processes on which qualifying natural habitats rely.	
Dew's Ponds	S1166 Triturus cristatus: Great crested newt	Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the Favourable Conservation Status of its Qualifying Features, by maintaining or restoring; The extent and distribution of the habitats of qualifying species, The structure and function of the habitats of qualifying species, The supporting processes on which the habitats of qualifying species rely, The populations of qualifying species, and, The distribution of qualifying species within the site.	None identified
Minsmere to Walberswick Heaths and Marshes	H4030 European dry heaths H1210 Annual vegetation of drift lines H1220 Perennial vegetation of stony banks A052(B) Anas crecca: Eurasian teal A021(B) Botaurus stellaris: Great bittern	Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the Favourable Conservation Status of its Qualifying Features, by maintaining or restoring; The extent and distribution of qualifying natural habitats and habitats,	Coastal squeeze, public access/disturbance, changes in species distributions, invasive species, inappropriate pest control, air pollution, water pollution, deer, fisheries (commercial marine and estuarine)

	<p>A081(B) Circus aeruginosus: Eurasian marsh harrier A082(NB) Circus cyaneus: Hen harrier A224(B) Caprimulgus europaeus: European nightjar A056(B) Anas clypeata: Northern shoveler A056(NB) Anas clypeata: Northern shoveler A051(B) Anas strepera: Gadwall A051(NB) Anas strepera: Gadwall A132(B) Recurvirostra avosetta: Pied avocet A195(B) Sterna albifrons: Little tern A394(NB) Anser albifrons albifrons: Greater white-fronted goose</p>	<p>The structure and function (including typical species) of qualifying natural habitats, and, The supporting processes on which qualifying natural habitats rely.</p>	
<p>Orfordness- Shingle Street</p>	<p>H1150: Coastal Lagoons H1210: Annual vegetation of drift lines H1220: Perennial vegetation of stony banks; Coastal shingle vegetation outside the reach of waves</p>	<p>Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the Favourable Conservation Status of its Qualifying Features by maintaining or restoring: The extent and distribution of qualifying natural habitats;</p>	<p>Not identified in Suffolk Coastal Final Draft Local Plan HRA.</p>

		The structure and function (including typical species) of qualifying natural habitats; and The supporting processes on which qualifying natural habitats rely.	
Staverton Park and The Thicks, Wantisden	H9190: Old acidophilous oak woods with <i>Quercus robur</i> on sandy plains; Dry oak-dominated woodland	Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the Favourable Conservation Status of its Qualifying Features by maintaining or restoring: The extent and distribution of qualifying natural habitats; The structure and function (including typical species) of qualifying natural habitats; and The supporting processes on which qualifying natural habitats rely.	Woodland management, disease, atmospheric pollution.
The Broads	H7210# Calcareous fens with <i>Cladium mariscus</i> and species of the <i>Caricion davallianae</i> S1016 <i>Vertigo moulinsiana</i> : Desmoulin's whorl snail H7230 Alkaline fens H6410 <i>Molinia</i> meadows on calcareous, peaty or clayey-siltladen soils (<i>Molinion caeruleae</i>) H91E0# Alluvial forests with <i>Alnus</i>	Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the Favourable Conservation Status of its Qualifying Features, by maintaining or restoring; The extent and distribution of qualifying natural habitats and habitats of qualifying species,	Water pollution, climate change, invasive species, siltation, inappropriate water levels, hydrological changes, water abstraction, change in land management, inappropriate ditch management, inappropriate scrub control, changes in species distributions, public access/disturbance, undergrazing, drainage, direct impact from 3rd party

	<p>glutinosa and Fraxinus excelsior (Alno-Padion, Alnion incanae, Salicion albae) H7140 Transition mires and quaking bogs H3140 Hard oligo-mesotrophic waters with benthic vegetation of Chara spp H3150 Natural eutrophic lakes with Magnopotamion or Hydrocharition-type vegetation S1355 Lutra lutra: Otter S1903 Liparis loeselii: Fen orchid S4056 Anisus vorticulus: Little ramshorn whirlpool snail</p>	<p>The structure and function (including typical species) of qualifying natural habitats, The structure and function of the habitats of qualifying species, The supporting processes on which qualifying natural habitats and the habitats of qualifying species rely, The populations of qualifying species, and, The distribution of qualifying species within the site.</p>	
Special Protection Areas			
<p>Alde-Ore Estuary (also Ramsar site)</p>	<p>A081: Eurasian marsh harrier (breeding) A132: Pied avocet (non-breeding) A132: Pied avocet (breeding) A151: Ruff (non-breeding) A162: Common redshank (nonbreeding) A183: Lesser black-backed gull (breeding) A191: Sandwich tern (breeding) A195: Little tern (breeding)</p>	<p>Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the aims of the Wild Birds Directive, by maintaining or restoring: The extent and distribution of the habitats of the qualifying features; The structure and function of the habitats of the qualifying features; The supporting processes on which the habitats of the qualifying features rely; The</p>	<p>Hydrological changes, public access/disturbance, inappropriate coastal management, coastal squeeze, inappropriate pest control, changes in species distributions, invasive species, air pollution, fisheries (commercial marine and estuarine) (Alde-Ore and Butley Estuaries SAC and Alde-Ore SPA)</p>

		<p>population of each of the qualifying features; and</p> <p>The distribution of the qualifying features within the site.</p>	
Benacre to Easton Bavents	<p>H1150# Coastal lagoons, A195(B) <i>Sterna albifrons</i>: Little tern A021(B) <i>Botaurus stellaris</i>: Great bittern A081(B) <i>Circus aeruginosus</i>: Eurasian marsh harrier</p>	<p>Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the aims of the Wild Birds Directive, by maintaining or restoring; The extent and distribution of the habitats of the qualifying features, The structure and function of the habitats of the qualifying features, The supporting processes on which the habitats of the qualifying features rely, The population of each of the qualifying features, and, The distribution of the qualifying features within the site.</p>	<p>Public access/disturbance, water pollution, physical modification, changes, in species distributions, fisheries (marine and estuarine).</p>
Broadlands (also Ramsar site)	<p>H7210# Calcareous fens with <i>Cladium mariscus</i> and species of the <i>Caricion davallianae</i> S1016 <i>Vertigo moulinsiana</i>: Desmoulin's whorl snail H7230 Alkaline fens H6410 <i>Molinia</i> meadows on calcareous, peaty or clayey-siltladen soils (<i>Molinion caeruleae</i>)</p>	<p>Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the Favourable Conservation Status of its Qualifying Features, by maintaining or restoring; The extent and distribution of qualifying natural habitats and habitats of qualifying species,</p>	<p>Water pollution, climate change, invasive species, siltation, inappropriate water levels, hydrological changes, water abstraction, change in land management, inappropriate ditch management, inappropriate scrub control, changes in species distributions, public access/disturbance, undergrazing, drainage, direct impact from 3rd party.</p>

	<p>H91E0# Alluvial forests with <i>Alnus glutinosa</i> and <i>Fraxinus excelsior</i> (<i>Alno-Padion</i>, <i>Alnion incanae</i>, <i>Salicion albae</i>) H7140 Transition mires and quaking bogs H3140 Hard oligo-mesotrophic waters with benthic vegetation of <i>Chara</i> spp H3150 Natural eutrophic lakes with Magnopotamion or Hydrocharition-type vegetation S1355 <i>Lutra lutra</i>: Otter S1903 <i>Liparis loeselii</i>: Fen orchid S4056 <i>Anisus vorticulus</i>: Little ramshorn whirlpool snail</p>	<p>The structure and function (including typical species) of qualifying natural habitats, The structure and function of the habitats of qualifying species, The supporting processes on which qualifying natural habitats and the habitats of qualifying species rely, The populations of qualifying species, and, The distribution of qualifying species within the site.</p>	
<p>Deben Estuary (also Ramsar site)</p>	<p>A046a: Dark bellied brent goose (non-breeding) A132: Pied avocet (non-breeding)</p>	<p>Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the aims of the Wild Birds Directive, by maintaining or restoring: The extent and distribution of the habitats of the qualifying features; The structure and function of the habitats of the qualifying features; The supporting processes on which the habitats of the qualifying features rely; The</p>	<p>Coastal squeeze, disturbance to birds, water and air pollution.</p>

		<p>population of each of the qualifying features; and The distribution of the qualifying features within the site.</p>	
Outer Thames Estuary	<p>A001: Red-throated Diver (Nonbreeding) A195: Common Tern (Breeding) A193: Little Tern (Breeding)</p>	<p>Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the aims of the Wild Birds Directive, by maintaining or restoring: The extent and distribution of the habitats of the qualifying features; The structure and function of the habitats of the qualifying features; The supporting processes on which the habitats of the qualifying features rely; The population of each of the qualifying features; and The distribution of the qualifying features within the site.</p>	<p>Not identified in Suffolk Coastal Final Draft Local Plan HRA. SIP identifies fisheries.</p>
Minsmere to Walberswick (also Ramsar site)	<p>H4030 European dry heaths H1210 Annual vegetation of drift lines H1220 Perennial vegetation of stony banks A052(B) Anas crecca: Eurasian teal A021(B) Botaurus stellaris: Great bittern A081(B) Circus aeruginosus: Eurasian marsh harrier</p>	<p>Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the aims of the Wild Birds Directive, by maintaining or restoring; The extent and distribution of the habitats of the qualifying features, The structure and function of the habitats of the qualifying features,</p>	<p>Coastal squeeze, public access/disturbance, changes in species distributions, invasive species, inappropriate pest control, air pollution, water pollution, deer, fisheries (commercial marine and estuarine)</p>

	<p>A082(NB) Circus cyaneus: Hen harrier A224(B) Caprimulgus europaeus: European nightjar A056(B) Anas clypeata: Northern shoveler A056(NB) Anas clypeata: Northern shoveler A051(B) Anas strepera: Gadwall A051(NB) Anas strepera: Gadwall A132(B) Recurvirostra avosetta: Pied avocet A195(B) Sterna albifrons: Little tern A394(NB) Anser albifrons albifrons: Greater white-fronted goose</p>	<p>The supporting processes on which the habitats of the qualifying features rely, The population of each of the qualifying features, and, The distribution of the qualifying features within the site.</p>	
<p>Outer Thames Estuary</p>	<p>A001 (W) Gavia stellate Redthroated Diver A195 (B) Sterna hirundo Common Tern A193 (B) Sternula albifrons Little Tern</p>	<p>Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the aims of the Wild Birds Directive, by maintaining or restoring; The extent and distribution of the habitats of the qualifying features, The structure and function of the habitats of the qualifying features, The supporting processes on which the habitats of the qualifying features rely, The population of each of the qualifying features, and,</p>	<p>Not identified in Suffolk Coastal Final Draft Local Plan HRA. SIP identifies fisheries.</p>

		The distribution of the qualifying features within the site.	
Sandlings	A224: European nightjar (breeding) A246: Woodlark (breeding)	Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the aims of the Wild Birds Directive, by maintaining or restoring: The extent and distribution of the habitats of the qualifying features; The structure and function of the habitats of the qualifying features; The supporting processes on which the habitats of the qualifying features rely; The population of each of the qualifying features; and The distribution of the qualifying features within the site.	Changes in species distributions, inappropriate scrub control, deer, air pollution, public access/disturbance.
Stour and Orwell Estuaries (also Ramsar site)	A046a: Dark bellied brent goose (non-breeding) A054: Northern pintail (nonbreeding) A132: Pied avocet (non-breeding) A141: Grey plover (non-breeding) A143: Red knot (non-breeding) A149: Dunlin (non-breeding) A156: Black-tailed godwit (nonbreeding) A162: Common redshank (nonbreeding) Waterbird assemblage	Ensure that the integrity of the site is maintained or restored as appropriate, and ensure that the site contributes to achieving the aims of the Wild Birds Directive, by maintaining or restoring: The extent and distribution of the habitats of the qualifying features; The structure and function of the habitats of the qualifying features; The supporting processes on which the habitats of the qualifying features rely; The population of each of the qualifying features; and	Coastal squeeze, disturbance to birds, air pollution and new development.

		The distribution of the qualifying features within the site.	
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Appendix 4: Natural England Consultation Response

Date: 12 November 2021
Our ref: 372722

Anthony.Taylor@eastsoffolk.gov.uk
BY EMAIL ONLY



Hombeam House
Crewe Business Park
Electra Way
Crewe
Cheshire
CW1 8GJ

T 0300 060 3900

Dear Mr. Taylor

Planning Consultation: Screening for the Strategic Environmental Assessment (SEA) and Habitats Regulation Assessment (HRA) for the Draft East Suffolk Cycling & Walking Strategy

Thank you for your consultation on the above dated 01 November 2021 which was received by Natural England on the same date.

Natural England is a non-departmental public body. Our statutory purpose is to ensure that the natural environment is conserved, enhanced, and managed for the benefit of present and future generations, thereby contributing to sustainable development.

Screening Opinion Request: Strategic Environmental Assessment and Habitats Regulation Assessment

Habitats Regulation Assessment

Natural England notes that your authority, as competent authority under the provisions of the Habitats Regulations, has screened the proposal to check for the likelihood of significant effects. Natural England is a statutory consultee on the appropriate assessment stage of the Habitats Regulations Assessment process, and a competent authority should have regard to Natural England's advice.

Your assessment concludes that the proposal can be screened out from further stages of assessment because significant effects are unlikely to occur, either alone or in combination. On the basis of the information provided, Natural England concurs with this view.

Strategic Environmental Assessment

Where plans could have significant environmental effects, they may require a Strategic Environmental Assessment (SEA) under the Environment Assessment of Plans and Programmes Regulations 2004 (as amended). Further guidance on deciding whether the proposals are likely to have significant environmental effects and the requirements for consulting Natural England on SEA are set out in the [planning practice guidance](#).

Planning practice guidance also outlines that if an appropriate assessment is required for your neighbourhood plan this will also engage the need for a SEA. One of the basic conditions that will be tested by the independent examiner is whether the neighbourhood plan is compatible with European obligations, including those under the SEA Directive. Where a SEA is required it should be prepared in accordance with [regulation 12](#) of the SEA Regulations.

It is our advice, on the basis of the material supplied with the consultation, that, in so far as our

strategic environmental interests (including but not limited to statutory designated sites, landscapes and protected species, geology and soils) are concerned, that there are unlikely to be significant environmental effects from the proposed plan and therefore we concur with your conclusion that an SEA is not required.

Natural England advises that where opportunities for cycling and walking enhancement have been identified in proximity to European sites particularly which would lead to increased recreation, careful consideration is given to potential implications of increased recreational pressures on these sites.

Natural England would encourage close collaboration with the Suffolk RAMs strategy to ensure that where increased recreational usage of these sites may take place there is sufficient mitigation to ensure vulnerable features are protected. For example where a new or improved cycle path intersects an entrance into a designated area, there could signage which highlights the designations and the relevant vulnerable features.

This advice has been provided on a draft plan and Natural England recognises that there may be further iterations within which new or altered proposals may feature. Consequently this advice constitutes the best advice that can be given based on the information provided so far. Its quality and detail is dependent upon the quality and depth of the information which has been provided. It does not constitute a statutory response or decision, which will be made by Natural England acting corporately in its role as statutory consultee to the competent authority after an application has been submitted. The advice given is therefore not binding in any way and is provided without prejudice to the consideration of any statutory consultation response or decision which may be made by Natural England in due course. The final judgement on any proposals by Natural England is reserved until an application is made and will be made on the information then available, including any modifications to the proposal made after receipt of discretionary advice. All pre-application advice is subject to review and revision in the light of changes in relevant considerations, including changes in relation to the facts, scientific knowledge/evidence, policy, guidance or law. Natural England will not accept any liability for the accuracy, adequacy or completeness of, nor will any express or implied warranty be given for, the advice. This exclusion does not extend to any fraudulent misrepresentation made by or on behalf of Natural England.

Natural England are happy to discuss any of the advice provided in this email.

Further general advice on neighbourhood planning and the natural environment, including natural environment information sources is provided in Annex I.

For any further consultations on your plan, please contact: consultations@naturalengland.org.uk.

Yours Faithfully

Niall Walkden
Senior Adviser | Sustainable Development | Norfolk and Suffolk Team

Annex 1 - Neighbourhood planning and the natural environment: information, issues and opportunities

Natural environment information sources

The [Magic](http://magic.defra.gov.uk/)¹ website will provide you with much of the nationally held natural environment data for your plan area. The most relevant layers for you to consider are: Agricultural Land Classification, Ancient Woodland, Areas of Outstanding Natural Beauty, Local Nature Reserves, National Parks (England), National Trails, Priority Habitat Inventory, public rights of way (on the Ordnance Survey base map) and Sites of Special Scientific Interest (including their impact risk zones). Local environmental record centres may hold a range of additional information on the natural environment. A list of local record centres is available [here](#)².

Priority habitats are those habitats of particular importance for nature conservation, and the list of them can be found [here](#)³. Most of these will be mapped either as Sites of Special Scientific Interest, on the Magic website or as Local Wildlife Sites. Your local planning authority should be able to supply you with the locations of Local Wildlife Sites.

National Character Areas (NCAs) divide England into 159 distinct natural areas. Each character area is defined by a unique combination of landscape, biodiversity, geodiversity and cultural and economic activity. NCA profiles contain descriptions of the area and statements of environmental opportunity, which may be useful to inform proposals in your plan. NCA information can be found [here](#)⁴.

There may also be a local landscape character assessment covering your area. This is a tool to help understand the character and local distinctiveness of the landscape and identify the features that give it a sense of place. It can help to inform, plan and manage change in the area. Your local planning authority should be able to help you access these if you can't find them online.

If your neighbourhood planning area is within or adjacent to a National Park or Area of Outstanding Natural Beauty (AONB), the relevant National Park/AONB Management Plan for the area will set out useful information about the protected landscape. You can access the plans on from the relevant National Park Authority or Area of Outstanding Natural Beauty website.

General mapped information on soil types and Agricultural Land Classification is available (under 'landscape') on the [Magic](http://magic.defra.gov.uk/)⁵ website and also from the [LandIS website](http://www.landis.org.uk/)⁶, which contains more information about obtaining soil data.

Natural environment issues to consider

The [National Planning Policy Framework](#)⁷ sets out national planning policy on protecting and enhancing the natural environment. [Planning Practice Guidance](#)⁸ sets out supporting guidance.

Your local planning authority should be able to provide you with further advice on the potential impacts of your plan or order on the natural environment and the need for any environmental assessments.

¹ <http://magic.defra.gov.uk/>

² <http://www.nbn-nfor.org.uk/nfor.php>

³ <http://web.archive.nationalarchives.gov.uk/20140711133551/http://www.naturalengland.org.uk/ourwork/conservation/biodiversity/protectandmanage/habsandpeciesimportance.aspx>

⁴ <https://www.gov.uk/government/publications/national-character-area-profiles-data-for-local-decision-making>

⁵ <http://magic.defra.gov.uk/>

⁶ <http://www.landis.org.uk/index.cfm>

⁷ https://assets.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/807247/NPPF_Feb_2019_revised.pdf

⁸ <http://planningguidance.planningportal.gov.uk/blog/guidance/natural-environment/>

Landscape

Your plans or orders may present opportunities to protect and enhance locally valued landscapes. You may want to consider identifying distinctive local landscape features or characteristics such as ponds, woodland or dry stone walls and think about how any new development proposals can respect and enhance local landscape character and distinctiveness.

If you are proposing development within or close to a protected landscape (National Park or Area of Outstanding Natural Beauty) or other sensitive location, we recommend that you carry out a landscape assessment of the proposal. Landscape assessments can help you to choose the most appropriate sites for development and help to avoid or minimise impacts of development on the landscape through careful siting, design and landscaping.

Wildlife habitats

Some proposals can have adverse impacts on designated wildlife sites or other priority habitats (listed [here⁹](#)), such as Sites of Special Scientific Interest or [Ancient woodland¹⁰](#). If there are likely to be any adverse impacts you'll need to think about how such impacts can be avoided, mitigated or, as a last resort, compensated for.

Priority and protected species

You'll also want to consider whether any proposals might affect priority species (listed [here¹¹](#)) or protected species. To help you do this, Natural England has produced advice [here¹²](#) to help understand the impact of particular developments on protected species.

Best and Most Versatile Agricultural Land

Soil is a finite resource that fulfils many important functions and services for society. It is a growing medium for food, timber and other crops, a store for carbon and water, a reservoir of biodiversity and a buffer against pollution. If you are proposing development, you should seek to use areas of poorer quality agricultural land in preference to that of a higher quality in line with National Planning Policy Framework para 171. For more information, see our publication [Agricultural Land Classification: protecting the best and most versatile agricultural land¹³](#).

Improving your natural environment

Your plan or order can offer exciting opportunities to enhance your local environment. If you are setting out policies on new development or proposing sites for development, you may wish to consider identifying what environmental features you want to be retained or enhanced or new features you would like to see created as part of any new development. Examples might include:

- Providing a new footpath through the new development to link into existing rights of way.
- Restoring a neglected hedgerow.
- Creating a new pond as an attractive feature on the site.
- Planting trees characteristic to the local area to make a positive contribution to the local landscape.
- Using native plants in landscaping schemes for better nectar and seed sources for bees and birds.
- Incorporating swift boxes or bat boxes into the design of new buildings.
- Think about how lighting can be best managed to encourage wildlife.
- Adding a green roof to new buildings.

⁹<http://webarchive.nationalarchives.gov.uk/20140711133551/http://www.naturalengland.org.uk/ourwork/conservation/biodiversity/protectandmanage/habsandspeciesimportance.aspx>

¹⁰ <https://www.gov.uk/guidance/ancient-woodland-and-veteran-trees-protection-surveys-licences>

¹¹ <http://webarchive.nationalarchives.gov.uk/20140711133551/http://www.naturalengland.org.uk/ourwork/conservation/biodiversity/protectandmanage/habsandspeciesimportance.aspx>

¹² <https://www.gov.uk/protected-species-and-sites-how-to-review-planning-proposals>

¹³ <http://publications.naturalengland.org.uk/publication/35012>

You may also want to consider enhancing your local area in other ways, for example by:

- Setting out in your plan how you would like to implement elements of a wider Green Infrastructure Strategy (if one exists) in your community.
- Assessing needs for accessible greenspace and setting out proposals to address any deficiencies or enhance provision.
- Identifying green areas of particular importance for special protection through Local Green Space designation (see [Planning Practice Guidance on this](#)¹⁴).
- Managing existing (and new) public spaces to be more wildlife friendly (e.g. by sowing wild flower strips in less used parts of parks, changing hedge cutting timings and frequency).
- Planting additional street trees.
- Identifying any improvements to the existing public right of way network, e.g. cutting back hedges, improving the surface, clearing litter or installing kissing gates) or extending the network to create missing links.
- Restoring neglected environmental features (e.g. coppicing a prominent hedge that is in poor condition, or clearing away an eyesore).

¹⁴ <http://planningguidance.planningportal.gov.uk/blog/guidance/open-space-sports-and-recreation-facilities-public-rights-of-way-and-local-green-space/local-green-space-designation/>

Equality Impact Assessment Screening Opinion

Cycling and Walking Strategy

September 2022



Introduction

1. It is the Council's duty under the Equality Act 2010 to undertake an Equality Impact Analysis at the time of formulating a decision, drafting a report, designing or amending a policy. This will ensure that the Council is considering and taking positive action where possible to promote access to services for all their communities, including their wider communities. The Equality Impact Assessment Screening Assessment will assess whether there is any impact upon any of the groups with protected characteristics under the Equalities Act, which are listed in the table below. If an adverse impact upon any of these groups is identified then a full Equalities Impact Assessment will be required.
2. The East Suffolk Cycling and Walking Strategy identifies potential cycling and walking opportunities across the District. The strategy focuses on the identification of opportunities to provide new cycling and walking infrastructure rather than the maintenance of existing infrastructure. The East Suffolk Cycling and Walking Strategy is not an SPD but it provides context and information to support detailed infrastructure proposals and inform decision making to support cycling, walking and equestrian use.
3. The Cycling and Walking Strategy is divided into three main chapters.
4. 1: Introduction. This chapter explains the purpose of the Cycling and Walking Strategy. It also explains how the Cycling and Walking Strategy was prepared and how it will be implemented.
5. 2: Implementation and Monitoring of the Strategy. This chapter explains how the recommendations of the Cycling and Walking Strategy will be delivered. The monitoring sector explains how the success in delivering these recommendations will be measured.
6. 3: Policy Context. This chapter describes the national and Local Plan policies that inform the preparation and delivery of the Cycling and Walking Strategy.
7. 4: Recommendations. This chapter identifies key cycling and walking routes that should be delivered. This chapter includes information about the following key routes:
 - Key Corridors
 - Ipswich to Melton Key Corridor
 - Ipswich to Felixstowe Key Corridor
 - Martlesham to Felixstowe Key Corridor
 - Felixstowe Internal Routes Key Corridor

Lowestoft Internal Key Corridor
Lowestoft to Hopton Key Corridor
Lowestoft to Kessingland Key Corridor
Lowestoft to Bungay Key Corridor
Leisure Routes
Site Allocations Recommendations
Community Recommendations.

8. The Cycling and Walking Strategy includes reference to the following policies at the national, county and local, or District level.

National Level

National Planning Policy Framework (NPPF)

District Level

The Cycling and Walking Strategy will help to implement policies contained in the Suffolk Coastal and Waveney Local Plans, all of which have been subject to an Equality Impact Assessment screening. The following Local Plan policies will be implemented by the Cycling and Walking Strategy:

Suffolk Coastal Local Plan (2020) Policy SCLP7.1: Sustainable Transport
Waveney Local Plan Policy (2019) policy WLP8.21: Sustainable Transport

Neighbourhood Plan policies may contain policies about the provision of cycling and walking infrastructure.

9. The Equality Act 2010 lists nine protected characteristics: age; disability; gender reassignment; marriage and civil partnership; pregnancy and maternity; race; religion or belief; sex; sexual orientation. East Suffolk Council has added a tenth characteristic, socio-economic deprivation, in addition to the nine protected characteristics listed in the legislation. This reflects the pockets of deprivation that exist across East Suffolk.

Screening of impact on different groups

	Groups	Likely Impact (positive/negative/no impact)	Reason for your decision
a	Age (Includes safeguarding issues)	Positive	The provision of improved cycle routes and footpaths will benefit those of different ages, including those who are elderly who will be better able to use improved footpaths and cycle lanes. Consultation documents will be available online, in libraries and in the Council's customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending hard copies by post, if needed. The consultation materials will be available to people of all ages and therefore will not discriminate in terms of age.
b	Disability	Positive	The provision of improved cycle and pedestrian routes will benefit those with a disability, who will be better able to use improved footpaths and cycle lanes. Consultation documents will be available online, in libraries and in the Council's customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending

			hard copies by post, if needed. The consultation will therefore not discriminate against those with a disability.
C	Gender reassignment	No impact	The Cycling and Walking Strategy provides guidance about improved cycling and walking infrastructure. It will therefore not discriminate those who have undergone gender reassignment. Consultation documents will be available online, in libraries and in the Council's customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending hard copies by post, if needed. This consultation will therefore not discriminate against those who have undergone gender reassignment.
D	Marriage and Civil Partnership	No impact	The Cycling and Walking Strategy provides guidance about improved cycling and walking infrastructure. It will therefore not discriminate against those who are married or in a civil partnership. Consultation documents will be available online, in libraries and in the Council's customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending hard copies by post, if needed. This consultation will therefore not discriminate

			against those who are married or in a civil partnership.
E	Pregnancy and maternity	No impact	The Cycling and Walking Strategy provides guidance about improved cycling and walking infrastructure. It will therefore not discriminate against those who are pregnant or on maternity leave. Consultation documents will be available online, in libraries and in the Council's customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending hard copies by post, if needed. This consultation will therefore not discriminate against those who are pregnant or on maternity leave.
F	Race	No impact	The Cycling and Walking Strategy provides guidance about improved cycling and walking infrastructure. It will therefore not discriminate against those from different racial backgrounds. Consultation documents will be available online, in libraries and in the Council's customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending hard copies by post, if needed. The consultation will therefore not discriminate against those from any racial background.

G	Religion or Belief	No impact	<p>The Cycling and Walking Strategy provides guidance about improved cycling and walking infrastructure. It will therefore not discriminate against those from different religions or beliefs. Consultation documents will be available online, in libraries and in the Council’s customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending hard copies by post, if needed. The consultation will therefore not discriminate against those of different religious beliefs.</p>
H	Sex	No impact	<p>The Cycling and Walking Strategy provides guidance about improved cycling and walking infrastructure. It will therefore not discriminate against those of different sexes. Consultation documents will be available online, in libraries and in the Council’s customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending hard copies by post, if needed. The consultation will therefore not discriminate against those of different sexual identities.</p>
I	Sexual orientation	No impact	<p>The Cycling and Walking Strategy provides guidance about improved cycling and walking infrastructure.</p>

			<p>It will therefore not discriminate against those of different sexual orientations. Consultation documents will be available online, in libraries and in the Council’s customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending hard copies by post, if needed. The consultation will therefore not discriminate against those of different sexual orientations.</p>
J	Socio-economic deprivation	No impact	<p>The Cycling and Walking Strategy provides guidance about improved cycling and walking infrastructure. It will therefore not discriminate against those who are experiencing social and economic deprivation. Consultation documents will be available online, in libraries and in the Council’s customer service centres and the Council has offered assistance in its publicity material for those unable to access the documents in the ways set out above. This may involve sending hard copies by post, if needed. The consultation will therefore not discriminate against those who are experiencing socio-economic deprivation.</p>

Consultation and Engagement

The initial consultation ran for seven weeks from 19th October to 7th December 2020. The Cycling and Walking Strategy was published at the same time as consultations on the draft Statement of Community Involvement (SCI) and the draft Recreational Disturbance Avoidance and Mitigation Strategy (RAMS) SPD. Those on the Council's planning policy consultation database were contacted directly by email or letter. Included in the consultation were:

- Suffolk County Council
- Highways England
- Sport England
- Local Cycling groups
- Town and Parish Councils
- Sustrans
- Cycling UK

The consultations were further advertised using posters, a press release, and social media posts. Over 800 individual responses were received.

Hard copies of the document were also made available free of charge by contacting the Planning Policy and Delivery team as the usual locations for viewing documents were closed to the public during this time, due to the pandemic.

A second public consultation will take place from 1st November to 10th January 2022 on the Draft Cycling and Walking Strategy. This Cycling and Walking Strategy consultation will be held at the same time as the Sustainable Construction SPD and Affordable Housing SPD (note the consultations on the Sustainable Construction SPD and Affordable Housing SPD have an earlier end date end date of 13th December). Consultation letters and emails covering all three consultations will be sent to consultees on the Council's planning policy mailing list. Posters and press releases covering all three consultations will take place, and information will be contained in the Council's Planning and Building Control Newsletter. The planning policy mailing list includes Town and Parish Councils, Suffolk County Council, East Suffolk Councillors, neighbouring district Councils including the Broads Authority, developers, agents, landowners, business associations, civic societies, infrastructure providers), and members of the public.

Copies of consultation documents will be available online, in libraries and in the Council's customer service centres. Due to the ongoing Covid-19 pandemic, contact details have

been provided in publicity material along with an offer of assistance in accessing the consultation material, should anyone be unable to view the documents in any of the ways set out above.

In view of the ongoing Covid-19 pandemic, the Council has set out measures to enable safe participation in the consultation and to ensure that those who wish to engage in the consultation are not disadvantaged. For those unable to view the consultation documents online, in libraries or in the Customer Service Centres, hard copies will be made available on request (free of charge) by post. In view of these measures the Council does not consider that this consultation will disadvantage any of the groups covered by this EQIA screening exercise.

Presentation in Different Languages

As part of a ten-week period of formal consultation, the document will be published on the Council's website, with hard copies available on request for those unable to access it online. The document may be requested in a different language. When such requests are received the Customer Services Team will be involved with ensuring this request is actioned.

Proposed Changes

The Council will analyse responses received during the public consultation and will make any appropriate changes as a result of comments received.

Conclusion

This EQIA screening exercise shows that the Cycling and Walking Strategy will not negatively impact upon any protected group or those experiencing socio-economic deprivation. Therefore, a full EQIA assessment is not considered necessary.

This EQIA screening exercise was revisited in the light of comments received during public consultation and subsequent changes that were made. It was concluded that this EQIA screening exercise is still relevant.



CABINET

Tuesday, 04 October 2022

Subject	Budget Monitoring Report Quarter 2 2022/23
Report by	Councillor Maurice Cook Cabinet Member with responsibility for Resources
Supporting Officer	Brian Mew Chief Finance Officer and Section 151 Officer Brian.Mew@eastsoffolk.gov.uk

Is the report Open or Exempt?	OPEN
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Category of Exempt Information and reason why it is NOT in the public interest to disclose the exempt information.	Not applicable
Wards Affected:	All Wards

Purpose and high-level overview

Purpose of Report:

This report provides Cabinet with an overview of the Council's projected financial performance for the financial year 2022/23 in respect of the General Fund, Reserves, Housing Revenue Account (HRA) and the Capital Programme.

Options:

None.

Recommendation/s:

1. That the Council's financial position for the period April to September 2022 together with projections of the full year outturn and reserve balances be noted.
2. That the areas of financial risk identified, the impact of which will be reflected in the Draft Medium-Term Financial Strategy due to be considered by Cabinet in November, be noted.
3. That the additional financial commitments approved in the first half of the year and their impact on the General Fund and reserve levels be noted.

Corporate Impact Assessment

Governance:

As set out in the Council's Finance Procedure Rules, the Chief Finance Officer is responsible for preparing and submitting reports on revenue budget forecasts to Cabinet. This report to Cabinet is intended to facilitate monitoring of the council's financial performance.

ESC policies and strategies that directly apply to the proposal:

- East Suffolk Strategic Plan
- East Suffolk Medium Term Financial Strategy
- Capital Programme
- Housing Revenue Account
- East Suffolk Treasury Management Strategy and Treasury Management Policy
- Annual Governance Statement
- Finance Procedure Rules
- Financial Management Code

Environmental:

There are no environmental implications identified.

Equalities and Diversity:

An Equality Impact Assessment is not required as the recommendations of this report do not require changes in policy and service delivery.

Financial: Detailed financial analysis and our commentary is included in Appendix A.
Human Resources: There are no HR implications directly arising from this report.
ICT: There are no ICT implications identified.
Legal: There are no legal implications identified.
Risk: There are no risks identified.
External Consultees: None.

Strategic Plan Priorities

Select the priorities of the Strategic Plan which are supported by this proposal: <i>(Select only one primary and as many secondary as appropriate)</i>		Primary priority	Secondary priorities
T01	Growing our Economy		
P01	Build the right environment for East Suffolk	<input type="checkbox"/>	<input type="checkbox"/>
P02	Attract and stimulate inward investment	<input type="checkbox"/>	<input type="checkbox"/>
P03	Maximise and grow the unique selling points of East Suffolk	<input type="checkbox"/>	<input type="checkbox"/>
P04	Business partnerships	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P05	Support and deliver infrastructure	<input type="checkbox"/>	<input type="checkbox"/>
T02	Enabling our Communities		
P06	Community Partnerships	<input type="checkbox"/>	<input type="checkbox"/>
P07	Taking positive action on what matters most	<input type="checkbox"/>	<input type="checkbox"/>
P08	Maximising health, well-being, and safety in our District	<input type="checkbox"/>	<input type="checkbox"/>
P09	Community Pride	<input type="checkbox"/>	<input type="checkbox"/>
T03	Maintaining Financial Sustainability		
P10	Organisational design and streamlining services	<input type="checkbox"/>	<input type="checkbox"/>
P11	Making best use of and investing in our assets	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P12	Being commercially astute	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P13	Optimising our financial investments and grant opportunities	<input type="checkbox"/>	<input checked="" type="checkbox"/>
P14	Review service delivery with partners	<input type="checkbox"/>	<input type="checkbox"/>
T04	Delivering Digital Transformation		
P15	Digital by default	<input type="checkbox"/>	<input type="checkbox"/>
P16	Lean and efficient streamlined services	<input type="checkbox"/>	<input type="checkbox"/>
P17	Effective use of data	<input type="checkbox"/>	<input type="checkbox"/>
P18	Skills and training	<input type="checkbox"/>	<input type="checkbox"/>
P19	District-wide digital infrastructure	<input type="checkbox"/>	<input type="checkbox"/>

T05	Caring for our Environment		
P20	Lead by example	<input type="checkbox"/>	<input type="checkbox"/>
P21	Minimise waste, reuse materials, increase recycling	<input type="checkbox"/>	<input type="checkbox"/>
P22	Renewable energy	<input type="checkbox"/>	<input type="checkbox"/>
P23	Protection, education, and influence	<input type="checkbox"/>	<input type="checkbox"/>
XXX	Governance		
XXX	How ESC governs itself as an authority	<input checked="" type="checkbox"/>	<input type="checkbox"/>
How does this proposal support the priorities selected?			
The report forms an important element of good governance, ensure financial sustainability and putting our assets to good use. It highlights areas of financial risk which will impact on the Council's Medium Term Financial Strategy (MTFS).			

Background and Justification for Recommendation

1	Background facts		
1.1	In February 2022, the Council approved a balanced budget for the 2022/23 financial year and set the Band D rate of Council Tax.		
1.2	During the last six months, to enable the Council to respond to changing circumstances, some variations to the approved General Fund budget, expenditure from earmarked reserves, and commitments to growth have been made by Cabinet and Council. These are detailed in Appendix A1.		
1.3	The Council is facing unprecedented financial challenges and uncertainties due to rising utility and fuel costs and a Local Government pay award yet to be agreed.		
1.4	Hybrid working of Council offices means utility costs for admin buildings have not significantly continue but will continue to be monitored.		
1.5	Indoor Leisure has the greatest exposure to utility price increases. The Council works closely with its Leisure Partners to monitor performance and presently cost pressures are being managed within existing budgets. There is growing uncertainty however, as to how long this position can be maintained.		
1.6	Exposure to rising fuel costs is via the Council's Operations Partnership. Presently this cost pressure is mitigated by savings on waste disposal costs.		
1.7	A 2022/23 pay offer has been made by the National Employers for Local Government but has not yet been agreed. Budget pressures are calculated on the current pay offer.		
1.8	To date there has been no confirmation of additional funding from Central Government to support Local Government with the above cost pressures.		
1.9	The Prime Minister and the Chancellor will be delivering a 'mini budget' on Friday 23 September 2022 to set out the Government's plans to tackle the cost of living. This may have implications for the figures presented in Appendix A. Any significant items emerging from Friday and of relevance to this report will be provided ahead of the Cabinet meeting and appended to this paper.		
1.10	The Council continues to face recruitment difficulties, particularly in areas of specialism, and some agency/contractor support is required. This is giving rise to a significant underspend on staff costs which will offset the cost pressure from the pending pay award. This is cover in Section 1.4 of Appendix A.		

2 Current position	
2.1	As at Quarter 2, an increase of £1.297m is forecast on the General Fund budget.
2.2	The current year revenue budget pressure is primarily due to inflation and the pending pay award.
2.3	Additional interest income of £0.500m is anticipated due to increased rates and is significant to help alleviate identified budget pressures.
2.4	Fees and Charges income to Quarter 2 was £10.49m. This is a small favourable variance of £0.030m when compared to the budget, £10.52m.
2.5	Car parking income has not returned to pre-pandemic levels and a reduction in the baseline income is forecast. Subject to further analysis of this area, a reduction to the annual income budget of £0.200m is currently projected.
2.6	Planning and Building Control income is ahead of the budget profile to Quarter 2 by over £0.400m. However, the current year forecast remains cautious on projecting a surplus year end position.
2.7	There are items which have been identified as having potential revenue budget implications for this financial year and over the MTFS period but cannot yet be quantified or quantified with a degree of certainty but are important to be highlighted. These are covered in Section 4 of Appendix A. As far as future years and next year's budget are concerned, the Draft Medium Term Financial Strategy 2023/24 – 2026/27 will be considered by Cabinet at its meeting on 1 November 2022.
2.8	Section 5 in Appendix A continues to show the Council in a strong position in respect of its reserves and balances, and able to manage short-term budget fluctuations over the period of uncertainty.

3 How to address current situation	
3.1	Earmarked reserves intended to balance year-to-year fluctuations such as the In-Year Savings Reserve and the Business Rates Equalisation Reserve will be utilised if necessary to mitigate any further financial risks arising in the second half of 2022/23 and to ensure that the outturn position is balanced.

4 Reason/s for recommendation	
4.1	The key focus of the Financial Sustainability theme is the development, monitoring and achievement of the savings and income increases required to address the Council's projected budget gaps of the MTFS.

Appendices

Appendices:	
Appendix A	Financial Performance Monitoring Report Quarter 2 2022-23
Appendix A1	Approved Budget Changes as at Quarter 2
Appendix A2	Variances to the Approved Budget as at Quarter 2
Appendix B	Update – Government Announcements 23 September 2022

Background reference papers:	
None	

Budget Monitoring Report

For the period 1 April 2022 - 30 September 2022

Contents

1.	General Fund	
1.1	Quarter 2 Budget Monitoring Overview.....	2
1.2	Approved Growth in Financial Commitments.....	3
1.3	Income from Fees and Charges.....	3-4
1.4	Establishment Costs.....	4-5
1.5	Council Tax and Business Rates Income.....	6
2.	Housing Revenue Account (HRA)	
2.1	Quarter 2 Budget Monitoring Overview.....	6-7
2.2	HRA Reserves.....	7-8
3.	Treasury Management.....	8
4.	Budget Implications to be Monitored.....	9
5.	General Fund Reserves.....	10
5.1	Earmarked Reserves.....	10-11
5.2	Port Health – Reserve.....	11-12
6.	Capital Programme	
6.1	General Fund.....	12-13
6.2	Housing Revenue Account (HRA).....	13
	Appendix A1 - Approved Budget Changes as at Quarter 2.....	14
	Appendix A2 - Variances to the Approved Budget as at Quarter 2.....	15

1. General Fund

1.1 Quarter 2 Budget Monitoring Overview

The summary in Figure 1 below presents a comparison of the original budget for 2022/23 and the projected net budget. The Approved Budget changes represent budget changes approved by Cabinet and Council since the Original Budget was approved in February 2022. Variances to Approved Budget are in-year impacts (costs and savings) which have been identified to date.

As of Quarter 2, the projection for year end is an increase against the Original Budget of £1.297m. Further details on the changes to the budget are provided in Appendix A1 and A2.

Figure 1 - General Fund Budget Variances Quarter 2

Service Areas	Original Budget	Approved Budget		Variances to Approved Budget	Forecast Outturn
	2022/23 £'000	Changes 2022/23 £'000	Approved Budget 2022/23 £'000	2022/23 £'000	2022/23 £'000
Senior and Corporate Management	2,667	38	2,705		2,705
Economic Development and Regeneration	3,351	260	3,611		3,611
Financial Services and Other Financial Transactions	728		728		728
Revenue and Benefits	2,372		2,372	(20)	2,352
Digital and Programme Management	2,977	66	3,043		3,043
Internal Audit Services	543		543		543
Human Resources	864		864		864
Legal and Democratic Services	2,312		2,312		2,312
Planning and Coastal Management	4,412		4,412		4,412
Customer Experience Strategic Management	2,362		2,362		2,362
Operations	11,008	21	11,029	627	11,656
Communities	2,460		2,460		2,460
Environmental Services and Port Health	1,505	153	1,658		1,658
Housing Services	2,389	94	2,483		2,483
Cross Cutting Items:					0
22/23 Pay Awards (ESC Employees)				1,000	1,000
Inflation - Utilities				250	250
Net Cost of Service	39,950	632	40,582	1,857	42,439
Non-Cost of Service Expenditure Adjustments (e.g. DRF,MRP)	2,951		2,951	(58)	2,893
Other Operating Expenditure (e.g. Parish Precepts, Levies)	6,996		6,996		6,996
Financing and Investment Net Income and Expenditure	(90)		(90)	(500)	(590)
Non-Specific Grant Income (e.g. NHB, Section 31 Grant)	(10,310)		(10,310)		(10,310)
Net Movements on Reserves	(12,132)	(354)	(12,486)		(12,486)
Net Budget Expenditure After Reserve Movements	27,365	278	27,643	1,299	28,942
Financed By:					
Council Tax Income (District Council)	(15,688)		(15,688)		(15,688)
Council Tax Income (Town & Parish Precepts)	(6,749)		(6,749)		(6,749)
Share of (Surplus)/Deficit on Collection Fund - Council Tax	(223)		(223)		(223)
Business Rates	(3,317)		(3,317)	(280)	(3,597)
Government Support Grants (RSG, Lower Tier, Rural Services, etc.)	(1,388)		(1,388)		(1,388)
Total Financing	(27,365)	0	(27,365)	(280)	(27,645)
Budget Gap	0	278	278	1,019	1,297

1.2 Approved Growth in Financial Commitments

Over the first half of the year, Cabinet and Council has approved various General Fund revenue budget requests to support communities, economic regeneration, and staff resourcing. This amounts to £3.770m of revenue spend for the period 2022/23 to 2026/27 and is summarised in Figure 2 below. Of the requests, £0.782m is to be funded from Earmarked Reserves and £0.682m from additional income. The remaining balance of £2.306m is growth to the General Fund over the MTFS period. Whilst the future years are not reflected as budget variances in this year, it is worth noting the increase to the budget for later years.

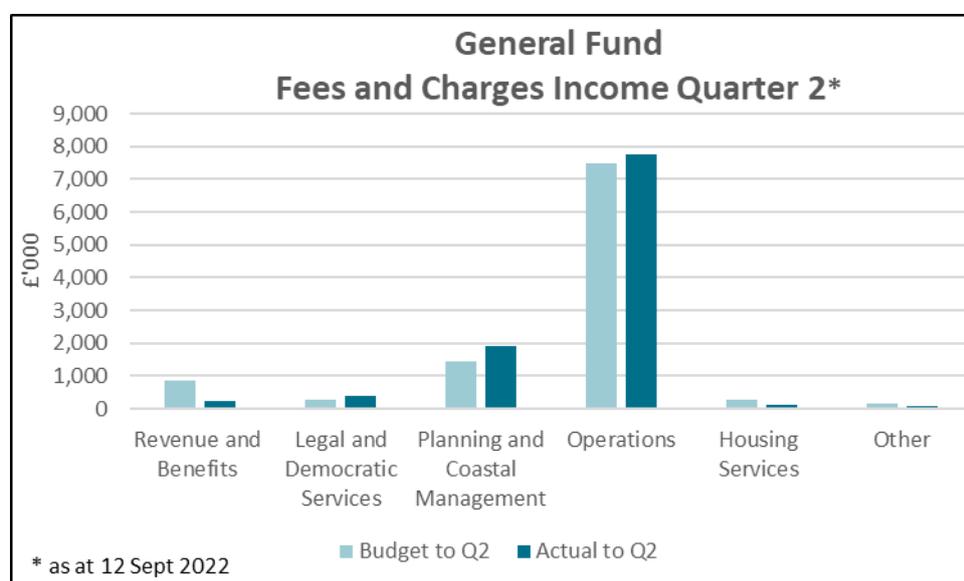
Figure 2 – Approved growth to date in 2022/23

	2022/23 £'000	2023/24 £'000	2024/25 £'000	2025/26 £'000	2026/27 £'000	Total £'000
Additional Budget Requirement	806	1,025	925	507	507	3,770
Funded (Reserves/Income)	(528)	(518)	(418)	0	0	(1,464)

1.3 Income from Fees and Charges

Figure 3 below compares the budget with projected income to the end of Quarter 2. Actual income to Quarter 2 was £10.49m. This is a small favourable variance of £0.030m when compared to the Budget, £10.52m. It is important not to view fees and charges in isolation, as there are typically costs associated with new income sources.

Figure 3 – General Fund Fees and Charges Income Quarter 2



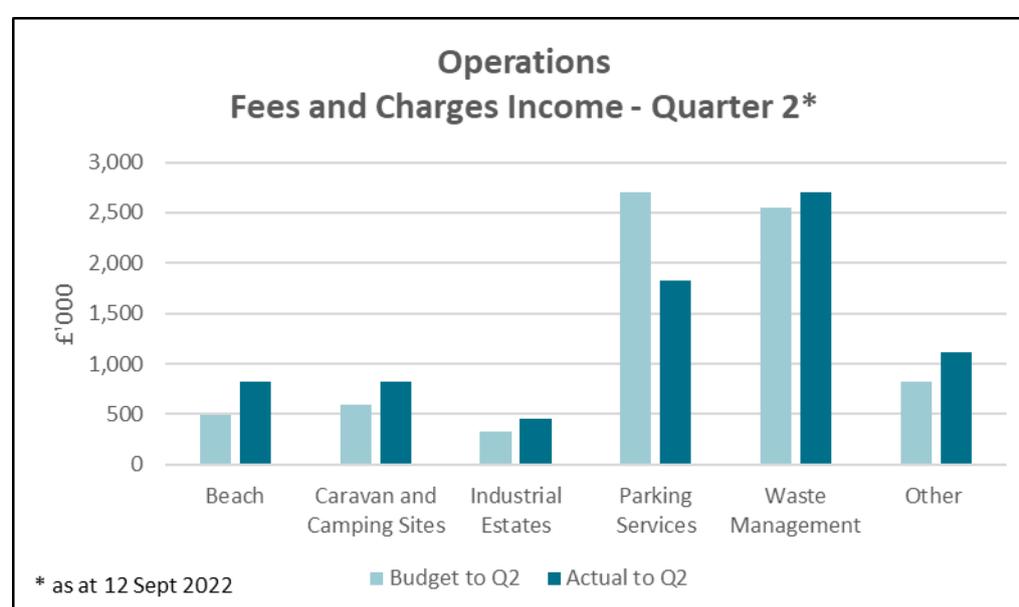
Income from Revenue and Benefits relates to Housing Benefit Overpayment and mirrors a favourable variance on the expenditure to the Housing Benefit Subsidy.

Planning and Building Control income is ahead of the budget profile to Quarter 2 by over £0.400m. However, the current year forecast remains cautious on projecting a surplus year end position.

As illustrated by the above chart, Operations is the largest source of income from Fees and Charges. The total annual income budget for Fees and Charges is in the region of £17.8m, and Operations accounts for over 60% of that income. Figure 4 below provides a breakdown of Quarter 2 Fees and Charges income within Operations. Due to the timing of this report, actual data is as at 12 September 2022, and it should be noted that further income will be received during the rest of the month.

Car parking income has not returned to pre-pandemic levels and a reduction in the baseline income is forecast. Subject to further analysis of this area, a reduction to the annual income budget of £0.200m is currently projected. Beach and Caravan and Camping Site income will include income received in advance from 2021/22 and the budget profiling should be revisited to ensure this is reflected correctly.

Figure 4 – Operations Fees and Charges Income Quarter 2



1.4 Establishment Costs

Figure 5 below shows actual staffing cost for the period April to August 2022 compared to the Budget for the same period. Due to the timing of this report, data was not available in respect of September staffing costs. As can be seen from the table, actual costs are significantly underbudget. Primarily this is due to recruitment challenges to fill vacancies, particularly in the areas of Asset Management, Legal, Internal Audit, Planning, and Coastal Management.

Figure 5 – General Fund Establishment Costs Quarter 2

Service Area	Budget to Q2*1 £'000	Actual to Q2*1 £'000	Variance to Q2*2 £'000	Comments
Communities	617	693	76	Externally funded roles
Customer Experience Strategic Management	898	853	(45)	Vacancies
Digital and Programme Management	691	656	(36)	Vacancies
Economic Development and Regeneration	779	565	(214)	Vacancies
Environmental Services	726	664	(62)	Vacancies
Financial Services and Other Financial Transactions	530	520	(11)	Vacant hours
Housing Services	1,019	932	(87)	Vacancies, mainly external funding
Human Resources	254	213	(41)	Corporate Apprenticeship budget*3
Internal Audit Services	246	222	(23)	Vacancies
Legal and Democratic Services	629	573	(55)	Vacancies
Operations	936	732	(204)	Vacancies
Planning and Coastal Management	2,637	2,280	(357)	Vacancies
Revenue and Benefits	1,346	1,236	(109)	Vacancies shared across ARP
Senior and Corporate Management	924	938	14	Corporate Project Support
Total	12,232	11,077	(1,154)	

*1 April to August 2022
*2 (underbudget) / overbudget
*3 Corporate Apprentice budget to be re-allocated

Due to vacancies not filled during the year, it has been necessary for some areas to cover the resourcing shortfall with agency/contractor support. This is currently estimated at £0.400m for the year and will offset an element of the underspend.

The annual budget also includes a provision for vacancy allowance of £0.597m (2% of staffing costing) for 2022/23, which again will partly offset the in-year underspend.

The total 2022/23 General Fund establishment budget for basic pay plus on-costs (Employer National Insurance and Superannuation) is £29.8m. The pay award for 2022/23 is yet to be agreed, however, a forecast of the budgetary impact of the current offer has been estimated at £1.4m. After taking into account the saving from unfilled posts, the estimated net impact of the pay award is £1.0m. This is summarised in the below table.

Figure 6 - General Fund Estimated Net Impact of 2022/23 Pay Award

	£'m
Estimated impact of pay award 2022/23	1.40
Less:	
Estimated vacancy saving for the year*1	(0.90)
Variance on pay award assumption (1.75% actual v 2% assumption)	(0.10)
Add:	
Additional agency/contractor costs (cover for vacancies)	0.40
Growth	0.20
Estimated Net Impact of Pay Award for 2022/23	1.00
*1 This is less than the £1.154m presented in Figure 5 to allow for the vacancy allowance included in the budget and where income is included in the budget for posts not filled.	

1.5 Council Tax and Business Rates Income

Council Tax income variances in-year will appear in the collection fund deficits or surpluses in the following year.

Regarding Business Rates income, there are two forecast budget changes identified at Quarter 2. The Pooling Benefit to the Council is currently estimated at £2.04m, around £0.20m up on the Original Budget. Business Rates income from Renewables is approximately £0.08m higher than the Original Budget of £1.20m. Figure 7 below summarises Council Tax and Business Rates income for the current year.

Figure 7 – 2022/23 Council Tax and Business Rate Income Forecast as at Quarter 2

	Original Budget 2022/23 £'000	Variations to Original Budget £'000	Forecast Outturn 2022/23 £'000
Council Tax Income (District Council)	(15,688)	0	(15,688)
Share of (Surplus) on Collection Fund - Council Tax	(223)	0	(223)
Business Rates	(3,317)	(280)	(3,597)

2. Housing Revenue Account (HRA)

2.1 Quarter 2 Budget Monitoring Overview

The Housing Revenue Account is a ring-fenced account designed to be self-sufficient in its operation and financing of social housing. All its revenues are reinvested back into the housing stock.

The budget summary in Figure 8 below presents a comparison of the Original Budget for 2022/23 and the forecast position as at Quarter 2. To date there have been no in-year HRA approved revenue budget changes by Cabinet or Council. The variances to the Approved Budget are in-year impacts (costs and savings) which have been identified to date. This currently reduces the movement from the HRA Working Balance by £0.961m and does not yet include the outcome of compliance work. The variances identified relate to:

Cost Pressures:

- Estimate for current year pay award £0.200m
- Inflationary pressure from utility costs, estimated at £0.250m
- Due to compliance work being undertaken in the HRA, there are associated consultancy costs and increase the budget requirement by £0.200m.

Additional Income/Savings:

- The HRA also benefits from a share of the additional interest income currently forecast. This is estimated at £0.111m.

- Due to delays in the new build programme, in particular Deben High School, the revenue financing of the capital programme will need to be re-phased to 2023/24. The current budget adjustment is forecast at £1.50m.

Figure 8 – HRA Budget Variances Quarter 2

	Original Budget 2022/23 £'000	Approved Budget Changes 2022/23 £'000	Approved Budget 2022/23 £'000	Variance to Approved Budget 2022/23 £'000	Forecast Outturn 2022/23 £'000
Income					
Dwelling Rent	(19,926)		(19,926)		(19,926)
Non-Dwelling Rent	(160)		(160)		(160)
Service & Other Charges	(1,098)		(1,098)		(1,098)
Leaseholders Charges for Services	(10)		(10)		(10)
Contribution towards Expenditure	(33)		(33)		(33)
Reimbursement of Costs	(276)		(276)		(276)
Interest Income	(169)		(169)	(111)	(280)
Total Income	(21,672)	0	(21,672)	(111)	(21,783)
Expenditure					
Repairs & Maintenance	6,141		6,141		6,141
Supervision & Management	3,683		3,683		3,683
Special Services	2,440		2,440	200	2,640
Rents, Rates and other Charges	146		146		146
Movement in Bad Debt Provision	31		31		31
Contribution to CDC	95		95		95
Capital Charges	3,650		3,650		3,650
Interest Charges	2,106		2,106		2,106
Revenue Contribution to Capital	5,863		5,863	(1,500)	4,363
Transfer to Earmarked Reserves	-		-		-
Total Expenditure	24,155	0	24,155	(1,300)	22,855
Cross Cutting Items:					
22/23 Pay Awards				200	200
Inflation - Utilities				250	250
Movement in the HRA balance	2,483	0	2483	(961)	1,522

2.2 HRA Reserves

In addition to the HRA Working Balance the HRA holds Earmarked Reserves. Figure 9 below, provides a forecast of HRA Reserves and balances as at Quarter 2.

Figure 9 - HRA Reserve Forecast as at Quarter 2

Reserves	Actual April 2022 £'000	Forecast April 2023 £'000	Forecast April 2024 £'000	Forecast April 2025 £'000	Forecast April 2026 £'000
HRA Working Balance	9,670	8,238	7,169	7,814	8,037
HRA Earmarked Reserves					
Debt Repayment Reserve	13,000	13,000	13,000	13,000	13,500
DHP Top Up Reserve	500	500	500	500	500
MMI Reserve	60	60	60	60	60
Impairment/Revaluation Reserve	256	256	256	256	256
Housing Development Reserve	6,300	4,160	2,160	0	0
Brampton Sinking Fund	1	1	1	1	1
Total of HRA Revenue Earmarked Reserves	20,117	17,977	15,977	13,817	14,317
Capital Earmarked Reserves (Major Repairs Reserve)	13,754	8,263	7,275	6,615	6,225

The DHP (Discretionary Housing Payments) Top Up Reserve was established in 2012/13 with the introduction of the Welfare Reform Act 2012. To date no use has been made of the reserve. The Council is currently exploring options how best to use this reserve, whilst retaining the original underlying principal, which was to support HRA tenants facing financial difficulties.

3. Treasury Management

On 4 August 2022, the Bank of England raised the base rate from 1.25% to 1.75% in response to inflation. It is currently predicted that the base rate will rise above 3% by the end of 2022.

Due to the current account interest rate rise, there is the potential for the Council to receive an additional £0.029m of interest income per month depending on the balances held.

The Council's investment portfolio in the short term is looking healthy with both the Property Investment and Multi Asset Funds still performing well.

The combination of factors outlined above indicates interest income to exceed the budget for the year by £0.500m for the General Fund, taking the total interest income to £1.15m. The Housing Revenue Account (HRA) also has a share of interest income, and this is estimated to be an additional £0.111m to the HRA for 2022/23.

Existing borrowing held by the Council is at fixed rates and is therefore not impacted by the current interest rate changes.

4. Budget Implications to be Monitored

There are items which have been identified as having potential budget implications for this financial year and over the MTFs period. Whilst some cannot yet be quantified or quantified with a degree of certainty it is important for them to be highlighted.

Figure 10 - Budget Implications to be Monitored

Budget Area to be Monitored	Areas Impact	Nature of Impact	Timing of Impact
Fuel cost inflation	Operations and HRA - partnership fee.	Increased partnership fee. This cost is currently mitigated within the existing partnership budget due to savings on waste disposal costs.	Current year and potentially the following two years.
Utility Cost inflation	Council owned property, in particular the Leisure Centres	Increased running costs of Council property and support to Leisure Centre Operators may be to be reviewed. Currently estimate £250k for the General Fund and the HRA.	Current year and potentially the following two years.
National pay award for 2022/23 is not yet agreed.	Council staffing costs and its Operations partner.	Current budget assumption is for a 2% increase for Council staffing. Net impact for the General Fund is currently estimated at £1.0m for direct staffing, £0.5m via the Operations Partnership, and £0.2m for HRA staff.	Current year and future years.
Other inflationary pressures on materials, supplies and services, and supply shortages.	Operations Partner and Council contracts (revenue and capital).	Increased contract cost and supplies and services across the Council.	Current year and potentially the following two years.
Workforces pay pressure/grade inflation.	Council staffing costs and Operations partner.	Increased staffing costs. Annual staffing vacancy allowance in the budget is at risk.	Impact being seen in the current year.
Recruitment difficulties	Particular area of specialism.	Agency and contract costs may be required which can be more costly. Currently estimated at just under £0.400m additional cost to the General Fund	Impact being seen in the current year.
Triennial review of the Pension Fund.	Council staffing costs and Operations partner.	Depending on the valuation report of the Pension Fund, employer superannuation rates can increase or decrease. More information on the likely impact should be available at Quarter 3.	Impacts in 2023/24 to 2025/26.
Felixstowe Dock strike action.	Port Health Account.	Loss of income. Currently estimated at £0.014m per strike day.	Impact in the current year.

5. General Fund Reserves

The unallocated General Fund balance is maintained at £6.0m, representing around 5% of budgeted gross expenditure (in the region of £130m).

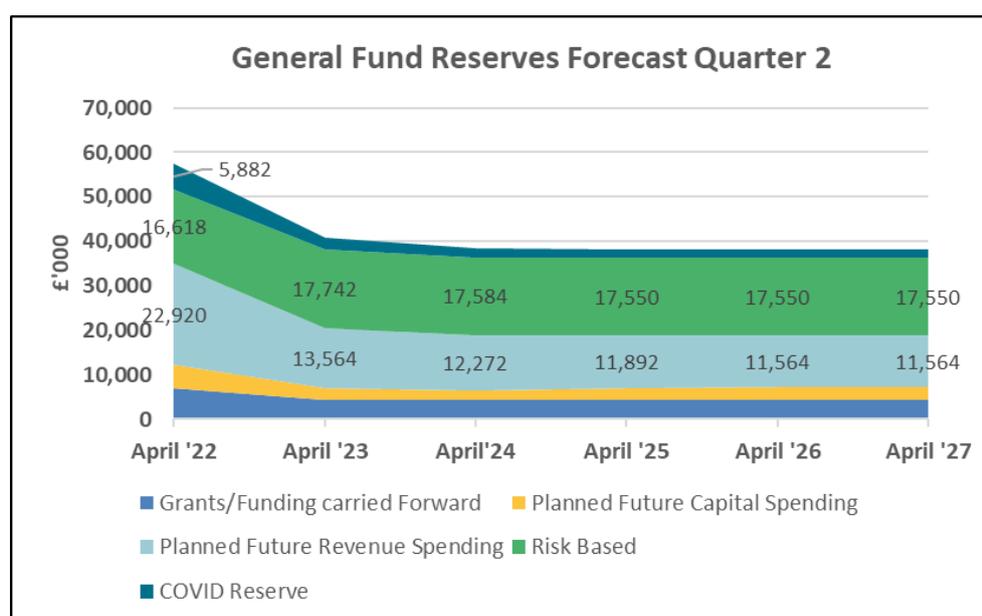
5.1 Earmarked Reserves

The Council holds several General Fund Earmarked Revenue Reserves which have been established to meet known or predicted commitments, and to hold balances of grants and external funding which is committed to future year spend. The Council reviews these reserves to ensure the levels continue to be appropriate and if no longer required, are returned to the General Fund. The total balance of General Fund Earmarked Reserves was £57.6m (excluding Port Health) as of 1 April 2022.

Earmarked Reserves are categorised as follows.

- **Grants/Funding Carried Forward** – this is external funding the Council has received for specific purposes and is drawn down from reserves when spend is incurred.
- **Planned Future Revenue Spending** – Council funding has been set aside for specific service areas and/or projects.
- **Planned Future Capital Spending** – this is revenue funding set aside to provide funding for the capital programme.
- **Risk Based** – Council funding has been set aside for unforeseen budget pressures and fluctuations in budget areas of volatility. This will include for example, pension capital costs, Business Rates income and Housing Benefit Subsidy.
- **Covid Reserve** - Consisting of Section 31 Grants to fund Covid related Business Rate Reliefs and the Local Authority support grant.

Figure 11- General Fund Reserve Forecast Quarter 2



The Covid reserve will be used further in 2022/23 to deal with accounting timing differences related to the pandemic impact on business rates, and to fund post pandemic grant work and recovery initiatives. There is currently in the region of £1m of the Covid support grant being held in the reserve. The balance of this funding will be monitored and transferred to an appropriate reserve at the end of the year if no longer required for post pandemic recovery and work.

Within the Risk Based Group of Reserves there is the Business Rate Equalisation Reserve. The balance on this reserve at the start of 2022/23 was £15.2m with minimal use of this reserve planned in the current year. This reserve can be called upon to balance in-year budget fluctuations.

The Planned Future Revenue Spending Reserves contains some key individual reserves which are used to fund future budget gap pressures and projects and initiatives to support the delivery of the Strategic Plan. These are set out in the Figure12 below.

Figure 12 – Key Reserves Within Planned Future Revenue Spending Reserves

	Actual April 2022 £'000	Forecast April 2023 £'000
In-Year Savings	3,572	2,728
New Homes Bonus	5,580	4,763
Transformation	3,565	1,294

5.2 Port Health – Reserve

In addition to the above, there is a reserve held by Port Health which is allocated as follows:

- **Planned Future Revenue Spending** – This is an unallocated balance, set to cover one year of Port Health costs.
- **Planned Future Capital Spending** – to provide Port Health revenue funding to support capital expenditure, e.g., vehicles and ICT equipment.
- **ICT** – this is a newly created balance as a buffer for expanding the PHILIS service. This balance will hold surpluses from the PHILIS operation to build funds for reinvestment in the service, both revenue and capital.
- **Grants/Funding Carried Forward** – to release grant funding received as an when the relevant spend is incurred. This element of the reserve includes Transition funding. The balance remaining on Transition funding as of 31 March 2022 was £0.822m. A further £0.998m has been received in the year to date, and it is forecast for this funding to be spent by 31 March 2023. The Finance Team are working closely with the Port Health Management Team to monitor use of the Funding.

Figure 13 – Port Health Reserve Forecast as at Quarter 2

Reserves	Actual April 2022 £'000	Forecast April 2023 £'000	Forecast April 2024 £'000	Forecast April 2025 £'000	Forecast April 2026 £'000	Forecast April 2027 £'000
Port Health - Planned Future Revenue Spending	2,071	1,962	2,500	2,500	2,500	2,500
Port Health - Planned Future Capital Spending	613	150	100	50	0	0
Port Health - ICT	400	91	91	91	91	91
Port Health - Grants/Funding Carried Forward	1,230	408	408	408	408	408
Total Earmarked Reserves	4,314	2,611	3,099	3,049	2,999	2,999

Due to the Felixstowe Dock strike action this is a cost to the Port Health Account from loss of income. Currently this is estimated at £0.014m a day. The loss of income due to strike action during August (eight days) is estimated at £0.113m. If the shortfall cannot be mitigated from an underspend in expenditure, the Port Health Reserve will be used.

6. Capital Programme

6.1 General Fund

As part of the budget setting process and the Capital Programme for the financial years 2022/23 to 2025/26 was approved by Full Council in January 2022. Since the approval of significantly. The current economic climate being a main factor, with schemes in the programme experiencing substantial inflationary cost pressures and phasing issues. It was therefore considered appropriate to bring forward a mid-year review of the General Fund Capital Programme for consideration by Cabinet on 6 September 2022 and Full Council on 28 September.

Figure 14: Extract from Report: 'Capital Programme Review 2022/23 to 2025/26'

SUMMARY - GENERAL FUND PROGRAMME	2022/23	2022/23	2023/24	2023/24	2024/25	2024/25	2025/26	2025/26	2022/23 to 2025/26	2022/23 to 2025/26
	£000 Original Budget	£000 Revised Budget	£000 Original Total	£000 Revised Total						
Capital Expenditure										
Economic Development & Regeneration	4,943	5,586	11,409	17,260	10,289	10,140	9,259	9,110	35,900	42,096
Environmental Services & Port Health	406	406	50	50	50	50	50	50	556	556
Financial Services	400	400	0	0	0	0	0	0	400	400
ICT - Digital & Programme Management	527	927	450	550	250	250	250	250	1,477	1,977
Operations	18,707	13,901	34,155	32,205	34,105	30,505	505	22,305	87,472	98,916
Planning & Coastal Management	19,432	19,432	22,009	31,109	36,774	36,774	36,200	36,200	114,415	123,515
Housing Improvement	1,000	1,000	1,000	1,000	1,000	1,000	1,500	1,500	4,500	4,500
Long Term Debtors	1,000	1,500	0	0	0	0	0	0	1,000	1,500
Total Capital Expenditure	46,415	43,152	69,073	82,174	82,468	78,719	47,764	69,415	245,720	273,460
Financed By:-										
Borrowing	19,205	8,595	27,500	21,100	30,200	26,400	2,700	24,200	79,605	80,295
Capital Receipt	0	1,000	0	4,000	0	0	0	0	0	5,000
Contributions	0	0	0	0	0	0	0	0	0	0
Grants	24,210	25,534	37,218	50,379	49,363	49,214	44,259	44,110	155,050	169,237
Reserves	3,000	8,023	4,355	6,695	2,905	3,105	805	1,105	11,065	18,928
Total Financing	46,415	43,152	69,073	82,174	82,468	78,719	47,764	69,415	245,720	273,460

Source: Cabinet meeting 6 September 2022, Agenda Item 13

The net adjustment to the 2022/23 Capital Programme spend was a downward revision of £3.26m, from £46.41m to £43.15m. This adjustment reflected the carry forward of budgets from 2021/22, new projects and rephasing of spend. A summary of the updated programme is provided below.

6.2 Housing Revenue Account (HRA)

Whilst the HRA Capital Programme is subject to the current pressures as the General Fund, the HRA consists of three main allocations - Repairs, Project Development and the New Build Programme, the management and precise allocation of which is delegated to the Cabinet and officers. As mentioned in the Capital Programme Review report, there is unlikely to be any need for the approval of additional funding at this point in the year. The HRA Capital Programme will be revised as part of the normal budget process later in the year.

Approved Budget Changes as at Quarter 2

The below are Approved Budget changes to the General Fund Revenue Budget, approved by Cabinet and Council since the Original Budget for 2022/23 was approved in February 2022.

Service Areas	Approved Budget Changes 2022/23 £'000	Comments
Senior and Corporate Management	38	Additional corporate capacity
Economic Development and Regeneration	260	Freeport East and First Light Festival funding*
Digital and Programme Management	66	Data analytical capacity
Operations	21	£21k Highways Technician/Engineer
Environmental Services and Port Health	153	Environmental Services – Resourcing & restructure
Housing Services	94	Tackling Long Term empties*
Net Movements on Reserves	(354)	Approved use of reserves
Total of Approved Budget Changes as at Quarter 2	278	

*Funded from Reserves

Variances to the Approved Budget as at Quarter 2

The below changes are variances to the Approved Budget that have been identified since the Original Budget for 2022/23 was approved in February 2022.

Service Areas	Variances to Approved Budget Changes 2022/23 £'000	Comments
Revenue and Benefits	(20)	Other service admin grants slightly more than budgeted
Operations	627	£500k impact of National Employers current pay offer for 22/23. £200k reduction in baseline car parking income, additional £50k from Beach Hut income, increase baseline income by £23k for yacht stations.
Cross Cutting Items:		
22/23 Pay Awards (ESC Employees)	1,000	Forecast based on current National Employers pay offer for 22/23 (net of potential saving on vacancies).
Inflation - Utilities	250	Estimate. The impact on Leisure Centres is not yet known.
Non-Cost of Service Expenditure Adjustments (e.g. DRF,MRP)	(58)	Revised to MRP based on capital programme outturn in 2021/22.
Financing and Investment Net Income and Expenditure	(500)	Anticipate additional interest income from Treasury Management activities due to increased interest rates.
Business Rates	(280)	Pooling Benefit increase by £200k and £80k additional Business Rates income from Renewables.
Total of Variances to the Approved Budget as at Quarter 2	1,019	

Budget Monitoring Report

For the period 1 April 2022 - 30 September 2022

1. Update

1.1 Mini-Budget / Growth Plan 2022 and Other Government Policy Announcements

As referred to in the Quarter 2 Budget Monitoring Report, on 23 September 2022, the Chancellor of the Exchequer announced a Mini-Budget / Growth Plan 2022. A link to the Growth Plan document is here: [BUDGET 2021: Protecting the jobs and livelihoods of the British people \(publishing.service.gov.uk\)](https://www.publishing.service.gov.uk/government/uploads/system/uploads/attachment_data/file/1031111/budget-2022-protecting-the-jobs-and-livelihoods-of-the-british-people.pdf) There have also been a number of other policy announcements that are significant in respect of the Council's financial position, including the Energy Bill Relief Scheme and Investment Zones. The purpose of this additional appendix is to provide a brief update on the announcements and measures that are directly relevant to the Council, not on all of the measures announced. No measures were announced in respect of the Council's key income streams of council tax or business rates, nor on any specific details regarding Government departmental spending, of which local government finance forms a part. However, the Chancellor has indicated that the Government "is sticking to spending settlements for this spending review period". The Office for Budget Responsibility (OBR) will publish a full forecast alongside the publication of a Medium Term Fiscal Plan on 23 November 2022 regarding the Government's fiscal policy, prior to the Provisional Local Government Finance Settlement in December.

1.2 Reversal of National Insurance increase

The Government has reversed the 1.25% increase to employer and employee National Insurance Contributions (NICs) with effect from 6 November 2022 in this financial year and has dropped the Health and Social Care Levy that was due to succeed it in 2023/24. The income intended for the NHS and social care system from this will come from general taxation instead.

The reduction in employer NICs will produce savings against budget for the Council in both the current and subsequent years. The estimated budget savings attributable to this are summarised in the table below. However, it should be noted that local government received funding of around £400m for the employer NICs increase through the 2022/23 local government finance settlement, although the actual amounts for local government, either collectively or individually, were never explicitly confirmed. If this funding is going to be removed from local government then the Settlement Funding Assessment (SFA) or Services Grant would have to be reduced in 2023/24 and 2024/25. If this is the case, there may consequently be some offset to these forecast savings, although it will probably be difficult to ascertain the quantum of this from the settlement.

Employers NIC Savings

East Suffolk Council	2022/23 Part Year (Nov 2022-Mar 2023) £'000	Per annum 2023/24 onwards £'000
<i>General Fund</i>	80	200
<i>Port Health</i>	15	30
<i>Housing Revenue Account</i>	20	45
Total	115	275

1.3 Investment Zones

New Investment Zones have been announced which will “provide time-limited tax reliefs, and planning liberalisation to support employment, investment and home ownership”. DLUHC is already in discussion with 38 mayoral combined authorities and other upper-tier authorities to host the new investment zones. Features of the investment zones will include:

- 100% relief from business rates in newly-occupied premises
- Local authorities will retain 100% of business rates above an agreed baseline, with no baseline reset for 25 years
- Enhanced capital allowances (100% relief in first year of qualifying expenditure in plant and machinery), and enhanced structures and buildings allowance
- Employer National Insurance relief (on earnings up to £50,270)

In the media release regarding Investment Zones, the following excerpt of the Notes to Editors is worth noting: *“Freeports will continue to support growth and investment in left-behind port regions of the UK. The Government will consider the potential for Freeports to convert to Investment Zones on a case-by-case basis, as part of the wider process for identifying new zones in the future.”*

1.4 Energy Bill Relief Scheme

On 21 September 2022 the Government outlined plans to help cut energy bills for businesses and published guidance. The Energy Bill Relief Scheme will see energy prices for non-domestic energy customers such as businesses, charities and public sector organisations – including councils and schools – reduced from what they would have been without government intervention. Suppliers will apply reductions to the bills of all eligible non-domestic customers. It will apply to fixed contracts agreed on or after 1 April 2022, as well as to deemed, variable and flexible tariffs and contracts. The scheme will apply to energy usage from 1 October 2022 to 31 March 2023, running for an initial six-month period for all non-domestic energy users. The financial implications of this measure to the Council are being worked on and will be reflected in the Medium Term Financial Strategy, 2023/24 Budget, and further Budget Monitoring reports.

1.5 Infrastructure Projects

The Growth Plan also sets out in Annex B the infrastructure projects that the government will prioritise for acceleration, across transport, energy and digital infrastructure, including a number that are relevant to East Suffolk.

1.6 Off Payroll Working – IR35

Finally, although they do not have any direct financial effect on the Council, the Government has announced the repeal of the 2017 and 2021 reforms to off payroll working rules known as IR 35, with effect from 6 April 2023. This affects workers who provide their services via an intermediary such as a personal service company. Local authority organisations are seeking confirmation that this also affects such arrangements with councils.